

CITY OF ONTARIO PLANNING COMMISSION/ HISTORIC PRESERVATION MEETING AGENDA

July 26, 2022

Ontario City Hall
303 East "B" Street, Ontario, California 91764

6:30 PM

WELCOME to a meeting of the Ontario Planning/Historic Preservation Commission.

All documents for public review are on file in the Planning Department located at 303 E. B Street, Ontario, CA 91764 and on the City website at www.ontarioca.gov/Agendas/PlanningCommission.

- *Anyone wishing to speak during public comment or on a particular item should fill out a green slip and submit it to the Secretary.*
- *Comments will be limited to 5 minutes. Speakers will be alerted when their time is up. Speakers are then to return to their seats and no further comments will be permitted.*
- *In accordance with State Law, remarks during public comment are to be limited to subjects within the Commission's jurisdiction. Remarks on other agenda items will be limited to those items.*
- *Remarks from those seated or standing in the back of the chambers will not be permitted. All those wishing to speak including Commissioners and Staff need to be recognized by the Chair before speaking.*
- *The City of Ontario will gladly accommodate disabled persons wishing to communicate at a public meeting. Should you need any type of special equipment or assistance in order to communicate at a public meeting, please inform the Planning Department at (909) 395-2036, a minimum of 72 hours prior to the scheduled meeting.*
- *Please turn off all communication devices (phones and beepers) or put them on non-audible mode (vibrate) so as not to cause a disruption in the Commission proceedings.*

ROLL CALL

Anderson __ Dean __ DeDiemar __ Gage __ Lampkin __ Ricci __ Willoughby __

PLEDGE OF ALLEGIANCE TO THE FLAG

ANNOUNCEMENTS

- 1) Agenda Items
- 2) Commissioner Items

PUBLIC COMMENTS

Citizens wishing to address the Planning/Historic Preservation Commission on any matter that is not on the agenda may do so at this time. Please state your name and address clearly for the record and limit your remarks to five minutes.

Please note that while the Planning/Historic Preservation Commission values your comments, the Commission cannot respond nor take action until such time as the matter may appear on the forthcoming agenda.

CONSENT CALENDAR ITEMS

All matters listed under CONSENT CALENDAR will be enacted by one summary motion in the order listed below. There will be no separate discussion on these items prior to the time the Commission votes on them, unless a member of the Commission or public requests a specific item be removed from the Consent Calendar for a separate vote. In that case, the balance of the items on the Consent Calendar will be voted on in summary motion and then those items removed for separate vote will be heard.

A-01. MINUTES APPROVAL

Planning/Historic Preservation Commission Minutes of June 28, 2022, approved as written.

A-02. ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE

NO. PDEV19-047: A hearing to consider a Development Plan to construct 10 detached single-family dwellings on 10 vacant parcels totaling approximately 1.83 acres of land generally located at the northwest corner of Olive Street and Orange Avenue, within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) zoning district. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-fill Development Projects) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 0108-481-18, 0108-481-19, 0108-481-20, 0108-481-21, 0108-481-22, 0108-481-23, 0108-481-24, 0108-481-25, 0108-481-26, and 0108-481-27) **submitted by Texton Construction Co., Inc.**

A-03. ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE

NO. PDEV21-020: A hearing to consider a Development Plan to construct a 45,000-square-foot industrial building on 2.02 acres of land located at 1044 and 1050 East Holt Boulevard, within the IP (Industrial Park) zoning district. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APNs: 1049-131-15 and 1049-131-016) **submitted by HK Ventures, Inc.**

PUBLIC HEARING ITEMS

For each of the items listed under PUBLIC HEARING ITEMS, the public will be provided an opportunity to speak. After a staff report is provided, the chairperson will open the public hearing. At

that time the applicant will be allowed five (5) minutes to make a presentation on the case. Members of the public will then be allowed five (5) minutes each to speak, unless there are a number of person's wishing to speak and then the Chairperson will allow only three (3) minutes, to accommodate for more persons. The Planning/Historic Preservation Commission may ask the speakers questions relative to the case and the testimony provided. The question period will not count against your time limit. After all persons have spoken, the applicant will be allowed three minutes to summarize or rebut any public testimony. The chairperson will then close the public hearing portion of the hearing and deliberate the matter.

HISTORIC PRESERVATION / PLANNING COMMISSION ITEMS

B. ENVIRONMENTAL ASSESSMENT, DEVELOPMENT PLAN AND VARIANCE REVIEW FOR FILE NOS. PDEV21-033 AND PVAR21-006:

A public hearing to consider a Development Plan (File No. PDEV21-033) to construct one 2,800-square-foot drive-thru restaurant with a 500-square-foot outdoor patio, in conjunction with a Variance (File No. PVAR21-006) to reduce the front drive aisle setback along an arterial street from 20 feet to 13 feet, on 0.42-acre of land located at 1610 East Fourth Street, within the CC (Community Commercial) zoning district. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) and Section 15305 (Class 5, Minor Alterations in Land Use Limitations) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APNs: 0110-181-11 and 0110-181-19) **submitted by Hannibal Petrossi.**

1. CEQA Determination

No action necessary – Exempt: CEQA Guidelines Sections § 15332 and 15305

2. File No. PVAR21-006 (Variance)

Motion to Approve / Deny

3. File No. PDEV21-033 (Development Plan)

Motion to Approve / Deny

C. ENVIRONMENTAL ASSESSMENT, PLANNED UNIT DEVELOPMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NOS. PUD20-001 & PDEV20-009:

A hearing to consider a Planned Unit Development (File No. PUD20-001) to establish development standards, design guidelines and infrastructure requirement for 0.81-acres of land, in conjunction with a Development Plan (File No. PDEV20-009) to construct a mixed-use project consisting of 59 multiple-family dwellings, 2,500 square feet of retail space, and two levels of podium parking, on property located at 549 West Holt Boulevard, within the MU-1 (Downtown Mixed-Use) zoning district. The project is categorically exempt from the requirements of the California Environmental Quality Act (“CEQA”) pursuant to Section 15332 (Class 32, In-Fill Development) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APN: 1049-021-09) **submitted by Kathy Huynh. City Council action is required on Planned Unit Development**

1. CEQA Determination

No action necessary – Exempt: CEQA Guidelines Section § 15332

2. File No. PUD20-001 (Planned Unit Development)

Motion to recommend Approval / Denial

3. File No. PDEV20-009 (Development Plan)

Motion to Approve / Deny

D. ENVIRONMENTAL ASSESSMENT AND SPECIFIC PLAN AMENDMENT REVIEW

FOR FILE NO. PSPA22-003: A public hearing to consider an amendment the Ontario Gateway Specific Plan, modifying Table 2.B (Permitted Land Uses by Planning areas) to allow "Auto Sales and Services" as a permitted land use in the Office Planning Area 1 land use district. The project is exempt from the requirements of the California Environmental Quality Act ("CEQA") pursuant to Section 15061(b)(3) and the guidelines promulgated thereunder, pursuant to Section 15061(b)(3) of the CEQA Guidelines, in that the activity is covered by the common sense exemption (general rule) that CEQA applies only to projects that have the potential for causing a significant effect on the environment. Where it can be seen with certainty that there is no possibility that the activity in question may have a significant effect on the environment, the activity is not subject to CEQA. The property affected by this Specific Plan Amendment is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APN: 0210-212-60) **submitted by the City of Ontario. City Council action is required.**

1. CEQA Determination

No action necessary – Exempt: CEQA Guidelines Section § 15061(b)(3)

2. File No. PSPA22-003 (Specific Plan Amendment)

Motion to recommend Approval/Denial

E. ENVIRONMENTAL ASSESSMENT, GENERAL PLAN AMENDMENT, AND COMMUNITY CLIMATE ACTION PLAN UPDATE FOR FILE NOS. PGPA20-002 AND PADV22-002:

A public hearing to consider certification of a Supplemental Environmental Impact Report (SEIR) (State Clearinghouse No. 2021070364), including the adoption of a Mitigation Monitoring and Reporting Program and a Statement of Overriding Considerations, in conjunction with the following: [1] a General Plan Amendment (File No. PGPA20-002) for The Ontario Plan (TOP) update, including its component parts: Vision, Governance Manual, and Policy Plan; and [2] an update to the Community Climate Action Plan (File No. PADV22-002). The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ONT ALUCP). The project site is also located within the Airport Influence area of Chino Airport and is consistent with policies and criteria set forth within the 2011 California Airport Land Use Planning Handbook published by the California Department of Transportation, Division of Aeronautics; **City initiated. City Council action is required.**

1. CEQA Determination

Motion to recommend Approval/Denial of the Certification of a Supplemental EIR

2. File No. PGPA20-002 (General Plan Amendment)

Motion to recommend Approval/Denial

3. File No. PADV22-002 (Community Climate Action Plan)

Motion to recommend Approval/Denial

MATTERS FROM THE PLANNING/HISTORIC PRESERVATION COMMISSION

1) Old Business

- Reports From Subcommittees

- Historic Preservation (Standing): Met on July 14, 2022.

2) New Business

3) Nominations for Special Recognition

DIRECTOR'S REPORT

1) Monthly Activity Report


If you wish to appeal any decision of the Planning/Historic Preservation Commission, you must do so within ten (10) days of the Commission action. Please contact the Planning Department for information regarding the appeal process.

If you challenge any action of the Planning/Historic Preservation Commission in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the Planning/Historic Preservation Commission at, or prior to, the public hearing.



I, Gwen Berendsen, Administrative Assistant, of the City of Ontario, or my designee, hereby certify that a true, accurate copy of the foregoing agenda was posted on **Friday, July 22, 2022**, at least 72 hours prior to the meeting per Government Code Section 54954.2 at 303 East "B" Street, Ontario.


Gwen Berendsen, Secretary Pro Tempore


Rudy Zeledon, Planning Director
Planning/Historic Preservation
Commission Secretary

**CITY OF ONTARIO PLANNING COMMISSION/
HISTORIC PRESERVATION MEETING**

MINUTES

June 28, 2022

CONTENTS	PAGE
PLEDGE OF ALLEGIANCE.....	2
ANNOUNCEMENTS.....	2
PUBLIC COMMENTS.....	2
CONSENT CALENDAR	
A-01. Minutes of May 24, 2022	2
A-02. File No. PDEV21-019	2
A-03. File No. PDEV21-027	3
PUBLIC HEARINGS	
B. File No. PDA22-003 – A,B,C and D	3
C. File No. PDEV22-014	4
D. File No. PDCA22-004	6
MATTERS FROM THE PLANNING COMMISSION.....	7
DIRECTOR’S REPORT.....	7
ADJOURNMENT	7

**CITY OF ONTARIO PLANNING COMMISSION/
HISTORIC PRESERVATION MEETING**

MINUTES

June 28, 2022

REGULAR MEETING: City Hall, 303 East B Street
Called to order by Chairman Willoughby at 6:30 PM

COMMISSIONERS

Present: Chairman Willoughby, Vice-Chairman DeDiemar, Anderson, Gage, Lampkin, and Ricci

Absent: Dean

OTHERS PRESENT: Planning Director Zeledon, City Attorney Guiboa, Principal Planner Ruddins, Senior Planner Hutter, Senior Planner Mejia, Associate Planner Aguilo, Assistant City Engineer Lee, and Planning Secretary Berendsen

PLEDGE OF ALLEGIANCE TO THE FLAG

The Pledge of Allegiance was led by Commissioner Lampkin.

ANNOUNCEMENTS

Mr. Zeledon stated that Item B & C will be taken together and before the Commissioners is a revised resolution for Item C.

Mr. Lampkin gave feedback on the California Preservation Foundation Conference several Commissioners attended.

PUBLIC COMMENTS

No one responded from the audience.

CONSENT CALENDAR ITEMS

A-01. MINUTES APPROVAL

Planning/Historic Preservation Commission Minutes of May 24, 2022, approved as written.

A-02. ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE

NO. PDEV21-019: A public hearing to consider a Development Plan to construct 73 single-family cluster homes (Lennar Homes) on 5.99 acres of land bounded by Merrill Avenue to the north, Southern California Edison easement to the west, and the San Bernardino County Flood Control District and the City of Eastvale to the south, within the PA 27 of the Subarea 29 Specific Plan. The environmental impacts of this project were previously reviewed in conjunction with File Nos. PSPA20-006 and PMTT20-012, for which an Addendum to the Subarea 29 Specific Plan Environmental Impact Report (State Clearinghouse No. 2004011009) was approved by the City Council on June 15, 2021. This application introduces no new significant environmental

impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP). The project site is also located within the Airport Influence area of Chino Airport and is consistent with policies and criteria set forth within the 2011 California Airport Land Use Planning Handbook published by the California Department of Transportation, Division of Aeronautics; (APN: 0218-331-42) **submitted by Lennar Homes of California, Inc.**

- A-03. ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV21-027:** A public hearing to consider Development Plan approval to construct 235 single-family dwellings on approximately 31.5 acres of land generally located on the south side of Chino Avenue, approximately 645 feet west of Archibald Avenue, within Planning Area 2 (Neighborhoods 5 & 6) of the Countryside Specific Plan. The environmental impacts of this project were previously reviewed in conjunction with the Countryside Specific Plan (File No. PSP04-001), for which an Environmental Impact Report (State Clearinghouse No. 2004071001) was certified by the City Council on April 18, 2006. This application introduces no new significant environmental impacts and all previously adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APNs: 0218-131-12, 0219-131-22, 0218-131-11, 0218-131-40, and 0218-131-43) **submitted by Lennar Homes of California, Inc.**

PLANNING COMMISSION ACTION

It was moved by DeDiemar, seconded by Ricci, to approve the Consent Calendar, including the May 24, 2022 minutes as written. It was approved (5-0), with Gage recusing himself as he was not at this meeting.

It was moved by Ricci, seconded by Anderson, to approve the remainder of the Consent Calendar, including Development Plan, File No. PDEV21-019, and Development Plan, File No. PDEV21-027, subject to conditions of approval. It was approved unanimously by those present (6-0).

PUBLIC HEARING ITEMS

- B. ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT AGREEMENT REVIEW FOR FILE NO. PDA22-003- A,B,C AND D:** A public hearing to consider a Development Agreement between the City of Ontario and OTC Owner, LLC, to establish the terms and conditions for 10.49 acres of and to develop a future mixed use development consisting of approximately 694 residential units and up to 71,200 square feet of commercial uses, on three parcels of land located at the southwest corner of Via Villaggio and Via Piemonte, southeast corner of Via Villaggio and Via Piemonte, and on the southwest corner of Ontario Center Parkway and Concours Street within the proposed mixed-use Subareas 8, 11, 16 and 17 of the Piemonte Overlay of the Ontario Center Specific. The environmental impacts of this project were previously reviewed in conjunction with an Amendment to the Piemonte Overlay of the Ontario Center Specific Plan, for which an Addendum to the Ontario Center Specific Plan Environmental Impact Report (State Clearinghouse No. 198941009) was adopted by the City Council on April 19, 2022. This application introduces no new significant environmental impacts, and all previously-adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP);(APNs: 0210-204-38, 0218-204-39 and a Portion of 0210-205-01); **submitted by OTC Owner, LLC. City Council action is required.**

- C. **ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV22-014**: A hearing to consider a Development Plan to construct four mixed-use buildings totaling 63,665 commercial square feet and 694 dwelling units (540,373 residential square feet) on 13.3 acres of land located at 4000 East Ontario Center Parkway and the southeast and southwest corner of Via Piemonte and Via Villagio, within the Mixed-use land use district of the Piemonte Overlay of the Ontario Center Specific Plan. The environmental impacts of this project were previously reviewed in conjunction with an Amendment to the Piemonte Overlay of the Ontario Center Specific Plan, for which an Addendum to the Ontario Center Specific Plan Environmental Impact Report (State Clearinghouse No. 198941009) was adopted by the City Council on April 19, 2022. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APNs: 0210-205-01, 0210-204-38, and 0210-204-39) **submitted by Adept Development.**

Senior Planner Hutter, presented the staff report. She stated that staff is recommending the Planning Commission recommend approval to City Council for File No. PDA22-003 -A,B,C & D, and to approve File No. PDEV22-014, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval.

Mr. Gage wanted clarification on the parking.

Ms. Hutter responded.

Mr. Lampkin wanted to know phasing for the project.

Mr. Zeledon responded.

Mr. Lampkin wanted to make sure Area A is closest to the

Mr. Zeledon stated that is correct.

Mr. Lampkin wanted to know the operational percentage of the Arena.

Mr. Zeledon responded and then explained about the parking and traffic plans that will be required.

Mr. Lampkin commented on a parking management plan and wanted to know about a security plan.

Mr. Zeledon stated there will be a police substation and expanded on the security plan.

Mr. Lampkin wanted to know how many levels of underground parking.

Mr. Zeledon stated one.

Mr. Lampkin wanted to know if public transit stops will be incorporated.

Mr. Zeledon responded.

Mr. Lampkin wanted to know about rideshare vehicle areas.

Mr. Zeledon responded.

Mr. Ricci wanted to know about delivery service parking areas.

Mr. Zeledon responded.

Mr. Ricci wanted to know about demographics for the project.

Ms. Hutter responded.

Mr. Zeledon expanded on the demographics.

Mr. Lampkin wanted to know about the signage plan and lighting plan.

Mr. Zeledon responded.

Ms. Anderson wanted clarification regarding the commercial and residential parking.

Mr. Zeledon responded.

PUBLIC TESTIMONY

Mr. Robert Montano, representing Adept Development, appeared and spoke in favor of the project.

Mr. Lampkin wanted to know about the landscape plan.

Mr. Montano responded.

Mr. Lampkin asked about reclaimed water.

Mr. Montano responded.

Mr. Lampkin wanted to know about broadcasted ticketed events.

Mr. Montano responded.

Mr. Gage wanted to know about management.

Mr. Montano responded.

Mr. Gage asked about commercial vendors and managing parking.

Mr. Montano responded.

As there was no one else wishing to speak, Chairman Willoughby closed the public testimony

Mr. Lampkin spoke in favor of the project.

Chairman Willoughby reopened the public hearing

Mr. Willoughby asked the applicant regarding the Conditions of Approval.

Mr. Montano agreed with all the Conditions of Approval.

Chairman Willoughby closed the public testimony.

PLANNING COMMISSION ACTION

It was moved by Lampkin, seconded by Ricci, to recommend adoption of a resolution to

approve the Development Agreement, File No., PDA22-003– A,B,C and D, subject to conditions of approval. Roll call vote: AYES, Anderson, DeDiemar, Gage, Lampkin, Ricci, and Willoughby; NOES, none; RECUSE, none; ABSENT, Dean. The motion was carried 6 to 0.

Mr. Gage spoke in favor of the project.

It was moved by Gage, seconded by Lampkin, to adopt a resolution to approve the Development Plan, File No., PDEV22-014, subject to conditions of approval. Roll call vote: AYES, Anderson, DeDiemar, Gage, Lampkin, Ricci, and Willoughby; NOES, none; RECUSE, none; ABSENT, Dean. The motion was carried 6 to 0.

D. ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT CODE AMENDMENT

REVIEW FOR FILE NO. PDCA22-004: A public hearing to consider certain revisions to the City of Ontario Development Code, establishing the Chino Airport Overlay zoning district and Reference L, Chino Airport Land Use Compatibility Plan. The proposed Development Code Amendment is exempt from the requirements of the California Environmental Quality Act (“CEQA”) and the guidelines promulgated thereunder, pursuant to Section 15061(b)(3) of the CEQA Guidelines. The proposed Development Code Amendment affects properties located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; **City Initiated. City Council action is required.**

Senior Planner Mejia, presented the staff report. She stated that staff is recommending the Planning Commission recommend approval to City Council for File No. PDCA22-004, pursuant to the facts and reasons contained in the staff report and attached resolution.

Mr. Ricci wanted to know the restrictions for 60db.

Ms. Mejia responded.

Mr. Lampkin wanted to clarify the policies regarding State law.

Ms. Mejia responded.

Mr. Lampkin wanted to know about real estate transaction disclosure areas.

Ms. Mejia responded.

Mr. Lampkin wanted to know how long real estate transactions have been in place.

Ms. Mejia responded.

Mr. Zeledon clarified real estate transactions.

Mr. Lampkin commented regarding Ontario being proactive.

PUBLIC TESTIMONY

As there was no one else wishing to speak, Chairman Willoughby closed the public testimony

Mr. Gage thanked Ms. Mejia for her presentation and spoke in favor of the project.

PLANNING COMMISSION ACTION

It was moved by Gage, seconded by DeDietmar, to recommend adoption of a resolution to approve the Development Code Amendment, File No., PDCA22-004. Roll call vote: AYES, Anderson, DeDietmar, Gage, Lampkin, Ricci, and Willoughby; NOES, none; RECUSE, none; ABSENT, Dean. The motion was carried 6 to 0.

MATTERS FROM THE PLANNING COMMISSION

Old Business Reports From Subcommittees

Historic Preservation (Standing): This subcommittee met on June 15, 2022.

Mr. Gage recapped the meeting.

Development Code Review (Ad-hoc): This subcommittee did not meet.

Zoning General Plan Consistency (Ad-hoc): This subcommittee did not meet.

New Business

NOMINATIONS FOR SPECIAL RECOGNITION

None at this time.

DIRECTOR'S REPORT

Mr. Zeledon stated Monthly Activity reports are before them.

ADJOURNMENT

Ricci motioned to adjourn; it was unanimously approved. The meeting was adjourned at 8:22 PM to the next regular meeting on July 26, 2022.

Secretary Pro Tempore

Chairman, Planning Commission

FILE NO: PDEV19-047

SUBJECT: A hearing to consider a Development Plan to construct 10 detached single-family dwellings on 10 vacant parcels totaling 1.83 acres of land generally located at the northwest corner of Olive Street and Orange Avenue, within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) zoning district; (APNs: 0108-481-18, 0108-481-19, 0108-481-20, 0108-481-21, 0108-481-22, 0108-481-23, 0108-481-24, 0108-481-25, 0108-481-26, 0108-481-27) **submitted by Texton Construction Co.**

PROPERTY OWNER: CFC-Olive L.P.

RECOMMENDED ACTION: That the Planning Commission consider and approve File No. PDEV19-047, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval contained in Attachment A to the resolution.

PROJECT SETTING: The project site is comprised of 1.83 acres of land generally located at the northwest corner of Olive Street and Orange Avenue, within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) zoning district, and is depicted in Figure 1: Project Location, below. The site is located in a well-established, traditional single-family residential neighborhood with improved streets, parkways, curb, and gutter. Residences adjacent to the project site are single-story with front loaded or side-on garages, two-story homes are also present in the surrounding area. San Antonio Christian School is north of the project site. The existing surrounding land uses, zoning, and general plan and specific plan land use designations are summarized in the “Surrounding Zoning & Land Uses” table located in the Technical Appendix of this report.

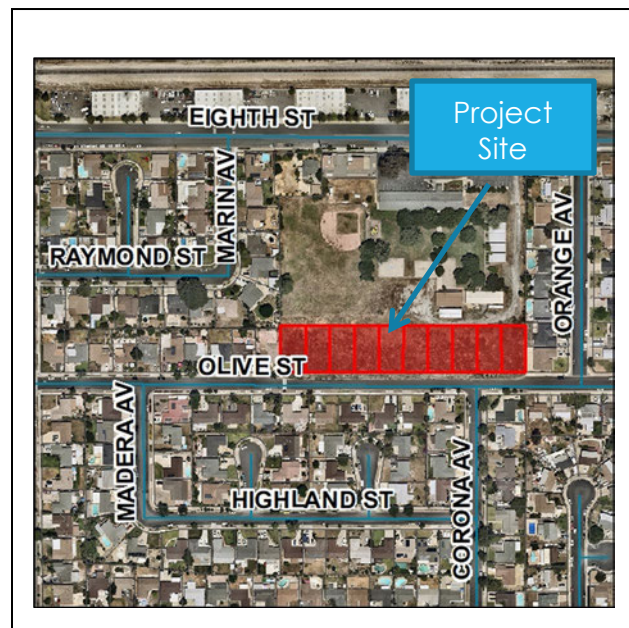



Figure 1: Project Location

Case Planner:	Elly Antuna, Associate Planner
Planning Director Approval:	
Submittal Date:	08/06/19

Hearing Body	Date	Decision	Action
DAB	07/18/22	Approval	Recommend
PC	07/26/22		Final
CC			

PROJECT ANALYSIS:

(1) Background — In 2003, a Tentative Tract Map (File No. PMTT03-014), submitted by San Antonio Jr. Academy, was approved to subdivide 8.5 acres of land located at 1722 East Eighth Street, into 12 lots. Ten of the lots that front Olive Street were intended to be developed with single-family dwellings, one parcel was proposed for a future well site, and was developed with a City well site in 2007. The final parcel consisted of the original school campus that was retained by San Antonio Jr. Academy. In 2005, the academy changed their name to San Antonio Christian School and remains in operation.

On August 6, 2019, a Development Plan (File No. PDEV19-047) application was submitted to construct 10 single-family dwelling units on the 10 lots fronting Olive Street. On January 29, 2020, a neighborhood meeting was held to receive comments regarding the project from surrounding property owners. Ninety notices were sent out, no calls were received by staff leading up to the meeting, 6 neighboring property owners and 2 applicant representatives attended the meeting. Overall, the residents were in support of the project. Subsequently, the property was sold, and the project was slightly modified. Revisions include new architectural styles and the reduction of building square footages by approximately 500-700 square feet to accommodate the future construction of Accessory Dwelling Units (ADU) at the rear of the lots. Review and approval of the proposed ADUs will be processed administratively as required by State law.

(2) Site Design/Building Layout — The Project proposes construction of 10 traditional single-family homes with 3 different floor plans (Exhibit A—Site Plan). Each lot is approximately 0.18-acre (7,840 SF) in area and measures 64 feet wide and 123 feet deep. Detached homes will be oriented towards Olive Street, with recessed front entries facing the street. Front yards are minimum 20 feet in depth, as measured from the Olive Street property line. Side yards will range from 5 feet to approximately 19 feet in width, and rear yards are approximately 50 feet in depth. Lots 1, 5, and 10 will have attached side-on garages at the rear of the lots. Lots 2, 4, 6 and 8 will provide attached side-on garages at the front of the lots. Lots 3, 7, and 9 will provide front facing two-car garages. The location of attached 2-car garages vary with each floor plan and are front-loaded or side-on at the front or rear of the home. Lots 1, 5, and 10 will be developed with single-story designs in order to achieve compatibility with the existing neighborhood.

(3) Site Access/Circulation — As a condition of project approval, Olive Street will be widened by an additional 8 feet, to the ultimate right-of-way of 60 feet. Required public right-of-way improvements include 8-inch curb and gutter, a 5-foot wide sidewalk, a 5-foot wide landscaped parkway, and 16-foot wide drive aprons will provide vehicular access to each property.

(4) Parking — The Project provides an enclosed 2-car garage for each dwelling, meeting the minimum Development Code requirement. Driveways can accommodate at least 2 additional off-street parking spaces.

(5) Architecture — Architectural styles proposed include Spanish, Bungalow, Cottage, Ranch, Farmhouse, and Craftsman. The transitional architectural styles combine elements of both traditional and modern architectural styles. All plans incorporate various design features such as horizontal and vertical building articulation, varied entry designs, and covered entryways. Each architectural style will include the following (see Exhibit B— Floor Plans and Elevations, attached):

- Spanish: Varying gable and shed roofs with “S” concrete roof tiles, stucco siding, decorative shaped entryways, decorative details at the gable ends, recessed windows, and decorative window/door framing.
- Craftsman: Varying gable and shed roofs with flat shake concrete tiles, a mix of stucco, horizontal lap, and brick siding, decorative wood posts, gable vents, and shutters.
- Cottage: Varying gable and hipped roofs with flat shake concrete roof tiles, stucco and stone siding, arched windows and entryways, shutters, and decorative window/door framing.
- Ranch: Cross-gable roofs with flat shake concrete tiles, a mix of stucco, horizontal lap and stone siding, decorative beams at the gable ends, and shutters.
- Farmhouse: Cross-gable roofs and shed dormers with flat shake concrete roof tiles, with a mix of stucco, horizontal lap and board and batten siding, and decorative beams and vents at the gable ends.
- Bungalow: Cross-gable roofs and shed dormers with flat shake concrete roof tiles, with a mix of stucco, horizontal lap and brick siding, decorative vents at the gable ends, shutters, and decorative window/door framing.

The Project proposes 3 floor plans. Plan 1 is a 1,928-square-foot, single-story design, with a side on garage attached to the rear of the residence and has 4 bedrooms and 3 bathrooms. Plan 2 is a 1,933-square-foot, 2-story design, with a side on garage attached to the front of the residence and has 3 bedrooms and 3 bathrooms. Plan 3 is a 2,119-square-foot, 2-story design, with an attached front-facing garage and has 4 bedrooms and 3 bathrooms.

Dwelling Unit Summary					
Plan Type No.	No. of Bedrooms/ Bathrooms	Total Living SF	Garage SF	No. of Stories	No. of Units
1	4 Bedroom/ 3 Bathroom	1,928 SF	Attached (Rear) Side On (2-car) 427 SF	1 Story	3
2	3 Bedroom/ 3 Bathroom	1,933 SF	Attached (Front) Side On (2-car) 427 SF	2 Story	4
3	4 Bedroom/ 3 Bathroom	2,119 SF	Attached (Front) (2-car) 415 SF	2 Story	3
TOTAL					10

(6) Landscaping — The front yard areas will be landscaped and irrigated. Each home will have a concrete driveway and a pedestrian pathway to the front entry of home. Landscape coverages range between 63 and 75 percent. Parkways will be planted with 24-inch box London Plane trees and groundcover. Front yards will be planted with 24-inch box Chitalpa trees and 15-gallon Western redbud trees. A variety of low or moderate water usage shrubs and groundcovers will complete each yard. The proposed on-site and public right-of-way improvements will contribute to a walkable, safe area for pedestrians along Olive Street.

(7) Utilities (drainage, sewer) — Public utilities (water and sewer) are available to serve the Project. Furthermore, the Applicant has submitted a Preliminary Water Quality Management Plan ("PWQMP"), which establishes the Project's compliance with storm quality discharge/water quality requirements. The PWQMP includes site design measure that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development ("LID") best management practices ("BMPs"), such as retention and infiltration. The proposed development will not substantially alter the existing drainage pattern.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

(1) City Council Goals.

- Operate in a Businesslike Manner
- Focus Resources in Ontario's Commercial and Residential Neighborhoods
- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)

(2) Vision.

Distinctive Development:

- Commercial and Residential Development
 - Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

(3) Governance.

Decision Making:

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.

➤ G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

(4) Policy Plan (General Plan)

Land Use Element:

▪ Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.

➤ LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.

➤ LU1-6 Complete Community: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).

▪ Goal LU2: Compatibility between a wide range of uses.

➤ LU2-6: Infrastructure Compatibility: We require infrastructure to be aesthetically pleasing and in context with the community character.

Housing Element:

▪ Goal H2: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.

➤ H2-5 Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices and other best practices.

▪ Goal H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.

Community Economics Element:

▪ Goal CE1: A complete community that provides for all incomes and stages of life.

➤ CE1-6 Diversity of Housing. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.

▪ Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.

➤ CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.

➤ CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.

➤ CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.

➤ CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Safety Element:

▪ Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.

➤ S1-1 Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

Community Design Element:

▪ Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

➤ CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.

➤ CD1-3 Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.

▪ Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.

➤ CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:

- Building volume, massing, and height to provide appropriate scale and proportion;
- A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

➤ CD2-2 Neighborhood Design. We create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:

- A pattern of smaller, walkable blocks that promote access, activity and safety;
- Variable setbacks and parcel sizes to accommodate a diversity of housing types;
- Traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows;
- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the “outdoor living room”), as appropriate; and
- Landscaped parkways, with sidewalks separated from the curb.

➤ CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.

➤ CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

➤ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➤ CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

▪ Goal CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

➤ CD3-1 Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.

➤ CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.

➤ CD3-3 Building Entrances. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces.

➤ CD3-5 Paving. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.

➤ CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.

▪ Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.

➤ CD5-1 Maintenance of Buildings and Property. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.

HOUSING ELEMENT COMPLIANCE: The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land

Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

ENVIRONMENTAL REVIEW: The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines, and meets each of the following conditions:

- (1) The Project is consistent with the applicable general plan designation and all applicable general plan policies as well as with applicable zoning designation and regulations. The proposed Project is located within the LDR (Low Density Residential) land use district of the Policy Plan Land Use Map, and the CC (Community Commercial) zoning district. The proposed Project is consistent with all applicable general plan policies, as well as with the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) zoning designation and applicable Development Code regulations.
- (2) The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses. The Project is proposed within the established boundaries of the City of Ontario, on a project site totaling 1.83 acre of land, which is surrounded by established residential land uses.
- (3) The Project site has no value as habitat for endangered, rare, or threatened species. The site was previously graded and is located on an existing private elementary school property that regularly maintains the Project site to prevent weed growth.
- (4) Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality. The proposed single-family residential development is similar to, and of no greater impact than other allowed residential uses and development projects within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) zoning district.
- (5) The site can be adequately served by all required utilities and public services. All necessary wet and dry utilities are available to the Project site.

CONDITIONS OF APPROVAL: See attached department reports.

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

	Existing Land Use	General Plan Designation	Zoning Designation
Site	Vacant	Vacant	LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac)
North	Educational Facility (Private School)	Educational Facility (Private School)	LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac)
South	Single Family Residential	Single Family Residential	LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac)
East	Single Family Residential	Single Family Residential	LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac)
West	Single Family Residential	Single Family Residential	LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac)

General Site & Building Statistics

Item	Required Min./Max.	Provided (Ranges)	Meets Y/N
Project area (in acres):	N/A	1.83 acres	N/A
Maximum project density (dwelling units/ac):	2.1 to 5.0 du/ac	5.5 du/ac	Y
Maximum coverage (in %):	50%	19% to 29%	Y
Minimum lot size (in SF):	7,200 SF	7,987 SF	Y
Minimum lot depth (in FT):	75 FT	123.57 FT	Y
Minimum lot width (in FT):	60 FT	64.28 FT to 64.66 FT	Y
Front yard setback (in FT):	20 FT – Front 10 FT – Front Garage Other (side-on condition)	20 FT – Front 20 FT – Front Garage Other (side-on condition)	Y
Side yard setback (in FT):	5 FT	5 FT to 23 FT	Y
Rear yard setback (in FT):	10 FT – 1st Floor Living Area 20 FT – 2nd/3rd Floor Living Area	41.3 FT to 55.2 FT	Y
Maximum height (in FT):	35 FT	19 FT 7 IN to 26 FT 4 IN	Y
Parking – resident:	2 spaces per dwelling within a garage	2 spaces per dwelling within a garage	Y

Dwelling Unit Statistics:

Unit Type	Size (in SF)	No. Bedrooms	No. Bathrooms	No. Stories	Lots
1	1,928 SF	4	3	1	1, 5 and 10
2	1,933 SF	3	3	2	2, 4, 6 and 8
3	2,119 SF	4	3	2	3, 7 and 9

Exhibit A—SITE PLAN

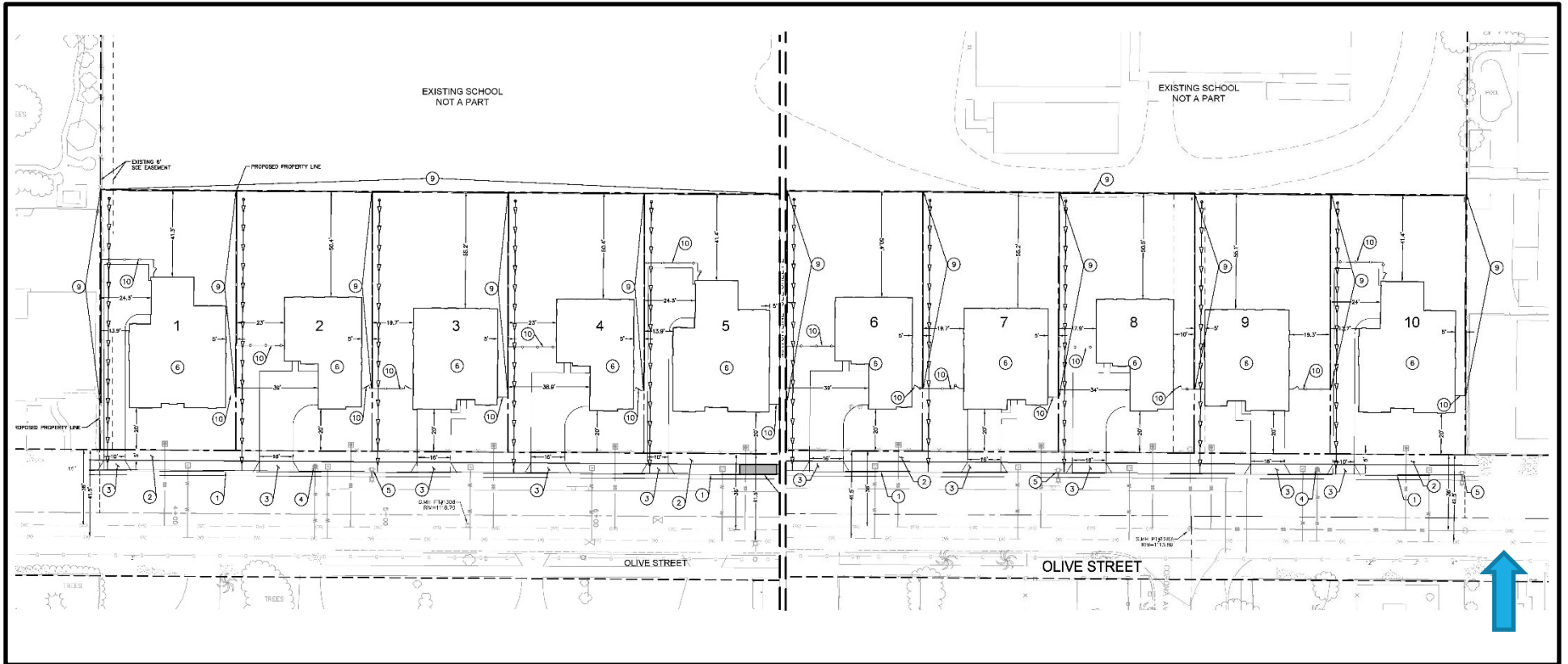


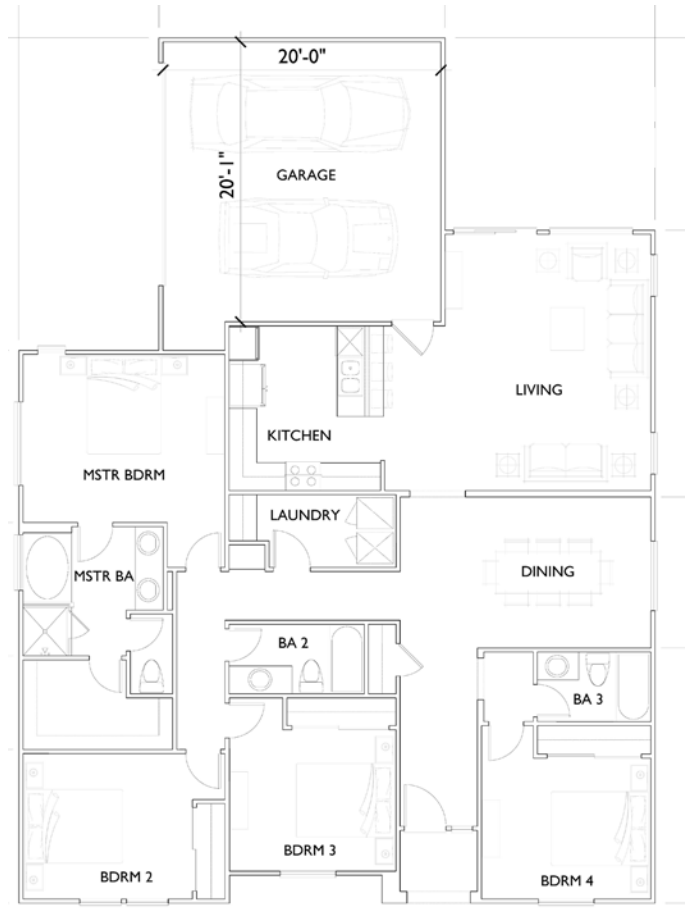
Exhibit B—ELEVATIONS



Plan 1A - Ranch



Plan 1B - Craftsman



**Plan 1 Floor Plan – 1,928 SF
4 Bedroom/3 Bath**

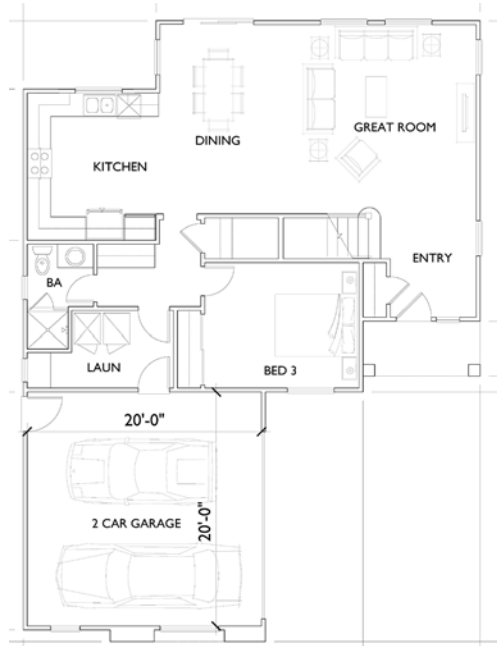
Exhibit B—ELEVATIONS (CONTINUED)



Plan 2A - Cottage



Plan 2B - Spanish



1ST FLOOR



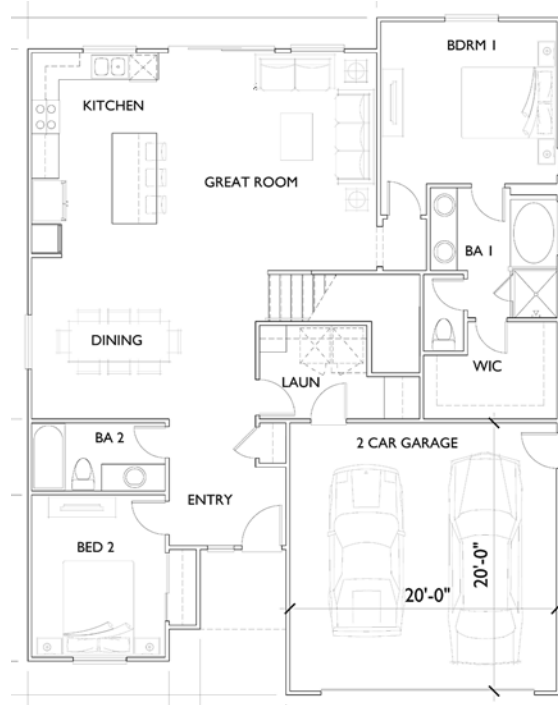
2ND FLOOR

**Plan 2 Floor Plan – 1,933 SF
3 Bedroom/3 Bath**

Exhibit B—ELEVATIONS (CONTINUED)



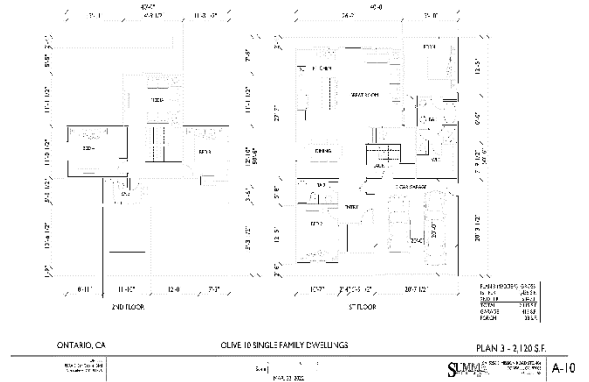
Plan 3A - Bungalow



1ST FLOOR



Plan 3B - Farmhouse



2ND FLOOR

**Plan 3 Floor Plan – 2,120 SF
 4 Bedroom/3 Bath**

Exhibit B—ELEVATIONS (CONTINUED)



3A BUNGALOW - TWO STORY - FRONT LOAD GARAGE



2A COTTAGE - TWO STORY - SWING IN GARAGE



1A RANCH - ONE STORY - REAR GARAGE



3B FARMHOUSE - TWO STORY - FRONT LOAD GARAGE

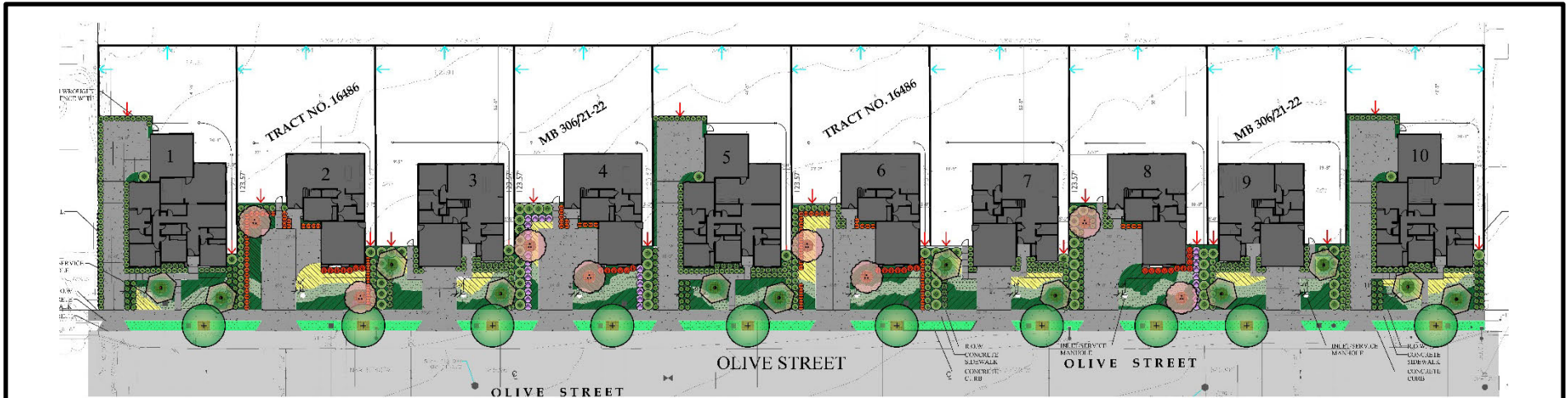


2B SPANISH - TWO STORY - SWING IN GARAGE



1B CRAFTSMAN - ONE STORY - REAR GARAGE

Exhibit C—LANDSCAPE PLAN



PLANT LEGEND

SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE	QUANTITY	WATER USE
	CERCIS OCCIDENTALIS MULTI TRUNK	WESTERN REDBUD	15 GAL	4	LOW
	CHITALPA PINK DAWN SINGLE TRUNK	CHITALPA	24" BOX	6	LOW
	PLATANUS HISPANICA CITY STREET TREE	LONDON PLANE TREE	24" BOX	11	MODERATE
	CALLISTEMON LITTLE JOHN	DWARF BOYLE BRUSH	5 GAL	95	LOW
	CALLIANDRA ERIOPHYLLA	FAIRY DUSTER	15 GAL	36	LOW
	DIANELLA LITTLE REV	DIANELLA	1 GAL	179	LOW
	FESTUCA MAIREI	ATLAS FESCUE	1 GAL	86	LOW
	FRANGULA EVE CASE	COFFEEBERRY	5 GAL	68	LOW
	HESPERALOE PARVIFLORA	RED YUCCA	5 GAL	30	LOW
	SALVIA CLEVELANDI WINDIED GILMAN	WINDIED GILMAN BLUE SAGE	5 GAL	29	LOW
	WESTRINGIA MORNING LIGHT	COAST ROSEMARY	5 GAL	103	LOW
	ACHILLEA MOONSHINE	FERN LEAF YARROW	1 GAL	@ 24" OC	LOW
	CAREX DIVVUSA	BERKLEY SEDGE	1 GAL	@ 18" OC	LOW
	PENNISETUM SPATHIOLATUM	SLENDER VILT GRASS	1 GAL	@ 24 OC	LOW
	PHYLLOPODIFLORA	KURAPPA	300	N/A	LOW
	ROSMARINUS HUNTINGTON CARPET	HUNTINGTON CARPET ROSEMARY	1 GAL	@ 36" OC	LOW
		3" MIN. DEEP LAYER OF MULCH			
		24" INLET AND STORM CHAMBER ACCESS PER GRADING PLAN			
		6" TAN SLUMPSTONE BLOCK WALL WITH DECORATIVE CAP			
		5' HIGH WROUGHT IRON FENCING WITH GATE			



RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PDEV19-047, A DEVELOPMENT PLAN TO CONSTRUCT 10 DETACHED SINGLE-FAMILY DWELLINGS ON 10 VACANT PARCELS TOTALING APPROXIMATELY 1.83 ACRES OF LAND GENERALLY LOCATED AT THE NORTHWEST CORNER OF OLIVE STREET AND ORANGE AVENUE, WITHIN THE LDR-5 (LOW DENSITY RESIDENTIAL – 2.1 TO 5.0 DU/AC) ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF—APNS: 0108-481-18, 0108-481-19, 0108-481-20, 0108-481-21, 0108-481-22, 0108-481-23, 0108-481-24, 0108-481-25, 0108-481-26, 0108-481-27.

WHEREAS, TEXTON CONSTRUCTION CO. ("Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV19-047, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 1.83 acres of land generally located at the northwest corner of Olive Street and Orange Avenue, within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) zoning district, and is presently vacant; and

WHEREAS, the property to the north of the Project site is within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) zoning district and is developed with an Educational Facility (Private School). The property to the east is within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) zoning district and is developed with Single Family Residential. The property to the south is within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) zoning district and is developed with Single Family Residential. The property to the west is within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) zoning district and is developed with Single Family Residential; and

WHEREAS, in November 2003, the Planning Commission approved the subdivision of 8.5 acres of land located at 1722 East Eighth Street, into 12 lots; and

WHEREAS, 10 of the lots that front Olive Street were proposed for future single-family development, one lot was developed with a well site for the City in 2007, and one lot was retained by San Antonio Jr. Academy (San Antonio Christian School) and remains in operation as a school; and

WHEREAS, the Project will facilitate the development of the 10 lots that front Olive Street with detached single-family dwellings; and

WHEREAS, lots 1, 5, and 10 of the Project will be developed with single-story designs with attached side-on garages located at the rear of the dwellings; and

WHEREAS, lots 2, 4, 6 and 8 of the Project will provide attached side-on garages at the front of the lots; and

WHEREAS, lots 3, 7, and 9 of the Project will provide front facing two-car garages; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act, commencing with Public Resources Code Section 21000 (hereinafter referred to as "CEQA"); and

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on July 18, 2022, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision No. DAB22-023, recommending that the Planning Commission approve the Application; and

WHEREAS, on July 26, 2022, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The administrative record has been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(2) The Project is categorically exempt from environmental review pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines, which consists of and meets each of the following conditions:

a. The Project is consistent with the applicable general plan designation and all applicable general plan policies as well as with applicable zoning designation and regulations. The proposed Project is located within the LDR (Low Density Residential) land use district of the Policy Plan Land Use Map, and the CC (Community Commercial) zoning district. The proposed project is consistent with all applicable general plan policies, as well as with the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) zoning designation and applicable Development Code regulations.

b. The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses. The Project is proposed within the established boundaries of the City of Ontario, on a project site totaling 1.83 acre of land, which is surrounded by established residential land uses.

c. The Project site has no value as habitat for endangered, rare, or threatened species. The site was previously graded and is located on an existing private elementary school property that regularly maintains the Project site to prevent weed growth.

d. Approval of the Project would not result in any significant effects relating to traffic, noise, air quality, or water quality. The proposed single-family residential

development is similar to, and of no greater impact than other allowed residential uses and development projects within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) zoning district.

e. The site can be adequately served by all required utilities and public services. All necessary wet and dry utilities are available to the Project site; and

(3) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

(4) The determination of CEQA exemption reflects the independent judgment of the Planning Commission.

SECTION 2: *Housing Element Compliance.* Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 3: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as “ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 4: Concluding Facts and Reasons. Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 through 3, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Project is located within the LDR (Low Density Residential) land use district of the Policy Plan Land Use Map, and the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) zoning district. The development standards and conditions under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.

(2) ***The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located.*** The Project has been designed consistent with the requirements of the City of Ontario Development Code and the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) zoning district, including standards relative to the particular land use proposed (Single Family Residential), as-well-as building intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions.

(3) ***The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed project.*** The Development Advisory Board has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: [i] the purposes of the Development Code are maintained; [ii] the project will not endanger the public health, safety or general welfare; [iii] the project will not result in any significant environmental impacts; [iv] the project will be in harmony with the area in which it is located; and [v] the project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan.

(4) ***The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development.*** The proposed Project has been reviewed for consistency with the general development standards and guidelines of the Development Code that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking and loading spaces, parking lot dimensions, design and landscaping, bicycle parking, on-site

landscaping, and fences and walls, as-well-as those development standards and guidelines specifically related to the particular land use being proposed (single-family residential). As a result of this review, the Planning Commission has determined that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the development standards and guidelines described in the Development Code.

SECTION 5: *Planning Commission Action.* Based upon the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as “Attachment A,” and incorporated herein by this reference.

SECTION 6: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 7: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East “B” Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 8: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26th day of July 2022, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Jim Willoughby
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on July 26, 2022, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**File No. PDEV19-047
Departmental Conditions of Approval**

(Departmental conditions of approval to follow this page)



LAND DEVELOPMENT DIVISION CONDITIONS OF APPROVAL

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

Date Prepared: 7/18/2022
File No: PDEV19-047
Related Files: PMTT03-014

Project Description: A Development Plan to construct 10 detached single-family dwellings on 10 vacant parcels totaling approximately 1.83 acres of land generally located at the northwest corner of Olive Street and Orange Avenue, within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) zoning district; (APNs: 0108-481-18, 0108-481-19, 0108-481-20, 0108-481-21, 0108-481-22, 0108-481-23, 0108-481-24, 0108-481-25, 0108-481-26, 0108-481-27); **submitted by Texton Construction Co.**

Prepared By: Elly Antuna, Associate Planner
Phone: 909.395.2414 (direct)
Email: eantuna@ontarioca.gov

The Planning Department, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits. Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

2.2 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility, and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.3 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b) Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

(c) Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(d) Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

2.4 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

(a) New block walls constructed along the perimeter of the project site and interior side property lines shall be constructed of tan slump stone block with a decorative concrete cap. Walls shall not exceed 6 FT in height.

(b) New block walls or fences constructed between side yard walls and the adjacent dwelling shall be setback a minimum of 1 FT from adjacent front wall plane. Block walls shall be constructed of tan slump stone block with a decorative concrete cap and fences shall be constructed of ornamental wrought iron with gates providing rear yard access. Block walls and fences shall not exceed 6 FT in height.

2.5 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(c) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

2.6 Architecture.

(a) Dwellings shall be constructed in the Ranch, Craftsman, Cottage, Spanish, Bungalow and Farmhouse architectural styles.

(b) Windows visible from the public right-of-way shall be recessed a minimum of 2" from building wall face and feature enhanced trim and/or sills.

(c) Stucco finishes shall be a 20/30 Sand Finish

2.7 Mechanical Equipment.

(a) All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.

2.8 Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

2.9 Disclosure Statements.

(a) A copy of the Public Report from the Department of Real Estate, prepared for the subdivision pursuant to Business and Professions Code Section 11000 et seq., shall be provided to each prospective buyer of the residential units and shall include a statement to the effect that:

(i) This tract is subject to noise from the Ontario International Airport and may be more severely impacted in the future.

2.10 Environmental Review.

(a) The proposed project is categorically exempt from the requirements of the California Environmental Quality Act of 1970 (CEQA), as amended, and the Guidelines promulgated thereunder, pursuant to **Section 15332 (Class 32, In-Fill Development Projects)** of the CEQA Guidelines, and meets all of the following conditions:

(i) The Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations;

(ii) The proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses;

(iii) The project site has no value as habitat for endangered, rare, or threatened species;

(iv) Approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and

(v) The Project site can be adequately served by all required utilities and public services.

2.11 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.12 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Exemption ("NOE") filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act ("CEQA"). The filing of an NOE is voluntary; however, failure to provide said fee within the time specified will result in the extension of the statute of limitations for the filing of a CEQA lawsuit from 30 days to 180 days.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

2.13 Additional Requirements.

(a) Lots 1, 5, and 10 shall feature single-story designs with attached side-on garages located at the rear of the dwellings.

(b) Lots 2, 4, 6 and 8 will provide attached side-on garages at the front of the lots.

(c) Lots 3, 7, and 9 will provide front facing two-car garages.



**ENGINEERING DEPARTMENT
CONDITIONS OF APPROVAL**

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Broadband Operations & Investment and Revenue Resources Department Conditions incorporated)

<input checked="" type="checkbox"/> DEVELOPMENT PLAN <input type="checkbox"/> OTHER	<input type="checkbox"/> PARCEL MAP <input type="checkbox"/> FOR CONDOMINIUM PURPOSES	<input type="checkbox"/> TRACT MAP
PROJECT FILE NO. <u>PDEV19-047</u> RELATED FILE NO(S). <u>TM-16486</u>		
<input checked="" type="checkbox"/> ORIGINAL <input type="checkbox"/> REVISED: <u> </u> / <u> </u> / <u> </u>		

CITY PROJECT ENGINEER & PHONE NO: Angela Truong (909) 395-2134

CITY PROJECT PLANNER & PHONE NO: Elly Antuna (909) 395-2414

DAB MEETING DATE: July 18, 2022

PROJECT NAME / DESCRIPTION: A Development Plan to construct 10 single-family dwellings on 10 vacant parcels totaling 1.83 acres of land within the LDR-5 (Low Density Residential – 2.1 to 5.0 DUs/Acre) zoning district. APN: 108-481-18, 19, 20, 21, 22, 23, 24, 25, 26 and 27

LOCATION: Northwest corner of East Olive Street and North Orange Avenue

APPLICANT: Texton Construction Co., Inc.

REVIEWED BY: Raymond Lee 6/22/22
 Raymond Lee, P.E. Date
 Assistant City Engineer

APPROVED BY: [Signature] 6.22.22
 Khoi Do, P.E. Date
 City Engineer



THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.

1. PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, APPLICANT SHALL: Check When Complete

- 1.01 Dedicate to the City of Ontario, the right-of-way, described below:
_____ feet on _____
Property line corner 'cut-back' required at the intersection of _____
and _____.
- 1.02 Dedicate to the City of Ontario, the following easement(s): _____

- 1.03 Restrict vehicular access to the site as follows: _____
- 1.04 Vacate the following street(s) and/or easement(s):
 - A. All interfering on-site easements shall be quitclaimed, vacated, and/or submit non-interference letter from affected owner/utility company.
- 1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.
- 1.06 Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.
- 1.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.
- 1.08 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Financial Services Department at (909) 395-2124 regarding this requirement.
 - (1) _____
 - (2) _____
- 1.09 Prepare a fully executed Subdivision Agreement (on City approved format and forms) with



accompanying security as required, or complete all public improvements.

- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ci.ontario.ca.us) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.
- 1.13 Ontario Ranch Developments:
 - 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
 - 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
 - 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions: _____

2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:

**A. GENERAL
 (Permits includes Grading, Building, Demolition and Encroachment)**

- 2.01 Record Parcel Map/Tract Map No. _____ pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 Submit a PDF of the recorded map to the City Engineer's office.
- 2.03 Note that the subject parcel is a recognized parcel in the City of Ontario per TM-16486.
- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of March 4, 1972.
- 2.05 Apply for a:
 - Certificate of Compliance with a Record of Survey;
 - Lot Line Adjustment (Record a Conforming Deed with the County of San Bernardino within six months of the recordation of the Lot Line Adjustment to conform the new LLA legal description. Submit a copy of the recorded Conforming Deed to the Engineering Department.);
 - Make a Dedication of Easement.



- 2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.

- 2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.

- 2.08 **Submit a soils/geology report.**

- 2.09 Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:
 - State of California Department of Transportation (Caltrans)
 - San Bernardino County Road Department (SBCRD)
 - San Bernardino County Flood Control District (SBCFCD)
 - Federal Emergency Management Agency (FEMA)
 - Cucamonga Valley Water District (CVWD) for sewer/water service
 - United States Army Corps of Engineers (USACE)
 - California Department of Fish & Game
 - Inland Empire Utilities Agency (IEUA)
 - Other: _____

- 2.10 Dedicate to the City of Ontario the right-of-way described below:

_____ feet on _____

Property line corner 'cut-back' required at the intersection of _____ and _____.

- 2.11 Dedicate to the City of Ontario the following easement(s): _____

- 2.12 **Vacate the following street(s) and/or easement(s):**
 - A. All interfering on-site easements shall be quitclaimed, vacated, and/or submit non-interference letter from affected owner/utility company.**

- 2.13 Ontario Ranch Developments:
 - 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.
 - 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay



any applicable fees as set forth by said agreement.

3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall.

2.14 Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at 100% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.

2.15 The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.

2.16 Pay all Development Impact Fees (DIF) to the Building Department. Final fee shall be determined based on the approved site plan.

2.17 Other conditions:

1) All the required improvements for this project shall be subject to completion of the required public improvements as specified in the Conditions of Approval for TM-16486.

2) As part of the precise grading plans submittal, provide a Final Utilities System Map that shows all existing and proposed utilities (Potable Water, Recycled Water, Sewer, Storm Drain, and other utilities), including each of the City's public utility's points of connection to the existing system.

3) The applicant/developer shall submit the previously approved off-site improvement plans approved under TM-16486 to verify conformance of updated City Standards. Any applicable plan check fees shall be paid by the developer.

4) The applicant/developer shall provide a fully executed Improvement Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.



B. PUBLIC IMPROVEMENTS
 (See attached Exhibit 'A' for plan check submittal requirements.)

2.17 Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):

Improvement	Olive Street	Street 2	Street 3	Street 4
Curb and Gutter	<input checked="" type="checkbox"/> New; 20 ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
AC Pavement	<input type="checkbox"/> Replacement <input checked="" type="checkbox"/> Widen approx. 7 additional feet along frontage to achieve an ultimate 18 feet, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Drive Approach	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Sidewalk	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
ADA Access Ramp	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Parkway	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/ irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
Raised Landscaped Median	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Fire Hydrant	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation



Sewer (see Sec. 2.C)	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Water (see Sec. 2.D)	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Street Light (see Sec. 2.F)	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2G)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Fiber Optics (see Sec. 2K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
Removal of Improvements	_____	_____	_____	_____
Other Improvements	_____	_____	_____	_____

Specific notes for improvements listed in item no. 2.17, above: _____

- 2.18 Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s): Olive Street from centerline to gutter. Limits may expand due to the multiple water services being constructed and the sewer main extension.
- 2.19 Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum



limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.

- 2.20 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide water service sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.
- 2.21 Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately for undergrounding of utilities in accordance with Section 7-7.302.e of the City's Municipal Code.
- 2.22 Other conditions: _____

C. SEWER

- 2.23 **An 8 inch sewer main is available for connection by this project in Olive Street. (Ref: Sewer plan bar code: S13032)**
- 2.24 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.25 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.
- 2.26 Other conditions:
 - 1) **The applicant/developer shall extend the existing sewer main by constructing a sewer main at the intersection of Corona Avenue and Olive Street to 130 feet easterly.**
 - 2) **The applicant/developer shall construct an 8" sewer lateral (private) stub-out for the school between lot 8 and 9. No permanent structure and/or tree cannot be within the recorded 15' wide private utilities easement (Tract Map 16486) for this sewer lateral. This private 8" sewer lateral is to start at the existing manhole at the intersection of Olive Street and Corona Avenue, continues northerly, terminates five to ten feet past the project northerly property line between lot 8 and 9.**
 - 3) **The applicant/developer shall install a manhole immediate north of the project southerly property line and a clean out just north of the project northerly property line.**

D. WATER

- 2.27 **A 6 inch water main is available for connection by this project in Olive Street. (Ref: Water plan bar code: W11164)**
- 2.28 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.29 Other conditions: _____
 - 1) **The applicant/developer shall be responsible to design and construct utilities with the minimum separation requirements in accordance with the Department of Drinking Water (DDW) Separation Requirements and California Code of Regulations (CCR) § 64572 Compliance. Refer to OMUC Exhibit herein.**

E. RECYCLED WATER

- 2.30 A _____ inch recycled water main is available for connection by this project in _____. (Ref: Recycled Water plan bar code: _____)
- 2.31 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.



2.32 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.

2.33 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.

Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.

2.34 Other conditions: _____

F. TRAFFIC / TRANSPORTATION

2.35 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer:
1. On-site and off-site circulation
2. Traffic level of service (LOS) at 'build-out' and future years
3. Impact at specific intersections as selected by the City Engineer

2.36 New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.

2.37 Other conditions:

1) The applicant/developer shall be responsible to design and construct street improvements along property frontage of Olive Street in accordance with conditions issued by City's Land Development Division. These, and all other street improvements required herein, shall include, but not limited to, concrete curb and gutter, sidewalk, LED street lights, signing and striping, and parkway landscaping.

2) The applicant/developer shall be responsible to design and construct the following streets to their ultimate half-width along the project frontage: Olive Street (60' R/W, 40' C/C).

3) The applicant/developer shall be responsible to design and construct in-fill public street lights and potential new service along its project frontage of Olive Street. Street lighting shall be LED-type and in accordance with the City's Traffic and Transportation Design Guidelines.

4) Driveways shall be designed in accordance with the City of Ontario Standard Drawing No. 1203.

5) All landscaping, block walls, and other obstructions shall be compatible with the stopping sight distance requirement per the City of Ontario Standard Drawing No. 1309.

6) The applicant/developer's engineer-of-record shall meet with City Engineering staff prior to designing and submitting for plan check the signing/striping and street lighting design plans to define limits of improvements.

G. DRAINAGE / HYDROLOGY

2.38 A _____ inch storm drain main is available to accept flows from this project in _____.
(Ref: Storm Drain plan bar code: _____)

2.39 Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.



- 2.40 An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100 year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.
- 2.41 Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.
- 2.42 Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.
- 2.43 **Other conditions:**
 - 1) **The applicant/developer shall dedicate a private drainage easement to the property to the north (existing school site APN: 108-481-28). Drainage easement shall be located on the area of the lots being used to convey flow to Olive Street from the school. The applicant/developer shall also enter into an agreement with the property owner of APN: 108-481-28 for the construction and maintenance of the drainage improvements being constructed to convey the historical flow. The agreement shall be reviewed and approved by the City Engineer and City Attorney. City Attorney fees shall be paid by the applicant/developer. See Section 2.39 above for required hydrology study and drainage analysis.**

H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)

- 2.44 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels.
 If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.
 Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.
- 2.45 **Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: <http://www.sbcounty.gov/dpw/land/npdes.asp>.**
- 2.46 Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.
- 2.47 Other conditions: _____

J. SPECIAL DISTRICTS



- 2.48 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.
- 2.49 Other conditions: _____

K. FIBER OPTIC

- 2.50 A _____ fiber optic line is available for connection by this project in _____. (Ref: Fiber Optic plan bar code: _____)
- 2.51 Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole. There is no existing fiber optic infrastructure near the site, see Fiber Optic Exhibit herein for proposed conduit and handhole locations.
- 2.52 Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Broadband Operations Department at (909) 395-2000, regarding this requirement.

3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:

- 3.01 Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.
- 3.02 Complete all requirements for recycled water usage.
 - 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.
 - 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.
 - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.
- 3.03 The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.
- 3.04 Ontario Ranch Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.
- 3.05 Confirm payment of all Development Impact Fees (DIF) to the Building Department.
- 3.06 Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studies



and reports (i.e. hydrology, traffic, WQMP, etc.).

4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:

- 4.01 Complete all Conditions of Approval listed under Sections 1-3 above.**
- 4.02 Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.**
- 4.03 The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall include a completed Acceptance and Bond Release Checklist, state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.**
- 4.04 Submit record drawings (PDF) for all public improvements identified within Section 2 of these Conditions of Approval.**



EXHIBIT 'A'

ENGINEERING DEPARTMENT First Plan Check Submittal Checklist

Project Number: PDEV 19-047

The following items are required to be included with the first plan check submittal:

1. **A copy of this check list**
2. **Payment of fee for Plan Checking**
3. **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4. **One (1) copy of project Conditions of Approval**
5. **Include a PDF (electronic submittal) of each required improvement plan at every submittal.**
6. Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).
7. **Three (3) sets of Public Street improvement plan with street cross-sections**
8. Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)
9. Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)
10. **Four (4) sets of Public Sewer improvement plan**
11. Five (5) sets of Public Storm Drain improvement plan
12. **Three (3) sets of Public Street Light improvement plan**
13. Three (3) sets of Signing and Striping improvement plan
14. **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
15. **Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)**
16. Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.
17. **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**
18. **One (1) copy of Hydrology/Drainage study**
19. **One (1) copy of Soils/Geology report**
20. Payment for Final Map/Parcel Map processing fee



21. Three (3) copies of Final Map/Parcel Map
22. One (1) copy of approved Tentative Map
23. One (1) copy of Preliminary Title Report (current within 30 days)
24. One (1) copy of Traverse Closure Calculations
25. One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.
26. Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
27. **Other: Non-interference letter from the easement holders listed on the Preliminary Title Report.**



CITY OF ONTARIO MEMORANDUM



DATE: June 9, 2022
TO: Angela Truong, Engineering
CC: Elly Antuna, Planning
FROM: Peter Tran, Utilities Engineering
SUBJECT: DPR #2 (***Changed of Ownership, Design, etc.***)- Conditions of Approval (#8380)
PROJECT NO.: PDEV19-047 (A Development Plan to construct 10 single-family dwelling on 10 vacant parcels)

BRIEF DESCRIPTION

A Development Plan to construct 10 single-family dwellings on 10 vacant parcels totaling approximately 1.83 acres of land generally located on the northwest corner of E Olive Street and N Orange Ave, within the LDR-5 (Low Density Residential – 2.1 to 5.0 DUs/Acre) zoning district (APNs: 0108-481-18, -19, -20, -21, -22, -23, -24, -25, -26, -27)..

THIS SUBMITTAL IS COMPLETE AND RECOMMENDED FOR APPROVAL.

CONDITIONS OF APPROVAL: *The Ontario Municipal Utilities Company (OMUC) recommends this application for approval subject to the conditions outlined below and compliance with the City's Design Development Guidelines, Specifications Design Criteria, and City Standards.*

General Conditions: (Add following to Section 2.A of Engineering Department COA)

1. **Standard Conditions of Approval:** Project shall comply with the requirements as set forth in the Amendment to the Standard Conditions of Approval for New Development Projects adopted by the City Council (Resolution No. 2017-027) on April 18, 2017; as well as project-specific conditions/requirements as outlined below:
2. **Final Utilities Systems Map (USM):** As part of the precise grading plans submittal, provide a Final Utilities Systems Map that shows all existing and proposed Utilities (Potable Water, Recycled Water, Sewer, Storm Drain, and other utilities) including each of the City's public utilities' points of connection to the existing systems
3. **Department of Drinking Water (DDW) Separation Requirements and California Code of Regulations (CCR) § 64572 Compliance:** All DDW Separation Requirements under CCR § 64572 must be met. In order to assure compliance with CCR § 64572, label the separation dimensions, measure from outside wall of the conveyances, between potable water to any other non-potable conveyance (sewer, storm drain, storm water, storm water infiltration, recycled water, recycled water irrigation, high pressure gas/petroleum, etc) publicly or privately maintained. Provide one label per sheet per conveyance and additional labels where separation dimensions and alignments change. For any facilities not currently meeting the following separation requirements, revise plans so that the facilities meet the following separations requirements:
 - a. **Minimum Separations (and Alignment Requirements) for Conveyances running Parallel to Potable Water Mains, Services, Laterals, and Conveyances (services, meters, fire hydrants, blowoff, airvac, etc.):**
 - i. **At least 10 feet horizontally from and one foot vertically above, any parallel pipeline conveying Sewer or Fluid Fuels.**
 - ii. **At least 4 feet horizontally from, and one foot vertically above, any parallel pipeline conveying Recycled Water, Storm Drainage, or Raw Ground Water.**
 - b. **Minimum Separations (and Alignment Requirements) for Conveyances Crossing Potable Water Mains, Services, Laterals and Conveyances:**
 - i. **Potable Water Conveyances shall cross no less than 45-degrees to and at least one foot above any pipeline conveying Sewer, Fluid Fuels, Recycled Water, Storm Drainage, Raw Ground Water, or Private Water.**
 - ii. **No connection joints shall be made in the Potable Water Conveyance within eight horizontal feet of crossing the non-potable conveyance.**
 - c. **Other Minimum Separations for Potable Water Mains, Services, Laterals and Conveyances:**

- i. 100 horizontal feet of the nearest edge of any sanitary landfill, wastewater disposal pond, or hazardous waste disposal site.
- ii. 25 horizontal feet of the nearest edge of any cesspool, septic tank, sewage leach field, or seepage pit.
- iii. 25 horizontal feet of the nearest edge of any underground hazardous material storage tank.
- iv. 25 horizontal feet of the nearest edge of any storm water retention, storm water infiltration, bioswale, or groundwater recharge site.

Sewer Conditions: (Add following to Section 2.C of Engineering Department COA)

4. Construct Sewer Main Extension: Extend the existing sewer main by constructing a sewer main at the intersection of Corona Avenue and Olive Street to 130 feet easterly.
5. Construct Sewer Lateral Stub-out and Appurtenances:
 - d. Construct an 8" sewer lateral (private) stub-out for the school between lot 8 and 9.
 - e. No permanent structure and/or tree cannot be within the recorded 15' wide private utilities easement (Tract Map 16486) for this sewer lateral.
 - f. This private 8" sewer lateral is to start at the existing manhole at the intersection of Olive Street and Corona Avenue, continues northerly, terminates five to ten feet past the project northerly property line between lot 8 and 9.
 - g. Install a manhole immediate north of the project southerly property line and a clean out just north of the project northerly property line.

ORANGE AVENUE

CORONA AVENUE

STREET

OLIVE

CONSTRUCTION NOTE 1
 CONSTRUCT (INSTALL) FIBER OPTIC CONDUIT AT A MINIMUM DEPTH OF 36". TRENCHING SHALL BE PER CITY STANDARD 1306. TWO (2) 2" HDPE SDR-11 (SMOOTHWALL) ROLL PIPE (1 ORANGE AND 1-ORANGE WITH BLACK STRIPE) OR EQUIVALENT. ALL UNUSED CONDUITS/MICRODUCTS SHALL BE PROTECTED WITH DUCTS WITH PLUGS THAT PROVIDE A POSITIVE SEAL. DUCTS THAT ARE OCCUPIED SHALL BE PROTECTED WITH INDUSTRY ACCEPTED DUCT SEAL COMPOUND. CONDUIT BANK REQUIRES (1) 12AWG HIGH STRENGTH (MIN. BREAK LOAD 452#) COPPER-CLAD STEEL W/30MIL HDPE ORANGE INSULATION FOR LOCATE/TRACER WIRE.

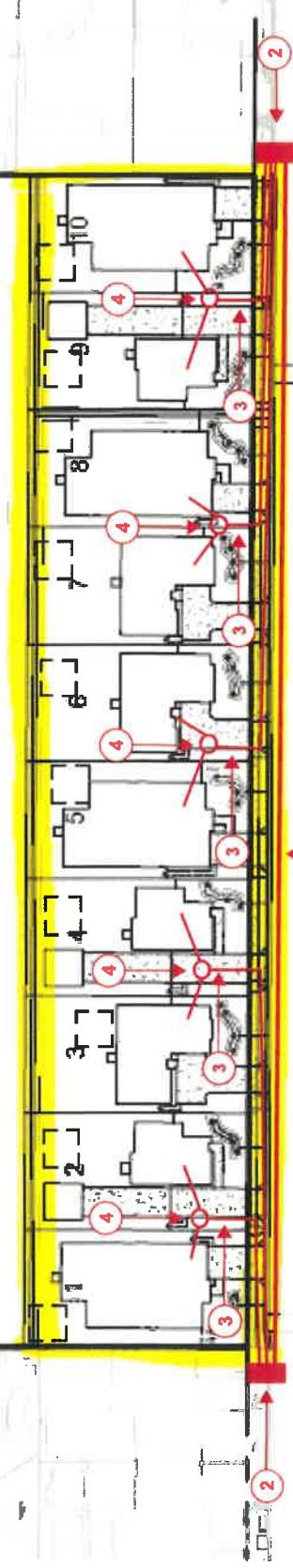
CONSTRUCTION NOTE 2
 ALL HANDHOLES (H-302) (RCA004836-00944) NEWBASIS; LESS THAN OR EQUAL TO 432 STRANDS. PER CITY STANDARD 1306. HANDHOLES SWEEPING INTO HANDHOLES SHALL ENTER IN FLUSH WITH THE CUT OUT MOUSE HOLES. ALIGNED PARALLEL TO THE BOTTOM OF THE BOX AND COME IN PERPENDICULAR TO THE WALL OF THE BOX. CONDUITS SHALL NOT ENTER AT ANY ANGLE OTHER THAN PARALLEL. PROVIDE 5" MIN CLEARANCE FROM EXISTING PROPOSED UTILITIES. ALL HANDHOLES WILL HAVE 1/4" INCH GALVANIZED WIRE BETWEEN THE HANDHOLES AND THE GRAVEL ITS PLACED ON.

CONSTRUCTION NOTE 3
 FROM THE NEAREST HANDHOLE TO THE BUILDING ENTRANCE. DESIGN AND INSTALL FIBER OPTIC CONDUIT AT A MINIMUM DEPTH OF 36-INCHES. TRENCHING SHALL BE PER CITY STANDARD FOR COMMERCIAL BUILDINGS: (1) 2-INCH HDPE SDR-11 (SMOOTHWALL) ROLL PIPE (ORANGE) DUCT. INSTALL LOCATE/TRACER WIRES MINIMUM 12AWG WITHIN CONDUIT BANK AND FIBER WARNING TAPE 16-INCH ABOVE THE UPPERMOST DUCT.

COMMERCIAL PROPERTIES SHALL TERMINATE CONDUIT IN AN ELECTRICAL ROOM ADJACENT TO THE WALL NO LESS THAN FIVE INCHES ABOVE THE FINISHED FLOOR. A 20" WIDTH X LENGTH 36" SPACE SHALL BE RESERVED ON THE PLYWOOD WALL FOR ONTARIO NET EQUIPMENT. THIS SPACE SHALL BE LABELED "ONTARIO NET ONLY". ONTARIO CONDUIT SHALL BE LABELED "ONTARIO NET".

LOCATION OF TELECOMMUNICATIONS ROOM IS CONCEPTUAL

CONSTRUCTION NOTE 4
 INSTALL FIBER OPTIC FLOWERPOT HR-FP (PLA1000101-00002) NEWBASIS. PER CITY STANDARD 1316 CONDUITS SWEEPING INTO THE FLOWERPOT SHALL ENTER IN FLUSH WITH THE CUT OUT MOUSE HOLES ALIGNED PARALLEL TO THE BOTTOM OF THE BOX. CONDUITS SHALL PROTRUDE FROM THE BASE OF THE FLOWERPOT, WITH 2" OF CLEARANCE FROM THE GRAVEL BASE. ALLOW MINIMUM 8" OF CLEARANCE BETWEEN THE CONDUIT AND FLOWERPOT LID FOR CABLE MANAGEMENT. PROVIDE 5" MIN CLEARANCE FROM EXISTING PROPOSED UTILITIES. ALL HANDHOLES WILL HAVE 1/4" INCH GALVANIZED WIRE BETWEEN THE HANDHOLES AS THE GRAVEL BASE. PROVIDE A 1316MM MICRODUCT WILL EXTEND FROM THE FLOWER POT TO EACH DWELLING UNIT



SHEET INDEX MAP
 SCALE: 1" = 100'



CITY OF ONTARIO MEMORANDUM

DEVELOPMENT PLAN REVIEW COMMENTS Broadband Operations Section

DATE: 06-22-22

PROJECT: PDEV19-047

LOCATION: Olive ST and Orange ST.

PROJECT ENGINEER: Angela

BROADBAND PLAN CHECKER: Cameron Chadwick - CChadwick@ontarioca.gov

A. General Comments:

1. The applicant/developer shall respond to these comments as well as the comments provided by the Environmental Section, Traffic & Transportation Division, Ontario Municipal Utilities Company and Broadband Operations and address all of them prior to the next submittal.
2. The applicant/developer shall address all additional redlined comments on the plans attached.
3. Provide plans in digital format (PDF) for future submittals.
4. Refer to the In-tract Fiber Network Design guideline on the City's website for additional in-tract conduit guidelines

B. The following items will be incorporated in the Conditions of Approval Report prior to the Development Advisory Board and/or Zoning Administrator Hearing upon all departments' comments being satisfactorily addressed:

1. Project shall be designed and constructed to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole in the Right-of-Way (ROW) and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole.
2. Contractor is responsible for locating and connecting conduit to existing OntarioNet hand holes on adjacent properties within a reasonable distance. There should be no "Gaps" in conduit between the contractor's development and the adjacent property. OntarioNet hand holes are typically located in the ROW at the extreme edge of a property.
3. Where a joint telecom or street light street crossing is required, include (2) 2" HDPE SDR-11 conduits or (1) 4" schedule 80 conduit sleeve. Terminate the street crossing conduit(s) in a new HH-3/22 OntarioNet hand hole in the right of way
4. The City requires a public utility easement for fiber optics on all private aisles/alley ways.
5. Hand holes - Design and install OntarioNet fiber optic hand hole HH-FP (10x00x10), HH-1 (13x24x18), HH-2 (17x30x24), HH-2A (24x36x30), HH-3 (30x48x36) and/or HH-4 (36x60x36) as needed. Respectively, Newbasis Part # PLA100010T-00002, PCA132418-00006, PCA-173024-90116, PCA-243630-90064, PCA-304836-90244 and PCA-366036-90146 or equivalent as specified per City Standard 1316. Conduits sweeping into hand holes shall enter in flush with the cut-out mouse holes aligned parallel to the bottom of

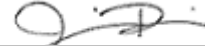
the box and come in perpendicular to the wall of the box. Conduits shall not enter at any angle other than parallel. Provide 5-foot minimum clearance from existing/proposed utilities. All hand holes will have ¼-inch galvanized wire between the hand holes and the gravel it is placed on.

6. ROW Conduit – Design and install fiber optic conduit at a minimum depth of 36-inch. Trenching shall be per City Standard 1306. Install (1) 2-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange) duct and (1) 2-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange with Black Stripe) duct. Conduit(s) between ROW hand holes and hand holes on private property shall be 2-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange) duct.
7. Building Entrance (Single Family) – Design and install 0.75-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange) duct from hand holes on property or hand holes in the ROW. Consult City's Fiber Team for design assistance.
8. Building Entrance (Multi-family and Commercial) - From the nearest handhole to the building entrance, design and install fiber optic conduit at a minimum depth of 36-inches. Trenching shall be per City Standard for Commercial Buildings. (1) 2-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange) duct. Install locate/tracer wires minimum 12AWG within conduit bank and fiber warning tape 18-inch above the uppermost duct
9. Multi-family and commercial properties shall terminate conduit in an electrical room adjacent to the wall no less than five inches above the finished floor. A 20" width X length 36" space shall be reserved on the plywood wall for OntarioNet equipment. This space shall be labeled "OntarioNet Only". Ontario Conduit shall be labeled "OntarioNet"
10. A minimum 1.5-inch joint use telecommunications conduit with pull-rope from the multi-family or commercial building communal telecom/electrical room/closet to each multi-family or commercial building unit shall be installed. See Structured Wiring Checklist on City's website for additional details.
11. Warning Tape - Contractor shall supply and install an approved non-detectable warning tape 18-inch above the uppermost conduit when backfilling trenches, pits or excavations greater than 10' in length. Warning Tape shall be non-detectable, Orange in color, 4-inch minimum width, 4 mil, 500% minimum elongation, with bold printed black letters "CAUTION - BURIED FIBER OPTIC CABLE BELOW" printed in bold black lettering no less than 2-inch high.
12. All hand holes, conduits, conduit banks, materials and installations are per the City's Fiber Optic Master Plan and City Fiber Optic Cable and Duct Standards. All hand holes, conduits and ducts shall be placed in the public right of way.
13. All unused conduits/ducts/microducts shall be protected with duct plugs that provide a positive seal. Ducts that are occupied shall be protected with industry accepted duct seal compound.
14. Locate/Tracer Wire - Conduit bank requires (1) 12AWG high strength (minimum break load 452#) copper-clad steel with 30mil HDPE orange insulation for locate/tracer wire. Contact City's Fiber Team for tracer wire specifications and see note 8.
15. Multi-family dwellings are considered commercial property.
16. Refer to the In-tract Fiber Network Design guideline on the City's website for additional in-tract conduit guidelines.

CITY OF ONTARIO
LANDSCAPE PLANNING DIVISION
 303 East "B" Street, Ontario, CA 91764

DAB CONDITIONS OF APPROVAL

Sign Off



Jamie Richardson, Sr. Landscape Planner

5/19/2022

Date

Reviewer's Name:

Jamie Richardson, Sr. Landscape Planner

Phone:

(909) 395-2615

D.A.B. File No.:

PDEV19-047

Case Planner:

Elly Antuna

Project Name and Location:

10 Unit Residential Project
 NW Corner of Olive Street and Orange Avenue

Applicant/Representative:

Texon Construction sean@texonco.com
 300 W Valley Blvd.
 San Gabriel, CA 91776



A Preliminary Landscape Plan (dated 5/4/2022) meets the Standard Conditions for New Development and has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction



A Preliminary Landscape Plan (dated) has not been approved. Corrections noted below are required prior to Preliminary Landscape Plan approval.

A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED AS INCOMPLETE.

Landscape construction plans with plan check number may be emailed to:
landscapeplancheck@ontarioca.gov

- After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections at a rate established by resolution of the City Council. Landscape construction plans with building permit number for plan check may be emailed to: landscapeplancheck@ontarioca.gov



CITY OF ONTARIO MEMORANDUM



DATE: June 20, 2022
TO: Elly Antuna, Planning Department
FROM: Blaine Ishii, Integrated Waste Department (via Transtech Engineers)
PROJECT NO.: PDEV19-047
ATTACHMENTS: None

BRIEF DESCRIPTION

10 SINGLE FAMILY DWELLINGS

THE INTEGRATED WASTE DEPARTMENT HAS THE FOLLOWING CONDITIONS. RESUBMITTAL OF PLANS IS NOT REQUIRED

The plan submitted adequately addressed the concerns of the Integrated Waste Department, but the Department is proposing the following conditions of approval.

Integrated Waste Conditions of Approval:

1. Each residential Dwellings shall have a minimum of three cans (Refuse, Recycling, and Green/Organics).
2. Residential Homes that have private landscaping shall require a minimum storage area of 36" x 96" to accommodate three containers: Refuse, Recycle, and Organic Waste (Green Waste).
3. External Refuse, Recycling, and Organic (Green Waste) Waste container storage shall be located in an interior courtyard, or rear or side yard with appropriate access. Automated cans shall not be stored in a location that is visible from the street, in front or street side yard, or in required parking areas.



CITY OF ONTARIO

MEMORANDUM

TO: Elly Antuna, Assistant Planner
Planning Department

FROM: Paul Ehrman, Deputy Fire Chief/Fire Marshal
Fire Department

DATE: August 21, 2019

SUBJECT: PDEV19-047 – A Development Plan to construct 10 single-family dwellings on 10 vacant parcels totaling approximately 1.83 acres of land generally located on the northwest corner of E Olive Street and N Orange Ave, within the LDR-5 (Low Density Residential – 2.1 to 5.0 DUs/Acre) zoning district (APNs: 0108-481-18, -19, -20, -21, -22, -23, -24, -25, -26, -27).

-
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.

SITE AND BUILDING FEATURES:

- A. 2016 CBC Type of Construction: Type V
- B. Type of Roof Materials: Ordinary
- C. Number of Stories: 2
- D. Total Square Footage: Varies, 2,400 – 2,900 Sq. Ft.
- E. 2016 CBC Occupancy Classification(s): R3

CONDITIONS OF APPROVAL:

1.0 GENERAL

- ☒ 1.1 The following are the Ontario Fire Department (“Fire Department”) requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards (“Standards.”) It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at www.ontarioca.gov, click on “Fire Department” and then on “Standards and Forms.”
- ☒ 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

2.0 FIRE DEPARTMENT ACCESS

- ☒ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- ☒ 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25’) inside and forty-five feet (45’) outside turning radius per Standard #B-005.
- ☒ 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150’) in length shall have an approved turn-around per Standard #B-002.
- ☒ 2.4 Access drive aisles which cross property lines shall be provided with CC&Rs, access easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of building plan check.
- ☒ 2.5 "No Parking-Fire Lane" signs and /or red painted curbs with lettering are required to be installed in interior access roadways, in locations where vehicle parking would obstruct the minimum clear width requirement. Installation shall be per Standard #B-001.
- ☒ 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-four (24) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by fire department and other emergency services.

3.0 WATER SUPPLY

- ☒ 3.1 The required fire flow per Fire Department standards, based on the 2016 California Fire Code, Appendix B, is 1500 gallons per minute (g.p.m.) for 2 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.

- ☒ 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.
- ☒ 3.4 The water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

4.0 FIRE PROTECTION SYSTEMS

- ☒ 4.2 Underground fire mains which cross property lines shall be provided with CC & R, easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of fire department plan check. The shared use of private fire mains or fire pumps is allowable only between immediately adjacent properties and shall not cross any public street.
- ☒ 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- ☒ 4.4 Wood frame buildings that are to be sprinkled shall have these systems in service (but not necessarily finalized) before the building is enclosed.

5.0 BUILDING CONSTRUCTION FEATURES

- ☒ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ☒ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- ☒ 5.3 Single station smoke alarms and carbon monoxide alarms are required to be installed per the California Building Code and the California Fire Code.
- ☒ 5.5 All residential chimneys shall be equipped with an approved spark arrester meeting the requirements of the California Building Code.



CITY OF ONTARIO

MEMORANDUM

TO: Elly Antuna, Assistant Planner

FROM: Officer Emily Hernandez, Police Department

DATE: August 27, 2019

SUBJECT: PDEV19-047– A DEVELOPMENT PLAN TO CONSTRUCT 10 SINGLE-FAMILY HOMES GENERALLY LOACTED AT THE NORTHWEST CORNER OF OLIVE STREET AND ORANGE AVENUE.

The “Standard Conditions of Approval” contained in Resolution No. 2017-027 apply. The applicant shall read and be thoroughly familiar with these conditions, including but not limited to, the requirements listed below.

- Required lighting for all walkways, driveways, doorways, parking areas, and other areas used by the public shall be provided and operate on photosensor at the prescribed foot-candle levels. Photometrics shall be provided to the Police Department. Photometrics shall include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- The Applicant shall comply with all construction site security requirements as stated in the Standard Conditions. This includes the provisions for perimeter lighting, site lighting, fencing and/or uniformed security.

The Applicant is invited to contact Officer Emily Hernandez at (909)408-1755 with any questions or concerns regarding these conditions.

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT



Project File No.: PDEV19-047
 Address: NWC Olive St & Orange Ave
 APN: 0108-481-18 thru 27
 Existing Land Use: Vacant
 Proposed Land Use: Development Plan to construct 10 Single Family Homes on 10 vacant parcels
 Site Acreage: 1.83 Proposed Structure Height: 26 ft
 ONT-IAC Project Review: N/A
 Airport Influence Area: ONT

Reviewed By: Lorena Mejia
 Contact Info: 909-395-2276
 Project Planner: Elly Antuna
 Date: 11/6/19
 CD No.: 2019-055
 PALU No.: n/a

The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="radio"/> High Terrain Zone	<input type="radio"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input checked="" type="checkbox"/> FAA Notification Surfaces	<input type="radio"/> Recorded Overflight Notification
<input type="radio"/> Zone 2	<input type="checkbox"/> 65 - 70 dB CNEL	<input checked="" type="checkbox"/> Airspace Obstruction Surfaces	<input checked="" type="checkbox"/> Real Estate Transaction Disclosure
<input type="radio"/> Zone 3	<input type="checkbox"/> 60 - 65 dB CNEL	<input type="radio"/> Airspace Avigation Easement Area	
<input type="radio"/> Zone 4		Allowable Height: 80 FT	
<input type="radio"/> Zone 5			

The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1
 Zone 2
 Zone 3
 Zone 4
 Zone 5
 Zone 6

Allowable Height: _____

CONSISTENCY DETERMINATION

This proposed Project is: Exempt from the ALUCP Consistent Consistent with Conditions Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

Real Estate Transaction Disclosure Required

Airport Planner Signature: _____

FILE NO: PDEV21-020

SUBJECT: A Development Plan to construct a 45,000-square-foot industrial building on 2.02 acres of land, located at 1044 and 1050 East Holt Boulevard, within the IP (Industrial Park) zoning district; (APNs: 1049-131-15 and 1049-131-016) **submitted by HK Ventures, Inc.**

PROPERTY OWNER: 1044 East Holt LLC

RECOMMENDED ACTION: That the Planning Commission consider and adopt the resolution approving the Development Plan, File No. PDEV21-020, pursuant to the facts and reasons contained in the staff report and resolution, and subject to the departmental conditions of approval contained in Attachment A to the resolution.


PROJECT SETTING: The Project site is comprised of two lots totaling 2.02 acres of land, located at the southwest corner of Holt Boulevard and Cucamonga Avenue (1044 and 1050 East Holt Boulevard), within the IP (Industrial Park) zoning district, and is depicted in Figure 1: Project Location, below. The property is undeveloped with a gentle 1 to 2 percent slope toward the southeast corner of the site. The Project site lacks right-of-way improvements along Holt Boulevard and Cucamonga Avenue, and there are several California Pepper (heritage) trees located along Cucamonga Avenue, which will be removed to accommodate the proposed development. The existing surrounding land uses, zoning, and general plan land use designations are summarized in the "Surrounding Zoning & Land Uses" table located in the Technical Appendix of this report.

PROJECT ANALYSIS:

(1) Background — On June 9, 2021, the Applicant submitted a Development Plan application requesting to construct



Figure 1: Project Location

Case Planner:	Jeanie Irene Aguilo
Planning Director Approval:	
Submittal Date:	06/09/2021

Hearing Body	Date	Decision	Action
DAB	07/18/2022	Approval	Recommend
PC	07/26/22		Final
CC			

a 45,000-square-foot industrial building on the Project site, having a floor area ratio ("FAR") of 0.52.

On July 18, 2022, the Development Advisory Board conducted a hearing to consider the Project, and concluded the hearing on that date, and unanimously voted to recommend that the Planning Commission approve the Application.

(2) Site Design/Building Layout — The proposed rectangular-shaped building is located toward the northernly portion of the Project site, with the office located at the southeast corner of the building and oriented to the east, towards Cucamonga Avenue. The building is setback 23 feet from the north (Holt Boulevard) property line, 191 feet from the south (rear) property line, 10 feet from the east (Cucamonga Avenue) property line, and a zero-setback condition is proposed along the west (interior) property line. The Project will provide a small off-street parking lot for visitors and office employees immediately south of the building's office area. The majority of the off-street parking spaces are located within the yard area, along the west and south property lines, which will serve warehouse employees (see Exhibit B—Site Plan, attached).

A yard area designed for tractor-trailer parking, truck maneuvering, loading activities, and outdoor staging, is located south of the proposed building. The yard area will be screened from public view (from Cucamonga Avenue) by the proposed building and a combination of landscaping and a 10-foot-high tilt-up screen wall with view-obstructing gates that will match the architecture of the building. An 8-foot-high masonry block wall will also be provided along the west and south property lines to screen and secure the yard area. Additionally, an outdoor employee patio area with an overhead trellis is proposed immediately south of the building, adjacent to the office area.

(3) Site Access/Circulation — The Project site will have one point of access from Cucamonga Avenue, via a 50-foot-wide driveway located near the southeast corner of the Project site. The driveway will provide vehicular access and truck access to the yard area and office parking lot. Pursuant to the conditions of approval, decorative pavement will be provided at the drive approach, which will extend from the back of the driveway apron to the intersecting parking lot drive aisle.

(4) Parking — The Project has provided off-street parking pursuant to the "Warehouse and Distribution" parking standards specified in the Development Code. The Project requires a total of 28 passenger vehicle parking spaces which have been provided. The off-street parking calculations for the building are as follows:

<i>Type of Use</i>	<i>Building Area</i>	<i>Parking Ratio</i>	<i>Spaces Required</i>	<i>Spaces Provided</i>
Warehouse / Distribution	42,750 SF	One space per 1,000 SF (0.001/SF) for portion of GFA <20,000 SF, plus 0.5 space per 1,000 SF (0.0005/SF) for GFA > 20,000 SF.	28	28

Type of Use	Building Area	Parking Ratio	Spaces Required	Spaces Provided
Office	2,250 SF	4 spaces per 1,000 SF (0.004/SF) of GFA. Parking is required when "general business offices" and other associated uses, exceed 10 percent of the building GFA.	0	0
TOTAL	45,000 SF		28	28

The City's off-street parking and loading standards require that the Project provide a minimum of one tractor-trailer parking space for each four dock-high loading spaces. There are 6 dock-high doors proposed, requiring one tractor-trailer parking space, and two spaces have been provided.

(5) Architecture — The proposed industrial warehouse building will be of concrete tilt-up construction and feature a modern architectural design with a grey, white, and blue color palette. Architecturally, the building incorporates smooth-painted concrete, form-liner textured accent panels, horizontal and vertical reveals, color blocking, clerestory windows with clear anodized aluminum mullions and blue reflective glazing, and painted steel canopies over the main office entry (see Exhibit C—Elevations, attached). The mechanical equipment for the building will be roof-mounted and obscured from public view by the parapet walls and, if necessary, equipment screens, which will incorporate design features consistent with the building architecture. Staff believes that the proposed Project illustrates the type of high-quality architecture promoted by the Development Code. This is exemplified through the use of:

- Articulation in the building footprint, incorporating a combination of recessed and popped-out wall areas; and
- Articulation in the building parapet/roof line, which serves to accentuate the building's entry and breaks up large expanses of building wall; and
- A mix of exterior colors, finishes, reveal lines, materials, and fixtures; and
- Incorporation of base and top treatments defined by changes in color, building reveals, and recessed wall areas.
- The building was designed to ensure that its massing and proportion, along with its colors and architectural detailing, are consistent on all four building elevations.

(6) Landscaping — The Project provides landscaping along Holt Boulevard, Cucamonga Avenue, and the tractor-trailer yard area. The Development Code requires a minimum 15 percent landscape coverage for corner lots, which has been provided. Additionally, the Project includes, street widening, the construction of a cul-de-sac at the terminus of Cucamonga Avenue, right-of-way improvements (curb, gutter, sidewalk, and parkway) and street trees. The proposed on-site and off-site landscape improvements will assist towards creating a walkable, safe area for pedestrians to access the Project site. The landscape plan incorporates a combination of 48-inch and 36-inch, along Holt Boulevard, which includes a mix of Coast Live Oak, Arizona Sycamore, Holly Oak, and Chinese Pistache trees. In addition, a mix of 24-inch box shade trees will be provided

throughout the Project site that includes Raywood Ash, Chitalpa, London Plane Tree, Crape Myrtle, and Cajeput trees. A variety of shrubs and groundcovers are also being provided, which are low water usage or drought tolerant (see Exhibit D—Landscape Plan, attached).

(7) Utilities (drainage, sewer) — Public utilities (water and sewer) are available to serve the project. Furthermore, the Applicant has submitted a Preliminary Water Quality Management Plan (“PWQMP”), which establishes the Project’s compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development (“LID”) best management practices (“BMPs”), such as retention and infiltration, biotreatment, and evapotranspiration. The PWQMP proposes the use of an underground stormwater chamber system located on the southern portion of the Project site. Any overflow drainage will be conveyed to the 36-inch parkway drain located within Cucamonga Avenue via a 12-inch diameter pipe connection to the catch basin. In addition, the Engineering Department has conditioned that the applicant provide approval from the Union Pacific Railroad accepting additional post development runoff; or design and construct a storm water detention facility on the project site in accordance with the approved hydrology study and improvement plans.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed Project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

(1) City Council Goals.

- Invest in the Growth and Evolution of the City’s Economy
- Maintain the Current High Level of Public Safety
- Operate in a Businesslike Manner
- Invest in the City’s Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)

(2) Governance.

Decision Making:

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.

- G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

(3) Policy Plan (General Plan)

Land Use Element:

- Goal LU2: Compatibility between a wide range of uses.
 - LU2-6: Infrastructure Compatibility: We require infrastructure to be aesthetically pleasing and in context with the community character.

Community Economics Element:

- Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.
 - CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.
 - CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.
 - CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.
 - CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Safety Element:

- Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.
 - S1-1 Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

Community Design Element:

- Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.
 - CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:

- Building volume, massing, and height to provide appropriate scale and proportion;
- A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

➤ CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.

➤ CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

➤ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➤ CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.

➤ CD2-11 Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.

➤ CD2-12 Site and Building Signage. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.

➤ CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

- Goal CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within

developments that are conveniently located, visually appealing and safe during all hours.

➤ CD3-1 Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.

➤ CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.

➤ CD3-3 Building Entrances. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces.

➤ CD3-5 Paving. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.

➤ CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.

▪ Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.

➤ CD5-1 Maintenance of Buildings and Property. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.

➤ CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

HOUSING ELEMENT COMPLIANCE: The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the Project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within

the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed Project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP.

ENVIRONMENTAL REVIEW: The Project is categorically exempt from the requirements of the California Environmental Quality Act ("CEQA") pursuant to Section 15332 (Class 32, In-Fill Development) of the CEQA Guidelines, and meets all of the follow conditions:

(1) The Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as with applicable zoning designation and regulations. The proposed Project is located within the BP (Business Park) land use district of the Policy Plan Land Use Map, and the IP (Industrial Park) zoning district. The proposed project is consistent with all applicable Policy Plan (general plan) policies, as well as with the requirements of the City of Ontario Development Code.

(2) The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses. The Project is proposed within the established boundaries of the City of Ontario, on a project site totaling 2.02 acres of land, which is surrounded by established industrial and administrative land uses to the west and east, respectively, and a mix of residential land uses to the north, across Holt Boulevard.

(3) The Project site has no value as habitat for endangered, rare, or threatened species. The site is void of any flora or fauna, was previously graded, and is regularly maintained to prevent the growth of weeds and grasses.

(4) Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality. The proposed industrial park land use is similar to, and of no greater impact than other allowed business park and industrial park land uses within the IP (Industrial Park) zoning district.

(5) The site can be adequately served by all required utilities and public services. The site is located within an area that is currently served by all required utilities and public services, including, but not limited to sewer, water, gas, electric, and municipal solid waste services.

CONDITIONS OF APPROVAL: See attached department reports.

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

	Existing Land Use	General Plan Designation	Zoning Designation	Specific Plan Land Use
Site	Vacant	Business Park (0.6 FAR)	IP (Industrial Park)	N/A
North	Sky Villa Trailer Park and Multi-Family Residential	Mixed Use 2 - East Holt	MU-2, East Holt Mixed-Use	N/A
South	Railroad	Rail	RC (Rail Corridor)	N/A
East	Office (Social Security Administration)	Business Park (0.6 FAR)	IP (Industrial Park)	N/A
West	Industrial (Under Construction)	Business Park (0.6 FAR)	IP (Industrial Park)	N/A

General Site & Building Statistics

Item	Proposed	Min./Max. Standard	Meets Y/N
Project Area:	2.02 AC	N/A	Y
Lot/Parcel Size:	85,913 SF	1.0 AC (Min.)	Y
Building Area:	45,000 SF	51,548 SF (Max.)	Y
Floor Area Ratio:	0.52	0.60 (Max.)	Y
Building Height:	42 FT	45 FT (Max.)	Y

Off-Street Parking:

Type of Use	Building Area	Parking Ratio	Spaces Required	Spaces Provided
Warehouse / Distribution	42,750 SF	One space per 1,000 SF (0.001/SF) for portion of GFA <20,000 SF, plus 0.5 space per 1,000 SF (0.0005/SF) for GFA > 20,000 SF.	28	28
Office	2,250 SF	4 spaces per 1,000 SF (0.004/SF) of GFA. Parking is required when "general business offices" and other associated uses, exceed 10 percent of the building GFA.	0	0
TOTAL	45,000 SF		28	28



Exhibit A—PROJECT LOCATION MAP

Exhibit B—SITE PLAN

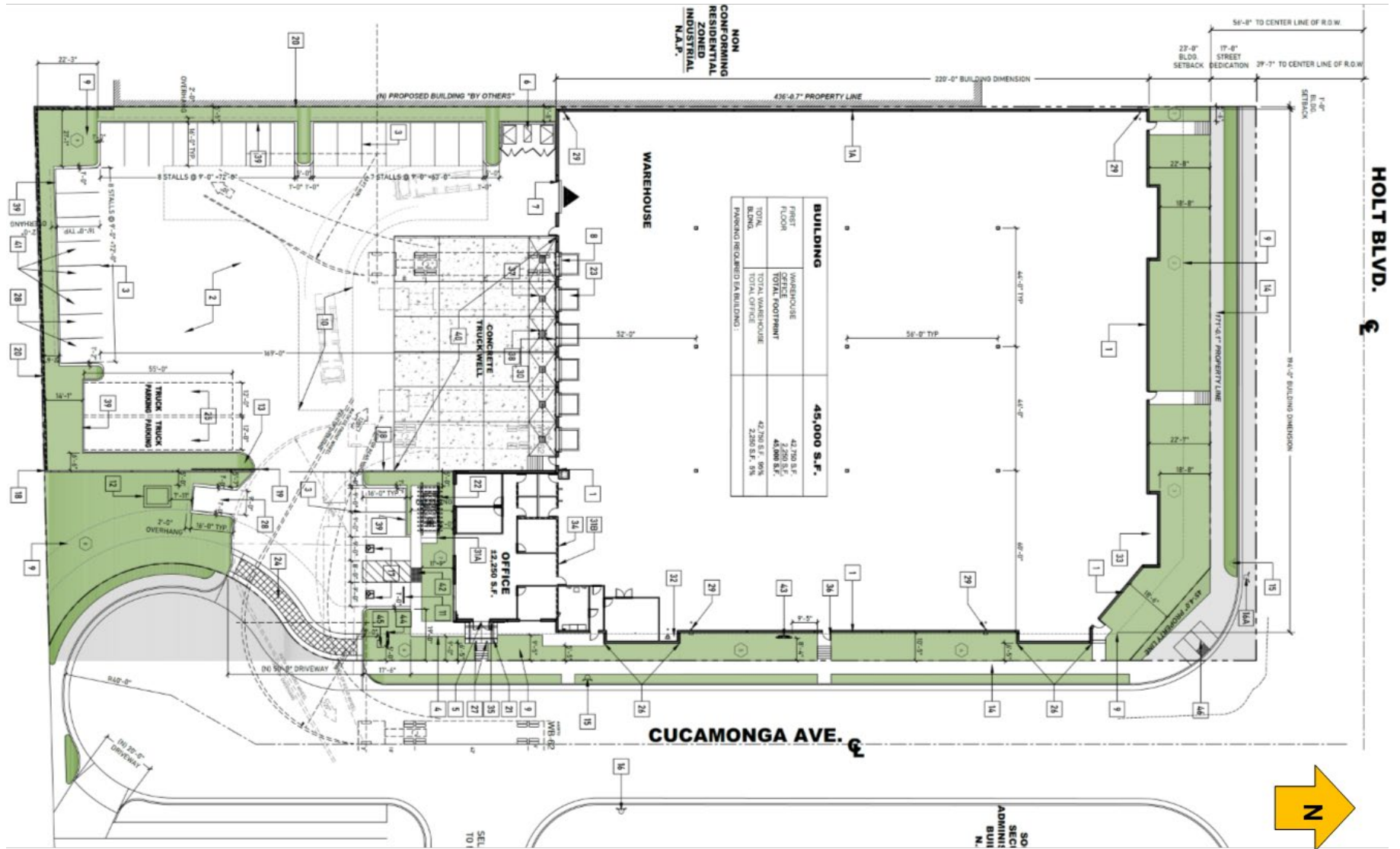


Exhibit C—ELEVATIONS



Exhibit D—LANDSCAPE PLAN



RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PDEV21-020, A DEVELOPMENT PLAN TO CONSTRUCT A 45,000-SQUARE-FOOT INDUSTRIAL BUILDING ON 2.02 ACRES OF LAND LOCATED AT 1044 AND 1050 EAST HOLT BOULEVARD, WITHIN THE IP (INDUSTRIAL PARK) ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF—APNS: 1049-131-15 AND 1049-131-016.

WHEREAS, HK VENTURES, INC. ("Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV21-020, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 2.02 acres of land, located at 1044 and 1050 East Holt Boulevard, within the IP (Industrial Park) zoning district, and is presently vacant; and

WHEREAS, the property to the north of the Project site (across Holt Boulevard) is within the MU-2 (East Holt Mixed-Use) zoning district and is currently developed with the Sky Villa Trailer Park and Multi-Family Residential apartments. The property to the east is within the IP (Industrial Park) zoning district and is currently developed with the Social Security Administration office. The property to the south is within the RC (Rail Corridor) zoning district and is developed with a railroad land use. The property to the west is within the IP (Industrial Park) zoning district and is currently under construction with an industrial building; and

WHEREAS, the Applicant submitted a Development Plan application requesting to construct a 45,000-square-foot industrial building on the Project site, having a floor area ratio ("FAR") of 0.52; and

WHEREAS, the proposed rectangular-shaped building is located toward the northernly portion of the Project site, with the office located at the southeast corner of the building and oriented to the east, towards Cucamonga Avenue; and

WHEREAS, the Project will provide a small off-street parking lot for visitors and office employees immediately south of the building's office area. The majority of the off-street parking spaces are located within the yard area, along the west and south property lines, which will serve warehouse employees; and

WHEREAS, a yard area designed for tractor-trailer parking, truck maneuvering, loading activities, and outdoor staging, is located south of the proposed building; and

WHEREAS, the Project site will have one point of access from Cucamonga Avenue, via a 50-foot-wide driveway located near the southeast corner of the Project site. The driveway will provide vehicular access and truck access to the yard area and office parking lot; and

WHEREAS, the Project has provided off-street parking pursuant to the "Warehouse and Distribution" parking standards specified in the Development Code. The Project requires a total of 28 passenger vehicle parking spaces which has been provided. In addition, the City's off-street parking and loading standards require that the Project provide a minimum of one tractor trailer parking space for each four dock-high loading spaces. There are 6 dock-high doors proposed requiring one tractor trailer parking space, and two spaces have been provided; and

WHEREAS, the proposed industrial warehouse building will be of concrete tilt-up construction and feature a modern architectural design with a grey, white, and blue color palette. Architecturally, the building incorporates smooth-painted concrete, form-liner textured accent panels, horizontal and vertical reveals, color blocking, clerestory windows with clear anodized aluminum mullions and blue reflective glazing, and painted steel canopies over the main office entry. The mechanical equipment for the building will be roof-mounted and obscured from public view by the parapet walls and, if necessary, equipment screens, which will incorporate design features consistent with the building architecture; and

WHEREAS, the Development Code requires a minimum 15 percent landscape coverage, which has been provided; and

WHEREAS, public utilities (water and sewer) are available to serve the Project. The PWQMP proposes the use of an underground stormwater chamber system located on the southern portion of the Project site. Any overflow drainage will be conveyed to the 36-inch parkway drain located within Cucamonga Avenue via a 12-inch diameter pipe connection to the catch basin. In addition, the Engineering Department has conditioned that the applicant provide approval from the Union Pacific Railroad accepting additional post development runoff; or design and construct a storm water detention facility on the project site in accordance with the approved hydrology study and improvement plans; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act, commencing with Public Resources Code Section 21000 (hereinafter referred to as "CEQA"); and

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the

application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on July 18, 2022, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision No. DAB22-024, recommending that the Planning Commission approve the Application; and

WHEREAS, on July 26, 2022, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the decision-making authority for the Project, the Planning Commission has reviewed and considered

the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The administrative record has been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(2) The Project is categorically exempt from the requirements of the California Environmental Quality Act ("CEQA") pursuant to Section 15332 (Class 32, Infill Development Projects) of the CEQA Guidelines and meets all of the following conditions:

(a) The Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as with applicable zoning designation and regulations. The proposed Project is located within the BP (Business Park) land use district of the Policy Plan Land Use Map, and the IP (Industrial Park) zoning district. The proposed project is consistent with all applicable Policy Plan (general plan) policies, as well as with the requirements of the City of Ontario Development Code.

(b) The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses. The Project is proposed within the established boundaries of the City of Ontario, on a project site totaling 2.02 acres of land, which is surrounded by established industrial and administrative land uses to the west and east, respectively, and a mix of residential land uses to the north, across Holt Boulevard, and a Union Pacific Railroad right-of way to the south.

(c) The Project site has no value as habitat for endangered, rare, or threatened species. The site is void of any flora or fauna, was previously graded, and is regularly maintained to prevent the growth of weeds and grasses.

(d) Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality. The proposed industrial park land use is similar to, and of no greater impact than other allowed business park and industrial park land uses within the IP (Industrial Park) zoning district.

(e) The site can be adequately served by all required utilities and public services. The site is located within an area that is currently served by all required utilities and public services, including, but not limited to sewer, water, gas, electric, and municipal solid waste services; and

(3) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

(4) The determination of CEQA exemption reflects the independent judgment of the Planning Commission.

SECTION 2: *Housing Element Compliance.* Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 3: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as “ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 4: *Concluding Facts and Reasons.* Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 through 3, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Project is located within the BP (Business Park) land use district of the Policy Plan Land Use Map, and the IP (Industrial Park) zoning district. The development standards and conditions

under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.

(2) ***The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located.*** The Project has been designed consistent with the requirements of the City of Ontario Development Code and the IP (Industrial Park) zoning district, including standards relative to the particular land use proposed (industrial building), as-well-as building intensity, building, and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions.

(3) ***The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed project.*** The Development Advisory Board has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: [i] the purposes of the Development Code are maintained; [ii] the project will not endanger the public health, safety or general welfare; [iii] the project will not result in any significant environmental impacts; [iv] the project will be in harmony with the area in which it is located; and [v] the project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan.

(4) ***The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development.*** The proposed Project has been reviewed for consistency with the general development standards and guidelines of the Development Code that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking and loading spaces, parking lot dimensions, design and landscaping, bicycle parking, on-site landscaping, and fences and walls, as-well-as those development standards and guidelines specifically related to the particular land use being proposed (industrial building). As a result of this review, the Planning Commission has determined that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the development standards and guidelines described in the Development Code.

SECTION 5: Planning Commission Action. Based upon the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set

forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 6: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 7: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 8: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26th day of July 2022, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Jim Willoughby
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. PC22-_____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on July 26, 2022, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**File No. PDEV21-020
Departmental Conditions of Approval**

(Departmental conditions of approval to follow this page)

Date Prepared: 7/7/2022

File No: PDEV21-020

Project Description: A hearing to consider a Development Plan to construct a 45,000-square-foot industrial building on 2.02 acres of land, located at 1044 and 1050 East Holt Boulevard, within the IP (Industrial Park) zoning district. (APNs: 1049-131-15 and 1049-131-016); **submitted by HK Ventures, Inc.**

Prepared By: Jeanie Irene Aguilo, Associate Planner
Phone: 909.395.2418 (direct)
Email: jaguilo@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits.

(a) Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

2.2 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility, and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.3 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b) Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

(c) Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(d) Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

2.4 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

2.5 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b) All drive approaches shall be provided with an enhanced pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.

(c) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(d) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

(e) Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).

(f) Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11).

2.6 Outdoor Loading and Storage Areas.

(a) Loading facilities shall be designed and constructed pursuant to Development Code Division 6.03 (Off-Street Parking and Loading).

(b) Areas designated for off-street parking, loading, and vehicular circulation and maneuvering, shall not be used for the outdoor storage of materials or equipment.

(c) Outdoor loading and storage areas, and loading doors, shall be screened from public view pursuant to the requirements of Development Code Paragraph 6.02.025.A.2 (Screening of Outdoor Loading and Storage Areas, and Loading Doors) Et Seq.

(d) Outdoor loading and storage areas shall be provided with gates that are view-obstructing by one of the following methods:

(i) Construct gates with a perforated metal sheet affixed to the inside of the gate surface (50 percent screen); or

(ii) Construct gates with minimum one-inch square tube steel pickets spaced at maximum 2-inches apart.

(e) The minimum gate height for screen wall openings shall be established based upon the corresponding wall height, as follows:

Screen Wall Height	Minimum Gate Height
14 feet:	10 feet
12 feet:	9 feet
10 feet:	8 feet
8 feet:	8 feet
6 feet:	6 feet

2.7 Site Lighting.

(a) All off-street parking facilities shall be provided with nighttime security lighting pursuant to Ontario Municipal Code Section 4-11.08 (Special Residential Building Provisions) and Section 4-11.09 (Special Commercial/Industrial Building Provisions), designed to confine emitted light to the parking areas. Parking facilities shall be lighted from sunset until sunrise, daily, and shall be operated by a photocell switch.

(b) Unless intended as part of a master lighting program, no operation, activity, or lighting fixture shall create illumination on any adjacent property.

2.8 Mechanical and Rooftop Equipment.

(a) All exterior roof-mounted mechanical, heating and air conditioning equipment, and all appurtenances thereto, shall be completely screened from public view by parapet walls or roof screens that are architecturally treated so as to be consistent with the building architecture.

(b) All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.

2.9 Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

2.10 Signs. All Project signage shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations).

2.11 Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

2.12 Environmental Review.

(a) The proposed project is categorically exempt from the requirements of the California Environmental Quality Act of 1970 (CEQA), as amended, and the Guidelines promulgated thereunder, pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines, and meets all of the following conditions:

(i) The Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations;

(ii) The proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses;

(iii) The project site has no value as habitat for endangered, rare, or threatened species;

(iv) Approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and

(v) The Project site can be adequately served by all required utilities and public services.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

2.13 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.14 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Exemption ("NOE") filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act ("CEQA"). The filing of a NOE is voluntary; however, failure to provide said fee within the time specified will result in the extension of the statute of limitations for the filing of a CEQA lawsuit from 30 days to 180 days.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

2.15 Additional Requirements.

(a) A Lot Line Adjustment shall be reviewed and approved by the City prior to building permit issuance.

(b) Provide a minimum 8-foot-tall decorative masonry block screen wall along the west and south property lines, which matches the architectural design of the building.

(c) The decorative masonry block screen wall proposed along the east side of the truck yard shall be 14 feet in height, so as to full screen the view of tractors, trailers, and loading doors from view of Cucamonga Avenue.

(d) The employee break area shall include an overhead trellis as shown on the site plan. Final design shall be subject to Planning Director review and approval.

(e) Architectural projections into the required setback shall not increase floor area. Final design shall be subject to Planning Director review and approval.

(f) A truck backing and maneuvering area having a minimum clear depth of 120 feet shall be maintained in front of the dock-high loading door area.

(g) Final sets of plans shall be provided after project approval per the directions to be provided by the Planning Department.

(h) Final project details, including but not limited to architecture, grading, landscaping, and recreation facilities, shall be subject to review and approval as part of the Plan Check process.

(i) All conditions of approval from all other City agencies and departments shall be complied with.



**ENGINEERING DEPARTMENT
CONDITIONS OF APPROVAL**

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Broadband Operations & Investment and Revenue Resources Department Conditions incorporated)

<input checked="" type="checkbox"/> DEVELOPMENT PLAN <input type="checkbox"/> OTHER	<input type="checkbox"/> PARCEL MAP <input type="checkbox"/> TRACT MAP <input type="checkbox"/> FOR CONDOMINIUM PURPOSES
PROJECT FILE NO. <u>PDEV21-020</u> RELATED FILE NO(S). _____	
<input checked="" type="checkbox"/> ORIGINAL <input type="checkbox"/> REVISED: <u> </u> / <u> </u> / <u> </u>	

CITY PROJECT ENGINEER & PHONE NO: David Zurita (909) 395-2155

CITY PROJECT PLANNER & PHONE NO: Jeanie I. Aguilo (909) 395-2418

DAB MEETING DATE: 07/18/2022

PROJECT NAME / DESCRIPTION: PDEV21-020 – A Development Plan to construct one (1) industrial building totaling 45,000 square feet on 1.98 acres of land, within the IP (Industrial Park) zoning district (APN: 1049-131-15 and -016).

LOCATION: SWC of Holt and Cucamonga

APPLICANT: HK Ventures Inc

REVIEWED BY: Raymond Lee 6/29/22
 Raymond Lee, P.E. Date
 Assistant City Engineer

APPROVED BY: [Signature] 7-1-22
 Khoi Do, P.E. Date
 City Engineer



accompanying security as required, or complete all public improvements.

- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ci.ontario.ca.us) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.
- 1.13 Ontario Ranch Developments:
 - 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
 - 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
 - 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions: _____

2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:

**A. GENERAL
 (Permits includes Grading, Building, Demolition and Encroachment)**

- 2.01 Record Parcel Map/Tract Map No. _____ pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 Submit a PDF of the recorded map to the City Engineer's office.
- 2.03 **Note that the subject parcels are recognized parcels in the City of Ontario per Map of Bon View Tract Lots 1-2 recorded in the County of San Bernardino Map Book 18 Page 24.**
- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of March 4, 1972.
- 2.05 **Apply for a:**
 - Certificate of Compliance with a Record of Survey;
 - Lot Line Adjustment (Record a Conforming Deed with the County of San Bernardino within six months of the recordation of the Lot Line Adjustment to conform the new LLA legal description. Submit a copy of the recorded Conforming Deed to the Engineering Department.);**
 - Make a Dedication of Easement.



- 2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.

- 2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.

- 2.08 **Submit a soils/geology report.**

- 2.09 **Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:**
 - State of California Department of Transportation (Caltrans)
 - San Bernardino County Road Department (SBCRD)
 - San Bernardino County Flood Control District (SBCFCD)
 - Federal Emergency Management Agency (FEMA)
 - Cucamonga Valley Water District (CVWD) for sewer/water service
 - United States Army Corps of Engineers (USACE)
 - California Department of Fish & Game
 - Inland Empire Utilities Agency (IEUA)
 - Other: Union Pacific Railroad**

- 2.10 **Dedicate to the City of Ontario the right-of-way described below:**
 1. **17-foot street dedication along entire property frontage of Holt Boulevard per the West Valley Connector project final plans (reference M-1254, E202000865).**
 2. **Property line corner 'cut-back' required at the south-west corner of the Holt Boulevard and Cucamonga Avenue intersection.**
 3. **Right-of-way necessary to construct offset cul-de-sac at the southeast corner of the property at Cucamonga Avenue.**

- 2.11 Dedicate to the City of Ontario the following easement(s): _____

- 2.12 **Vacate the following street(s) and/or easement(s):**
 1. **All interfering on-site easements shall be quitclaimed, vacated, and/or submit non-interference letter from affected owner/utility company.**

- 2.13 Ontario Ranch Developments:
 - 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.
 - 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust



control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.

3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall.

- 2.14 **Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at 100% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.**
- 2.15 **The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.**
- 2.16 **Pay all Development Impact Fees (DIF) to the Building Department. Storm Drain Development Impact Fee, approximately \$45,090.00, shall be paid to the Building Department. Final fee shall be determined based on the approved site plan.**
- 2.17 **Other conditions:**
 - a. **Submit a Final Utilities System Map (USM) as part of the precise grading plan submittal that conforms to the Conceptual USM emailed on 3/23/2022 and meets all the of the City's USM requirements. These requirements include to show and label all existing and proposed utilities (including all appurtenances such as backflow devices, DCDAs, etc.), sizes, points of connection, and any easements. The final utility design shall comply with all Divisions of Drinking Water (CCR §64572) Separation Requirements.**

B. PUBLIC IMPROVEMENTS
 (See attached Exhibit 'A' for plan check submittal requirements.)

- 2.18 **Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):**

Improvement	Holt Blvd.	Cucamonga Ave.	Street 3	Street 4
Curb and Gutter	<input type="checkbox"/> New; ___ ft. from C/L <input checked="" type="checkbox"/> Replace damaged^{2.18.a} <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New; 24 ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
AC Pavement	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input checked="" type="checkbox"/> Widen to achieve a half street of 24-ft paved	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing



Drive Approach	<input type="checkbox"/> New <input checked="" type="checkbox"/> Abandon existing approach	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Sidewalk	<input type="checkbox"/> New <input checked="" type="checkbox"/> Replace damaged panels 2.18.a	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
ADA Access Ramp	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace 2.18.a	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Parkway	<input type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/ temporary irrigation)	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
Raised Landscaped Median	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Fire Hydrant	<input checked="" type="checkbox"/> New <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Sewer (see Sec. 2.C)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Water (see Sec. 2.D)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Services (Fire, Irrigation, Domestic)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Street Light (see Sec. 2.F)	<input checked="" type="checkbox"/> New <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify	<input type="checkbox"/> New <input type="checkbox"/> Modify	<input type="checkbox"/> New <input type="checkbox"/> Modify	<input type="checkbox"/> New <input type="checkbox"/> Modify



	existing	existing	existing	existing
Storm Drain (see Sec. 2G)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Relocate outlet ^{2.18.c}	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Fiber Optics (see Sec. 2K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
Removal of Improvements	<input checked="" type="checkbox"/> Fire Hydrant ^{2.30.d}			
Other Improvements				

Specific notes for improvements listed in item no. 2.18, above:

- a. If at the time of construction, the West Valley Connector Project improvements have not been constructed along Holt Boulevard, the applicant is responsible to build interim frontage improvements necessary to accommodate pedestrian access. (i.e ADA ramp, sidewalk, etc.)
- b. The applicant/ developer shall be responsible for relocating/abandoning all existing water meters, fire hydrants, pull boxes, and other appurtenances to their ultimate location per the Parson's final plans for the West Valley Connector.
- c. The applicant/developer shall design and construct the extension and relocation of the existing storm drain outlet on Cucamonga Avenue. The outlet shall be at its ultimate location.
- d. The applicant/developer shall design and construct an offset cul-de-sac at the termination of the south end of Cucamonga Avenue, including street improvements (sidewalk, curb and gutter, parkway & irrigation, etc.) past the project frontage, connecting to the existing improvements on the East side of Cucamonga Avenue.
- e. Construct new drive approach for the property affected by the construction of the new cul-de-sac.

- 2.19 Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s):
 - a. Cucamonga Avenue along entire frontage from centerline to gutter including the entire cul-de-sac. Exact limits to be determined during plan check.
 - b. Holt Boulevard along entire frontage a minimum of 12-feet adjacent to the gutter. Exact limits to be determined during plan check.
- 2.20 Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.
- 2.21 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide water service sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.
- 2.22 Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately , for undergrounding of utilities in accordance with Section 7-7.302.e of the City's Municipal Code.



2.23 Other conditions: _____

C. SEWER

2.24 An 8-inch sewer main is available for connection by this project in Cucamonga Avenue. (Ref: Sewer plan bar code: S11546)

2.25 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately _____ feet away.

2.26 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.

2.27 Other conditions:

- a. Install a sewer lateral connected to the 8-inch City of Ontario sewer main in Cucamonga Avenue with a monitoring manhole located at the back of the property line.
- b. The Occupant/Applicant shall apply for a Wastewater Discharge Permit for their Establishment, and shall comply with all the requirements of the Wastewater Discharge Permit (https://www.ontarioca.gov/sites/default/files/Ontario-Files/Municipal-Utilities-Company/1_commercial_and_industrial_wastewater_permit_application_fillable.pdf). Requirements of the Wastewater Discharge Permit may include, but not limited to: Installation of wastewater pretreatment equipment, such as grease interceptors or clarifiers. For wastewater permit application questions, please contact: omucenvironmental@ontarioca.gov

D. WATER

2.28 An 8-inch water main is available for connection by this project in Cucamonga Avenue.

2.29 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately _____ feet away.

2.30 Other conditions:

- a. Install two potable water services (one for the building's use and the other for irrigation use) each with a meter and backflow prevention device that meets the current City Standards.
- b. Install one fire water service with a double check detector assembly that meets the current City Standards.
- c. Install one fire hydrant in Cucamonga Avenue along the Project's frontage within 100' of the proposed fire department connection.
- d. The existing fire hydrant at the SWC of Cucamonga Avenue and Holt Boulevard shall be relocated by abandoning the existing fire hydrant service back to the main and installing a new fire hydrant service connected to the 12-inch potable water main in Holt Boulevard. The fire hydrant shall be located a minimum of 5' back of the BCR.

E. RECYCLED WATER

2.31 A _____ inch recycled water main is available for connection by this project in _____. (Ref: Recycled Water plan bar code: _____)

2.32 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.

2.33 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.



- 2.34 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.

Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.

- 2.35 Other conditions: _____

F. TRAFFIC / TRANSPORTATION

- 2.36 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer:
 - 1. On-site and off-site circulation
 - 2. Traffic level of service (LOS) at 'build-out' and future years
 - 3. Impact at specific intersections as selected by the City Engineer

- 2.37 New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.

- 2.38 Other conditions:
 - a. **Property frontage along Holt Boulevard and Cucamonga Avenue shall be signed "No Parking Anytime".**
 - b. **Access to Cucamonga Avenue shall be restricted to right-in/right-out access only once the West Valley Connector Project raised median is installed along Holt.**
 - c. **The applicant/developer shall be responsible to design and construct in-fill public streetlights and a potential new service along it project frontage on Holt Boulevard and Cucamonga Avenue. Street lighting shall be LED-type and in accordance with City's Traffic and Transportation Design Guidelines. Streetlights on Holt shall be installed at their ultimate location.**
 - d. **All landscaping, block walls, and other obstructions shall be compatible with the stopping sight distance requirements per City of Ontario Standard Drawing No. 1309.**

G. DRAINAGE / HYDROLOGY

- 2.39 A _____ inch storm drain main is available to accept flows from this project in _____.
(Ref: Storm Drain plan bar code: _____)

- 2.40 **Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.**

- 2.41 An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100 year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.

- 2.42 Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.

- 2.43 Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.

- 2.44 Other conditions:



- a. Provide approval from Union Pacific Railroad accepting additional post development runoff; or design and construct a storm water detention facility on the project site where the 100-year post-development peak flow to the railroad right-of-way shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.

H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)

- 2.45 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels.
If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.
Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.
- 2.46 Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: <http://www.sbcountry.gov/dpw/land/npdes.asp>.
- 2.47 Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.
- 2.48 Other conditions:

J. SPECIAL DISTRICTS

- 2.49 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.
- 2.50 Other conditions: _____

K. FIBER OPTIC

- 2.51 A _____ fiber optic line is available for connection by this project in _____.
(Ref: Fiber Optic plan bar code: _____)
- 2.52 Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole per provided broadband exhibit.



- 2.53 Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Broadband Operations Department at (909) 395-2000, regarding this requirement.

3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:

- 3.01 Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.
- 3.02 Complete all requirements for recycled water usage.
 - 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.
 - 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.
 - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.
- 3.03 The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.
- 3.04 Ontario Ranch Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.
- 3.05 Confirm payment of all Development Impact Fees (DIF) to the Building Department.
- 3.06 Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).

4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:

- 4.01 Complete all Conditions of Approval listed under Sections 1-3 above.
- 4.02 Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.
- 4.03 The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall include a completed Acceptance and Bond Release Checklist, state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.
- 4.04 Submit record drawings (PDF) for all public improvements identified within Section 2 of these Conditions of Approval.



EXHIBIT 'A'

**ENGINEERING DEPARTMENT
First Plan Check Submittal Checklist**

Project Number: PDEV21-020

The following items are required to be included with the first plan check submittal:

1. **A copy of this check list**
2. **Payment of fee for Plan Checking**
3. **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4. **One (1) copy of project Conditions of Approval**
5. **Include a PDF (electronic submittal) of each required improvement plan at every submittal.**
6. **Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).**
7. **Three (3) sets of Public Street improvement plan with street cross-sections**
8. **Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)**
9. **Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)**
10. **Four (4) sets of Public Sewer improvement plan**
11. **Five (5) sets of Public Storm Drain improvement plan**
12. **Three (3) sets of Public Street Light improvement plan**
13. **Three (3) sets of Signing and Striping improvement plan**
14. **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
15. **Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)**
16. **Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.**
17. **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**
18. **One (1) copy of Hydrology/Drainage study**
19. **One (1) copy of Soils/Geology report**
20. **Payment for Final Map/Parcel Map processing fee**

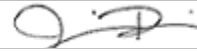


- 21. Three (3) copies of Final Map/Parcel Map
- 22. One (1) copy of approved Tentative Map
- 23. **One (1) copy of Preliminary Title Report (current within 30 days)**
- 24. One (1) copy of Traverse Closure Calculations
- 25. **One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.**
- 26. Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
- 27. Other: _____

CITY OF ONTARIO
LANDSCAPE PLANNING DIVISION
 303 East "B" Street, Ontario, CA 91764

CONDITIONS OF APPROVAL

Sign Off



Jamie Richardson, Sr. Landscape Planner

06/10/22

Date

Reviewer's Name:

Jamie Richardson, Sr. Landscape Planner

Phone:

(909) 395-2615

D.A.B. File No.:

PDEV21-020

Case Planner:

Jeanie Aguilo

Project Name and Location:

Twins Industrial Park
 SWC Holt Blvd. and Cucamonga Ave.

Applicant/Representative:

HK Ventures, Inc.
 3 Pointe Dr., Suite 217
 Brea, CA 92821

<input checked="" type="checkbox"/>	Preliminary Plans (dated 5/10/2022) meet the Standard Conditions for New Development and have been approved considering that the following conditions below be met upon submittal of the landscape construction documents.
<input type="checkbox"/>	Preliminary Plans (dated) have not been approved. Corrections noted below are required before Preliminary Landscape Plan approval.

A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED INCOMPLETE.
 Landscape construction plans with plan check number may be emailed to: landscapeplancheck@ontarioca.gov
DIGITAL SUBMITTALS MUST BE 10MB OR LESS.

Civil/ Site Plans

1. Replacement and mitigation for removed trees shall equal the trunk diameter of heritage trees removed per the Development Code Tree Preservation Policy and Protection Measures, section 6.05.020. **There is 104" of heritage tree diameter that requires mitigation; see #2 for mitigation:**
2. Show on demo plans and landscape construction plans trees to be preserved, removed or mitigation measures for trees removed, such as:
 - a. New 15 gallon trees min 1" diameter trunk, in addition to trees required. **Planting of 104 15-gallon trees in addition to required trees.**
 - b. New 24" box trees min 1.5" diameter trunk, in addition to trees required. **Planting of 69 24" box trees in addition to required trees.**
 - c. Upsizing trees on the plan one size larger such as 15 gallon to 24" box, or 24" to 36" box size. **Identify on plan.**
 - d. Monetary value of the trees removed as identified in the "Guide for Plant Appraisal," approved certified arborist plant appraiser, or may be equal to the value of the installation cost of planting, fertilizing, staking, and irrigating 15-gallon trees (100\$ each) to the City of Ontario Historic Preservation Fund for city tree planting or city approved combination of the above items. **\$10,400 in monetary value or a combination of A-D.**

Landscape Plans

3. After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections.

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT



Project File No.: PDEV21-020
 Address: 1044 & 1050 East Holt Blvd.
 APN: 1049-131-15 & 16
 Existing Land Use: Vacant
 Proposed Land Use: Development Plan to construct two industrial buildings totaling 50,121 SF
 Site Acreage: 2.02 Proposed Structure Height: 40 FT
 ONT-IAC Project Review: n/a
 Airport Influence Area: ONT

Reviewed By: Lorena Mejia
 Contact Info: 909-395-2276
 Project Planner: Jeanie Aguilo
 Date: 8/17/2021
 CD No.: 2021-034
 PALU No.: n/a

The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="checkbox"/> High Terrain Zone	<input checked="" type="checkbox"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input checked="" type="checkbox"/> FAA Notification Surfaces	<input type="checkbox"/> Recorded Overflight Notification
<input type="radio"/> Zone 2	<input checked="" type="checkbox"/> 65 - 70 dB CNEL	<input checked="" type="checkbox"/> Airspace Obstruction Surfaces	<input type="radio"/> Real Estate Transaction Disclosure
<input type="checkbox"/> Zone 3	<input type="checkbox"/> 60 - 65 dB CNEL	<input type="checkbox"/> Airspace Avigation Easement Area	
<input type="radio"/> Zone 4		Allowable Height: 100 FT	
<input type="radio"/> Zone 5			

The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1
 Zone 2
 Zone 3
 Zone 4
 Zone 5
 Zone 6

Allowable Height: _____

CONSISTENCY DETERMINATION

This proposed Project is: Exempt from the ALUCP
 Consistent
 Consistent with Conditions
 Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

Airport Planner Signature: _____



CITY OF ONTARIO

MEMORANDUM

TO: Jeanie Irene Aguilo, Associate Planner
Planning Department

FROM: Mike Gerken, Deputy Fire Chief/Fire Marshal
Fire Department

DATE: June 23, 2021

SUBJECT: PDEV21-020 - A Development Plan to construct a 45,000-square-foot industrial building on 2.02 acres of land, located at 1044 and 1050 East Holt Boulevard, within the IP (Industrial Park) zoning district; (APNs: 1049-131-15 and 1049-131-016) submitted by HK Ventures, Inc.

-
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.

SITE AND BUILDING FEATURES:

- A. 2019 CBC Type of Construction: Type V-A
- B. Type of Roof Materials: Ordinary
- C. Ground Floor Area(s): 14,460 Sq. Ft.
- D. Number of Stories: 4
- E. Total Square Footage: 57,060 Sq. Ft.
- F. 2019 CBC Occupancy Classification(s): R-1

CONDITIONS OF APPROVAL:

1.0 GENERAL

- ☒ 1.1 The following are the Ontario Fire Department (“Fire Department”) requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards (“Standards.”) It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at www.ontarioca.gov/Fire/Prevention.
- ☒ 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

2.0 FIRE DEPARTMENT ACCESS

- ☒ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- ☒ 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25’) inside and forty-five feet (45’) outside turning radius per Standard #B-005.
- ☒ 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150’) in length shall have an approved turn-around per Standard #B-002.
- ☒ 2.4 Access drive aisles which cross property lines shall be provided with CC&Rs, access easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of building plan check.
- ☒ 2.5 "No Parking-Fire Lane" signs and /or red painted curbs with lettering are required to be installed in interior access roadways, in locations where vehicle parking would obstruct the minimum clear width requirement. Installation shall be per Standard #B-001.
- ☒ 2.6 Security gates or other barriers on fire access roadways shall be provided with a Knox brand key switch or padlock to allow Fire Department access. See Standards #B-003, B-004 and H-001.
- ☒ 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-four (24) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by fire department and other emergency services.

3.0 WATER SUPPLY

- ☒ 3.1 The required fire flow per Fire Department standards, based on the 2019 California Fire Code, Appendix B, is 3250 gallons per minute (g.p.m.) for 4 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.
- ☒ 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.
- ☒ 3.4 The water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

4.0 FIRE PROTECTION SYSTEMS

- ☒ 4.1 On-site private fire hydrants are required per Standard #D-005, and identified in accordance with Standard #D-002. Installation and locations(s) are subject to the approval of the Fire Department. An application with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- ☒ 4.2 Underground fire mains which cross property lines shall be provided with CC & R, easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of fire department plan check. The shared use of private fire mains or fire pumps is allowable only between immediately adjacent properties and shall not cross any public street.
- ☒ 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 13. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- ☒ 4.4 Wood frame buildings that are to be sprinkled shall have these systems in service (but not necessarily finalized) before the building is enclosed.
- ☒ 4.5 Fire Department Connections (FDC) shall be located on the address side of the building within one hundred fifty feet (150') of a public fire hydrant on the same side of the street. Provide identification for all fire sprinkler control valves and fire department connections per Standard #D-007. Raised curbs adjacent to Fire Department connection(s) shall be painted red, five feet either side, per City standards.
- ☒ 4.6 A fire alarm system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 72. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.

- ☒ 4.7 Portable fire extinguishers are required to be installed prior to occupancy per Standard #C-001. Please contact the Fire Prevention Bureau to determine the exact number, type and placement required.

5.0 BUILDING CONSTRUCTION FEATURES

- ☒ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ☒ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- ☒ 5.3 Single station smoke alarms and carbon monoxide alarms are required to be installed per the California Building Code and the California Fire Code.
- ☒ 5.4 Multiple unit building complexes shall have building directories provided at the main entrances. The directories shall be designed to the requirements of the Fire Department, see Section 9-1 6.06 of the Ontario Municipal Code and Standard #H-003.
- ☒ 5.6 Knox ® brand key-box(es) shall be installed in location(s) acceptable to the Fire Department. All Knox boxes shall be monitored for tamper by the building fire alarm system. See Standard #H-001 for specific requirements.



CITY OF ONTARIO

MEMORANDUM

TO: Jeanie Irene Aguilo, Associate Planner

FROM: Officer Bill Lee, Police Department

DATE: June 16, 2021

SUBJECT: PDEV21-020- A Development Plan to construct a 45,000-square-foot industrial building on 2.02 acres of land, located at 1044 and 1050 East Holt Boulevard, within the IP (Industrial Park) zoning district; (APNs: 1049-131-15 and 1049-131-016) submitted by HK Ventures, Inc.

The “Standard Conditions of Approval” contained in Resolution No. 2017-027 apply. The applicant shall read and be thoroughly familiar with these conditions, including, but not limited to, the requirements below.

- Required lighting for all walkways, driveways, doorways, parking lots, hallways and other areas used by the public shall be provided. Lights shall operate via photosensor. Photometrics shall be provided to the Police Department and include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- Rooftop addresses shall be installed on the buildings as stated in the Standard Conditions. The numbers shall be at a minimum 6 feet tall and 2 foot wide, in reflective white paint on a flat black background, and oriented with the bottom of the numbers towards the addressed street. Associated letters shall also be included.
- First floor common stairwells shall be constructed to either allow for visibility through the stairwell risers or to prohibit public access to the areas behind stairwells.
- The Applicant shall comply with construction site security requirements as stated in the Standard Conditions.

The Applicant is invited to contact Officer Bill Lee at (909) 408-1672 with any questions or concerns regarding these conditions.

FILE NOS: PDEV21-033 and PVAR21-006

SUBJECT: A Development Plan (File No. PDEV21-033) to construct one 2,800-square-foot drive-thru restaurant (The Habit Burger Grill) and 500-square-foot outdoor patio, in conjunction with a Variance (File No. PVAR21-006) to reduce the front drive aisle setback along an arterial street from 20 feet to 13 feet, on 0.42-acre of land located at 1610 East Fourth Street, within the CC (Community Commercial) zoning district (APNs: 0110-181-11 and 0110-181-19); **submitted by Hannibal Petrossi.**


PROPERTY OWNER: 1600 E. 4th Street, LLC

RECOMMENDED ACTION: That the Planning Commission consider and approve File Nos. PDEV21-033 and PVAR21-006, pursuant to the facts and reasons contained in the staff report and attached resolutions, and subject to the conditions of approval contained in the attached departmental reports.

PROJECT SETTING: The project site is comprised of 0.42-acre of land located at 1610 East Fourth Street, within the CC (Community Commercial) zoning district, and is depicted in Figure 1: Project Location Map, below. The property to the north of the Project site is within the HDR-45 (High-Density Residential – 25.1 to 45.0 du/ac) zoning district and the ICC Overlay (Interim Community Commercial Overlay) and is developed with a drive-thru and gas station. The property to the east is within the CC (Community Commercial) zoning district and is developed with a drive-thru restaurant. The property to the south is within the CC (Community Commercial) zoning district and is developed with service and retail commercial land uses. The property to the west is within the CC (Community Commercial) zoning district and is



Figure 1: Project Location Map

Case Planner:	Alexis Vaughn
Planning Director Approval:	
Submittal Date:	09/14/2021

Hearing Body	Date	Decision	Action
DAB	July 18, 2022	Approval	Recommend
PC	July 26, 2022		Final
CC			

developed with a motel. The existing surrounding land uses, zoning, and general plan and specific plan land use designations are summarized in the “Surrounding Zoning & Land Uses” table located in the Technical Appendix of this report.

PROJECT ANALYSIS:

(1) Background — The Applicant is requesting to construct a 2,800-square-foot fast food drive-thru restaurant and 500-square-foot outdoor patio to accommodate The Habit Burger Grill restaurant. Additionally, the Applicant is requesting Variance approval to reduce the required front drive aisle setback along an arterial street from 20 feet to 13 feet, which will allow for the drive-thru facility to be constructed. The site has remained vacant since it was graded in 1993. The surrounding commercial shopping center was primarily constructed between the early 1990's and early 2000's, and the anchor tenant space was previously occupied by K-Mart, which closed in 2018. The shopping center has remained under-utilized over the past few decades, and the Project is part of the property owners' effort to catalyze revitalization of the commercial shopping center.

(2) Site Design/Building Layout — The Project site is located at the northwest corner of the existing commercial shopping center and occupies two parcels (see Exhibit A—Project Aerial). The westerly parcel primarily contains the proposed building and drive-thru canopy. The remaining parcel, which includes a thin strip of land at the northerly edge of the Project site and large rectangle on the eastern portion of the Project site, primarily contains the proposed landscape setback, drive-thru lane, and parking lot. The entrance to the drive-thru is proposed near the center of the Project site, accessible from the parking lot, and exits at the southwest corner of the Project site, to the existing drive aisle located along the Project's southerly boundary.

The pedestrian entrance to the building is located on the eastern elevation, and a pedestrian walkway connects the building to the public sidewalk located north of the Project site. The drive-thru order menu boards are proposed along the northern elevation, and the pickup window is proposed along the western elevation of the building (see Exhibit B—Site Plan). The floor plan has been designed with the dining area primarily occupying the eastern one-half of the building, and the kitchen/employee space occupying the western one-half of the building (see Exhibit C—Floor Plan). An outdoor patio (500 square feet) has also been provided on the north side of the building. In addition, a new trash enclosure is proposed for the Project, located south of the site, within the common areas of the shopping center.

(3) Site Access/Circulation — The shopping center is served by one primary and one secondary vehicular access point from Fourth Street. The proposed Project will not alter the location of any shopping center vehicular access points or drive aisles.

(4) Parking — The existing shopping center contains approximately 137,000 square feet of commercial space, consisting of an assortment of retail and service-oriented uses that requires approximately 680 parking spaces. The center has been developed with

726 parking spaces, which are shared between the five existing buildings, providing a parking surplus of 46 spaces. The Covenants, Conditions, and Restrictions ("CC&Rs") for the shopping center include provisions for shared parking and access throughout the center, except for the Project site. As such, the Project has been conditioned to revise the CC&Rs to include the Project site and developed parking spaces, as well as to include the site in the fair share breakdown of property management fees.

The Development Code requires fast food restaurants to provide 13.3 spaces for each 1,000 square feet of gross floor area, with a credit of one parking space for each 24 lineal feet of the drive-thru lane behind the pickup window. The proposed 2,800-square-foot building, with 150 lineal feet of drive-thru lane, would require 31 parking spaces (37 spaces for the building and a credit of 6 spaces for the drive-thru lane), and 14 spaces have been proposed on-site, resulting in a parking deficit of 17 spaces. However, given the existing shopping center's surplus of 46 parking spaces, staff believes that the proposed use, as conditioned, will not adversely affect the overall parking demand within the existing shopping center and sufficient parking will continue to be provided to support both the proposed use and existing tenants.

(5) Architecture — The proposed building has a contemporary architectural design, which incorporates varying roof and parapet heights and a decorative cornice treatment (see Exhibit D—Elevations and Rendering). The exterior walls are treated with a combination of stone veneer, metal, and stucco finishes, with a contrasting color palette in neutral earth tones. The building incorporates a 360-degree stone wainscot, which also covers the entire height of a tower element at the northeast corner of the building. The north elevation includes vine trellises to help soften the appearance of the building and drive-thru lane from Fourth Street. Additionally, all four sides of the building incorporate a metal awning and/or canopy for visual interest. A covered canopy will be provided over the drive-thru pickup window, which has been conditioned to be redesigned during the plan check process, to include a more substantial post/column/beam to better complement the building's architecture.

The mechanical equipment will be roof-mounted and obscured from public view by parapet walls and/or equipment screens, which will incorporate design features consistent with the building's architecture. Staff believes that the proposed Project illustrates the type of high-quality architecture promoted by the Development Code. This is exemplified through use of:

- Articulation in the elevations, incorporating a combination of recessed and popped-out areas;
- Articulation in the building parapet/roof line, which serves to accentuate the building's entries and breaks up large expanses of building wall;
- A mix of exterior materials, finishes, and fixtures; and
- Incorporation of base and top treatments defined by changes in color and materials, and recessed wall areas designed to ensure that the building's massing

and proportion, along with its colors and architectural detailing, are consistent on all building walls, giving a four-sided (360-degree) appearance.

(6) Landscaping — The Project provides landscaping improvements throughout and beyond the Project site, including areas along the street frontage and adjacent to the center's main access drive, around the perimeter of the building, along the drive-thru lane, and around the new trash enclosure. The Development Code requires a minimum of 15 percent landscaping for drive-thru facilities, and the Project site is proposing 23 percent landscape coverage, which exceeds the minimum requirement (see Exhibit E—Landscape Plan). The Project proposes a low garden wall and plantings to screen the drive-thru lane and help shield glare from headlights to adjacent streets. The proposed plant palette includes an array of trees, shrubs, vines, screen plantings, and ground cover, including Engelmann Oak, Chinese Pistache, Brisbane Box, Western Redbud, Crape Myrtle, Bay Laurel, Little Ollie Olive, Fern Pine, Jerusalem Sage, Wynabbie Gem Coast Rosemary, Little John Weeping Bottlebrush, Kangaroo Paw, Lily, Yucca, Trumpet Vine, Silver Carpet Dymondia, and Starfire Verbena, among other plantings.

(7) Signage — The Project will be required to submit a sign plan application prior to the installation of signage at the site. The conceptual sign location and design have been included into the proposed building elevations for reference.

(8) Utilities (drainage, sewer) — Public utilities (water and sewer) are available to serve the Project. Furthermore, the Applicant has submitted a Preliminary Water Quality Management Plan ("PWQMP") which establishes the Project's compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development ("LID") best management practices (BMPs), such as retention and infiltration. The proposed development will not substantially alter the existing drainage pattern. An underground storm drain infiltration system has been proposed to be located under the easterly parking spaces on the Project site.

(9) Variance — The Applicant is requesting approval of a Variance to reduce the required front drive aisle setback along an arterial street (Fourth Street), from 20 to 13 feet. The Project site is odd in that it consists of two parcels – one small parcel that would accommodate a building, which is surrounded by another parcel that would contain a common facility (off-street parking). The entire building must exist on one lot and cannot cross parcel lines, which further impacts the orientation and placement of the drive-thru facility and restaurant building. The 0.42-acre Project site is substandard in terms of lot size for the development of drive-thru facilities, as the Development Code typically requires a minimum one-acre site; however, the Code also allows for a reduction in the site size when integrated within a shopping center, as this Project is. Additionally, an existing drive-thru business in the same shopping center has a drive-thru lane with a 12.5-foot setback from the Fourth Street property line. Due to these substantial site constraints, as well as the orientation of internal drive aisles within the existing shopping center, the Variance

application is necessary to successfully facilitate the related Development Plan application.

Furthermore, Staff believes that the Variance request is consistent with TOP Goal LU3, which promotes flexibility in order to respond to special conditions and circumstances in order to achieve the TOP Vision. Additionally, it is Staff's belief that the proposed development of the Project site will help catalyze revitalization of the underused commercial shopping center by introducing a new, high-quality development. For these reasons, Staff supports granting the Variance request to reduce the front drive-aisle setback in order to allow for the construction of the proposed drive-thru facility.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (general plan), and City Council Priorities components of The Ontario Plan ("TOP"). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

- (1) City Council Goals.
 - Invest in the Growth and Evolution of the City's Economy
 - Operate in a Businesslike Manner
 - Focus Resources in Ontario's Commercial and Residential Neighborhoods
- (2) Vision.

Distinctive Development:

- Commercial and Residential Development
 - Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

- (3) Governance.

Decision Making:

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
 - G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision.

(4) Policy Plan (General Plan)

Land Use Element:

➤ LU1-6 Complete Community: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).

- Goal LU2: Compatibility between a wide range of uses.

Community Economics Element:

- Goal CE1: A complete community that provides for all incomes and stages of life.

- Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.

➤ CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.

➤ CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.

➤ CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.

➤ CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Community Design Element:

- Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

➤ CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.

➤ CD1-2 Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.

➤ CD1-3 Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.

▪ Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.

➤ CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:

- Building volume, massing, and height to provide appropriate scale and proportion;
- A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

➤ CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.

➤ CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

➤ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➤ CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.

➤ CD2-12 Site and Building Signage. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be

designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.

➤ CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

➤ CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.

➤ CD3-3 Building Entrances. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces.

➤ CD3-5 Paving. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.

➤ CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.

▪ Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.

➤ CD5-1 Maintenance of Buildings and Property. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.

HOUSING ELEMENT COMPLIANCE: The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and

found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

ENVIRONMENTAL REVIEW: The Development Plan, File No. PDEV21-033, is categorically exempt from the requirements of the California Environmental Quality Act ("CEQA") pursuant to Section 15332 (Class 32, In-Fill Development) of the CEQA Guidelines, and meets all of the follow conditions:

(1) The Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as with applicable zoning designation and regulations. The proposed Project is located within the GC (General Commercial) land use district of the Policy Plan Land Use Map, and the CC (Community Commercial) zoning district. The proposed project is consistent with all applicable Policy Plan (general plan) policies, as well as with the requirements of the City of Ontario Development Code.

(2) The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses. The Project is proposed within the established boundaries of the City of Ontario, on a project site totaling 0.42-acre of land, which is surrounded by established commercial land uses.

(3) The Project site has no value as habitat for endangered, rare, or threatened species. The site is void of any flora or fauna, was previously graded, and is regularly maintained to prevent the growth of weeds and grasses.

(4) Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality. The proposed drive-thru restaurant is similar to, and of no greater impact than other allowed retail uses and commercial development projects within the CC (Community Commercial) zoning district.

(5) The site can be adequately served by all required utilities and public services. The site is located within an area that is currently served by all required utilities and public services, including, but not limited to sewer, water, gas, electric and municipal solid waste services.

The proposed Variance, File No. PVAR21-006, is categorically exempt from the requirements of CEQA pursuant to Section 15305 (Class 5, Minor Alterations in Land Use Limitations) of the CEQA Guidelines, which consist of minor alterations in land use limitations in areas with an average slope of less than 20 percent and which do not result in any changes in land use or density. The Variance requests a reduction in the front drive aisle setback along an arterial street, from 20 feet to 13 feet, and will not result in the creation of any new parcel.

CONDITIONS OF APPROVAL: See attached department reports.

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

	Existing Land Use	General Plan Designation	Zoning Designation	Specific Plan Land Use
<i>Site:</i>	Vacant	General Commercial (0.4 FAR)	CC (Community Commercial)	N/A
<i>North:</i>	Drive-thru restaurant and gas station	High Density Residential (25.1-45 du/ac), I-10/Grove Interchange Area Overlay	HDR-45, ICC Overlay (Interim Community Commercial)	N/A
<i>South:</i>	Commercial – Services and Retail	General Commercial (0.4 FAR)	CC (Community Commercial)	N/A
<i>East:</i>	Drive-thru restaurant	General Commercial (0.4 FAR)	CC (Community Commercial)	N/A
<i>West:</i>	Motel	General Commercial (0.4 FAR)	CC (Community Commercial)	N/A

General Site & Building Statistics

<i>Item</i>	<i>Proposed</i>	<i>Min./Max. Standard</i>	<i>Meets Y/N</i>
<i>Project Area:</i>	0.42 AC	N/A	N/A
<i>Building Area:</i>	2,800 SF	N/A	N/A
<i>Floor Area Ratio:</i>	0.15	0.4 (Max.)	
<i>Building Height:</i>	28	72 (Max.)	Y

Off-Street Parking:

<i>Type of Use</i>	<i>Building Area</i>	<i>Parking Ratio</i>	<i>Spaces Required</i>	<i>Spaces Provided</i>
<i>Fast Foot Restaurant</i>	2,800 SF	13.3 spaces per 1,000 SF, credit of 1 space per 24 lineal foot of drive-thru lane	31	14

Exhibit A—PROJECT AERIAL

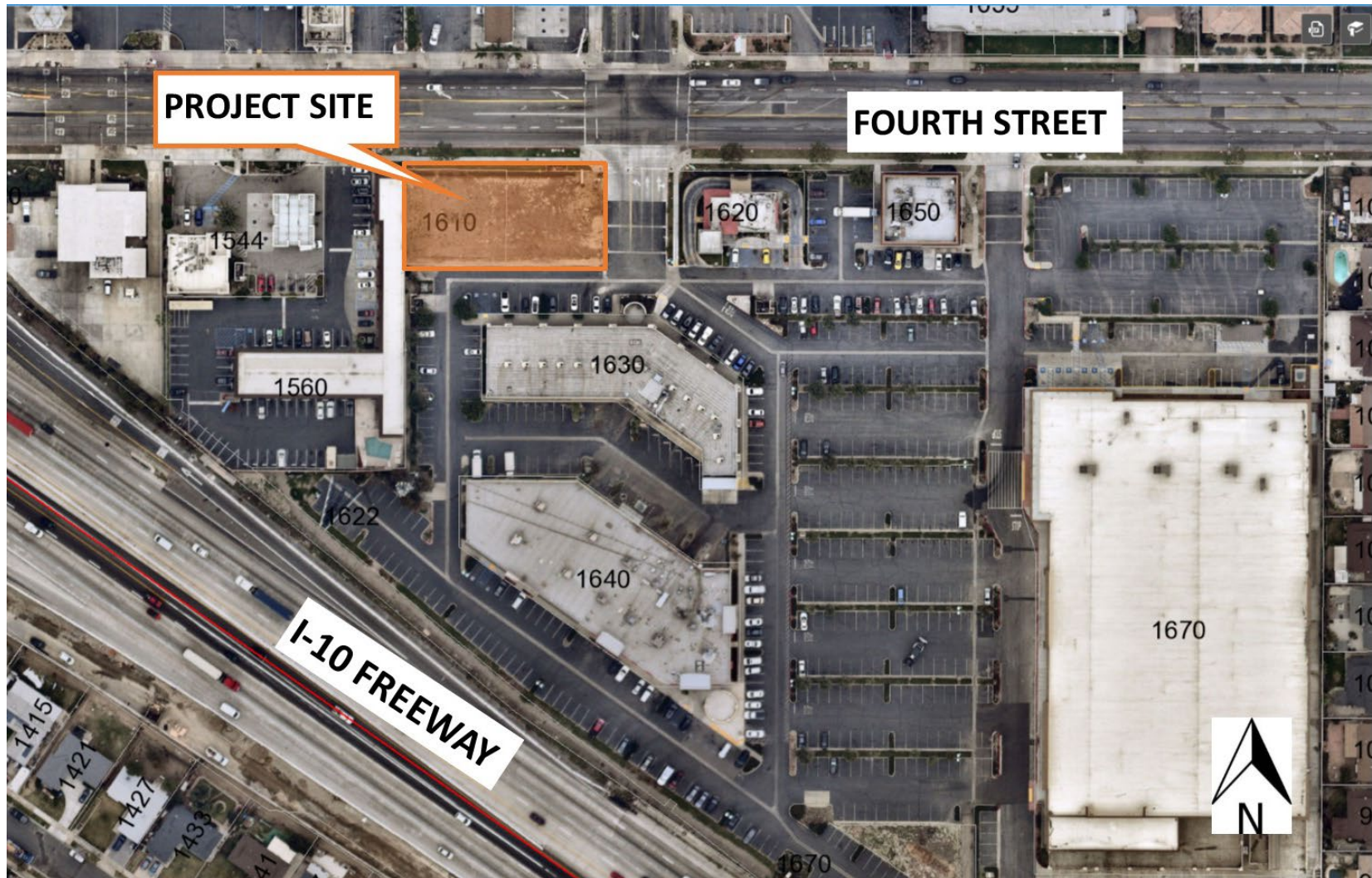


Exhibit B—SITE PLAN

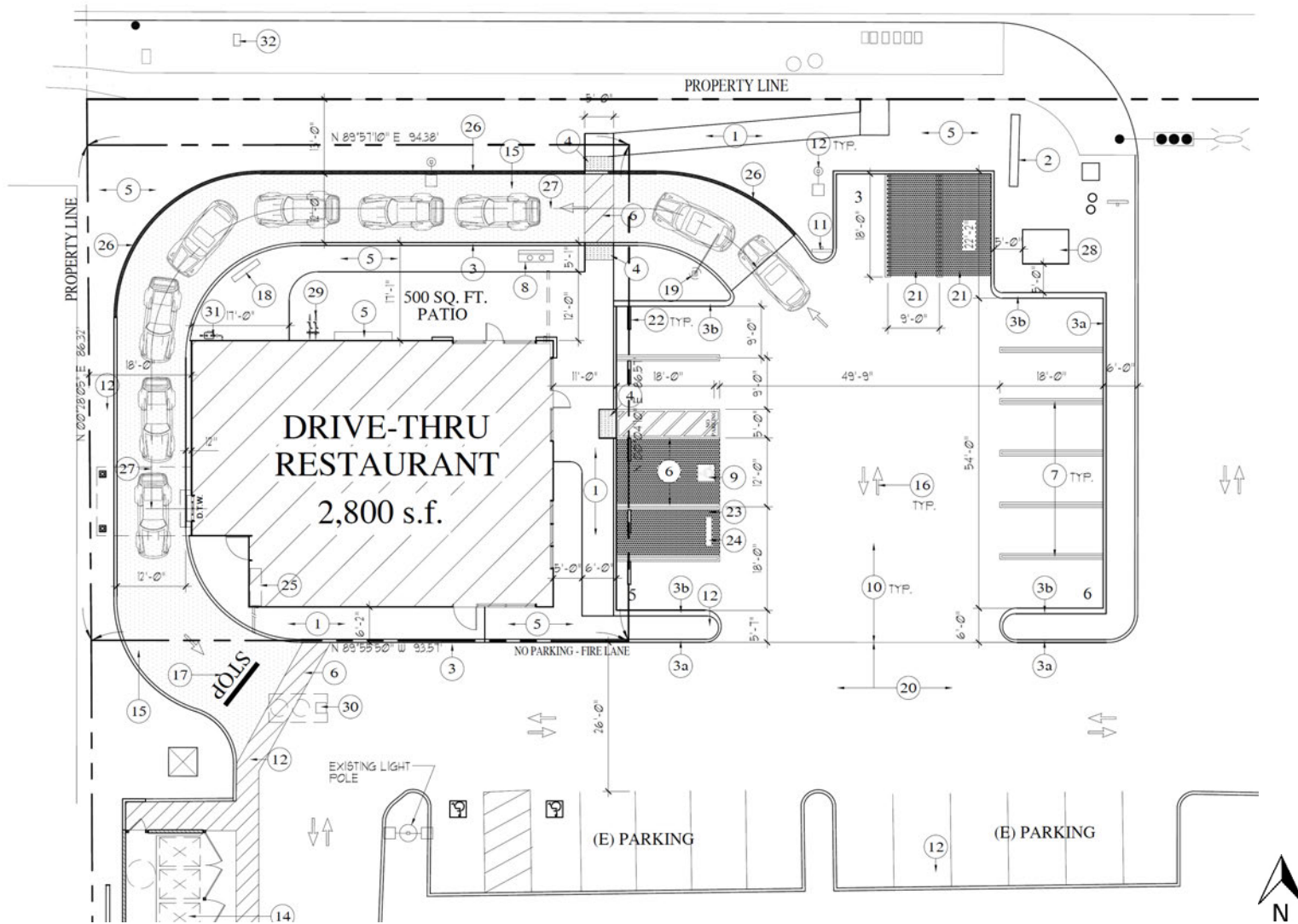


Exhibit C—FLOOR PLAN



Exhibit D—ELEVATIONS AND RENDERING

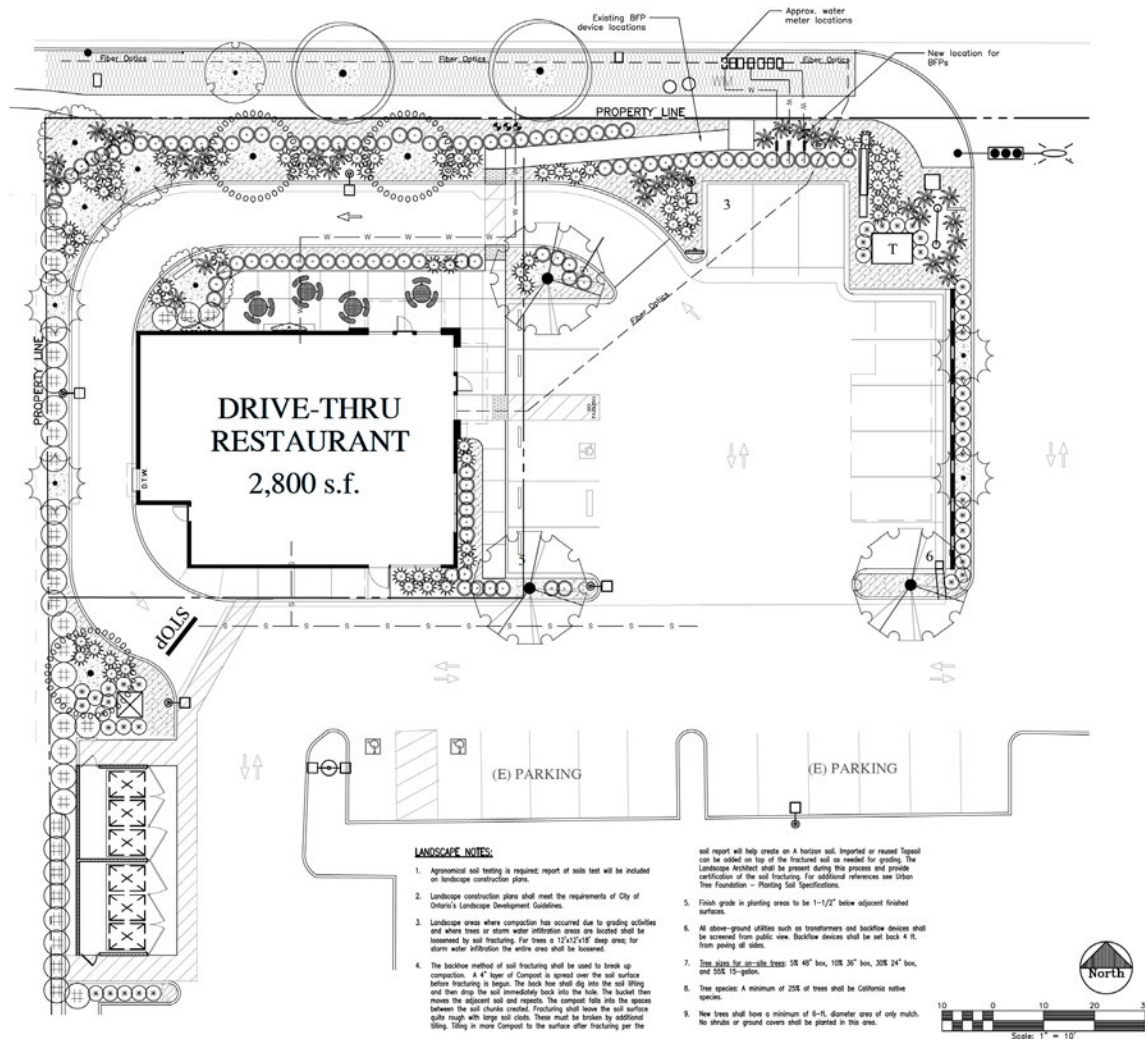


Exhibit D—ELEVATIONS AND RENDERING CONTINUED



Northeast Corner

Exhibit E—LANDSCAPE PLAN



RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PVAR21-006, A VARIANCE TO REDUCE THE FRONT DRIVE AISLE SETBACK ALONG AN ARTERIAL STREET FROM 20 FEET TO 13 FEET, ON 0.42 ACRES OF LAND LOCATED AT 1619 EAST FOURTH STREET, WITHIN THE CC (COMMUNITY COMMERCIAL) ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF—APNS: 0110-181-11 AND 0110-181-19.

WHEREAS, HANNIBAL PETROSSI ("Applicant") has filed an Application for the approval of a Variance, File No. PVAR21-006, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 0.42-acre of land generally located at 1610 East Fourth Street, within the CC (Community Commercial) zoning district, and is presently vacant; and

WHEREAS, the property to the north of the Project site is within the HDR-45 (High-Density Residential – 25.1 to 45.0 du/ac) zoning district and ICC Overlay (Interim Community Commercial Overlay) and is developed with a drive-thru and gas station. The property to the east is within the CC (Community Commercial) zoning district and is developed with a drive-thru restaurant. The property to the south is within the CC (Community Commercial) zoning district and is developed with service and retail commercial land uses. The property to the west is within the CC (Community Commercial) zoning district and is developed with a motel; and

WHEREAS, a Development Plan, File No. PDEV21-033, to construct a 2,800 square-foot drive-thru restaurant and 500 square-foot outdoor patio is being processed in conjunction with the Variance; and

WHEREAS, the Applicant is requesting approval of a Variance to reduce the required front drive aisle setback, along an arterial street, from 20 to 13 feet. The additional space has been requested to facilitate efficient, safe, and orderly development of the site, to ensure that the development will be economically feasible, and to make the proposed development consistent with the existing drive-thru located within the same commercial shopping center. The Variance will also allow for other Development Code standards to be met, such as landscape coverage and a new trash enclosure to serve the Project; and

WHEREAS, the Variance request is consistent with TOP Goal LU3, which promotes flexibility in order to respond to special conditions and circumstances in order to achieve the Vision. For these reasons, staff supports granting the Variance request to

reduce the front drive-aisle setback in order to allow for the construction of the proposed drive-thru facility and enable the Project site to remain consistent with existing, neighboring developments; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act, commencing with Public Resources Code Section 21000 (hereinafter referred to as "CEQA"); and

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan ("ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on July 18, 2022, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision No. DAB22-025, recommending that the Planning Commission approve the Application; and

WHEREAS, on July 26, 2022, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The administrative record has been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(2) The Project is categorically exempt from the requirements of CEQA pursuant to Section 15305 (Class 5, Minor Alterations in Land Use Limitations) of the CEQA Guidelines, which consist of minor alterations in land use limitations in areas with an average slope of less than 20 percent and which do not result in any changes in land use or density. The Variance requests a reduction in the front drive aisle setback along an arterial street, from 20 feet to 13 feet, and will not result in the creation of any new parcel; and

(3) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

(4) The determination of CEQA exemption reflects the independent judgment of the Planning Commission.

SECTION 2: Housing Element Compliance. Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the Project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 3: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as “ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 4: *Concluding Facts and Reasons.* Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 through 3, above, the Planning Commission hereby concludes as follows:

(1) ***The strict or literal interpretation and enforcement of the specified regulation would result in practical difficulty or unnecessary physical hardship inconsistent with the objectives of the development regulations contained in this Development Code.*** The existing shopping center was built between the early 1990s and early 2000s; however, the Project site has remained vacant since it was graded in 1993. Since development of the shopping site, Development Codes have been updated, and the strict application of the drive aisle setbacks would not allow the applicant to feasibly build the proposed restaurant and drive-thru facility. The Project site contains two parcels, and as the building cannot cross parcel lines, this further limits the placement and orientation of restaurant and drive-thru lane. The 0.42-acre Project site is substandard in size, as the Development Code requires a minimum of one acre to develop drive-thru facilities. Additionally, the adjacent restaurant’s drive-thru lane, located in the same commercial center, is set back 12.5 feet to the north property line. As such, the Variance application is necessary to facilitate the related Development Plan application and various other site improvements, such as compliance with building setbacks, landscaping, and trash enclosure standards. Strict interpretation and enforcement of the Development Plan’s drive aisle setbacks would result in practical difficulty or unnecessary

physical hardship inconsistent with the objectives of the development regulations contained in the Ontario Development Code. Further, TOP Goal LU3 allows for flexible response to conditions and circumstances in order to achieve the Vision; and

(2) ***There are exceptional or extraordinary circumstances or conditions applicable to the property involved, or to the intended use of the property, that do not apply generally to other properties in the vicinity and in the same zoning district.*** The requested relief from the minimum drive aisle setback along Fourth Street (an arterial street) will allow for greater design flexibility and will serve to provide an economically viable development on the Project site. The Variance application will enable the Project to be consistent with the other drive-thru facility in the same commercial shopping center. In addition, the setback deviations will provide the Applicant the ability to comply with other Development Code regulations, such as landscaping, building setbacks, and safe and effective site circulation. Therefore, the strict or literal interpretation and enforcement of the specified regulations would deprive the applicant of privileges enjoyed by owners of other properties in the same zoning district and reduce the applicant's ability to create a well-designed, economically viable project; and

(3) ***The strict or literal interpretation and enforcement of the specified regulation would deprive the Applicant of privileges enjoyed by the owners of other properties in the same zoning district.*** The requested relief to reduce the front drive aisle setback along an arterial street from 20 feet to 13 feet will allow for greater design flexibility and will serve to equalize development rights between the Applicant and owners of property in the same zoning district, located within the area of the Project site, as the adjacent drive-thru restaurant within the same shopping center was constructed with a 12.5-foot drive aisle setback to the same street; and

(4) ***The granting of the Variance will not be detrimental to the public health, safety or welfare, or be materially injurious to properties or improvements in the vicinity.*** A thorough review and analysis of the proposed Variance and its potential to adversely impact properties surrounding the subject site was completed by staff. As a result of this review, certain design considerations will be incorporated into the Project as conditions of approval, to mitigate identified impacts to an acceptable level, including the use of upgraded materials, the inclusion of certain architectural design elements on building exteriors, intensified landscape elements, and orientation of menu boards away from the adjacent motel land use; and

(5) ***The proposed Variance is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and the purposes of any applicable specific plan or planned unit development, and the purposes of this Development Code.*** The proposed Project is located with the General Commercial (0.4 FAR) land use district of the Policy Plan Land Use Map, and the CC (Community Commercial) zoning district. With

the Project's conditions of approval and approval of the related Variance request, the development standards and conditions under which the proposed Project will be constructed and maintained are consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan. The Project will serve to intensify commercial development in the region, will provide an additional convenience to visitors of the shopping site, and may help to catalyze overall revitalization of the surrounding commercial shopping center.

SECTION 5: Planning Commission Action. Based upon the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 6: Indemnification. The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 7: Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 8: Certification to Adoption. The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26th day of July, 2022, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Jim Willoughby
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on July 26, 2022, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**File No. PVAR21-006
Departmental Conditions of Approval**

(Departmental conditions of approval to follow this page)



LAND DEVELOPMENT DIVISION CONDITIONS OF APPROVAL

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

Date Prepared: 6/9/2022

File No: PVAR21-006

Related Files: PDEV21-033

Project Description: A Development Plan (File No. PDEV21-033) to construct one 2,800 square-foot drive-thru restaurant and 500 square-foot outdoor patio, in conjunction with a Variance (File No. PVAR21-006) request to reduce the front drive aisle setback along an arterial street from 20 feet to 13 feet, on 0.42 acres of land located at 1610 East Fourth Street, within the CC (Community Commercial) zoning district (APNs: 0110-181-11 and 0110-181-19); **submitted by Hannibal Petrossi.**

Prepared By: Alexis Vaughn, Associate Planner
Phone: 909.395.2416 (direct)
Email: avaughn@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits.

(a) Variance approval shall become null and void one year following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director, except that a Variance approved in conjunction with a Development Plan shall have the same time limits as said Development Plan. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

2.2 Environmental Review.

(a) The proposed project is categorically exempt from the requirements of the California Environmental Quality Act of 1970 (CEQA), as amended, and the Guidelines

promulgated thereunder, pursuant to Section 15305 (Class 5, Minor Alterations in Land Use Limitations) of the CEQA Guidelines.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

2.3 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.4 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Exemption ("NOE") filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act ("CEQA"). The filing of an NOE is voluntary; however, failure to provide said fee within the time specified will result in the extension of the statute of limitations for the filing of a CEQA lawsuit from 30 days to 180 days.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

2.5 Additional Requirements.

(a) The Variance application approval is contingent on approval of the related Development Plan, File No. PDEV21-033.

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PDEV21-033, A DEVELOPMENT PLAN TO CONSTRUCT ONE 2,800-SQUARE-FOOT DRIVE-THRU RESTAURANT AND 500 SQUARE-FOOT OUTDOOR PATIO ON 0.42-ACRE OF LAND LOCATED AT 1610 EAST FOURTH STREET, WITHIN THE CC (COMMUNITY COMMERCIAL) ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF—APNS: 0110-181-11 AND 0110-181-19.

WHEREAS, HANNIBAL PETROSSI ("Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV21-033, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 0.42-acre of land generally located at 1610 East Fourth Street within the CC (Community Commercial) zoning district, and is presently vacant; and

WHEREAS, the property to the north of the Project site is within the HDR-45 (High-Density Residential - 25.1 to 45.0 du/ac) zoning district and the ICC Overlay (Interim Community Commercial Overlay) and is developed with a drive-thru and gas station. The property to the east is within the CC (Community Commercial) zoning district and is developed with a drive-thru restaurant. The property to the south is within the CC (Community Commercial) zoning district and is developed with service and retail commercial land uses. The property to the west is within the CC (Community Commercial) zoning district and is developed with a motel; and

WHEREAS, a Variance (File No. PVAR21-006) to reduce the required front drive aisle setback from 20 to 13 feet is being processed in conjunction with the Development Plan to facilitate the construction of a 2,800-square-foot drive-thru restaurant and 500 square-foot outdoor patio; and

WHEREAS, the Project site is located on two vacant parcels of land located near the northwest corner of an existing commercial shopping center. The westerly parcel will primarily accommodate the restaurant building and drive-thru trellis, and the easterly parcel will primarily accommodate the site's parking lot, landscape, and drive-thru. The entrance to the drive-thru will be centrally located to the Project site, and the drive-thru will traverse west and exit south to the southerly Project boundary. The pedestrian entry to the building will be located on the eastern elevation; and

WHEREAS, the floor plan has been designed with the dining area occupying the eastern half of the building and the kitchen and employee areas the western half of the

building. A 500 square-foot outdoor patio will be located along the building's eastern elevation, adjacent to the parking lot; and

WHEREAS, the shopping center is presently served by one primary and one secondary vehicular access point along Fourth Street and the Project will not alter the location of any vehicular access points to the shopping center nor drive aisles internal to the shopping center; and

WHEREAS, the Development Code requires fast food restaurants to provide 13.3 spaces for each 1,000 square feet of gross floor area, with a credit of one parking space for each 24 lineal feet of the drive-thru lane behind the pickup window. The proposed 2,800 square-foot building with 150 lineal-foot drive-thru lane would require 31 parking spaces (37 spaces for the building and a credit of 6 spaces for the drive-thru lane), and 14 spaces have been proposed on-site, resulting in a parking deficit of 17 spaces; and

WHEREAS, the CC&Rs for the shopping center include provisions for shared parking and access throughout the center, but currently exclude the Project site. As such, the Project has been conditioned to update the CC&Rs to incorporate the Project site and assign fair share costs accordingly. The commercial shopping center has been developed with 726 parking spaces, which are shared between the five existing buildings, and 680 spaces would be required to serve the center, resulting in a surplus of 46 parking spaces; and

WHEREAS, given the existing shopping center's surplus of 46 parking spaces and established shared parking agreement, the proposed use, as conditioned, will not adversely affect the overall parking demand within the existing shopping center and sufficient parking will continue to be provided to support both the proposed use and existing tenants; and

WHEREAS, the proposed building has a contemporary architectural design, which incorporates varying roof and parapet heights and a cornice treatment. The exterior walls are treated with a combination of stone veneer, metal, and stucco materials, with a contrasting color palette in neutral earth tones. The building utilizes 360-degree stone wainscot, which is also incorporated all the way up a tower element at the northeast portion of the building. The north elevation includes vine trellises to help soften the appearance of the building and drive-thru lane from Fourth Street, and all four sides of the building incorporate a metal awning and/or canopy for visual interest. A covered canopy will be provided over the drive-thru pickup window, which has been conditioned to be redesigned in the plan check process with a more substantial post/column/beam to better complement the building's architecture; and

WHEREAS, the Project provides landscaping improvements throughout and beyond the Project site, including areas along the street frontage and adjacent to the

center's main access drive, around the perimeter of the building, along the drive-thru lane, and around the new trash enclosure. The Development Code requires a minimum of 15 percent landscaping for drive-thru facilities, and the Project site is proposing 23 percent landscape coverage, which exceeds the minimum requirement. The Project proposes a low garden wall and plantings to screen the drive-thru lane and help shield glare from headlights to adjacent streets; and

WHEREAS, Public utilities (water and sewer) are available to serve the Project. Furthermore, the Applicant has submitted a Preliminary Water Quality Management Plan (PWQMP) which establishes the Project's compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development (LID) best management practices (BMPs), such as retention and infiltration. The proposed development will not substantially alter the existing drainage pattern; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act, commencing with Public Resources Code Section 21000 (hereinafter referred to as "CEQA"); and

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan ("ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on July 18, 2022, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision No. DAB22-026, recommending that the Planning Commission approve the Application; and

WHEREAS, on July 26, 2022, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The administrative record has been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(2) The Project is categorically exempt from environmental review pursuant to Section 15332 (Class 32, In-Fill Development) of the CEQA Guidelines and meets all of the follow conditions:

(a) The Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as with applicable zoning designation and regulations. The proposed Project is located within the GC (General Commercial) land use district of the Policy Plan Land Use Map, and the CC (Community Commercial) zoning district. The proposed project is consistent with all applicable Policy Plan (general plan) policies, as well as with the requirements of the City of Ontario Development Code.

(b) The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses. The Project is

proposed within the established boundaries of the City of Ontario, on a project site totaling 0.42-acre of land, which is surrounded by established commercial land uses.

(c) The Project site has no value as habitat for endangered, rare, or threatened species. The site is void of any flora or fauna, was previously graded, and is regularly maintained to prevent the growth of weeds and grasses.

(d) Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality. The proposed drive-thru restaurant is similar to, and of no greater impact than other allowed retail uses and commercial development projects within the CC (Community Commercial) zoning district.

(e) The site can be adequately served by all required utilities and public services. The site is located within an area that is currently served by all required utilities and public services, including, but not limited to sewer, water, gas, electric and municipal solid waste services; and

(3) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

(4) The determination of CEQA exemption reflects the independent judgment of the Planning Commission.

SECTION 2: *Housing Element Compliance.* Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the Project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 3: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (“ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-

making authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 4: Concluding Facts and Reasons. Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 through 3, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Project is located within the General Commercial land use district of the Policy Plan Land Use Map, and the CC (Community Commercial) zoning district. With the Project's conditions of approval and the related Variance file, the development standards and conditions under which the proposed Project will be constructed and maintained is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan. The proposed drive-thru restaurant will add commercial options and an additional convenience to patrons of the commercial shopping center, as well as provide landscaping, drainage, parking, and other visual improvements to the vacant site. The development has potential to contribute to the revitalization of the existing shopping center; and

(2) ***The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located.*** With implementation of the Project's conditions of approval and approval of the related Variance file, the Project has been designed consistent with the requirements of the City of Ontario Development Code and the CC (Community Commercial) zoning district, including standards relative to the particular land use proposed (drive-thru restaurant), as well as building intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions. The Project will be located within an existing shopping center and is adjacent to other commercial land uses; as such, the Project will not impose any impacts to privacy or views. The hotel located to the west of the Project site does not have windows facing the Project site, and no other residential-type land uses exist in close proximity to the Project site; and

(3) ***The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the Project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed Project.*** The Planning Commission has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: [i] the purposes of the Development Code are maintained; [ii] the Project will not endanger the public health, safety or general welfare; [iii] the Project will not result in any significant environmental impacts; [iv] the Project will be in harmony with the area in which it is located; and [v] the Project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan. With implementation of the Project's conditions of approval and approval of the related Variance application, the Project will improve the vacant site with a well-designed new building, paving, infrastructure, and landscaping, and will visually complete the existing shopping center from Fourth Street; and

(4) ***The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development.*** The proposed Project has been reviewed for consistency with the general development standards and guidelines of the Development Code that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking and loading spaces, parking lot dimensions, design and landscaping, bicycle parking, on-site landscaping, and fences and walls, as-well-as those development standards and guidelines specifically related to the particular land use being proposed (drive-thru restaurant). As a result of this review, the Planning Commission has determined that the Project, when implemented in conjunction with the conditions of approval and related Variance application, will be consistent with the development standards and guidelines described in the Development Code.

SECTION 5: Planning Commission Action. Based upon the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 6: Indemnification. The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 7: Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East “B” Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 8: Certification to Adoption. The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26th day of July, 2022, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Jim Willoughby
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. PC22-_____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on July 26, 2022, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**File No. PDEV21-033
Departmental Conditions of Approval**

(Departmental conditions of approval to follow this page)



LAND DEVELOPMENT DIVISION CONDITIONS OF APPROVAL

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

Date Prepared: 6/9/2022

File No: PDEV21-033

Related Files: PVAR21-006

Project Description: A Development Plan (File No. PDEV21-033) to construct one 2,800 square-foot drive-thru restaurant and 500 square-foot outdoor patio, in conjunction with a Variance (File No. PVAR21-006) request to reduce the front drive aisle setback along an arterial street from 20 feet to 13 feet, on 0.42 acres of land located at 1610 East Fourth Street, within the CC (Community Commercial) zoning district (APNs: 0110-181-11 and 0110-181-19); **submitted by Hannibal Petrossi.**

Prepared By: Alexis Vaughn, Associate Planner
Phone: 909.395.2416 (direct)
Email: avaughn@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits. Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

2.2 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department, and any design

requirements that were not satisfied in the entitlement review shall be accommodated in plan check.

(b) The project site shall be developed in conformance with the approved plans on file with the City, and/or updated where required by City staff. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.3 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b) Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

(c) Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(d) Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

2.4 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

(a) A low garden wall shall be constructed to screen the drive-thru lane and shall extend from the easterly entrance to the drive-thru and terminate after the curve at the northwest portion of the site. The wall shall be treated to match the colors and architecture of the building. Final wall design and location subject to Planning Department review and approval.

2.5 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(c) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

(d) Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).

(e) Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11). Location of bicycle parking facilities shall be shown on construction plans at plan check submittal and shall be subject to Planning review and approval.

2.6 Site Lighting.

(a) All off-street parking facilities shall be provided with nighttime security lighting pursuant to Ontario Municipal Code Section 4-11.08 (Special Residential Building Provisions) and Section 4-11.09 (Special Commercial/Industrial Building Provisions), designed to confine emitted light to the parking areas. Parking facilities shall be lighted from sunset until sunrise, daily, and shall be operated by a photocell switch.

(b) Unless intended as part of a master lighting program, no operation, activity, or lighting fixture shall create illumination on any adjacent property.

2.7 Mechanical and Rooftop Equipment.

(a) All exterior roof-mounted mechanical, heating and air conditioning equipment, and all appurtenances thereto, shall be completely screened from public view by parapet walls or roof screens that are architecturally treated so as to be consistent with the building architecture.

(b) All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.

2.8 Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

2.9 Signs. All Project signage shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations). A sign permit application shall be submitted for review and approval prior to installation of any signage for the site.

2.10 Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise). All noise-generating units (such as menu talk boxes) shall be oriented away from the neighboring motel to the extent possible.

2.11 Covenants, Conditions and Restrictions (CC&Rs)/Mutual Access and Maintenance Agreements.

(a) The Applicant shall submit, for review and approval, an application for CC&R and revised CC&R document for the shopping center. The revised CC&Rs shall incorporate the Project and shall be recorded prior to the issuance of a building permit.

(b) The CC&Rs shall be in a form and contain provisions satisfactory to the City. The articles of incorporation for the property owners' association and the CC&Rs shall be reviewed and approved by the City.

(c) CC&Rs shall ensure reciprocal parking and access between parcels, and common maintenance of:

(i) Landscaping and irrigation systems within common areas;
(ii) Landscaping and irrigation systems within parkways adjacent to the project site, including that portion of any public highway right-of-way between the property line or right-of-way boundary line and the curb line and also the area enclosed within the curb lines of a median divider (Ontario Municipal Code Section 7-3.03), pursuant to Ontario Municipal Code Section 5-22-02;

(iii) Shared parking facilities and access drives; and
(iv) Utility and drainage easements.

(d) CC&Rs shall include authorization for the City's local law enforcement officers to enforce City and State traffic and penal codes within the project area.

(e) The CC&Rs shall grant the City of Ontario the right of enforcement of the CC&R provisions.

(f) A specific methodology/procedure shall be established within the CC&Rs for enforcement of its provisions by the City of Ontario, if adequate maintenance of the development does not occur, such as, but not limited to, provisions that would grant the City the right of access to correct maintenance issues and assess the property owners association for all costs incurred.

2.12 Environmental Review.

(a) The proposed Development Plan is categorically exempt from the requirements of the California Environmental Quality Act of 1970 (CEQA), as amended, and the Guidelines promulgated thereunder, pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines, and meets all of the following conditions:

(i) The Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations;

(ii) The proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses;

(iii) The project site has no value as habitat for endangered, rare, or threatened species;

(iv) Approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and

(v) The Project site can be adequately served by all required utilities and public services.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

2.13 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.14 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Exemption ("NOE") filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act ("CEQA"). The filing of an NOE is voluntary; however, failure to provide said fee within the time specified will result in the extension of the statute of limitations for the filing of a CEQA lawsuit from 30 days to 180 days.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

(c) The Applicant shall pay applicable fees upon submittal of a CC&R application.

2.15 Additional Requirements.

(a) The Applicant shall work with City Staff during plan check to address all outstanding requirements that were not satisfied on the entitlement review, including but not limited to the following:

(i) Provide decorative mesh screening on all four sides of the trash enclosure, to close the gap between the structure's roof and walls/gates.

(ii) Provide a more substantial trellis structure for the drive-thru that incorporates more substantive design, including thicker columns/posts that are proportionate to the architectural design of the main building.

(iii) Coordinate the location of the trellis with the grading and landscape plans. Trees shall not be placed underneath the trellis.

(b) Development Plan shall not be final and complete until the related Variance, File No. PVAR21-006, has been approved by the required Approving Authority.



**ENGINEERING DEPARTMENT
CONDITIONS OF APPROVAL**

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Broadband Operations & Investment and Revenue Resources Department Conditions incorporated)

<input checked="" type="checkbox"/> DEVELOPMENT PLAN <input type="checkbox"/> OTHER	<input type="checkbox"/> PARCEL MAP <input type="checkbox"/> TRACT MAP <input type="checkbox"/> FOR CONDOMINIUM PURPOSES
PROJECT FILE NO. <u>PDEV21-033</u> RELATED FILE NO(S). _____	
<input checked="" type="checkbox"/> ORIGINAL <input type="checkbox"/> REVISED: __/__/__	

CITY PROJECT ENGINEER & PHONE NO: David Zurita (909) 395-2155

CITY PROJECT PLANNER & PHONE NO: Alexis Vaughn (909) 395-2416

DAB MEETING DATE: July 18, 2022

PROJECT NAME / DESCRIPTION: PDEV21-033; A Development Plan to construct a 2800 sqft fast-food restaurant (The Habit Burger Grill) on 0.19 acres of land within the CC (Community Commercial) zoning district.

LOCATION: 1610 East Fourth Street (APN: 0110-181-11)

APPLICANT: Hannibal Petrossi

REVIEWED BY: Raymond Lee 6/23/22
 Raymond Lee, P.E. Date
 Assistant City Engineer

APPROVED BY: [Signature] 7-1-22
 Khoi Do, P.E. Date
 City Engineer



THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.

1. PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, APPLICANT SHALL: Check When Complete

- 1.01 Dedicate to the City of Ontario, the right-of-way, described below:
_____ feet on _____
Property line corner 'cut-back' required at the intersection of _____
and _____.
- 1.02 Dedicate to the City of Ontario, the following easement(s): _____

- 1.03 Restrict vehicular access to the site as follows: _____
- 1.04 Vacate the following street(s) and/or easement(s):
 - A. All interfering on-site easements shall be quitclaimed, vacated, and/or submit non-interference letter from affected owner/utility company.
- 1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.
- 1.06 Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.
- 1.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.
- 1.08 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Financial Services Department at (909) 395-2124 regarding this requirement.
 - (1) _____
 - (2) _____
- 1.09 Prepare a fully executed Subdivision Agreement (on City approved format and forms) with



accompanying security as required, or complete all public improvements.

- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ontarioca.gov) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.
- 1.13 Ontario Ranch Developments:
 - 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
 - 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
 - 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions: _____

2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:

**A. GENERAL
(Permits includes Grading, Building, Demolition and Encroachment)**

- 2.01 Record Parcel Map/Tract Map No. _____ pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 Submit a PDF of the recorded map to the City Engineer's office.
- 2.03 **Note that the subject parcel is a recognized parcel in the City of Ontario per Parcel Map 14389 Parcel 1 recorded in the County of San Bernardino Parcel Map Book 173 Pages 72-75.**
- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of March 4, 1972.
- 2.05 Apply for a:
 - Certificate of Compliance with a Record of Survey;
 - Lot Line Adjustment (Record a Conforming Deed with the County of San Bernardino within six months of the recordation of the Lot Line Adjustment to conform the new LLA legal description. Submit a copy of the recorded Conforming Deed to the Engineering Department.);
 - Make a Dedication of Easement.



- 2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.

- 2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.

- 2.08 Submit a soils/geology report.

- 2.09 Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:
 - State of California Department of Transportation (Caltrans)
 - San Bernardino County Road Department (SBCRD)
 - San Bernardino County Flood Control District (SBCFCD)
 - Federal Emergency Management Agency (FEMA)
 - Cucamonga Valley Water District (CVWD) for sewer/water service
 - United States Army Corps of Engineers (USACE)
 - California Department of Fish & Game
 - Inland Empire Utilities Agency (IEUA)
 - Other: _____

- 2.10 Dedicate to the City of Ontario the right-of-way described below:

_____ feet on _____

Property line corner 'cut-back' required at the intersection of _____ and _____.

- 2.11 Dedicate to the City of Ontario the following easement(s): _____

- 2.12 Vacate the following street(s) and/or easement(s):
 - A. All interfering on-site easements shall be quitclaimed, vacated, and/or submit non-interference letter from affected owner/utility company.

- 2.13 Ontario Ranch Developments:
 - 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.
 - 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay



any applicable fees as set forth by said agreement.

3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall.

- 2.14 **Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at 100% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.**
- 2.15 The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.
- 2.16 **Pay all Development Impact Fees (DIF) to the Building Department. Storm Drain Development Impact Fee, approximately \$4,030.09, shall be paid to the Building Department. Final fee shall be determined based on the approved site plan.**
- 2.17 Other conditions: _____

B. PUBLIC IMPROVEMENTS
 (See attached Exhibit 'A' for plan check submittal requirements.)

- 2.18 **Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):**

Improvement	Fourth Street	Street 2	Street 3	Street 4
Curb and Gutter	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
AC Pavement	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Drive Approach	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Sidewalk	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace



ADA Access Ramp	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Parkway	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
Raised Landscaped Median	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Fire Hydrant	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Sewer (see Sec. 2.C)	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral; if applicable, see 2.27.a	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Water (see Sec. 2.D)	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Services (Irrigation, Fire, Domestic Water); if applicable, see 2.30.c	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Street Light (see Sec. 2.F)	<input checked="" type="checkbox"/> Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2.G)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral



Fiber Optics (see Sec. 2K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
Removal of Improvements	_____	_____	_____	_____
Other Improvements	_____	_____	_____	_____

Specific notes for improvements listed in item no. 2.18, above: _____

- 2.19 **Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s):**
 a. **In-lieu of constructing a 2" asphalt concrete (AC) grind and overlay, the developer shall pay an in-lieu fee.**
- 2.20 Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.
- 2.21 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide water service sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.
- 2.22 Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately _____, for undergrounding of utilities in accordance with Section 7-7.302.e of the City's Municipal Code.
- 2.23 Other conditions: _____

C. SEWER

- 2.24 **A 10-inch sewer main is available for connection by this project in Fourth Street. (Ref: Sewer plan bar code: S13114)**
- 2.25 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.26 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.
- 2.27 **Other conditions:**
 - a. **If the onsite private sewer main is not available for connection, then the connection of the proposed sewer lateral must be connected to the existing sewer main in Fourth Street.**
 - b. **Provide a separate sewer lateral with a grease interceptor and connect it down stream of the domestic waster sewer lateral.**
 - c. **The Occupant/Applicant shall apply for a Wastewater Discharge Permit for their establishment and shall comply will all the requirements of the Wastewater Discharge Permit. The application can be downloaded from the City's Utilities website in the drop-down of the "Pretreatment" section (www.ontarioca.gov/OMUC/Utilities). Please select the appropriate application that applies to your type of business. Requirements of the**



Wastewater Discharge Permit may include, but not limited to: Installation of wastewater pretreatment equipment, such as grease interceptors or clarifiers. For wastewater permit application questions, please contact:

Michael Birmelin, Environmental Program Manager

omucenvironmental@ontarioca.gov

Phone: (909) 395-2661

D. WATER

- 2.28 A 12-inch water main is available for connection by this project in Fourth Street. (Ref: Water plan bar code: W11353)
- 2.29 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.30 Other conditions:
 - a. Each meter connected to the Public Potable Water System serving more than one (1) single family residential unit, any non-residential unit, or irrigation requires a backflow prevention device.
 - i. The backflow device must be located directly behind the meter with a single 90-degree bend in the pipe between the backflow and the meter.
 - ii. As measured along the pipeline between the meter and backflow, the backflow assembly shall be installed with a separation of 3' minimum to 5' maximum, from the concrete pad to the property line or adjacent back of sidewalk, whichever is closer, to minimize the length of pipe between the meter and the backflow.
 - b. The proposed relocation of the existing backflows as shown on the conceptual entitlement plans does not meet backflow location requirements and must be redesigned per the requirements above.
 - c. If the existing bank of water meters fronting 4th Street is not available for connection, then new domestic water services must connect to the existing 12" water main in the Pressure Zone 1212 (PZ1212) in 4th Street to service the project site. This may require redesigning the sidewalk ramp not to conflict with the backflows or abandon the existing water meters back to main and install new water meters to align with the proposed backflow locations.

E. RECYCLED WATER

- 2.31 A _____ inch recycled water main is available for connection by this project in _____. (Ref: Recycled Water plan bar code: _____)
- 2.32 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.
- 2.33 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.
- 2.34 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.

Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.
- 2.35 Other conditions: _____

F. TRAFFIC / TRANSPORTATION



- 2.36 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer:
 - 1. On-site and off-site circulation
 - 2. Traffic level of service (LOS) at 'build-out' and future years
 - 3. Impact at specific intersections as selected by the City Engineer
- 2.37 New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.
- 2.37 **Other conditions:**
 - a. **Property frontage along Fourth Street shall be signed "No Parking Anytime".**
 - b. **The applicant/developer shall provide reciprocal access between the project site and all other adjacent properties.**

G. DRAINAGE / HYDROLOGY

- 2.39 A _____ inch storm drain main is available to accept flows from this project in _____. (Ref: Storm Drain plan bar code: _____)
- 2.40 Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.
- 2.41 An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100 year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.
- 2.42 Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.
- 2.43 Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.
- 2.44 Other conditions: _____

H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)

- 2.45 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels. If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted. Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.
- 2.46 **Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: <http://www.sbcounty.gov/dpw/land/npdes.asp>.**



- 2.47 Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.
- 2.48 Other conditions: _____

J. SPECIAL DISTRICTS

- 2.49 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.
- 2.50 Other conditions: _____

K. FIBER OPTIC

- 2.51 A _____ fiber optic line is available for connection by this project in _____. (Ref: Fiber Optic plan bar code: _____)
- 2.52 Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole. See Fiber Optic Exhibit herein
- 2.53 Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Broadband Operations Department at (909) 395-2000, regarding this requirement.

3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:

- 3.01 Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.
- 3.02 Complete all requirements for recycled water usage.
 - 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.
 - 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.
 - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.



- 3.03 The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.
- 3.04 Ontario Ranch Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.
- 3.05 **Confirm payment of all Development Impact Fees (DIF) to the Building Department.**
- 3.06 **Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).**

4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:

- 4.01 **Complete all Conditions of Approval listed under Sections 1-3 above.**
- 4.02 **Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.**
- 4.03 **The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall include a completed Acceptance and Bond Release Checklist, state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.**
- 4.04 **Submit record drawings (PDF) for all public improvements identified within Section 2 of these Conditions of Approval.**



EXHIBIT 'A'

ENGINEERING DEPARTMENT First Plan Check Submittal Checklist

Project Number: PDEV21-033

The following items are required to be included with the first plan check submittal:

1. **A copy of this check list**
2. **Payment of fee for Plan Checking**
3. **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4. **One (1) copy of project Conditions of Approval**
5. **Include a PDF (electronic submittal) of each required improvement plan at every submittal.**
6. Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).
7. Three (3) sets of Public Street improvement plan with street cross-sections
8. Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)
9. Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)
10. Four (4) sets of Public Sewer improvement plan
11. Five (5) sets of Public Storm Drain improvement plan
12. **Three (3) sets of Public Street Light improvement plan**
13. Three (3) sets of Signing and Striping improvement plan
14. **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
15. **Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)**
16. Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.
17. **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**
18. One (1) copy of Hydrology/Drainage study
19. **One (1) copy of Soils/Geology report**
20. Payment for Final Map/Parcel Map processing fee



- 21. Three (3) copies of Final Map/Parcel Map
- 22. One (1) copy of approved Tentative Map
- 23. One (1) copy of Preliminary Title Report (current within 30 days)
- 24. One (1) copy of Traverse Closure Calculations
- 25. **One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.**
- 26. Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
- 27. Other: _____



CITY OF ONTARIO

MEMORANDUM

TO: Alexis Vaughn, Assistant Planner
Planning Department

FROM: Mike Gerken, Deputy Fire Chief/Fire Marshal
Fire Department

DATE: September 29, 2021

SUBJECT: PDEV21-033 - A Development Plan to construct a 2800-square foot fast-food restaurant (The Habit Burger Grill) on 0.19 acres of land located at 1610 E 4th Street, within the CC (Community Commercial) zoning district (APN: 110-18-111).

-
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.

SITE AND BUILDING FEATURES:

- A. 2019 CBC Type of Construction: V-B
- B. Type of Roof Materials: Ordinary
- C. Ground Floor Area(s): 2,800 Sq. Ft.
- D. Number of Stories: 1
- E. Total Square Footage: 2,800 Sq. Ft.
- F. 2019 CBC Occupancy Classification(s): A-2

CONDITIONS OF APPROVAL:

1.0 GENERAL

- ☒ 1.1 The following are the Ontario Fire Department (“Fire Department”) requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards (“Standards.”) It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at www.ontarioca.gov/Fire/Prevention.
- ☒ 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

2.0 FIRE DEPARTMENT ACCESS

- ☒ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- ☒ 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25’) inside and forty-five feet (45’) outside turning radius per Standard #B-005.
- ☒ 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150’) in length shall have an approved turn-around per Standard #B-002.
- ☒ 2.4 Access drive aisles which cross property lines shall be provided with CC&Rs, access easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of building plan check.
- ☒ 2.5 "No Parking-Fire Lane" signs and /or red painted curbs with lettering are required to be installed in interior access roadways, in locations where vehicle parking would obstruct the minimum clear width requirement. Installation shall be per Standard #B-001.
- ☒ 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-four (24) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by fire department and other emergency services.

3.0 WATER SUPPLY

- ☒ 3.4 The water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

4.0 FIRE PROTECTION SYSTEMS

- ☒ 4.2 Underground fire mains which cross property lines shall be provided with CC & R, easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of fire department plan check. The shared use of private fire mains or fire pumps is allowable only between immediately adjacent properties and shall not cross any public street.
- ☒ 4.6 A fire alarm system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 72. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- ☒ 4.7 Portable fire extinguishers are required to be installed prior to occupancy per Standard #C-001. Please contact the Fire Prevention Bureau to determine the exact number, type and placement required.
- ☒ 4.8 A fixed fire extinguishing system is required for the protection of hood, duct, plenum and cooking surfaces. This system must comply with National Fire Protection Association (NFPA) Standards 17A and 96. An application with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.

5.0 BUILDING CONSTRUCTION FEATURES

- ☒ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ☒ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- ☒ 5.6 Knox ® brand key-box(es) shall be installed in location(s) acceptable to the Fire Department. All Knox boxes shall be monitored for tamper by the building fire alarm system. See Standard #H-001 for specific requirements.
- ☒ 5.7 Placards shall be installed in acceptable locations on buildings that store, use or handle hazardous materials in excess of the quantities specified in the CFC. Placards shall meet the requirements of National Fire Protection Association (NFPA) Standard 704.

6.0 OTHER SPECIAL USES

- ☒ 6.1 The storage, use, dispensing, or handling of any hazardous materials shall be approved by the Fire Department, and adequate fire protection features shall be required. If hazardous materials are proposed, a Fire Department Hazardous Materials Information Packet, including

Disclosure Form and Information Worksheet, shall be completed and submitted with Material Safety Data Sheets to the Fire Department along with building construction plans.



CITY OF ONTARIO

MEMORANDUM

TO: Alexis Vaughn, Planning Department

FROM: Antonio Galban, Police Department

DATE: September 23, 2021

SUBJECT: PDEV21-033 – A DEVELOPMENT PLAN TO CONSTRUCT A FAST FOOD RESTAURANT WITH DRIVE-THRU AT 1610 EAST FOURTH STREET.

The “Standard Conditions of Approval” contained in Resolution No. 2017-027 apply. The applicant shall read and be thoroughly familiar with these conditions, including but not limited to, the requirements listed below.

- Required lighting for all walkways, driveways, doorways, parking areas, and other areas used by the public shall be provided and operate on photosensor. Photometrics shall be provided to the Police Department. Photometrics shall include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- Rooftop addresses shall be installed on the building as stated in the Standard Conditions. The numbers shall be at a minimum 3 feet tall and 1 foot wide, in reflective white paint on a flat black background, and oriented with the bottom of the numbers towards the addressed street.
- The Applicant shall comply with all construction site security requirements as stated in the Standard Conditions.

In addition, the Ontario Police Department places the following conditions on the project:

- The Applicant shall install a video surveillance system on the site. Cameras shall cover at a minimum all entry doors, all cash registers, and at least one camera shall capture any vehicle utilizing the drive-thru. Cameras shall be positioned so as to maximize the coverage of patrons and vehicles in these areas. Cameras shall record at least 15 frames per second and at a minimum of 720p of resolution. Recordings shall be stored for a minimum of 30 days and made available upon request to any member of the Ontario Police Department.
- The applicant will be responsible for keeping the grounds of the business clean from debris and litter.

- Graffiti abatement by the business owner/licensee, or management shall be immediate and on-going on the premises, but in no event shall graffiti be allowed unabated on the premises for more than 72 hours. Abatement shall take the form of removal, or shall be covered/painted over with a color reasonably matching the color of the existing building, structure, or other surface being abated. Additionally, the business owner/licensee, or management shall notify the City within 24 hours at (909) 395-2626 (graffiti hotline) of any graffiti elsewhere on the property not under the business owner/licensee's or management control so that it may be abated by the property owner and/or the City's graffiti team.
- Trash enclosure shall be fully secured by locks and screens/grates in order to reduce crime and encampment opportunities for homeless persons.
- Maintain all landscaping on property to a standard that all ground covering shrubbery and hedges are no taller than 2 feet (24") and the lower canopy of all trees is no lower than 6 feet (72").

The Applicant is invited to call Antonio Galban at (909) 408-1006 regarding any questions or concerns.



CITY OF ONTARIO MEMORANDUM



DATE: June 1, 2022
TO: David Zurita, Engineering
CC: Alexis Vaughn, Planning
FROM: Peter Tran, Utilities Engineering
SUBJECT: DPR #3 – Conditions of Approval (COA) (#8277)
PROJECT NO.: PDEV21-033 (A Development Plan to construct a 2800-sqft Habit Burger)

BRIEF DESCRIPTION

A Development Plan to construct a 2800-square foot fast-food restaurant (The Habit Burger Grill) on 0.19 acres of land located at 1610 E 4th Street, within the CC (Community Commercial) zoning district (APN: 110-18-111).

THIS SUBMITTAL IS COMPLETE AND RECOMMENDED FOR APPROVAL.

CONDITIONS OF APPROVAL: *The Ontario Municipal Utilities Company (OMUC) recommends this application for approval subject to the conditions outlined below and compliance with the City's Design Development Guidelines, Specifications Design Criteria, and City Standards.*

General Conditions: (Add following to Section 2.A of Engineering Department COA)

1. **Standard Conditions of Approval:** Project shall comply with the requirements as set forth in the Amendment to the Standard Conditions of Approval for New Development Projects adopted by the City Council (Resolution No. 2017-027) on April 18, 2017; as well as project-specific conditions/requirements as outlined below:
2. **Final Utilities Systems Map (USM):** As part of the precise grading plans submittal, provide a Final Utilities Systems Map that shows all existing and proposed Utilities (Potable Water, Recycled Water, Sewer, Storm Drain, and other utilities) including each of the City's public utilities' points of connection to the existing systems.

Potable Water Conditions: (Add following to Section 2.D of Engineering Department COA)

3. **Backflow Prevention:**
 - a. Each meter connected to the Public Potable Water System serving any use for more than one (1) single family residential unit or any non-residential unit or irrigation requires a backflow prevention device. A meter connected to the Public Potable Water System serving only one (1) single family residential unit (and an Accessory Dwelling Unit and/or Junior Accessory Dwelling Unit) does not require a backflow device.
 - i. The backflow device must be located directly behind the meter with a single 90-degree bend in the pipe between the backflow and the meter, located at the backflow device.
 - ii. As measured along the pipeline between the meter and backflow, the backflow assembly shall be installed with a separation of 3' minimum to 5' maximum, from the concrete pad to the property line or adjacent back of sidewalk, whichever is closer, to minimize the length of pipe between the meter and the backflow.
 - b. Proposed backflow relocation: The proposed relocation of the existing backflows as shown on the conceptual entitlement plans does not meet backflow location requirements and must be redesigned per the requirements above.
4. **Domestic Water Services:** If the existing bank of water meters fronting 4th Street is not available for connection, then new domestic water services must connect to the existing 12" water main in the Pressure Zone 1212 (PZ1212) in 4th Street to service the project site. This may require redesigning the sidewalk ramp not to conflict with the backflows or abandon the existing water meters back to main and install new water meters to align with the proposed backflow locations.

Sewer Conditions: (Add following to Section 2.C of Engineering Department COA)

5. **Sewer Lateral:** If the onsite private sewer main is not available for connection, then the connection of the proposed sewer lateral must be connected to the existing sewer main in 4th Street.

6. Grease Interceptor: Provide a separate sewer lateral with a grease interceptor and connect it down stream of the domestic waste sewer lateral.
7. The Occupant/Applicant shall apply for a Wastewater Discharge Permit for their establishment and shall comply will all the requirements of the Wastewater Discharge Permit. The application can be downloaded from the City's Utilities website in the drop-down of the "Pretreatment" section (www.ontarioca.gov/OMUC/Utilities). Please select the appropriate application that applies to your type of business. Requirements of the Wastewater Discharge Permit may include, but not limited to: Installation of wastewater pretreatment equipment, such as grease interceptors or clarifiers. For wastewater permit application questions, please contact:

Michael Birmelin, Environmental Programs Manager
omucenvironmental@ontarioca.gov
Phone: (909) 395-2661



CITY OF ONTARIO

MEMORANDUM

TO: Scott Murphy, Community Development Director
Rudy Zeledon, Planning Director (Copy of memo only)
Diane Ayala, Advanced Planning Division (Copy of memo only)
Charity Hernandez, Economic Development
Matt Montieth, Building Department
Raymond Lee, Engineering Department
Jamie Richardson, Landscape Planning Division
Dennis Mejia, Municipal Utility Company
Gabriel Gutierrez, Police Department
Mike Gerken, Deputy Fire Chief/Fire Marshal
Jay Bautista, T. E., Traffic/Transportation Manager
Lorena Mejia, Airport Planning
Eric Woosley, Engineering/NPDES
Angela Magana, Community Improvement (Copy of memo only)
Jimmy Chang, IPA Department

NATHAN

FROM: Alexis Vaughn, Assistant Planner

DATE: September 21, 2021

SUBJECT: FILE #: PDEV21-033 Finance Acct#:

The following project has been submitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by .

- Note:**
- Only DAB action is required
 - Both DAB and Planning Commission actions are required
 - Only Planning Commission action is required
 - DAB, Planning Commission and City Council actions are required
 - Only Zoning Administrator action is required

PROJECT DESCRIPTION: A Development Plan to construct a 2800-square foot fast-food restaurant (The Habit Burger Grill) on 0.19 acres of land located at 1610 E 4th Street, within the CC (Community Commercial) zoning district (APN: 110-18-111).

- The plan does adequately address the departmental concerns at this time.
 - No comments
 - Report attached (1 copy and email 1 copy)
 - Standard Conditions of Approval apply
- The plan does not adequately address the departmental concerns.
 - The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

ENG./TRAF.
Department

Signature

ENG. ASST
Title

10/27/21
Date



CITY OF ONTARIO

MEMORANDUM

Development Plan Review

Engineering Department:
Transportation Section

Project: PDEV21-033

Date: October 26th, 2021

Location: 1610 East 4th Street

By: Nathan Kuan

The Transportation Division recommends the following to be incorporated into the Project's Conditions-of-Approval:

Conditions:

1. Property frontage along 4th Street shall be signed "No Parking Anytime".
2. The Applicant/Developer shall provide reciprocal access between the project site and all other adjacent properties.

nk;



CITY OF ONTARIO

MEMORANDUM

TO: Scott Murphy, Community Development Director (Copy of memo only)
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Gabriel Gutierrez, Police Department
Paul Erhman, Deputy Fire Chief/Fire Marshal
Jay Bautista, Traffic/Transportation Manager
Lorena Mejia, Airport Planning
~~Eric Woosley, Engineering/NPDES~~ Nathan P.
Angela Magana, Community Improvement (Copy of memo only)
Jimmy Chang, IPA Department
Ben Mayorga, Integrated Waste

Revision #2

FROM: Alexis Vaughn, Assistant Planner

DATE: March 18, 2022

SUBJECT: FILE #: PDEV21-033

Finance Acct#:

The following project has been resubmitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by .

PROJECT DESCRIPTION: A Development Plan to construct a 2800-square foot fast-food restaurant (The Habit Burger Grill) on 0.19 acres of land located at 1610 E 4th Street, within the CC (Community Commercial) zoning district (APN: 110-18-111).

The plan does adequately address the departmental concerns at this time.

No comments

See previous report for Conditions

Report attached (1 copy and email 1 copy)

Standard Conditions of Approval apply

The plan does not adequately address the departmental concerns.

The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

Engineering-ENV
Department

Signature

Engineering Assistant
Title

4/11/2022
Date



CITY OF ONTARIO

MEMORANDUM

TO: David Zurita, ENGINEERING DEPARTMENT
FROM: Nathan S. Pino
DATE: April 11, 2022
SUBJECT: File No. PDEV21-033

The Site Plan for this project approved for DAB based on the following condition:

- This project will be conditioned in the Engineering DAB Report to prepare a WQMP based on the approved PWQMP.
 - The WQMP template is available at: <http://www.sbcounty.gov/dpw/land/npdes.asp> or on the City's website under Engineering/Environmental Services.

Note: Activities resulting in one acre or more land disturbance are required to obtain coverage under the Construction General Permit (CGP). The owner is the legally responsible person (LRP) of the site and shall have a Stormwater Pollution Prevention Plan (SWPPP) developed and submitted through the SMARTS website at <https://smarts.waterboards.ca.gov/smarts/faces/SwSmartsLogin.xhtml>



CITY OF ONTARIO

MEMORANDUM

TO: Scott Murphy, Community Development Director
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Jay Bautista, T. E., Traffic/Transportation Manager
Lorena Mejia, Airport Planning
Eric Woosley, Engineering/NPDES
Angela Magana, Community Improvement (Copy of memo only)
Jimmy Chang, IPA Department

FROM: Alexis Vaughn, Assistant Planner

DATE: September 21, 2021

SUBJECT: FILE #: PDEV21-033

Finance Acct#:

The following project has been submitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by .

- Note:
- Only DAB action is required
 - Both DAB and Planning Commission actions are required
 - Only Planning Commission action is required
 - DAB, Planning Commission and City Council actions are required
 - Only Zoning Administrator action is required

PROJECT DESCRIPTION: A Development Plan to construct a 2800-square foot fast-food restaurant (The Habit Burger Grill) on 0.19 acres of land located at 1610 E 4th Street, within the CC (Community Commercial) zoning district (APN: 110-18-111).

- The plan does adequately address the departmental concerns at this time.
 - No comments
 - Report attached (1 copy and email 1 copy)
 - Standard Conditions of Approval apply
- The plan does not adequately address the departmental concerns.
 - The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

Department

Signature

Title

Date

CITY OF ONTARIO
BROADBAND OPERATIONS
303 East "B" Street, Ontario, CA 91764

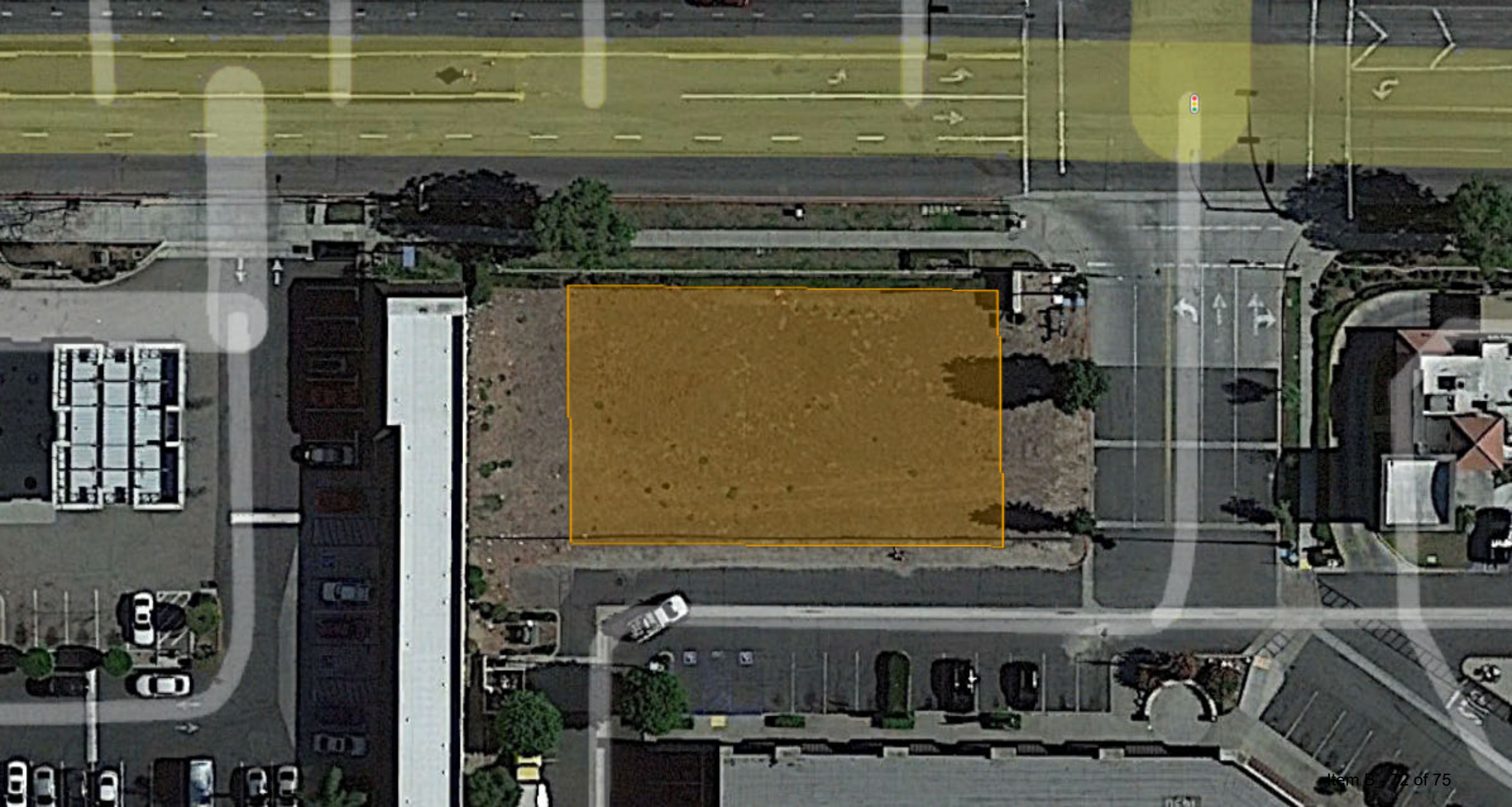
CONDITIONS OF APPROVAL	
Sign Off	
Broadband Operations	10/04/21

Reviewer's Name Cameron Chadwick	Phone 909-395-2090
File # PDEV21-033	Project Engineer: David
Project Name and Location: Habit burger. 1610 E. 4 th St.	
Sent to: David	

<input type="checkbox"/>	Plan does adequately address the departmental concerns at this time. No Comments.
<input checked="" type="checkbox"/>	Plan does adequately address the departmental concerns at this time. Report below.
<input type="checkbox"/>	Plan does not adequately address the departmental concerns. The conditions contained below must be met prior to scheduling for Development Advisory Board.

Req'd for Project	CONDITIONS OF APPROVAL - {ENTER FILE# AND ANY OTHER IDENTIFYING NUMBER HERE}		
<input checked="" type="checkbox"/>	<input type="checkbox"/>	1.	Project shall be designed and constructed to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole in the Right-of-Way (ROW) and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole.
<input type="checkbox"/>	<input type="checkbox"/>	2.	Contractor is responsible for locating and connecting conduit to existing OntarioNet hand holes on adjacent properties within a reasonable distance. There should be no "Gaps" in conduit between the contractor's development and the adjacent property. OntarioNet hand holes are typically located in the ROW at the extreme edge of a property.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	3.	Where a joint telcom or street light street crossing is required, include (2) 2" hdpe sdr-11 conduits or (1) 4" schedule 80 conduit sleeve. Terminate the street crossing conduit(s) in a new hh-3/22 ontarionet hand hole in the right of way
<input checked="" type="checkbox"/>	<input type="checkbox"/>	4.	The City requires a public utility easement for fiber optics on all private aisles/alley ways.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	5.	Hand holes - Design and install OntarioNet fiber optic hand hole HH-2 (17x30x24), HH-2A (24x36x30), HH-3 (30x48x36) and/or HH-4 (36x60x36) as needed. Respectively Newbasis Part # PCA-173024-90116, PCA-243630-90064, PCA-304836-90244 and PCA-366036-90146 per City Standard 1316. Conduits sweeping into hand holes shall enter in flush with the cut-out mouse holes aligned parallel to the bottom of the box and come in perpendicular to the wall of the box. Conduits shall not enter at any angle other than parallel. Provide 5 foot minimum clearance from existing/proposed utilities. All hand holes will have ¼-inch galvanized wire between the hand holes and the gravel it is placed on.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	6.	ROW Conduit – Design and install fiber optic conduit at a minimum depth of 36-inch. Trenching shall be per City Standard 1306. Install (1) 2-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange) duct and (1) 2-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange with Black Stripe) duct. Conduit(s) between ROW hand holes and hand holes on private property shall be 2-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange) duct.
<input type="checkbox"/>	<input type="checkbox"/>	7.	Building Entrance (Single Family) – Design and install 0.75-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange) duct from hand holes on property or hand holes in the ROW. Consult City's Fiber Team for design assistance.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	8.	Building Entrance (Multi-family and Commercial) - From the nearest handhole to the building entrance, design and install fiber optic conduit at a minimum depth of 36-inches. Trenching shall be per City Standard for Commercial Buildings. (1) 2-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange) duct. Install locate/tracer wires minimum 12AWG within conduit bank and fiber warning tape 18-inch above the uppermost duct

Req'd for Project	CONDITIONS OF APPROVAL - {ENTER FILE# AND ANY OTHER IDENTIFYING NUMBER HERE}	
<input checked="" type="checkbox"/>	<input type="checkbox"/>	9. Multi-family and commercial properties shall terminate conduit in an electrical room adjacent to the wall no less than five inches above the finished floor. A 20" width X length 36" space shall be reserved on the plywood wall for OntarioNet equipment. This space shall be labeled "OntarioNet Only". Ontario Conduit shall be labeled "OntarioNet"
<input checked="" type="checkbox"/>	<input type="checkbox"/>	10. A minimum 1.5-inch joint use telecommunications conduit with pull-rope from the multi-family or commercial building communal telecomm/electrical room/closet to each multi-family or commercial building unit shall be installed. See Structured Wiring Checklist on City's website for additional details.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	11. Warning Tape - Contractor shall supply and install an approved non-detectable warning tape 18-inch above the uppermost conduit when backfilling trenches, pits or excavations greater than 10' in length. Warning Tape shall be non-detectable, Orange in color, 4-inch minimum width, 4 mil, 500% minimum elongation, with bold printed black letters "CAUTION - BURIED FIBER OPTIC CABLE BELOW" printed in bold black lettering no less than 2-inch high.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	12. All hand holes, conduits, conduit banks, materials and installations are per the City's Fiber Optic Master Plan and City Fiber Optic Cable and Duct Standards. All hand holes, conduits and ducts shall be placed in the public right of way.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	13. All unused conduits/ducts/microducts shall be protected with duct plugs that provide a positive seal. Ducts that are occupied shall be protected with industry accepted duct seal compound.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	14. Locate/Tracer Wire - Conduit bank requires (1) 12AWG high strength (minimum break load 452#) copper-clad steel with 30mil HDPE orange insulation for locate/tracer wire. Contact City's Fiber Team for tracer wire specifications and see note 8.
<input type="checkbox"/>	<input type="checkbox"/>	15. Developer to install 3 inch SCE conduit stub for future City fiber optic meter pedestal within an 8-foot wide, 5-foot deep reserved area for City fiber optic network cabinet. A 3-foot clearance must be maintained around the cabinet and the meter. HH4 shall be placed near the reserved area for cable entrance to network cabinet. The pedestal and network cabinet will be supplied and installed by the City. The service submittal to SCE will be coordinated by the City.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	16. Multi-family dwellings are considered commercial property.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	17. Refer to the In-tract Fiber Network Design guideline on the City's website for additional in-tract conduit guidelines.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	18. Please contact City's Fiber Team at OntarioNet@ontarioca.gov for conduit design assistance.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	19. For additional information please refer to the City's Fiber Optic Master Plan.
<input type="checkbox"/>	<input type="checkbox"/>	20. Please see attached corrections.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	21. Please provide plans in digital format (PDF) on future revisions.





CITY OF ONTARIO

MEMORANDUM

TO: Scott Murphy, Community Development Director (Copy of memo only)
Rudy Zeledon, Planning Director (Copy of memo only)
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Dennis Mejia, Municipal Utility Company
Gabriel Gutierrez, Police Department
Paul Erhman, Deputy Fire Chief/Fire Marshal
Jay Bautista, Traffic/Transportation Manager
Lorena Mejia, Airport Planning
Eric Woosley, Engineering/NPDES
Angela Magana, Community Improvement (Copy of memo only)
Jimmy Chang, IPA Department
Ben Mayorga, Integrated Waste

Revision #2

FROM: Alexis Vaughn, Assistant Planner

DATE: March 18, 2022

SUBJECT: FILE #: PDEV21-033

Finance Acct#:

The following project has been resubmitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by .

PROJECT DESCRIPTION: A Development Plan to construct a 2800-square foot fast-food restaurant (The Habit Burger Grill) on 0.19 acres of land located at 1610 E 4th Street, within the CC (Community Commercial) zoning district (APN: 110-18-111).

- The plan does adequately address the departmental concerns at this time.
- No comments
 - See previous report for Conditions
 - Report attached (1 copy and email 1 copy)
 - Standard Conditions of Approval apply
- The plan does not adequately address the departmental concerns.
- The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

Department

Signature

Title

Date

Landscape Planning Division J.P. Landscape Planner 4/18/22

CITY OF ONTARIO
LANDSCAPE PLANNING DIVISION
 303 East "B" Street, Ontario, CA 91764

CONDITIONS OF APPROVAL

Sign Off



04/18/2022

Jamie Richardson, Sr. Landscape Planner

Date

Reviewer's Name:

Jamie Richardson, Sr. Landscape Planner

Phone:

(909) 395-2615

D.A.B. File No.:

PDEV21-033

Case Planner:

Alexis Vaughn

Project Name and Location:

The Habit
 1610 E 4th Street

Applicant/Representative:

Hannibal Petrossi (949) 833-3240 hp@petroassassoc.com
 18831 Bardeen Avenue, Suite 100
 Newport Beach, CA 92660



Preliminary Plans (dated 3/18/2022) meet the Standard Conditions for New Development and have been approved considering that the following conditions below be met upon submittal of the landscape construction documents.



Preliminary Plans (dated) have not been approved. Corrections noted below are required before Preliminary Landscape Plan approval.

A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED AS INCOMPLETE.

Landscape construction plans with plan check number may be emailed to:
landscapeplancheck@ontarioca.gov

Civil/ Site Plans

1. Reduce the length of the ramp, avoid switchback, show grades, and slopes. Use max slope 8.33%.
2. Note on grading plans: for compaction to be no greater than 85% at landscape areas. All finished grades at 1 1/2" below finished surfaces. Slopes to be maximum 3:1.
3. Relocate utilities, lights, and signage out of required tree locations. Trees are required at parking lot island planters and setbacks to allow for a tree every 4 spaces and at end islands.
4. Before permit issuance, stormwater infiltration devices located in landscape areas shall be reviewed and plans approved by the Landscape Planning Division. Any stormwater devices in parkway areas shall not displace street trees.
5. Show transformers set back 5' from paving all sides; dimension on plans. Coordinate with landscape plans.

Landscape Plans

6. Locate light standards, fire hydrants, water, and sewer lines to not conflict with required tree locations. Coordinate civil plans with landscape plans; see #3 above.
7. Show all utilities on the landscape plans. Coordinate so utilities are clear of tree locations.
8. Show parking lot trees in island planters. Trees are required at parking lot island planters and setbacks to allow for a tree every 4 spaces and at end islands. Add a tree in the southern island planter adjacent to the trash enclosure.
9. Landscape construction plans shall meet the requirements of the Landscape Development Guidelines. See <http://www.ontarioca.gov/landscape-planning/standards>
10. After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections at a rate established by the resolution of the City Council.

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT



Project File No.: PDEV21-033
 Address: 1610 E Fourth Street
 APN: 110-18-111
 Existing Land Use: Vacant
 Proposed Land Use: Development Plan to construct 2,800 SF fast food restaurant (The Habit Grill)
 Site Acreage: 0.19 Proposed Structure Height: 32FT
 ONT-IAC Project Review: n/a
 Airport Influence Area: ONT

Reviewed By: Lorena Mejia
 Contact Info: 909-395-2276
 Project Planner: Alexis Vaughn
 Date: 12/23/2021
 CD No.: 2021-054
 PALU No.: n/a

The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input checked="" type="checkbox"/> High Terrain Zone	<input type="checkbox"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input checked="" type="checkbox"/> FAA Notification Surfaces	<input type="checkbox"/> Recorded Overflight Notification
<input type="radio"/> Zone 2	<input type="checkbox"/> 65 - 70 dB CNEL	<input checked="" type="checkbox"/> Airspace Obstruction Surfaces	<input checked="" type="checkbox"/> Real Estate Transaction Disclosure
<input type="checkbox"/> Zone 3	<input type="checkbox"/> 60 - 65 dB CNEL	<input type="checkbox"/> Airspace Avigation Easement Area	
<input type="radio"/> Zone 4		Allowable Height: <u>70 FT</u>	
<input type="radio"/> Zone 5			

The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1
 Zone 2
 Zone 3
 Zone 4
 Zone 5
 Zone 6
 Allowable Height: _____

CONSISTENCY DETERMINATION

This proposed Project is: Exempt from the ALUCP
 Consistent
 Consistent with Conditions
 Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

Airport Planner Signature: _____

FILE NOS: PUD20-001 and PDEV20-009

SUBJECT: A Planned Unit Development (File No. PUD20-001) to establish development standards, design guidelines and infrastructure requirement for 0.81-acres of land, in conjunction with a Development Plan (File No. PDEV20-009) to construct a mixed-use project consisting of 59 multiple-family dwellings (three-stories of residential units) over two-stories of podium parking and 2,500 square feet of first floor retail space, on property located at 549 West Holt Boulevard, within the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District) zoning district; (APN: 1049-021-09) **submitted by Kathy Huynh. City Council action required for PUD only.**


PROPERTY OWNER: Roosevelt Property Management

RECOMMENDED ACTION: That the Planning Commission consider and 1) adopt the resolution recommending the City Council approve the Planned Unit Development, File No. PUD20-001, and 2) adopt the resolution approving the Development Plan, File No. PDEV20-009, pursuant to the facts and reasons contained in the staff report and resolution, and subject to the departmental conditions of approval included as Attachment A to the resolution.

PROJECT SETTING: The project site is comprised of 0.81-acres of vacant land located at 549 West Holt Boulevard, within the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard) zoning district and is depicted in Figure 1: Project Location Map. The property is relatively flat, with a gentle slope to the southwest corner of the site and is covered with perennial



Figure 1: Project Location

Case Planner:	Luis E. Batres
Planning Director Approval:	
Submittal Date:	4/13/20

Hearing Body	Date	Decision	Action
DAB	7/6/22	Approval	Recommend
PC	7/26/22		Recommend (PUD) Final (PDEV)
CC	9/6/22		Final (PUD)

grasses. The Project site is rectangular in shape, with a lot depth of approximately 270 feet and a lot width of 140 feet. The property to the north of the Project site is within the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard) zoning district and is currently developed with the Ontario Village Commercial Shopping Center. The property to the south is within the IL (Light Industrial) land use district and is developed with a mix of industrial uses. The properties to the east and west are located within the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard) zoning district and are developed with multi-tenant commercial buildings. The existing surrounding land uses, zoning, and general plan and specific plan land use designations are summarized in the "Surrounding Zoning & Land Uses" table located in the Technical Appendix of this report.

PROJECT ANALYSIS:

(1) Background — On April 13, 2020, the Applicant submitted a Planned Unit Development application (File No. PUD20-001, Roosevelt Tower Planned Unit Development) that will establish the development standards, design guidelines, and infrastructure requirements for the Project site. In conjunction with the Planned Unit Development ("PUD"), the Applicant submitted a Development Plan (File No. PDEV20-009) application requesting approval to construct a mixed-use project consisting of 59 multiple-family dwellings and 2,500 square feet of retail space (four tenant spaces), and two levels of podium parking. Pursuit to the Ontario Development Code, the PUD requires approval by both the Planning Commission and City Council.

On July 6, 2022, the Development Advisory Board ("DAB") conducted a hearing and unanimously voted to recommend that the Planning Commission approve the subject Development Plan (File No. PDEV20-009). The DAB's purview does not extend to the proposed Planned Unit Development.

(2) Planned Unit Development —The Ontario Plan Policy Plan (general plan) designates the project area as Mixed-Use, permitting residential uses at a density of 25.0 to 75.0 du/ac, and a development intensity of up to 2.0 FAR for office and retail development. The Ontario Plan describes the Downtown Mixed-Use Area as an intensive vertical and horizontal mixture of retail, office and residential uses in a pedestrian friendly atmosphere. The most intensive uses are envisioned along Euclid and Holt Avenue. The Downtown Mixed-Use District is also intended to provide new housing opportunities that will also provide increased demand for retail in more concentrated, strategic locations. The densities and intensities of the Mixed-Use land use designation represented within The Ontario Plan ("TOP") are the intended level of anticipated development; however, individual projects may vary depending upon an approved master plan, such as an area plan, specific plan, or PUD.

The Policy Plan specifies that the Downtown Mixed-Use Area is to be implemented through the approval of an Area Plan or PUD prior to the development of properties within the area. In compliance with this requirement, the Applicant has submitted the "Roosevelt Tower Planned Unit Development" (included as Attachment "A" to the

attached resolution), which is consistent with this vision and the goals and policies of the Policy Plan.

The purpose of the PUD is to secure a fuller realization of the Policy Plan than would result from the strict application of present zoning district regulations and to:

- Promote high standards in urban design;
- Encourage the development of exceptionally high quality, mixed-use, while establishing regulations and standards for uses with unique regulatory and design needs; and
- Ensure harmonious relationships with surrounding land uses.

A PUD is comparable to a Specific Plan in that it sets development regulations that are unique to a specific area; however, it is also unlike a Specific Plan in that a PUD is typically intended to apply to a single development project or several interrelated development projects that function together as a single, comprehensive project.

(a) Land Use and Development Regulations — Consistent with the requirements of the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard) zoning district, the "Roosevelt Tower Planned Unit Development" land use restrictions only permit commercial uses, or a mix of commercial and residential uses in the same building or on the same site, or stand-alone multiple-family projects. The maximum building height allowed is 75 feet. The minimum building setbacks allowed are two feet from the Holt Boulevard (north) property line, two feet from the Brooks Street (south) property line, and one-foot from the east and west property lines.

(b) Landscaping and Drainage — A conceptual landscape plan is required to be submitted with the Development Plan for construction of the project site. The plan will be required to specify all landscape and hardscape elements for the development plan site. Water conservation will be provided through low water using plant materials, hydro zones, water efficient irrigation and weather-based controllers. Landscaped areas may be used for storm water infiltration through vegetated swales, retention basins, or dry wells as needed with the use of appropriate planting materials. Broad canopy shade trees can be used to reduce heat gain on buildings, paving and parking areas. The landscape design shall meet the requirements of the Landscape Development Standards and shall create well-functioning spaces within a sustainable design. Trees along street frontages shall comply with the variety, size, and spacing as directed by the City of Ontario Master Street Tree Plan.

(c) Off-Street Parking and Circulation\Access — Parking, circulation, transit and infrastructure improvements will be provided as follows:

(i) **Parking:** Off-street parking is required to be provided on-site, at the rates required by the City of Ontario Development Code. No on-street parking will be allowed on Holt Boulevard, in front of the Project site.

(ii) **Circulation/Access:** Vehicular access for the project site is provided at two points, including one 20-foot-wide driveway on Holt Boulevard, at the northeast corner of the Project site, and one 20-foot-wide driveway on Brooks Street, at the southeast corner of the Project site. After improvements are made from the West Valley Connector (“BRT”) Project, vehicular access from Holt Boulevard will be limited to right-in/right-out movements. Pedestrian connections to the project site will be provided from sidewalks within the PUD site.

(iii) **Mass Transit:** The PUD, along with the future development entitlements for the site, will require the necessary infrastructure to serve the site and provide the necessary rights-of-way dedications for street widening and to accommodate the future BRT line for the SBX West Valley Connector.

(iv) **Street Improvements:** The City of Ontario, as part of its street widening plan, and the developers of the PUD site, will be responsible to improve Holt Boulevard and Brooks Street to their ultimate widths. The proposed project will be required to dedicate 20 feet along the Project’s Holt Boulevard street frontage and two feet along the Project’s Brooks Street frontage.

(d) Architecture/Design Guidelines — The architectural design and details, outlined within the PUD, focus on the following elements:

- Appropriate scale and massing;
- Architectural style and design that are complementary and appropriate to its setting;
- Use of high-quality materials that will create visual interest, high quality, durable and appropriate for the architectural style;
- Appropriate site planning to ensure building orientation visibility from the street;
- Roof form; and
- Building entry design.

The PUD requires the architectural theme of the project area to be contemporary or a modern style that generally responds to the region’s climate, including such features as outdoor circulation, outdoor courtyards/plazas, recessed window frames, awnings, and decorative canopies/trellises. Additionally, buildings are to be oriented toward Holt Boulevard, and incorporate entry elements, interesting architecture, enhanced materials, and a pedestrian scale, to provide connection between the development and the public street.

Due to the size and scale of the PUD area, all sides of the buildings will be visible from the public street. As such, appropriate massing becomes more important to maintain the human-scale of the development and create a visually pleasing environment. Bay windows, stepped buildings, height changes, and setback variations between stores help to break up large buildings, as well as indicate entries and store locations to customers.

(3) Development Plan—In conjunction with the Planned Unit Development, the Applicant is requesting Development Plan approval (File No. PDEV20-009) to construct a mixed-use project consisting of 59 multiple-family dwellings (three-stories of residential units) over two-stories of podium parking and 2,500 square feet of first floor retail space. Staff has worked with the Applicant to design a project that meets the goals and requirements of the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District) zoning district, the Roosevelt Tower PUD, and the goals and policies of The Ontario Plan (“TOP”). The project characteristics are described below:

(a) Site Design/Building Layout —The proposed building is set back five feet from the Holt Boulevard (north) property line, two feet from the Brooks Street (south) property line, and one-foot from the east and west property lines. The residential structures above the two stories of podium parking will be set back 17 feet from Holt Boulevard (north) property line, 18 feet from the Brooks Street (south) property line and a zero-setback has been provided along the east and west property lines.

The Project has been oriented in a north-south orientation, with the front of the building and primary building entrance facing Holt Boulevard. The rear of the building will face Brooks Street.

The building is proposed at five-stories in height (68 feet), comprised of three-stories of residential units constructed over two-stories of podium parking. Additionally, 2,500 square feet of retail space has been incorporated into the building's ground floor, facing the Project's Holt Boulevard street frontage. The retail space is divided into four separate units ranging from 540 to 760 square feet in area (see Exhibits B—Site Plan and Exhibit C—Floor Plans, attached). The retail unit characteristics are shown in Table 1, below.

Table 1: Retail Unit Characteristics

Unit No.	Area (in SF)
Unit 1	760
Unit 2	600
Unit 3	600
Unit 4	540

The Project has been designed with two points of vehicular access, one on Holt Boulevard (north) and the second on Brooks Street (south). Holt Boulevard will serve as the primary access point for both residential and commercial units. The residential parking area is gated, limiting access to residents only. Designated parking for the commercial component is located on the ground floor, immediately behind the four retail units (see Exhibit C—Floor Plans, attached).

The Project is proposed at a residential density of 72.8 du/ac, consisting of 13 studio units, 30 one-bedroom units, and 16 two-bedroom units. The residential units range from 403 to 987 square feet in area. The residential unit characteristics are shown in Table 2, below.

Table 2: Residential Unit Characteristics

Plan No.	Area (In SF)	Total Units	No. Bedrooms	No. Baths
U-1	403	13	Studio	1 bath
U-2	987	6	2-bedrooms	2 baths
U-3	804	3	2 bedrooms	1 bath
U-4	571	21	1 bedroom	1 bath
U-5	532	9	1 bedroom	1 bath
U-6	860	7	2 bedrooms	2 baths

(b) Site Access/Circulation — The project has been designed in conformance with the development regulations, standards and design guidelines of the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District) zoning district and the Roosevelt Tower PUD. The project proposes two points of vehicular access. Primary access is provided from Holt Boulevard, at the northeast corner of the Project site. Secondary access is provided from Brooks Street, at the southeast corner of the Project site. Pedestrian access into the building is proposed at the northwest corner of the building, through a lobby with elevator access. Pedestrian access to the retail units is proposed from the Holt Boulevard public sidewalk. Residential units will be accessed through interior corridors located on each floor. Emergency access stairs have been incorporated at all four corners of the building (see Exhibits B—Site Plan and Exhibit C—Floor Plans, attached).

(c) Parking —The project has been parked in accordance with the Ontario Development Code and the Roosevelt Tower PUD requirements. The Project's total required number of off-street parking spaces is 127 and 128 off-street parking spaces have been provided, exceeding the minimum requirement. All required parking will be located within the first two floors of the 5-story structure. Parking was calculated as shown on Table 3, below.

Table 3: Off-Street Parking Requirements

Use	Ratio	Area (in SF) or No. Dwelling Units (DUs)	Total Required	Provided
Commercial	4 spaces/1,000 SF	2,500 SF	10	10
Studio	1.5 spaces/DU	13 DUs	19	19
One-Bedroom	1.75 spaces/DU	30 DUs	52	53 **
Two-Bedroom	2 spaces/DU	16 DUs	32	32

Table 3: Off-Street Parking Requirements

Use	Ratio	Area (in SF) or No. Dwelling Units (DUs)	Total Required	Provided
Guest	1 Portion DUs <50: 0.25 space/DU Portion DUs 50 to 100: 0.2 space/DU	50 DUs 9 DUs	14	14
Total			127	128

*Note: **14 residential parking spaces (12%) are tandem*

(d) Architecture —The Project proposes a Contemporary Architectural Design, exemplifying the type of high-quality architecture promoted by the Ontario Development Code, The Ontario Plan, and the Roosevelt Tower PUD (see Figure 2: Holt Boulevard Perspective, Figure 3: Brooks Street Perspective, and Exhibit E—Elevations, attached). The mass and scale of the building along the Holt Boulevard frontage will serve to establish an urban sensibility, as envisioned by the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District) zoning district. Additionally, the varying stacked massing of the building, in combination with the articulated roof line, deep exterior wall insets, and the various story step-downs along the front of the building, provides visual interest and helps soften the pedestrian interface between the building façade and the street frontage along both Holt Boulevard and Brooks Street.

Special attention was given to the use of color, finished materials, building form, massing, and the use of architectural details. This is exemplified through the use of:

- Articulation in the building's roof lines;
- Focal architectural towers that have been incorporated at different heights to add interest;
- A variety of horizontal cement board siding;
- Concrete Masonry;
- Spandrel glass;
- Cantilevered balconies and decorative canopies at the third, fourth and fifth floors;
- Extensive vision glass at the first and second floors;
- Exterior smooth stucco finish;
- Enhanced lighting fixtures;
- Front and side facing decorative railings at the third, fourth and fifth floor balconies;
- Incorporation of contrasting exterior building colors to add interest;
- Decorative aluminum storefront framing; and
- Deep insets and pop-outs (2.5 feet to 16 feet) at key areas of the building.



Figure 2: Holt Boulevard Perspective



Figure 3: Brooks Street Perspective

(e) Landscaping — The project exceeds the minimum landscape requirements established by the City's Development Code and the Roosevelt Tower PUD (see Exhibit F—Landscape Plan and Exhibit G—Landscape Plan Perspective, attached). The project will provide an average of 66 square feet of private open space per unit (60 square feet minimum required), in the form of private balconies, and 251.5 square feet of common open space for each unit (250 square feet minimum required). The common open space (14,853 square feet) will be provided on the third floor (podium deck) of the proposed five-story building, which is fully landscaped and amenitized. The third floor includes an exercise room, entertainment area, restrooms, built-in barbecues and serving counters, rubberized play surface, a decorative steel shade structure, decorative pavers, wood decking, decorative masonry block raised planters, decorative benches, lounge seating, dining tables and chairs, several decorative umbrella shades, two play structures and ping pong tables. The Holt Boulevard and Brooks Street setback areas both feature decorative paving and decorative potted plants.

(f) Signage —The project is required to comply with the Ontario Development Code and the Roosevelt Towers PUD. A sign program is required to be submitted for review and approval prior to any signs being installed on the property.

(g) Utilities (drainage, sewer) —The applicant has submitted a Preliminary Water Quality Management Plan ("PWQMP"), which establishes the Project's compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development ("LID") best management practices ("BMPs"), such as retention and infiltration, bio treatment, and evapotranspiration. The PWQMP proposes an underground infiltration system located beneath the podium parking structure along the south portion of the building. The system is designed to accept runoff from the building's roof and third floor courtyard.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

(1) City Council Goals.

- Invest in the Growth and Evolution of the City's Economy;
- Operate in a Businesslike Manner;
- Focus Resources in Ontario's Commercial and Residential Neighborhoods;

and

- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)

(2) Vision.

Distinctive Development:

- Commercial and Residential Development

➤ Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

(3) Governance.

Decision Making:

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.

➤ G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

(4) Policy Plan (General Plan)

Land Use Element:

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.

➤ LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.

➤ LU1-6 Complete Community: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).

- Goal LU2: Compatibility between a wide range of uses.

➤ LU2-6: Infrastructure Compatibility: We require infrastructure to be aesthetically pleasing and in context with the community character.

Housing Element:

- Goal H2: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.

- H2-5 Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices and other best practices.

- Goal H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.

- H5-2 Family Housing. We support the development of larger rental apartments that are appropriate for families with children, including, as feasible, the provision of services, recreation and other amenities.

Community Economics Element:

- Goal CE1: A complete community that provides for all incomes and stages of life.

- CE1-6 Diversity of Housing. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.

- Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.

- CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.

- CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.

- CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.

➤ CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Safety Element:

▪ Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.

➤ S1-1 Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

Community Design Element:

▪ Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

➤ CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.

➤ CD1-2 Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.

➤ CD1-3 Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.

▪ Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.

➤ CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:

- Building volume, massing, and height to provide appropriate scale and proportion;
- A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

➤ CD2-2 Neighborhood Design. We create distinct residential neighborhoods

that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:

- A pattern of smaller, walkable blocks that promote access, activity and safety;
- Variable setbacks and parcel sizes to accommodate a diversity of housing types;
- Traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows;
- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the “outdoor living room”), as appropriate; and
- Landscaped parkways, with sidewalks separated from the curb.

➤ CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.

➤ CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

➤ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➤ CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.

➤ CD2-11 Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.

➤ CD2-12 Site and Building Signage. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.

➤ CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

▪ Goal CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

➤ CD3-1 Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.

➤ CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.

➤ CD3-3 Building Entrances. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces.

➤ CD3-5 Paving. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.

➤ CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.

▪ Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.

➤ CD5-1 Maintenance of Buildings and Property. We require all public and privately-owned buildings and property (including trails and easements) to be properly and consistently maintained.

➤ CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

HOUSING ELEMENT COMPLIANCE: The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires

that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

ENVIRONMENTAL REVIEW: The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development) of the CEQA Guidelines, which consists of:

(1) The Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as with applicable zoning designation and regulations. The proposed Project is located within the MU (Mixed Use) land use district of the Policy Plan Land Use Map, and the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District) zoning district. The proposed project is consistent with all applicable Policy Plan (general plan) policies, as well as with the requirements of the Roosevelt Tower PUD, and applicable Development Code regulations.

(2) The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses. The Project is proposed within the established boundaries of the City of Ontario, on a project site totaling 0.81-acre of land, which is surrounded by established commercial land uses.

(3) The Project site has no value as habitat for endangered, rare, or threatened species. The site is void of any flora or fauna, was previously graded, and is regularly maintained to prevent the growth of weeds and grasses.

(4) Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality. The proposed mixed-use development is similar to, and of no greater impact than other allowed residential uses and commercial development projects within the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District) zoning district.

(5) The site can be adequately served by all required utilities and public services. The site is located within an area that is currently served by all required utilities and public services, including, but not limited to sewer, water, gas, electric and municipal solid waste services.

CONDITIONS OF APPROVAL: See attached department reports.

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

	Existing Land Use	General Plan Designation	Zoning Designation	Specific Plan Land Use
Site	Vacant Land	MU (Mixed-Use)	MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District)	N/A
North	Ontario Village Commercial Shopping Center & Auto Smog Shop	MU (Mixed-Use)	MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District)	N/A
South	Industrial	Industrial	IL (Light Industrial)	N/A
East	Multi-Tenant Commercial	MU (Mixed-Use)	MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District)	N/A
West	Commercial	MU (Mixed-Use)	MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District)	N/A

Off-Street Parking:

Use	Ratio	Area (in SF) or No. Dwelling Units (DUs)	Total Required	Provided
Commercial	4 spaces/1,000 SF	2,500 SF	10	10
Studio	1.5 spaces/DU	13 DUs	19	19
One-Bedroom	1.75 spaces/DU	30 DUs	52	53 **
Two-Bedroom	2 spaces/DU	16 DUs	32	32
Guest	Portion DUs <50: 0.25 space/du Portion DUs 50 to 100: 0.2 space/DU	50 DUs 9 DUs	14	14
Total			127	128

Note: **14 residential parking spaces (12%) are tandem

General Site & Building Statistics

Item	Required Min./Max.	Provided (Ranges)	Meets Y/N
Project area (in acres):	0.81 Acres	0.81 Acres	Y
Maximum project density (dwelling units/ac):	25.1 -75.0 du/ac	72.8 du/ac	Y
Front yard setback (in FT):	2'	5'	Y
Side yard setback (in FT):	1'	1'	Y
Rear yard setback (in FT):	2'	2'	Y
Maximum dwelling units:	59	59	Y

<i>Item</i>	<i>Required Min./Max.</i>	<i>Provided (Ranges)</i>	<i>Meets Y/N</i>
<i>Maximum height (in FT):</i>	75'	68'	Y
<i>Parking – resident:</i>	11	118	Y
<i>Commercial -Parking</i>	10	10	Y
<i>Parking – guest:</i>	14	14	Y
<i>Open space – private:</i>	60 sq. ft.	66 sq. ft.	Y
<i>Open space – common:</i>	250 sq. ft. per unit	251.5 sq. ft. per unit	Y

Dwelling Unit Statistics:

<i>Plan No.</i>	<i>Area (In SF)</i>	<i>Total Units</i>	<i>No. Bedrooms</i>	<i>No. Baths</i>	<i>Private Open Space (in FT)</i>
U-1	403	13	Studio	1 bath	66
U-2	987	6	2-bedrooms	2 baths	66
U-3	804	3	2 bedrooms	1 bath	66
U-4	571	21	1 bedroom	1 bath	66
U-5	532	9	1 bedroom	1 bath	66
U-6	860	7	2 bedrooms	2 baths	66

Exhibit A—PROJECT LOCATION MAP



Exhibit C—FLOOR PLANS

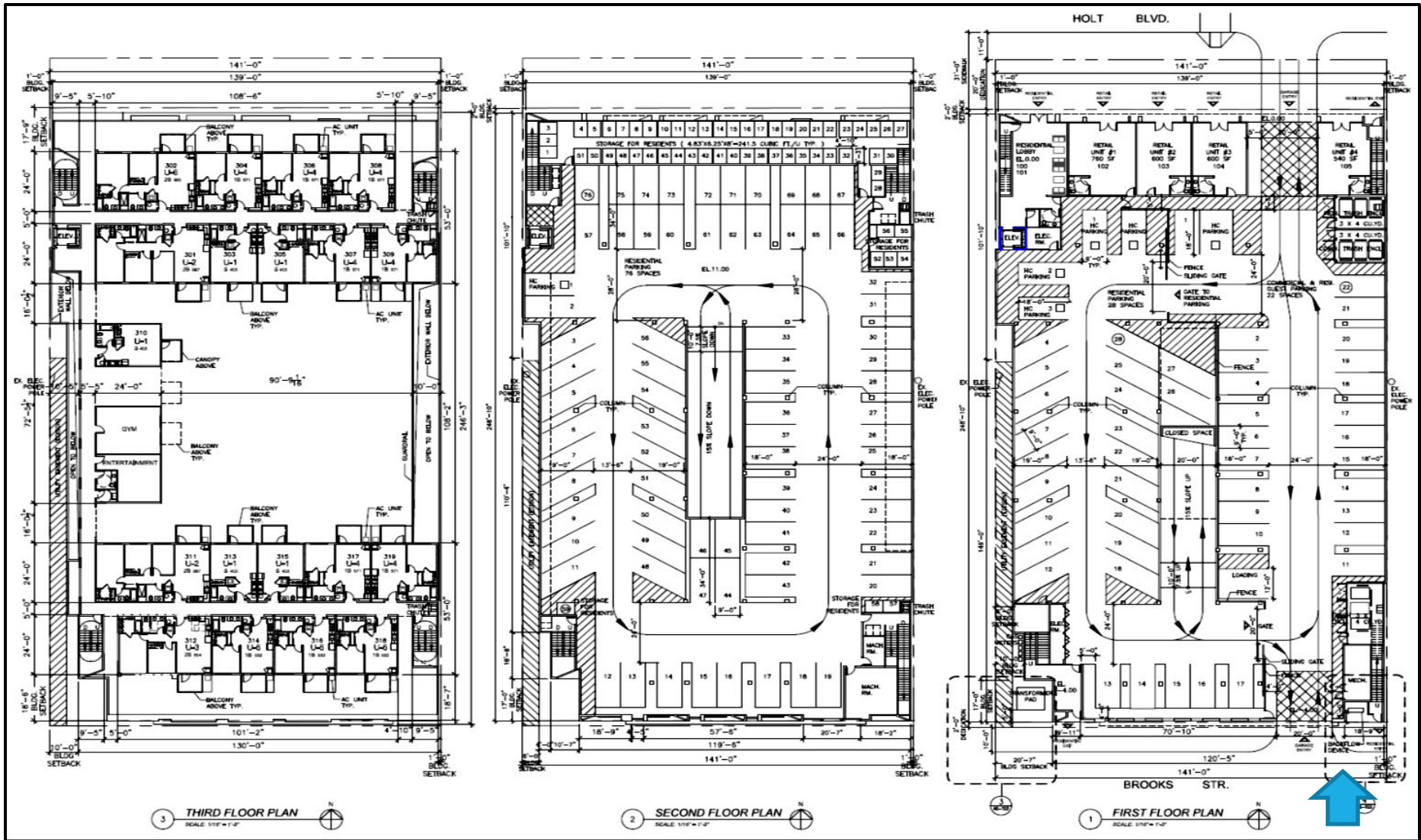


Exhibit D—FLOOR PLANS

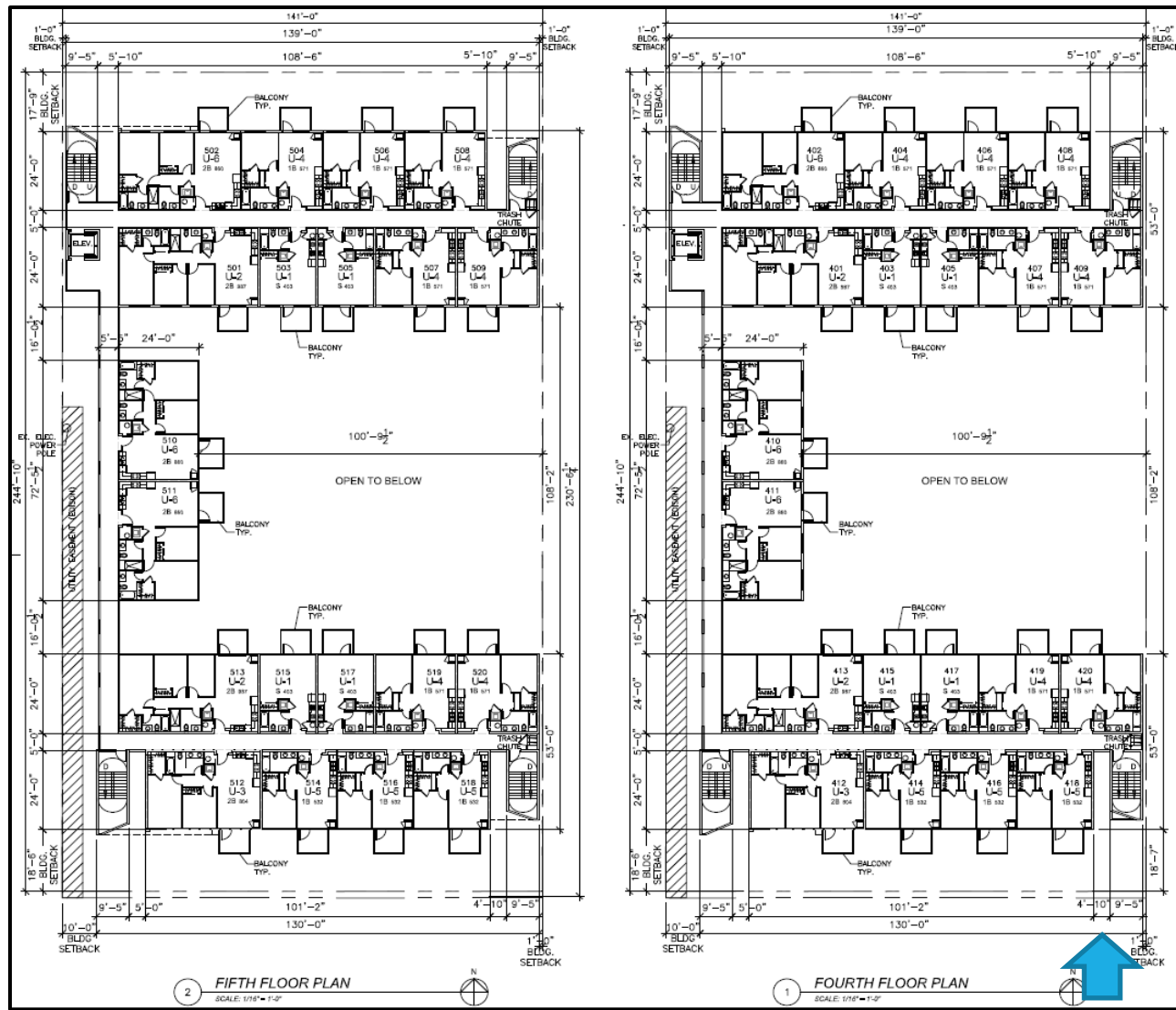


Exhibit E—ELEVATIONS



Exhibit E—ELEVATIONS (Continued)



Exhibit F— LANDSCAPE PLAN

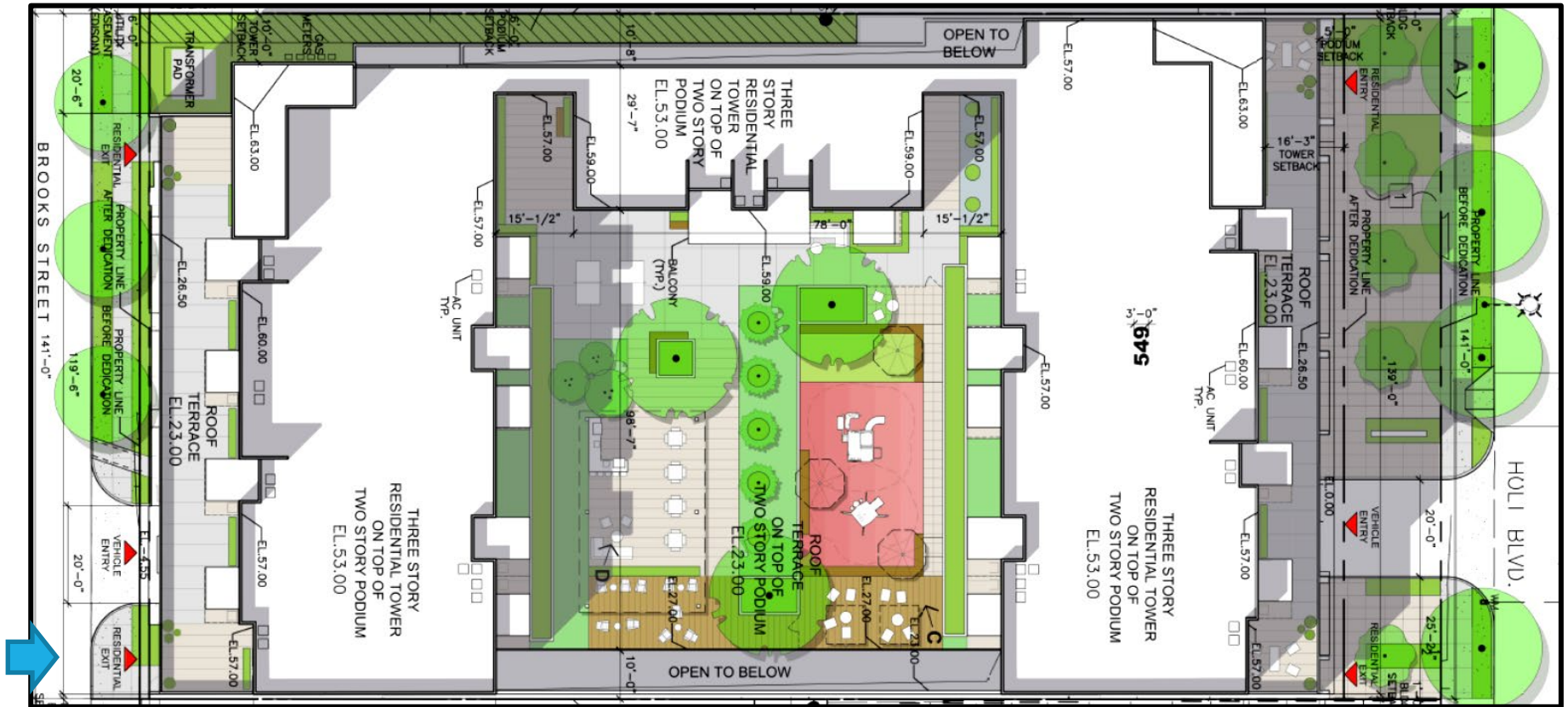


Exhibit G—LANDSCAPE PLAN PERSPECTIVES



RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING THE CITY COUNCIL APPROVE FILE NO. PUD20-001, A PLANNED UNIT DEVELOPMENT (ROOSEVELT TOWER PLANNED UNIT DEVELOPMENT) TO ESTABLISH DEVELOPMENT STANDARDS, DESIGN GUIDELINES AND INFRASTRUCTURE REQUIRMENTS ON 0.81 ACRES OF LAND LOCATED AT 549 WEST HOLT BOULEVARD, WITHIN THE MU-1/LUA-3 (DOWNTOWN MIXED-USE/HOLT BOULEVARD) ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF—APN: 1049-021-09.

WHEREAS, Kathy Huynh ("Applicant") has filed an Application for the approval of a Planned Unit Development, File No. PUD20-001, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 0.81 acres of land located at 549 West Holt Boulevard, within the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard) zoning district and the site is presently vacant land; and

WHEREAS, the property to the north of the Project site is within the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard) zoning district and is developed adjacent to the Ontario Village Commercial Shopping Center. The property to the south is within the IL (Light Industrial) zoning district and is developed with a mix of industrial uses. The property to the east and west are within the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard) zoning district and are developed with multi-tenant commercial uses; and

WHEREAS, in conjunction with the Planned Unit Development (Roosevelt Tower Planned Unit Development) application, the Applicant has submitted a Development Plan Application (File No. PDEV20-009) application to construct a mixed-use project consisting of 59 multiple-family dwellings (three-stories of residential units over two-stories of podium parking and 2,500 square feet of first floor retail space) on the Project site. Approval of the Development Plan application will not be final and complete until the Roosevelt Tower Planned Unit Development, File No. PUD20-001, has been approved by the City Council; and

WHEREAS, the Ontario Plan Policy Plan (general plan) designates the Project area as MU (Mixed-Use). The Ontario Plan describes the Downtown Mixed-Use Area as an intensive vertical and horizontal mixture of retail, office, and residential uses in a pedestrian friendly atmosphere. The most intensive uses are envisioned along Euclid and Holt Avenue. The Downtown Mixed Use is also intended to provide new housing opportunities that will also provide increased demand for retail in more concentrated, strategic locations. The densities and intensities of the mixed-use designation represented within The Ontario Plan ("TOP") are the intended level of anticipated

development; however, individual projects may vary depending upon an approved master plan, such as an area plan, specific plan, or planned unit development; and

WHEREAS, the Policy Plan specifies that the Downtown Mixed-Use Area is to be implemented through the approval of an Area Plan or Planned Unit Development ("PUD") prior to the development of properties within the area. In compliance with this requirement, the Applicant has submitted the "Roosevelt Tower Planned Unit Development" (included as "Attachment A" to this resolution), which is consistent with this vision and the goals and policies of the Policy Plan; and

WHEREAS, the purpose of the PUD is to secure a fuller realization of the Policy Plan than would result from the strict application of present zoning district regulations and to:

- Promote high standards in urban design;
- Encourage the development of exceptionally high quality, mixed-use, medium to high intensity projects, while establishing regulations and standards for uses with unique regulatory and design needs;
- Ensure harmonious relationships with surrounding land uses; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act, commencing with Public Resources Code Section 21000 (hereinafter referred to as "CEQA"); and

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and make recommendation to the City Council on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside,

and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (“ALUCP”), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on July 26, 2022, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the recommending authority for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The administrative record has been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(2) The Project is categorically exempt from environmental review pursuant to Section 15332 (Class 32, In-Fill Development,) of the CEQA Guidelines, which consists of projects characterized as infill developments, meeting the following conditions:

a) The Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations. The proposed Project is located within the MU (Mixed Use) land use district of the Policy Plan Land Use Map, and the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District) zoning district. The development standards and conditions under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans and exhibits of the Policy Plan (General Plan) component of the Ontario Plan.

b) The proposed development occurs within city limits, on a Project site of no more than five acres and is substantially surrounded by urban uses. The Project site is 0.81-acres of land, located within the incorporated boundaries of the City of Ontario. Furthermore, the site is situated within a fully urbanized area that is characterized by a mix of industrial and multi-tenant commercial developments.

c) The Project site has no value as habitat for endangered, rare, or threatened species. The site is currently vacant land, with no value to endangered, rare or threatened species.

d) Approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality. Conditions of approval have been imposed on the Project, which will mitigate any significant effects relating to traffic, noise, air quality, or water quality.

e) The Project can be adequately served by all required utilities and public services. The site is located within an area that is currently served by all required utilities and public services, including, but not limited to sewer, water, gas, electric and municipal solid waste services; and

(3) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

(4) The determination of CEQA exemption reflects the independent judgment of the Planning Commission.

SECTION 2: *Housing Element Compliance.* Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the recommending authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the Project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 3: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as “ONT”), which encompasses lands

within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the recommending authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the Planning Commission, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 4: Concluding Facts and Reasons. Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 through 3, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed PUD, or amendment thereto, is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Project is located within the MU (Mixed Use) land use district of the Policy Plan Land Use Map, and the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District) zoning district. The development standards and conditions under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.

(2) ***The proposed PUD, or amendment thereto, would not be detrimental to the public interest, health, safety, convenience, or general welfare of the City.*** The Planning Commission has required certain safeguards, and imposed certain conditions of approval, which have been established to ensure that: [i] the purposes of the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District) zoning district and the Roosevelt Tower PUD are maintained; [ii] the Project will not endanger the public health, safety or general welfare; [iii] the Project will not result in any significant environmental impacts; [iv] the Project will be in harmony with the area in which it is located; and [v] the Project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan vision, and the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard) zoning district and the Roosevelt Tower PUD.

(3) ***In the case of an application affecting specific property, the proposed PUD, or amendment thereto, will not adversely affect the harmonious relationship with adjacent properties and land uses.*** A thorough review and analysis of the

proposed Project and its potential to adversely impact properties surrounding the subject site was completed by City staff. As a result of this review, certain design considerations were incorporated into the Project to mitigate identified impacts to an acceptable level, including the use of upgraded materials, the inclusion of certain architectural design elements on building exteriors, intensified landscape elements, and decorative hardscape elements. In addition, conditions of approval have been imposed on the Project, which will mitigate any significant effects relating to traffic, noise, air quality, or water quality.

(4) ***In the case of an application affecting specific property, the subject site is physically suitable, including, but not limited to, parcel size, shape, access, and availability of utilities, for the request and anticipated development.*** In preparing the proposed PUD, a thorough review and analysis of the proposed Project and the Project site's physical suitability for the proposed Project was completed, including analysis of the Project size, shape, intensity of development, building height, building setbacks, site access, site landscaping and drainage, fences and walls, vehicle circulation, pedestrian connections, availability of mass transit, necessary street dedication and easements, public right-of-way improvements, availability of utilities and other infrastructure needs, off-street parking and circulation, building orientation and streetscapes, architectural character, building materials and color, and site signage.

(5) ***The proposed PUD is superior to that which could be obtained through the application of the Development Code or a specific plan.*** The proposed PUD addresses aspects of the Project that are specifically related to the proposed Development Plan, File No. PDEV20-009, filed in conjunction with the PUD, including necessary building setbacks, site access points, off-street parking, site circulation, and architectural character.

SECTION 5: Planning Commission Action. Based upon the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby RECOMMENDS THE CITY COUNCIL APPROVE the herein described PUD, attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 6: Indemnification. The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 7: Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 8: **Certification to Adoption.** The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26th day of July 2022, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Jim Willoughby
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on July 26, 2022, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:
File No. PUD20-001
(Roosevelt Tower Planned Unit Development)

(Document to follow this page)



ROOSEVELT TOWER

HOLT BOULEVARD, ONTARIO, CALIFORNIA



MAY 02, 2022

PLANNED UNIT DEVELOPMENT

The Roosevelt Tower Planned Unit Development

Table of Contents

1 – Introduction	6
2 – Objectives	9
2.1 – The Ontario Plan (TOP) Objectives.....	9
2.2 – PUD District Purpose And Objectives	19
3 – Allowable Uses	20
3.1 – Residential Use	21
3.2 – Retail Use	21
3.3 – Permitted Use Table/List	21
4 – Development Regulations	26
4.1 – Residential Density	26
4.2 – Building Height	27
4.3 – Setbacks	28
4.4 – Open Space Requirements	31
4.5 – Parking Requirements	34
4.6 – Vehicular Access	36
4.7 – Service Facilities	38
4.8 – Landscaping	40
4.9 – Screening	47
5 – Circulation	48
5.1 – Site Accessibility	48
5.2 – Vehicular Circulation	48
5.3 – Pedestrian Circulation	48
5.4 – Integrated Waste Pickup Circulation	49
6 – Design Guidelines	54
6.1 – Building Orientation	54
6.2 – Architectural Character	57
6.3 – Massing and Scale	57
6.4 – Architectural Details	57
6.5 – Materials and Color	58
6.6 – On-Site Streetscapes and Pedestrian Walkways	58
6.7 – On-Site Lighting	58

6.8 – Signs 59

6.9 – Off-Site Improvements 59

7 – Infrastructures and Utilities 61

7.1 – Infrastructure 61

7.2 – Street Improvements 61

7.3 – Public Utilities 62

7.4 - Street Dedication, Vacation and Easements 62

8 – CEQA Compliance 67

8.1 – Environmental Performance Standard 67

9 – Administration 70

9.1 – Items Not Addressed in PUD 70

9.2 – Development Applications 70

9.3 – Administrative Exceptions 70

9.4 – Severability 70

Exhibits

Exhibit 1-1: Vicinity Map 5

Exhibit 1-2: Location Map 6

Exhibit 1-3: Land Use Map 7

Exhibit 1-4: Existing Site Map 8

Exhibit 1-5: Proposed Site Map 8

Exhibit 3-1: Land Use Plan 20

Exhibit 4-1: Podium Diagram 26

Exhibit 4-2: Height Diagram 27

Exhibit 4-3: Setback Diagram 29

Exhibit 4-4: Projection Diagram 30

Exhibit 4-5: Private Open Space Diagram 32

Exhibit 4-6: Common Open Space 33

Exhibit 4-7: On-Street Parking 36

Exhibit 4-8: Trash Enclosures Site Plan 39

Exhibit 4-9: Colored Landscape Plan 42

Exhibit 4-10: Holt Boulevard Planting Palette 43

Exhibit 4-11: Common Space Planting Palette 44

Exhibit 4-12: Brooks Street Planting Palette 45

Exhibit 4-13: Landscaping Rendering..... 46

Exhibit 5-1: Circulation Plan 53

Exhibit 6-1: Architectural Rendering 55

Exhibit 6-2: Architectural Rendering 55

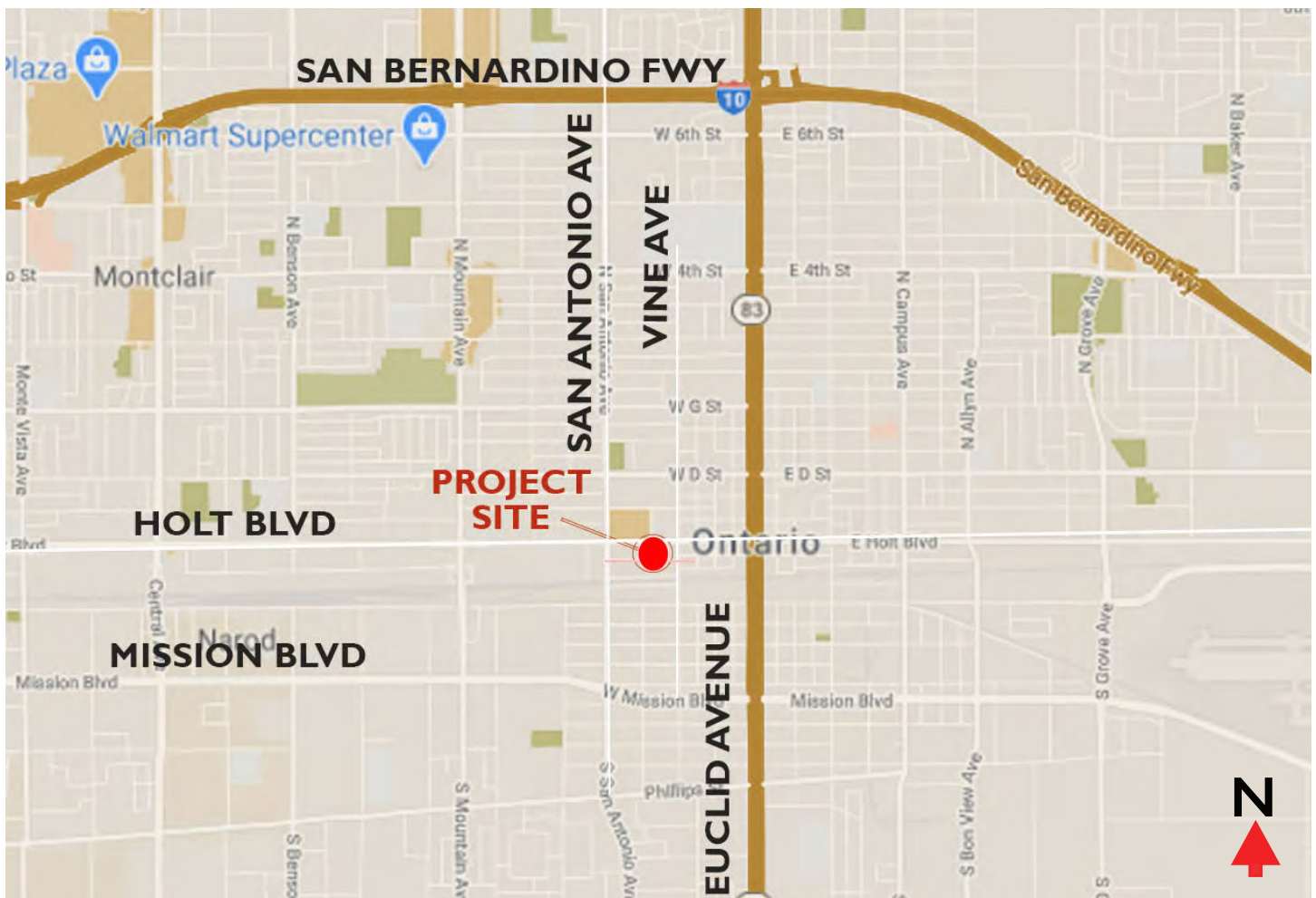
Exhibit 6-3: Architectural Rendering 56

Exhibit 6-4: Architectural Rendering 56
 Exhibit 6-5: Streetscape Examples 60
 Exhibit 7-1: Civil Plan 63
 Exhibit 7-2: Utility Plan 64
 Exhibit 7-3: Easement and Dedication Locations 65
 Exhibit 7-4: Street Sections 66

Tables

Table 3-1: Permitted Use Table 22
 Table 4-1: Minimum Building Setbacks 28
 Table 4-2: Maximum Encroachments into Building Setbacks 28
 Table 4-3: Private Open Space 31
 Table 4-4: Minimum Parking Requirements 35

Exhibit 1-1: Vicinity Map



1 – INTRODUCTION

This document is intended to function as a set of planning and design principles, development regulations and performance standards to guide and govern the development of a 57-Unit Residential and Mixed-use Complex located at 549 W. Holt Boulevard in the City of Ontario. The development is a 5-story residential and retail complex bounded by Holt Avenue on its north side and Brooks Street at its south side (see Exhibit 1-2: Location Map).

The 35,085 SF project site is located within the Ontario Plan Downtown Mixed-Use District (MU-1) which requires the establishment of a Planned Unit Development (PUD) prior to development. The Planned Unit Development (PUD) is set forth in the Ontario Development code section 4.01.030 (Planned Unit Development (PUD) and Amendments). Upon approval, this PUD will establish the land use and development standards for this particular project site. The Roosevelt Tower on Holt Planned Unit Development (PUD) will replace the existing zone district designations and zoning standards that apply to the affected properties. Unless otherwise defined herein, definitions and interpretations contained in the Development Code shall apply.

City staff and private developers shall rely on this PUD to determine whether precise plans for development will be adequate and will meet the City's land use and design objectives.

Exhibit 1-2: Location Map

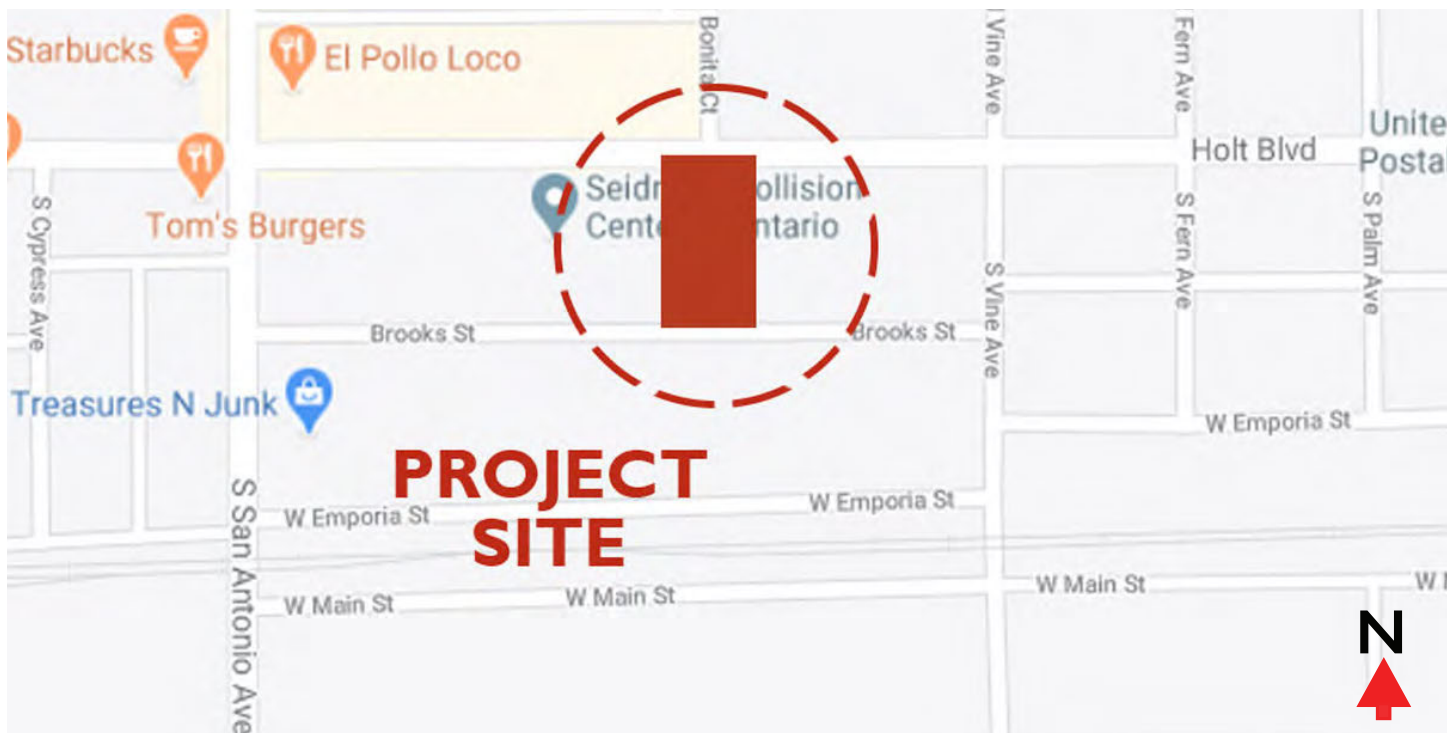


Exhibit 1-3: Land Use Map: MU-1 (Mixed-Use Zone 1)

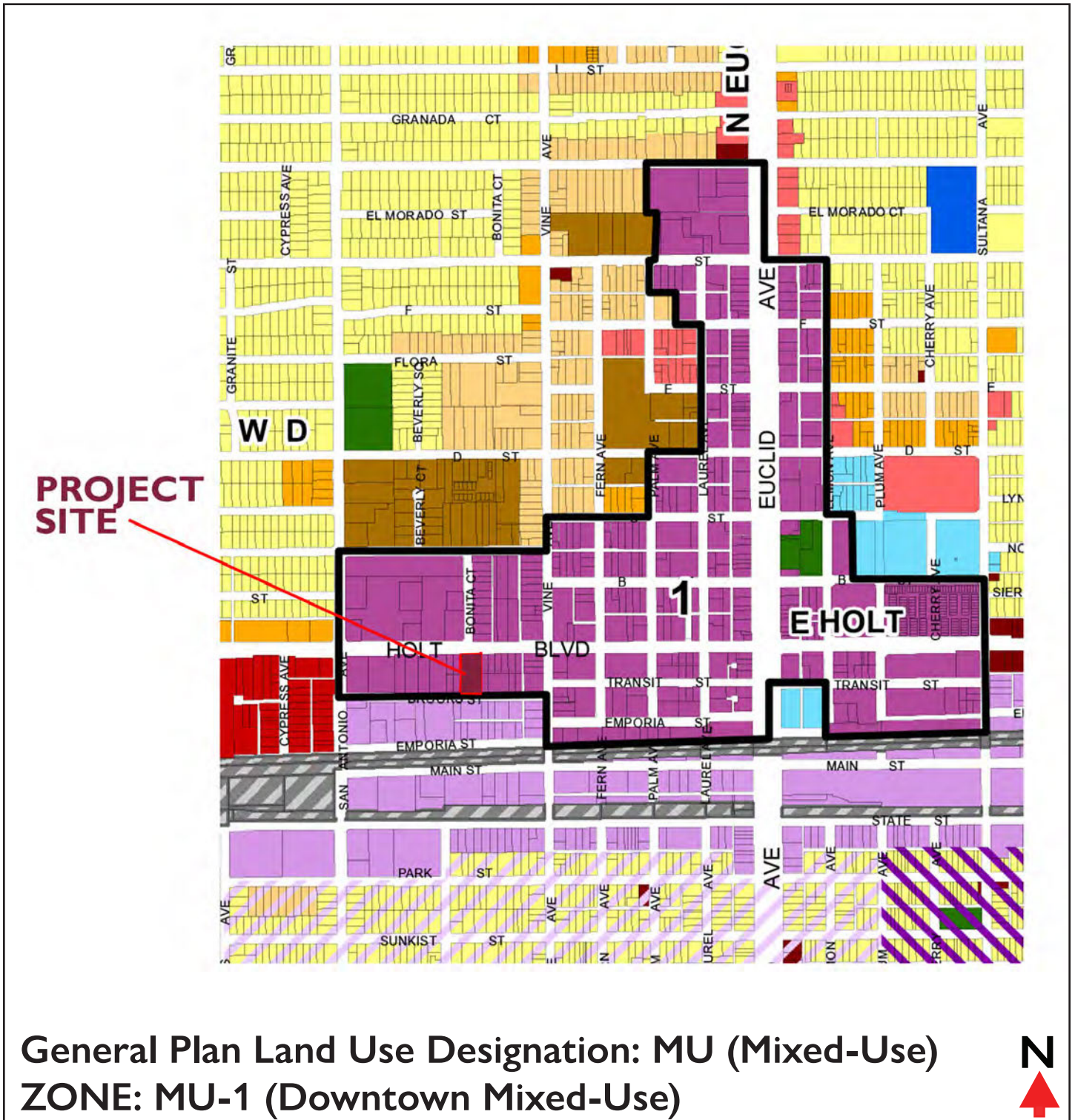


Exhibit 1-4: Existing Site Map



Exhibit 1-5: Proposed Site Map



2 – P.U.D. OBJECTIVES

2.1 – The Ontario Plan (TOP) Objectives

The Ontario Plan, which includes the City's General Plan, designates the project area as part of the Downtown Mixed Use District. The Downtown Mixed Use District designation requires approval of an Area Plan or Planned Unit Development to develop the property. Additionally, the designation specifies a residential density range of 25.0 to 75.0 units per acre and a maximum floor area ratio (FAR) of 2.0 for office and retail uses.

The intent of this designation is to:

1. Create “an intensive vertical and horizontal mixture of residential and retail uses in a pedestrian friendly atmosphere;
2. Ensure the historic character is enhanced when required or appropriate; and
3. Concentrate intensive Land uses on Holt Avenue.

The Holt & Brooks PUD is consistent with the principles, goals and policies contained within the components that make up The Ontario Plan (TOP), including: (1) Vision, (2) Policy Plan (General Plan), and (3) City Council Priorities. The policies furthered by this PUD are as follows:

1. Vision

On February 13, 2007, the Ontario City Council adopted The Ontario Vision. The central theme that motivates the Vision is - “A sustained, community-wide prosperity which continuously adds value and yields benefits. “Everything the City does and every action the City takes is done with the simple yet comprehensive theme in mind, from the design quality of the built environment, to the intent of designing socio-economic programs, to the way in which its leaders govern as a community. In discussing a Vision that would endure for the lifetime of The Ontario Plan (30 years or more), the City Council recognized that there are four components that serve as the basic building blocks that set the foundation for a unified and prosperous community. These foundational blocks must be expressed and widely accepted throughout the Plan's lifetime. They are:

1. A Dynamic Balance that enables our community to confront the continued dynamic growth of the region and technological change with confidence and a sense of opportunity.

2. A Prosperous Economy that sustains the perception and reality of prosperity across our entire community that positively impacts all the people of Ontario and is broadly - though not uniformly - shared.
3. Distinctive Development that integrates our varied and diverse focal points, districts, villages, and neighborhoods to provide a feeling of coherence without sacrificing uniqueness.
4. Recognized Leadership in local governance that stimulates excellence and serves to unify the people of Ontario in support of best practices in conducting public endeavors.

Holt & Brooks PUD will implement the Ontario Vision in several ways that will further the City's desire to be a sustainable and prosperous community.

DISTINCTIVE DEVELOPMENT

Commercial and Residential Development

- The PUD will support new multifamily developments and existing residential neighborhoods by providing the increased demand for retail in more concentrated, strategic location (e.g., at major intersections) as intended for the Downtown Mixed-Use Area.
- The PUD will seek to ensure a mix of retail and commercial uses that will provide a variety of goods and service to the surround community and the future Bus Rapid Transit (BRT) users.

Design Quality

- The PUD will require a well-designed projects that convey a visual interest and character through:
 - ▶ Appropriate scale and massing
 - ▶ Architectural style and design that are complementary and appropriate to its setting.
 - ▶ The use of high quality materials that will create visual interest, high quality, durable and appropriate for the architectural style.

- ▶ Appropriate Site Planning to ensure building orientation visibility from the street, adequate number of driveways and sidewalks. Building setbacks, parking lot orientation, adequate refuse storage areas and well designed street frontages.

Public Safety

- The PUD will ensure that proper exterior lighting standards be located and designed to minimize direct glare beyond the projects parking levels and into any future residential developments to the west and east of the project site. In addition, working with the Police Department, to ensure that all lighting standards comply with the one-foot candle illumination to provide a level of security and public safety within the project's retail areas as well as in the existing commercial areas adjacent to the project site.

2. Policy Plan

LAND USE ELEMENT

LU1 Balance

Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.

The PUD, through the development of retail and commercial uses, will help provide jobs to help support the existing and new development within the sunounding area.

Policies:

- *LU1-3 Adequate Capacity. We require adequate infrastructure and services for all development.*

The PUD, along with the future development entitlements for the site, will require the necessary infrastructure to serve the site and provide the necessary right-of-ways dedication for street widening and to accommodate the future Bus Rapid Transit (BRT) for the SBX West Valley Connector.

- *LU1-4 Mobility. We require development and urban design, where appropriate, that reduces reliance on the automobile and capitalizes on multi-modal transportation opportunities.*

LU2 Compatibility

Goal LU2: Compatibility between a wide range of uses.

The PUD, through land use regulations for permitted uses, have ensured that those permitted uses allowed within the PUD are compatible with the surrounding area and not allow those uses that may have potential to create adverse impacts to the surrounding area.

Policies:

- *LU2-1: Land Use Decisions: We minimize adverse impacts on adjacent properties when considering land use and zoning requests.*

The project area is zoned MU-2 (Downtown Mixed Use) and require that a PUD be implemented for the project area to establish the development standards, land use regulations and design guidelines. The permitted uses within PUD have been established to provide retail and commercial uses to serve the surrounding residential area. Careful consideration has been given to ensure that permitted uses that may have potential to create adverse impacts to the surrounding area are not allowed.

- *LU2-2 Buffers. We require new uses to provide mitigation or buffers between existing uses where potential adverse impacts could occur.*

The PUD, through site planning, we will require the that future building developed on the site shall be placed along the frontages of Holt Boulevard to keep a buffer between the future residential development.

- *LU2-4 Regulation of Nuisances. We regulate the location, concentration and operations of potential nuisances.*

The permitted uses within PUD have been established to provide retail and commercial uses to serve the surrounding residential area. Careful consideration has been given to ensure that permitted uses that may have potential to create adverse impacts to the surrounding area are not allowed. Those uses with operations that may have potential to create nuisances, will require discretionary approval through the Conditional Use Permit process.

- *LU2-5 Regulation of Uses. We regulate the location, concentration and operations of uses that have impacts on surrounding land uses.*

The project area is zoned MU-1 (Downtown Mixed Use) and requires that a PUD be implemented for the project area to establish the development standards, land use regulations and design guidelines. The permitted uses within PUD have been established to provide retail and commercial uses to serve the surrounding residential area. Careful consideration has been given to ensure that permitted uses that may have potential to create adverse impacts to the surrounding area are not allowed.

LU3 Flexibility

Goal LU3 Staff, regulations and processes that support and allow flexible response to conditions and circumstances in order to achieve the Vision.

The Ontario Plan describes the Downtown Mixed Use Area as an intensive vertical and horizontal mixture of retail, office, and residential uses in a pedestrian friendly atmosphere. The historic character is enhanced. To provide flexibility the densities and intensities of the mixed use designation represented within TOP are the intended level of anticipated development; however, individual projects may vary depending upon an approved master plan, such as an area plan, specific plan, or planned unit development. The project area is zoned MU-1 (Downtown Mixed Use) and requires that a PUD be implemented for the project area to establish the development standards, land use regulations and design guidelines.

Policies:

- *LU3-1 Development Standards. We maintain clear development standards which allow flexibility to achieve our Vision.*

To provide flexibility the densities and intensities of the mixed use designation represented within TOP are the intended level of anticipated development; however, individual projects may vary depending upon an approved master plan, such as an area plan, specific plan, or planned unit development. The project area is zoned MU-1 (Downtown Mixed Use) and requires that a PUD be implemented for the project area to establish the development standards, land use regulations and design guidelines. The development standards within the PUD are intended to ensure quality and appropriate development to achieve TOP Vision for well-designed projects that convey visual interest.

COMMUNITY ECONOMICS ELEMENT

CE1 Complete Community

Goal CE1: A complete community that provides for all incomes and stages of life. The permitted uses within PUD have been established to provide retail and commercial uses to serve the surrounding residential area and contribute to the City's tax sales base.

CE2 Place-Making

Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.

The PUD will seek to ensure a mix a retail and commercial uses that will provide a variety of goods and service to the surround community and the future Buss Rapid Transit (BRT) users. The PUD area will be distinctive through the requirement of a well-designed projects that convey visual interest and character through:

- ▶ Appropriate scale and massing.
- ▶ Architectural style and design that are complementary and appropriate to for its setting.
- ▶ The use of high quality materials that will create visual interest, high quality, durable and appropriate for the architectural style.

- ▶ Appropriate Site Planning that to ensure building orientation visibility form the street, adequate number of driveways and sidewalks. Building setbacks, parking lot orientation, adequate refuse storage areas and well-designed street frontages.

MOBILITY ELEMENT

M3 Public Transit

Goal M3: A public transit system that is a viable alternative to automobile travel and meets basic transportation needs of the transit dependent.

The PUD, along with the future development entitlements for the site, will require the necessary infrastructure to serve the site and provide the necessary right-of-ways dedication for street widening and to accommodate the future Bus Rapid Transit (BRT) for the SBX West Valley Connector and bus stop along the frontage of Holt Boulevard.

COMMUNITY DESIGN

CD1 Image & Identity

Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

- To provide flexibility the densities and intensities of the mixed use designation represented within TOP are the intended level of anticipated development; however, individual projects may vary depending upon an approved master plan, such as an area plan, specific plan, or planned unit development. The project area is zoned MU-1 (Downtown Mixed Use) and requires that a PUD be implemented for the project area to establish the development standards, land use regulations and design guidelines. The development standards within the PUD are intended to ensure quality and appropriate development to achieve TOP Vision for well-designed projects that covey visual interest.

Policies:

- *CD1-2 Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.*

The entire project area is covered by the Ontario Plan Policy Plan (General Plan) and has been designated as mixed-use (Holt Boulevard) and as described in the Ontario Plan Downtown Mixed-Use Growth Area. This PUD area will be distinctive through the requirement of a well-designed projects that convey visual interest and character through:

- ▶ Appropriate scale and massing.
 - ▶ Architectural style and design that are complementary and appropriate to its setting.
 - ▶ The use of high quality materials that will create visual interest, high quality, durable and appropriate for the architectural style.
 - ▶ Appropriate Site Planning that to ensure building orientation visibility form the street, adequate number of driveways and sidewalks. Building setbacks, parking lot orientation, adequate refuse storage areas and well designed street frontages.
- *CD1-4 Transportation Corridors. We will enhance our major transportation corridors within the City through landscape, hardscape, signage and lighting.*

The PUD, along with the future development entitlements for the site, will require the necessary infrastructure to serve the site and provide the necessary rights-of-way dedications for street widening and to accommodate the future Bus Rapid Transit (BRT) for the SBX west Valley Connector and any future bus stop site frontage along Holt Boulevard. In addition, the PUD will require adequate landscaping, hardscape, signage and lighting to the prominent areas along Holt Boulevard as well as the secondary face of the project along Brooks Street to the south of the project site.

CD2 Design Quality

Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.

Policies:

- *CD2-I Quality Architecture. We encourage all development projects to convey visual interest and character through:*
 - building volume, massing, and height to provide appropriate scale and proportion;

- a true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
- exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

The PUD area will be distinctive through the requirement of a well-designed projects that convey visual interest and character through:

- ▶ Appropriate scale and massing
 - ▶ Architectural style and design that are complementary and appropriate to for its setting.
 - ▶ The use of high quality materials that will create visual interest, high quality, durable and appropriate for the architectural style.
 - ▶ Appropriate Site Planning that to ensure building orientation visibility from the street, adequate number of driveways and sidewalks. Building setbacks, parking lot orientation, adequate refuse storage areas and well designed street frontages.
- *CD2-3 Commercial Centers. We desire commercial centers to be distinctive, pedestrian friendly, functional and vibrant with a range of businesses, places to gather, and connectivity to the neighborhoods they serve.*

The permitted uses within PUD have been established to provide retail and commercial uses to serve the surrounding residential area. These retail and commercial uses will be provided to the area currently lacking close access to a retail center. The PUD will seek to capitalize on the location of the station by ensuring a mix a retail and commercial uses business that will provide a variety of goods and service to the surround community and the future Buss Rapid Transit (BRT) users.

- *CD2-5 Streetscapes. We design new and, when necessary, retrofit existing streets to improve walkability, bicycling and transit integration, strengthen connectivity, and enhance community identity through improvements to the public right of way such as sidewalks, street trees, parkways, curbs, street lighting and street furniture.*

The PUD, along with the future development entitlements for the site, will require the necessary infrastructure to serve the site and provide the necessary right-of-ways dedication for street widening and to accommodate the future Bus Rapid Transit (BRT) for the SBX West Valley Connector. In addition, the future development entitlements will require site improvements to the public right of way such as sidewalks, street trees, parkways, curbs, street lighting and street furniture.

- *CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.*

The PUD development standards that landscape design, materials and will be used to enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

- *CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.*

The PUD development standards will ensure water conservation be provided through low water using plant materials, hydro zones, water efficient irrigation and weather based controllers. Landscaped areas may be used for storm water infiltration through vegetated swales, retention basins, or dry wells as needed with the use of appropriate planting materials. Broad canopy shade trees will be used to reduce heat gain on buildings, paving and parking areas.

3. City Council Priorities

- Invest in the Growth and Evolution of the City's Economy
- Maintain the Current High Level of Public Safety
- Operate in a Businesslike Manner
- Pursue City's Goals and Objectives by Working with Other Governmental Agencies
- Focus Resources in Ontario's Commercial and Residential Neighborhoods

- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)
- Encourage, Provide or Support Enhanced Recreational, Educational, Cultural and Healthy City Programs, Policies and Activities
- Ensure the Development of a Well Planned, Balanced, and Self-Sustaining Community in Ontario Ranch

2.2 – PUD District Purpose And Objectives

The purpose of The Roosevelt Tower (PUD) is to secure a fuller realization of the general plan than that which would result from the application of present zone district regulations; to promote high standards in urban design; to encourage the development of exceptionally high quality residential and commercial uses, while establishing regulations and standards for uses with special conditions and regulatory needs to ensure harmonious relationships with other land uses.

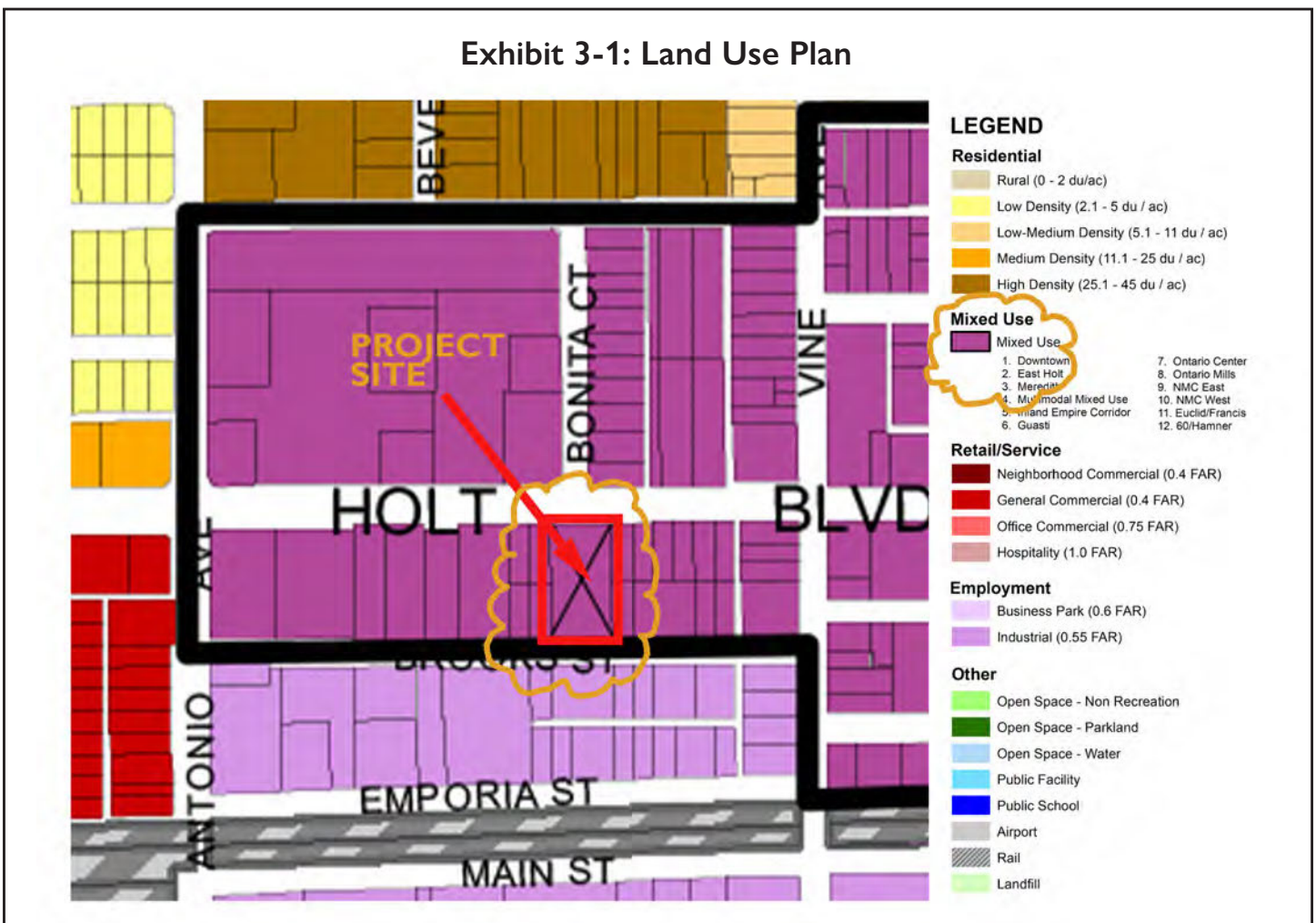
The intent of the PUD District is to:

- Promote development projects that are consistent with the land use/downtown revitalization objectives of the City Redevelopment Project area.
- Promote residential and community commercial uses to meet the needs of the projects surroundings residential uses.
- Promote the plaza areas within the commercial center to providing for social interaction.
- Orient buildings to the street, wherever possible to create an accessible urban edge and sense of arrival.
- Incorporate landscaping to enhance the environment.

3 – ALLOWABLE USES

The project site is located within the Downtown Mixed Use Area Policy Plan (General Plan) land use district, and the MU-1 (Downtown Mixed-Use) zoning district, which implements the Downtown Mixed Use Area. The MU-1 zoning district is established to accommodate an intensive mixture of vertical and horizontal retail and office uses at a development intensity of up to 2.0 FAR, and residential uses at a density of 25 to 75 dwelling units per acre. Development projects within the MU-1 zoning district are intended to maintain a pedestrian friendly atmosphere, while at the same time enhancing the historic character of the area. The most intensive uses within this district are envisioned along Euclid Avenue and Holt Boulevard.

Consistent with the intent of the Downtown Mixed Use Area and the MU-1 zoning district, the project site is designated for Multiple-Family Residential (MFR) land uses. Exhibit 3-1 (Land Use Plan), below, depicts the Planned Unit Development boundary and land use designation.



3.1 – Residential Use

The project site is proposed to be developed with high quality, multi-family housing units, either two or three levels over two levels of podium parking. The buildings will be a combination of podium-style stacked flat units, with parking beneath, and townhome style units, with first floor garage access.

3.2 – Retail Use

As part of the mixed-use character of the project, a 2,740 SF retail component will be incorporated on the first level of the podium of the building. This total area is well within the 2.0 FAR allowed by city and is in line with the city's vision for Holt Avenue to be an intensive vertical and horizontal mixture of retail, office, and residential uses, in a pedestrian friendly atmosphere.

3.3 – Permitted Use Table/List

Some of the permitted land uses within The Roosevelt Tower PUD are as follows:

1. Multiple-family dwellings;
2. Recreation facilities ancillary to multiple-family residential development projects, for use by project residents and their guests, including, but not limited to, pools, spas, tennis courts, clubhouse or recreation building, playgrounds or tot lots, and other similar amenities appropriate to serve a multiple-family residential development project;
3. Temporary uses (as permitted within the residential districts of the City of Ontario Development Code subject to an Administrative Permit);
4. Leasing/Administrative Office (for on-site property and facilities management only);
5. Motor vehicle parking ancillary to a multiple-family residential development project;
and
6. Other land use compatible with multiple-family residential development projects, as determined appropriate by the Zoning Administrator.

Table 3-1: Permitted Use Table	
Land Use	MU-1 Commercial
EDUCATIONAL	
Vocational/Trade Schools	C
Private Schools	
MEDICAL	P
Animal Hospital/Veterinarian	P
Ambulance Service (Office Only - No Storage of Vehicles)	P
Medical Office	P
MEDICAL OFFICE/FAMILY CLINIC	
Pharmacies and Drug Stores w/Drive-thru Facilities	P
Industrial Clinic	P
PUBLIC FACILITIES	
Police/Fire Station	P
Police Storefront/Substation	P
RETAIL/COMMERCIAL	
Discount Variety Store	P
Hobby, Toy and Game Store	P
Home Appliance Store	P
Jewelry Store	P

Table 3-1: Permitted Use Table

Land Use	MU-1 Commercial
Luggage and Leather Goods	P
Music and Video Stores	P
Office Supply, Stationery and Gift Stores	P
Pet and Pet Supply Store	P
FOOD AND BEVERAGE STORES	
Bakery	P
Delicatessen	P
Convenience Market	P
Grocery Store	P
Specialty Food Stores	P
SERVICES	
Advertising Agency	P
Data Processing Services	P
Data Processing Services	P
Photography Studio	P
Photocopying and Duplicating Services	P

Table 3-1: Permitted Use Table	
Land Use	MU-1 Commercial
FINANCIAL SERVICES	
Banks, Credit Unions and other Despository Institutions	P
Banks, Credit Unions and other Despository Institutions with Drive-thru (See Development Ordinance Section 9-101305 D)	C
PERSONAL SERVICES	
Barber Shop and Beauty/Nail Salon	P
Dry Cleaners	P
Laundry - Commercial	C
Pet Grooming	P
Tailor	P
Travel Agency	P
REPAIR SERVICES	
Computer, Home Electronics, and Small Home Appliances (when ancillary to another use)	P
Electrical Equipment	P
Jewelry and Watches/Clocks	P
Locksmith/Key Shop	P

“P” (permitted) shall mean the land use, activity, or facility within the specified zoning district is permitted by right of being in the proper zoning district.

“C” (conditionally permitted) shall mean the land use, activity, or facility within the specified zoning district is subject to the granting of a Conditional Use Permit pursuant to Section 4.02.025 (Conditional Use Permits) of this Development Code.

“NP” (Not Permitted) shall mean the land use, activity, or facility within the specified zoning district is not permitted by right of being in the proper zoning district.

All other uses not listed as Permitted or Conditional are prohibited unless a finding can be made by the Planning Director that the use is similar to, and no more objectionable than, a permitted or conditional use.

4 – DEVELOPMENT REGULATIONS

4.1 – Residential Density

Residential density shall range from a minimum 25.0 to a maximum 75.0 dwelling units per acre.

As part of the mixed-use character of the project, a 2,740 SF retail component will be incorporated on the first level of the podium of the building. This total area is well within the 2.0 FAR allowed by city and is in line with the city's vision for Holt Avenue to be an intensive vertical and horizontal mixture of retail, office, and residential uses in a pedestrian friendly atmosphere.

The small retail component of the project has also been incorporated to add convenience to the residents of the project and adjacent properties, help foster neighborhood cohesion, and reduce the need for vehicular traffic.

Exhibit 4-1: Front Elevation Retail Podium



4.2 – Building Height

No structure shall exceed 67' 6" in height, except that the maximum height may be exceeded by roof mounted equipment, architectural projections, chimneys, elevator towers, parapet walls and any other roof top structures, by up to 10% of the allowed building height or as restricted by the Airport Height Constraints. No rooftop equipment shall be visible from anywhere on the project site, public streets, or adjacent properties, and shall be fully screened with appropriate architectural parapet walls or appropriate roof treatments. Roof-mounted equipment shall not exceed the height of the structures and appurtenances used to screen the equipment.

Exhibit 4-2: Height Diagram



4.3 – Setbacks

Building setbacks shall be as set forth in Table 4-1 (Minimum Building Setbacks), and shall comply with Exhibit 4-3 (Setback Diagram).

Table 4-1: Minimum Building Setbacks

Setbacks	Distance
Minimum Street Setbacks	
Holt Boulevard	2 Feet
Brooks Street	2 Feet
Side Setback (East) Side Setback (West)	1 foot provided 1 foot provided

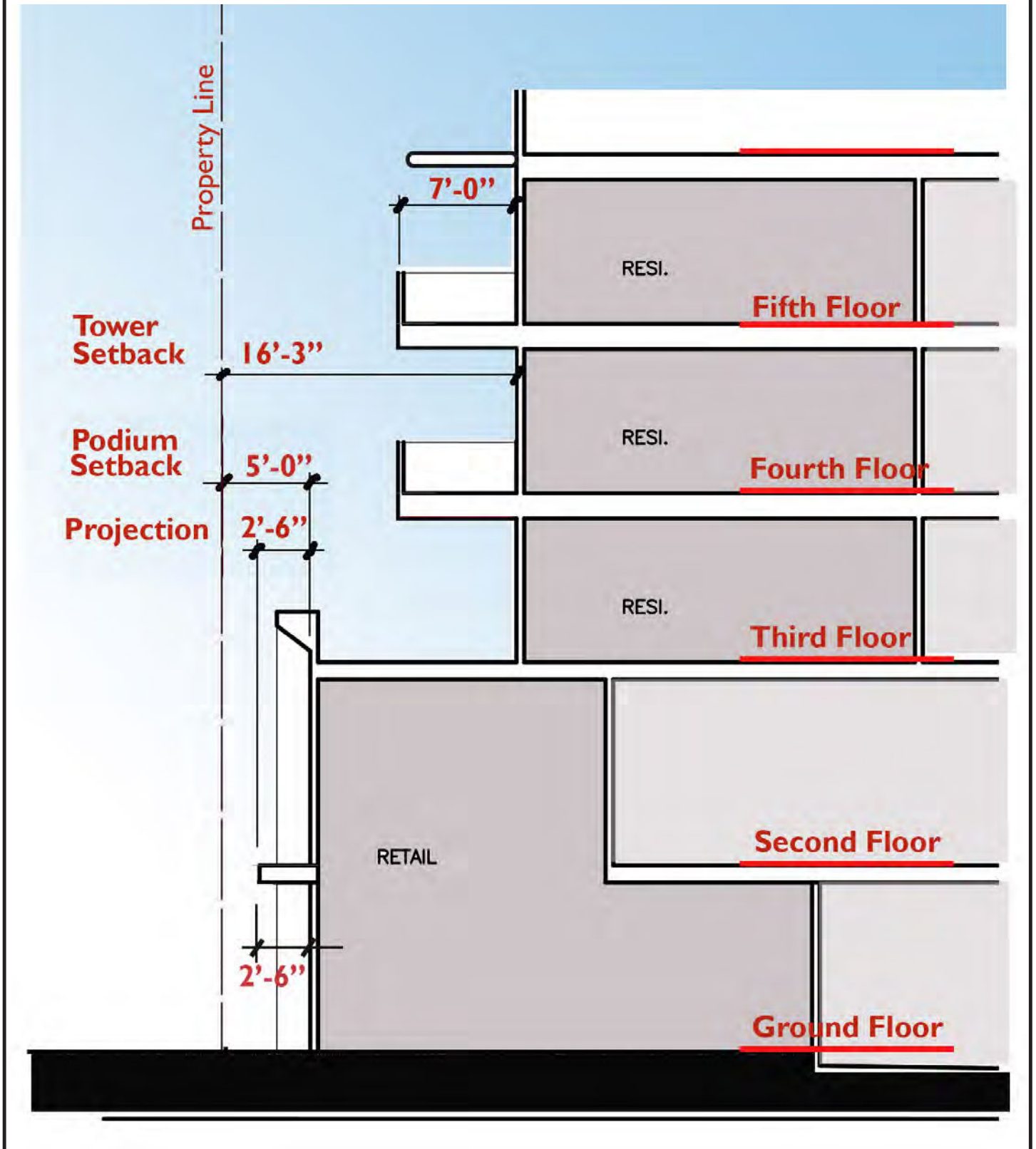
Certain types of features and equipment are allowed to encroach into the required setback areas in accordance with Table 4-2 (Encroachments into Setbacks) including but not limited to architectural projections, balconies, and underground utility structures such as electrical transformer vaults. Utility structures may encroach into the public right-of-way and/or public utility easement, subject to the approval of the City.

Table 4-2: Maximum Encroachments Into Building Setbacks

Projection	Encroachment
Architectural Feature	1/2 of Required Setback
Patios/Porches	1/2 of Required Setback
Balconies (Occurs on 3rd and 4th Floors)	1/2 of Required Setback at 1st Floor; 3 Feet or More Allowed at 2nd or Higher Floors

Note: No projections may encroach into the storm drain easement

Exhibit 4-4: Projection Diagram



4.4 – Open Space Requirements

Open space shall be provided for passive and active recreation opportunities within the project site. Two types of open space are required, private and common open space. These open space areas are for the use of the on-site residents and their guests. Required street set-back areas are not counted towards common open space requirements.

4.4.1 – Private Open Space

Private open space shall be provided for each residential unit in order to provide private outdoor areas which can be enjoyed for the exclusive use by the occupant of the residential unit and their guests. Types of areas considered private open space include balconies, decks, and enclosed patios and yards.

Private open space shall be provided for all residential units, and shall have direct access from each residential unit. Residential units shall have a minimum size of private open space as defined in Table 4-3 (Private Open Space). The space may be provided in multiple areas (e.g. two balconies, a yard and a balcony, etc.), as long as the total area of the spaces meets the minimum private open space requirement for the unit. The minimum dimension for private open space shall be no less than 6 feet in any direction.

Table 4-3: Private Open Space

Number Of Bedrooms	Open Space
1 Bedroom	60 SF
2 Bedrooms	60 SF
3 Bedrooms	60 SF

For multiple-family dwellings, private open space located at the dwelling's main living level shall have a minimum contiguous clear area of 60 SF, with a minimum clear horizontal dimension of 7 FT and a minimum clear vertical dimension of 8 FT. Other open space areas located on balconies or roof decks shall have a minimum contiguous clear area of 50 SF, with a minimum clear horizontal dimension of 6 FT and a minimum clear vertical dimension of 8 FT.

Exhibit 4-5: Private Open Space Diagram



STUDIO



2 BEDROOM (TYPE III)



2 BEDROOM (TYPE II)



1 BEDROOM (TYPE I)



1 BEDROOM (TYPE II)



2 BEDROOM (TYPE I)

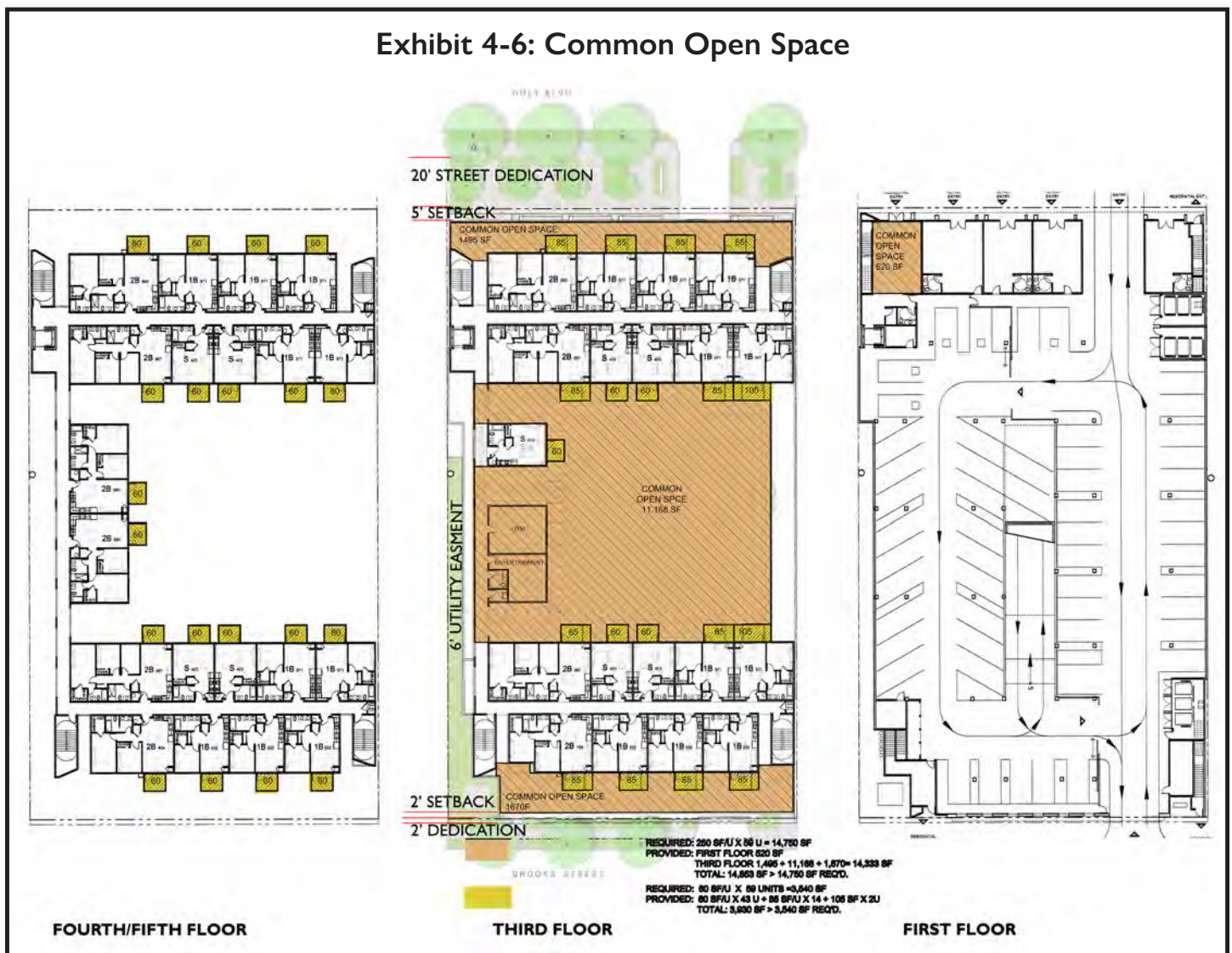
UNIT PLANS

4.4.2 – Common Open Space

Common open space of 150 square feet min, per unit will be required and shall be provided to allow for both passive and active types of recreation, along with site landscape amenities. These areas are for use by project residents and their guests, and may include indoor recreational facilities. Recreational facilities provided pursuant to Section 4.4.3 shall be provided in common open space areas. The amount of required common open space is based on the number of dwellings developed, and shall be provided at the rate of 235 square feet of common open space for each dwelling unit.

Areas not considered common open space include: [1] parking lots; [2] walkways along or between buildings; [3] parking lot landscaping; [4] street setback areas; and [5] other areas not intended for active or passive recreation.

Exhibit 4-6: Common Open Space



4.4.3 – Recreational Facilities

Access to recreational facilities is important in multiple-family residential projects, as they provide needed facilities for the residents of the community. These facilities shall be centrally located on the site, making the facilities accessible to all project residents. A total of at least three recreational facilities (indoor or outdoor) shall be provided on-site. The facilities can be comprised of a combination of both similar and different recreational facilities to meet the recreational facilities requirement (e.g. two pools and one playground or one clubhouse, one pool, and one tot lot, etc.). Recreational facilities may be indoor or outdoor, and may include: [1] clubhouses; [2] pools; [3] community buildings; [4] playgrounds or tot lots; and [5] other indoor or outdoor recreational facilities deemed appropriate by the City. All recreational facilities shall be for the exclusive use of project residents and their guests.

4.5 – Parking Requirements

4.5.1 – On-Site Resident Parking Requirements

The number of parking spaces shall be provided based on the number of bedrooms contained within each dwelling unit, and is subject to the following requirements:

- All required resident parking spaces shall be provided on-site;
- Each dwelling shall have at least one reserved parking space that is clearly marked;
- Reserved parking spaces shall be located within 150 feet of the dwelling it serves;
- At least one covered parking space shall be provided for each dwelling. This can be achieved through use of garages, carports, or tuck-under parking;
- Resident's assigned parking space(s) shall be used for the parking of occupant's operable automobile(s), only;
- Tandem parking is allowed but subject to review;
- Required guest parking spaces shall be provided on-site.
- Parking for dwellings shall be provided on-site, pursuant to the requirements of Table 4-4 (Minimum Parking Requirements), this can be included on the leasing contract.

Table 4-4: Minimum Parking Requirements

Use	Parking Spaces
Multi-Family Dwellings: Studio One-Bedroom Unit Two Bedroom Unit *No Three Bedrooms for this project	1.50 spaces per dwelling (at least one space shall be in a garage, carport, or parking structure) 1.75 spaces per dwelling (at least one space shall be in a garage, carport, or parking structure) 2.0 spaces per dwelling (at least one space shall be in a garage, carport, or parking structure)
Guest Parking	1 space per every 5 units
Commercial/Retail	1.0 space per 250 square feet

4.5.2 – On-Street Guest Parking Requirements

Guest-parking must be provided on-site.

- On-street parking may be allowed to satisfy guest parking requirements only but shall not be counted towards the number of guests parking the code requires.
- On-street parking shall only be designated along Brooks Street, the public street along the south side of the development site. No on-street parking will be allowed along Holt Boulevard and on any other areas deemed by the City Engineer, specially on street corners where sight distance are required.

4.5.3 – Parking Standards

All parking space size and location requirements shall conform to the standards set forth in Section 6.03.045 (Off-Street Parking Standards) of the Ontario Development Code.

Exhibit 4-7: On-Street Parking



4.6 – Vehicular Access

Vehicular access to the site is permitted from Holt Boulevard and Brooks Street. After improvements are made from the West Valley Connector (BRT) Project, vehicular access from Holt Boulevard will be limited to right-in/right-out.

4.6.1 – Vehicle Circulation

The city of Ontario, as part of its intersection widening plan, and the developers of the PUD site shall be responsible to improve Holt Boulevard and Brooks Street to their ultimate widths. These streets will provide vehicular access to the site.

4.6.2 – Pedestrian Connections

Pedestrian connections shall be provided to each building from the public street within the PUD site. The Mixed-Use Commercial land use district shall also provide pedestrian pathways to connect all of the commercial developments.

4.6.3 – Street Dedications and Easements

The developers shall dedicate right-of-way along all street frontages as necessary to increase the right-of-way to ultimate width, including corner cutbacks per City Standard Drawing No. 1301. Additionally, developers shall adhere to the Proposed Holt Blvd and Brooks Street Widening Improvements R/W Requirements. The ultimate widths surrounding the PUD are as follows:

Holt Boulevard - up to 60' half width
Brooks Street - up to 30' half width

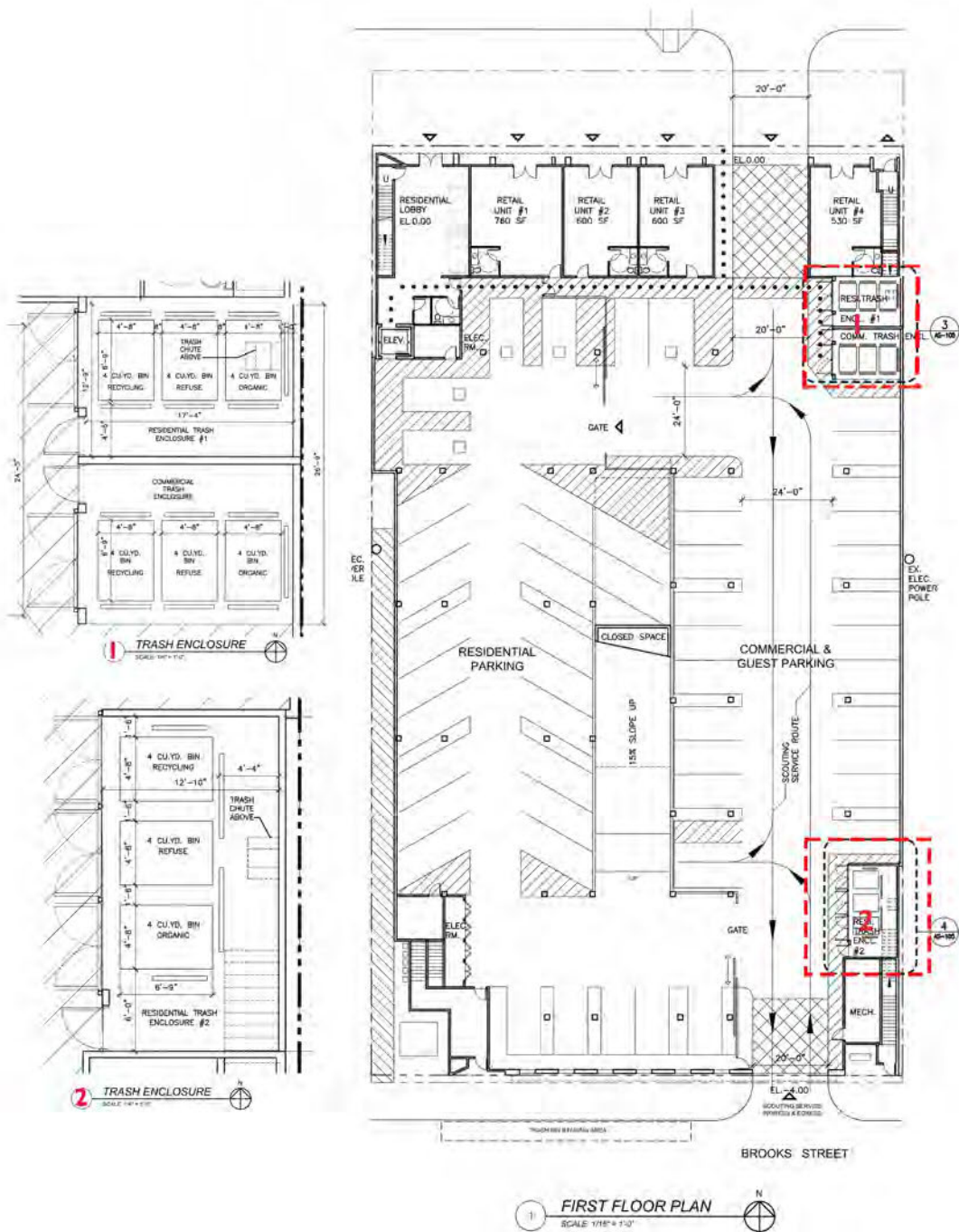
4.6.4 – Public Right-Of-Way Improvements

The public right-of-way shall be improved with each proposed development. The public right-of-way improvements required are to include, but are not limited to the following: street pavement, curb & gutter, parkway landscaping, parkway irrigation, public sidewalk, traffic signing and striping, street lights, bus stop, bus shelter and amenities. The extent of the required improvements shall be determined for each development plan. At a minimum, the improvements shall incorporate all items along the street frontages of the properties to be developed with proper transitioning if the entire block frontage is not being installed with a particular development. The city may do some or all of the improvements as part of its Proposed Holt Blvd and Brooks Street Widening Improvements.

4.7 – Service Facilities

- Loading/unloading for residential and commercial uses and trash pickup service shall be limited to on-site private drive aisles. Location, signage, and any hourly restrictions to be imposed on such areas, shall be subject to approval by the City.
- On-street loading shall be prohibited.
- The number of enclosures, and their precise locations, dimensions, and design shall be provided consistent with City standards.
- Trash enclosures shall be designed to contain separate containers for the collection of refuse and recyclable materials, with an adequate number of containers provided to allow for the collection of both refuse and recyclable materials generated by the development, pursuant to standards established by the Ontario Municipal Utilities Company. (See Exhibit 4.7: Trash Enclosures Site Plan)
- Trash enclosures shall meet the minimum design standards depicted in the standard drawings adopted by the City, which shall include: [1] a minimum 6-FT high decorative masonry wall, with appropriate view-obstructing gates for container access, [2] separate pedestrian access that is designed to screen the interior of the enclosure from view from the exterior and prevent refuse dispersion, and [3] a decorative overhead roof structure to protect bins containing recyclable materials from adverse environmental conditions, which might render the collected materials unusable, and screen trash bins from view of the upper floors of adjacent dwellings. Furthermore, trash enclosures shall be architecturally enhanced, and shall be consistent with the architectural design of adjacent buildings.
- Trash enclosure dimensions shall be of adequate size to accommodate containers consistent with the City's current methods of collection within the area in which the project is located.
- Signs clearly identifying all recycling and refuse collection areas, and the materials accepted for recycling shall be posted adjacent to all points of access to each trash enclosure.
- Trash enclosures shall be located on site within the boundary of the project site.
- Particular care shall be given when placing trash enclosures immediately adjacent to dwelling units; however, no trash enclosure shall be located within 10 feet of the livable portion of a structure.
- Trash enclosures shall be bordered by a minimum 5-foot high concrete wall on all exposed sides, excluding the side with bin access gates.
- All service facilities shall be screened in accordance with Section 4.9 (Screening) of this Planned Unit Development.

Exhibit 4-8: Trash Enclosures Site Plan



Note: "All Priority Land Use(PLU): Land use consisting of high-density residential, defined as a land use with at least ten (10) dwelling units per acre, industrial, commercial, mixed urban, and public transportation station land uses within the project area shall comply with the statewide Trash Provisions adopted by the State Water resources Control Board (SWRCB) and trash requirements in the most current San Bernardino"

4.8 – Landscaping

The site design at Roosevelt Towers includes a diverse palette of drought tolerant native and adapted plants selected to provide a low maintenance and colorful landscape while also meeting the challenging requirements of planting on structure.

The building frontage along Holt Avenue consists of a 4' wide parkway planted with Evergreen Ash trees and a low groundcover of Dwarf Coyote Brush. Setback from the sidewalk are a series of large rectangular planting areas, approximately 150sf each, configured within the building module, allowing views and access to the future retail facades. These large planter areas contain a variety of low water groundcovers with native deciduous Western Redbud trees (triangulated with the Evergreen Ash). The hardscape in this area consists of standard grey concrete along the public sidewalk with integral colored sand finish concrete along the building façade and between planters. **Decorative paving and hardscape within the 20' dedication shall be maintained by the property owner.**

Along Brooks Street the same Baccharis groundcover runs along the parkway with Podocarpus henkelii as the street tree. Larger foundation shrubs such as Sweet Bay and New Zealand Flax soften the façade and provide screening to utilities in this area.

4.8.1 – Site Landscaping

A conceptual landscape and irrigation plan shall be submitted along with the Development Plan for this Planned Unit Development. The plan shall specify all landscaping and hardscape for the entire project site. Detailed Landscape and Irrigation Plans shall be required prior to the issuance of building permits. The detailed plans shall show location of ground mounted utility boxes and equipment, along with the methods of screening for these items from the public right-of-way and adjacent residences where possible. The irrigation system shall be designed to utilize recycled water pursuant to State and local codes, ordinances, and laws, and applicable building and plumbing codes.

The landscape and irrigation plan shall be designed with water conservation in mind, utilizing “California friendly” species and drought tolerant planting materials. The landscaping and irrigation shall comply with AB 1881, and all other laws and regulations related to planting materials. All trees that are to remain on-site shall be properly protected in place during construction to ensure tree health is preserved.

Tree plantings along the Brooks Street storm drain easement shall be restricted to those species allowed by the San Bernardino Flood Control District.

4.8.2 – Planting Palette

The planting palette shall be comprised of “California friendly” drought tolerant planting materials compatible with the overall architectural style of the Planned Unit Development. The following palette has been established to meet the needs of this Planned Unit Development:

4.8.3 – Common and Open Space Landscaping

The 3rd floor contains a variety of spaces, with landscaping acting as an organizing feature while still providing screening, softening, color, beauty, and shade.

As one approaches the main public amenity space through the central corridor a row of potted Dwarf Olives sits in a bed of low green planters which carries the eye through the space and also bisects the space into two smaller zones. At the terminus of this view is one of 3 specimen Ficus trees sitting in an 8’ x 12’ x 42” high raised planter, providing ample space for root structure. To the north of the olive row is the Children’s Play Area, a 26’ x 52’ semi contained area with two play structures, wood bench seating, umbrellas for shade, and a small turf area for flexible play.

To the south, adjacent to the gymnasium, is a flexible open space for exercise, yoga, or small recreational games such as table tennis. Heading east is a large 26’ x 40’ shade structure above a flexible dining and lounge space including two BBQ cooking stations. This shade structure partially expands to the east to cover a long wood (or composite) deck seating area. The planting areas in and around the 3rd floor amenity space can be divided into 3 groups. The first group are the series of “built in” raised planters. These contain the 3 Ficus trees, and also run along the north and south edge of the public amenity space. The longer planters would sit at 42” height, and contain a mix plants including Dwarf Bottlebrush, Sage, and Coast Rosemary. The second category is the series of narrow linear planters that are used for screening the residences while softening the building façade. These would be “off the shelf” rectangular planters, potentially lightweight precast concrete or fiberglass, containing upright plants for screening such as Sweet Bay or Bamboo. Finally, the last group includes low profile green tray plantings. These would be of medium depth, approximately 8”, and may be contract grown ahead of time to provide an immediate impact on day one. Species may include sedums, low ornamental grasses, and other small succulents.

All planting areas on the 3rd floor will be connected to the buildings irrigation system, and appropriately plumbed for drainage.

Exhibit 4-9: Colored Landscape Plan



Exhibit 4-10: Holt Boulevard Planting Palette



HARDSCAPE LEGEND		
ITEM	DESCRIPTION	REFERENCE IMAGE
1	STANDARD GREY CONCRETE PAVING AT CITY SIDEWALK	
2	DECORATIVE CONCRETE PAVING: MEDIUM SANDBLAST FINISH WITH DAVIS INTERNAL COLOR #44 PESSLE AND SMOOTH JOINTS	01/L-105
3	OUTDOOR PORCELAIN PEDESTAL PAVERS: 24" SQ AND 24" X 48" MIX OF FLAT ASH AND FLAT SMOLE COLOR BY TLETECH OR APPROVED EQUAL	02/L-105
4	WOOD DECKING: 2 X 4 REDWOOD OR COMPOSITE DECKING (TO BE DETERMINED). INSTALLED 4" MINIMUM OVER SLAB WITH COMPOSITE WOOD SLEEPER SYSTEM	03/L-105
5	DECORATIVE WATERPROOFING COATING: MAOAT BY WESTCOAT OR EQUAL WITH CUSTOM COLOR AND SCORING	04/L-105
6	42" TALL CONCRETE BLOCK PLANTER WALL: 8 X 12 CMU IN STACKED PATTERN, BURNISHED DARK GREY 250 BY ORCO OR APPROVED EQUAL	05/L-105
7	5' TALL HORIZONTAL PRIVACY FENCING TO MATCH WOOD DECKING	06/L-105
8	42" HIGH STEEL GUARDRAIL	07/L-105
9	42" HIGH SELF CLOSING GATE WITH LATCH	
10	CUSTOM BENCH SEATING TO MATCH WOOD DECKING. 18" HIGH X 24" DEEP	
11	BUILT IN BARBECUES AND SERVING COUNTER	
12	RUBBERIZED PLAY SURFACING	08/L-105
13	OVERHEAD SHADE STRUCTURE WITH CORRUGATED STEEL ROOF	09/L-105
14	11'-6" WIDE X 4' TALL MONUMENT SIGN AT ENTRY DRIVE	

SITE AMENITY LEGEND (SEE CUTSHEETS FOR ADDITIONAL INFORMATION)			
ITEM	DESCRIPTION	ITEM	DESCRIPTION
SA-1	LOUNGE SEATING TYPE A	SA-4	UMBRELLA TYPE B
SA-2	LOUNGE SEATING TYPE B	SA-7	6-23 MONTH OLD PLAY STRUCTURE
SA-3	LOUNGE SEATING TYPE C	SA-8	2-5 YEAR OLD PLAY STRUCTURE
SA-4	DINING SEATING	SA-9	PING PONG TABLES
SA-5	UMBRELLA TYPE A		



KEY PLAN

LANDSCAPE ALONG HOLT BOULEVARD

TREE LEGEND					
SYMBOL	BOTANIC NAME COMMON NAME	SIZE	QTY.	WATER USE	CALIFORNIA NATIVE
STREET TREES					
[Symbol]	FRAXINUS LINDEI EVERGREEN ASH (HOLT BLVD.)	24" BOX STD.	4	MED	NO
	PODOCARPUS HENKELII LONG LEAVED YELLOWWOOD (BROOKS STREET)	24" BOX STD.	4	MED	NO
ON SITE TREES					
[Symbol]	CERCIS OCCIDENTALIS WESTERN REDBUD	36" BOX MULTI	5	LOW	YES
[Symbol]	CHILOPSIS LINEARIS DESERT WILLOW	15 GAL MULTI	3	LOW	YES
[Symbol]	FIGUS MICROCARPA 'GREEN GEM' GREEN GEM INDICA FIG LAUREL	48" BOX STD	3	MED	NO
[Symbol]	OLEA EUROPEA 'MONTEA' LITTLE OLIVE DWARF OLIVE	24" BOX MULTI	5	LOW	NO

SHRUB AND GROUNDCOVER LEGEND					
ITEM	DESCRIPTION	REPRESENTATIVE SPECIES	WATER USE	CA NATIVE	
1	PARKWAY GROUND COVER	BACCHARIS PILLULARIS / DWARF COYOTE BRUSH	LOW	YES	
2	LOW SHRUB AND GROUND COVER	ACHILLEA MILLEFOLIUM / COMMON YARROW BACCHARIS PILLULARIS / DWARF COYOTE BRUSH CAREX OVIULSA / BERKLEY REDGE DANIELA REV. LITTLE REV. / LITTLE REV. FLAX LILLY LANTANA SELLOVIANA / TRAILING LANTANA	LOW LOW LOW LOW	YES YES YES NO	
3	MEDIUM HEIGHT SHRUBS AND PERENNIALS	ARGEMONE SPP. / KANGAROO PAW CALL LITTLE JOHN / DWARF BOTTLEBRUSH CISTUS SPP. / ROCKROSE FURCHAEA SPP. / FURCHAEA JUNCUS PATENS / CALIFORNIA GREY WISN LEUCOPHYLLUM SPP. / TEXAS RANGER SALVA ALLEN CHOISERING / ALLEN CHOIC SAGE WESTRINGIA FRUTICOSA / COAST ROSEMARY	LOW LOW LOW LOW LOW LOW	NO NO NO YES NO YES	
4	LOW GROUND COVER IN 6" X 6" DEEP PREPARED TRAYS, LIVEROOF OR EQUAL	CAREX OVIULSA / BERKLEY REDGE LAMPBRANTHUS SPP. / ICE PLANT SEDUM SPP. / STONE CRUP SIBBICOD SPP.	LOW LOW LOW LOW	YES NO YES NO	
5	PLANTERS IN SHADE	ALCUBIA JAP. / MR. GOLDSTRICK MR. GOLDSTRICK ALCUBIA FATSIA JAPONICA / JAPANESE ARALIA	MED MED	NO NO	
6	SCREENING AT PATIO AND GROUND FLOOR UTILITIES	BAMBUSA MULT. / ALPHONSE KARR ALPHONSE KARR BAMBOO BAMBUSA MULT. / GOLDEN QOODDESS GOLDEN QOODDESS BAMBOO LAURUS NOBILIS / SWEET BAY PHORALUM TENAX / NEW ZEALAND FLAX	MED MED MED LOW MED	NO NO NO NO NO	
7	TURF SEATING	MARATHON II SOD WITH SUBSURFACE DRP IRRIGATION	HIGH	NO	

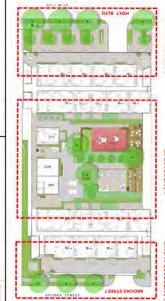
Exhibit 4-11: Common Space 3rd Floor Planting Palette

TREE LEGEND		SIZE	QTY.	WATER USE	CALIFORNIA NATIVE
SYMBOL	SCIENTIFIC NAME COMMON NAME				
STREET TREES					
○	FRAXINUS (LIRIOD.) FRAXINUS (LIRIOD.) FRAXINUS (LIRIOD.)	24" BOX STD.	4	MED	NO
○	PODOCARPUS HENKELI PODOCARPUS HENKELI PODOCARPUS HENKELI	24" BOX STD.	4	MED	NO
○	ON SITE TREES				
○	CECAIS COULE (LIRIOD.) WESTERN REDGIRD	24" BOX MULTI	5	LOW	YES
○	CHILOPSIS LINEARIS DESERT WILLOW	15 GAL MULTI	3	LOW	YES
○	FAUS NICKERSONIA ORENIGUNDA (LIRIOD.)	10" BOX STD.	3	MED	NO
○	OLEA EUROPEA (MONTANA) LITTLE OLIVE (DWARF CULTIVAR)	24" BOX MULTI	5	LOW	NO

SHRUB AND GROUND COVER LEGEND		WATER USE	NATIVE
ITEM	DESCRIPTION		
1	PROSTRATE GROUND COVER	LOW	YES
2	LOW SHRUB AND GROUND COVER	LOW	YES
3	MEDIUM SHRUB PERENNIALS	LOW	NO
4	LOW GROUND COVER IN 8" POTS	LOW	NO
5	PLANTERS IN SHADES	LOW	NO
6	SHRUBS IN PATIOS AND UTILITY	LOW	NO
7	TURF SEATING	HIGH	NO

HARDSCAPE LEGEND		REFERENCE IMAGE
ITEM	DESCRIPTION	
1	STANDARD GREY CONCRETE PAVING AT CITY SIDEWALK	01 / L-105
2	DECORATIVE CONCRETE PAVING, MEDIUM SANDBLAST FINISH WITH DIVERSE INTERIOR COLOR PALETTE AND SAWCUT JOINTS	02 / L-105
3	PLANTER BOXES WITH 2" TYPICAL SLOPE TO DRAIN TO EXISTING EDGE OF PLANTER BOXES	03 / L-105
4	WOOD DECKING, 2x4 REDWOOD OR COMPOSITE DECKING TO BE USED IN ALL AREAS. DECKING TO BE INSTALLED AT MINIMUM OVER SLAB WITH COMPOSITE TRIM	04 / L-105
5	DECORATIVE, METAL-LOOKING COATING, MATCHING BY HESTON OR EQUAL, WITH CUSTOM COLOR AND FINISHES	05 / L-105
6	4" TALL CONCRETE BLOCK PARTIAL WALLS, 8x12 CM IN SET BACK PATTERNS, BARRIERS DARK GREY 2x8 FT ON APPROX. EQUAL	06 / L-105
7	5" TALL HORIZONTAL PRIVACY FENCING TO MATCH WOOD DECKING	07 / L-105
8	4" HIGH STEEL GUARDRAIL	08 / L-105
9	4" HIGH SELF-CLOSING GATE WITH LATCH	09 / L-105
10	CUSTOM SEATING (SEE CUTSHEETS FOR ADDITIONAL INFORMATION)	
11	BUILT IN BARSTOOLS AND SERVING COUNTER	
12	RUBBERIZED PLAY SURFACING	
13	OVERHEAD SHADE STRUCTURE WITH CORRUGATED STEEL ROOF	
14	11'-8" WIDE X 4' TALL MONUMENT SIGN AT ENTRY DRIVE	

SITE AMENITY LEGEND (SEE CUTSHEETS FOR ADDITIONAL INFORMATION)	
ITEM	DESCRIPTION
SA.1	LOUNGE SEATING TYPE A
SA.2	LOUNGE SEATING TYPE B
SA.3	LOUNGE SEATING TYPE C
SA.4	DINING SEATING
SA.5	UMBRELLA TYPE A



KEY PLAN

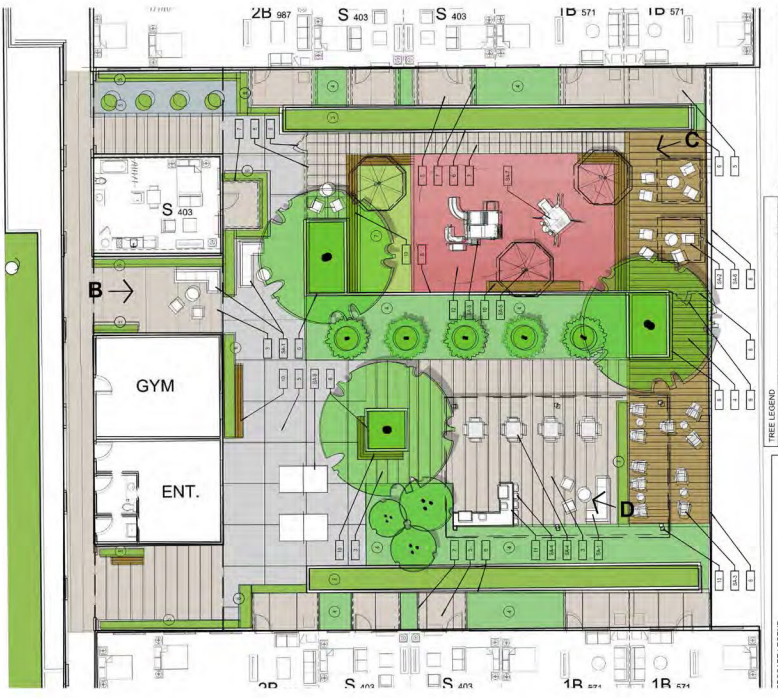


Exhibit 4-12: Brooks Street Planting Palette



HARDSCAPE LEGEND		
ITEM	DESCRIPTION	REFERENCE IMAGE
1	STANDARD GREY CONCRETE PAVING AT CITY SIDEWALK	
2	DECORATIVE CONCRETE PAVING, MEDIUM SANDBLAST FINISH WITH DAVIS INTERIOR, COLOR #641 PEBBLE AND SERRAJO JOINTS	01/L-105
3	OUTDOOR PORCELAIN PEDESTAL PAVERS, 24" SO AND 24" X 48" MIX OF FLAT ASH AND FLAT SMOLE COLOR BY TILETECH OR APPROVED EQUAL	02/L-105
4	WOOD DECKING- 2 x 4 REDWOOD OR COMPOSITE DECKING (TO BE DETERMINED), INSTALLED 4" MINIMUM OVER SLAB WITH COMPOSITE WOOD SLEEPER SYSTEM	03/L-105
5	DECORATIVE WATERPROOFING COATING- MACOAT BY WESTCOAT OR EQUAL WITH CUSTOM COLOR AND SCORING	04/L-105
6	42" TALL CONCRETE BLOCK PLANTER WALL, 8" X 12 CMU IN STACKED PATTERN, BURNISHED DARK GREY 250 BY ORCO OR APPROVED EQUAL	05/L-105
7	8' TALL HORIZONTAL PRIVACY FENCING TO MATCH WOOD DECKING	06/L-105
8	42" HIGH STEEL GUARDRAIL	07/L-105
9	42" HIGH SELF CLOSING GATE WITH LATCH	
10	CUSTOM BENCH SEATING TO MATCH WOOD DECKING, 18" HIGH X 24" DEEP	
11	BUILT IN BARBECUES AND SERVING COUNTER	
12	RUBBERIZED PLAY SURFACING	08/L-105
13	OVERHEAD SHADE STRUCTURE WITH CORRUGATED STEEL ROOF	09/L-105
14	11'6" WIDE X 4' TALL MONUMENT SIGN AT ENTRY DRIVE	

SITE AMENITY LEGEND (SEE CUTSHEETS FOR ADDITIONAL INFORMATION)			
ITEM	DESCRIPTION	ITEM	DESCRIPTION
SA.1	LOUNGE SEATING TYPE A	SA.6	UMBRELLA TYPE B
SA.2	LOUNGE SEATING TYPE B	SA.7	6-23 MONTH OLD PLAY STRUCTURE
SA.3	LOUNGE SEATING TYPE C	SA.8	2-5 YEAR OLD PLAY STRUCTURE
SA.4	DINING SEATING	SA.9	PING PONG TABLES
SA.5	UMBRELLA TYPE A		



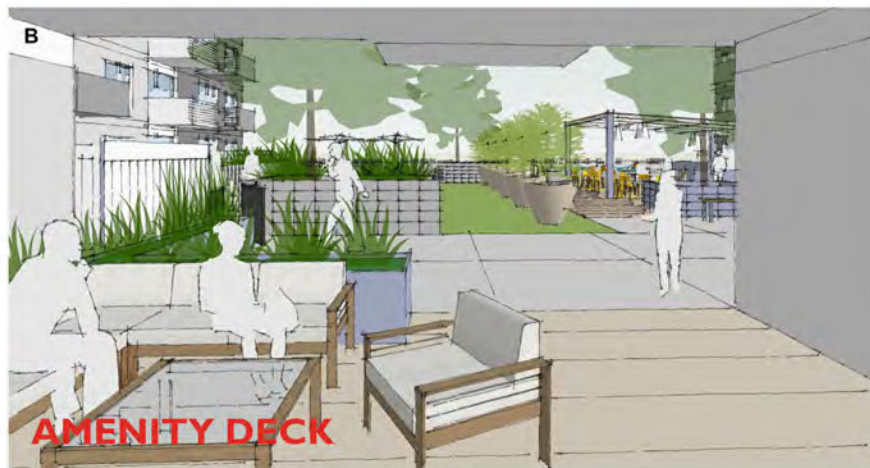
KEY PLAN

LANDSCAPING ALONG BROOKS STREET

TREE LEGEND						
SYMBOL	BOTANICAL NAME COMMON NAME	SIZE	QTY.	WATER USE	CALIFORNIA NATIVE	
STREET TREES						
●	FRAXINUS ULMET EVERGREEN ASH (HOLT FB.VD.)	24" BOX STD.	4	MED	NO	
●	PODOCARPUS HENKELII (LONG LEAFED YELLOWWOOD (BROOKS STREET))	24" BOX STD.	4	MED	NO	
ON SITE TREES						
○	CERCIS OCCIDENTALIS WESTERN REDBUD	8" BOX MULTI	5	LOW	YES	
○	CHLOPSIS LINEARIS DESERT WILLOW	15 GAL MULTI	3	LOW	YES	
○	FICUS MICROCARPA 'GREEN GEM' GREEN GEM MONICA FIG LAUREL	48" BOX STD.	3	MED	NO	
○	Olea EUROPEA 'MONTRA' LITTLE OLLIE DWARF OLIVE	24" BOX MULTI	5	LOW	NO	

SHRUB AND GROUND COVER LEGEND					
ITEM	DESCRIPTION	REPRESENTATIVE SPECIES	WATER USE	CA NATIVE	
1	PARKWAY GROUND COVER	BACCHARIS PILLULARIS / DWARF COYOTE BRUSH	LOW	YES	
2	LOW SHRUB AND GROUND COVER	ACHILLEA MELLEOLINI / COMMON YARROW BACCHARIS PILLULARIS / DWARF COYOTE BRUSH CAREX DIVULSA / BERKELEY SEDGE DANIELA REV LITTLE REV / LITTLE REV FLAX LULY LANTANA TELLOWANKI TREKING LANTANA	LOW LOW LOW LOW LOW	YES YES YES NO NO	
3	MEDIUM HEIGHT SHRUB AND PERENNIALS	ANGIOSANTHOS SPP. / KANGAROO PAW CALL LITTLE JOHN / DWARF BOTTLEBRUSH CESTUS SPP. / ROOFRIDGE FURCRAEA SPP. / FURCRAEA JUNCUS PATENS / CALIFORNIA GREY RUSH LEUCOPHYLLUM SPP. / TEXAS PANICLA SALVIA ALLEN CHICKERING / ALLEN CHICK, SAGE WESTRINGIA FRUITCOSEA / COAST ROSEMARY	LOW LOW LOW LOW LOW LOW LOW LOW	NO NO NO YES NO YES YES YES	
4	LOW GROUND COVER IN 8" SP / DEEP PREPLANTED TRAYS, LIVEROOF OR EQUAL	CAREX DIVULSA / BERKELEY SEDGE LAMPBRANTHUS SPP. / CE PLANT SEDUM SPP. / STONE CRISP SENECIO SPP.	LOW LOW LOW LOW	YES NO NO NO	
5	PLANTERS IN SHADE	AUCUBA JAP 'MR GOLDSTRIKE' MR GOLDSTRIKE AUCUBA FATSIA JAPONICA / JAPANESE ARAUKA	MED MED	NO NO	
6	SCREENING AT PATIOS AND GROUND FLOOR UTILITIES	BAMBUSA MULT 'ALPHONSE KARR' ALPHONSE KARR BAMBOO BAMBUSA MULT 'GOLDEN COCOONSS' GOLDEN COCOONSS BAMBOO LAURUS NOBILIS / SWEET BAY PHORSYLIUM TENAX / NEW ZEALAND FLAX	MED MED LOW MED	NO NO NO NO	
7	TURF SEATING	MARATHON 4 800 WITH SUBSURFACE DRIP IRRIGATION	HIGH	NO	

Exhibit 4-13: Landscape Rendering



4.8.4 – Parking Lot Landscaping

Parking lot landscaping shall be provided pursuant to Subsection D (Landscaping of Off-Street Parking Facilities) of Ontario Development Code Section 6.05.030 (Required Landscape Areas).

4.8.5 – Compliance with State and Federal Laws

Provide landscaping and an irrigation system, which promotes the conservation of water as required by the Water Conservation in Landscaping Act of 2006 (AB 1881), commencing with California Government Code Section 65591.

4.9 – Screening

- All roof and ground mounted mechanical equipment shall be screened pursuant to the requirements of the Ontario Development Code.
- Trash enclosures shall be screened with enclosures that are architecturally compatible with the surrounding buildings.
- Screening shall include plant and building materials compatible with the project design so it is well integrated and hidden within the project area.
- Building and plant materials used for screening shall be compatible with the architectural style and planting palette used on the project area.
- All ground level screening shall comply with the requirements of Ontario Development Code Section 6.02.030 (Protection of Intersection Visibility).

4.9.1 – Fences, Walls, and Hedges

Fences, walls, and hedges shall comply with Section 6.02.030 (Protection of Intersection Visibility) of the Ontario Development Code, Engineering Department corner sight distance standards, and all other applicable city standards. Fences and walls shall be made of decorative materials that are compatible with, or enhance, the overall architectural character of the project. All fences, walls, and hedges shall be in scale with the development, and shall be used for screening, site enhancement, and creating a safer living environment for residents and their guests. All decorative walls, monuments, and/or other similar features, shall not encroach in to the public street right-of-way.

5 – CIRCULATION

5.1 – Site Accessibility

The site shall be designed to promote safety for residents by only allowing limited vehicular and pedestrian access into and across the site. This can be done through building orientation and placement, to minimize the use of gates and fencing; however, amenities shall be properly gated, limiting access to residents and their guests.

5.2 – Vehicular Circulation

The project site shall be designed to reduce the number of dead end aisles in the parking lot areas, and provide all guest parking outside of gated areas (on site) to ensure accessibility. Location of drive aisles and entries shall be approved by the City. Vehicular circulation shall be designed in a way that promotes pedestrian safety and proper access to all parking areas.

5.3 – Pedestrian Circulation

Site design shall provide for safe pedestrian circulation across the project site by separating pedestrian areas from areas with vehicular access. This includes, but is not limited to, accessibility from parking lot areas to unit entries, site amenities, and perimeter sidewalk areas. Fencing and gates may be used to limit public access to resident-only areas.

The 20' street-dedication on Holt Boulevard provides a unique opportunity to create a pedestrian promenade that will connect residential foot traffic between residential units, parking, and community spaces throughout the site. The space shall be welcoming and designed with landscape features that help define the walking path across the project area. Since no vehicular traffic will be allowed on this widened sidewalk, it is expected that pedestrians will be able to walk safely along the exterior area of the development where retail amenity area also located.

Lighting and visibility for enhanced security shall be taken into consideration in designing this area, and shall be compatible with the architectural theme. Buildings shall have windows that look on to this open space area to enhance the views of the residences and help create defensible spaces.

5.4 – Integrated Waste Pickup Circulation

The use of scouting trash pickup service since 32 foot trash pickup truck will not be able to access the site and indicate where the staging of the six bins on the adjacent street, Brooks Street for the two trash enclosures will be on trash pickup day. Integrated Waste Management Report (IWMR) requirements:

The IWMR shall meet the following minimum requirements:

The Integrated Waste Management Report shall address the management of all integrated waste (Refuse, Recycling, Organics, etc.) including, but not limited to: types of waste generated, amount of waste expected and the corresponding sizing of receptacles, all waste diversions, all staging and collection operations, any use of private haulers, and the property management regulations and practices for the site.

The IWMR shall demonstrate compliance with the latest version of the City's Integrated Waste Planning Manual (available online at: <http://www.ontarioca.gov/government-departments-municipal-utilities-company/integrated-waste>) as well as the applicable State regulations and shall contain, at a minimum, the following elements:

1. A discussion on the types of waste generated (refuse, recycling, green waste, organics, etc.) by all uses on the site and the generation rates and total waste for each type. If a final use is not known the applicant shall make a reasonable assumption of the land use expected or proceed as directed by the City.
2. A discussion on pre-treatment if the project will be required to use and install pre-treatment devices or services and an exhibit showing and labeling the location of the pre-treatment devices.
3. A discussion on the types of City Services that will be utilized and the sizing and number of receptacles (Bins, Compactors, etc.)
4. A discussion on waste that is diverted and the diversion services that will be used.
5. A table utilizing the metrics on Page 8 of the Planning Manual and calculating the volume (gallons or cubic yards), quantity, and service schedule for each type of can and bin required for each Service Category (refuse, recycled, etc.). The table shall include all waste that is diverted and the diversion service(s).
6. An exhibit(s) showing layout and architectural details for enclosures, compactors, roll-offs and non-standard containers proposed by the project. Include specification "cut" sheets for non-standard containers.

7. A discussion on the staging and collection operation and an exhibit identifying these locations on the site and any other relevant details. The discussion shall address circulation, accessibility, ingress, egress, pavement type, parking, the role of the property management association, and the role of private haulers or scouting services.
8. A discussion on private haulers describing their services and supplying the names, contact information, and City of Ontario Commercial Recycler Collection Permit numbers for each hauler. The discussion shall include an exhibit showing the staging and collection areas for private haulers.
Note: All private third party organics and recycling haulers shall be formally approved and permitted by the City of Ontario and shall meet City Code requirements, otherwise the City shall provide the collection services. If the third party haulers do not have a City of Ontario Commercial Recycler Collection Permit, the discussion shall state whether the third party hauler is charging a fee for service or not.
9. A discussion on the proposed property management association, their role in the integrated waste management plan for the site and the by-laws and regulations that relate to integrated waste (no parking, solid waste collection day coordination, enforcement of the SWHP, etc.).
10. A discussion on any variances that are requested and the need for the variance along with any relevant exhibits.
11. Appendix: A copy of the Final Solid Waste Handling Plan (See City SWHP requirements).
12. Appendix: All exhibits and standard plans proposed for the project.
13. Appendix: A copy of the proposed association by-laws and regulations.
14. Appendix: A copy of the final Integrated Waste Variance Form (if a variance is requested).

Solid Waste Handling Plan (SWHP) requirements:

The SWHP shall meet, at a minimum, the following requirements:

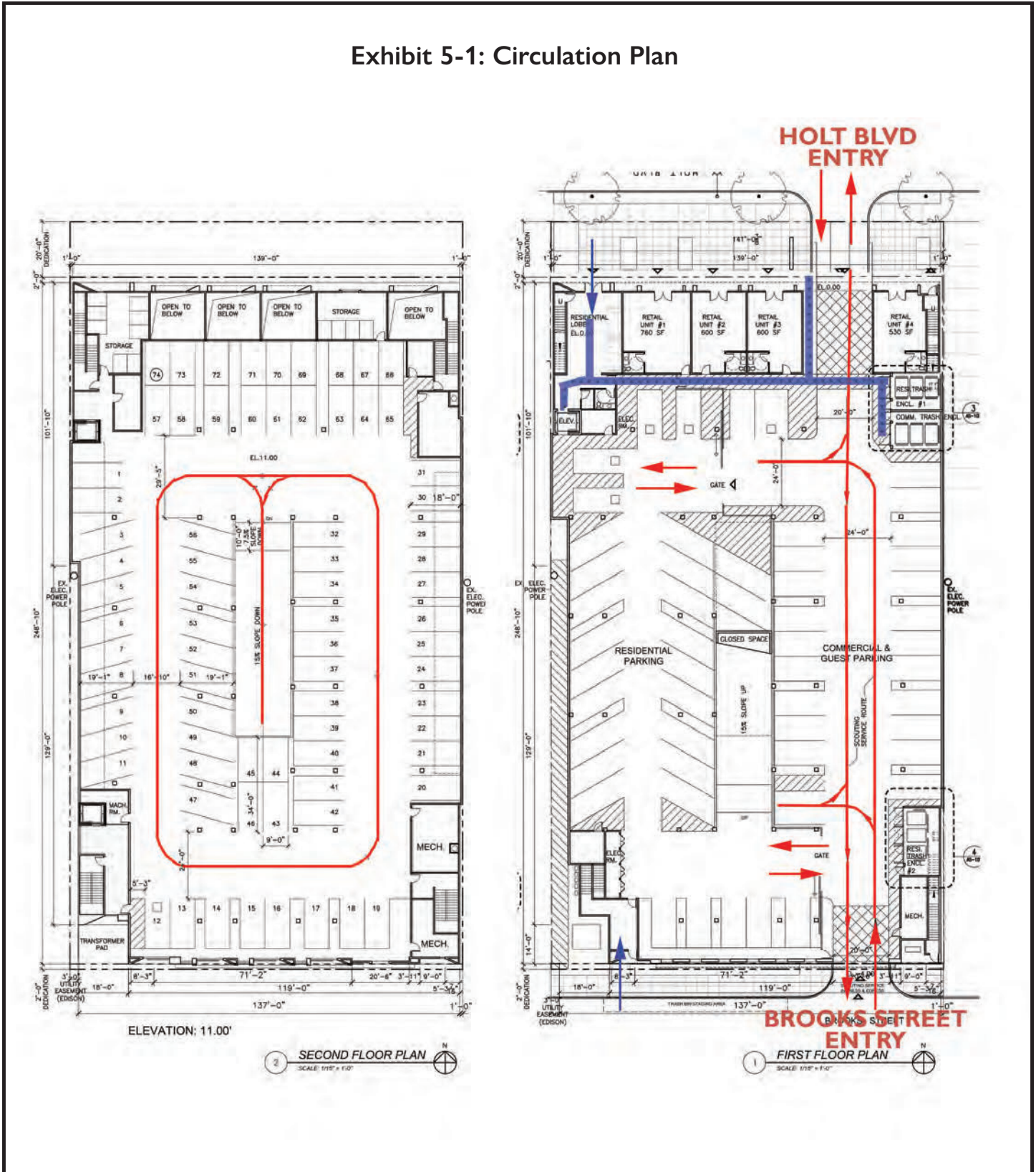
1. SWHP Content and Format: The Solid Waste Handling Plan shall demonstrate compliance with the Services Standards in the City's Solid Waste Planning Manual (available online at: <http://www.ontarioca.gov/government-departments-municipal-utilities-company/integrated-waste>) and shall contain, at a minimum, the following elements:

- a. A statement identifying the Service Requirements being used (e.g. Single Family Detached with automated cans, Multi-family/Commercial/Industrial with bins and enclosures, etc.) and describing the solid waste handling operation (for instance, will there be scouting services, etc.).
- b. A table utilizing the metrics on Page 8 of the Planning Manual and calculating the volume (gallons or cubic yards), quantity, and service schedule for each type of can and bin required for each Service Category (refuse, recycled, etc.).
- c. An Engineering Site Plan drawn to scale that shows:
 - i. Minimum plan scale of scale of 1:100. Larger scales are preferred and should be scaled to fill the sheet and show as much detail as clearly as possible on one sheet; multiple sheets may be used if entire project area cannot fit on one sheet at 1:100 scale.
 - ii. A detail of the Solid Waste Vehicle with dimensions and annotation that states the minimum turning radii and path of travel widths actually being used on the plan.
 - iii. The Solid Waste Vehicle turning movements and paths of travel in each direction of travel and at all intersections. All paths of travel shall be 15 feet wide minimum.
 - iv. All parking stalls and parallel parking spaces along all streets, alleys, or aisles.
 - v. All proposed curbs and areas designated and striped/signed as “No Parking”.
 - vi. All proposed trash enclosures and the ADA paths of travel from the buildings.
 - vii. A detail for each enclosure footprint delineating the number and size of the bins in order to demonstrate that the enclosure is adequately sized and oriented, if enclosures and bins are proposed.
 - viii. All proposed locations of automated cans shown as a 26-inch by 26-inch can pad with 20-inches between can pads and 40-inches between can pads and Parking spaces, mailboxes and other obstructions (Can Collection Area). Can Collection Areas shall be located along designated paths of travel and cannot be located along dead end alleys, motor courts, driveways, or private streets; use multi-family standards for enclosures in these cases.

2. Can Collection Area (CCA) Locations: If CCAs are being proposed in lieu of bin enclosures for residential units located along dead end alleys, motor courts, driveways, or private streets, then the SWHP shall comply with the following requirements:
 - a. CCAs cannot conflict or compete with potential parking areas. Proposed CCAs must be designated as “no parking” at all times with appropriate striping and signage.
 - b. Each residential unit must have a designated CCA and each CCA must delineated with markings so that its location and the unit it is designated for are easily identifiable.
 - c. Solid Waste Handling Plan shall include a detail showing how the CCAs will be delineated and identifiable.

3. Private Third Party Hauler: If any Solid Waste Collections are going to be provided by a private third party hauler, include on the SWHP:
 - a. A statement describing the service.
 - b. The names, contact information, and City of Ontario Commercial Recycler Collection Permit numbers of all private third party haulers. Note: all private third party organics and recycling haulers must be formally approved and permitted by the City of Ontario and meet City Code requirements, otherwise the City must provide the collection services. If the third party haulers do not have a City of Ontario Commercial Recycler Collection Permit, state if the third party hauler is charging a fee for service.
 - c. Show & label staging and collection areas for private third party haulers.

Exhibit 5-1: Circulation Plan



6 – DESIGN GUIDELINES

The entire project area is covered by the Ontario Plan Policy Plan (General Plan) and has been designated as mixed-use (Holt Boulevard) and as described in the Ontario Plan Downtown Mixed-Use Growth Area. This PUD area will be distinctive through the requirement of a well-designed projects that convey visual interest and character through:

- ▶ Appropriate scale and massing.
- ▶ Architectural style and design that are complementary and appropriate to its setting.
- ▶ The use of high quality materials that will create visual interest, high quality, durable and appropriate for the architectural style.
- ▶ Appropriate Site Planning to ensure building orientation visibility from the street, adequate number of driveways and sidewalks. Building setbacks, parking lot orientation, adequate refuse storage areas and well designed street frontages.

6.1 – Building Orientation

Building orientation shall be designed to minimize noise impacts, aide in providing property site safety, create proper accessibility to site amenities and parking, and to maximize views from residential units. Design should also be done in a way to create defensible spaces improving site safety.

- Orientation towards the exterior of the project area shall be carefully evaluated to ensure a proper relationship.
- Buildings shall be clustered in a way that creates defensible spaces providing views of interior open spaces and amenities. For units without garage spaces buildings shall be oriented to create minimal distances from assigned parking spaces to primary or secondary residential unit entries.

The proposed development units was set-backed 2' on Holt Blvd and 2' on Brooks St both streets to form a strong retail and parking podium development adding a more interesting dimensional form to the architecture. The overall look if the development is to appeal to the younger demographics of the city. A sample rendering shown below:

Exhibit 6-1: Architectural Elevations



North Elevation

Exhibit 6-2: Architectural Elevations



South Elevation

Exhibit 6-3: Architectural Rendering



North Perspective

Exhibit 6-4: Architectural Rendering



South Perspective

6.2 – Architectural Character

The Roosevelt Tower design leans toward Contemporary and Modern Minimalist Style. The design will make use of smooth cement plaster exterior finishes, simple square glazed window and storefront openings and decorative metal canopies throughout the development. Large angular forms, creating a series of ‘boxes’ are used on both north and south exterior facades to emulate the simple and uncluttered lines - a predominant character of the modernist architectural style used by Nuetra and Schindler popularized in the 1930’s. The use of strong accent colors all throughout the exterior elevations were meant to offer fresh and playful character that will differentiate the development from the other similar adjacent development along Holt Boulevard.

6.3 – Massing and Scale

All buildings shall be in scale with the surrounding community, with a majority of the building placement to occur toward the boundary of the project site. The prominent building elevations shall face both Holt Boulevard and Brooks Street. Buildings shall be 2 to 5 stories in height, containing a varied amount of units in each building, with varying façade planes so as not to create a “monotonous” look. Amenities and site design shall be developed with the pedestrian scale in mind.

6.4 – Architectural Details

Architectural variations should occur between buildings to help distinguish each building and create variety within the proposed development. Architectural details should be consistent with the architectural style proposed. Details shall complement and enhance openings, and accentuate the overall design of each building. Detailing can be achieved through the use of different colors, window and door treatments, and a variety in exterior textures and materials (stone, siding, etc.) to create interesting elevations.

Decorative awnings should be placed where feasible, over south facing windows, and adequate shade should be provided for outdoor spaces. Coverings shall use materials compatible with the architecture style to create a comprehensive building design. Exposed beams shall be integrated into the building design.

6.5 – Materials and Color

Materials and colors shall be compatible with, and complementary to, the overall architectural style of the project. Larger building masses shall use a combination of dark and light colors to break up building planes, and use light colored trim to help accentuate roof features. All colors shall be complementary, and the project shall utilize “warm tone” colors. Materials shall be a mixture of stucco, wood, rich stones and masonry siding, in a variety of complementary colors. A concrete or composite roof material shall be used.

6.6 – On-Site Streetscapes and Pedestrian Walkways

Street furniture and decorative paving should be used throughout the project area to enhance architectural design. The use of complementary materials and textures should be used to create a continuity and consistency that is desired throughout the project area.

A variety of decorative elements should be incorporated into the design of the project area, including, but not limited to, benches, tree grates, bike racks, enhanced paving, trash receptacles, and decorative bollards.

A selection of appropriate elements shall consider sidewalk widths, size and spacing of street trees, importance of the pedestrian path, and the requirement to maintain a minimum 4-foot wide walkway for pedestrian movement in all areas. All streetscape features shall comply with the Americans with Disabilities Act (ADA), along with accessibility requirements for public services, such as police and fire.

6.7 – On-Site Lighting

Decorative light fixtures compatible with the architectural style of the buildings shall be provided on-site.

A minimum of one footcandle of light shall be provided within parking areas and 0.5 footcandles of light within pedestrian pathways, in compliance with the City of Ontario Police Department standards.

6.8 – Signs

Signs within the project area shall comply with the Division 8.01 (Sign Regulations) of the Ontario Development Code. A Sign Program shall be reviewed and approved by the City for this proposed Mixed-Use project.

6.9 – Off-Site Improvements

All off-site improvements shall be installed in accordance with City standards, and to the satisfaction of the City Engineer.

Note: All Priority Land Use (PLU): Land use consisting of high-density residential, defined as a land use with at least ten (10) dwelling units per acre, industrial, commercial, mixed urban, and public transportation station land uses within the project area shall comply with the statewide Trash Provisions adopted by the State Water Resources Control Board (SWRCB) and trash requirements in the most current San Bernardino.

Exhibit 6-5: Streetscape Examples



7 – INFRASTRUCTURE AND UTILITIES

7.1 – Infrastructure

All improvements and alterations to public infrastructure (sewer, water, recycled water, storm drain, etc.) shall obtain approval from all appropriate agencies.

If infiltration is deemed feasible by a licensed geotechnical engineer, on-site storm water drainage facilities shall be provided for capture and infiltration of a 2-yr, 24-hour storm event, consistent with the San Bernardino County MS4 Program's Water Quality Management Plan (WQMP) requirements for new development. Storm water capture and infiltration facilities include the utilization of not proposed MaxWell Drywells and underground storm water infiltration chambers.

7.2 – Street Improvements

Street improvements (street, sidewalk, parkway, curb, gutter, street lights, etc.) shall be constructed with the development of the project site. Improvements will be required on the streets adjacent to the project area including Holt Boulevard and Brooks Street. Design and construction of street improvements shall be in accordance with the City standards, and to the satisfaction of the City Engineer. Exhibit 7-4 (Street Sections) identifies the improvements for each street adjacent to the project site.

Upon development of the project site, the developer shall construct the right-of-way improvements adjacent to the project site, to the centerline of the street (street, curb, gutter, parkway, sidewalk, etc.). Curb returns at entry and exit driveways to the project site on Holt Boulevard and Brooks Street, shall be designed and constructed with a minimum 10-foot radius. All Priority Land Use (PLU): Land use consisting of high-density residential, defined as a land use with at least ten (10) dwelling units per acre, industrial, commercial, mixed urban, and public transportation station land uses within the project area shall comply with the statewide Trash Provisions adopted by the State Water Resources Control Board (SWRCB) and trash requirements in the most current San Bernardino. See Exhibit 7-1 (Civil Plan).

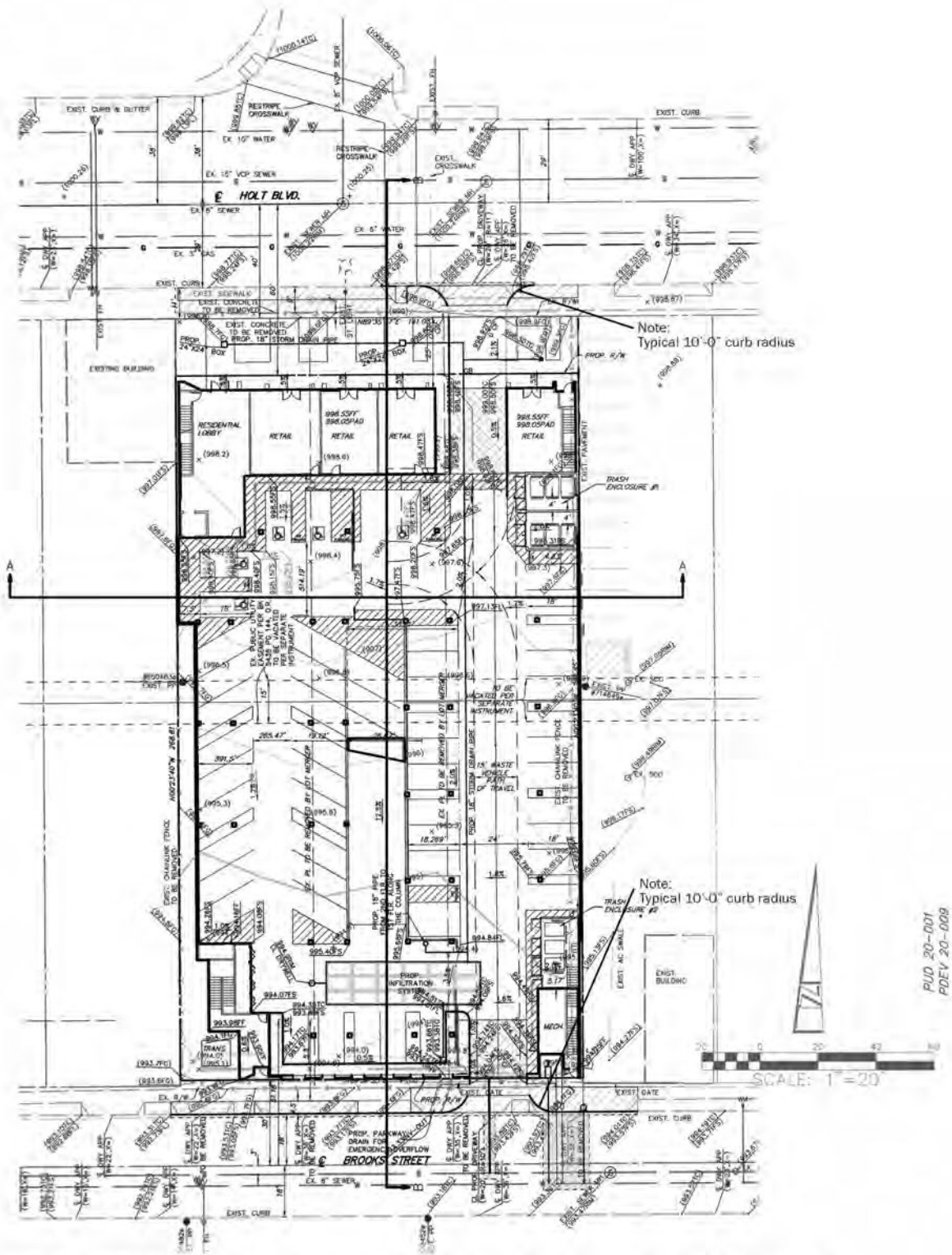
7.3 – Public Utilities

All existing dry utilities (electricity, cable, telephone, gas, etc.) along Holt Boulevard and Brooks Street shall be relocated and re-routed as required and approved by the affected utility agencies/companies.

7.4 – Street Dedication, Vacation and Easements

The ultimate Holt Boulevard right-of-way is 60 feet, measured from centerline, and will require an additional 20 feet of street dedication. The ultimate Brooks Street right-of-way is 30 feet, measured from centerline, and will require an additional 2 feet of street dedication.

Exhibit 7-1: Civil Plan



Note: "All Priority Land Use(PLU): Land use consisting of high-density residential, defined as a land use with at least ten (10) dwelling units per acre, industrial, commercial, mixed urban, and public transportation station land uses within the project area shall comply with the statewide Trash Provisions adopted by the State Water resources Control Board (SWRCB) and trash requirements in the most current San Bernardino"

Exhibit 7-2: Utility Plan

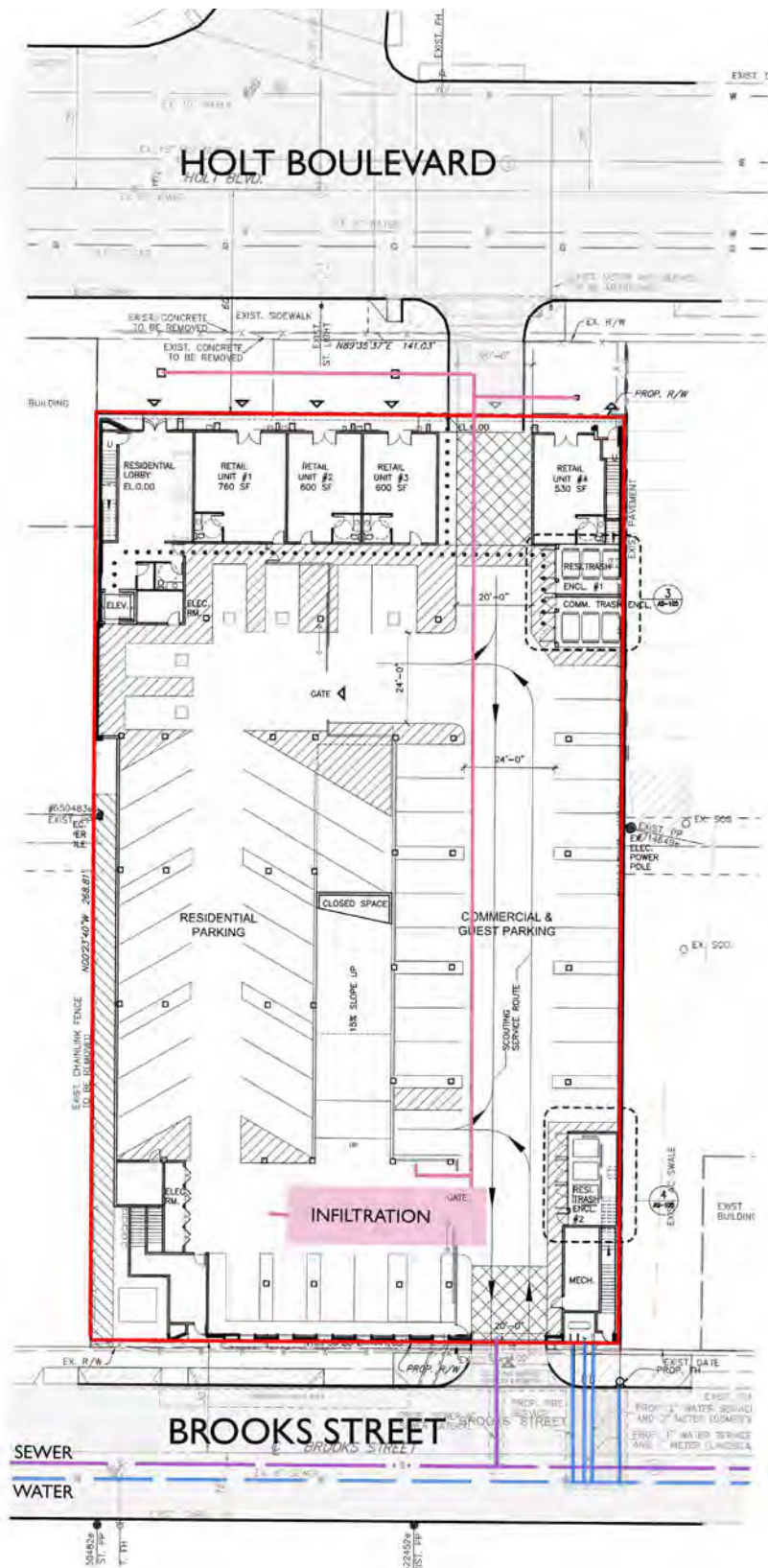


Exhibit 7-3: Easement and Dedication Locations

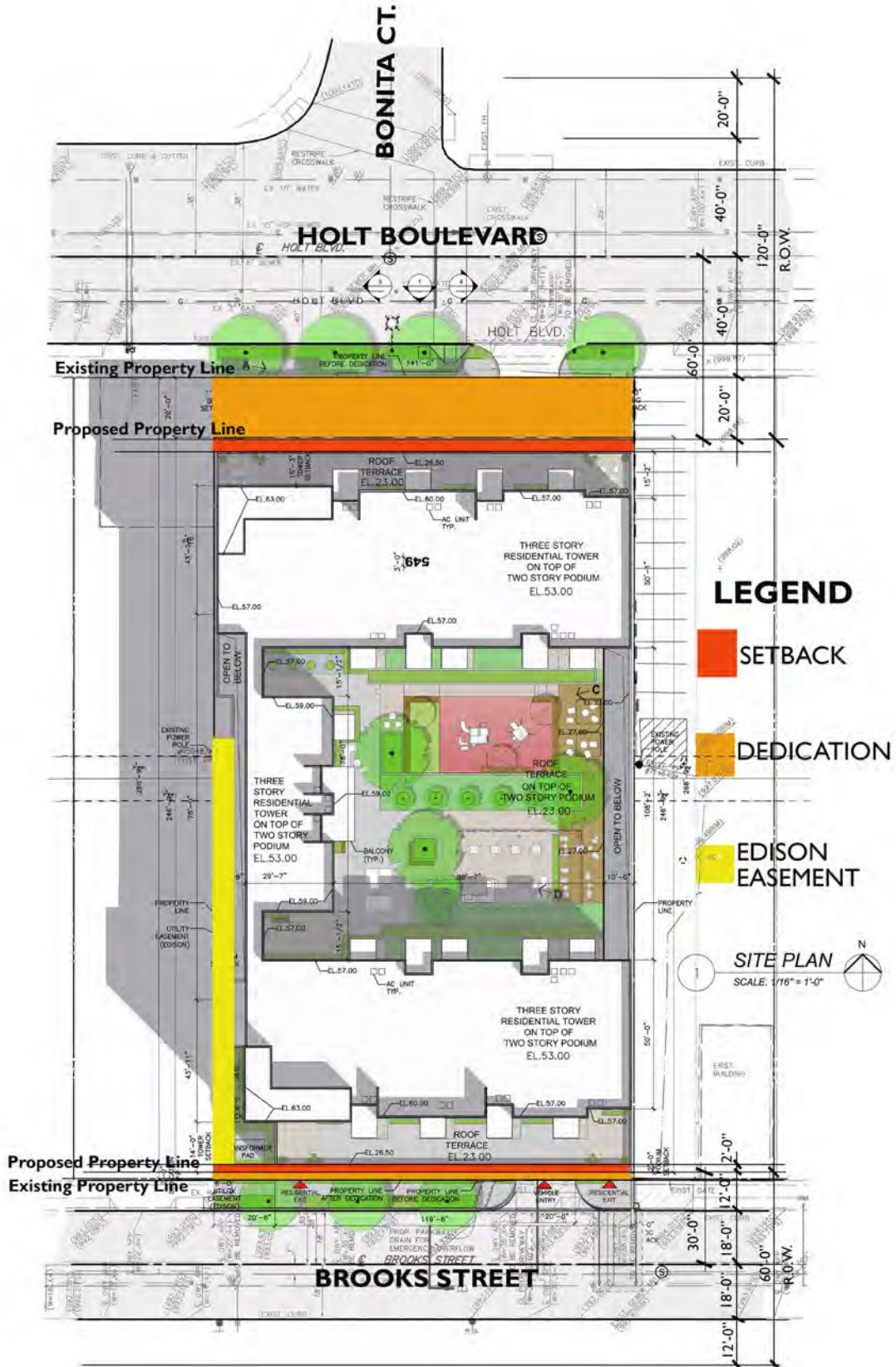
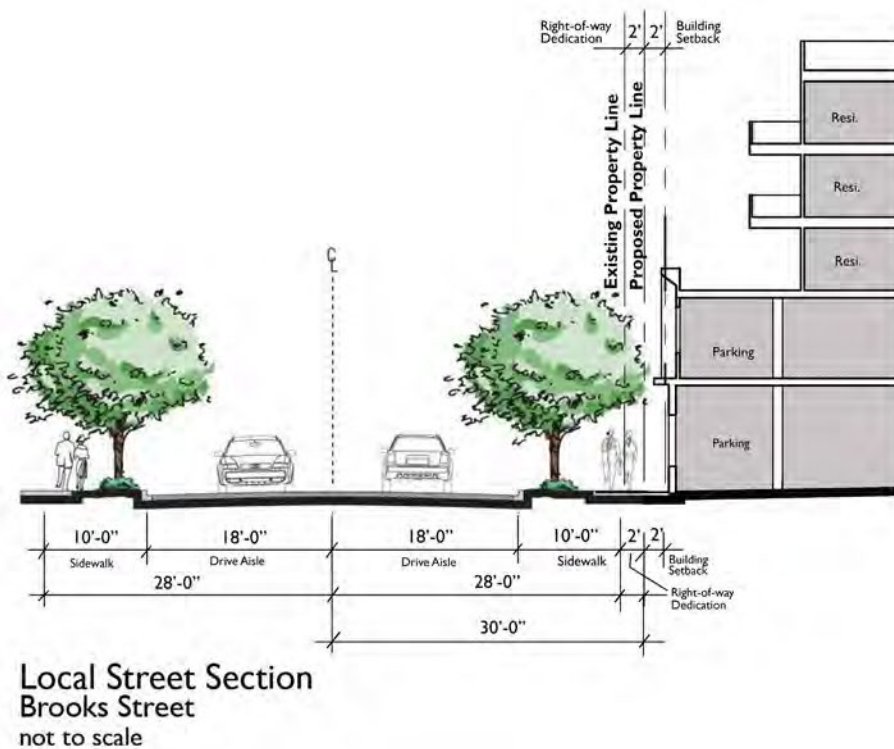
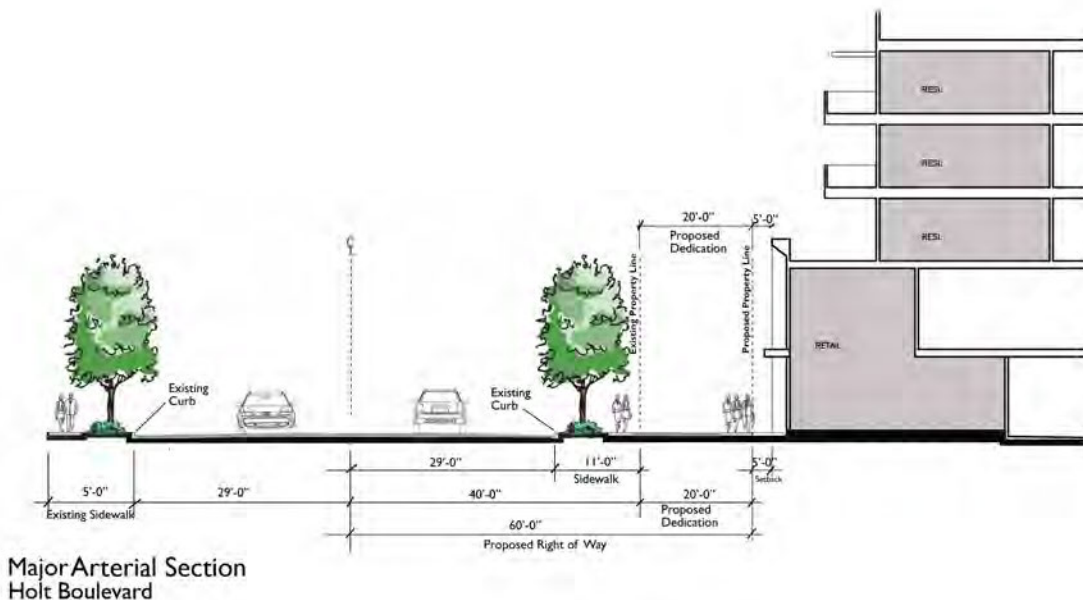


Exhibit 7- 4: Street Sections



8 – CEQA COMPLIANCE

An Addendum to The Ontario Plan Environmental Impact Report (SCH No. 2008101140), prepared in conjunction with File No. PGPA06-001, and certified by the City of Ontario City Council on January 27, 2010, was prepared for the project. The Approving Authority for the Project reviewed and considered the information contained in the Addendum, the initial study, and the administrative record for the Project, including all submitted written and oral evidence. Based upon the facts and information contained in the Addendum, the initial study, and the administrative record, including all written and oral evidence, the Approving Authority found as follows:

- The Addendum and administrative record have been completed in compliance with the California Environmental Quality Act (CEQA), the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and
- The Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Approving Authority; and
- There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts; and
- The project will not introduce any new significant environmental impacts beyond those previously analyzed in the Environmental Impact Report, and all mitigation measures previously adopted by the Environmental Impact Report, are incorporated herein by this reference.

8.1 – Environmental Performance Standards

Due to the proximity of the project area to rail lines and Holt Avenue which are both generators of noise and emissions, standards have been established to mitigate these environmental impacts.

8.1.1 – Noise

To ensure a proper standard of living, noise levels shall not exceed 65 dBA for exterior noise levels and 45 dBA for interior noise levels. In order to achieve these noise levels the following mitigations shall be followed:

- Prior to the issuance of building permits, the developer/owner shall retain an acoustic engineer to conduct an acoustic analysis and identify, where appropriate, site design features (e.g., setbacks, berms, or sound walls), and/or required building acoustical improvements (e.g., sound transmission class rated windows, doors and attic baffling), to ensure compliance with the City's Noise Compatibility Criteria, and the California State Building Code and California Noise Insulation Standards (CCR Titles 24 and 21).
- Prior to the issuance of building permits, the developer/owner shall retain an acoustical engineer to evaluate the potential for trains to create perceptible levels of vibration indoors. If vibration-related impacts are found, mitigation measures, such as use of concrete, iron, steel, or masonry materials to ensure that levels of vibration amplification are within acceptable limits to building occupants, shall be implemented. Pursuant to the Federal Transit Administration vibration-annoyance criteria, these acceptable limits are 78 VdB during the daytime and 72 VdB during the nighttime for residential uses.
- Construction activities shall only occur between the hours of 7:00 AM and 6:00 PM on weekdays, and 9:00 AM and 6:00 PM on Saturdays and Sundays;
- Structural noise attenuation requirements contained in Ontario Municipal Code Title 8, Chapter 15, Article 3 (Building Requirements for New Residential Construction in the 65 CNEL to 70 CNEL Noise Zone) are required to be incorporated into exterior façade units located along the eastern, western, and northern project perimeter.
- Structural noise attenuation requirements contained in Ontario Municipal Code Title 8, Chapter 15, Article 2 (Building Requirements for New Residential Construction in the 70 CNEL to 75 CNEL Noise Zone) are required to be incorporated into exterior façade units located along the southern project perimeter;
- Balconies on exterior façade units located along the project's east, south and north frontages shall have a solid balcony barrier of at least 5 feet per study findings. On exterior façade units located along the project's south perimeter street frontage, no balconies or yard areas shall be permitted; and
- If required upon findings of a study, a 6-foot high solid decorative masonry block wall with self-closing, sound insulated gates shall be constructed between the buildings located along the southern edge of the project site. This will ensure exterior noise for outdoor facilities within the project will not exceed 65 dB CNEL.

8.1.2 – Air Quality

- Due to the proximity of the active rail lines to the south of the project area, air quality is a concern for the project site resulting from the diesel emissions from trains. Listed below are standards for reducing the air quality impacts of the diesel emissions.
- All residential living areas shall be equipped with air filtration systems operating under a positive pressure rated at MERV 12 or higher;
- The HOA or property management will replace all unit filters on a routine basis, determined by industry standards for the filter and air conditioning HVAC systems selected;
- The active outdoor recreation areas should be shifted northward where feasible, to provide the greatest possible distance setback from the closest railroad tracks; and
- A dense tree canopy shall be established along the southern site boundary to act as a living bio-filter for particulate air pollution.

9 – ADMINISTRATION

9.1 – Items Not Addressed in PUD

Any terms or regulations pertaining to design, development, subdivision, administration and interpretation, and nonconforming use, structures and, which are not addressed in this PLANNED UNIT DEVELOPMENT, shall be governed by the City of Ontario Development Code.

9.2 – Development Applications

Development Plan approval, pursuant to the requirements of Ontario Development Code Section 4.02.025 (Development Plans), shall be required for the physical alteration of a lot, the construction of a building, or the addition or significant alteration of an existing building. A Development Plan application shall be submitted to the Planning Department on a City application form pursuant to the requirements of Ontario Development Code Division 2.02 (Application Filing and Processing), commencing with Subsection B (Discretionary Permits and Actions) of Section 2.02.015 (Application Processing Procedures).

9.3 – Administrative Exceptions

Minor exceptions to the development standards set forth in this document may be granted by the Zoning Administrator in accordance with Section 4.02.020.C (Administrative Exceptions) of the Ontario Development Code, not to exceed 10 percent from minimum residential setback and separation requirements.

9.4 – Severability

If any portion of this Planned Unit Development is held to be invalid, unconstitutional, or unenforceable by a court of competent jurisdiction, the determination shall not affect the validity of the remaining portions of this Planned Unit Development. Moreover, the decision shall not affect, impair, or nullify this Planned Unit Development, either in whole or in part, and the remainder of this Planned Unit Development shall continue in full force and effect.

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PDEV20-009, A DEVELOPMENT PLAN TO CONSTRUCT A MIXED-USE PROJECT CONSISTING OF 59 MULTIPLE-FAMILY DWELLINGS, 2,500 SQUARE FEET OF RETAIL SPACE, AND TWO LEVELS OF PODIUM PARKING ON 0.81-ACRE OF LAND LOCATED AT 549 WEST HOLT BOULEVARD, WITHIN THE MU-1/LUA-3 (DOWNTOWN MIXED-USE/HOLT BOULEVARD) ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF—APN: 1049-021-09.

WHEREAS, Kathy Huynh ("Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV20-009, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 0.81-acre of land located at 549 West Holt Boulevard, within the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard) zoning district and the site is presently vacant land; and

WHEREAS, the property to the north of the Project site is within the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard) zoning district and is developed with the Ontario Village Commercial Shopping Center and auto smog shop. The property to the south is within the IL (Light Industrial) zoning district and is developed with industrial type uses. The property to the east is within the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard) zoning district and is developed with multi-tenant commercial uses. The property to the west is within the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard) zoning district and is developed with multi-tenant commercial uses; and

WHEREAS, in conjunction with the Development Plan application, the Applicant submitted a Planned Unit Development application (File No. PUD20-001, Roosevelt Tower Planned Unit Development) that will establish the development standards, design guidelines and infrastructure requirements for the 0.81-acre Project site. Approval of the Development Plan is contingent upon City Council approval of the Roosevelt Tower Planned Unit Development; and

WHEREAS, staff has worked with the Applicant to design a project that meets the goals and requirements of the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard) zoning district, the Roosevelt Tower Planned Unit Development, and the goals and policies of The Ontario Plan ("TOP"); and

WHEREAS, the proposed building is set back five feet from the Holt Boulevard (north) property line, two feet from the Brooks Street (south) property line, and one-foot from the east and west property lines. The residential structures above the two stories of

podium parking will be set back 17 feet from the Holt Boulevard (north) property line, 18-feet from the Brooks Street (south) property line, and a zero-setback has been proposed from the east and west property lines; and

WHEREAS, the building is proposed at five-stories in height (68 feet), comprised of three-stories of residential units constructed over two-stories of podium parking. Additionally, 2,500 square feet of retail space has been incorporated into the building's first floor, facing the Project's Holt Boulevard street frontage. The retail space is divided into four separate units ranging from 540 to 760 square feet in area; and

WHEREAS, the project has been designed in conformance with the development regulations, standards, and design guidelines of the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District) zoning district and the Roosevelt Tower PUD. The project proposes two points of vehicular access. Primary access is provided from Holt Boulevard, at the northeast corner of the Project site. Secondary access is provided from Brooks Street, at the southeast corner of the Project site. Pedestrian access into the building is proposed at the northwest corner of the building, through a lobby with elevator access. Pedestrian access to the retail units is proposed from the Holt Boulevard public sidewalk. Residential units will be accessed through interior corridors located on each floor. Emergency access stairs have been incorporated at all four corners of the building; and

WHEREAS, the Project is proposed at a retail density of 72.8 du/ac, consisting of 13 studio units, 30 one-bedroom units, and 16 two-bedroom units. The residential units range from 403 to 987 square feet in area; and

WHEREAS, the project has been parked in accordance with the Ontario Development Code and the Roosevelt Tower Planned Unit Development requirements. The total required number of parking spaces is 127, which has been exceeded. All required parking will be located within the first two floors of the 5-story structure; and

WHEREAS, the Project proposes a Contemporary Architectural Design, exemplifying the type of high-quality architecture promoted by the Ontario Development Code, The Ontario Plan, and the Roosevelt Tower PUD. The mass and scale of the building along the Holt Boulevard frontage will serve to establish an urban sensibility, as envisioned by the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District) zoning district. Additionally, the varying stacked massing of the building, in combination with the articulated roof line, deep exterior wall insets, and the various story step-downs along the front of the building, provides visual interest and helps soften the pedestrian interface between the building façade and the street frontage along both Holt Boulevard and Brooks Street; and

WHEREAS, the project exceeds the minimum landscape requirements established by the City's Development Code and the Roosevelt Tower PUD. The project

will provide an average of 66 square feet of private open space per unit (60 square feet minimum required), in the form of private balconies, and 251.5 square feet of common open space for each unit (250 square feet minimum required). The common open space (14,853 square feet) will be provided on the third floor (podium deck) of the proposed 5-story building, which is fully landscaped and amenitized. The third floor includes an exercise room, entertainment area, restrooms, built-in barbecues and serving counters, rubberized play surface, a decorative steel shade structure, decorative pavers, wood decking, decorative masonry block raised planters, decorative benches, lounge seating, dining tables and chairs, several decorative umbrella shades, two play structures and ping pong tables; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act, commencing with Public Resources Code Section 21000 (hereinafter referred to as "CEQA"); and

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on July 6, 2022, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision No. DAB22-022, recommending that the Planning Commission approve the Application; and

WHEREAS, on July 26, 2022, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The administrative record has been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(2) The Project is categorically exempt from environmental review pursuant to Section 15332 (Class 32, In-Fill Development,) of the CEQA Guidelines, which consists of projects characterized as infill developments, meeting the following conditions:

(a) The Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as with applicable zoning designation and regulations. The proposed Project is located within the MU (Mixed Use) land use district of the Policy Plan Land Use Map, and the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District) zoning district. The proposed project is consistent with all applicable Policy Plan (general plan) policies, as well as with the requirements of the Roosevelt Tower PUD, and applicable Development Code regulations.

(b) The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses. The Project is proposed within the established boundaries of the City of Ontario, on a project site totaling 0.81-acre of land, which is surrounded by established commercial land uses.

(c) The Project site has no value as habitat for endangered, rare, or threatened species. The site is void of any flora or fauna, was previously graded, and is regularly maintained to prevent the growth of weeds and grasses.

(d) Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality. The proposed mixed-use development is similar to, and of no greater impact than other allowed residential uses and commercial development projects within the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District) zoning district.

(e) The site can be adequately served by all required utilities and public services. The site is located within an area that is currently served by all required utilities and public services, including, but not limited to sewer, water, gas, electric and municipal solid waste services; and

(3) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

(4) The determination of CEQA exemption reflects the independent judgment of the Planning Commission.

SECTION 2: *Housing Element Compliance.* Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 3: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as “ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting

documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 4: *Concluding Facts and Reasons.* Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 through 3, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Project is located within the MU (Mixed Use) land use district of the Policy Plan Land Use Map, and the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District) zoning district and the Roosevelt Tower Planned Unit Development (File No. PUD20-001). The development standards and conditions under which the proposed Project will be constructed and maintained are consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan. The Project consist of a mixed-use project composed of 59 multiple-family dwellings and 2,500 square feet of retail space (4 commercial tenant spaces), with two levels of podium parking. The project will provide housing and additional commercial services in the area, consistent with the TOP Policy LU1-6 (Complete Community). In addition, the Project will contribute to the establishment of a dynamic, progressive city containing distinct neighborhoods and districts that foster a positive sense of identity and belonging among residents, visitors, and businesses (Goal CD1). Furthermore, the Project will promote the City's policy to take actions that are consistent with the City being a leading urban center in Southern California, while recognizing the diverse character of our existing viable neighborhoods (Policy CD1-1); and

(2) ***The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located.*** The Project has been designed consistent with the requirements of the City of Ontario Development Code and the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District) zoning district and the Roosevelt Tower Planned Unit Development (File No. PUD20-001), including standards relative to the proposed 5-story structure, as well as building intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions. The project site is bordered by commercial and

automotive uses on the north, commercial multi-tenant land uses to the east and west, and industrial land uses to the south. The proposed five-story mixed-use project will not impose any privacy or view issues. The maximum building height allowed within the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District) zoning district and the Roosevelt Tower Planned Unit Development (File No. PUD20-001) is 75 feet, the proposed building will have a maximum height of 68 feet; and

(3) ***The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed project.*** The Planning Commission has required certain safeguards, and imposed certain conditions of approval, which have been established to ensure that: [i] the purposes of the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District) zoning district and the Roosevelt Tower Planned Unit Development (File No. PUD20-001) are maintained; [ii] the project will not endanger the public health, safety or general welfare; [iii] the project will not result in any significant environmental impacts; [iv] the project will be in harmony with the area in which it is located; and [v] the project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan vision, and the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District) zoning district and the Roosevelt Tower Planned Unit Development (File No. PUD20-001); and

(4) ***The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development.*** The proposed Project has been reviewed for consistency with the general development standards and guidelines of the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District) zoning district and the Roosevelt Tower Planned Unit Development (File No. PUD20-001), that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking and loading spaces, parking lot dimensions, design and landscaping, bicycle parking, on-site landscaping, and fences and walls, as-well-as those development standards and guidelines specifically related to the particular land use being proposed (5-story structure consisting of a mixed-use project composed of 59 multiple-family dwellings and 2,500 square feet of retail space (4 commercial retail tenant spaces), and two levels of podium parking). The project, as proposed, meets or exceeds all design guidelines, including parking, landscaping, setbacks, height, and circulation. As a result of this review, the Planning Commission has determined that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the development standards and guidelines described in the MU-1/LUA-3 (Downtown Mixed-Use/Holt Boulevard District) zoning district and the Roosevelt Tower Planned Unit Development (File No. PUD20-001).

SECTION 5: *Planning Commission Action.* Based upon the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as “Attachment A,” and incorporated herein by this reference.

SECTION 6: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 7: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East “B” Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 8: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26th day of July 2022, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Jim Willoughby
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on July 26, 2022, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**File No.: PDEV20-009
Departmental Conditions of Approval**

(Departmental conditions of approval to follow this page)



LAND DEVELOPMENT DIVISION CONDITIONS OF APPROVAL

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

Date Prepared: 6/20/2022

File No: PDEV20-009

Related Files: PUD20-001

Project Description: A hearing to consider a Development Plan to construct a mixed-use project consisting of 59 multiple-family dwellings and 2,500 square feet of retail space, with two levels of parking, on approximately 0.81-acres of land located at 549 West Holt Boulevard, within the MU-1 (Downtown Mixed-Use) zoning district. (APN: 1049-021-09); **submitted by Kathy Huynh.**

Prepared By: Luis E. Batres, Senior Planner
Phone: 909.395.2431
Email: Lbatres@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits.

(a) Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

2.2 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.3 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b) Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

(c) Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(d) Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

2.4 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

2.5 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b) All drive approaches shall be provided with an enhanced pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.

(c) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(d) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

(e) Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).

(f) Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11).

2.6 Outdoor Loading and Storage Areas.

(a) Loading facilities shall be designed and constructed pursuant to Development Code Division 6.03 (Off-Street Parking and Loading).

(b) Areas designated for off-street parking, loading, and vehicular circulation and maneuvering, shall not be used for the outdoor storage of materials or equipment.

2.7 Site Lighting.

(a) All off-street parking facilities shall be provided with nighttime security lighting pursuant to Ontario Municipal Code Section 4-11.08 (Special Residential Building Provisions) and Section 4-11.09 (Special Commercial/Industrial Building Provisions), designed to confine emitted light to the parking areas. Parking facilities shall be lighted from sunset until sunrise, daily, and shall be operated by a photocell switch.

(b) Unless intended as part of a master lighting program, no operation, activity, or lighting fixture shall create illumination on any adjacent property.

2.8 Mechanical and Rooftop Equipment.

(a) All exterior roof-mounted mechanical, heating and air conditioning equipment, and all appurtenances thereto, shall be completely screened from public view by parapet walls or roof screens that are architecturally treated so as to be consistent with the building architecture.

(b) All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.

2.9 Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

2.10 Signs. All Project signage shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations).

2.11 Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

2.12 Environmental Review.

(a) The proposed project is categorically exempt from the requirements of the California Environmental Quality Act of 1970 (CEQA), as amended, and the Guidelines promulgated thereunder, pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines, and meets all the following conditions:

(i) The Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations;

(ii) The proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses;

(iii) The project site has no value as habitat for endangered, rare, or threatened species;

(iv) Approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and

(v) The Project site can be adequately served by all required utilities and public services.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

2.13 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.14 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Exemption ("NOE") filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act ("CEQA"). The filing of an NOE is voluntary; however, failure to provide said fee within the time specified will result in the extension of the statute of limitations for the filing of a CEQA lawsuit from 30 days to 180 days.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

2.15 Additional Requirements.

- (a)** The proposed exterior stucco shall have a smooth finish.
- (b)** A Sign Program shall be submitted for review and approval prior to any signs being installed on the property/building.
- (c)** All 14 guest parking spaces shall be labeled as "Guest Parking".
- (d)** During the plan check process, the Applicant shall submit a color parking plan that illustrates what parking space(s) will be assigned to each residential unit.
- (e)** Any publicly exposed concrete shall have a smooth finish.
- (f)** Applicant shall work with staff during the plan check process, and prior to building occupancy, to add decorative potted plants with an approved irrigation system, along the north and south elevations of the building.
- (g)** Approval of the Development Plan (File No. PDEV20-009) is contingent upon the City Council approval of the related PUD (File No. PUD20-001).

CITY OF ONTARIO
LANDSCAPE PLANNING DIVISION
 303 East "B" Street, Ontario, CA 91764

DAB CONDITIONS OF APPROVAL

Sign Off



Jamie Richardson, Sr. Landscape Planner

2/07/2022

Date

Reviewer's Name: Jamie Richardson, Sr. Landscape Planner		Phone: (909) 395-2615
D.A.B. File No.: PDEV20-009		Case Planner: Luis Batres
Project Name and Location: Mixed Use Retail and Multi Family Downtown Project 549 W Holt Blvd		
Applicant/Representative: SHA Landscape Architecture- Ben McCoy bmccoy@sha-design.com 480 N Indian Hill Blvd. Suite 1D Claremont CA, 91711		
<input checked="" type="checkbox"/>	A Preliminary Landscape Plan (dated 1/6/2022) meets the Standard Conditions for New Development and has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.	
<input type="checkbox"/>	A Preliminary Landscape Plan (dated) has not been approved. Corrections noted below are required prior to Preliminary Landscape Plan approval.	
<p>A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED AS INCOMPLETE. Landscape construction plans with plan check number may be emailed to: landscapeplancheck@ontarioca.gov DIGITAL SUBMITTALS MUST BE 10MB OR LESS.</p>		

Civil/ Site Plans

1. Show backflow devices set back 4' from paving all sides. Locate on level grade in a locking enclosure.
2. Locate utilities including light standards, fire hydrants, water, drain and sewer lines to not conflict with required tree locations. Coordinate civil plans with landscape plans.
3. Note for compaction to be no greater than 85% at landscape areas. All finished grades at 1 ½" below finished surfaces. Slopes to be maximum 3:1.
4. Show existing and ultimate right of way for Holt Blvd.
5. Add Note to Grading and Landscape Plans: Landscape areas where compaction has occurred due to grading activities and where trees or storm water infiltration areas are located shall be loosened by soil fracturing. For trees a 12'x12'x18" deep area; for storm water infiltration the entire area shall be loosened. Add the following information on the plans: The back hoe method of soil fracturing shall be used to break up compaction. A 4" layer of Compost is spread over the soil surface before fracturing is begun. The back hoe shall dig into the soil lifting and then drop the soil immediately back into the hole. The bucket then moves to the adjacent soil and repeats. The Compost falls into the spaces between the soil chunks created. Fracturing shall leave the soil surface quite rough with large soil clods. These must be broken by additional tilling. Tilling in more Compost to the surface after fracturing per the soil report will help create an A horizon soil. Imported or reused Topsoil can be added on top of the fractured soil as needed for grading. The Landscape Architect shall be present during this process and provide certification of the soil fracturing. For additional reference see Urban Tree Foundation – Planting Soil Specifications.

Landscape Plans

6. Street tree for Holt Blvd is the Fraxinus oxycarpa 'Raywood Ash.'
7. Limit use of Bamboo to containers; invasive.
8. Replace Phormium with Dianella.
9. Provide details or exhibits showing the play equipment; include manufacturer, colors schemes and type of activities.
10. Show backflow devices with 36" high strappy leaf shrub screening and trash enclosures and transformers, a 4'-5' high evergreen hedge screening. Do not encircle utility, show as masses and duplicate masses in other locations on regular intervals.
11. Show all utilities on the landscape plans. Coordinate so utilities are clear of tree locations.
12. Detail irrigation dripline outside of mulched root zone.
13. Designer or developer to provide agronomical soil testing and include report on landscape construction plans.
14. After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections at a rate established by resolution of the City Council.
15. Landscape construction plans with building permit number for plan check may be emailed to: landscapeplancheck@ontarioca.gov

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT



Project File No.: PUD20-001 & PDEV20-009

Address: 549 West Holt Blvd

APN: 1049-021-09

Existing Land Use: Vacant

Proposed Land Use: PUD for 549 West Holt Blvd & Development Plan to construct a Mixed-Use Residential (57 Multi-family units) and Commercial (2,744 SF) building

Site Acreage: 0.81 Proposed Structure Height: 57 FT

ONT-IAC Project Review: n/a

Airport Influence Area: ONT

Reviewed By:

Lorena Mejia

Contact Info:

909-395-2276

Project Planner:

Luis Batres

Date: 12/23/2020

CD No.: 2020-023

PALU No.: n/a

The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="radio"/> High Terrain Zone	<input type="radio"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input checked="" type="checkbox"/> FAA Notification Surfaces	<input checked="" type="checkbox"/> Recorded Overflight Notification
<input type="radio"/> Zone 2	<input type="radio"/> 65 - 70 dB CNEL	<input checked="" type="checkbox"/> Airspace Obstruction Surfaces	<input type="radio"/> Real Estate Transaction Disclosure
<input type="radio"/> Zone 3	<input checked="" type="checkbox"/> 60 - 65 dB CNEL	<input type="radio"/> Airspace Avigation Easement Area	
<input type="radio"/> Zone 4		Allowable Height: 110 FT	
<input type="radio"/> Zone 5			

The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1
 Zone 2
 Zone 3
 Zone 4
 Zone 5
 Zone 6

Allowable Height: _____

CONSISTENCY DETERMINATION

This proposed Project is: Exempt from the ALUCP Consistent Consistent with Conditions Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

Airport Planner Signature: _____

CITY OF ONTARIO

MEMORANDUM

TO: PLANNING DEPARTMENT, Luis Batres
FROM: BUILDING DEPARTMENT, Kevin Shear
DATE: April 17, 2020
SUBJECT: PDEV20-009

-
- The plan **does** adequately address the departmental concerns at this time.
 No comments
 Report below.

Conditions of Approval

1. Standard Conditions of Approval apply.

KS:lr



CITY OF ONTARIO

MEMORANDUM

TO: Luis Batres, Senior Planner
Planning Department

FROM: Mike Gerken, Deputy Fire Chief/Fire Marshal
Fire Department

DATE: February 16, 2021

SUBJECT: PDEV20-009 - A Development Plan to construct a mixed-use project consisting of 59 multiple-family dwellings and 2,744 square feet of retail space (4-Units) on approximately 0.81-acre (35,085 sq. ft.) of land located at 549 West Holt Boulevard, within the MU-1 (Downtown Mixed Use) zoning district (APN: 1049-021-09). Related Files: PUD-20-001 and PPRE19-002. (*Revision 1*)

-
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply. See previous report for Conditions.



CITY OF ONTARIO

MEMORANDUM

TO: Luis Batres, Senior Planner
Planning Department

FROM: Paul Ehrman, Deputy Fire Chief/Fire Marshal
Fire Department

DATE: April 27, 2020

SUBJECT: PDEV20-009 – A Development Plan to construct a mixed use project consisting of 57 multiple-family dwellings and 2,744 square feet of retail space on approximately 0.81 acre of land located at 549 W Holt Blvd, within the Downtown Mixed Use (MU-1) zoning district (APN(s): 1049-021-09). Related File(s): PUD20-001 & PPRE19-002.

-
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.

SITE AND BUILDING FEATURES:

- A. 2016 CBC Type of Construction: Not Listed (Type V)
- B. Type of Roof Materials: Ordinary
- C. Ground Floor Area(s): 32,954 Sq. Ft.
- D. Number of Stories: 5
- E. Total Square Footage: 110,972 Sq. Ft.
- F. 2016 CBC Occupancy Classification(s): R2

CONDITIONS OF APPROVAL:

1.0 GENERAL

- 1.1 The following are the Ontario Fire Department (“Fire Department”) requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards (“Standards.”) It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at www.ontarioca.gov, click on “Fire Department” and then on “Standards and Forms.”
- 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

2.0 FIRE DEPARTMENT ACCESS

- 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25’) inside and forty-five feet (45’) outside turning radius per Standard #B-005.
- 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150’) in length shall have an approved turn-around per Standard #B-002.
- 2.4 Access drive aisles which cross property lines shall be provided with CC&Rs, access easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of building plan check.
- 2.5 "No Parking-Fire Lane" signs and /or red painted curbs with lettering are required to be installed in interior access roadways, in locations where vehicle parking would obstruct the minimum clear width requirement. Installation shall be per Standard #B-001.
- 2.6 Security gates or other barriers on fire access roadways shall be provided with a Knox brand key switch or padlock to allow Fire Department access. See Standards #B-003, B-004 and H-001.
- 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-four (24) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by Fire Department and other emergency services.

3.0 WATER SUPPLY

- ☒ 3.1 The required fire flow per Fire Department standards, based on the 2016 California Fire Code, Appendix B, is 4000 gallons per minute (g.p.m.) for 3 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.
- ☒ 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.
- ☒ 3.3 Buildings that exceed 100,000 square feet in floor area shall provide an onsite looped fire protection water line around the building(s.) The loops shall be required to have two or more points of connection from a public circulating water main.
- ☒ 3.4 The water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

4.0 FIRE PROTECTION SYSTEMS

- ☒ 4.2 Underground fire mains which cross property lines shall be provided with CC & R, easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of Fire Department plan check. The shared use of private fire mains or fire pumps is allowable only between immediately adjacent properties and shall not cross any public street.
- ☒ 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- ☒ 4.4 Wood frame buildings that are to be sprinkled shall have these systems in service (but not necessarily finalized) before the building is enclosed.
- ☒ 4.5 Fire Department Connections (FDC) shall be located on the address side of the building within one hundred fifty feet (150') of a public fire hydrant on the same side of the street. Provide identification for all fire sprinkler control valves and Fire Department connections per Standard #D-007. Raised curbs adjacent to Fire Department connection(s) shall be painted red, five feet either side, per City standards.
- ☒ 4.6 A fire alarm system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 72. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.

- ☒ 4.7 Portable fire extinguishers are required to be installed prior to occupancy per Standard #C-001. Please contact the Fire Prevention Bureau to determine the exact number, type and placement required.

5.0 BUILDING CONSTRUCTION FEATURES

- ☒ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ☒ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- ☒ 5.3 Single station smoke alarms and carbon monoxide alarms are required to be installed per the California Building Code and the California Fire Code.
- ☒ 5.4 Multiple unit building complexes shall have building directories provided at the main entrances. The directories shall be designed to the requirements of the Fire Department, see Section 9-1 6.06 of the Ontario Municipal Code and Standard #H-003. .
- ☒ 5.6 Knox ® brand key-box(es) shall be installed in location(s) acceptable to the Fire Department. All Knox boxes shall be monitored for tamper by the building fire alarm system. See Standard #H-001 for specific requirements.
- ☒ 5.7 Placards shall be installed in acceptable locations on buildings that store, use or handle hazardous materials in excess of the quantities specified in the CFC. Placards shall meet the requirements of National Fire Protection Association (NFPA) Standard 704.

6.0 OTHER SPECIAL USES

- ☒ 6.1 The storage, use, dispensing, or handling of any hazardous materials shall be approved by the Fire Department, and adequate fire protection features shall be required. If hazardous materials are proposed, a Fire Department Hazardous Materials Information Packet, including Disclosure Form and Information Worksheet, shall be completed and submitted with Material Safety Data Sheets to the Fire Department along with building construction plans.

7.0 PROJECT SPECIFIC CONDITIONS

- ☒ 7.1 NOTE: Due to the fact of residential dwellings above, the proposed parking structure shall be required to be 100% sprinklered



CITY OF ONTARIO

MEMORANDUM

TO: Luis Batres, Senior Planner

FROM: Officer Bill Lee, Police Department

DATE: April 13, 2021

SUBJECT: PDEV20-009: A DEVELOPMENT PLAN TO A CONSTRUCT A MIXED-USE PROJECT CONSISTING OF 57 MULTIPLE-FAMILY DWELLINGS AND 2,744 SQUARE FEET OF RETAIL SPACE AT 549 W. HOLT BOULEVARD.

The “Standard Conditions of Approval” contained in Resolution No. 2017-027 apply. The applicant shall read and be thoroughly familiar with these conditions, including but not limited to, the requirements listed below.

- Areas outside apartments, duplexes, and condominiums, are to be provided with a photocell operated exterior lighting system, which switches on at sunset and switches off at sunrise. This is to include walkways, doorways, and other areas used by the public and common areas. Minimum 0.5 foot-candle of light shall be maintained in all common areas. Planned landscaping shall not obstruct lighting.
- During hours of darkness, all parking lots and carports shall be provided with minimum one foot-candle of light, measured on the parking surface. Lighting devices shall be fully protected with weather and vandalism resistant covers.
- First floor and all common stairwells shall be constructed to either allow for visibility through the stairwell risers or to prohibit public access to the areas behind stairwells. Prevention of public access shall not be constructed to allow for easy disregard of barrier and shall fully prohibit access to area.
- Parking garages, stairwells, blind spots and any hidden areas shall have Convex mirrors to allow for visibility to the areas.
- The Applicant shall comply with all construction site security requirements as stated in the Standard Conditions. This includes the provisions for perimeter lighting, site lighting, fencing and/or uniformed security.
- Rooftop addresses shall be installed on the buildings as stated in the Standard Conditions. The numbers shall be at a minimum 3 feet tall and 1 foot wide, in reflective white paint on a flat black background, and oriented with the bottom of the numbers towards the addressed street. Each building and/ or suite shall be labeled with the corresponding address and letter if applicable.

- Common areas for residents shall not be accessible or open to the public and shall be clearly marked restricted. Entry into these areas shall require an individual code, key, fob or remote access.
- Maintain all landscaping on property to a standard that all ground covering shrubbery and hedges are no taller than 2 feet (24") and the lower canopy of all trees is no lower than 6 feet (72").
- Trash Enclosures shall prohibit public access. Trash enclosures shall remain locked and require code, key, fob or remote access.

The Applicant is invited to call Bill Lee at (909) 408-1671 with any questions regarding these conditions.



**ENGINEERING DEPARTMENT
CONDITIONS OF APPROVAL**

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Broadband Operations & Investment and Revenue Resources Department Conditions incorporated)

<input checked="" type="checkbox"/> DEVELOPMENT PLAN <input type="checkbox"/> OTHER	<input type="checkbox"/> PARCEL MAP <input type="checkbox"/> TRACT MAP <input type="checkbox"/> FOR CONDOMINIUM PURPOSES
PROJECT FILE NO. PDEV20-009 RELATED FILE NO(S). PUD20-001 & PPRE19-002	
<input checked="" type="checkbox"/> ORIGINAL <input type="checkbox"/> REVISED: __/__/__	

CITY PROJECT ENGINEER & PHONE NO: Yesenia Lopez (909) 395-2103 *pl*

CITY PROJECT PLANNER & PHONE NO: Luis Batres (909) 395-2431

DAB MEETING DATE: July 6, 2022

PROJECT NAME / DESCRIPTION: PDEV20-009, A Development Plan to construct a mixed-use project consisting of 57 multiple-family dwellings and 2,744 square feet of retail space on approximately 0.81 acre of land located at 549 W Holt Blvd within the Downtown Mixed Use (MU-1) zoning district. APN: 1049-021-09

LOCATION: 549 W Holt Blvd

APPLICANT: Creative Design Associates

REVIEWED BY: Raymond Lee 6/16/22
 Raymond Lee, P.E. Date
 Assistant City Engineer

APPROVED BY: [Signature] 6-20-22
 Khoi Do, P.E. Date
 City Engineer



THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.

1. PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, APPLICANT SHALL: Check When Complete

- 1.01 Dedicate to the City of Ontario, the right-of-way, described below:
 _____ feet on _____
 Property line corner 'cut-back' required at the intersection of _____
 and _____.
- 1.02 Dedicate to the City of Ontario, the following easement(s): _____

- 1.03 Restrict vehicular access to the site as follows: _____
- 1.04 Vacate the following street(s) and/or easement(s):
 A. All interfering on-site easements shall be quitclaimed, vacated, and/or submit non-interference letter from affected owner/utility company.
- 1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.
- 1.06 Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.
- 1.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.
- 1.08 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Financial Services Department at (909) 395-2124 regarding this requirement.
 (1) _____
 (2) _____
- 1.09 Prepare a fully executed Subdivision Agreement (on City approved format and forms) with



accompanying security as required, or complete all public improvements.

- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ci.ontario.ca.us) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.
- 1.13 Ontario Ranch Developments:
 - 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
 - 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
 - 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions: _____

2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:

**A. GENERAL
(Permits includes Grading, Building, Demolition and Encroachment)**

- 2.01 Record Parcel Map/Tract Map No. _____ pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 Submit a PDF of the recorded map to the City Engineer's office.
- 2.03 **Note that the subject parcel will be a recognized parcel in the City of Ontario per the required Lot Line Adjustment.**
- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of March 4, 1972.
- 2.05 **Apply for a:**
 - Certificate of Compliance with a Record of Survey;
 - Lot Line Adjustment (Record a Conforming Deed with the County of San Bernardino within six months of the recordation of the Lot Line Adjustment to conform the new LLA legal description. Submit a copy of the recorded Conforming Deed to the Engineering Department.);**
 - Make a Dedication of Easement.



- 2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.

- 2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.

- 2.08 **Submit a soils/geology report.**

- 2.09 **Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:**
 - State of California Department of Transportation (Caltrans)
 - San Bernardino County Road Department (SBCRD)
 - San Bernardino County Flood Control District (SBCFCD)
 - Federal Emergency Management Agency (FEMA)
 - Cucamonga Valley Water District (CVWD) for sewer/water service
 - United States Army Corps of Engineers (USACE)
 - California Department of Fish & Game
 - Inland Empire Utilities Agency (IEUA)
 - Other: Submit a non-interference letter from the easement holders listed on the title report.**

- 2.10 **Dedicate to the City of Ontario the right-of-way described below:**
20 feet on Holt Boulevard and 2 feet on Brooks Street.

- 2.11 Dedicate to the City of Ontario the following easement(s): _____

- 2.12 **Vacate the following street(s) and/or easement(s):**
 - A. The entire Public Utility Easement located on-site per Book 5438, Page 144.**
 - B. All interfering on-site easements shall be quitclaimed, vacated, and/or submit non-interference letter from affected owner/utility company.**

- 2.13 Ontario Ranch Developments:
 - 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.
 - 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay



any applicable fees as set forth by said agreement.

3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall.

- 2.14 **Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at 100% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.**
- 2.15 **The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.**
- 2.16 **Pay all Development Impact Fees (DIF) to the Building Department. Storm Drain Development Impact Fee shall be paid to the Building Department. Final fee shall be determined based on the approved site plan.**
- 2.17 **Other conditions:**
 - **Provide a Final Utilities Systems Map that shows all existing and proposed Utilities (potable water, recycled water, sewer, storm drain, and other utilities) including each of the City's public utilities' points of connection to the existing systems.**



B. PUBLIC IMPROVEMENTS

(See attached Exhibit 'A' for plan check submittal requirements.)

- 2.18 Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):

Improvement	Holt Blvd.	Brooks St.	Street 3	Street 4
Curb and Gutter	<input type="checkbox"/> New; ___ ft. from C/L <input checked="" type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New; 18 ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
AC Pavement	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Drive Approach	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace <input checked="" type="checkbox"/> In-fill existing drive approach(es) to be abandoned	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Sidewalk	<input checked="" type="checkbox"/> New <input checked="" type="checkbox"/> Replace damaged	<input checked="" type="checkbox"/> New <input checked="" type="checkbox"/> Replace damaged	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
ADA Access Ramp	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Parkway	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
Raised Landscaped Median	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace



Fire Hydrant	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Sewer (see Sec. 2.C)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Water (see Sec. 2.D)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Services	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input type="checkbox"/> New <input checked="" type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Street Light (see Sec. 2.F)	<input checked="" type="checkbox"/> Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2G)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Fiber Optics (see Sec. 2K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
Removal of Improvements	Existing water meter and service to be abandoned	Existing drive approaches	_____	_____
Other Improvements	_____	Construct a new commercial driveway on previously shared drive approach east of property	_____	_____



Specific notes for improvements listed in item no. 2.18, above: _____

- 2.19 **Slurry entire street width across the project frontage on the following street(s):**
 - **Brooks Street**
- 2.20 Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.
- 2.21 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide water service sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.
- 2.22 Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately _____, for undergrounding of utilities in accordance with Section 7-7.302.e of the City's Municipal Code.
- 2.23 **Other conditions:**
 - **Construct each trash enclosure to accommodate three (3) 4CY bins (one 4CY bin for refuse, one 4CY bin for recycling, and one 4CY bin for organics) for each of the two proposed trash enclosures.**
 - **This site will be required to have trash pickup scouting service and staging of bins for pickup.**

C. SEWER

- 2.24 **An 8 inch sewer main is available for connection by this project in Brooks St. (Ref: Sewer plan bar code: S11825)**
- 2.25 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.26 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.
- 2.27 **Other conditions:**
 - **Construct 6 inch sewer service and connect to existing 8 inch sewer main on Brooks Street.**

D. WATER

- 2.28 **An 8 inch water main is available for connection by this project in Brooks St. (Ref: Water plan bar code: unknown)**
- 2.29 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.30 **Other conditions:**
 - **Abandon existing water meter and service on Holt Blvd.**
 - **Construct separate fire, domestic and irrigation services per the latest City Design Guidelines/Standard Drawings.**
 - **The proposed water services must be at minimum 10-ft away from all proposed sewer laterals.**



E. RECYCLED WATER

- 2.31 A _____ inch recycled water main is available for connection by this project in _____.
(Ref: Recycled Water plan bar code: _____)
- 2.32 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.
- 2.33 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.
- 2.34 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.

Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.

- 2.35 Other conditions: _____

F. TRAFFIC / TRANSPORTATION

- 2.36 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer:
 - 1. On-site and off-site circulation
 - 2. Traffic level of service (LOS) at 'build-out' and future years
 - 3. Impact at specific intersections as selected by the City Engineer
- 2.37 New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.
- 2.38 **Other conditions:**
 - **Install an ADA pedestrian ramp located on the project frontage on Holt Boulevard. This ADA pedestrian ramp will be aligned with the existing crosswalk on the east side of the Holt Boulevard and Bonita Court intersection.**
 - **Design and construct proposed driveway onto Holt Boulevard in accordance with City of Ontario Standard Drawing No. 1204 for Commercial Driveways. The proposed driveway shall be restricted to right-in/right-out access only once the West Valley Connector Project striped median is installed along Holt Boulevard and/or once the City TCC Project installs a pedestrian hybrid beacon on Holt Boulevard at the Bonita Court crosswalk.**
 - **Holt Boulevard shall be signed "No Parking Anytime" along the property frontage.**
 - **Parking on the Brooks Street property frontage shall be restricted 20 feet on either side of the proposed driveway with red curb.**
 - **Retrofit existing street light to LED along its property frontage on Holt Boulevard. Street lighting shall be LED-type and in accordance with City's Traffic and Transportation Design Guidelines.**
 - **Design and construct in-fill public street lights and potential new service along its project frontage on Brooks Street. Street lighting shall be LED-type and in accordance with City's Traffic and Transportation Design Guidelines.**



- The Applicant/Developer's engineer-of-record shall meet with City Engineering staff prior to designing and submitting for plan check the street lighting design plans to define limits of improvements.

G. DRAINAGE / HYDROLOGY

- 2.39 A _____ inch storm drain main is available to accept flows from this project in _____.
(Ref: Storm Drain plan bar code: _____)
- 2.40 **Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.**
- 2.41 **An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100 year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.**
- 2.42 Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.
- 2.43 Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.
- 2.44 Other conditions: _____

H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)

- 2.45 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels.
If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.
Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.
- 2.46 **Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: <http://www.sbccounty.gov/dpw/land/npdes.asp>.**
- 2.47 Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.



2.48 Other conditions:

J. SPECIAL DISTRICTS

2.49 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.

2.50 Other conditions: _____

K. FIBER OPTIC

2.51 A _____ fiber optic line is available for connection by this project in _____. (Ref: Fiber Optic plan bar code: _____)

2.52 **Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the east and west end of the property line on Holt Boulevard. See Fiber Optic Exhibit herein.**

2.53 **Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Broadband Operations Department at (909) 395-2000, regarding this requirement.**

3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:

3.01 **Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.**

3.02 Complete all requirements for recycled water usage.

1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.

2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.

3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.

3.03 **The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.**

3.04 Ontario Ranch Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.



- 3.05 Confirm payment of all Development Impact Fees (DIF) to the Building Department.
- 3.06 Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).

4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:

- 4.01 Complete all Conditions of Approval listed under Sections 1-3 above.
- 4.02 Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.
- 4.03 The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall include a completed Acceptance and Bond Release Checklist, state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.
- 4.04 Submit record drawings (PDF) for all public improvements identified within Section 2 of these Conditions of Approval.



EXHIBIT 'A'

**ENGINEERING DEPARTMENT
First Plan Check Submittal Checklist**

Project Number: PDEV 20-009

The following items are required to be included with the first plan check submittal:

1. **A copy of this check list**
2. **Payment of fee for Plan Checking**
3. **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4. **One (1) copy of project Conditions of Approval**
5. **Include a PDF (electronic submittal) of each required improvement plan at every submittal.**
6. Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).
7. Three (3) sets of Public Street improvement plan with street cross-sections
8. Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)
9. Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)
10. Four (4) sets of Public Sewer improvement plan
11. Five (5) sets of Public Storm Drain improvement plan
12. **Three (3) sets of Public Street Light improvement plan**
13. **Three (3) sets of Signing and Striping improvement plan**
14. **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
15. **Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)**
16. Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.
17. **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**
18. **One (1) copy of Hydrology/Drainage study**
19. **One (1) copy of Soils/Geology report**
20. Payment for Final Map/Parcel Map processing fee



21. Three (3) copies of Final Map/Parcel Map
22. One (1) copy of approved Tentative Map
23. **One (1) copy of Preliminary Title Report (current within 30 days)**
24. **One (1) copy of Traverse Closure Calculations**
25. **One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.**
26. Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
27. **Other:**
 - **Two (2) copies each of Vacation application and Lot Line Adjustment, processing fee, supporting documents, Legal and Plat.**
 - **One (1) Integrated Waste Management Report (IWMR) that complies with the IWMR requirements.**

FILE NO.: PSPA22-003

SUBJECT: An amendment the Ontario Gateway Specific Plan, modifying Table 2.B (Permitted Land Uses by Planning areas) to allow "Auto Sales and Services" as a permitted land use in the Office Planning Area 1 land use district; **City Initiated. City Council action is required.**


PROPERTY OWNER: Prime A Investments LLC

RECOMMENDED ACTION: That the Planning Commission consider and approve the attached resolution recommending that the City Council approve File No. PSPA22-003 pursuant to the facts and reasons contained in the staff report and resolution.

PROJECT SETTING: The Ontario Gateway Specific Plan is comprised of approximately 41.3 acres of land generally located on the south side of Interstate 10, on the east side of Haven Avenue, on the north side of the Union Pacific Railroad right-of-way, and approximately 460 feet west of Ponderosa Avenue, and is depicted in Figure 1: Project



Figure 1: Project Location Map

Case Planner:	Charles Mercier, Principal Planner
Planning Director Approval:	
Submittal Date:	6/30/2022

Hearing Body	Date	Decision	Action
DAB	N/A	N/A	N/A
PC	6/30/2022		Recommend
CC	9/6/2022		Final

Location Map, above. The proposed Specific Plan Amendment will directly affect the Office Planning Area 1 land use district, which is comprised of approximately 7 acres of land generally located at the northwesterly corner of the Specific Plan area.

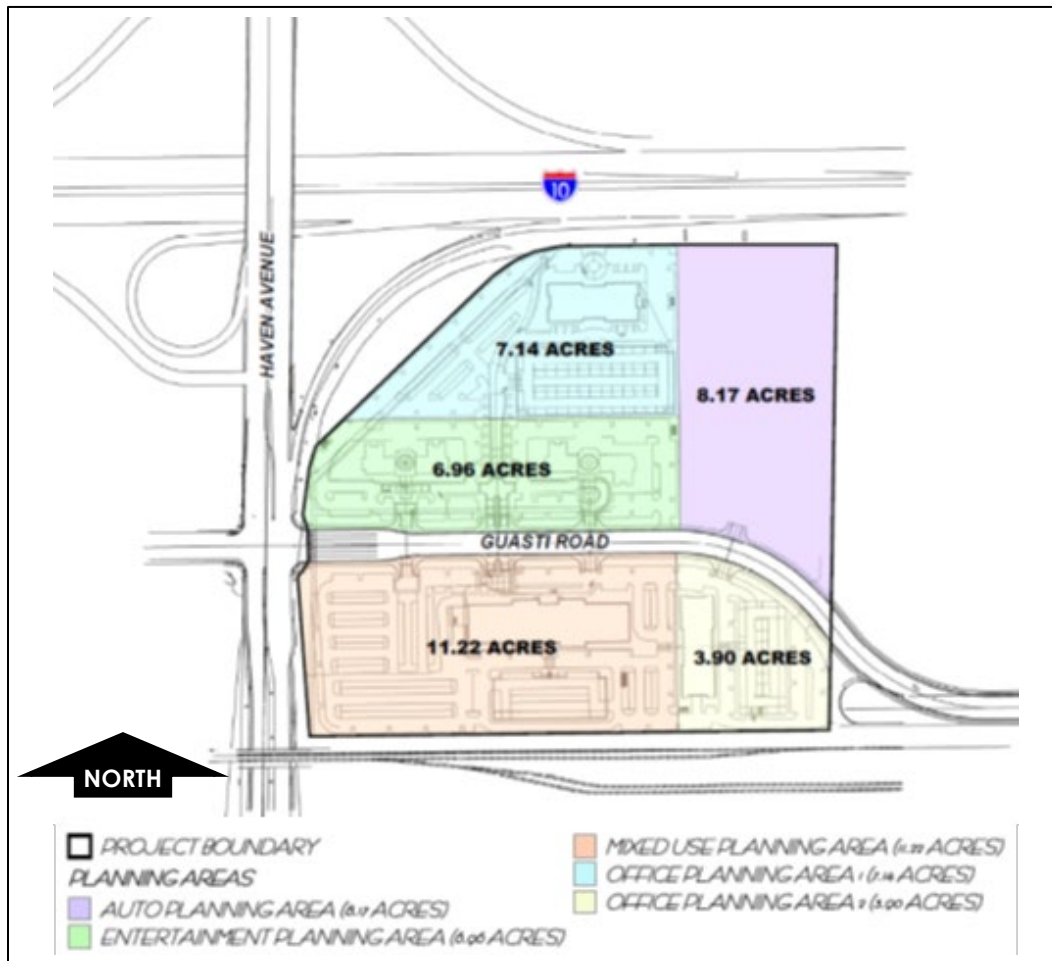


Figure 2: Ontario Gateway Specific Plan Land Use Map

Interstate 10 abuts the Ontario Gateway Specific Plan on the north side. The property to the east is within the IL (Light Industrial) zoning district and is developed with a truck repair facility. The property to the south is within the RC (Rail Corridor) zoning district and is developed with Union Pacific Railroad rail lines. The property to the west is within the Centrelake Specific Plan zoning district and includes vacant land and office land uses. The existing surrounding land uses, zoning, general plan, and specific plan land use designations are summarized in the "Surrounding Zoning & Land Uses" table located in the Technical Appendix of this report.

PROJECT ANALYSIS:

On July 3, 2007, the City Council adopted Ordinance No. 2863 approving File No. PSP05-005, the Ontario Gateway Specific Plan, which establishing the land use designations, development standards, and design guidelines for the Specific Plan area.

The City of Ontario has initiated an Amendment to the Ontario Gateway Specific Plan, revising Table 2.B, Permitted Land Uses by Planning Area. The proposed amendment will allow "Auto Sales and Services" as a Permitted land use within the Office Planning Area 1 land use district.

The Ontario Gateway Specific Plan intends the Office Planning Area 1 land use district for offices up to 10 stories, service-type retail businesses, and a parking structure. The Specific Plan currently prohibits Auto Sales and Services within the Office Planning Area 1 land use district. However, the land use district does permit by right, the establishment of Car Rental Agencies, which includes the short-term rental and long-term leasing of passenger cars, passenger trucks (light duty), and passenger vans. Car Rental Agencies share similar operational aspects with Auto Sales and Services, such as automobile display, storage, and servicing activities. Therefore, Staff believes it's appropriate to include Auto Sales and Services as a permitted land use in the Office Planning Area 1 land use district.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

(1) City Council Goals.

- Invest in the Growth and Evolution of the City's Economy
- Operate in a Businesslike Manner
- Focus Resources in Ontario's Commercial and Residential Neighborhoods

(2) Vision.

Distinctive Development:

- Commercial and Residential Development
 - Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

(3) Governance.

Decision Making:

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.

- G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

(4) Policy Plan (General Plan)

Land Use Element:

- Goal LU2: Compatibility between a wide range of uses.

- LU2-1: *Land Use Decision*. We minimize adverse impacts on adjacent properties when considering land use and zoning requests.

- LU2-5: *Regulation of Uses*. We regulate the location, concentration, and operations of uses that have impacts on surrounding land uses.

Community Economics Element:

- Goal CE3: Decision-making deliberations that incorporate the full short-term and long-term economic and fiscal implications of proposed City Council actions.

- *Fiscal Impact Disclosure*. We require requests for City Council action to disclose the full fiscal impacts, including direct and indirect costs.

HOUSING ELEMENT COMPLIANCE: The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and

overflight impacts of current and future airport activity. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

ENVIRONMENTAL REVIEW: The proposed Development Code Amendment is exempt from the requirements of the California Environmental Quality Act ("CEQA") and the guidelines promulgated thereunder, pursuant to Section 15061(b)(3) of the CEQA Guidelines, in that the activity is covered by the "common sense exemption" (general rule) that CEQA applies only to projects that have the potential for causing a significant effect on the environment. Where it can be seen with certainty that there is no possibility that the activity in question may have a significant effect on the environment, the activity is not subject to CEQA.

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

	<i>Existing Land Use</i>	<i>General Plan Designation</i>	<i>Zoning Designation</i>	<i>Specific Plan Land Use</i>
<i>Site</i>	Vacant	Office Commercial	Ontario Gateway Specific Plan	Office Planning Area 1
<i>North</i>	Interstate 10	Interstate 10	Interstate 10	Interstate 10
<i>South</i>	Union Pacific Railroad Rail Lines	Rail	RC (Rail Corridor)	N/A
<i>East</i>	Truck Repair	Business Park	IL (Light Industrial)	N/A
<i>West</i>	Vacant & Offices	Office Commercial	Centrelake Specific Plan	Commercial/Hotel and Office

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING THE CITY COUNCIL APPROVE FILE NO. PSPA22-003, AN AMENDMENT TO THE ONTARIO GATEWAY SPECIFIC PLAN, MODIFYING TABLE 2.B (PERMITTED LAND USES BY PLANNING AREAS) TO ALLOW "AUTO SALES AND SERVICES" AS A PERMITTED LAND USE IN THE OFFICE PLANNING AREA 1 LAND USE DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF.

WHEREAS, The City of Ontario ("Applicant") has initiated an Application for the approval of a Specific Plan Amendment, File No. PSPA22-003, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to the Ontario Gateway Specific Plan, which is comprised of approximately 41.3 acres of land generally located on the south side of Interstate 10, on the east side of Haven Avenue, on the north side of the Union Pacific Railroad right-of-way, and approximately 460 feet west of Ponderosa Avenue, and for the most part is fully developed with commercial land uses; and

WHEREAS, Interstate 10 abuts the Ontario Gateway Specific Plan on the north side. The property to the east is within the IL (Light Industrial) zoning district and is developed with a truck repair facility. The property to the south is within the RC (Rail Corridor) zoning district and is developed with Union Pacific Railroad rail lines. The property to the west is within the Centrelake Specific Plan zoning district and includes vacant land and office land uses; and

WHEREAS, the proposed Specific Plan Amendment will directly affect the Office Planning Area 1 land use district, which is comprised of approximately 7 acres of land generally located at the northwesterly corner of the Ontario Gateway Specific Plan area; and

WHEREAS, on July 3, 2007, the City Council adopted Ordinance No. 2863 approving File No. PSP05-005, the Ontario Gateway Specific Plan, which establishing the land use designations, development standards, and design guidelines for the Specific Plan area; and

WHEREAS, the City of Ontario has initiated an Amendment to the Ontario Gateway Specific Plan, revising Table 2.B, Permitted Land Uses by Planning Area. The proposed amendment will allow "Auto Sales and Services" as a Permitted land use within the Office Planning Area 1 land use district; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act, commencing with Public Resources Code Section 21000 (hereinafter referred to as "CEQA"); and

WHEREAS, the Project is exempt from the requirements of the California Environmental Quality Act ("CEQA") pursuant to the "common sense exemption" (CEQA Guidelines Section 15061(b)(3)); and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and make recommendation to the City Council on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan ("ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on July 26, 2022, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the recommending authority for the Project, the Planning Commission has reviewed and

considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The administrative record has been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(2) is exempt from the requirements of the California Environmental Quality Act (“CEQA”) and the guidelines promulgated thereunder, pursuant to Section 15061(b)(3) of the CEQA Guidelines, in that the activity is covered by the “common sense exemption” (general rule) that CEQA applies only to projects that have the potential for causing a significant effect on the environment. Where it can be seen with certainty that there is no possibility that the activity in question may have a significant effect on the environment, the activity is not subject to CEQA.; and

(3) The determination of CEQA exemption reflects the independent judgment of the Planning Commission.

SECTION 2: *Housing Element Compliance.* Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the recommending authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 3: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as “ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the recommending authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table

2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 4: Concluding Facts and Reasons. Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 through 3, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed Specific Plan Amendment is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed amendment to the Ontario Gateway Specific Plan will modify Table 2.B (Permitted Land Uses by Planning Areas), to allow Auto Sales and Services as a permitted use within the Office Planning Area 1 land use district. In order to take advantage or remove impediments to achieving our Vision, we require the ability to quickly respond to changing market needs. Policy LU3-3 (*Land Use Flexibility*) of the Policy Plan (general plan) encourages the consideration of uses not typically permitted within a land use category if doing so improves the livability, gathering places and activity nodes.

(2) ***The proposed Specific Plan Amendment thereto, would not be detrimental to the public interest, health, safety, convenience, or general welfare of the City.*** Allowing Auto Sales and Services as a permitted use within the Office Planning Area 1 land use district will be in conformance with Policy Plan Land Use Plan and will comply with the Policy Plan goals and policies applicable to the Specific Plan. The proposed amendment to the Ontario Gateway Specific Plan will not be detrimental to the public interest, health, safety, convenience, or general welfare of the City because it will further the intent of the Ontario Gateway Specific Plan by expanding the list of desirable service-type retail businesses permitted within the Specific Plan area.

(3) ***In the case of an application affecting specific property, the proposed Specific Plan Amendment will not adversely affect the harmonious relationship with adjacent properties and land uses.*** Properties adjacent to the subject Office Planning Area 1 land use district include a new car dealership (Mercedes-Benz of Ontario) that adjoins Office Planning Area 1 on the east side and two hotels adjoin Office Planning Area 1 on the south side. To the north and west of Office Planning Area 1 are Interstate 10 and Haven Avenue, respectively. The addition of Auto Sales and Services as a permitted land use in Office Planning Area 1 will simply allow for a land use type that already exists on a neighboring property and compliments the other existing adjacent land uses.

(4) ***In the case of an application affecting specific property, the subject site is physically suitable, including, but not limited to, parcel size, shape, access, and availability of utilities, for the request and anticipated development.*** The proposed amendment to the Ontario Gateway Specific Plan will modify Table 2.B (Permitted Land Uses by Planning Areas), to allow Auto Sales and Services as a permitted use within the Office Planning Area 1 land use district. This land use district consists of a single parcel of land that is physically suitable accommodate the proposed land use in terms of parcel size (+/- 7 acres in area), shape (more or less rectangular in shape), access (single point of access provided from Guasti Road), and availability of utilities (all wet and dry utilities are available to the Office Planning Area 1 land use district).

SECTION 5: Planning Commission Action. Based upon the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby RECOMMENDS THE CITY COUNCIL APPROVE THE HEREIN DESCRIBED APPLICATION as shown in "Attachment A," and incorporated herein by this reference.

SECTION 6: Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 7: Certification to Adoption. The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26th day of July 2022, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Jim Willoughby
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. PC22-_____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on July 26, 2022, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**Portion of Ontario Gateway Specific Plan
 Table 2.B: Permitted Land Uses by Planning Areas**

TABLE 2.B: PERMITTED LAND USES BY PLANNING AREAS

Uses	Mixed Use Planning Area	Entertainment Planning Area ¹	Office Planning Area 1	Office Planning Area 2 ²	Auto Planning Area
Public Facilities					
<i>Government Office.</i> Activities typically include, but are not limited to, management, administration, or clerical services performed by public, quasi-public, and utility agencies.	P	—	P	—	—
Non-Profit/Service Organizations					
<i>Non-Profit Organization, Charitable, Philanthropic, Service, and other Non-profit Organization Offices.</i> Activities typically include, but are not limited to, facilities for office and group gatherings conducted indoors.	P	—	P	—	—
<i>Campaign Offices</i>	P	—	P	—	—
Transportation					
<i>Limousine/Taxi Service</i>	P	P	P	P	P
Commercial					
<i>Alcohol Beverage Sales.</i> Activities typically include the sale, subject to required license for the sale of alcoholic beverages.	C	C	C	—	—
<i>Auto Sales and Services.</i> Activities include, but are not limited to, the sale of new and used automobiles, automotive and light truck repair; retail sales of goods for automobiles and light trucks; and the cleaning and washing of automobiles and light trucks. Uses typically include, but are not limited to, repair of engines, brakes, electrical, etc., and car washes.	—	—	P	—	P
<i>Car Rental Agency</i>	C	A	P	—	P

FILE NOS.: PGPA20-002 and PADV22-002


SUBJECT: The certification of a Supplemental Environmental Impact Report ("SEIR") (State Clearinghouse No. 2021070364), including the adoption of a Mitigation Monitoring and Reporting Program and a Statement of Overriding Considerations, in conjunction with the following: [1] The Ontario Plan ("TOP") 2050 update General Plan Amendment (File No. PGPA20-002) to the Vision, Governance Manual, and Policy Plan components of TOP, and [2] an update to the Community Climate Action Plan (File No. PADV22-002); **City Initiated. City Council action is required.**

RECOMMENDED ACTION: That the Planning Commission consider and recommend, pursuant to the facts and reasons contained in the staff report and the attached resolutions, that the City Council take the following actions:

- (1) Adopt the Resolution certifying the Supplemental Environmental Impact Report ("SEIR") (State Clearinghouse No. 2021070364), including the adoption of a Mitigation Monitoring and Reporting Program and a Statement of Overriding Considerations; and
- (2) Adopt the resolution approving The Ontario Plan ("TOP") 2050 update General Plan Amendment (File No. PGPA20-002) to the Vision, Governance Manual, and Policy Plan components of TOP; and
- (3) Adopt the resolution approving the update to the Community Climate Action Plan ("CCAP") (File No. PADV22-002).

PROJECT SETTING: The City of Ontario is in the southwestern portion of San Bernardino County and is surrounded by the cities of Chino and Montclair and unincorporated San Bernardino County to the west; the cities of Upland and Rancho Cucamonga to the north; the City of Fontana and unincorporated San Bernardino County to the east; and the cities of Eastvale and Jurupa Valley to the south as depicted in Figure 1: Project Location Map, below.

The City of Ontario is situated in the central part of the Upper Santa Ana River Valley, bounded by the San Gabriel Mountains to the north; the Chino Hills, Puente Hills, and San Jose Hills to the west; the Santa Ana River to the south; and Lytle Creek Wash on the east. The City comprises approximately 50 square miles (32,022 acres), including the 8,200-acre Ontario Ranch in the southern part of the City—formerly known as the New Model Colony ("NMC") and formerly the City's sphere of influence ("SOI"). The northern, more urbanized

Case Planner:	Tom Grahn, Senior Planner
Planning Director Approval:	
Submittal Date:	N/A

Hearing Body	Date	Decision	Action
DAB	N/A	N/A	N/A
PC	07/26/2022		Recommend
CC	08/16/2022		Final

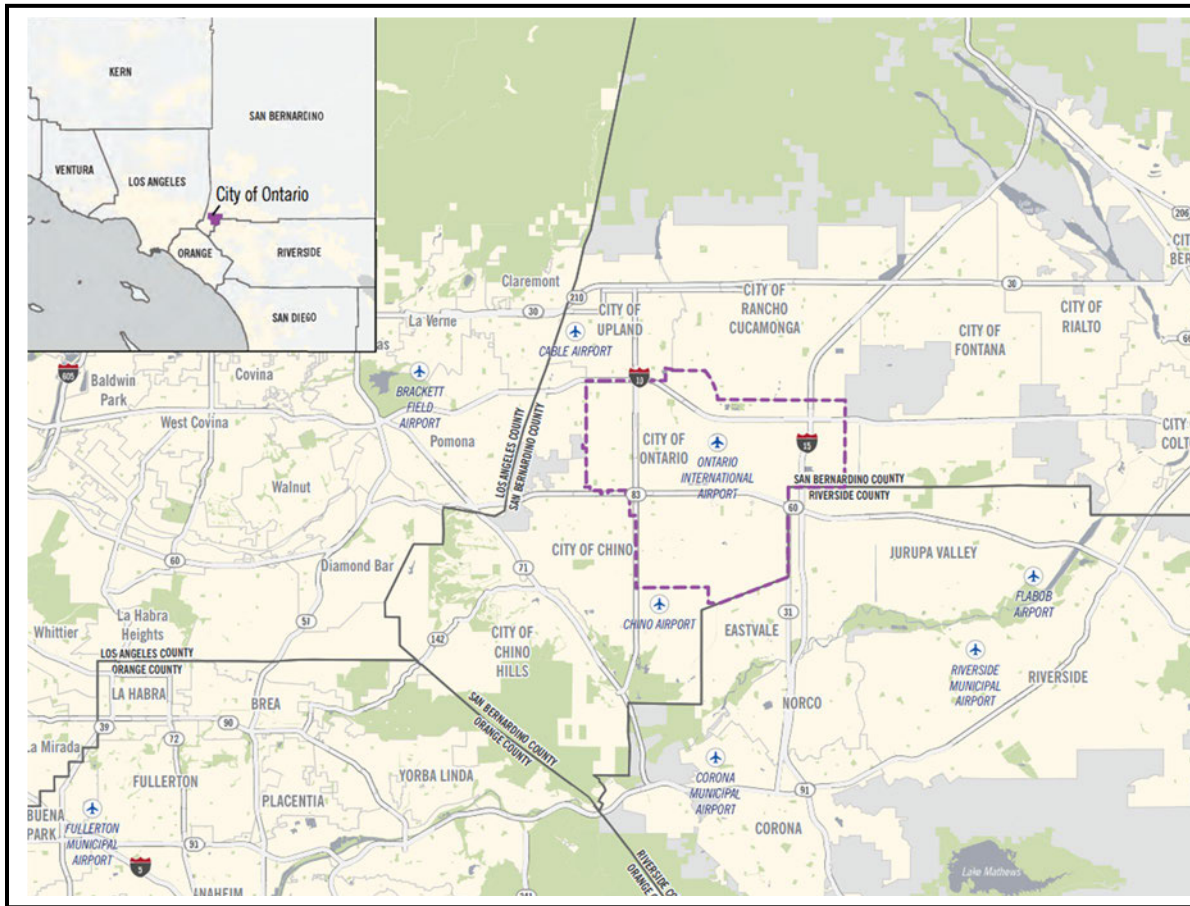


Figure 1: Project Location Map

part of the City is known as the Old Model Colony ("OMC") in reference to the City's founding as a model colony for cities in terms of layout and infrastructure. Generally, the City is bounded by Benson Avenue and Euclid Avenue on the west; Interstate 10 (I-10), 8th Street, and 4th Street on the north; Etiwanda Avenue and Hammer Avenue on the east; and Merrill Avenue and the San Bernardino County/Riverside County boundary on the south. Regional circulation to and through the City is provided by Interstate 10 (I-10) and State Route 60 (SR 60), east to west, and by I-15 and SR 83 (Euclid Avenue), north to south. The City is also home to the Ontario International Airport and proximate to Chino Airport.

PROJECT REVIEW:

(1) Background — The City of Ontario's general plan, The Ontario Plan ("TOP"), was comprehensively updated in 2010. Since 1992, the City has adopted numerous general plan amendments ("GPA"), including the 1998 New Model Colony GPA, GPAs for the 2001, 2010, and 2013 Housing Element updates, and various project related GPAs. The State general plan guidelines recommend general plans be updated every ten years. State law also requires housing elements to be updated every eight years. Since 2010,

significant state and regional issues affecting the City have developed. An updated general plan is needed to address these issues, meet state mandates, and guide the City through the year 2050 and towards a prosperous future.

Although general plans and their basic content are mandated by the state, cities and counties have the option of organizing and expanding their content to meet the needs of their jurisdiction. TOP meets the state mandate of a general plan and much more. In 2020, the City contracted with PlaceWorks to update the general plan and its component parts. PlaceWorks was tasked with preparing a general plan update that will continue to remain relevant through time, function like a business plan, and provide a foundation for City governance. The state-mandated general plan (the Policy Plan) is just one component of The Ontario Plan.

(2) Project Description — The Ontario Plan serves as the framework for the City's business plan and provides a foundation for the City to operate as a municipal corporation. TOP includes six key components: Vision; Governance Manual; Policy Plan; Council Priorities; Tracking and Feedback; and Implementation. The Governance Manual and the Policy Plan are the only components that contain policies. Each individual policy has been crafted to support a goal but may also strengthen other goals and policies within other sections in the policy components. These policies were designed to be internally consistent and used together to guide the City's actions.

Originally adopted in 2010, the Vision still provides a strong foundation for the TOP and only required minor modifications. The Governance Manual received minor refinements to reflect new information and available technologies that could improve municipal operations and decision making.

The TOP 2050 Update focuses on technical updates to the Policy Plan to comply with state housing mandates and conform with new state laws related to community health, environmental justice, climate adaptation, resiliency, and mobility. The majority of updates created through the proposed project weave refinements throughout the existing structure of the Policy Plan.

Three component parts were not revised as part of the TOP 2050 update, including Council Priorities, Implementation, and Tracking and Feedback. The Council Priorities are updated concurrently with the annual budgeting process. The Implementation Plan will be updated to reflect changes to the Policy Plan. The Tracking and Feedback component will be advanced and formalized through additional coordination and software improvements.

TOP components include the following:

- The **Vision** describes the future community of Ontario. Its basic purpose is to state a clear and comprehensive picture of where the City wants to be in 2050. It is the rationale and motivation for everything the City does. The four Points of Vision

include: Dynamic Balance, Prosperous Economy, Distinctive Development, and Recognized Leadership.

- The **Governance Manual** describes the foundation for conducting the public's business on behalf of the present and future people of Ontario. It explains how TOP is a tool for decision-making and communication. The Governance Manual is one of TOP's two policy components. It contains five sections: Decision-Making, Communication, Implementation, External Factors, and Municipal Finance.
- The **Policy Plan** connects intent with action through a broad range of goals and policies that will guide the long-term growth and development required for the City to achieve its Vision. It also satisfies the California Government Code requirement for a general plan. The Land Use Plan includes land use designations to guide and regulate land use patterns, distributions, densities, and intensities in the City, including residential, employment, retail, recreation, and public uses. The Policy Plan is one of two policy components of TOP and includes nine elements which are discussed below in the Policy Plan section.
- **Council Priorities** define the short-term direction in City actions and initiatives. They are the primary means for exercising leadership in carrying out the TOP and realizing the Vision through the adoption of the annual Budget.
- The **Implementation** component consists of short and mid-term actions taken to carry out TOP policies. These City actions include day to day activities, Capital Improvement Plan, master plans and initiatives.
- The **Tracking and Feedback** component allows the City to learn from experience and redirect efforts, as needed. Tracking and Feedback will inform decision makers about progress toward the Vision, as well as track progress toward the Governance Manual, Council Priorities, Policy Plan, and Implementation components on a regular basis. Feedback will help inform the public about how the City is doing toward achieving TOP goals.

(3) Public Participation — The Ontario Plan has been influenced by several fact-gathering processes, used to gain information and insight from a broad range of persons, professions, and the public. The fact-gathering and public participation process was broad and inclusive and included the following elements:

- June 14, 2021 — Property Owner Open House — Staff hosted a virtual open house to provide an overview of the TOP 2050 Project and land use designation changes on specific properties as part of the City's housing strategy to fulfill the 6th Regional Housing Needs Assessment (RHNA) Cycle site inventory obligation that are part of the Housing Element update.

- June 24, 2021 — Community Health Improvement Association — Staff hosted a virtual workshop to present the TOP 2050 update and to receive comments, and answer questions from the public.
- June 28, 2021 — Community Workshop #1 — Staff hosted a virtual workshop to present the TOP 2050 update, including the City's housing strategy to receive comments, and answer questions from the public.
- July 2, 2021 — In compliance with Senate Bill 18 and Assembly Bill 52 notification requirements (Tribal Consultation), the City mailed a separate project notice to 16 local tribes identified by the Native American Heritage Commission (NAHC). No consultation requests were received during the requisite 90-day response period.
- July 21, 2021 to September 30, 2021 — Environmental Justice Survey — City staff deployed an Environmental Justice Survey to hear about environmental justice issues affecting the community. The survey was accessible by scanning a QR code utilizing a smartphone via the Ontario Plan website, social media, posters displayed at all the city community centers, and City iPads at community events.
- July 29, 2021 — Community Workshop #2 — Staff hosted a virtual workshop to facilitate a guided discussion of the environmental justice and survey, community health, and resiliency policies, and share the City's preferred land use plan and housing strategy. The intent of Community Workshop #2 was to further inform the community of policy updates and gather feedback regarding the preferred land use plan. This workshop also included the Community Health Improvement Association meeting group.
- August 25, 2021 and August 26, 2021 — Concerts in the Park — Staff hosted a booth at the public events to provide an overview of the TOP 2050 Project. Visitors were encouraged to participate in an on-line survey regarding the TOP 2050 update.
- March 19, 2022 — Taste of Ontario — Staff hosted a booth at the public event and provided an overview of the TOP 2050 project, snacks, and interactive game.
- March 31, 2022, April 28, 2022, May 26, 2022, and June 30, 2022 — Open House/Office Hours with City Staff — Staff hosted a series of Open House/Office Hours available to the public. Topics presented include environmental justice, climate adaptation and resiliency, mobility, and economic considerations. Public questions include land use, industrial development, farming, and the availability of water.
- May 9, 2022 — The Notice of Availability ("NOA") for the draft Supplemental Environmental Impact Report ("SEIR") for The Ontario Plan 2050 (State Clearing House No. 2021070364) was posted to the City website. The NOA indicated that

the SEIR was available for review and that the City would be accepting comments through June 23, 2022.

- June 6, 2022, June 15, 2022, June 16, 2022 — Teen Action Committee ("TAC") — Staff hosted the TAC and provided an overview of TOP 2050 Project and climate adaptation and resiliency.

TOP 2050 UPDATE:

(1) The Policy Plan Background — The Policy Plan and its elements include an introduction and purpose section to frame the component's context and intent. Consistent with the intent for TOP to be a living document with long term relevance, the Policy Plan and its nine elements contain high-level guiding principles which proclaim the fundamental concepts critical to achieving the Vision. Each element includes sections with an introduction, a goal and policies, all of which sustain the Vision. The Policy Plan elements include Land Use, Housing, Parks and Recreation, Environmental Resources, Community Economics, Safety, Mobility, Community Design, and Social Resources.

The Policy Plan stands alone but is strengthened by being a core component of the larger framework of TOP. The Policy Plan was designed to maintain relevance over time; therefore, it is principle-based and relies on implementation plans for details and actions.

The Policy Plan's nine elements have been organized topically to best meet the City's needs. Concepts that are critical to the Vision, like economic prosperity, complete community, sustainability, and environmental justice are woven throughout the nine elements. Integrating key concepts into each element reinforces them throughout the document. The seven state mandatory elements are covered in one or more of the Policy Plan Elements. For instance, Noise is addressed in both Land Use and Safety. Open Space is addressed in Land Use, Parks and Recreation, Environmental Resources and Mobility.

State Mandated Elements	
Element	Policy Plan (General Plan)
Land Use	Land Use
Circulation	Mobility
Housing	Housing
Open Space	Land Use; Parks and Recreation; Environmental Resources; Mobility
Conservation	Environmental Resources
Noise	Safety; Land Use
Safety	Safety; Land Use; Adaptation & Resiliency

Optional Elements (Non-Mandated)	
Element	Policy Plan (General Plan)
The state does not require or limit the adoption of optional elements.	Community Design
	Community Economics
	Social Resources
	Environmental Justice (Interwoven among elements)

ANALYSIS OF POLICY PLAN ELEMENTS:

(1) Land Use Element — The Land Use Element (Sections LU-1 to LU-5) guides and regulates land use patterns, densities, and intensities throughout the City, including residential, employment, retail, recreation, and public uses. It includes five sections: Balance, Compatibility, Flexibility, Phased Growth, and Airport Planning. This element provides for uses and development that add value to the community, in terms of function, design and fiscal return. The Land Use Plan was designed to provide certainty, flexibility, and accommodate the evolution of development types. The Land Use Element is a key to the development of a physically diverse and complete community.

The Policy Plan Preferred Land Use Plan, Figure LU-01, establishes twenty land use designations within five major categories: Residential, Mixed Use, Retail/Service, Employment, and Other. In addition, three Overlay Zones are also identified.

The Map does not assign a public designation to all publicly owned facilities. Only permanent existing public facilities, such as City Hall, Library, and existing schools are designated as Public Facility or Public School. Future and non-permanent public facilities, such as future schools, are designated with the appropriate underlying land use. As schools and other permanent public facilities are developed, those sites will be noted on the Map accordingly with an amendment to the Policy Plan Map.

Office uses can be found within the Mixed Use, Retail/Service and Employment categories. The Mixed-Use designation provides for a mix of residential, office, and retail uses.

The land use designations in the City will remain as designated under the current General Plan, with adjustments made to the projected land use mix and boundaries of the mixed-use areas. The Buildout Statistical Summary table provides a statistical summary of the buildout potential associated with the TOP 2050 Policy Plan compared to existing conditions and compared to the buildout potential under the currently approved TOP ("approved project" or "approved TOP").

Buildout Statistical Summary				
Scenario	Units	Population	Nonresidential Square Feet	Employment
Existing Conditions	52,466	179,597	156,065,382	131,999
Approved TOP	104,163	357,957	260,399,271	313,067
Proposed TOP	129,562	410,492	261,491,779	296,002
Net Difference (Proposed TOP - Approved TOP)	25,399	52,535	1,092,508	-17,065

(2) Housing Element — The Housing Element (Sections H-1 to H-5 and the Housing Element Technical Report) provides direction for the production, preservation, and improvement of housing in Ontario. The Housing Element includes five sections: Neighborhoods & Housing, Housing Supply & Diversity, Governmental Regulation, Housing Assistance, and Special Needs. The Housing Element Technical Report assesses housing needs through demographic trends and projections.

The Housing Element is one of the seven mandated elements of the general plan, which each city and county in the State is required to periodically adopt (generally every eight years). The Housing Element is required to adequately plan to meet the existing and projected housing needs of all economic segments of the community. The Housing Element was prepared to address the 6th Cycle RHNA period from October 15, 2021, through October 15, 2029, and it was also updated to comply with recently adopted housing element law. The Housing Element is the only element of the general plan reviewed and certified by the State to ensure compliance with State law.

The Housing Element adoption was separated from the balance of The Ontario Plan 2050 Update and was recommended for approval by the Planning Commission on December 21, 2021 and was approved by the City Council on March 1, 2022. The Housing Element was submitted to the California Department of Housing and Community Development ("HCD") for review in November 2021, and March 2022. On both occasions, HCD requested revision and additional analysis of the document.

The Housing Element is the only element that includes implementation as part of the Policy Plan. An adequate supply of housing is a key component of a complete and economically sustainable community. The City recognizes that economic growth is dependent on growing jobs and providing housing for our workforce. As such, central to the Housing Element's goals and policies is the concept of matching the type and affordability of housing with the needs of the City's current and future workforce. The number of housing units projected beyond the 2021-2029 Housing Element planning period through to 2050 (buildout of the Land Use Plan) are shown below.

The City's RHNA allocation is defined in the Housing Element as Quantified Objectives. A summary of the housing implementation strategies that will be used, as well as the Quantified Objectives, are provided below:

City of Ontario 6th Cycle RHNA Allocation by Income Category	
Income Category (Percent of Annual Mean Income)	RHNA Allocation
Very Low (31 to 50)	5,640
Low (51 to 80)	3,286
Moderate (81 to 120)	3,329
Above Moderate (121 or more)	8,599
Total	20,854

Source: SCAG 2021

Implementation Program Summary
<ul style="list-style-type: none"> • Neighborhoods and Housing • Housing Supply and Diversity • Governmental Constraints • Housing Assistance • Special Needs Housing

Consistent with state law, the Housing Element includes a parcel-level analysis of potential housing sites within the City. Timing of the housing development within these sites is dependent on residential real estate and financial credit conditions, as well as site conditions such as lot size and configuration, existing buildings, and adequacy of infrastructure. Larger, mostly vacant and underutilized sites with adequate infrastructure are more likely to develop first.

(3) Parks and Recreation Element — The Parks and Recreation Element (Sections PR-1 to PR-2) includes an analysis of existing park facilities and examines opportunities for new park spaces to accommodate the current and future needs. It also guides the development of recreational corridors that connect local and regional recreational resources and open space. This element includes two sections: Planning & Design, and Programming.

(4) Environmental Resources Element — The Environmental Resources Element (Sections ER-1 to ER-5) guides management of environmental resources and infrastructure and provides policies that support systems integration, resource conservation and regeneration, energy independence, and environmental justice. The Environmental Resources Element includes five sections: Water & Wastewater, Solid Waste & Recycling, Energy, Air Quality, and Biological, Mineral & Agricultural Resources.

This element includes goals and policies to reduce global warming gas emissions, conserve water, and minimize the community's waste stream.

(5) Community Economics Element — The Community Economics Element (Sections CE-1 to CE-3) establishes goals and policies for attracting private investment, increasing the quantity and quality of jobs, expanding and diversifying the City's revenue sources, and guiding the City's financial decisions. Central themes to this element include "*create, maintain and grow economic value*" and the vision of Ontario as a "*complete community*". This element includes three sections: Complete Community, Placemaking, and Fiscal Decision-Making. This element is considered optional but critical to the vision of a prosperous economy.

(6) Safety Element — The Safety Element (Sections S-1 to S-9) provides policies to minimize exposure to man-made and natural disasters and their impacts on life, property, and commerce. It addresses geology and seismicity, flooding, hazardous materials, noise, fire hazards and prevention, emergency preparedness, crime prevention, and adaptation and resiliency. This element includes nine sections: Seismic & Geological Hazards, Flood Hazards, Fire & Rescue Hazards, Noise Hazards, Wind-Related Hazards, Hazardous Materials & Waste, Law Enforcement, Emergency Management, and Energy Resiliency.

(7) Mobility Element — The Mobility Element (Sections M-1 to M-5) guides the design and improvement of our multifaceted transportation system to meet the current and future needs of our residents, businesses, and the region. This element includes an evaluation of forecasts of future traffic levels and includes a variety of travel modes, including automobile, transit, rail, air, and non-motorized forms of travel. The Mobility Element includes mobility policies that will accommodate Ontario's Vision and projected growth. The Element includes five sections: Roadway System, Active Transportation, Public Transit, Goods Movement, and Regional Transportation. This Element provides policies to ensure mobility options are integrated into land use planning, in terms aesthetically pleasing and functional roadways, transportation facilities, linkages, and nodes.

(8) Community Design — The Community Design Element (Sections CD-1 to CD-5) establishes the goals and policies that will create a built environment that fosters the enjoyment, financial benefit, and well-being of the entire community and protect the built environment as an investment. The Community Design Element reflects the City's current high-quality design expectations and process. The Element includes five sections: Image and Identity, Design Quality, Urban, Mixed Use, and Transit-Oriented Place Types, Historic Preservation, and Protection of Investment. This element is a key to the functional and aesthetic future of the City and primarily supports the Land Use, Mobility, Safety, and Community Economics Elements. This element also includes eight Place Types Categories for areas that are geographically large, culturally unique, and regionally significant Districts that include: Downtown District, Airport/Metro Entertainment District, Guasti

District, South Euclid District, South Employment District, East Holt Boulevard Corridor, Inland Empire Boulevard Corridor, and Neighborhood Center Infill (a-j).

(9) Social Resources Element — The Social Resources Element (Section SR-1 to SR-5) provides goals and policies for the health and well-being of the community, addressing the City's role in such issues as health care, education, and community services for the full spectrum of our population. This element includes five sections: Health, Education, Community & Leisure Activities, Library, and Entertainment & Culture. Although this is an optional element, its goals and policies are necessary to fulfill the Vision of a prosperous economy through enrichment of community education, health, culture, and the arts.

COMMUNITY CLIMATE ACTION PLAN (CCAP) UPDATE:

(1) Background — On December 16, 2014, the City of Ontario adopted its Community Climate Action Plan ("CCAP"), which was prepared in response to Assembly Bill 32 ("AB 32") (2006), the California Global Warming Solutions Act. AB 32 required the California Air Resources Board ("CARB") to adopt State-wide regulations to achieve the maximum technologically feasible and cost-effective greenhouse gas ("GHG") emission reduction measures to help mitigate risks associated with climate change, improve energy efficiency, expand the use of renewable resources and cleaner transportation, and reduce waste. These regulations, in the form of the AB 32 Scoping Plan adopted by CARB, laid out California's plan for achieving the required GHG reductions. The Scoping Plan identifies the measures the State will undertake, and the expected GHG reductions associated with these measures, but the State does not have direct jurisdictional control over many of the activities that product GHG emissions within the state, so the AB 32 Scoping Plan identified local governments' responsibilities for achieving the State GHG reduction goals. To accomplish the identified GHG reduction measures the City adopted its CCAP.

The CCAP was designed to comply with the direction of AB 32 and associated implementation programs, while attempting to distribute the GHG reduction strategies evenly across the full spectrum of GHG generating activities. It was a comprehensive approach to all sectors of the community to participate and implement feasible strategies to reduce GHG emissions generated within the City consistent with statewide GHG emission reduction efforts. Where possible, it encouraged the implementation of voluntary programs and incentives to achieve the City's GHG reduction goals.

The CCAP consists of 1) an inventory of GHG emissions that identifies and quantifies existing emissions and projected future emissions, 2) a reduction target to reduce existing GHG emissions to 30 percent below business as usual (BAU) year 2020 projected levels, 3) policies and measures to reduce existing emissions to meet the reduction target consistent with the adopted TOP EIR, and 4) streamlined environmental review – with respect to GHG emissions – for development projects that are consistent with the CCAP.

The CCAP was intended to be a dynamic program, in that it required implementation, monitoring, evaluation, and adaptation. Its emission reduction measures were organized by the following:

- State and regional measures include adopted, implemented, and proposed state and regional measures that do not require additional City action and that will result in quantifiable GHG emission reductions in the inventory.
- Local measures include community wide quantifiable measures that would further reduce GHG emissions for the City.
- Performance standards for new development, providing a streamlined and flexible program for new residential and nonresidential projects to reduce their emissions.

The implementation of the 2014 CCAP resulted in a per capita GHG emissions steady decline from 2008 levels. Per capita emissions in Ontario decreased from approximately 8.25 MTCO_{2e} in 2008 to approximately 5.55 MTCO_{2e} per person in 2019, a decrease of 33 percent. Overall sectors showed an emissions decrease with the following four sectors experienced the largest decreases in annual GHG emissions between 2008 and 2019 including land use and sequestration, agriculture, water and wastewater, and nonresidential energy, which reduced emissions by 77 percent, 75 percent, 56 percent, and 54 percent, respectively.

(2) Community Climate Action Plan Update — The 2022 CCAP is the City's strategic plan to reduce GHG emissions and foster a sustainable community through 2050 and beyond. TOP directs the preparation, ongoing implementation, and update of the CCAP, providing the framework for the City to reduce its community wide GHG emissions in a manner consistent with the adopted state reduction target for 2030 and the longer-term goals for 2045 and 2050. The CCAP allows for City decision-makers, staff, and the community to understand the sources and magnitude of local GHG emissions, reduce GHG emissions, and prioritize steps to achieve GHG emission-reduced targets.

The 2022 CCAP is consistent with CEQA Guidelines for the reduction of GHG emissions. This allows the CCAP to support and streamline environmental review of GHG emissions for future development projects and serves as the programmatic tiering document for review of the climate change impacts of projects under CEQA and is included in the SEIR for the TOP 2050 Update.

The 2022 CCAP is an update of the 2014 CCAP, providing updated analyses and background information, an expanded set of GHG reduction strategies, and a planning horizon out to 2050. It contains an inventory of the community's GHG emissions from the transportation, energy, waste, off-road equipment, agriculture, water and wastewater sectors, land use sequestration, stationary sources, and goals strategies and actions to

address climate change adaptation and GHG emissions. The CCAP also presents a work plan for the City to implement strategies and track progress over time.

In conjunction with existing and planned local and state programs, these updated strategies provide a flexible path to reduce the community's GHG emissions to meet the City's 2030 and 2050 emission targets and to support the State's adopted target for 2030 and its goal to achieve carbon neutrality by 2045. The City's GHG reduction targets are to reduce emissions to a level below 6.0 MTCO_{2e} per capita by 2030 and 2.0 MTCO_{2e} per capita by 2050, as shown below.

Per Capita GHG Emissions Targets		
Target	2030 Per Capita	2050 Per Capita
State	6.0 MTCO _{2e} per person	2.0 MTCO _{2e} per person
City Strategies with All Reductions	3.52 MTCO_{2e} per person	1.31 MTCO_{2e} per person

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670, et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan.

(1) Ontario International Airport — On April 19, 2011, the City of Ontario adopted the Ontario International Airport Land Use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport ("ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed Project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ONT ALUCP.

(2) Chino Airport — On July 19, 2022, the City of Ontario adopted the Chino Airport Land Use Compatibility Plan (ALUCP), establishing the Airport Influence Area for Chino Airport ("CNO"), which encompasses lands within the City of Ontario, and addresses four compatibility factors, which includes safety, noise, airspace protection, and overflight impacts. A portion of the proposed Project is located within the Airport Influence Area of Chino Airport and was evaluated and found to be consistent with the policies and criteria of the CNO ALUCP.

ENVIRONMENTAL REVIEW: A Draft Supplemental Environmental Impact Report ("SEIR") has been prepared for The Ontario Plan and addressed the following areas that may have a potentially significant impact on the environment between the adopted Plan and the proposed Project (2050 Update). These areas included: Aesthetics, Agriculture and Forestry Resources, Air Quality, Biological Resources, Cultural Resources, Energy,

Geology and Soils, Greenhouse Gas Emissions, Hazards and Hazardous Materials, Hydrology and Water Quality, Land Use and Planning, Mineral Resources, Noise, Population and Housing, Public Services, Recreation, Transportation, Tribal Cultural Resources, Utilities and Service Systems, and Wildfire. Through the EIR process these potential impacts were analyzed, revisions were incorporated into the plan and/or mitigation measures were identified that reduced the potential environmental impacts to a level that was less than significant. Of the 20 areas analyzed by the SEIR, all but four of the impact areas were mitigated to a level of less than significant. The four remaining impact areas, even with the mitigation measures, could not be reduced to less than significant and the impacts would remain significant, unavoidable, and adverse after mitigation measures are applied. These areas include:

- Air Quality — Buildout of The Ontario Plan will cumulatively contribute to the nonattainment designations of the South Coast Air Basin ("SoCAB"), create short-term emissions from construction activities that exceed South Coast Air Quality Management District's (AQMD) regional significance thresholds and cumulatively contribute to the nonattainment designations, generate long-term emissions that would exceed SCAQMD's regional significance thresholds and cumulatively contribute to the SoCAB nonattainment designations, and could expose sensitive receptors to substantial concentrations of toxic air contaminants ("TAC");
- Cultural Resources — Although protective regulations are in place and preservation policies are included in The Ontario Plan, implementation of the proposed Land Use Plan has the potential to impact Tier III historic resources. Major modification or demolition of Tier III resources may be appropriate under certain circumstances;
- Noise — Buildout of the Proposed Land Use Plan will result in impacts related to the proximity of construction activities to sensitive users, construction vibration impacts from individual development projects, and from excessive airport-related noise related impacts from traffic on local roadways and construction activities in the City. These activities will substantially increase noise levels and affect noise-sensitive uses; and
- Transportation — Total vehicle miles traveled ("VMT") would increase under the Proposed Land Use Plan due to increased residential development. Future development projects will need to consider transportation demand management ("TDM") measures consistent with the Mobility Element.

The City of Ontario circulated the Draft SEIR on May 9, 2022, through June 23, 2022, for public review. At the end of the public review period 43 comments were received. Areas of concern include loss of agricultural land, environmental impacts associated with warehouse development, and request for a warehouse moratorium. Refer to the Final Supplemental Environmental Impact Report, Section 2. Response to Comments.

While these impacts will be significant and unavoidable, the Staff believes that the benefits of The Ontario Plan outweigh the potential unavoidable, adverse impacts of the plan. As a result, Staff recommends that the Planning Commission forward a recommendation to the City Council to adopt a Statement of Overriding Considerations for those impacts that could not be fully mitigated to a level of less than significant.

Furthermore, to ensure that the mitigation measures are implemented, a Mitigation Monitoring and Reporting Program has been prepared for the Project pursuant to CEQA Guidelines Section 15097, which specifies responsible agencies/departments, monitoring frequency, timing and method of verification and possible sanctions for non-compliance with mitigation measures. The environmental documentation for this project is available for public review at the Planning Department public counter.

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING THE CITY COUNCIL CERTIFY THE PROGRAM SUPPLEMENTAL ENVIRONMENTAL IMPACT REPORT (STATE CLEARINGHOUSE NO. 2021070364) FOR THE ONTARIO PLAN 2050 TECHNICAL UPDATE FOR FILE NO. PGPA20-002 PART A AND ADOPT ENVIRONMENTAL FINDINGS PURSUANT TO THE CALIFORNIA ENVIRONMENTAL QUALITY ACT, A STATEMENT OF OVERRIDING CONSIDERATIONS AND A MITIGATION MONITORING AND REPORTING PROGRAM.

WHEREAS, the City of Ontario Certified the Environmental Impact Report prepared for The Ontario Plan and associated Statement of Overriding Considerations, and issued Resolution No. 2010-003; and

WHEREAS, the City of Ontario adopted the Policy Plan (General Plan) as part of the component framework for The Ontario Plan on January 27, 2010, and issued Resolution No. 2010-004; and

WHEREAS, the Supplemental Environmental Impact Report ("SEIR"), for the General Plan Amendment for The Ontario Plan 2050 Technical Update consists of the Draft SEIR and the comments and responses to comments made on the Draft SEIR; and

WHEREAS, the SEIR Statement of Overriding Considerations, and Mitigation Monitoring and Reporting Program, File No. PGPA20-002, for The Ontario Plan 2050 Technical Update, have been prepared in accordance with the California Environmental Quality Act ("CEQA"), the State CEQA Guidelines, and the City of Ontario Guidelines for implementation of CEQA; and

WHEREAS, the SEIR consists of the Draft SEIR and the comments and responses to comments made on the Draft SEIR; and

WHEREAS, the SEIR for the General Plan Amendment for The Ontario Plan was circulated for a 45-day public review period and a notice of its availability was published in a local newspaper, submitted to the state clearinghouse, and posted in the Office of the Clerk of the Board of Supervisors of San Bernardino County; and

WHEREAS, copies of the SEIR were distributed to the Planning Commission, City departments, and federal, state, regional, local, and other agencies and individuals; and

WHEREAS, the SEIR for the General Plan Amendment for The Ontario Plan has been prepared to address the environmental effects of The Ontario Plan 2050 Technical Update which establishes a Vision, Plan Governance Manual, Policy Plan, City Council Priorities, Implementation Plan and Tracking and Feedback for the City of Ontario; and

WHEREAS, on July 26, 2022, the Planning Commission of the City of Ontario conducted a duly noticed public hearing on the SEIR at which time all persons wishing to testify were heard and the SEIR was fully studied; and

WHEREAS, all other legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the recommending body for the Project, the Planning Commission has reviewed and considered the information contained in the Supplemental Environmental Impact Report (SEIR) and supporting documentation. Based upon the facts and information contained in the SEIR (SCH# 2021070364) and supporting documentation, the Planning Commission recommends that the City Council make the following findings:

- (1) The SEIR contains a complete and accurate reporting of the environmental impacts associated with the Project; and
- (2) The SEIR was completed in compliance with CEQA and the Guidelines promulgated thereunder; and
- (3) The SEIR reflects the independent judgment of the Planning Commission.

SECTION 2: Planning Commission Action. Based on the entire record before the Planning Commission and all written and oral evidence presented, the Planning Commission recommends the City Council finds the SEIR complies with CEQA for the following reasons:

- (a) The SEIR analyzes the environmental effects of the General Plan Amendment and finds that, if The Ontario Plan 2050 Technical Update is adopted as proposed by this plan, and with the implementation of proposed mitigation measures, the following impacts will still be significant and unavoidable: air quality, cultural resources (historic), noise (mobile source, construction-related noise, construction-related vibration, noise compatibility), and transportation (increase in vehicle miles traveled); and
- (b) The Planning Commission further finds that the Findings of Facts and Statement of Overriding Considerations, demonstrates that The Ontario Plan's benefits outweigh its significant and unavoidable impacts, and therefore the City would be justified in approving The Ontario Plan notwithstanding said significant and unavoidable impacts.

SECTION 3: Recommendation. Based upon the findings and conclusions set forth in Sections 1 and 2 above, the Planning Commission hereby recommends that the City Council certify the SEIR, adopt a Statement of Overriding Considerations, and that the associated Mitigation Monitoring and Reporting Program also be approved by the City Council.

SECTION 4: Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 5: Certification to Adoption. The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26th day of July 2022, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Jim Willoughby
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____, was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on July 26, 2022, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

***PROGRAM SUPPLEMENTAL ENVIRONMENTAL IMPACT
REPORT (STATE CLEARINGHOUSE NO. 2021070364)***

A Component of The Ontario Plan 2050

July 2022 | Final Supplemental Environmental Impact Report
State Clearinghouse No. 2021070364

THE ONTARIO PLAN 2050

City of Ontario

Prepared for:

City of Ontario

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Table of Contents

Section	Page
1. INTRODUCTION.....	1-1
1.1 INTRODUCTION.....	1-1
1.2 FORMAT OF THE FEIR.....	1-1
1.3 CEQA REQUIREMENTS REGARDING COMMENTS AND RESPONSES.....	1-2
2. RESPONSE TO COMMENTS.....	2-1
2.1 INTRODUCTION.....	2-1
2.2 SUMMARY OF GENERAL RESPONSES.....	2-2
2.2.1 Scenic Vistas.....	2-2
2.2.2 Nonrenewal of Williamson Act Contracts.....	2-3
2.2.3 Environmental Justice Policies.....	2-3
2.2.4 Air Quality Impacts.....	2-6
2.3 AGENCY AND TRIBAL COMMENTS.....	2-7
2.4 RESIDENT AND ORGANIZATION COMMENTS.....	2-23
3. REVISIONS TO THE DRAFT EIR.....	3-1
3.1 INTRODUCTION.....	3-1
3.2 DRAFT SEIR REVISIONS IN RESPONSE TO WRITTEN COMMENTS.....	3-1

APPENDICES

- Appendix A. Revised Draft SEIR Figures
- Appendix B. Environmental Justice Goals, Policies, and Actions

Table of Contents

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1. Introduction

1.1 INTRODUCTION

This Final Environmental Impact Report (FEIR) has been prepared in accordance with the California Environmental Quality Act (CEQA) as amended (Public Resources Code Section 21000 et seq.) and CEQA Guidelines (California Code of Regulations Section 15000 et seq.).

According to the CEQA Guidelines, Section 15132, the FEIR shall consist of:

- (a) The Draft Environmental Impact Report (Draft EIR) or a revision of the Draft;
- (b) Comments and recommendations received on the Draft EIR either verbatim or in summary;
- (c) A list of persons, organizations, and public agencies comments on the Draft EIR;
- (d) The responses of the Lead Agency to significant environmental points raised in the review and consultation process; and
- (e) Any other information added by the Lead Agency.

This document contains responses to comments received on the Draft Supplemental Environmental Impact Report (SEIR) for The Ontario Plan 2050 (TOP 2050) during the public review period, which began on Monday, May 9, 2022, and closed Thursday, June 23, 2022. This document has been prepared in accordance with CEQA and the CEQA Guidelines and represents the independent judgment of the Lead Agency. This document and the circulated Draft SEIR comprise the FEIR, in accordance with CEQA Guidelines, Section 15132.

1.2 FORMAT OF THE FEIR

This document is organized as follows:

Section 1, Introduction. This section describes CEQA requirements and content of this FEIR.

Section 2, Response to Comments. This section provides a list of agencies and interested persons commenting on the Draft SEIR; copies of comment letters received during the public review period, and individual responses to written comments. To facilitate review of the responses, each comment letter has been reproduced and assigned a number (A-1 through A-2 for letters received from agencies and Tribes, and R-1 through R-43 for letters received from residents and organizations). Individual comments have been numbered for each letter and the letter is followed by responses with references to the corresponding comment number.

1. Introduction

Section 3. Revisions to the Draft EIR. This section contains revisions to the Draft SEIR text and figures as a result of the comments received by agencies and interested persons as described in Section 2, and/or errors and omissions discovered subsequent to release of the Draft SEIR for public review.

The responses to comments contain material and revisions that will be added to the text of the FEIR. The City of Ontario has reviewed this material and determined that none of this material constitutes the type of significant new information that requires recirculation of the Draft SEIR for further public comment under CEQA Guidelines Section 15088.5. None of this new material indicates that the project will result in a significant new environmental impact not previously disclosed in the Draft SEIR. Additionally, none of this material indicates that there would be a substantial increase in the severity of a previously identified environmental impact that will not be mitigated, or that there would be any of the other circumstances requiring recirculation described in Section 15088.5.

1.3 CEQA REQUIREMENTS REGARDING COMMENTS AND RESPONSES

CEQA Guidelines Section 15204 (a) outlines parameters for submitting comments and reminds persons and public agencies that the focus of review and comment of Draft SEIRs should be “on the sufficiency of the document in identifying and analyzing possible impacts on the environment and ways in which significant effects of the project might be avoided or mitigated. Comments are most helpful when they suggest additional specific alternatives or mitigation measures that would provide better ways to avoid or mitigate the significant environmental effects. At the same time, reviewers should be aware that the adequacy of an EIR is determined in terms of what is reasonably feasible. ...CEQA does not require a lead agency to conduct every test or perform all research, study, and experimentation recommended or demanded by commenters. When responding to comments, lead agencies need only respond to significant environmental issues and do not need to provide all information requested by reviewers, as long as a good faith effort at full disclosure is made in the EIR.”

CEQA Guidelines Section 15204 (c) further advises, “Reviewers should explain the basis for their comments, and should submit data or references offering facts, reasonable assumptions based on facts, or expert opinion supported by facts in support of the comments. Pursuant to Section 15064, an effect shall not be considered significant in the absence of substantial evidence.” Section 15204 (d) also states, “Each responsible agency and trustee agency shall focus its comments on environmental information germane to that agency’s statutory responsibility.” Section 15204 (e) states, “This section shall not be used to restrict the ability of reviewers to comment on the general adequacy of a document or of the lead agency to reject comments not focused as recommended by this section.”

Written responses are prepared consistent with Section 15088 of Title 14 of the California Code of Regulations. Pursuant to this section, the level of detail contained in the response may correspond to the level of detail provided in the comment (i.e., responses to general comments may be general). In accordance with CEQA, Public Resources Code Section 21092.5, copies of the written responses to public agencies will be forwarded to those agencies at least 10 days prior to certifying the environmental impact report. The responses will be forwarded with copies of this FEIR, as permitted by CEQA, and will conform to the legal standards established for response to comments on Draft EIRs.

2. Response to Comments

2.1 INTRODUCTION

Section 15088 of the CEQA Guidelines requires the Lead Agency (City of Ontario) to evaluate comments on environmental issues received from public agencies and interested parties who reviewed the Draft SEIR and prepare written responses. This section provides written responses received on the Draft SEIR and the City of Ontario's responses to each comment. Comment letters and specific comments are given letters and numbers for reference purposes.

The following is a list of agencies and persons that submitted comments on the Draft SEIR during the public review period.

Number Reference	Commenting Person/Agency	Date of Comment	Page No.
Agencies & Tribes			
A1	Quechan Historic Preservation	May 14, 2022	2-7
A2	City of Chino	June 23, 2022	2-11
Residents & Organizations			
R1	Critical Action and Social Advocacy (CASA) Pitzer	June 15, 2022	2-23
R2	Inland Equity Partnership	June 23, 2022	2-27
R3	Robert Redford Conservancy at Pitzer College; Southern California Agricultural Land Foundation; Center for Community Action and Environmental Justice; League of United Latin American Citizens (LULAC) de Inland Empire; and Sierra Club San Geronio Chapter	June 23, 2022	2-31
R4	Lozeau Drury, LLP on behalf of the Supporters Alliance for Environmental Responsibility (SAFER)	June 23, 2022	2-63
R5	Robert Barragan	June 14, 2022	2-73
R6	Vanessa Yambot	June 14, 2022	2-79
R7	Ilianna Romero	June 14, 2022	2-83
R8	Verlynn Gojil	June 14, 2022	2-89
R9	Inmi Chao	June 14, 2022	2-93
R10	Katie Crofford	June 16, 2022	2-99
R11	Elvia Velazco	June 16, 2022	2-105
R12	Aldo Velazco	June 16, 2022	2-111
R13	Inez Cardenas	June 16, 2022	2-117
R14	Caroline Lanspa	June 16, 2022	2-121
R15	Daniel Lo	June 16, 2022	2-125
R16	Eva Ramirez	June 16, 2022	2-131
R17	Cara Butler	June 17, 2022	2-135
R18	Antoinette Anderson	June 17, 2022	2-139
R19	Jackie Deans	June 17, 2022	2-145

2. Response to Comments

Number Reference	Commenting Person/Agency	Date of Comment	Page No.
R20	Danielle Martinez	June 17, 2022	2-149
R21	Cynthia Serrano	June 19, 2022	2-155
R22	Tina Silva	June 19, 2022	2-161
R23	Jamie Ortega	June 21, 2022	2-167
R24	Dessa D'Aquila Bentson	June 21, 2022	2-171
R25	Malina Shaw	June 21, 2022	2-175
R26	Lisa Collinsworth	June 21, 2022	2-181
R27	Jarius Martinez	June 21, 2022	2-185
R28	Raquel Alba	June 21, 2022	2-189
R29	Jocelyn Dimacali	June 22, 2022	2-193
R30	Amber Bound	June 22, 2022	2-197
R31	Andrea Alvarado	June 22, 2022	2-201
R32	Deborah Rodriguez	June 22, 2022	2-205
R33	Amanda Wheeler	June 22, 2022	2-209
R34	Dena Apodaca	June 22, 2022	2-213
R35	Andrei Pitsko	June 22, 2022	2-217
R36	Randy Bekendam	June 22, 2022	2-221
R37	Elizabeth Rocco	June 23, 2022	2-227
R38	Martha Lopez	June 23, 2022	2-231
R39	Karen and Guy Caparelli	June 23, 2022	2-235
R40	Elias Luviano	June 23, 2022	2-239
R41	Nazareth Bracamontes	June 23, 2022	2-243
R42	Estela Ballon	June 23, 2022	2-247
R43	Rosemary Ramos	June 23, 2022	2-251

2.2 SUMMARY OF GENERAL RESPONSES

Several common issues were raised in the comment letters, such as Letters R1 through R43, received during the public review period for the Draft SEIR. This section provides a summary of general responses to recurring comments raised by responding agencies and the public. The summary of general responses provides a more comprehensive explanation and response to these common issues raised during the public review period.

2.2.1 Scenic Vistas

The Draft SEIR includes an evaluation of impacts to scenic vistas. Scenic resources within the City are identified on page 5.1-3 through 5.1-4. It should be noted that protected viewsheds are public viewsheds of scenic resources and not private viewsheds. As such, scenic vistas identified on page 5.1-4 include views of scenic vistas from the City's formal and informal trail network. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Neither the 2010 Certified EIR nor the Draft SEIR identifies impacts to scenic vistas. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code, which, as described in Section 5.1.1.1, *Regulatory*

2. Response to Comments

Background, of the Draft SEIR and includes general development requirements for development density, screening and setback, signing, landscaping, lighting, height limitations, and other aspects related to the aesthetic of the City.

2.2.2 Nonrenewal of Williamson Act Contracts

The Draft SEIR includes an analysis of conflicts with existing zoning for agricultural use or Williamson Act contract on pages 5.2-12 through 5.2-13. The baseline for the Draft SEIR is the current TOP. The current TOP does not have Agriculture land use designations.

Statewide concern over the rapid conversion of agricultural land to urban uses led to the enactment of the Land Conservation Act of 1965, commonly referred to as the Williamson Act. This Act enabled the County of San Bernardino to establish an Agricultural Preserve where property owners could contract with the County to preserve the property and quality for a property tax assessment based upon use and economic yield rather than the full market value of the property. With the annexation of the New Model Colony in 1999, the City of Ontario assumed responsibility for administering land conservation contracts on those properties.

A Land Conservation Contract (LCC) has a minimum term of 10 years and is automatically renewed each year for an additional year, thereby keeping the term of the contract to a period of 10 years. The perpetual contract continues until the City or the owner files a Notice of Non-renewal, which will then terminate the contract at the end of its remaining term, usually 9 years. When a Non-renewal is filed, the property tax assessment gradually reverts back to being computed based upon full market value rather than use.

The Draft SEIR identifies the current Williamson Act contract lands within the City in Figure 5.12-12. As identified in the Draft SEIR, any land held in a Williamson Act contract would have to be filed for nonrenewal, and the contract would have to be allowed to expire before any development occurs on it. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of these contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. As such, impacts from the Proposed Project in this respect would be less than significant.

2.2.3 Environmental Justice Policies

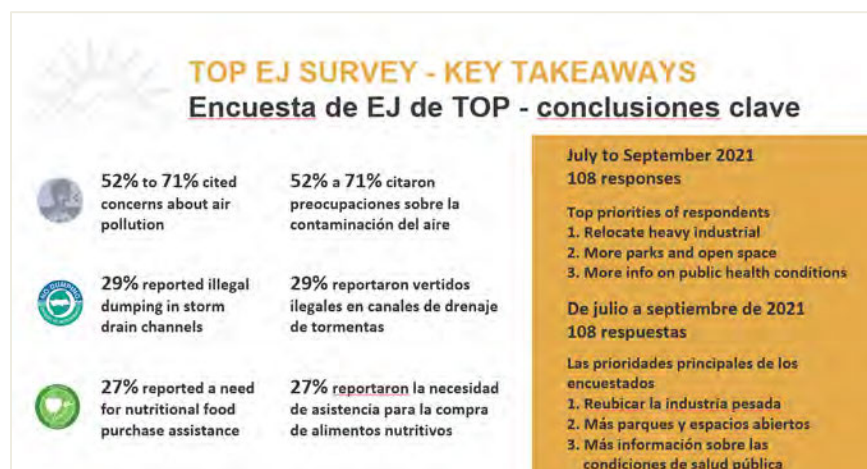
California Government Code Section 65302(h)(1)(A) requires jurisdictions when updating their general plan to identify objectives and policies to reduce the unique or compounded health risks in disadvantaged communities by means that include, but are not limited to, the reduction of pollution exposure, including the improvement of air quality, and the promotion of public facilities, food access, safe and sanitary homes, and physical activity. The City has opted for an “environmental justice in all policies” approach in TOP 2050 to ensure the topic is present alongside the multitude of issues and topics that affect our residents and resources. TOP 2050 includes policies that address impacts to disadvantaged communities and access to healthy foods (see Policy SR-1.2).

In addition to weaving environmental justice policies throughout TOP 2050 (see Appendix B), the City also conducted outreach with community. As part of its outreach strategy, the City of Ontario emphasized the topic of environmental justice through advertising materials, stakeholder interviews, survey materials, educational

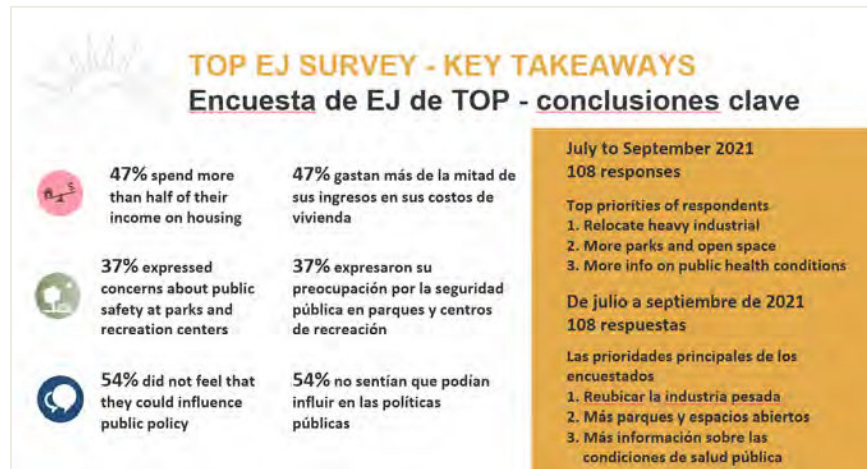
2. Response to Comments

materials, and online and in-person outreach activities. The City also posted announcements of the outreach activities in English and Spanish through various social media platforms (e.g., Twitter, Facebook, and Instagram). As this effort was conducted during COVID, the City conducted a substantial amount of outreach online through virtual meetings, workshops, and office hours to maintain compliance with local and state regulations. Public input from the outreach activities led directly to the addition of new policies to TOP:

- **Public Workshops.** One online property owner workshop and two online public workshops were conducted on June 14, 2021, June 28, 2021, and July 29, 2021. At the property owner workshop, the City provided an overview of the TOP 2050 Update and land use designation changes. At the first public workshop, the City introduced and discussed the overall land use strategy and requirements and issues related to environmental justice and climate adaptation and resiliency. At the second public workshop, the City also presented areas that could be identified as environmental justice communities based on state modeling data and discussed aspects of the Environmental Justice Survey with attendees in order for them to reach out to their neighborhoods. Workshop materials were provided in English and Spanish, a Spanish translation was provided during the question-and-answer portions of the workshops, and recordings of the presentation (in English and Spanish) were published on the City’s website shortly after the workshops.
- **Environmental Justice Survey.** The City conducted an online Environmental Justice Survey between July to September 2021. This survey was available in English and Spanish and was completed by 108 individuals. The purpose of this survey was to gather the perspectives of local community members living in the City’s environmental justice communities. Survey participants were asked if they were exposed to certain pollutants, if they had access to healthy food, necessary resources, and amenities, experienced structural damages due to environmental hazards, to select TOP actions to improve current conditions, and their demographics. A summary of the results was presented at many of the meetings and is also provided below:



2. Response to Comments



- **Environmental Justice and Community Health Stakeholders.** Throughout June and July 2021, the City reached out to local organizations that serve as advocates and service providers on topics related to environmental justice, including Community Health Improvement Association (CHIA), Center for Community Action and Environmental Justice (CCA EJ), and Caramel Connections Foundation. Outreach was conducted via telephone, email, interviews, distribution of the survey, and during meetings (of the local organization or at an event held by the City). Information requested and obtained include the organization’s perspective on important EJ issues, current or potential programs and services, and potential new or revised policies and regulations. CHIA was the most responsive organization, providing additional door-to-door outreach TOP 2050 flyer distribution, while the others indicated priorities. Some organizations indicated that they were unable to respond, undergoing a transition in leadership, or had priorities in other communities. For those organizations that did not respond, the City repeated attempts through phone calls, emails, and other means as appropriate to ensure that these organizations were aware of the City’s activities and looked forward to hearing from any stakeholders that were interested.
- **Environmental Justice 101 Story Map.** The City developed and published a bilingual (English and Spanish) “Environmental Justice 101” Story Map on its website. The Story Map contains introductory information about environmental justice, explains and illustrates what disadvantaged communities are, state laws, and CalEnviroScreen (CES) 4.0.
- **Project Scoping Meeting.** The City provided updated about the Supplemental Environmental Review (SEIR) and provided space for public input and comment on the scope of the environmental analysis. The meeting was held online on August 5, 2021 and was conducted in both English and Spanish. The meeting materials were presented in both English and Spanish and Spanish translation was provided during the question-and-answer portions of the meeting. Finally, recordings of the meeting (in English and Spanish) were published on the City’s website shortly after the meeting.
- **Office Hours.** The City conducted Office Hours on a monthly basis in 2022 to provide the community with periodic updates and offer a low-pressure platform for the community to ask questions and provide

2. Response to Comments

their perspectives regarding the technical update, including the topic of environmental justice. The first three Office Hours were conducted online (3/31/22, 4/28/22, and 5/26/22) while the fourth was conducted in person (6/30/22). The Office Hours were conducted in both English and Spanish. The meeting materials were presented in both English and Spanish and Spanish translation was provided during the question-and-answer portions for each meeting. Finally, recordings of the online Office Hours (in English and Spanish) were published on the City's website shortly after each meeting. The dates for all four office hours were advertised through the City's social media accounts and through a bilingual (English and Spanish) insert mailed with the utility bill sent in February 2022 to every resident and business owner in Ontario.

- **Teen Action Committee.** The City held three meetings with the Teen Action Committee, which is the City's largest community-oriented teen organization in Ontario and addresses a wide variety of community issues. The City held meetings on June 6, 15, and 16 in 2022 at the Dorothy A. Quesada Community Center, De Anza Community & Teen Center, and Westwind Community Center. The City facilitated discussions and obtained feedback concerning issues related to environmental justice and community health.
- **General Outreach.** The City also attended outreach events to share the TOP 2050 Update with the community at two Concert in the Park events and the Taste of Ontario where the City displayed the Draft Land Use Plan and gathered input on what folks would like to see in their neighborhood. The City provided a variety of giveaways at the events and encourage participation in an interactive City building bloc activity.

It is important to note that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.

2.2.4 Air Quality Impacts

Air quality impacts associated with buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project (see Impact 5.3-1).

Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project (see Impact 5.3-4). It should be noted that Policy ER-4.9, New Localized Air Pollution Sources Near Existing Sensitive Receptors, would ensure mobile sources of toxic air contaminants (TACs) not covered under South Coast Air Quality Management District (South Coast AQMD) permits are considered during subsequent project-level environmental review by the City of Ontario. Individual development projects would be required to achieve the incremental risk thresholds established by South Coast AQMD. However, implementation of TOP 2050 would generate TACs that could contribute to elevated levels in the air basin.

2. Response to Comments

2.3 AGENCY AND TRIBAL COMMENTS

LETTER A1 – Quechan Historic Preservation (1 page)

From: Quechan Historic Preservation <historicpreservation@quechantribe.com>
Sent: Saturday, May 14, 2022 10:59 AM
To: Allison Dagg <adagg@placeworks.com>
Subject: RE: Notice of Availability of a Draft Supplemental EIR for The Ontario Plan 2050

This email is to inform you that we do not wish to comment on this project. We defer to the more local Tribes and support their determinations on this matter.

A1-1

2. Response to Comments

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2. Response to Comments

A1. Response to Comments from Quechan Historic Preservation, dated May 14, 2022.

A1-1 The Commenter indicates that they do not wish to comment and defer to local Tribes. Therefore, as the comment does not raise an issue with the analysis of the DEIR, no further response is necessary.

2. Response to Comments


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2. Response to Comments

LETTER A2 – City of Chino (5 pages)

EUNICE M. ULLOA
Mayor

MARC LUCIO
Mayor Pro Tem



CITY of CHINO

KAREN C. COMSTOCK
CHRISTOPHER FLORES
WALT POCOCK
Council Members

ANTHONY ARROYO
Interim City Manager

June 23, 2022

Thomas Grahn, Senior Planner
City of Ontario Planning Department
303 East "B" Street
Ontario, CA 91764

Re: Notice of Availability for a Supplemental EIR, The Ontario Plan 2050 (SCH #2021070364)

Dear Mr. Grahn:


This letter is in response to the Notice of Availability (NOA) for The Ontario Plan 2050 Supplemental EIR, made available on May 9, 2022. Thank you for giving the City of Chino the opportunity to review the information and provide comment. The City's comment is outlined below:

Land Development/Engineering

1. Figures 5.19-4 and Figure 7 (Infrastructure Report) are not consistent with the City of Chino's Master Plan of Drainage and should be updated. Both figures depict a SB County Flood Control Channel on Euclid south of Merrill rather than a proposed storm drain per the City's Master Plan. Intro
A2-1

Public Works Water/ Environmental Division

2. General: Update to APN 105311101, 105311103, and 105311108 (Chino parcels) which are shown on the housing element for this 13.5 acre site as low-medium density. A2-2
3. Storm Drain: Future expansion of the storm water system with the proposed land uses do not seem to be incorporated for Campus Avenue between Schaefer and Edison. The City of Chino's previous planning conversation understood the area would have facilities and the temporary retention installed on our site could be removed in the future. Additionally, the City of Chino has paid local storm drain development impact fees for future phases of construction of our property. A2-3
4. Sewer: Please confirm, sewer is planned for Campus and that storm facilities are not planned for Campus. A2-4
5. Water: A2-5
 - a. One of the tables had Ontario's CDA and WFA water supplies mixed-up. Verify information.

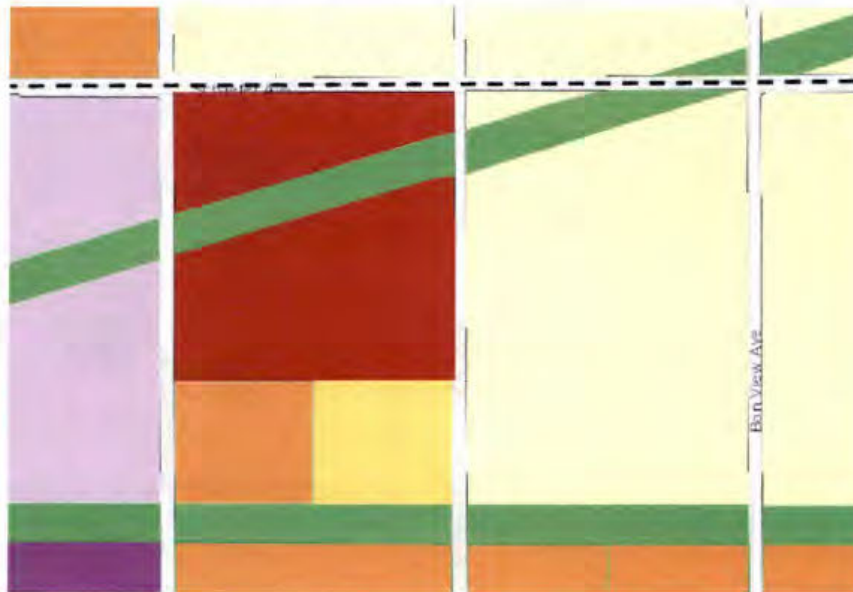


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(909) 334-3250 • (909) 334-3720 Fax
Web Site: www.cityofchino.org

2. Response to Comments

Thomas Grahn, Senior Planner
City of Ontario Planning Department
Re: Notice of Availability for a Supplemental EIR: The Ontario Plan 2050 (SCH #2021070364)
June 23, 2022
Page 2

- b. The City of Chino Eastside Water Treatment Facility (EWTF) property currently is zoned to have some open space through the property. The City of Ontario should include amendments to TOP to divert the OS from the City of Chino's water treatment facility since it will not be feasible to implement a trail or recreation through this area. The parcels should also be categorized as PF-Public Facility. Please note that the Land Use Figure 1-3 available on the city's site does not match the ESRI GIS site (which is pictured below).



A2-6

- c. On Page 5.17-3, second paragraph, the MWD IRP Update is supposed to have annual reports. Would including the information from the latest annual report be necessary to understand the current IRP target status on the long-range planning effecting Ontario? The report does not provide assurance that supply obtained from MWD will continue as a reliable source. Further in the document the MWD IRP Update is cited to be from 2009 which only covers long term planning from 2025 to 2035, therefore, relying on the information of such document as reference to project the long-term planning to 2050 does not seem to be adequate.

A2-7

- d. On Page 5.17-3, fourth paragraph, why is the latest DWR Reliability Report 2008 when it is published every two years per statement in first sentence of the paragraph. Furthermore, the ultimate future conditions of the reliability are only

A2-8

2. Response to Comments

Thomas Grahn, Senior Planner
City of Ontario Planning Department
Re: Notice of Availability for a Supplemental EIR: The Ontario Plan 2050 (SCH #2021070364)
June 23, 2022
Page 3

extended to 2027 rather than the intended long term planning of the Ontario document which is 2050.

A2-8
cont.

e. On Page 5.17-12, a table is presented to show water demand calculations through 2030. Although the paragraph below the table seems to be incomplete and justify the City via water conservation and transfer of water rights there should be adequate water supply. It would be helpful for The Plan to show projections through 2050 given the intended planning year of the update.

A2-9

f. On Page 5.17-12, mitigation impact 17-3 seems to indicate that City intends to develop additional groundwater sources. City of Ontario should identify potential locations now so that the potential impacts are better described since it could adversely affect City of Chino groundwater well locations.

A2-10

Traffic / Transportation

6. Continue to coordinate with the City of Chino on Active Transportation connections (bikes & peds) between agencies including across Euclid Avenue (SR-83). There is an opportunity to ensure Euclid Avenue not only provides passenger and commercial vehicle movement but to provide a primary north-south bike and pedestrian corridor connecting the region through multi-modal uses.

A2-11

7. Continue to coordinate Truck Routes with the City of Chino to avoid conflicts and reinforce primary truck routes between agencies.

A2-12

8. With industrial land use along Merrill Avenue, is Merrill Avenue correctly classified and designed to accommodate buildout growth of Ontario Ranch, Chino, Eastvale & Jurupa Valley anticipated traffic demand? Although VMT is the CEQA metric for environmental impact, LOS for roadway capacity and intersection delay should continue to be used to provide acceptable LOS at buildout of the City of Ontario Plan 2050 to ensure reliable goods movement in this area.

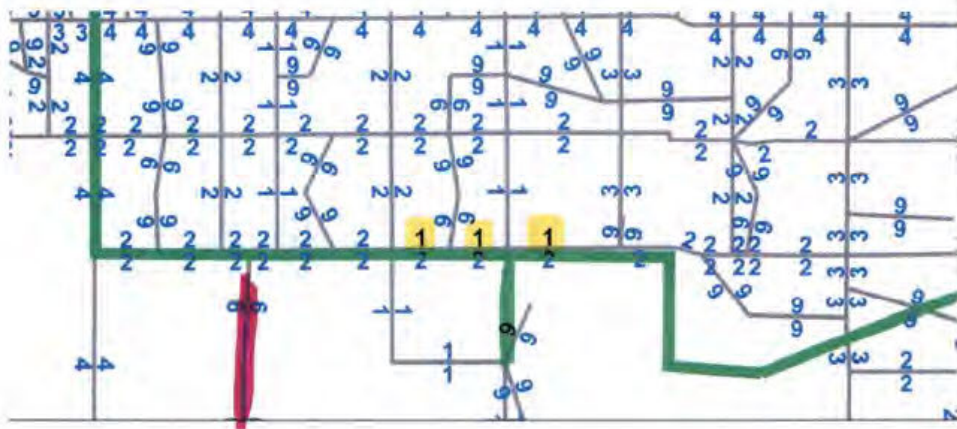
A2-13

9. EIR Appendix J & K: VMT & LOS Memorandum: Figures 1-4 of the traffic model do not show Hellman Avenue in Chino connecting to Merrill Avenue (Green). Also, Figure 3 incorrectly assumes there will be a roadway through the Chino Airport (Magenta). Also in Figure 3, the westbound Merrill Avenue segment between Walker (Flight) and Carpenter incorrectly indicates a single lane and not 2-lanes as is currently under construction (Yellow).

A2-14

2. Response to Comments

Thomas Grahn, Senior Planner
City of Ontario Planning Department
Re: Notice of Availability for a Supplemental EIR: The Ontario Plan 2050 (SCH #2021070364)
June 23, 2022
Page 4



A2-14
cont.

Planning

10. The current TOP Land Use as shown on Figure ES-4 indicates the current approved land uses for the City of Ontario. Figure ES-5 indicates the proposed land use changes associated with this SEIR. There are significant land use changes proposed to properties located north of Merrill Avenue, south of Eucalyptus Avenue between Sultana Avenue and Grove Avenue. The City of Chino and the City of Ontario share Merrill Avenue for truck and vehicular access to this area. The *Draft Proposed Housing Strategy and Land Use Scenario Map* provides greater detail related to the specific land use changes, particularly the acreage of land that is proposed to be changed. In the specific section noted, approximately 178 acres of land are proposed to be changed from Low Medium Density Residential (LMDR) (5.11-11.00 du/ac) to Industrial (IND) (0.55 FAR). In this area 47 acres of land are proposed to be changed from LMDR to Business Park (BP) (0.60 FAR). Also, in this area 72 acres of land are proposed to be changed from BP to IND. This results in a total of approximately 47 acres of BP designated land and approximately 250 acres of IND designated land in this area. This is a considerable increase of BP and IND designated land that will likely have a significant impact to truck traffic along Merrill Avenue, Euclid Avenue, and other streets in the City of Chino. It is unclear in the EIR if the BP/IND land has been transferred from other areas of the City of Ontario or if these are newly proposed land uses in this area. Either way, more detail related to VMT and specifically truck traffic increases to this area based on the potential industrial building square footage on this land are needed to determine the impacts to traffic and air quality in the vicinity.

A2-15

11. Table ES-1 provides for a comparison of the changes between the approved TOP and proposed TOP. The table indicates that there will be an increase in Nonresidential square footage of 1,092,508 square feet. It is unclear from that table where the increased

A2-16

2. Response to Comments

Thomas Grahn, Senior Planner
City of Ontario Planning Department
Re: Notice of Availability for a Supplemental EIR: The Ontario Plan 2050 (SCH #2021070364)
June 23, 2022
Page 5

Nonresidential square footage is proposed to be located or if the increase is a result of a reduction in one area of the City and an increase in another, with that number being the net result. Additionally, as noted in Comment 10, there are approximately 300 acres of land north of Merrill Avenue with a proposed land use change to BP and IND, the additional proposed nonresidential square footage seems far less than what could be developed on the 300 acres of land. More details of where the additional Nonresidential square footage is to be located is required, and what the Nonresidential land uses consist of in those areas. Particularly the additional proposed amount of Business Park and/or Industrial square footage that could be developed within the 300 acres of land as noted in Comment 10. This information needs to be provided to identify the various environmental impacts to this area.

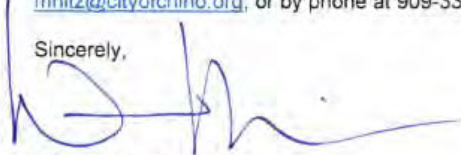
A2-16
cont.

12. Chapter 7.5 of the SEIR provides for a Reduced Industrial Alternative to the proposed project, which indicates it would eliminate 8.1 million square feet of industrial development in the City of Ontario. This section identifies that there are 338 additional acres of Industrial (IND) designated land. As noted in Comment 10, 250 acres of land located north of Merrill Avenue is proposed to be changed to IND. This section is not specific as to where the reduction in industrial development would occur with this alternative and how that reduction would impact truck traffic specifically. More details are needed related to what proposed land use changes and which specific parcels would be associated with the utilization of the Reduced Industrial Alternative. A land use map outlining the Reduced Industrial Alternative is required along with the details needed as outlined in Comments 10 and 11 above to determine the environmental impacts to the vicinity of Merrill Avenue associated with the Reduced Industrial Alternative.

A2-17

If you have any questions, please contact Michael Hitz, Principal Planner, by email at mhitz@cityofchino.org, or by phone at 909-334-3448.

Sincerely,



Warren Morelion, AICP
City Planner

cc: Michael Hitz, AICP, Principal Planner
Jesus Plasencia, P.E., Assistant City Engineer
Dennis Ralls, T.E., Transportation Manager

2. Response to Comments

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2. Response to Comments

A2. Response to Comments from Warren Morelion, AICP, City Planner, City of Chino, dated June 23, 2022.

Intro Response to comments provided on behalf of the City of Chino are provided in responses A2-1 through A2-17 below.

A2-1 At the request of the Commenter, the symbology on Figure 5.19-4 has been revised to show the City of Chino proposed storm drain (see Chapter 3, *Revisions to the Draft EIR*, and Appendix A, *Revised Draft SEIR Figures*).

A2-2 Comment noted. The parcels referenced are Chino-owned parcels in the City of Ontario. This is a comment on the Housing Element, which was adopted in March 2022. These three parcels are currently and are recommended to remain designated for Low-Density Residential (LDR). Additionally, these parcels are not listed in the Sites Inventory (Housing Element Tables B-1 and B-2) and are not shown on any of the maps showing the opportunity areas (see Housing Element Figures 5-1 through 5-11) of the adopted Housing Element. Finally, these parcels are not outlined as Housing Element Sites on the City's Proposed Housing Strategy and Land Use Scenario (Draft) map that is posted on the City's webpage for TOP 2050.

A2-3 In 2014, City of Chino developed their Eastside Water Treatment Facility on the southeast corner of Schaefer Avenue and Campus Avenue. Since no permanent downstream storm drain was available, development of the Eastside Water Treatment Facility was required to detain stormwater on site. There is no proposed storm drain in Campus Avenue. The City of Chino site is accounted for in the tributary areas that drain to Line C-15 (BNVW-XIV-1) in Bon View Avenue and Line C-16 (EULD-XIV-3) in Edison Avenue (attached is a detailed hydrology map that illustrates this). Development of the project included payment of required Development Impact Fees. The fulfilment of their Development Impact Fee obligation pays for their fair-share cost of the master plan facilities that would serve the property.

A2-4 See response to Comment A2-3. The fulfilment of their Development Impact Fee obligation pays for their fair-share cost of the master plan facilities that would serve the property.

A2-5 The Commenter was mistakenly reviewing the 2010 EIR utilities chapter and not the TOP 2050 SEIR. There is no table in the utilities section of the TOP 2050 SEIR that provides Ontario's Chino Basin Desalter Authority (CDA) and the Water Facilities Authority (WFA) water supplies.

To the extent Chino intended to comment on the 2010 EIR, a subsequent CEQA review that addresses changes to the project is required to provide only the information necessary to make the previous EIR adequate for the project as revised. It is not required to reconsider the analysis in the prior EIR (*California Coastkeeper Alliance v. State Lands*

2. Response to Comments

Commission (2021) 64 CalApp.5th 36, 58). The project impacts reviewed in the prior EIR are properly treated as part of the environmental baseline in a subsequent or supplemental EIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). The prior CEQA review for a project cannot be attacked in a case challenging the adequacy of a subsequent or supplemental EIR that is prepared to address changes to the project.

- A2-6 The City's proposed Official Land Use Map in the Policy Plan identifies utility easements with an Open Space – Non-Recreational land use designation. An easement for Southern California Edison high voltage (500-kV) transmission lines runs through the properties that contain the City of Chino Eastside Water Treatment Facility (Chino EWTF). Such easements must remain clear of obstruction to ensure free access to and the operation and maintenance of the transmission lines. Accordingly, this portion of the parcels referenced in the comment are appropriately designated as nonrecreational open space. While open space facilities, such as unprogrammed grass areas, trails, or bikeways can be developed on these easements, the City of Ontario does not intend for the easement to support active open space, such as parklands, trails, or bikeways south of Schaefer Avenue (the parcels that contain the Chino EWTF are south of Schaefer Avenue), as shown on the City's proposed map of multipurpose trails and bikeways (TOP 2050 Figure M-02) and the proposed map of park and recreational facilities (TOP 2050 Figure PR-01).
- A2-7 The Commenter is mistakenly reviewing the 2010 EIR utilities chapter and provides a comment regarding the Municipal Water District (MWD) Integrated Resources Plan (IRP) Update on page 5.17-3. TOP 2050 SEIR utilities section is Section 5.19 and not Section 5.17 and there is no discussion of MWD IRP on page 3 of the TOP 2050 SEIR utilities section.
- A2-8 As stated previously, this comment refers to the 2010 EIR utilities chapter and discusses a DWR Reliability Report from 2008. There is no discussion of the DWR Reliability Report in the TOP 2050 SEIR utilities section and the long-term planning for water supply is extended to 2050 in the current utilities section.
- A2-9 The Commenter references a table on page 5.17-12 that provides water demand calculations and asks for projections through 2050. Again, this table is from the previous 2010 EIR utilities chapter; the current TOP 2050 SEIR does provide water demand projections through 2050.
- A2-10 The Commenter provides a comment regarding mitigation measure 17-3 from the previous 2010 EIR. This mitigation measure has been deleted from the current TOP 2050 SEIR.
- A2-11 The City will continue to coordinate with the City of Chino on bike and pedestrian planning.

2. Response to Comments

- A2-12 The City will continue to coordinate with the City of Chino on designation of Truck Routes.
- A2-13 The roadway is being widening consistent with the Master Plan of Arterial Highways which designates Merrill Avenue as #11 with a 108-foot right-of-way (ROW) width and 84-foot curb-to-curb width. This allows for at least two lanes in each direction and a center turn lane. The forecasting and capacity assessment included as Appendix K to the Draft SEIR determined four lanes are adequate to accommodate future growth. The roadway is also being designed as a truck route and designed to handle heavier loads. It should be noted that under the Senate Bill 743, vehicle miles traveled (VMT)-related metric(s) that evaluate the significance of transportation-related impacts under CEQA for development projects, land use plans, and transportation infrastructure projects are required beginning on July 1, 2020. The legislation does not preclude the application of local general plan policies, zoning codes, conditions of approval, or any other planning requirements that require evaluation of level of service (LOS), but these metrics may no longer constitute the sole basis for determining transportation impacts under CEQA. The Draft SEIR Section 5.17, *Transportation*, evaluates the VMT impacts of TOP 2050 compared to that of the Approved Project. As a result, LOS for roadway capacity and intersection delay is not utilized for evaluating impacts in the SEIR. However, TOP 2050 Mobility Element includes roadway classifications (see Figure 5.17-3, *Roadway Classification*), which are informed by the LOS Memorandum included in Appendix K of the Draft SEIR.
- A2-14 Regarding Hellman Avenue, the model is a simplification of the roadway network and not all roadways are necessarily depicted in the model. Traffic volume that would be distributed to Hellman Avenue is otherwise distributed to nearby roadways, which are shown to operate at acceptable levels of service. This aggregation of minor roadways has no bearing on the SEIR's conclusions.
- The magenta line drawn on the Commenter's map and reference to it as a "roadway" shown in the model roadway network is incorrect. The line highlighted in magenta is a 'centroid connector'. This connector indicates all of the local roadways and driveways that would access Merrill Avenue from the traffic analysis zone (TAZ) and is not meant to represent a roadway through the airport.
- The Commenter is correct. The model incorrectly shows one westbound lane under future conditions. However, this lower capacity does not result in a shift in traffic to adjacent roadways as there is adequate capacity to handle the forecast traffic volumes. Therefore, this error does not alter findings or conclusions.
- A2-15 The comment regarding the increase in land designated as Industrial and Business Park associated with TOP 2050 compared to the current TOP is noted.

2. Response to Comments

The *City of Ontario Resolution Adopting Vehicle Miles Traveled Thresholds* were adopted in June 2020. The City's guidelines require use of the San Bernardino Traffic Analysis Model (SBTAM) to evaluate how a proposed project changes VMT in the City of Ontario.

It is important to note that the Governor's Office of Planning and Research (OPR) VMT Guidelines¹, shifts the CEQA transportation metrics to promote outcomes that reduce reliance on automobile travel. Neither the Technical Advisory nor CEQA Guidelines Section 15064.3(a) directly address how to analyze transportation impacts associated with changes to goods movement, which is largely carried out by heavy-duty trucks. CEQA Guidelines Section 15064.3(a) specifies that VMT to be analyzed is defined as the amount and distance of *automobile* travel (emphasis added) attributable to a project. The term 'automobile' refers to on-road *passenger vehicles, specifically cars and light trucks* (emphasis added).² SB 743 is not intended to require the inclusion of heavy-duty truck trips, utility vehicles, or other types of vehicles in the VMT analysis.³ In the case of trucks (other than light trucks), based on CARB's 2017 Scoping Plan, the state's primary strategy for the goods movement sector is not in VMT reduction, but in advances in technology (zero-emissions [ZE] and near-zero-emissions [NZE] control strategies).⁴ This is consistent with observations that transportation demand management strategies (TDM) for VMT reductions identified by the San Bernardino County Transportation Authority (SBCTA) and the California Air Pollution Control Officer's Association (CAPOCA) do not apply to truck VMT. However, the City's VMT threshold conservatively includes VMT from both passenger vehicles and trucks under the origin-destination (OD) criteria.

The VMT methodology is described on page 6 of VMT Memorandum (see Appendix J to the Draft SEIR). The City of Ontario's VMT threshold for the General Plan under Project analysis states that "A significant impact would occur if the project VMT/SP⁵ (for the land use plan) exceeds the Citywide average for Service Population under General Plan Buildout Conditions." VMT/SP means all VMT generated by all trips that either begin or end in Ontario, including trucks, divided by the City's service population, which consists of all people living and/or working in Ontario.

¹ Governor's Office of Planning and Research. 2018, December. Technical Advisory on Evaluating Transportation Impacts in CEQA. https://opr.ca.gov/docs/20190122-743_Technical_Advisory.pdf

² Governor's Office of Planning and Research. 2018, December. Technical Advisory on Evaluating Transportation Impacts in CEQA, https://www.opr.ca.gov/docs/20190122-743_Technical_Advisory.pdf

³ PlaceWorks coordinated with staff at OPR on January 12, 2021 to confirm how to address heavy-duty freight VMT in CEQA documents. OPR staff identified that the intent of SB 743 was to address passenger vehicle VMT impact and not freight VMT, as cited under CEQA Guidelines Section 15064.3(a). Therefore, lead agencies could exclude freight VMT from transportation VMT impact analyses under CEQA.

⁴ California Air Resources Board. 2017, November. California's 2017 Climate Change Scoping Plan: The Strategy for Achieving California's 2030 Greenhouse Gas Target, https://ww2.arb.ca.gov/sites/default/files/classic/cc/scopingplan/scoping_plan_2017.pdf

⁵ Service Population (SP)

2. Response to Comments

Under the Project-level VMT significance threshold, the proposed Plan reduces VMT/SP as compared to the current Plan. The City's cumulative VMT threshold for the General Plan analysis states that "A significant impact would occur if the project caused total daily VMT within the City to be higher than the no project alternative under cumulative conditions." Compared to the Approved Project, the Proposed Plan is adding capacity for 25,399 residential units and 1,092,508 square feet of nonresidential square footage. This includes additional capacity for 5,189,888 square feet of Business Park (BP) and 8,099,949 square feet of Industrial (IND), and reduced capacity for 8,990,532 square feet of Mixed Use (MU) and 3,206,797 square feet of Commercial and Hospitality (NC, GC, OC, and HOS), which results in an absolute increase in VMT.

However, as demonstrated by the reduction in VMT/SP, this increase in residents and jobs results in less VMT per person. The Proposed Project increases both housing and jobs, pursuant to the Southern California Association of Government's (SCAG) Sustainable Communities Strategy (SCS) of maintaining and improving the jobs/housing balance. The Proposed Project demonstrates that it accomplishes this goal because the VMT per resident and per employee decreases as compared to the current TOP.

Furthermore, the SEIR consider emissions from freight and freight-related VMT in emissions modeling conducted for TOP 2050 included in Section 5.3, *Air Quality*, and Section 5.8, *Greenhouse Gas Emissions*. For example, modeling in Appendix C provides a breakdown of daily VMT for passenger vehicles, light-heavy duty trucks, medium-heavy duty truck, and heavy-heavy duty truck emission (see page C-17) as well as emissions by vehicle type (see page C-18 through C-25). Therefore, the SEIR considers the impacts associated with the increase in land designated for Industrial and Business Park. Despite the increase in truck VMT, Table 5.3-10, *City of Ontario Regional Criteria Air Pollutant Emissions Forecast*, shows that NO_x and particulate matter (PM₁₀ and PM_{2.5}) would decrease compared to the Approved Project. Only volatile organic compounds (VOCs) would increase compared to the Approved Project and the vast majority of those emissions would be associated with an increase in consumer product use. As identified in Section 5.3, *Air Quality*, this was identified as a significant unavoidable impact of TOP 2050.

- A2-16 Section 3.4.2.3, *Areas of Change*, in the Draft SEIR, provides a detailed description of the land use changes of TOP 2050 compared to the current TOP. Figure 3-6, *Areas of Change*, in the Draft SEIR also provides the location of the focused land use changes. As identified in this section, the majority of changes are concentrated in four growth areas and the Ontario Ranch.
- A2-17 See response to Comment A2-16. The reduction in Industrial development would primarily occur in the four growth areas. At the request of the Commenter, a new Figure 7-1, *Reduced Industrial Alternative Parcels*, has been incorporated into the SEIR (see Chapter 3, *Revisions to the Draft EIR*, and Appendix A, *Revised Draft SEIR Figures*).

2. Response to Comments

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2. Response to Comments

2.4 RESIDENT AND ORGANIZATION COMMENTS

LETTER R1 – Critical Action and Social Advocacy (CASA) Pitzer (1 page)

<p>From: CASA Pitzer <casapitzer@gmail.com> Sent: Wednesday, June 15, 2022 2:29 PM To: Thomas Grahn <TGrahn@ontarioca.gov> Subject: Public Comment on Ontario General Plan</p>	
<p>1. Our community organization is Critical Action and Social Advocacy (CASA [pitzer.edu]) Pitzer and we are based out of Ontario. This is a notification that we, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan.</p>	
<p>2. On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR).</p>	R1-1
<p>3. These findings from the Draft SEIR examines the potential environmental impacts as a result of this project which includes the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.</p>	
<p>4. This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes:</p>	
<p>5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance.</p>	R1-2
<p>5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert.</p>	R1-3
<p>5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP.</p>	R1-4
<p>5.3-3 would generate long term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations.</p>	R1-5
<p>5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations.</p>	R1-6
<p>5. In order to be able to make sensible decisions which include participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.</p>	R1-7

2. Response to Comments

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2. Response to Comments

R1. Response to Comments from Critical Action and Social Advocacy (CASA) Pitzer, dated June 15, 2022.


- R1-1 Responses to the Critical Action and Social Advocacy (CASA) Pitzer's comments are addressed in response to Comments R1-2 through R1-6 below.
- R1-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute as substantial evidence of a potential environmental impact of TOP 2050.
- R1-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R1-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R1-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R1-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R1-7 Responses to CASA Pitzer's comments are addressed in response to Comments R1-2 through R1-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R2 – Inland Equity Partnership (1 page)

	
<p>Inland Equity Partnership is writing on behalf of a coalition of organizations representing the policy, academic, environmental, business, social justice, and affordable housing communities. Inland Equity Partnership is an anti-poverty advocacy coalition who has identified health care and housing as the two primary drivers of poverty. One of our areas of focus is to preserve affordable housing for low income persons living in Riverside and San Bernardino Counties. The Draft TOP2050 General Plan contains deficiencies.</p>	R2-1
<p>The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). TOP2050 will have significant and unavoidable impacts:</p>	R2-2
<p>5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance.</p>	
<p>5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act</p>	R2-3
<p>5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations.</p>	R2-4
<p>5. In order to be able to make sensible decisions which include participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.</p>	R2-5
<p>Further, without the adoption of a compliant housing element and the needed policy changes to bring the city into compliance with state law, the City of Ontario is losing local control of the development of housing in the city. Inland Equity Partnership is urging the city to delay the draft of the TOP2050 plan until it completes a compliant housing element.</p>	
<p>If you have any questions or would like to discuss our comments, please contact Maribel Nunez at (562) 569-4051 or maribel@inlandequitypartnership.org</p>	

2. Response to Comments

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2. Response to Comments

R2. Response to Comments from Maribel Nunez, Inland Equity Partnership, dated June 23, 2022.

- R2-1 Responses to Inland Equity Partnership's comments are addressed in response to Comments R2-2 through R2-4 below.
- R2-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R2-3 See Section 2.2, *Summary of General Responses*, and Section 2.2.2, *Nonrenewal of Williamson Act Contracts*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542).
- R2-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R2-5 The Housing Element is required to be updated on an eight-year cycle, as established by the State of California. The City's 6th cycle (2021-2029) Housing Element Update was recently adopted on March 1, 2022. Responses to Inland Equity Partnership's comments are addressed in response to Comments R2-2 through R2-4 above.

2. Response to Comments

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2. Response to Comments

LETTER R3 – Susan A. Philips, Professor of Environmental Analysis, Robert Redford Conservancy at Pitzer College; Randy Bekendam, Executive Director, Southern California Agricultural Land Foundation; Ana Gonzales, Director, Center for Community Action and Environmental Justice; Anthony Noriega, Director, District 5, League of United Latin American Citizens (LULAC) de Inland Empire; and Kim Floyd, Conservation Chair, Sierra Club San Geronio Chapter (15 pages)

**Robert Redford
CONSERVANCY**
for Southern California Sustainability
PITZER COLLEGE



LEAGUE of UNITED LATIN
AMERICAN CITIZENS



June 23, 2022

Dear Kimberly Ruddins,

Comment to City of Ontario re: General Plan and CCAP Supplement

We begin by requesting additional time for the DEIR comments and public discussion of the Supplemental General Plan and the Community Climate Action Plan. Ontario is undergoing an unprecedented amount of change. The shift to elections by district and accompanying redistricting process have converged with TOP 2050 process and accompanying Community Climate Action Plan, and all of these have converged with the pandemic. The combination of the pandemic environment, potential shifts within Council, and these key planning matters with summer deadlines, threatens to compromise meaningful community engagement and the integrity of these processes, as well as creating a sense of overwhelm for residents and City staff. This time of instability within the City's landscape and structure is a challenging moment in which to co-locate such complex planning and governance processes. We therefore request the following:

- **One additional year** to read, process, meet about, interpret, and respond to the DEIR and CCAP documents, in order to separate them in Ontario's calendar from the redistricting process. This will increase the likelihood of more democratic engagement with the General Plan.
- **A separation of between 3-6 months** between the Community Climate Action Plan and the General Plan supplement. Though interlinked, these are two separate and lengthy documents. To expect residents and community members to tackle both at the same time compromises Ontario's values of community-based deliberation and engagement.

R3-1

2. Response to Comments

<p>Though they exist in tandem, each of these documents is substantial and would benefit from separate deliberation.</p> <ul style="list-style-type: none">● A separation of between 3-6 months between the redistricting process and the General Plan and CCAP deliberations. <p>More time will also allow the City of Ontario to include a more robust engagement with the environmental justice element (described below), which is critical to the success of Ontario's future planning.</p>	R3-1 cont.
<p>Prior to adequate environmental justice engagement, we request a halt to approvals for all industrial development projects for a period of one year, until emissions and GHG numbers have been recalculated (see request below) and planning processes are completed with an EJ element in a robustly democratic, and temporally separate, manner.</p> <p>Ontario residents have expressed repeated surprise and dismay regarding the greenlighting of industrial projects adjacent their neighborhoods since the pandemic began. Ontario's landscape is changing at an unprecedented rate, particularly within South Ontario. Ontario communities have been distracted by health, social, and economic challenges and thus need time to catch up. Adding more time to the calendar in order to treat these issues separately will ensure that greater community participation and democratic decision-making accompanies these processes.</p>	R3-2
<p>Aside from timing, several additional factors are of concern with the DEIR and CCAP. We review them below.</p> <p>CCAP</p> <p>The CCAP excludes heavy duty trucks that travel to Ontario's warehouses within its local GHG emissions. We understand that you are using the United States Community Protocol for this analysis and encourage you to contribute a larger portion of the document to the Global Protocol for Cities. Ontario's key role in the global economy has been intentional, and truck, air, and train traffic that increase GHG and pollutants emissions need to be weighed more heavily.</p>	R3-3
<p>According to your CCAP storymap, "Emissions from fuel use at major industrial facilities are permitted by state and regional air quality authorities. Stationary source emissions are informational and are not included in the community total." It is unclear to us whether the City is considering warehouses as stationary source infrastructure, and we note that warehouse infrastructure has been intentionally created and approved by the City over a period of decades. The City is therefore responsible for the consequences of that municipal decision-making. Due its significant, and intentional, role within the global economy, Ontario should claim responsibility for major and growing warehouse clusters that will contribute thousands of additional HDDT trips per day. According to the Global Protocol, cited in the CCAP, "The first step for cities to</p>	R3-4

2. Response to Comments

<p>realize their potential is to identify and measure where their emissions come from -- you can't cut what you don't count."</p>	R3-4 cont.
<p>It is of critical importance to create control measures for warehouse growth and associated HDDT within the CCAP and TOP 2050.</p>	
<p>We therefore request the following:</p> <ul style="list-style-type: none">• The addition of control measures for industrial development and accompanying HDDT VMT.• A more thorough review of industrial uses numbers, and in particular warehouse and logistics clusters, within the TOP and Climate Action Plan.	R3-5
<p>There are always two pieces of any emissions reduction strategy when calculating an emissions inventory:</p> <ol style="list-style-type: none">1. Cleaner technology (i.e., emissions rate)2. Emissions activity (i.e., emissions growth management measures)	
<p>Intrinsically, emissions can be reduced by reducing the rate of emissions per activity or by reducing the activity generating the activity, or any combination. The City of Ontario does an admirable job of outlining technologies, smart growth patterns, active transportation corridors, and so on, which reduce the emissions rate. The incentives and emissions reductions from the introduction of these programs will help to achieve emissions reductions over the course of TOP.</p> <p>However, such community based solutions and zero emissions technologies are only half of the puzzle. Programs that aim to reduce industrial emissions activity do not appear to be part of any of the proposed TOP or CCAP. None of the strategies listed in Table ES-4 List of 2022 CCAP GHG Reduction Strategies include a control measure for logistics growth.</p>	R3-6
<p>The impact of the logistics infrastructure within the City of Ontario needs to be addressed as a specific category: this includes the airport, freight train, and warehouse logistics activity clusters that Ontario continues to build out at an unprecedented rate.</p>	R3-7
<p>According to the WarehouseCITY tool (https://radicalresearch.shinyapps.io/WarehouseCITY/), there are ~592 warehouses (likely an undercount due to inaccuracies within the assessor's labeling) with 120M sq. ft. and 80k truck trips. This is shown below in Figure 1.</p>	R3-8

2. Response to Comments

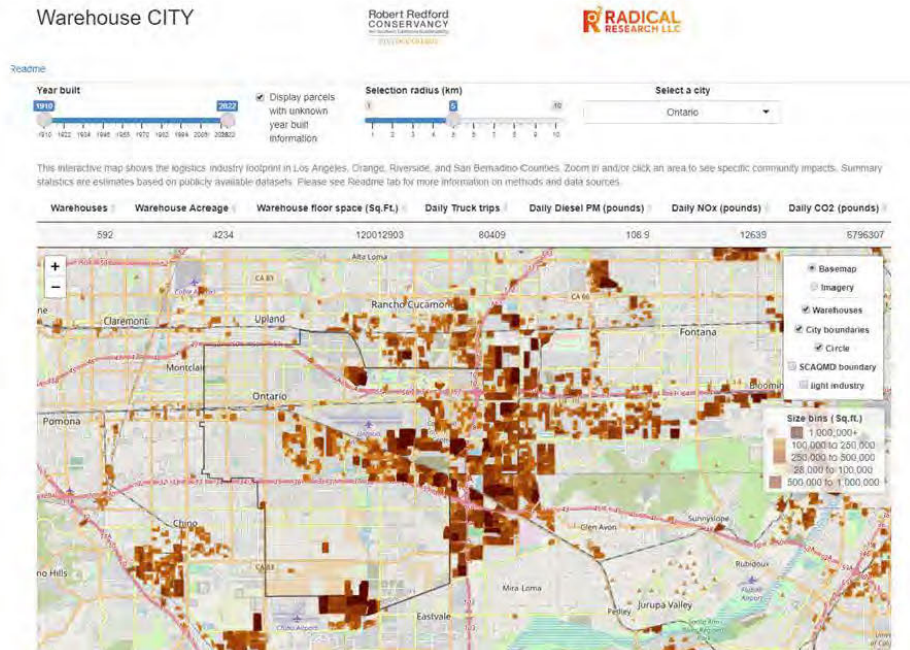


Figure 1 – Warehouses in Ontario in Warehouse CITY alpha v1.0.5

We note that the square footage of warehouses has doubled since 2008, which means that the associated diesel VMT has also doubled, resulting in double the GHG emissions from this emissions category. Table 1 of the CCAP shows a combined 19% decrease in light-duty vehicles and a 25% increase in heavy-duty vehicles to get the -9% reductions, based on an emissions factor analysis per mile that is weighted heavily towards light-duty vehicles. Table 2 shows a 9% increase in VMT (1.96B to 2.15B), whereas Table 5 suggests that 2008 and 2019 heavy-duty VMT changed by only 2% from 2008-2019 and went down by 25% in 2016. Based on the forms of data to which we have access, these outcomes seem unlikely. In addition to the WarehouseCITY tool, CalTrans truck traffic volumes at key Ontario intersections along the 60 FWY demonstrate steady increases in HDDT VMT until 2020, when the pandemic disrupted supply chain patterns as shown in Figure 2 below.

R3-8
cont.

2. Response to Comments

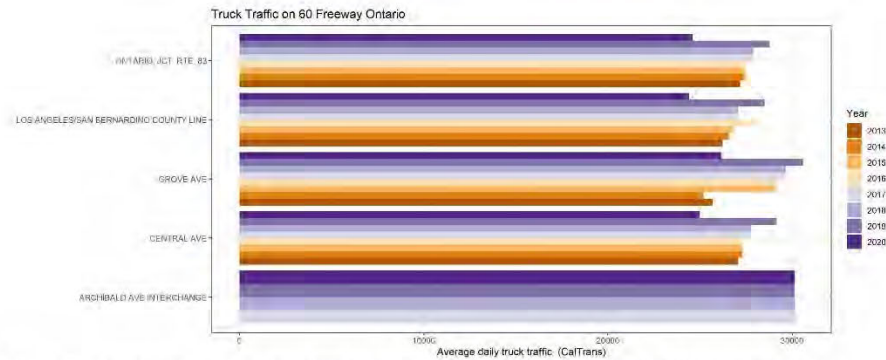


Figure 2. Truck traffic counts along the 60 Freeway from 2013-2020. Data accessed from CalTrans at <https://dot.ca.gov/programs/traffic-operations/census>

If we restrict the WarehouseCITY map to warehouses built from 2008 - 2019, we note an additional 249 warehouses (50M sq.ft.), and an additional ~33.8k truck trips daily. The tool estimates that truck trips went up 88%, not the 2% claimed by the City. As a result of the discrepancy in diesel VMT estimates, we believe the City GHG estimates are underestimated and incorrectly miss the growth in MTCO_{2e} due to increased warehouse land-use. Our tool suggests that MTCO_{2e} from trucks associated with new warehouses is 2,857,003 CO₂ pounds daily, 365 days/2000 lbs. This equals ~0.52 MTCO_{2e} growth from freight. This is reasonably consistent with your Table 5 estimate of 0.74 MTCO_{2e} in 2008 and our 88% increases. In other words, the rate should go from 0.74 MTCO_{2e} in 2006 to 1.27 MTCO_{2e} in 2019 due to the increased HDV traffic, which mostly offsets declining light-duty vehicle emissions over the same time period. This leads to a 2% increase in transportation emissions, rather than a 13% decline.

Our cumulative impact tool shows that the current rate of GHG emissions associated with Ontario warehouses is in excess of 6.75 million pounds of CO₂ per day, given an assumed HDDT VMT of 25 miles per day. According to current standards, the social cost of carbon is \$51 per ton. This calculation was developed to provide guidance for federal clean air policy. This number is based on older mathematical models and there is wide agreement that it is a radical underestimation of carbon's true cost. Utilizing the \$51 standard, the annual estimated cost of carbon stemming from Ontario's warehouse infrastructure already exceeds \$63 million per year. This number will grow by 3.8% per year if warehouse VMT follows current growth projections. There is no indication here of a decreasing or flat trend in truck VMT. Clearly, this has had significant consequences in terms of pollution and GHGs, as well as traffic, noise, safety, and congestion.

In terms of GHGs, our tool estimates that **Ontario warehouses have increased CO₂ emissions by approximately 1.27 MTCO_{2e} per year.**

R3-8
cont.

2. Response to Comments

It is essential that Ontario consider the benefits of minimizing or halting the further growth of logistics infrastructure to reduce both carbon and pollutant detriments. Ontario already has an outsized logistics-based footprint. Despite the perceived benefits that this infrastructure brings, the costs remain high. Halting emissions growth of logistics based industrial development is the cheapest way to reduce projected GHGs as well as criteria pollutants.

- We request that the numbers of pollution emissions and GHGs be redone to account for logistics infrastructure and that Ontario analyze its TOP and CCAP based on the new numbers that result from that calculation.
- We request that automobile VMT and freight VMT be separated out within the body of the CCAP and TOP.
- 2020 should be excluded from any cumulative calculation of VMT.
- Ontario should recalculate and account for the social cost of carbon emissions associated with its warehouse infrastructure.
- Analyze the logistics sector and projected warehouse growth through an environmental justice lens.

R3-9

Current numbers give an inflated impression of the health of the City. For example, page 10 of CCAP Background (page 32 of the document) cites California EnviroScreen Data indicating that all neighborhoods are in the 50th percentile but then only a vague reference that some kinds of pollutants are "higher" and clustered in poorer neighborhoods. It is important to cite the actual numbers here, which are in the high 90s, and to create mitigation measures for this exposure. The CCAP currently minimizes the environmental justice impact of logistics as well as indicating that it is unavoidable; this minimizes the contribution of logistics to GHG and pollutants and absolves the City of responsibility for that decision-making. Recalculation will likely demonstrate offsets to gains in other areas, and compromise the achievement of Ontario's goals of sustainability and resilience.

R3-10

Additional Traffic Issues

We understand that the San Bernardino Traffic Analysis Model (SBTAM) was utilized for TOP 2050 but we note the lack of an independent traffic analysis specifically for the City of Ontario. We are concerned that not to do so leaves analysis and decision making in the hands of private entities (developers and environmental consultants) associated with specific projects rather than the general plan as a whole. This ceding of the City's proper authority is problematic. It fundamentally removes the public voice from the planning process that comes from concerted outreach associated with the General Plan, the breadth of which envelopes a much broader scope than any one individual siting decision.

R3-11

2. Response to Comments

We request a traffic analysis, particularly regarding logistics and including mitigation suggestions.

We request more information on the specific measures:

- M1-2: Mitigation of Impacts. We require development to mitigate its traffic impacts.
- M1-6: Reduce Vehicle Miles Traveled. We will strive to reduce VMT through a combination of land use, transportation projects, travel demand management strategies, and other trip reduction measures in coordination with development projects and public capital improvement projects.
- M4-2: Regional Planning. We work with regional and subregional transportation agencies and adjacent cities to plan and implement goods movement strategies, including those regional truck routes, plans and projects that improve mobility, deliver support the efficient movement of goods efficiently, and minimize negative environmental impacts.
- M4-4: Environmental Considerations. We support both local and regional efforts to reduce/eliminate the negative environmental impacts of goods movement through the planning and implementation of truck routing and the development of a plan to evaluate the future needs of clean fueling/recharging and electrified truck parking.
- M4-5: Air Cargo. We support and promote an LAONT that accommodates 1.6 million tons of cargo per year as long as the impacts associated with that level of operations are planned for and mitigated.
- M5-2: Land Use Compatibility with Regional Transportation Facilities. We work with LAWA ONT, railroads, Caltrans, SANBAG SBCTA, and other transportation agencies to minimize impacts.

R3-12

We request that a traffic analysis be included, and that the 2020 pandemic numbers be excluded from this traffic analysis.

One of the most troubling aspects of TOP 2050 is Impact 5.17-2: "Because the VMT impact is citywide, mitigation measures to reduce VMT would need to focus on changing or improving the citywide travel patterns, transportation network, or infrastructure. Given the uncertainty of the effectiveness of implementing these types of mitigation measures at a citywide level and of their effectiveness at reducing citywide VMT, these measure are not considered feasible for TOP 2050."

R3-13

The uncertainty of mitigation measures in the face of citywide VMT impacts is not a reason to claim infeasibility. In fact the opposite is true—uncertainty creates an even more pressing

2. Response to Comments

imperative to act in the interest of the common good of Ontario residents, particularly for EJ communities.

We request that the City of Ontario develop specific measures, including limits on industrial HDDT VMT, limits on industrial construction, downsized development plans, and other measures to mitigate this increase in VMT, particularly of HDDT.

R3-13
cont.

Lack of inclusion of an Environmental Justice Element

Ontario adopted its Environmental Justice Element in early 2022. It is not possible to find consistency or robust engagement with EJ principles as required by Planning and Zoning Law within the TOP or CCAP document. While we understand that the EJ element is not required to be its own section, we see little evidence of meaningful engagement with EJ principles in the TOP 2050 or in the CCAP.

For example, TOP 2050 does not make mention of SB 1000 the State's Environmental Justice Local Land Use planning. A history of poor and discriminatory land use practices has put the majority of polluting industries in the backyards of the most vulnerable local communities, directly next to homes and schools. The DEIR and CCAP both state that its focus would be on technical updates that conform to the policy plan and State laws related to environmental justice but lack consistency. Per SB 1000, "The bill would also require the environmental justice element, or related environmental justice goals, policies, and objectives integrated in other elements, to identify objectives and policies to reduce the unique or compounded health risks in disadvantaged communities, as specified, identify objectives and policies to promote civil engagement in the public decision making process, and identify objectives and policies that prioritize improvements and programs that address the needs of disadvantaged communities."

R3-14

TOP 2050 mentions EJ but lacks true genuine EJ elements. The Ontario Plan does not demonstrate what mitigation efforts will take place to minimize health risks to disadvantaged communities. In fact, it mentions that emissions from construction and operation are "UNAVOIDABLE cumulative health risks", per AQMP:

"Buildout of the proposed land use plan associated with TOP 2050 could contribute to an increase in frequency or severity of air quality violations and delay attainment of the AAQS or interim emission reductions in the AQMP, and emissions generated from buildout would result in a significant air quality impact. Therefore, like the Approved Project, the Proposed Project would be inconsistent with the AQMP. As identified in Impact 5.3-3, the Proposed Project would result in a substantial increase in VOC compared to the Approved Project. Therefore, TOP 2050 would result in a substantial increase in magnitude of impacts compared to the Proposed Project."

R3-15

According to the SCAQMD's MATES tool, all Ontario zip codes are in the top 90+% of cancer risk, largely as a result of their proximity to many of the busiest heavy-duty truck routes in the

2. Response to Comments

state. Additionally, California EnviroScreen data place Ontario in the high 90th percentile (up to 99%) in pollution levels, as stated above.

R3-15
cont.

In addition to the late adoption of the Environmental Justice Element, we are concerned that the community process related to the TOP/CCAP has taken place during the pandemic. We therefore request

- A delay of one additional year, as stated above, so that the City can further incorporate EJ element into the TOP/CCAP
- A delay of one additional year, as stated above, so that the City can engage in more robust community outreach regarding its EJ elements within the CCAP and TOP;
- The inclusion of a breakout environmental justice element or clear demarcations of EJ-relevant analysis within the existing sections of the TOP and CCAP.
- Changes to CCAP as stated above (control measures for industrial development)
- A redesign of the General Plan to maximize green buffers between housing and commercial/industrial developments
- No more construction of warehouses near residential developments
- Truck routes that avoid residential communities

R3-16

As measured against the AG's example linked above, Ontario's EJ element needs additional development. The City of Ontario clearly requires extra time for analyzing the additional elements of the plan through EJ principles.

Climate Concerns: Further Consideration of Nature-Based Solutions

In the twelve years since the 2010 General Plan, the exigencies of climate change have begun to hit us at an unprecedented rate. There has been a great deal of research and action, globally, nationally, and at the state-level, regarding the value of nature-based solutions to climate change. We are happy to see an accounting of that within the Land Sequestration section on p 39. We commend the City on its tree plan for the Euclid median and for the thousand trees planted since 2019. We request that the City conduct a shade equity tree analysis in the neighborhoods south of Holt. We would like to see two additional things:

R3-17

- 1) An accounting of GHGs in the TOP build out and conversion of all farmland to developed land
- 2) An accounting of GHGs within the TOP with a significant area ~300 acres (described below) reserved for regenerative agriculture that will act as a carbon sink to offset additional emissions associated with development

R3-18

2. Response to Comments

Predicted drought, increased heat, increased wind and other extreme weather events, supply chain disruptions, economic instability, unprecedented loss of biodiversity, and food shortages will begin to challenge our collective survival within the next 5-10 years. The beginning presence of all these factors has already been felt. The windstorm of last January is just one example of this.

R3-18
cont.

In order for Ontario to be the most effective city, it can be in creating a healthy airshed and prioritizing the needs of EJ communities and all its residents, prioritizing nature-based solutions to offset Ontario's massive logistics footprint within the TOP 2050 is critical.

We are happy that the CCAP involves the mention of natural and working lands in the form of tree planting, urban greenspace, and the Great Park, but we would like to see strategies associated with this component in the TOP—and particularly the inclusion of prime farmland. Ontario's massive footprint could begin to be offset by maximizing the green benefit, food production, biodiversity conservation, and carbon sequestration potential of the "donut hole." **TOP 2050 should include provisions for carbon sequestration in open and working lands as well as urban tree planting.** Two major land-based assets distinguish Ontario from nearby cities: Ontario's enviable tree canopy along the Euclid median and the amount of open farmland left in South Ontario. Just as trees and the historic Euclid median are treated as a heritage landscape that contributes to community health, the former agriculture preserve—much of which remains intact—could be strategically reenvisioned to increase community and environmental health and to begin to offset logistics-related omissions.

R3-19

R3-20

We request that you create alternate plans for open and working lands within the TOP, including agriculture, that are appropriate to Ontario's landscape.

"The CCAP projects that all agricultural land is projected to be developed with urban uses by 2050, reducing emissions from the city's agricultural sector to zero emissions in 2050." This statement excludes the potential of farmland to create clean energy through agrivoltaics and to act as a natural carbon sink that could provide carbon sequestration instead of emissions.

We request that you analyze several scenarios that weigh the carbon benefits of retaining agricultural land, particularly in the area below The Great Park.

R3-21

Sustainable agriculture, along with other open and working lands, create carbon negative/carbon sink scenarios as well as contributing to community health, water filtration, and pollution remediation. This will be particularly important as heat and drought increase. In addition, farmland can be utilized for the generation of energy, which could help to offset new strains on the grid, as well as local sources of food.

Area of Controversy: The Importance of Prime Farmland

Agriculture is such an important part of Ontario's legacy. Unlike many cities in Southern California, Ontario still has a portion of its land as agriculture and remains in productive use.

R3-22

2. Response to Comments

Ontario can continue to showcase its roots as an agricultural center. In 2015, the zoning maps shows more than six square miles zoned for agriculture. Now fewer than seven years later, the Ontario Plan ignores this legacy and plans to re-zone the agricultural lands as industrial lands. Pasadena and Pomona wish they would have set aside lands to showcase their heritage and invest in their future. Ontario has the chance, as well as the chance to showcase continued links between Ontario and innovation in educational facilities, architectural innovation, and reformulated sustainable farms.

R3-22
cont.

From an economic perspective, prime farmland has tremendous value that cannot be understated. While economic benefit cannot compete with the cold, hard cash that warehouse development offers, think of the public health benefit and dollars saved through the decrease of intergenerational asthma that now runs in Ontario families, think of chronic, diet-related illnesses such as diabetes and heart disease.

Section 1.7 outlines "Areas of Controversy," noting that "In accordance with Section 15123(b)(2) of the CEQA Guidelines, the EIR summary must identify areas of controversy known to the lead agency, including issues raised by agencies and the public." We contest that statement that "The City of Ontario has no knowledge of expressed opposition to the Proposed Project." Despite the outlined community process and feedback contained therein, the opposition to the loss of agricultural land to industrial development within the City of Ontario is growing. The City received over 1000 comments of opposition to the Ontario Logistics Center Specific Plan and destruction of associated farmland, along with comments from 25 agencies in opposition to the proposed project. In addition, residents of Ontario affiliated with Ontario for Agriculture collected almost 2000 signatures from Ontario residents in order to engage in an attempted ballot referendum that was meant to preserve this farmland. While this fell short of the over 8000 signatures required to successfully pass the referendum, 2000 signatures within 30 days represents a significant opposition to the loss of agricultural land. **We, therefore note that the City of Ontario needs to take into account and mitigate for this area of controversy.**

R3-23

While we know that all farmland has already been rezoned to residential (and industrial/commercial), some of the rezoned land includes Prop 70 land and Williamson Act Land.: "Impact 5.2-1. The Proposed Project would not convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance in the City of Ontario to non-agricultural use. Less than significant The City of Ontario's land use plan no longer designates agricultural land uses in the City. No mitigation measures are required. Less than significant Impact 5.2-2. The Proposed Project would not conflict with existing zoning for agricultural use, or a Williamson Act contract. Less than significant The City of Ontario's land use plan no longer designates agricultural land uses in the City. No mitigation measures are required. Less than significant." We disagree that no mitigation is required, both because Prop 70 land is planned for development in TOP 2050 and because of the intrinsic importance not just of farmland preservation but of farmland transformation.

R3-24

The value of open and working lands is only now being realized as we face the climate crisis head on. Ontario could become a leader in California's Nature's Based Solution to fight climate

2. Response to Comments

change and conserve biodiversity. In October 2020, Governor Newsom signed an Executive Order N-80-20 committing California to conserving 30% of land by 2030. The 30x30 plan is an international, national, and state-wide movement to establish a current baseline for conserved areas—including farmland—as a nature-based solution to climate change.

The proposed conversion of working lands to development and in particular industrial uses within TOP 2050 is an irreversible change that goes against Governor Newsom and CNRA's call for the state to "Expand Nature-Based Solutions." In Executive order N-82-20 Governor Newsom calls for protecting working lands as climate-smart solutions to worsening climate change.

R3-24
cont.

According to the California Department of Conservation, this TOP represents a permanent decrease in the amount of farmland in the state of California. Development on all prime farmland is an unwise decision when climate-related disasters disrupt the supply chain, when predicted climate-related food shortages threaten the well-being of thousands in our already food insecure region, when heat and drought will strain water and energy systems to the limit, and when shrinking biodiversity threatens the very fabric of our ecosystems. We need to prioritize building health-promoting infrastructure rather than pollution promoting infrastructure, like warehouses. With poor air-quality days, days over 90 degrees, and traffic all increasing, we need to protect, preserve, and invest in green spaces and working lands that can act as regional lungs and help communities breathe more easily.

We request that Ontario's General Plan retain an Agricultural Heritage and Innovation Zone, with conservation easements for agriculture deeded in perpetuity.

We request that this Agricultural Heritage and Innovation Zone include all Prop 70 lands remaining in Ontario.

Specifically, we request the creation of at least a 350-acre Agriculture Preserve south of the great park to Eucalyptus. This farmland preserve area could include some low- and possibly medium-density housing.

R3-25

This zone would go from the southern border of the Great Park to the North, from Sultana to the West, to the Cucamonga Creek wash, which is a prominent natural boundary, to the East. The zone could incorporate low-medium density housing and includes appropriate uses within the airport overlay zone.

The agricultural heritage and innovation zone would act as a natural and compelling buffer between residents and the warehouse district. That strip includes 40 acres of Prop 70 land and will act as an excellent neighbor to the Great Park. The General Plan should also include the remaining Prop 70 lands as part of the Agricultural Preservation Zone.

One 2004 [City of Ontario Planning document](#) provides an analysis of the historic significance of the properties and concludes that "the district is significant at the local,

R3-26

2. Response to Comments

regional, and state levels" (p 65). That document suggests to "Consider purchasing one operating dairy facility or coordinating with a dairy farmer to continue to operate the facility as a hands-on research facility, living history museum, or educational facility that would be open to the public for a nominal fee" (p 70). In other words, the document provides compelling evidence for valuing this farmland as Ontario's cultural heritage, even before the current climate-induced interest in nature-based solutions have begun to elevate this profile.

There is expected to be up to \$1 billion in state funding for projects that fall within the 30x30 strategy. The elimination of prime farmland, as proposed, limits Ontario's capacity to deliver a healthy quality of life to its residents and be a leader in innovating solutions. Climate-related challenges are not in our distant future. They are happening now. Power outages, extreme heat and wind events, drought, natural disasters—all of these are straining our systems beyond capacity. Increased heat, lack of water percolation, loss of biodiversity, and increased fossil fuel consumption associated with development will only exacerbate these problems.

See the 30x30 report in its entirety.

https://resources.ca.gov/-/media/CNRA-Website/Files/Initiatives/Expanding-Nature-Based-Solutions/FINAL_DesignDraft_NWL_100821_508-opt.pdf

"Climate smart cropland management can also contribute to climate resilience through:

- Supporting resilient communities and natural systems - Agricultural lands provide food security to California, and can support climate resilience through provision of habitat, food management, pollination services, buffering against fires, and groundwater recharge.⁷³ These outcomes benefit all Californians, and are particularly important for rural, climate-vulnerable communities*
- Supporting resilient economies - Agriculture is a critical sector of California's economy. Climate smart cropland management promotes a resilient agricultural economy, can buffer communities from extreme heat, and provide local food sources for the future. "*

Ontario is in an enviable position vis a vis other cities due to the open land assets that it is currently slating for development. The Great Park plan is a wonderful start, but we request that the following be considered as the opposition to farmland loss within the city is growing.

While we know that Prop 70's implementation in San Bernardino County has a complex history, Prop 70 utilized approximately \$20M of public money to protect farmland. It is a violation of the public trust to piggy-back on an EIR process that is also likely to facilitate rezoning of Prop 70 land in the future. The Planning and Conservation League (PCL) in 2006 wrote to request protection of and proper use for Proposition 70 funding.

CEQA Considerations for DEIR TOP

R3-26
cont.

R3-27

R3-28

2. Response to Comments

The "policy of the state" reflected in the California Environmental Quality Act is that projects with significant environmental impacts *may not* be approved "if there are feasible alternatives or feasible mitigation measures available which would substantially lessen the significant environmental effects . . ." (Pub. Resources Code § 21002.) More specifically, CEQA states:

Pursuant to the policy stated in Sections 21002 and 21002.1, no public agency shall approve or carry out a project for which an environmental impact report has been certified which identifies one or more significant effects on the environment that would occur if the project is approved or carried out

unless ...:

(a) . . . (3) Specific economic, legal, social, technological, or other considerations . . . make infeasible the mitigation measures or alternatives identified in the environmental impact report.

(Pub. Resources Code § 21081.)

It is settled law that:

CEQA contains *substantive* provisions with which agencies must comply. The most important ... is the provision requiring agencies to deny approval of a project with significant adverse effects when feasible alternatives or feasible mitigation measures can substantially lessen such effects.

(*Sierra Club v. Gilroy City Council* (1990) 222 Cal.App.3d 30, 41, italics added.)
Indeed, in a case decided on July 7, 2006, a court analyzed the "substantive mandate" of CEQA, then set aside a college campus master plan project because there was not substantial evidence to support a college district's findings of economic and legal infeasibility of mitigation. (*County of San Diego v. Grossmont-Cuyamaca Community College* (2006) –Cal. App.--, slip opinion, pp. 18-29.)

"Feasible" is defined as "capable of being accomplished in a successful manner within a reasonable period of time, taking into account economic, environmental, social, and technological factors." (Pub. Resources Code § 21061.1.) The definition *does not* require the agreement of the project applicant. "Each public agency shall mitigate or avoid the significant effects on the environment of projects that it carries out or approves *whenever* it is feasible to do so." (*Lincoln Place Tenants Ass'n v. City of Los Angeles* (2005) 130 Cal.App.4th 1491, 1508, emphasis added.)

TOP 2050 has impacts that can and must be mitigated. Land use and transportation have a strong influence on people's lifestyles and in promoting healthy communities as well as exposing people to significant detriments in the form of pollution emissions, traffic, noise, safety concerns, and congestion. While Ontario describes itself as a "balanced city," the amount of investment in logistics-based land uses takes up a disproportionate amount of the City's landscape. We are alarmed that the City has adopted a design that places housing next to industrial developments, and we urge the City to consider agricultural land buffers and other mitigation measures for the impacts of the plan. These are inappropriately labeled as

R3-28
cont.

2. Response to Comments

insignificant, but they in fact may be avoided by properly adopting mitigation measures and alternatives.

Signed,

Susan A. Phillips, Robert Redford Conservancy
Professor of Environmental Analysis

Randy Bekendam, Executive Director
Southern California Agricultural Land Foundation

Ana Gonzalez, Director
Center for Community Action and Environmental Justice

Anthony Noriega, Director
District 5, LULAC de Inland Empire

Kim Floyd, Conservation Chair
Sierra Club San Geronimo Chapter

R3-28
cont.

2. Response to Comments

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2. Response to Comments

R3. Response to Comments from Susan A. Philips, Professor of Environmental Analysis, Robert Redford Conservancy at Pitzer College; Randy Bekendam, Executive Director, Southern California Agricultural Land Foundation; Ana Gonzales, Director, Center for Community Action and Environmental Justice; Anthony Noriega, Director, District 5, LULAC de Inland Empire; and Kim Floyd, Conservation Chair, Sierra Club San Gorgonio Chapter dated June 23, 2022.

R3-1 The Commenter requests additional time for the Draft SEIR review period of TOP 2050 and the Community Climate Action Plan, as well as a separation of 3 to 6 months between the two documents. The public review period of 45 days, beginning on May 9, 2022, and ending on June 23, 2022, met CEQA's requirement of 45 days, pursuant to CEQA Guidelines Section 15105(a) for Draft EIRs submitted to the State Clearinghouse for public review by State agencies. The request for additional time for TOP 2050 and the Community Climate Action Plan (CCAP) 2022 update will be forwarded to the decision-makers for their review and consideration. Additionally, see Section 2.2.3, *Environmental Justice Policies*, for a description of the outreach conducted for TOP 2050.

R3-2 The request for an industrial/warehouse mortarium within Ontario is noted and will be forwarded to the decision-makers for their review and consideration.

See response to Comments R3-3 through R3-10 regarding emissions modeling for the CCAP and SEIR. Greenhouse gas (GHG) emissions modeling conducted for the CCAP and Draft SEIR was conducted using agency-approved protocols, including the U.S. Community Protocol and the Global Protocol. Section 5.8, *Greenhouse Gas Emissions*, of the Draft SEIR provides an accurate assessment of the existing, Approved Project, and Proposed Project GHG emissions in the City. No changes to the SEIR are warranted.

See response to Comments R3-14 through R3-16 and Section 2.2, *Summary of General Responses*, Section 2.2.3, *Environmental Justice Policies*, regarding environmental justice policies in the Policy Plan. The City has opted for an “environmental justice in all policies” approach in TOP 2050 to ensure the topic is present alongside the multitude of issues and topics that affect our residents and resources. It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.

In accordance with CEQA Guidelines Section 15163, the CEQA baseline for this SEIR is the Approved Project. The Draft SEIR evaluates the potential impacts from the change in land use designations of the current TOP compared to TOP 2050, including the increase in Industrial and Business Park designations, within Ontario Ranch and the Original Model Colony (OMC) of Ontario.

R3-3 The Commenter is incorrect that the CCAP excludes heavy-duty trucks that travel to Ontario's warehouses. The CCAP is based on transportation modeling conducted by Fehr & Peers for TOP 2050. Vehicle miles traveled (VMT) modeling is consistent with the City

2. Response to Comments

of Ontario Resolution Adopting Vehicle Miles Traveled Thresholds, which was adopted in June 2020. The City's guidelines require use of the San Bernardino Traffic Analysis Model (SBTAM) to evaluate how a proposed project changes VMT in the City of Ontario. The SBTAM origin-destination (OD) modeling accounts for both passenger vehicle and truck traffic in Ontario that is generated by land uses within the City. As a result, the GHG modeling in the CCAP and Section 5.8, *Greenhouse Gas Emissions*, of the Draft SEIR accounts for emissions from both passenger vehicles and trucks generated by land uses in the City.

- R3-4 The Commenter cites the CCAP storymap citation regarding industrial facilities. It should be noted that for air quality and GHG emissions modeling "industrial" sector emission specifically reference emissions that are regulated by an Air District. This is also known as permitted emissions and includes emissions sources such as boilers, emergency generators, gas stations, and other emissions from equipment that requires a permit from an Air District. Stationary source infrastructure does not include cargo handling equipment and truck idling emissions that are area source emissions, which are not directly regulated by an Air District.

The CCAP GHG emissions inventory and forecasts include warehouse/industrial mobile-source and area source emissions. Mobile source emissions from light-duty vehicles and heavy trucks associated with warehouse/industrial properties and activities are included under the on-road transportation sector emissions. Emissions from cargo handling equipment are included under the off-road equipment sector. The energy used by these facilities is included in the nonresidential energy sector emissions.

- R3-5 The Commenter is requesting additional control measures for industrial development and truck VMT. See response to Comment R3-6.

See response to Comment R3-3. The Draft SEIR and CCAP include emissions from industrial/warehouse developments, including truck VMT. It should be noted that Appendix C-17 of the Draft SEIR includes a detailed accounting of light-heavy duty trucks (LHDT), medium-heavy duty trucks (MHDT), heavy-heavy duty trucks (HHDT), and passenger vehicle VMT provided by Fehr and Peers and modeling with SBTAM. The primary driver for demand for new warehouse is e-commerce. E-commerce requires three times the warehouse space to carry a wider variety of goods expected to be delivered within two days to local consumers. Warehousing that is close to consumers conserves fuel and improve efficiency, and ultimately reduces VM. The California Air Resources Board's (CARB) Mobile Source Strategy⁶ outlines State strategies to reduce emissions from goods movement and trucks, this includes the Truck and Bus Regulation, Heavy-

⁶ California Air Resources Board. 2020, October 28. 2020 Mobile Source Strategy.
https://ww2.arb.ca.gov/sites/default/files/2021-12/2020_Mobile_Source_Strategy.pdf

2. Response to Comments

Duty Omnibus Regulation, Advanced Clean Truck Regulation, and the proposed Advanced Clean Fleets Regulation.

The CCAP includes control measures for warehouse development including requiring that most new warehouses will use electricity as their sole energy source for building appliances and equipment, and that this electricity will be generated by on-site solar panels. The CCAP also considers energy efficiency retrofits, fuel switching, and on-site solar panels for some existing warehouse developments. This is in addition to state policies that are applicable to warehouse developments, including increased supplies of electricity from renewable sources and stricter building codes requiring new buildings to be constructed to meet higher energy efficiency standards. It should be noted that in the South Coast AQMD region, warehouses that are 100,000 square feet or larger are subject to South Coast AQMD's Warehouse Actions and Investments to Reduce Emissions (WAIRE) Program (Rule 2305). Mitigation Measure AQ-1 would also ensure that new warehouse development consider additional mitigation measure that would reduce emissions below the South Coast AQMD regional significance thresholds including electric truck charging.

The Draft SEIR and CCAP considers an increase in warehousing in the City of Ontario. Emissions forecasts are based on the land use statistics included in Chapter 3, Project Description, of the Draft SEIR. The Draft SEIR considers the increase in warehousing between the current TOP and TOP 2050. Table 3-3, TOP 2050 Proposed Land Use Summary, includes a detailed summary of the nonresidential square feet and jobs for Business Park (BP) and Industrial (IND) land uses in the City. The CCAP considers the increase in emissions from existing conditions to buildout of TOP 2050.

R3-6 The SEIR is a programmatic, plan-level analysis of impacts of the City's general plan, TOP 2050. The SEIR evaluates impacts of TOP 2050 compared to the impacts associated with the current TOP in accordance with evaluates the physical environmental impacts associated with Section 15163 of the CEQA Guidelines.

It should be noted that the SEIR and CCAP evaluate emissions within the jurisdictional control of the City of Ontario. Permitted sources of emissions (i.e., emissions that are regulated under the California or Federal Clean Air Act) are outside of the City's jurisdictional control. Furthermore, such sources are subject to increasingly stringent emissions reductions programs outline in the South Coast Air Quality Management District's Air Quality Management Plan (AQMP).

The SEIR evaluated impacts associated with TOP 2050. Alternatives which would restrict the amount of Industrial and Business Park land uses allowed in the City are evaluated as an alternative to the Proposed Project in Chapter 7, *Alternatives to the Proposed Project*. Specifically, the Reduced Industrial Alternative evaluated an alternative that would reduce Industrial development on 338 acres in the City. This reduction in Industrial development would primarily occur in the four growth area and Ontario Ranch, as identified in Section

2. Response to Comments

3.4.2.3, *Areas of Change*, in the Draft SEIR. As a result, the SEIR considers a plan that reduces industrial emissions activity in the City.

R3-7 As identified in the SEIR and CCAP, the emissions inventory and forecast consider the following sectors within the jurisdiction control (see response to Comment R3-6) of the City of Ontario: On-Road Transportation, Energy, Solid Waste, Off-Road Equipment, Agriculture, Water and Water, and Land Use and Sequestration. This methodology is consistent with the U.S. Community Protocol and Global Protocol. Freight trains are not within the jurisdictional control of the City of Ontario.

Buildout of the land uses in the City of Ontario associated with the Approved Project or Proposed Project would not directly result in an increase in trains or air traffic. The Proposed Project does not result in a change in land use associated with the Ontario International Airport or railroads in the City. Freight traffic is managed independently by the Burlington Northern Santa Fe railroad and Union Pacific Railroad. Interstate freight trains and airplanes are subject to Federal Regulations and exempt from regulation by CARB and South Coast AQMD. CARB's Mobile Source Strategy⁷ provides the State's strategy to reduce emissions from airplanes and trains in California. Additionally, the Commenter hasn't provided evidence that there would be an increase in trains or airplanes as a result of the increase in Industrial use in the City.

The City does have land use authority associated with warehouses in the City. Warehouses are allowed in the Industrial and Business Park land use designations. The SEIR does consider emissions from freight and freight-related VMT in emissions modeling conducted for TOP 2050 included in Section 5.3, *Air Quality*, and Section 5.8, *Greenhouse Gas Emissions*. For example, modeling in Appendix C provides a breakdown of daily VMT for passenger vehicles, light-heavy duty trucks, medium-heavy duty truck, and heavy-heavy duty truck emission (see page C-17) as well as emissions by vehicle type (see page C-18 through C-25). Therefore, the SEIR considers the impacts associated with the increase in land designated for Industrial and Business Park. Despite the increase in truck VMT, Table 5.3-10, City of Ontario Regional Criteria Air Pollutant Emissions Forecast, shows that NO_x and particulate matter (PM₁₀ and PM_{2.5}) would decrease compared to the Approved Project. Only volatile organic compounds (VOCs) would increase compared to the Approved Project and the vast majority of those emissions would be associated with an increase in consumer product use. As identified in Section 5.3, *Air Quality*, this was identified as a significant unavoidable impact of TOP 2050.

R3-8 Modeling of passenger vehicle and heavy-duty truck VMT is based on SBTAM. This is consistent with the *City of Ontario Resolution Adopting Vehicle Miles Traveled Thresholds*, which were adopted in June 2020. The Commenter claims that warehouse square footage has

⁷ California Air Resources Board. 2021. October 28. 2020 Mobile Source Strategy. https://ww2.arb.ca.gov/sites/default/files/2021-12/2020_Mobile_Source_Strategy.pdf

2. Response to Comments

doubled since 2008, therefore diesel VMT has also doubled. This is an unsubstantiated claim that is counter to the traffic data collected to calibrate/validate the City's transportation demand model (a focused model based on SBTAM), and inconsistent with the Southern California Association of Governments (SCAG) Regional Transportation Plan/Sustainable Communities Strategy (RTP/SCS). The data referred to by the Commenter as "a radical underestimation" is based on both a regionally and federally approved regional transportation plan model, the SCAG RTP/SCS. It should be noted that the figure referencing truck traffic counts on SR-60 includes pass-through traffic that does not have an origin or destination in the City of Ontario. Additionally, pursuant to CEQA Guidelines 15163, the CEQA baseline for the SEIR is the Approved Project not 2008 conditions or 2019 conditions.

Table 1 in the CCAP shows a 19 percent decline in the GHG emissions per mile from light-duty vehicles and a 25 percent increase in GHG emissions per mile from heavy-duty vehicles, translating to an overall 9 percent decline in GHG emissions per mile from the average vehicle. As illustrated in Tables 2 and 5, both heavy-duty and light-duty VMT increased from 2008 to 2019.

GHG emissions modeling conducted for the CCAP and Draft SEIR was conducted using agency-approved protocols, including the U.S. Community Protocol and the Global Protocol. The webtool cited by the Commenter is not an agency-approved tool for estimating GHG emissions in a GHG reduction plan or EIR. Section 5.8, *Greenhouse Gas Emissions*, of the Draft SEIR provides an accurate assessment of the existing, Approved Project, and Proposed Project GHG emissions in the City. No changes to the SEIR are warranted.

As described on page 1-4 of the Draft SEIR, the environmental impacts associated with the Proposed Project are defined as the incremental impacts between the current TOP and TOP 2050. To accurately assess the incremental impact, this SEIR analyzes the difference between the buildout of the current TOP to buildout of TOP 2050 (i.e., compares "buildout" to "buildout"). Impacts are assessed for the net land use changes under the Proposed Project, as described in Section 3.4.2.3, *Areas of Change*. While warehouse square footage in the City has doubled since 2008, this growth is associated with the Approved Project and not the Proposed Project. Therefore, the comments regarding the increase in truck traffic in the City and on State Route 60 since 2008 are noted.

This comment identifies the social cost of carbon and does not raise any issues related to the Proposed Project or the Proposed Project's impact on the physical environment under CEQA. No further response is necessary.

R3-9 The request for an industrial/warehouse moratorium within Ontario will be forwarded to the decision-makers for their review and consideration.

2. Response to Comments

See response to Comment R3-4 regarding the inclusion of emissions from heavy trucks and cargo handling equipment from warehouses in the CCAP and Draft SEIR.

See response to Comment R3-5. Appendix C-17 of the Draft SEIR includes a detailed accounting of light-heavy duty trucks (LHDT), medium-heavy duty trucks (MHDT), heavy-heavy duty trucks (HHDT), and passenger vehicle VMT provided by Fehr and Peers and modeling with SBTAM. The truck VMT conducted by Fehr & Peers includes trucks from all land uses in the City including warehouses, grocery stores, hospitals, restaurants, the Ontario Mills Outlets, and more. It is not possible to disaggregate the magnitude of GHG emissions from warehouses in the energy, solid waste, and water/wastewater sectors as these are based on aggregated data sources, either with other nonresidential activities or with all other land uses in Ontario. However, since truck VMT was provided, it can be estimated that heavy-duty VMT comprises 28 percent of total 2019 on-road mobile source GHG emissions.

The measures put into place to slow the spread of COVID-19 resulted in significant changes in human activity and VMT. Most notable are the temporary reductions in both heavy-duty and light-duty VMT across the state's highways and local roads, and the resulting temporary emission reductions. In California, VMT fell to its lowest point in early- to mid-April, with an approximately 25 percent reduction in heavy-duty VMT and 50 to 60 percent reduction in light-duty VMT. Since that time, both heavy-duty and light-duty VMT have steadily increased, with heavy-duty VMT returning to pre-COVID-19 levels in early June.⁸ Furthermore, VMT modeling conducted for the Proposed Project in TOP 2050 and the CCAP are based on SBTAM and counts were conducted in 2019 prior to the start of the pandemic, as identified on page 2 of the LOS Memorandum (Appendix K to the Draft SEIR). Forecasts were prepared utilizing traffic models that assume trends consistent with pre-pandemic conditions. No changes to the Draft SEIR or VMT analysis are warranted.

This comment identifies the social cost of carbon and does not raise any issues related to the Proposed Project or the Proposed Project's impact on the physical environment under CEQA. The CCAP and Draft SEIR consider the increase in GHG emissions from development in the City, include new warehouses. No further response is necessary.

See Section 2.2, *Summary of General Responses*, and Section 2.2.3, *Environmental Justice Policies*, regarding environmental justice policies in the Policy Plan. The City has opted for an "environmental justice in all policies" approach in TOP 2050 to ensure the topic is present alongside the multitude of issues and topics that affect our residents and resources. Environmental justice is not a CEQA issue, such that the comment's claims about the

⁸ California Air Resources Board. 2021. October 28. 2020 Mobile Source Strategy. https://ww2.arb.ca.gov/sites/default/files/2021-12/2020_Mobile_Source_Strategy.pdf

2. Response to Comments

minimization of environmental justice “impacts of logistics” are not comments on the adequacy of the SEIR. See also response to Comments R3-14 through 3-16.

R3-10 This comment is requesting additional data in the CCAP and does not raise any issues related to the Proposed Project or the Proposed Project’s impact on the physical environment under CEQA. Section 5.2, *Air Quality*, of the Draft SEIR includes the results of CalEnviroScreen (CES) 4.0 indicator results for the City of Ontario for pollution burden (see Figure 5.3-4) and asthma (see Figure 5.3-4). As identified in Figure 5.3-4, most of Ontario ranks within the 75th percentile or higher for pollution burden. Additionally, page 5.3-22 cites that most census tracts in Ontario rank in the 60th to 80th percentiles for asthma.

R3-11 The comment incorrectly states "the lack of independent traffic analysis specifically for the City of Ontario". The City independently developed a Request for Proposals and went through the appropriate procurement process for hiring a planning and environmental consulting team to update TOP. The selection process followed all State and City procurement requirements. The selected team consists of several expert subconsultants to conduct analyses required for each resource area. The City's Engineering Department developed the scope of work for the transportation analysis, including direction on the VMT analysis, provisions of assumed future roadway design and buildout for the consulting team to update in the City's focus model, which is a more detailed model based on SBTAM that provides more granularity for analyzing operations in the City, direction on future bus rapid transit (BRT) operations, and data and input on bike and pedestrian facility plans.

R3-12 A traffic analysis was conducted for TOP 2050 and is included in Appendix J (VMT) and Appendix K (Level of Service) of the Draft SEIR. No revisions to the Draft SEIR are warranted.

This comment is requesting additional information on the policies in TOP 2050 and does not raise any issues related to the Proposed Project or the Proposed Project’s impact on the physical environment under CEQA. The Mobility Element of the Policy Plan provides additional detail on the purpose of the specific measures M-1.2, M-1.6, M-4.2, M-4.4, M-4.5, and M-5.2 cited by the Commenter. Measures M-1.2 and M-1.6 are applied upon receipt of development applications or initiation of a public works project. All other measures included in the Commenter's letter are on-going and intended to inform the community that the City works with regional partners (e.g., SBCTA, Caltrans, Ontario Airport, etc.) to achieve goals towards reducing impacts, notably emissions.

See response to Comment R3-9 regarding the effect of the pandemic on VMT. The traffic analysis utilized traffic data from 2019 (pre-pandemic), as identified on page 2 of the LOS Memorandum (Appendix K to the Draft SEIR). Forecasts were prepared utilizing traffic

2. Response to Comments

models that assume trends consistent with pre-pandemic conditions. No changes to the Draft SEIR or VMT analysis are warranted.

- R3-13 The VMT methodology is described on page 6 of VMT Memorandum (see Appendix J to the Draft SEIR). There are two thresholds that are analyzed: project (Origin-Destination approach) and cumulative (Boundary Method approach).

The City's cumulative VMT threshold for the General Plan analysis states that "A significant impact would occur if the project caused total daily VMT within the City to be higher than the no project alternative under cumulative conditions." The cumulative threshold is based on the Boundary Method, which considers all VMT within the boundary of the City regardless of whether or not the City has jurisdictional authority over these trips. Compared to the Approved Project, the Proposed Plan is adding capacity for 25,399 residential units and 1,092,508 square feet of nonresidential square footage. This includes additional capacity for 5,189,888 square feet of Business Park (BP) and 8,099,949 square feet of Industrial (IND), and reduced capacity for 8,990,532 square feet of Mixed Use (MU) and 3,206,797 square feet of Commercial and Hospitality (NC, GC, OC, and HOS), which results in an absolute increase in VMT. As a result, VMT impacts were identified as a significant unavoidable impact in Section 5.17, *Transportation*, of the Draft SEIR.

The City of Ontario's VMT threshold for the General Plan under project analysis states that "A significant impact would occur if the project VMT/SP (for the land use plan) exceeds the Citywide average for Service Population under General Plan Buildout Conditions." VMT/SP means all VMT generated by all trips that either begin or end in Ontario, including trucks, divided by the City's service population, which consists of all people living and/or working in Ontario. Under the project-level VMT significance threshold, TOP 2050 reduces VMT/SP as compared to the current TOP, which indicates that the Proposed Project is a more transportation-efficient plan. Likewise, Appendix J shows that VMT per employee and VMT per resident also would decrease with the Proposed Project. This is because the Proposed Project increases both housing and jobs and is therefore consistent with the Southern California Association of Government's (SCAG) Sustainable Communities Strategy (SCS) of maintaining and improving the jobs/housing balance. The Proposed Project demonstrates that it accomplishes this goal because the VMT per resident and per employee decreases as compared to the current TOP. As a result, TOP 2050 result in beneficial impact relative the SB 743 goal of encouraging infill development and densification towards more efficient travel and lower VMT per person. As a result, while the Draft SEIR identified a significant unavoidable impact to VMT for cumulative VMT growth, TOP 2050 is a more transportation-efficient plan.

The Commenter has not identified additional mitigation measures that could be incorporated in the SEIR to mitigate the Proposed Project's significant unavoidable VMT

2. Response to Comments

impact. In the case of trucks (other than light trucks), based on CARB's 2017 Scoping Plan, the state's primary strategy for the goods movement sector is not in VMT reduction, but in advances in technology (zero-emissions [ZE] and near-zero-emissions [NZE] control strategies).⁹ This is consistent with observations that transportation demand management strategies (TDM) for VMT reductions identified by the San Bernardino County Transportation Authority (SBCTA) and the California Air Pollution Control Officer's Association (CAPCOA) do not apply to truck VMT.

See response to Comment R3-6. Limits on industrial development in the City is a Project alternative. Placing limits on heavy-duty truck travel is not a feasible mitigation measure as trucks are utilized for goods movement and a warehouse operator does not have the ability to restrict truck movements offsite. Likewise, placing limits on the amount of industrial construction or downsizing industrial development plans is not feasible at a general plan-level. Alternatives which would restrict the amount of Industrial land uses allowed in the City are evaluated as an alternative to the Proposed Project in Chapter 7, *Alternatives to the Proposed Project*.

- R3-14 See Section 2.2, *Summary of General Responses*, and Section 2.2.3, *Environmental Justice Policies*, regarding environmental justice policies in the Policy Plan. The City has opted for an "environmental justice in all policies" approach in TOP 2050 to ensure the topic is present alongside the multitude of issues and topics that affect our residents and resources. It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.

The City of Ontario complied with requirements in place for noticing in accordance with Governor Newsom's Executive Orders N-54-20 (April 22, 2020) and N-80-20 (September 23, 2020) in response to the threat of COVID-19, which expired on September 30, 2021. TOP 2050 complied with the requirements of Senate Bill 1000. As part of TOP 2050 disadvantaged communities were identified using CES 4.0. See Section 2.2, *Summary of General Responses*, and Section 2.2.3, *Environmental Justice Policies*. It should be noted that the Scoping Meeting presentation was translated to Spanish and posted on the City's website.

- R3-15 See response to Comment R3-15. As identified in Section 5.3, *Air Quality*, long-term regional (Impact 5.3-3) and localized (Impact 5.3-4) impacts were identified as significant unavoidable impacts of TOP 2050. The MATES V results and the CES4 pollution burden map were incorporated into Section 5.3 (see Figure 5.3-1 and Figure 5.3-4, respectively).

For long-term regional impacts, the increase in volatile organic compounds (VOCs) associated with TOP 2050 is primarily from consumer product use as a result of the substantial increase in housing units in the City. For localized impacts, impacts are

⁹ California Air Resources Board. 2017, November. California's 2017 Climate Change Scoping Plan: The Strategy for Achieving California's 2030 Greenhouse Gas Target, https://ww2.arb.ca.gov/sites/default/files/classic/cc/scopingplan/scoping_plan_2017.pdf

2. Response to Comments

associated with the cumulative increase in toxic air contaminants (TACs) associated with warehouse development projects in the City. To address this impact, the City incorporated a new Policy ER-4.9, which requires new developments that generate 100 trucks per day or more or 40 trucks with transport refrigeration units (TRUs) within 1,000 feet of a sensitive land use to conduct a health risk assessment to mitigate cancer and acute health risks as well as additional public outreach to residents within 500 feet of the project site. No additional measures have been identified by the Commenter.

R3-16 See also response to Comment R3-14 regarding outreach conducted for TOP 2050, which occurred during the COVID 19 pandemic. The City of Ontario complied with requirements in place for noticing in accordance with Governor Newsom’s Executive Orders N-54-20 (April 22, 2020) and N-80-20 (September 23, 2020) in response to the threat of COVID-19, which expired on September 30, 2021.

The Commenter requests a delay of one additional year for the approval of the Proposed Project. See response to Comments R3-1 and R3-2 regarding the delay of approval. The request for additional time for TOP 2050 and the CCAP will be forwarded to the decision-makers for their review and consideration.

See Section 2.2, *Summary of General Responses*, and Section 2.2.3, *Environmental Justice Policies*, regarding environmental justice policies in the Policy Plan. The City has opted for an “environmental justice in all policies” approach in TOP 2050 to ensure the topic is present alongside the multitude of issues and topics that affect our residents and resources. It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.

See response to Comments R3-5 and 3-6. Alternatives which would restrict the amount of Industrial and Business Park land uses allowed in the City are evaluated as an alternative to the Proposed Project in Chapter 7, *Alternatives to the Proposed Project*.

TOP 2050 includes Land Use Element Policy LU-2.1, Land Use Decisions, which requires new development to minimize impacts on adjacent properties, which could include green buffers as well as other site design measures such as placement of docking bays, sound walls, landscaping, and other setback requirements.

The request for an industrial/warehouse moratorium within Ontario will be forwarded to the decision-makers for their review and consideration.

Section 5.17, *Transportation*, Figure 5.17-1, *Truck Routes*, of the Draft SEIR illustrates the existing truck routes in the City of Ontario as well as connections to truck routes in adjacent cities. As identified in the Policy Plan, to mitigate impacts the City is committed to building grade-separated rail crossings, designing and locating industrial and warehousing land uses, and developing truck routes to protect our neighborhoods from truck traffic (see Policy M-4.1).

2. Response to Comments

R3-17 The request for the City to conduct a Shade Equity Tree Analysis for the neighborhoods south of Holt Boulevard will be forwarded to the decision-makers for their review and consideration.

R3-18 The Draft SEIR and the CCAP included GHG emissions from the agricultural land uses in the City. As noted in response to Comment R3-2, in accordance with CEQA Guidelines Section 15163, the CEQA baseline for this SEIR is the Approved Project. As a result, for the SEIR, there are no changes in the agricultural sector emissions of the Proposed Project compared to the Approved Project since the current TOP does not have agricultural land use designations. However, both the Draft SEIR and the CCAP include the existing agricultural sector emissions estimates and the reduction in emissions from this sector.

The GHG analysis in the Draft SEIR and the CCAP do not treat agricultural land uses as a sink for GHG emissions. While crops do sequester carbon due to the growth in biomass, the Intergovernmental Panel on Climate Change’s guidance¹⁰ on the topic notes that for annual crops, the increase in biomass is equal to the loss in biomass from harvest and crop mortality, and thus there is no net accumulation of carbon in agricultural biomass. As more than 90 percent of the crop activity in Ontario is estimated to be annual crops, there is no substantive increase in agricultural biomass in Ontario. Additionally, agriculture does generate GHG emissions from nitrogen used for fertilizer, enteric fermentation and manure management associated with livestock, fuel use from agricultural equipment, and other activities.

The GHG analysis does consider that an amount of carbon is sequestered in the soil of agricultural lands, but as this does not change annually, this source only shows as an increase or decrease in GHG emissions if the agricultural land is expanded or developed. Loss of this sequestration from the development of agricultural lands is accounted for in the “Land Use Change” emissions shown in Table 14 of the CCAP.

This comment is requesting nature-based solutions to offset the carbon footprint of industrial land uses. GHG emissions were not identified as a significant impact of the Proposed Project and this comment does not raise any issues related to the Proposed Project or the Proposed Project’s impact on the physical environment under CEQA. Since agricultural land uses are often a source of GHG emissions and not a sink, a regenerative agricultural preserve would not offset the carbon footprint of warehouses in the City.

R3-19 An evaluation of the environment impacts of TOP 2050 compared to the current TOP is included in Section 5.2, *Agriculture and Forestry Resources*. The Commenter’s concerns

¹⁰ Intergovernmental Panel on Climate Change. 2019 Refinement to the 2006 IPCC Guidelines for National Greenhouse Gas Inventories, Volume 4: Agriculture, Forestry, and Other Land Use; Section 5.2.1.1 <https://www.ipcc-nggip.iges.or.jp/public/2019rf/vol4.html>

2. Response to Comments

about the loss of agricultural land are noted will be forwarded to the decision-makers for their review and consideration.

As noted in response to Comment R3-18, agricultural land uses in Ontario are not a net sink for GHG emissions.

As identified in the CCAP, the City has planted 1,000 trees in public property since 2019. In fiscal year 2021-2022 the City has planted more than 6,000 trees in private and public property. Urban tree planting is a component of the CCAP. Table 14 in the CCAP notes the decrease in Ontario's GHG emissions resulting from urban tree biomass.

R3-20 See response to Comment R3-19. The request to create alternative plans for open and working lands (including agriculture) will be forwarded to the decision-makers for their review and consideration.

R3-21 TOP 2050 is a focused updated to the current TOP. Neither the current TOP nor TOP 2050 envision agricultural land uses in Ontario Ranch at buildout of the Proposed Project, aside from the Farm Hub in the Great Park. As a result, the CCAP is consistent with the buildout assumptions of TOP 2050 and the current TOP.

Regarding agrivoltaics, the City has a Right-to-Farm Ordinance and the Community Climate Action Plan (CCAP) supports the installation of solar panels and solar panels are permitted within the City, including Ontario Ranch.

See response to Comments R3-2, R3-6, and R3-18. The Proposed Project would not result in any additional land use changes to agricultural land in Ontario compared to the Approved Project. The request to analyze carbon benefits of retaining agricultural land will be forwarded to decision-makers for their review and consideration.

As noted in response to Comments R3-18 and R3-22, agricultural land uses in Ontario are not a sink for GHG emissions.

R3-22 An evaluation of the environment impacts of TOP 2050 compared to the current TOP is included in Section 5.2, *Agriculture and Forestry Resources*. Figure 5.2-1 shows Farmland in the City of Ontario and Figure 5.2-2 shows the Williamson Act lands in Ontario.

In accordance with CEQA Guidelines Section 15163, the Draft SEIR evaluates the potential impacts from the change in land use designations and policies of the current TOP compared to TOP 2050. While there is currently agricultural use in the Ontario Ranch, the planning framework to convert the land to nonagricultural land already occurred as a result of adoption of the current TOP in 2010, and prior to that as a result of annexation of the New Model Colony in 1999 from the County of San Bernardino. The Proposed Project would not result in any additional conversion of agricultural land in Ontario compared to the Approved Project.

2. Response to Comments

In accordance with CEQA Guidelines Section 15163, the Supplement to the EIR need contain only the information necessary to make the previous EIR adequate for the project as revised. The 2010 Certified EIR included an Agricultural Land Preserve Alternative, which was considered and rejected during the scoping process. The 2010 Certified EIR found that overall continuation of agriculture in this area is likely not feasible, given the social pressures on these areas to meet the increasing population, and also because agricultural uses in this area are being driven out by competition from operations in other sectors. In addition, many of the support services the dairies rely on, including feed/hay for the cattle, have closed as a result of closure of many of the dairies, and few dairies remain. Additionally, this alternative did not meet the objectives of TOP of meeting demands for urban growth.

The Commenter's concerns about the loss of agricultural land are noted will be forwarded to the decision-makers for their review and consideration.

R3-23 Discussion of the loss of agricultural farmlands has been added to Section 1.7, *Areas of Controversy*, of Chapter 1, *Executive Summary* (see Chapter 3, *Revisions to the Draft EIR*).

R3-24 See Section 2.2, *Summary of General Responses*, and Section 2.2.2, *Nonrenewal of Williamson Act Contracts*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. As a result, no impacts were identified in the SEIR and no mitigation is required.

In the CCAP, GHG Reduction Strategies 4 (Green Roofs), 5 (Urban Cooling), 22 (Water Efficient Landscapes and Water Recycling), 26 (Climate Change Awareness and Education), and 27 (Carbon Sequestration) all make use of nature-based solutions to reduce Ontario's GHG emissions. The California Air Resources Board's *Draft 2022 Scoping Plan* identifies that the natural and working land are a source of GHG emissions and not a sink of GHG emissions in 2045.¹¹ The land use types considered in the natural working lands sector include cropland. However, agricultural use is a source, not a sink for GHG emission. The 2022 Scoping Plan identifies implementation of smart agricultural practices to reduce GHG emissions from cropland, such as improving water use efficiency, increasing pollinator habitat, and reducing synthetic fertilizer and pesticide use.

R3-25 See response to Comment R3-22 regarding alternatives identified in the 2010 Certified EIR. This comment identifies a request to create an agricultural preserve in the Ontario Ranch and does not raise any issues related to the Proposed Project or the Proposed Project's impact on the physical environment under CEQA. The request for creation of

¹¹ California Air Resources Board. 2022, May. Draft 2022 Scoping Plan. Appendix I – Natural and Working Lands Technical Support Document. <https://ww2.arb.ca.gov/sites/default/files/2022-05/2022-draft-sp-appendix-i-nwl-modeling.pdf>

2. Response to Comments

the Southern California Agricultural Land Foundation Agricultural Heritage and Innovation (AHI) zone within and/or adjacent to the Great Park will be forwarded to the decision-makers for their review and consideration. The 340-acre Great Park allows for continues agricultural use and existing agricultural use is allowed to continue under the City's Right-to-Farm Ordinance. Additionally, the Great Park envisions creation of a Farm Hub as well as potential agricultural uses.

- R3-26 In accordance with CEQA Guidelines Section 15163, the Supplement to the EIR need contain only the information necessary to make the previous EIR adequate for the project as revised. Section 5.5, *Cultural Resources*, identifies that Ontario has eight historic districts, and four proposed and five potential historic districts are deemed eligible for listing (see Figure 5.5-1). The Proposed Project would result in an increase in land use intensity compared to the Approved Project but would not result in development in areas of the City that were not planned for development under the Current TOP.

The 2004 document cited by the Commenter identifies two potential districts based on their associated with the mechanization of the dairy industry and building type. However, significant alteration of land and regional context has occurred since this 2004 study and the 2004 study identified that additional evaluation would be necessary prior to making the determination that dairies qualified as a historic district. The request for the City to evaluate the potential value of dairies as a historic district will be forwarded to the City for their review and consideration.

See response to Comment R3-24 regarding smart agricultural practices.

- R3-27 The Commenter's concerns about the loss of agricultural land and request to create an agricultural preserve are noted will be forwarded to the decision-makers for their review and consideration. It should be noted that the City of Ontario is not a participant in the San Bernardino County Proposition 70 implementation. The Southern California Agricultural Land Foundation managed 200 acres of farmland within Ontario Ranch through a 1988 Park Bond Act with San Bernardino County. The County of San Bernardino deed restrictions remain in effect until either a conservation easement is recorded or until the County of San Bernardino sells or exchanges the property. For the City of Ontario, the City evaluated conversation of agricultural land to non-agricultural use as part of the 2010 Certified EIR and prior to that as a result of annexation of the New Model Colony in 1999 from the County of San Bernardino. However, the Proposed Project would not result in any additional conversion of agricultural land in Ontario compared to the Approved Project (see Section 5.2, *Agriculture and Forestry Resources*, of the Draft SEIR).

- R3-28 TOP 2050 SEIR will include findings consistent with the requirements of the Public Resources Code and the CEQA Guidelines. As identified in response to Comment R3-22, and consistent with CEQA Guidelines Section 15163, the Draft SEIR evaluates the

2. Response to Comments

potential impacts from the change in land use designations and policies of the current TOP compared to TOP 2050. While there is currently agricultural use in the Ontario Ranch, the planning framework to convert the land to nonagricultural land already occurred as a result of adoption of the current TOP in 2010, and prior to that as a result of annexation of the New Model Colony in 1999 from the County of San Bernardino. The Proposed Project would not result in any additional conversion of agricultural land in Ontario compared to the Approved Project.

The Supplement to the EIR need contain only the information necessary to make the previous EIR adequate for the project as revised. The 2010 Certified EIR considered and rejected an alternative that would result in the creation of an Agricultural Land Preserve Alternative.


See response to Comment R3-16. TOP 2050 includes Land Use Element Policy LU-2.1, Land Use Decisions, which requires new development to minimize impacts on adjacent properties, which could include green buffers as well as other site design measures such as placement of docking bays, sound walls, landscaping, and other setback requirements. Additionally, TOP 2050 includes a new Policy ER-4.9, which requires new developments that generate 100 trucks per day or more or 40 trucks with transport refrigeration units (TRUs) within 1,000 feet of sensitive uses to conduct a health risk assessment to mitigate cancer and acute health risks as well as additional public outreach to residents within 500 feet of the project site.

2. Response to Comments

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2. Response to Comments

LETTER R4 – Supporters Alliance for Environmental Responsibility (SAFER) (7 pages)



June 23, 2022

Via E-mail

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Re: Comment on the California Environmental Quality Act Draft Supplemental Environmental Impact Report for The Ontario Plan (TOP) 2050 (SCH No. 2021070364)

Dear Honorable Chairperson Willoughby, Vice Chairperson DeDiemar, Commissioners Anderson, Dean, Gage, Lampkin, and Ricci, Director Zeledon, Mr. Grahn, and Ms. Ruddins:

I am writing on behalf of Supporters Alliance for Environmental Responsibility ("SAFER") regarding the California Environmental Quality Act ("CEQA") Draft Supplemental Environmental Impact Report ("SEIR") prepared for The Ontario Plan (TOP) 2050 (SCH No. 2021070364), including all actions related or referring to the proposed updates to the City of Ontario's ("City") TOP and its related program EIR certified in 2010. This also includes updates to the Policy Plan, which serves as the legally required General Plan for the City and is also contained in TOP ("Project").

SAFER seeks to ensure that future projects have the proper level of CEQA review, including project-specific environmental review. The Draft SEIR for the Ontario Plan is a

R4-1

2. Response to Comments

The Ontario Plan (TOP) 2050
Comment on Draft Supplemental EIR
June 23, 2022
Page 2 of 7

programmatic EIR. Many projects will require project-level CEQA review to analyze and mitigate project-specific impacts that cannot be known at the programmatic level SAFER requests that the City:

- a) Refrain from relying on the TOP 2050 program-level SEIR for any future specific development project that may result in significant environmental impacts that were not previously analyzed in the SEIR;
- b) Conduct a project-level CEQA review for all future specific development projects that could have significant environmental impacts that were not analyzed in TOP 2050 or the SEIR; and
- c) Update and recirculate a draft SEIR that includes a written commitment by the City to perform project-level CEQA review for all future specific development projects that may result in significant environmental impacts that were not analyzed in TOP 2050 or the SEIR.

Since CEQA requires the City to carry out project-level CEQA review for specific development projects that may have significant environmental impacts that were not previously analyzed, rather than rely on a past program EIR prepared for a General Plan, the City should update the TOP 2050 Draft SEIR to reflect this requirement. By including a written commitment in the SEIR guaranteeing that the City will conduct the appropriate level of CEQA review for development projects that may have significant environmental impacts that were not analyzed in TOP 2050 or the SEIR, the City is likely to avoid CEQA violations of this nature in the future.

I. PROJECT BACKGROUND AND DESCRIPTION

The Ontario Plan (TOP) is the City's policy and implementation framework that guides the long-term growth and improvement of Ontario through six interrelated components of city governance:

- A Vision that provides a sense of the purpose and mission for city governance and sets the tone for the other components of TOP. The Vision's central theme is a sustained, community-wide prosperity that continuously adds value and yields benefits.
- A Governance Manual that establishes a set of goals and policies to promote consistent City leadership based on the principles of regional leadership, transparency, long-term value, accountability, and inclusivity.
- A Policy Plan that serves as the City's legally required General Plan and that states long-term goals, principles, and policies to achieve Ontario's Vision through nine elements: land use, housing, mobility, safety, environmental

R4-1
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R4-2

2. Response to Comments

The Ontario Plan (TOP) 2050
Comment on Draft Supplemental EIR
June 23, 2022
Page 3 of 7

resources, parks and recreation, community economics, community design, and social resources.

- A list of City Council Priorities that shape the City's ongoing annual budgeting process, with a focus on a variety of short- and long-term goals and objectives.
- An Implementation Plan that identifies the actions needed to carry out the TOP's policies. This includes initiatives by the City as well as decisions on public and private development projects and City activity Programs.
- A Tracking and Feedback system that charts the City's progress toward achieving the Policy Plan goals, providing data and analysis that enables decision makers to make strategic course corrections in response to changing circumstances and monitor ongoing operational effectiveness.

(NOA Draft SEIR, p. 2.)

The proposed Project is an update to TOP to guide the City's development and conservation for the next 30 years through 2050. The Project is referred to as TOP 2050 and "is a focused effort, with particular emphasis on conducting technical refinements to the Policy Plan to comply with state housing mandates; conform with new state laws related to community health, environmental justice, climate adaption, resiliency, and mobility; bring long-term growth and fiscal projections into alignment with current economic conditions; and advance the Tracking and Feedback system and Implementation Plan." (*Id.*) According to the City, "the majority of updates created through the proposed project will weave refinements throughout the existing structure of the Policy Plan, which is organized into nine broad categories," including land use, housing, parks and recreation, environmental resources, community economics, safety, mobility, community design, and social resources. (*Id.*, pp. 2-3.)

The TOP 2050's programmatic, general-plan-level SEIR is intended to review the environmental impacts of updating the City's TOP, which includes updates to its General Plan (also referred to as its Policy Plan), and to supplement the program EIR prepared for the original TOP in 2010. Currently, the Draft SEIR does not include any guarantee that project-level CEQA review will be carried out for specific development projects that may have significant impacts on the environment.

II. DISCUSSION

A. Under CEQA's Tiering Provisions, the Program-Level SEIR may not be Relied upon for Future Specific Development Projects that Could Result in Significant Environmental Impacts that were not Previously Analyzed in TOP 2050 or the SEIR.

CEQA permits agencies to "tier" CEQA documents, in which general matters and

R4-2
cont.

R4-3

2. Response to Comments

The Ontario Plan (TOP) 2050
Comment on Draft Supplemental EIR
June 23, 2022
Page 4 of 7

environmental effects are considered in a document “prepared for a policy, plan, program or ordinance followed by narrower or site-specific [environmental review] which incorporate by reference the discussion in any prior [environmental review] and which concentrate on the environmental effects which (a) are capable of being mitigated, or (b) were not analyzed as significant effects on the environment in the prior [EIR].” (Pub. Res. Code [“PRC”] § 21068.5.) “[T]iering is appropriate when it helps a public agency to focus upon the issues ripe for decision at each level of environmental review and in order to exclude duplicative analysis of environmental effects examined in previous [environmental reviews].” (*Id.* § 21093.) CEQA regulations strongly promote tiering of environmental review.

“Later activities in the program must be examined in light of the program [document] to determine whether an additional environmental document must be prepared.” (14 CCR § 15168(c).) The first consideration is whether the activity proposed is covered by the program. (*Id.* § 15168(c)(2).) If a later project is outside the scope of the program, then it is treated as a separate project and the previous environmental review may not be relied upon in further review. (See *Sierra Club v. County of Sonoma* (1992) 6 Cal.App.4th 1307, 1320-21.) The second consideration is whether the “later activity would have effects that were not examined in the program.” (14 CCR § 15168(c)(1).) A program environmental review may only serve “to the extent that it contemplates and adequately analyzes the potential environmental impacts of the project....” (*Sierra Nevada Conservation v. County of El Dorado* (2012) 202 Cal.App.4th 1156, 1171 [quoting *Citizens for Responsible Equitable Envtl. Dev. v. City of San Diego Redevelopment Agency* (2005) 134 Cal.App.4th 598, 615.]) If the program environmental review does not evaluate the environmental impacts of the project, a tiered CEQA document must be completed before the project is approved. (*Id.* at 1184.)

For these inquiries, the “fair argument test” applies. (*Sierra Club*, 6 Cal.App.4th at 1318; see also *Sierra Club v. County of San Diego* (2014) 231 Cal.App.4th 1152, 1164 [“when a prior EIR has been prepared and certified for a program or plan, the question for a court reviewing an agency’s decision not to use a tiered EIR for a later project ‘is one of law, i.e., ‘the sufficiency of the evidence to support a fair argument.’”] [quoting *Sierra Club*, 6 Cal.App.4th at 1318.]) Under the fair argument test, a new EIR must be prepared “whenever it can be fairly argued on the basis of substantial evidence that the project may have significant environmental impact. (*Sierra Club*, 6 Cal.App.4th at 1316 [quotations and citations omitted].) When applying the fair argument test, “deference to the agency’s determination is not appropriate and its decision not to require an EIR can be upheld only when there is no credible evidence to the contrary.” (*Id.* at 1318.) “[I]f there is substantial evidence in the record that the later project may arguably have a significant adverse effect on the environment which was not examined in the prior program EIR, doubts must be resolved in favor of environmental review and the agency must prepare a new tiered EIR, notwithstanding the existence of contrary evidence.” (*Id.* at 1319.)

Given CEQA’s regulations and the case law, the City may not rely on the program-level SEIR prepared for TOP 2050 for future specific development projects that may result in significant environmental impacts that were not previously considered in the SEIR.

R4-3
cont.

2. Response to Comments

The Ontario Plan (TOP) 2050
Comment on Draft Supplemental EIR
June 23, 2022
Page 5 of 7

B. The City must Conduct a Project-Level CEQA Review for any Specific Development Project that may Result in Significant Environmental Impacts that were not Analyzed in TOP 2050 or the SEIR.

According to the Draft SEIR prepared by the City for TOP 2050, the following potential significant environmental impacts related to future specific development projects were not considered by the program-level SEIR:

Construction activities associated with TOP 2050 would occur over the buildout horizon of the plan, causing short-term emissions of criteria air pollutants. However, *information regarding specific development projects, soil types, and the locations of receptors would be needed in order to quantify the level of impact associated with construction activity.* Due to the scale of development activity associated with buildout of TOP 2050, emissions would likely exceed the South Coast AQMD regional significance thresholds. In accordance with the South Coast AQMD methodology, emissions that exceed the regional significance thresholds would cumulatively contribute to the nonattainment designations of the SoCAB. (Draft SEIR, p. 5.3-41 [emph. added].)

Air quality emissions related to construction must be addressed on a project-by-project basis. For TOP 2050, which is a broad-based policy plan, it is not possible to determine whether the scale and phasing of individual projects would exceed the South Coast AQMD's short-term regional or localized construction emissions thresholds. (*Id.* [emph. added].)

As a long-range planning document, *TOP 2050 lacks sufficient detail on specific development projects* that would potentially be developed in the future; *therefore, it is not possible to determine what types of TACs would be generated on an individual site.* Because the exact nature of the future industrial uses is not known, the quantity of TACs generated by the Proposed Project is also unknown. Furthermore, for warehouse development projects, cancer risk is predominately associated with diesel-powered cargo handling equipment rather than onsite truck idling. *There is insufficient information available at this level of analysis to conduct a reasonable or scientifically valid analysis of DPM associated with onsite diesel-powered cargo handling equipment and trucks, or other sources of TACs. Thus, for programmatic, general-plan-level assessments, it is not feasible to conduct regional dispersion modeling to determine the incremental contribution of risks associated with land use changes.* (*Id.*, pp. 5.3-45-46 [emph. added].)

Specific development projects in the City that have the potential to generate potentially significant risks associated with the release of TACs are required to undergo an analysis of their potential health risks associated with TACs based upon the specific details of each individual project... Overall, *because there are no specific development projects identified or approved under the Proposed Project and the location and exact nature of future development projects are unknown, determining health risk at this*

R4-4

2. Response to Comments

The Ontario Plan (TOP) 2050
Comment on Draft Supplemental EIR
June 23, 2022
Page 6 of 7

time is considered speculative pursuant to Section 15145 of the CEQA Guidelines. Health risk impacts from development of industrial and commercial land uses are considered a potentially significant cumulative impact. With the increase in industrial land uses under TOP 2050, the Proposed Project could potentially result in a substantial increase in magnitude of impacts compared to that of the Approved [TOP 2010] Project. (*Id.*, p. 5.3-46 [emph. added].)

This SEIR quantifies the increase in criteria air pollutants emissions in the City. However, *at a programmatic level analysis, it is not feasible to quantify the increase in TACs from stationary sources* associated with the Proposed Project *or meaningfully correlate how regional criteria air pollutant emissions above the South Coast AQMD significance thresholds correlate with basinwide health impacts.* (*Id.*, p. 5.3-55 [emph. added].)

At the time a development project is proposed adjacent to or near a known or potential historic structure or resource, *the project-level CEQA document of the development project would need to identify any impacts, direct or indirect, that the project could have on the identified historic structure or resource.* (*Id.*, p. 5.5-18 [emph. added].)

If the lead agency determines that the project may have a significant effect on unique archaeological resources, the project-level CEQA document being prepared for the development project *is required to address and mitigate the impacts* of those resources. (Draft SEIR, p. 5.5-19 [emph. added].)

While *specific project level data for individual developments for TOP 2050 (such as construction equipment, duration, and phasing) are not available*, construction could generate noise levels in excess of 80 dBA Leq and generate noise disturbances for prolonged periods of time at noise-sensitive receptors. (*Id.*, p. 5.13-18, -28 [emph. added].)

Like the Approved [TOP 2010] Project, the Proposed Project would have similar impacts because *specific project-level data for individual developments for TOP 2050 (such as construction equipment) are not available*, and construction could generate excessive vibration levels at sensitive receptor locations. Vibration-related noise impacts from the Proposed Project that would accommodate buildout of TOP 2050 would be potentially significant. (*Id.*, p. 5.13-35 [emph. added].)

Given the lack of information that the Draft SEIR has regarding specific development projects to be carried out in the future, at the very least, the City must conduct a project-level CEQA review for any specific development project that could result in significant impacts to air quality, greenhouse gas emissions, health risks, hazards and hazardous materials, noise, and archaeological, cultural, and historical resources.

R4-4
cont.

2. Response to Comments

The Ontario Plan (TOP) 2050
Comment on Draft Supplemental EIR
June 23, 2022
Page 7 of 7

C. An Updated Draft SEIR Should be Recirculated that Includes a Commitment that the City will Conduct Project-Level CEQA Review for all Future Specific Development Projects that may Result in Significant Environmental Impacts Not Previously Analyzed in TOP 2050 or the SEIR.

The Draft SEIR hints at the potential need for the City to conduct project-level CEQA review for future specific development projects. However, the SEIR includes no commitment or guarantee that the City will conduct a separate project-level environmental review for future specific development projects that may have significant environmental impacts not previously analyzed. Therefore, SAFER requests that this City provide the public with a written commitment guaranteeing that project-level CEQA review will occur for all specific development projects that may have significant impacts on the environment that were not previously analyzed in TOP 2050 or the SEIR. In doing so, the City is more likely to avoid violating CEQA by not conducting project-level environmental review of specific development projects that could result in significant environmental impacts that were not previously analyzed in the SEIR. Thus, the City should update and recirculate a new draft SEIR that includes a guarantee from the City that CEQA review at the project level will be conducted for all future development projects that may have significant environmental impacts not considered by the TOP 2050 and the SEIR.

R4-5

III. CONCLUSION

For the foregoing reasons, SAFER requests that City staff revise the Draft SEIR to include a written commitment from the City to conduct project-level CEQA review for all future specific development projects with potentially significant environmental impacts, as described above. Please include this letter hereto in the record of proceedings for this project. Thank you for your attention to these comments.

R4-6

Sincerely,



Victoria Yundt
LOZEAU | DRURY LLP

2. Response to Comments

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2. Response to Comments

R4. Response to Comments from Victoria Yundt, Lozeau | Drury, LLP, representing the Supporters Alliance for Environmental Responsibility (SAFER), dated June 23, 2022.

- R4-1 Responses to the Supporters Alliance for Environmental Responsibility's (SAFER) comments are addressed in response to Comments R4-2 through R4-5 below. The City of Ontario will determine the appropriate CEQA approach for future discretionary projects based on the CEQA Guidelines and discretionary action involved. It would not be appropriate to identify the type of future environmental analysis that may be required for future discretionary projects in TOP 2050 SEIR.
- R4-2 This comment describes the framework of TOP 2050 and does not raise any issues related to the Proposed Project or the Proposed Project's impact on the physical environment under CEQA. No response is warranted.
- R4-3 This comment identifies that future project-level CEQA review may be necessary for future discretionary projects and does not raise any issues related to the Proposed Project or the Proposed Project's impact on the physical environment under CEQA. No response is warranted. The City of Ontario will determine the appropriate CEQA approach for future discretionary projects based on the CEQA Guidelines and discretionary action involved.
- R4-4 See response to Comment R4-3 above. Comments regarding statements on air quality impacts identified in Section 5.3, *Air Quality*, of the Draft SEIR are noted. Mitigation Measure 3-1, AQ-1, and TOP 2050 Policy ER-4.9 would require future discretionary projects to prepare site-specific technical studies to mitigate air quality impacts below the South Coast AQMD thresholds.
- R4-5 See response to Comments R4-2 through R4-4 above. Pursuant to CEQA Guidelines Section 15088.5, a Draft EIR is required to be recirculated only when "significant new information" is added to the EIR after circulation of the Draft EIR. The Proposed Project's Draft SEIR was prepared in accordance with CEQA Guidelines. The Draft SEIR comprehensively assesses the significant environmental effects of the project, provides a reasonable range of alternatives to the Proposed Project, and feasible mitigation measures to reduce and avoid potentially significant environmental impacts. No "significant new information" has been added to the revised Draft SEIR; and therefore, the Draft SEIR are not inadequate and does not need to be recirculated.
- R4-6 Responses to SAFER's comments are addressed in response to Comments R4-1 through R4-5 above.

2. Response to Comments

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2. Response to Comments

LETTER R5 – Robert Barragan (3 pages)

Public Comment / General Plan

Claudia Y. Isbell

From: SeamlessDocs <noreply@seamlessdocs.com>
Sent: Tuesday, June 14, 2022 6:11 PM
To: Public Comments
Subject: Public Comment from Robert Barragan



New Submission

A new submission was received for the form below. Log into your Submission Manager to review and process it.

Form name	Public Speaker Form
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Submission Details:

Agenda Item Number or "Public Comment"
Ontario Districting & General Plan

Speaker Full Name
Robert Barragan

Address (optional)
Street Address
[REDACTED]

City
Ontario

State

1

2. Response to Comments

CA

Zip

91761

Phone Number (optional)

██████████

Name of Organization

None

Subject on which the person wishes to speak?

Against

Briefly describe what you would like to say

My name is Robert Barragan, 20 years resident of the City of Ontario. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan. On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities. This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes: 5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance. 5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in

R5-1

R5-2

R5-3

2. Response to Comments

violation of 65302(h)(1)(A) Access to food; creating a food desert. 5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP. 5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations. 5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations. 5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.

R5-3 cont.

R5-4

R5-5

R5-6

R5-7

Email to receive a copy of your submission

View the submission and any attachments by following the link below and using this unique access code: XXXXXXXXXX

[View Submission \[mail.seamlessdocs.com\]](mailto:seamlessdocs.com)

City of Ontario - City Clerks | 303 East B Street Ontario, CA 91764

2. Response to Comments

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2. Response to Comments

R5. Response to Comments from Robert Barragan, dated June 14, 2022.

- R5-1 Responses to Robert Barragan's comments are addressed in response to Comments R5-2 through R5-6 below.
- R5-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R5-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R5-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R5-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R5-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R5-7 Responses to Robert Barragan's comments are addressed in response to Comments R5-2 through R5-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R6 – Vanessa Yambot (1 page)

Claudia Y. Isbell

From: Vanessa Yambot [REDACTED]
Sent: Tuesday, June 14, 2022 6:34 PM
To: Public Comments
Subject: Public Comment

1. My name is Vanessa Yambot, a local resident for 30 years. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan. | R6-1

2. On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). | R6-1

3. These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: **Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.**

4. This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes: | R6-2

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance. | R6-2

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert. | R6-3

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP. | R6-4

5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations. | R6-5

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations. | R6-6

5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents. | R6-7

1

2. Response to Comments

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2. Response to Comments

R6. Response to Comments from Vanessa Yambot, dated June 14, 2022.

- R6-1 Responses to Vanessa Yambot's comments are addressed in response to Comments R6-2 through R6-6 below.
- R6-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R6-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R6-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R6-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R6-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R6-7 Responses to Vanessa Yambot's comments are addressed in response to Comments R6-2 through R6-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments


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2. Response to Comments

LETTER R7 – Ilianna Romero (3 pages)

Claudia Y. Isbell

From: SeamlessDocs <noreply@seamlessdocs.com>
Sent: Tuesday, June 14, 2022 8:50 PM
To: Public Comments
Subject: Public Comment from Ilianna Romero



New Submission

A new submission was received for the form below. Log into your Submission Manager to review and process it.

Form name	Public Speaker Form
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Submission Details:

Agenda Item Number or "Public Comment"
Public Comment

Speaker Full Name
Ilianna Romero

Address (optional)
Street Address
[REDACTED]

City
Alta Loma

State

1

2. Response to Comments

CA

Zip

91737

Subject on which the person wishes to speak?

Against

Briefly describe what you would like to say

On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities. This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes: 5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance. 5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert. 5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP. 5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations. 5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General

R7-1

R7-2

R7-3

R7-4

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R7-6

R7-7

2. Response to Comments

Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.

R7-7
cont.

Email to receive a copy of your submission

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2. Response to Comments

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2. Response to Comments

R7. Response to Comments from Ilianna Romero, dated June 14, 2022.

- R7-1 Responses to Ilianna Romero's comments are addressed in response to Comments R7-2 through R7-6 below.
- R7-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R7-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R7-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R7-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R7-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R7-7 Responses to Ilianna Romero's comments are addressed in response to Comments R7-2 through R7-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R8 – Verlynn Gojit (1 page)

Claudia Y. Isbell

From: Verlynn Gojit [REDACTED]
Sent: Tuesday, June 14, 2022 9:19 PM
To: Public Comments
Subject: Save Amy's Farm!

1. My name is Verlynn Gojit. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan. R8-1
2. On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). R8-1
3. These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: **Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.**
4. This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes:

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance. R8-2

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert. R8-3

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP. R8-4

5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations. R8-5

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations. R8-6

5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents. R8-7

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[REDACTED]

2. Response to Comments

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2. Response to Comments

R8. Response to Comments from Verlynn Gojit, dated June 14, 2022.

- R8-1 Responses to Verlynn Gojit's comments are addressed in response to Comments R8-2 through R8-6 below.
- R8-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R8-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R8-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R8-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R8-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R8-7 Responses to Verlynn Gojit's comments are addressed in response to Comments R8-2 through R8-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments


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2. Response to Comments

LETTER R9 – Inmi Chao (3 pages)

Claudia Y. Isbell

From: SeamlessDocs <noreply@seamlessdocs.com>
Sent: Tuesday, June 14, 2022 10:16 PM
To: Public Comments
Subject: Public Comment from Inmi Chao



New Submission

A new submission was received for the form below. Log into your Submission Manager to review and process it.

Form name	Public Speaker Form
------------------	---------------------

Submission Details:

Agenda Item Number or "Public Comment"
TOP2050 General Plan

Speaker Full Name
Inmi Chao

Address (optional)
State
CA

Subject on which the person wishes to speak?
Against

Briefly describe what you would like to say

1

2. Response to Comments

My name is Inmi Chao. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan. On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities. This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes: 5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance. 5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert. 5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP. 5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations. 5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations. 5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural

R9-1

R9-2

R9-3

R9-4

R9-5

R9-6

R9-7

2. Response to Comments

Resources and utilities, as well as, the health and quality of life of Ontario residents.

R9-7
cont.

Email to receive a copy of your submission

[REDACTED]

View the submission and any attachments by following the link below and using this unique access code: [REDACTED]

[View Submission \[mail.seamlessdocs.com\]](#)

City of Ontario - City Clerks | 303 East B Street Ontario, CA 91764

2. Response to Comments

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2. Response to Comments

R9. Response to Comments from Inmi Chao, dated June 14, 2022.

- R9-1 Responses to Inmi Chao's comments are addressed in response to Comments R9-2 through R9-6 below.
- R9-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R9-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R9-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R9-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R9-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R9-7 Responses to Inmi Chao's comments are addressed in response to Comments R9-2 through R9-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments


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2. Response to Comments

LETTER R10 – Katie Crofford (3 pages)

Claudia Y. Isbell

From: SeamlessDocs <noreply@seamlessdocs.com>
Sent: Thursday, June 16, 2022 10:29 AM
To: Public Comments
Subject: Public Comment from Katie Crofford



New Submission

A new submission was received for the form below. Log into your Submission Manager to review and process it.

Form name	Public Speaker Form
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Submission Details:

Agenda Item Number or "Public Comment"
Public Comment

Speaker Full Name
Katie Crofford

Phone Number (optional)
[REDACTED]

Subject on which the person wishes to speak?
Against

Briefly describe what you would like to say
My name is Katie Crofford. This is a notification that I, along with a

R10-1

1

2. Response to Comments

<p>number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan. On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities. This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes: TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance. Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert. Additional population growth & associated emissions would exceed South Coast AQMD's AQMP. Would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.</p>	R10-1 cont.
	R10-2
	R10-3
	R10-4
	R10-5
	R10-6
	R10-7

2. Response to Comments

Email to receive a copy of your submission
[REDACTED]

View the submission and any attachments by following the link below
and using this unique access code: [REDACTED]

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2. Response to Comments

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2. Response to Comments

R10. Response to Comments from Katie Crofford, dated June 16, 2022.

- R10-1 Responses to Katie Crofford's comments are addressed in response to Comments R10-2 through R10-6 below.
- R10-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R10-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R10-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R10-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R10-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R10-7 Responses to Katie Crofford's comments are addressed in response to Comments R10-2 through R10-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments


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2. Response to Comments

LETTER R11 – Elvia Velazco (3 pages)

Claudia Y. Isbell

From: SeamlessDocs <noreply@seamlessdocs.com>
Sent: Thursday, June 16, 2022 10:39 AM
To: Public Comments
Subject: Public Comment from Elvira Rios



New Submission

A new submission was received for the form below. Log into your Submission Manager to review and process it.

Form name	Public Speaker Form
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Submission Details:

Agenda Item Number or "Public Comment"
My name is Elvira Velazco, educator at YouthBuild Charter School of CA and an 8 year resident of the City of Ontario. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan.

Speaker Full Name
Elvira Rios

Address (optional)
Street Address
[REDACTED]

1

2. Response to Comments

City

Ontario

State

CA

Zip

91761-6751

Phone Number (optional)

[REDACTED]

Subject on which the person wishes to speak?

Against

Briefly describe what you would like to say

My name is Elvira Velazco, educator at YouthBuild Charter School of CA and an 8 year resident of the City of Ontario. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan. On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities. This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes: 5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance. 5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All

R11-1

R11-2

R11-3

2. Response to Comments

agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert. 5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP. 5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations. 5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations. 5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.

R11-3
cont.

R11-4

R11-5

R11-6

R11-7

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[View Submission \[mail.seamlessdocs.com\]](mailto:[REDACTED]@mail.seamlessdocs.com)

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2. Response to Comments

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2. Response to Comments

R11. Response to Comments from Elvia Velazco, dated June 16, 2022.

- R11-1 Responses to Elvia Velazco's comments are addressed in response to Comments R11-2 through R11-6 below.
- R11-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R11-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R11-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R11-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R11-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R11-7 Responses to Elvia Velazco's comments are addressed in response to Comments R11-2 through R11-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments


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2. Response to Comments

LETTER R12 –Aldo Velazco (3 pages)

Claudia Y. Isbell

From: SeamlessDocs <noreply@seamlessdocs.com>
Sent: Thursday, June 16, 2022 10:41 AM
To: Public Comments
Subject: Public Comment from Aldo Velazco



New Submission

A new submission was received for the form below. Log into your Submission Manager to review and process it.

Form name	Public Speaker Form
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Submission Details:

Agenda Item Number or "Public Comment"
My name is Aldo Velazco, and I am an 8 year resident of the City of Ontario. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan.

Speaker Full Name
Aldo Velazco

Address (optional)
Street Address
[REDACTED]

1

2. Response to Comments

City

Ontario

State

CA

Zip

91761-6751

Phone Number (optional)

[REDACTED]

Subject on which the person wishes to speak?

Against

Briefly describe what you would like to say

My name is Aldo Velazco, and I am an 8 year resident of the City of Ontario. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan. On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities. This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes: 5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance. 5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to

R12-1

R12-2

R12-3

2. Response to Comments

industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert. 5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP. 5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations. 5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations. 5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.

R12-3
cont.

R12-4

R12-5

R12-6

R12-7

Email to receive a copy of your submission

View the submission and any attachments by following the link below and using this unique access code: [REDACTED]

[View Submission \[mail.seamlessdocs.com\]](mailto:[REDACTED]@mail.seamlessdocs.com)

City of Ontario - City Clerks | 303 East B Street Ontario, CA 91764

2. Response to Comments

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2. Response to Comments

R12. Response to Comments from Aldo Velazco, dated June 16, 2022.

- R12-1 Responses to Aldo Velazco's comments are addressed in response to Comments R12-2 through R12-6 below.
- R12-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R12-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R12-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R12-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R12-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R12-7 Responses to Aldo Velazco's comments are addressed in response to Comments R12-2 through R12-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R13 – Inez Cardenas (1 page)

Claudia Y. Isbell

From: i.cardenas8309 - [REDACTED]
Sent: Thursday, June 16, 2022 10:43 AM
To: Public Comments
Cc: info@amysfarm.com
Subject: Public Comment to Save Farmland

My name is Inez Cardenas, a 20+ resident of the City of Ontario. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan.

On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). R13-1

These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.

This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes: R13-2

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance.

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert. R13-3

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP. R13-4

5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations. R13-5

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations. R13-6

5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents. R13-7

1

2. Response to Comments

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2. Response to Comments

R13. Response to Comments from Inez Cardenas, dated June 16, 2022.

- R13-1 Responses to Inez Cardenas' comments are addressed in response to Comments R13-2 through R13-6 below.
- R13-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R13-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R13-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R13-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R13-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R13-7 Responses to Inez Cardenas' comments are addressed in response to Comments R13-2 through R13-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R14 – Caroline Lanspa (1 page)

Claudia Y. Isbell

From: Caroline Lanspa [REDACTED]
Sent: Thursday, June 16, 2022 10:52 AM
To: Public Comments
Subject: Opposing approval of the TOP2050 General Plan

Dear City of Ontario,

My name is Caroline Lanspa. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan.

On behalf of the residents and citizens of the surrounding community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). R14-1

These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: **Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.**

This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which include:

R14-2

5. 1-1 TOP2050 would not alter scenic vistas – which is not true as many homes are now being blocked by large warehouse walls which prevent residents from seeing over or past any distance.

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert. R14-3

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP. R14-4

5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations. R14-5

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations. R14-6

In order to be able to make sensible decisions that include the participation of Ontario community and surrounding community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents. R14-7

I appreciate the opportunity to voice my concern.
Best,
Caroline

Sent from [Outlook \[aka.ms\]](#)

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2. Response to Comments

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2. Response to Comments

R14. Response to Comments from Caroline Lanspa, dated June 16, 2022.

- R14-1 Responses to Caroline Lanspa's comments are addressed in response to Comments R14-2 through R14-6 below.
- R14-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R14-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R14-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R14-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R14-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R14-7 Responses to Caroline Lanspa's comments are addressed in response to Comments R14-2 through R14-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments


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2. Response to Comments

LETTER R15 – Daniel Lo (3 pages)

Claudia Y. Isbell

From: SeamlessDocs <noreply@seamlessdocs.com>
Sent: Thursday, June 16, 2022 5:19 PM
To: Public Comments
Subject: Public Comment from Daniel Lo



New Submission

A new submission was received for the form below. Log into your Submission Manager to review and process it.

Form name	Public Speaker Form
------------------	---------------------

Submission Details:

Agenda Item Number or "Public Comment"
Public Comment

Speaker Full Name
Daniel Lo

Subject on which the person wishes to speak?
Against

Briefly describe what you would like to say
My name is Daniel Lo and 12 year resident of the City of Ontario. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050

R15-1

1

2. Response to Comments

General Plan. On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities. This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes: 5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance. 5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert. 5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP. 5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations. 5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.

R15-1
cont.

R15-2

R15-3

R15-4

R15-5

R15-6

R15-7

2. Response to Comments

Email to receive a copy of your submission

View the submission and any attachments by following the link below
and using this unique access code: [REDACTED]

[View Submission \[mail.seamlessdocs.com\]](#)

City of Ontario - City Clerks | 303 East B Street Ontario, CA 91764

2. Response to Comments

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2. Response to Comments

R15. Response to Comments from Daniel Lo, dated June 16, 2022.

- R15-1 Responses to Daniel Lo's comments are addressed in response to Comments R15-2 through R15-6 below.
- R15-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R15-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R15-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R15-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R15-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R15-7 Responses to Daniel Lo's comments are addressed in response to Comments R15-2 through R15-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R16 – Eva Ramirez (1 page)

From: Eva Ramirez <[REDACTED]>
Sent: Thursday, June 16, 2022 9:15:30 PM
To: Kimberly Ruddins <Kruddins@ontarioca.gov>
Subject: Public Comment on The Ontario Plan 2050

My name is Eva Ramirez, I am a long-time resident of the City of Ontario. My parents moved to the City in the 70s and raised their kids here, two of us, along with our families, still live in the City. I am writing on behalf of myself and my family members to respectfully request that you **not approve the TOP2050 General Plan**.

The residents of Ontario are already experiencing water shortages as well as power outages. Furthermore, classrooms, medical facilities, and services are maxed out. For example, to get an appointment at the doctor currently, or be seen for urgent care or the emergency room, takes hours and hours, this has already been going on for several years, pre-pandemic. There are insufficient services currently and would only get further stretched with this additional growth. Furthermore, our roads are overly crowded and our air is overly polluted, especially with the recent boom in warehouse and industrial development that has brought so many large trucks to our roads. We respectfully request that any growth, especially of this magnitude, be given further consideration and include well-thought-out, detailed mitigation plans to protect the health and quality of life of residents.

There are various other cities in the region that have already, or are in the process of, implementing warehouse moratoriums to further study the impacts on health of having warehouses in the community. Some cities have adopted ordinances tightening regulations for warehouse/industrial development. We ask that the City of Ontario consider a moratorium on warehouses as well, and take time to consider major tightening of regulations in order to provide greater protection for the residents.

The TOP2050 proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects. However, the findings of the Draft Supplemental Environmental Impact Report (SEIR) show that this project will have significant and unavoidable impacts and will violate laws on the Federal, State and local levels, which include:

4.1.7. - The Project would convert Prime Farmland to urban development, and it would conflict with existing Williamson Act contract lands. This unavoidable significant impact is consistent with findings of the City's TOP EIR, which implemented the interim Agricultural Overlay District in anticipation of future development for the site. Even with implementation of regulatory requirements, standard conditions of approval, and consideration of mitigation, the Project would result in significant and unavoidable impacts.

Please do not approve TOP2050 General Plan, thereby limiting Greenhouse gas emissions, noise, and traffic in our beloved City, while protecting Ontario air quality, biological resources, agricultural resources, cultural resources, Tribal Cultural Resources and utilities, and the health and quality of life for Ontario residents.

I would appreciate a response confirming your receipt of my email. Thank you so much for your time and attention.

Best,
Eva Ramirez

R16-1

R16-2

R16-3

2. Response to Comments

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2. Response to Comments

R16. Response to Comments from Eva Ramirez, dated June 16, 2022.

- R16-1 Responses to Eva Ramirez's comments are addressed in response to Comments R16-2 below.
- R16-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.2, *Nonrenewal of Williamson Act Contracts*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542).
- R16-3 Responses to Eva Ramirez's comments are addressed in response to Comment R16-2 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R17 – Cara Butler (1 page)

-----Original Message-----
From: "CZ" [REDACTED]
Sent: Friday, June 17, 2022 9:01am
To: info@amysfarm.com
Subject: comment to City of Ontario

1. My name is Cara Butler
This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan.
2. On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). R17-1
3. These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: **Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.**
4. This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes:

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance. R17-2

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert. R17-3

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP. R17-4

Thank you,

Cara Butler

2. Response to Comments

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2. Response to Comments

R17. Response to Comments from Cara Butler, dated June 17, 2022.

- R17-1 Responses to Cara Butler's comments are addressed in response to Comments R17-2 through R17-4 below.
- R17-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R17-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R17-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.

2. Response to Comments


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2. Response to Comments

LETTER R18 – Antoinette Anderson (3 pages)

Claudia Y. Isbell

From: SeamlessDocs <noreply@seamlessdocs.com>
Sent: Friday, June 17, 2022 12:24 PM
To: Public Comments
Subject: Public Comment from Antoinette Anderson



New Submission

A new submission was received for the form below. Log into your Submission Manager to review and process it.

Form name	Public Speaker Form
------------------	---------------------

Submission Details:

Agenda Item Number or "Public Comment"
"Public Comment"

Speaker Full Name
Antoinette Anderson

Subject on which the person wishes to speak?
Against

Briefly describe what you would like to say
1. My name is Antoinette Anderson. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan. 2. On behalf of

R18-1

1

2. Response to Comments

<p>the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). 3. These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities. 4. This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes: 5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance. 5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert. 5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP. 5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations. 5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations. 5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.</p>	R18-1 cont.
	R18-2
	R18-3
	R18-4
	R18-5
	R18-6
	R18-7

2. Response to Comments

Email to receive a copy of your submission

View the submission and any attachments by following the link below
and using this unique access code: [REDACTED]

[View Submission \[mail.seamlessdocs.com\]](#)

City of Ontario - City Clerks | 303 East B Street Ontario, CA 91764

2. Response to Comments

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2. Response to Comments

R18. Response to Comments from Antoinette Anderson, dated June 17, 2022.

- R18-1 Responses to Antoinette Anderson's comments are addressed in response to Comments R18-2 through R18-6 below.
- R18-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R18-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R18-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R18-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R18-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R18-7 Responses to Antoinette Anderson's comments are addressed in response to Comments R18-2 through R18-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R19 – Jackie Deans (1 page)

From: Jackie Deans [REDACTED]
Sent: Friday, June 17, 2022 12:34:50 AM
To: Kimberly Ruddins <Kruddins@ontarioca.gov>
Subject: Ontario plan 2050

I am writing on behalf of myself and my family members to respectfully request that you **not approve the TOP2050 General Plan**.

The residents of Ontario are already experiencing water shortages as well as power outages. Furthermore, classrooms, medical facilities, and services are maxed out. For example, to get an appointment at the doctor currently, or be seen for urgent care or the emergency room, takes hours and hours, this has already been going on for several years, pre-pandemic. There are insufficient services currently and would only get further stretched with this additional growth. Furthermore, our roads are overly crowded and our air is overly polluted, especially with the recent boom in warehouse and industrial development that has brought so many large trucks to our roads. We respectfully request that any growth, especially of this magnitude, be given further consideration and include well-thought-out, detailed mitigation plans to protect the health and quality of life of residents.

There are various other cities in the region that have already, or are in the process of, implementing warehouse moratoriums to further study the impacts on health of having warehouses in the community. Some cities have adopted ordinances tightening regulations for warehouse/industrial development. We ask that the City of Ontario consider a moratorium on warehouses as well, and take time to consider major tightening of regulations in order to provide greater protection for the residents.

The TOP2050 proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects. However, the findings of the Draft Supplemental Environmental Impact Report (SEIR) show that this project will have significant and unavoidable impacts and will violate laws on the Federal, State and local levels, which include:

4.1.7. - The Project would convert Prime Farmland to urban development, and it would conflict with existing Williamson Act contract lands. This unavoidable significant impact is consistent with findings of the City's TOP EIR, which implemented the interim Agricultural Overlay District in anticipation of future development for the site. Even with implementation of regulatory requirements, standard conditions of approval, and consideration of mitigation, the Project would result in significant and unavoidable impacts.

Please do not approve TOP2050 General Plan, thereby limiting Greenhouse gas emissions, noise, and traffic in our beloved City, while protecting Ontario air quality, biological resources, agricultural resources, cultural resources, Tribal Cultural Resources and utilities, and the health and quality of life for Ontario residents.

Respectfully,
Jackie

[Sent from Yahoo Mail for iPhone \[overview.mail.yahoo.com\]](#)

R19-1

R19-2

R19-3

2. Response to Comments

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2. Response to Comments

R19. Response to Comments from Jackie Deans, dated June 17, 2022.

- R19-1 Responses to Jackie Dean's comments are addressed in response to Comments R19-2 below.
- R19-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.2, *Nonrenewal of Williamson Act Contracts*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542).
- R19-3 Responses to Jackie Dean's comments are addressed in response to Comment R19-2 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments


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2. Response to Comments

LETTER R20 – Danielle Martinez (3 pages)

Claudia Y. Isbell

From: SeamlessDocs <noreply@seamlessdocs.com>
Sent: Thursday, June 16, 2022 5:19 PM
To: Public Comments
Subject: Public Comment from Daniel Lo



New Submission

A new submission was received for the form below. Log into your Submission Manager to review and process it.

Form name	Public Speaker Form
------------------	---------------------

Submission Details:

Agenda Item Number or "Public Comment"
Public Comment

Speaker Full Name
Daniel Lo

Subject on which the person wishes to speak?
Against

Briefly describe what you would like to say
My name is Daniel Lo and 12 year resident of the City of Ontario. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050

R15-1

1

2. Response to Comments

CA

Zip

92399

Phone Number (optional)

[REDACTED]

Name of Organization

SBCSS

Subject on which the person wishes to speak?

Against

Briefly describe what you would like to say

My name is Danielle Martinez, Member of San Bernardino County Superintendent of Schools. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan. On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities. This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes: 5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance. 5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in

R20-1

R20-2

R20-3

2. Response to Comments

violation of 65302(h)(1)(A) Access to food; creating a food desert. 5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP. 5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations. 5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations. 5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.

R20-3 cont.

R20-4

R20-5

R20-6

R20-7

Email to receive a copy of your submission

[REDACTED]

View the submission and any attachments by following the link below and using this unique access code: [REDACTED]

[View Submission \[mail.seamlessdocs.com\]](mailto:seamlessdocs.com)

City of Ontario - City Clerks | 303 East B Street Ontario, CA 91764

2. Response to Comments

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2. Response to Comments

R20. Response to Comments from Danielle Martinez, dated June 17, 2022.

- R20-1 Responses to Danielle Martinez’s comments are addressed in response to Comments R20-2 through R20-6 below.
- R20-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City’s Development Code. Finally, the Commenter’s concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R20-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R20-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R20-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R20-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R20-7 Responses to Danielle Martinez’s comments are addressed in response to Comments R20-2 through R20-6 above. The Commenter’s opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments


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2. Response to Comments

LETTER R21 – Cynthia Serrano (3 pages)

Claudia Y. Isbell

From: SeamlessDocs <noreply@seamlessdocs.com>
Sent: Sunday, June 19, 2022 1:11 PM
To: Public Comments
Subject: Public Comment from Cynthia Serrano



New Submission

A new submission was received for the form below. Log into your Submission Manager to review and process it.

Form name	Public Speaker Form
------------------	---------------------

Submission Details:

Agenda Item Number or "Public Comment"
Public Comment

Speaker Full Name
Cynthia Serrano

Address (optional)
Street Address
[REDACTED]

City
Ontario

State

1

2. Response to Comments

CA

Zip

91762

Subject on which the person wishes to speak?

Against

Briefly describe what you would like to say

My name is Cynthia Serrano and I am a 2 year resident of the City of Ontario. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan. On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities. This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes: 5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance. 5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert. 5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP. 5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations. 5.3-4 Operation of industrial and warehousing land uses

R21-1

R21-2

R21-3

R21-4

R21-5

R21-6

2. Response to Comments

would expose sensitive receptors to substantial toxic air contaminant concentrations. 5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.

R21-6 cont.

R21-7

Email to receive a copy of your submission

[REDACTED]

View the submission and any attachments by following the link below and using this unique access code: [REDACTED]

[View Submission \[mail.seamlessdocs.com\]](mailto:seamlessdocs.com)

City of Ontario - City Clerks | 303 East B Street Ontario, CA 91764

2. Response to Comments

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2. Response to Comments

R21. Response to Comments from Cynthia Serrano, dated June 19, 2022.

- R21-1 Responses to Cynthia Serrano’s comments are addressed in response to Comments R21-2 through R21-6 below.
- R21-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City’s Development Code. Finally, the Commenter’s concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R21-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R21-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R21-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R21-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R21-7 Responses to Cynthia Serrano’s comments are addressed in response to Comments R21-2 through R21-6 above. The Commenter’s opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments


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2. Response to Comments

LETTER R22 – Tina Silva (3 pages)

Claudia Y. Isbell

From: SeamlessDocs <noreply@seamlessdocs.com>
Sent: Sunday, June 19, 2022 3:03 PM
To: Public Comments
Subject: Public Comment from TINA SILVA



New Submission

A new submission was received for the form below. Log into your Submission Manager to review and process it.

Form name	Public Speaker Form
------------------	---------------------

Submission Details:

Agenda Item Number or "Public Comment"
PUBLIC COMMENT

Speaker Full Name
TINA SILVA

Address (optional)
Street Address
[REDACTED]

City
ONTARIO

State

1

2. Response to Comments

CA

Zip

91762

Subject on which the person wishes to speak?

Against

Briefly describe what you would like to say

My name is Tina Silva and I have been a resident of Ontario for 12 years. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan. On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities. This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes: 5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance. 5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert. 5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP. 5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations. 5.3-4 Operation of industrial and warehousing land uses

R22-1

R22-2

R22-3

R22-4

R22-5

R22-6

2. Response to Comments

would expose sensitive receptors to substantial toxic air contaminant concentrations. 5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.

R22-6 cont.

R22-7

Email to receive a copy of your submission

View the submission and any attachments by following the link below and using this unique access code: [REDACTED]

[View Submission \[mail.seamlessdocs.com\]](#)

City of Ontario - City Clerks | 303 East B Street Ontario, CA 91764

2. Response to Comments

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2. Response to Comments

R22. Response to Comments from Tina Silva, dated June 19, 2022.

- R22-1 Responses to Tina Silva's comments are addressed in response to Comments R22-2 through R22-6 below.
- R22-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R22-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R22-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R22-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R22-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R22-7 Responses to Tina Silva's comments are addressed in response to Comments R22-2 through R22-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R23 – Jamie Ortega (1 page)

From: Jamie G [REDACTED]
Sent: Tuesday, June 21, 2022 4:42 AM
To: Kimberly Ruddins <kruddins@ontarioca.gov>
Subject: Delay the approval of the TOP2050

1. My name is Jamie Ortega, Member of LULAC #3286 and 10 year resident of the City of Ontario. This is notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan. R23-1

2. On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR).

3. These findings from the Draft SEIR examines the potential environmental impacts as a result of this project which includes the following: **Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.**

4. This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes:

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance. R23-2

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert. R23-3

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP. R23-4

5.3-3 would generate long term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations. R23-5

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations. R23-6

5. In order to be able to make sensible decisions which include participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents. R23-7

Best,
Jamie Ortega

2. Response to Comments

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2. Response to Comments

R23. Response to Comments from Jamie Ortega, dated June 21, 2022.

- R23-1 Responses to Jamie Ortega's comments are addressed in response to Comments R23-2 through R23-6 below.
- R23-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R23-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R23-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R23-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R23-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R23-7 Responses to Jamie Ortega's comments are addressed in response to Comments R23-2 through R23-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R24 – Dessa D’Aquila Bentson (1 page)

-----Original Message-----
From: [REDACTED]
Sent: Tuesday, June 21, 2022 6:09am
To: info@amysfarm.com
Subject: Re: Public Comment Template to save farmland

1. My name is Dessa D’Aquila Bentson. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission’s approval of the TOP2050 General Plan.

2. On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). R24-1

3. These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: **Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.**

4. This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes:

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance. R24-2

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert. R24-3

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD’s AQMP. R24-4

5.3-3 would generate long-term emissions in excess of South Coast AQMD’s threshold criteria & cumulatively contribute to the South Coast Air Basin’s nonattainment designations. R24-5

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations. R24-6

5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents. R24-7
Sent from my iPhone

2. Response to Comments

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2. Response to Comments

R24. Response to Comments from Dessa D'Aquila Bentson, dated June 21, 2022.

- R24-1 Responses to Dessa D'Aquila Bentson's comments are addressed in response to Comments R24-2 through R24-6 below.
- R24-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R24-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R24-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R24-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R24-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R24-7 Responses to Dessa D'Aquila Bentson's comments are addressed in response to Comments R24-2 through R24-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments


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2. Response to Comments

LETTER R25 – Malina Shaw (3 pages)

Claudia Y. Isbell

From: SeamlessDocs <noreply@seamlessdocs.com>
Sent: Tuesday, June 21, 2022 10:00 AM
To: Public Comments
Subject: Public Comment from Malina Shaw



New Submission

A new submission was received for the form below. Log into your Submission Manager to review and process it.

Form name	Public Speaker Form
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Submission Details:

Agenda Item Number or "Public Comment"
STOP TOP2050

Speaker Full Name
Malina Shaw

Address (optional)
Street Address
[REDACTED]

City
Ontario

State

1

2. Response to Comments

CA

Zip

91762

Phone Number (optional)

██████████

Subject on which the person wishes to speak?

Against

Briefly describe what you would like to say

My name is Malina Shaw, 8 year resident of the City of Ontario. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan. On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities. This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes: 5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance. 5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert. 5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP. 5.3-3 would generate long-term emissions in excess

R25-1

R25-2

R25-3

R25-4

R25-5

2. Response to Comments

of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations. 5.3-4
Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations.

R25-5
cont.

R25-6

Email to receive a copy of your submission

[Redacted]

View the submission and any attachments by following the link below and using this unique access code: [Redacted]

[View Submission \[mail.seamlessdocs.com\]](mailto:seamlessdocs.com)

City of Ontario - City Clerks | 303 East B Street Ontario, CA 91764

2. Response to Comments

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2. Response to Comments

R25. Response to Comments from Malina Shaw, dated June 21, 2022.

- R25-1 Responses to Malina Shaw's comments are addressed in response to Comments R25-2 through R25-6 below.
- R25-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R25-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R25-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R25-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R25-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R25-7 Responses to Malina Shaw's comments are addressed in response to Comments R25-2 through R25-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R26 – Lisa Collinsworth (1 page)

<p>1. My name is <u>Lisa Collinsworth</u>, and <u>51 year</u> resident of the City of Ontario. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan.</p>	R26-1
<p>2. On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR).</p>	R26-1
<p>3. These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.</p>	
<p>4. This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes:</p>	
<p>5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance.</p>	R26-2
<p>5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert.</p>	R26-3
<p>5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP.</p>	R26-4
<p>5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations.</p>	R26-5
<p>5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations.</p>	R26-6
<p>5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.</p>	R26-7

2. Response to Comments

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2. Response to Comments

R26. Response to Comments from Lisa Collinsworth, dated June 21, 2022.

- R26-1 Responses to Lisa Collinsworth's comments are addressed in response to Comments R26-2 through R26-6 below.
- R26-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R26-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R26-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R26-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R26-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R26-7 Responses to Lisa Collinsworth's comments are addressed in response to Comments R26-2 through R26-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R27 – Jarius Martinez (1 page)

-----Original Message-----
From: "Jarius Martinez" [REDACTED]
Sent: Tuesday, June 21, 2022 12:58pm
To: info@amysfarm.com
Subject: Public Comment

1. My name is Jarius Martinez and I am a lifelong resident of the Inland Empire and went to school in Ontario. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan.

2. On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR).

3. These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: **Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.**

4. This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes:

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance.

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food, creating a food desert.

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP.

5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations.

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations.

5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.

R27-1

R27-2

R27-3

R27-4

R27-5

R27-6

R27-7

2. Response to Comments

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2. Response to Comments

R27. Response to Comments from Jarius Martinez, dated June 21, 2022.

- R27-1 Responses to Jarius Martinez's comments are addressed in response to Comments R27-2 through R27-6 below.
- R27-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R27-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
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- R27-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R27-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R27-7 Responses to Jarius Martinez's comments are addressed in response to Comments R27-2 through R27-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R28 – Raquel Alba (1 page)

-----Original Message-----
From: "Raquel Garcia" <[REDACTED]>
Sent: Tuesday, June 21, 2022 2:39pm
To: info@amysfarm.com
Subject: Public comments

Hi, my name is Raquel Alba. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan.

On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). R28-1

These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.

This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes: R28-2

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance.

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert. R28-3

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP. R28-4

5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations. R28-5

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations. R28-6

5. In order to be able to make sensible decisions that include the participation of Ontario communi organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents. R28-7

Please take these considerations to heart.

Sincerely,
Raquel Alba

2. Response to Comments

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2. Response to Comments

R28. Response to Comments from Raquel Alba, dated June 21, 2022.

- R28-1 Responses to Raquel Alba's comments are addressed in response to Comments R28-2 through R28-6 below.
- R28-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R28-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R28-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R28-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R28-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R28-7 Responses to Raquel Alba's comments are addressed in response to Comments R28-2 through R28-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R29 – Jocelyn Dimacali (2 pages)

From: Jo Dmac [REDACTED]
Sent: Wednesday, June 22, 2022 3:44 PM
To: Thomas Grahn <TGrahn@ontarioca.gov>
Subject: STOP Ontario General Plan 2050

Hi, my name is Jocelyn Dimacali. I have been a resident of Ontario for four years. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan.

On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR).

R29-1

These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.

This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes:

R29-2

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance.

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert.

R29-3

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP.

R29-4

5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations.

R29-5

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations.

R29-6

5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.

R29-7

2. Response to Comments

Please take these considerations seriously.

Sincerely,

Jocelyn Dimacali

R29-7
cont.

2. Response to Comments

R29. Response to Comments from Jocelyn Dimacali, dated June 22, 2022.

- R29-1 Responses to Jocelyn Dimacali's comments are addressed in response to Comments R29-2 through R29-6 below.
- R29-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R29-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R29-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R29-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R29-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R29-7 Responses to Jocelyn Dimacali's comments are addressed in response to Comments R29-2 through R29-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R30 – Amber Bound (1 page)

From: Bound, Amber - [REDACTED]
Sent: Wednesday, June 22, 2022 3:50 PM
To: Thomas Grahn <TGrahn@ontarioca.gov>
Subject: Ontario General Plan 2050

Hi, my name is Amber Bound. I have been a resident of Ontario for 20 years. This is a notification that I, along with several individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan.

On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR).

These findings from the Draft SEIR examine the potential environmental impacts because of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.

This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State, and local levels which includes:

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we cannot see over or past any distance.

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food, creating a food desert.

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP.

5.3-3 would generate long-term emissions more than South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations.

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations.

5. To be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources, and utilities, as well as the health and quality of life of Ontario residents.

Please take these considerations seriously.

Sincerely,

Amber Bound – she/her
Lead Library Tech
abound@mtsac.edu
Mt San Antonio College Library

R30-1

R30-2

R30-3

R30-4

R30-5

R30-6

R30-7

2. Response to Comments

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2. Response to Comments

R30. Response to Comments from Amber Bound, dated June 22, 2022.

- R30-1 Responses to Amber Bound's comments are addressed in response to Comments R30-2 through R30-6 below.
- R30-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R30-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R30-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R30-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R30-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R30-7 Responses to Amber Bound's comments are addressed in response to Comments R30-2 through R30-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R31 – Andrea Alvarado (1 page)

-----Original Message-----
From: Drea Alvarado [REDACTED]
Sent: Wednesday, June 22, 2022 4:45 PM
To: Thomas Grahn <T.Grahn@ontarioca.gov>
Subject: Ontario Plan 2050

Hi, my name is Andrea Alvarado . I have been a resident of Ontario for over 22 years. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission’s approval of the TOP2050 General Plan.

On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR).

These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.

This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes:

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance.

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert.

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD’s AQMP.

5.3-3 would generate long-term emissions in excess of South Coast AQMD’s threshold criteria & cumulatively contribute to the South Coast Air Basin’s nonattainment designations.

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations.

5. In order to be able to make sensible decisions that include the participation of Ontario community organization residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.

Please take these considerations seriously.

Sincerely,
Andrea Alvarado

R31-1

R31-2

R31-3

R31-4

R31-5

R31-6

R31-7

2. Response to Comments

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2. Response to Comments

R31. Response to Comments from Andrea Alvarado, dated June 22, 2022.

- R31-1 Responses to Andrea Alvarado's comments are addressed in response to Comments R31-2 through R31-6 below.
- R31-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R31-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
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- R31-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R31-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R31-7 Responses to Andrea Alvarado's comments are addressed in response to Comments R31-2 through R31-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R32 – Deborah Rodriguez (1 page)

From: [Debbie Rodriguez](#)
To: tgrabn@ontario.gov
Subject: Ontario general plan 2050
Date: Wednesday, June 22, 2022 5:36:37 PM

Thomas Grabn

Public comments give organizations legal standing to sue to stop the process of approving the general plan and in return receive community benefit agreements that are developed with the health of our community members in mind.

Hi, my name is Deborah Rodriguez. I have been a resident of Ontario for 73 years. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan.

R32-1

On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR).

These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.

This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes:

R32-2

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance.

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert.

R32-3

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP.

R32-4

5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations.

R32-5

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations.

R32-6

5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.

R32-7

Please take these considerations seriously.

Sincerely,

Deborah J Rodriguez

2. Response to Comments

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2. Response to Comments

R32. Response to Comments from Deborah Rodriguez, dated June 22, 2022.

- R32-1 Responses to Deborah Rodriguez’s comments are addressed in response to Comments R32-2 through R32-6 below.
- R32-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City’s Development Code. Finally, the Commenter’s concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R32-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R32-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R32-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R32-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R32-7 Responses to Deborah Rodriguez’s comments are addressed in response to Comments R32-2 through R32-6 above. The Commenter’s opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R33 – Amanda Wheeler (2 pages)

From: Amanda Wheeler [REDACTED]
Sent: Wednesday, June 22, 2022 5:39 PM
To: Thomas Grahn <TGrahn@ontarioca.gov>
Subject:

Hi, my name is Amanda Wheeler. I have been a resident of Ontario for 6 years. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan.

On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR).

These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.

This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes:

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance.

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert.

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP.

5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations.

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations.

5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.

R33-1

R33-2

R33-3

R33-4

R33-5

R33-6

R33-7

2. Response to Comments

Please take these considerations seriously.

Sincerely,
Amanda Wheeler

R33-7
cont.

2. Response to Comments

R33. Response to Comments from Amanda Wheeler, dated June 22, 2022.

- R33-1 Responses to Amanda Wheeler’s comments are addressed in response to Comments R33-2 through R33-6 below.
- R33-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City’s Development Code. Finally, the Commenter’s concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R33-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R33-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R33-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R33-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R33-7 Responses to Amanda Wheeler’s comments are addressed in response to Comments R33-2 through R33-6 above. The Commenter’s opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R34 – Dena Apodaca (2 pages)

From: Dena Apodaca [REDACTED]
Sent: Wednesday, June 22, 2022 9:00 PM
To: Thomas Grahn <TGrahn@ontarioca.gov>
Subject: TOP2050 General Plan

Hello my name is Dena Apodaca. I have been a resident of Ontario for over 40 years. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan.

On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR).

R34-1

These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.

This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes:

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance.

R34-2

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert.

R34-3

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP.

R34-4

5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations.

R34-5

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations.

R34-6

2. Response to Comments

5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.

R34-7

Please take these considerations seriously.

Sincerely,

Dena Apodaca

2. Response to Comments

R34. Response to Comments from Dena Apodaca, dated June 22, 2022.

- R34-1 Responses to Dena Apodaca's comments are addressed in response to Comments R34-2 through R34-6 below.
- R34-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R34-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R34-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R34-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R34-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R34-7 Responses to Dena Apodaca's comments are addressed in response to Comments R34-2 through R34-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R35 – Andrei Pitsko (2 pages)

From: [REDACTED]
Sent: Wednesday, June 22, 2022 9:57 PM
To: Thomas Grahn <T.Grahn@ontarioca.gov>
Subject: Public comment about General Plan 2050

Hi, my name is Andrei Pitsko. I have been a resident of Ontario for 2 years
This notification that I, along with a number of individuals are opposed to the Planning Commission's approval of TOP2050 General Plan

I request that you take action to deny the approval of the TOP2050 General Plan

On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land for Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). R35-1

These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.

This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes: R35-2

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance.

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert. R35-3

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP. R35-4

5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations. R35-5

2. Response to Comments

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations.

R35-6

5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.

R35-7

Please take these considerations seriously.

2. Response to Comments

R35. Response to Comments from Andrei Pitsko, dated June 22, 2022.

- R35-1 Responses to Andrei Pitsko's comments are addressed in response to Comments R35-2 through R35-6 below.
- R35-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R35-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R35-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R35-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R35-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R35-7 Responses to Andrei Pitsko's comments are addressed in response to Comments R35-2 through R35-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R36 – Randy Bekendam (2 pages)

Dear Mayor and City Council members:

We would like to provide public comment on the City of Ontario's Preferred Land Use Plan dated 9-22-2021.

Over the past few years, conditions of Covid have limited public engagement and participation in the political process as well as limited movement through and use of the community during this same time period. Also, in this same time period, we have seen the city accelerate certain aspects of municipal development (such as the development of warehouses) and development of long term municipal plans. The disruptions of Covid continue into the present and we would like to elevate our concern for stewarding good public process despite a period of significant disruption and stress for most citizens. We do not want to take steps that a truly knowledgeable and consenting public would not take under ordinary circumstances. We believe more efforts are needed to get true public input about the loss of agricultural land, environmental impacts, community benefits, and the proposed developments put forth in the Preferred Land Use Plan.

We would like to provide the reminder that it is much easier to alter the plan, than to alter harms that are caused as a result of building according to a plan that overlooked community and environmental harm. We believe that this is more of a "measure twice, cut once" type of scenario and would like the city to publish and present its complete findings on community and environmental benefits and harms of the current plan compared to alternatives presented by the community before moving forward with this plan. We are requesting that no implementation of the current plan take place for at least 1 years time so that the public has an opportunity to appropriately engage and the city has sufficient time to incorporate public input and address the public's concerns.

Our community has great vision for community and environmental health, self determination, compassion, sustainability, beauty, harmony, cooperation. Our community also has basic needs for food, housing, and community. Our plans must ensure that these goals and needs can be satisfied for the residents of our city. If instead the plans favor loss of our most basic collective resources and the exporting of our shared wealth to a powerful and well-resourced minority, we must slow down and be discerning! We must slow down and get more creative!

Currently, much of the Ontario Ranch remains undeveloped as is envisioned by the Preferred Land Use Plan, including the development of a conceptual Great Park. We support the development of the Great Park and encourage expanding on this vision with concrete plans for community and environmental stewardship. The Southern California Agricultural Land Foundation proposes the addition of another concept: a permanent Agricultural Heritage and Innovation (AHI) zone adjacent to the southern border of the Great Park. The suggested boundaries of this AHI zone would be Sultana Ave. to the west, the Great Park to the north, Cucamonga Creek Wash to the east and Eucalyptus Ave to the South. We recommend an AHI zone of a minimum 300 acres that is intended to be placed--**and is actually placed**--under a conservation easement once ownership is secured. The Southern California Agricultural Land Foundation is available to administer the easement.

R36-1

R36-2

2. Response to Comments

Elements and benefits of the AHI zone include

- A much needed buffer between the warehouses south of Eucalyptus Ave and the Great Park
- Conversion of the 300+ acres to regenerative agricultural practices to draw down legacy carbon and provide nutrient dense fruits and vegetables to alleviate food insecurity in Ontario
- Incorporation of agrivoltaics to move ontario towards carbon neutrality
- Incorporation of approximately 120 much needed affordable housing units (A2 zoning)
- Creation of habitat for endangered species
- Preservation of a dairy as a living history museum and educational facility, open to the public
- Preservation of Amy's Farm

R36-3

We would like to see our comments addressed in a meaningful way. Please provide the documentation we have requested regarding community and environmental impacts of the proposed plan and alternative plans you have received. Please publish and present these findings in partnership with concerned citizen groups. We would like to meet with you to discuss further development and review of any preferred land use plans, timelines and community engagement efforts, and would be happy to connect you to other locally based community groups which share our concerns for the welfare of our community and the local environment.

R36-4

Randy Bekendam

Randy Bekendam
On behalf of the Southern California Agricultural Land Foundation

2. Response to Comments

R36. Response to Comments from Randy Bekendam, dated June 22, 2022.

R36-1 Responses to Randy Bekendam's comments are addressed in response to Comments R36-2 through R36-4 below.

The City of Ontario complied with requirements in place for noticing in accordance with Governor Newsom's Executive Orders N-54-20 (April 22, 2020) and N-80-20 (September 23, 2020) in response to the threat of COVID-19, which expired on September 30, 2021. TOP 2050 complied with the requirements of Senate Bill 1000. As part of TOP 2050 disadvantaged communities were identified using CES 4.0. See Section 2.2, *Summary of General Responses*, and Section 2.2.3, *Environmental Justice Policies*.

The public review period of 45 days, beginning on May 9, 2022, and ending on June 23, 2022, met CEQA's requirement of 45 days, pursuant to CEQA Guidelines Section 15105(a) for Draft EIRs submitted to the State Clearinghouse for public review by State agencies.

An evaluation of the environment impacts of TOP 2050 compared to the current TOP is included in Section 5.2, *Agriculture and Forestry Resources*. The Commenter's concerns about the loss of agricultural land are noted and the request for additional time will be forwarded to the decision-makers for their review and consideration.

R36-2 The SEIR is a programmatic, plan-level analysis of impacts of the City's general plan, TOP 2050. The SEIR evaluates impacts of TOP 2050 compared to the impacts associated with the current TOP in accordance with evaluates the physical environmental impacts associated with Section 15163 of the CEQA Guidelines. In accordance with CEQA Guidelines Section 15163, the CEQA baseline for this SEIR is the Approved Project. The Draft SEIR evaluates the potential impacts from the change in land use designations and policies of the current TOP compared to TOP 2050. While there is currently agricultural use in the Ontario Ranch, the planning framework to convert the land to nonagricultural land already occurred as a result of adoption of the current TOP in 2010, and prior to that as a result of annexation of the New Model Colony in 1999 from the County of San Bernardino. The Proposed Project would not result in any additional land use changes to agricultural land in Ontario.

This comment identifies a request to create an agricultural preserve in the Ontario Ranch and does not raise any issues related to the Proposed Project or the Proposed Project's impact on the physical environment under CEQA. The request for creation of the Southern California Agricultural Land Foundation Agricultural Heritage and Innovation (AHI) zone within and/or adjacent to the Great Park will be forwarded to the decision-makers for their review and consideration. The 340-acre Great Park allows for continues agricultural use and existing agricultural use is allowed to continue under the City's Right-to-Farm Ordinance. Additionally, the Great Park envisions creation of a Farm Hub as well as potential agricultural uses.

2. Response to Comments

R36-3 See response to Comment R36-2 regarding creation of an AHI Zone.

TOP 2050 includes Land Use Element Policy LU2-1, Land Use Decisions, which requires new development to minimize impacts on adjacent properties, which could include green buffers as well as other site design measures such as placement of docking bays, sound walls, landscaping, and other setback requirements. Future industrial land uses would be required to adhere to Policy ER-4.9, which requires new developments that generate 100 trucks per day or more or 40 trucks with transport refrigeration units (TRUs) within 1,000 feet of a sensitive use to conduct a health risk assessment to mitigate cancer and acute health risks as well as additional public outreach to residents within 500 feet of the project site. The request to provide a buffer between warehouses and the Great Park will be forwarded to decision-makers for their review and consideration.

The GHG analysis in the Draft SEIR and the CCAP do not treat agricultural land uses as a sink for GHG emissions. While crops do sequester carbon due to the growth in biomass, the Intergovernmental Panel on Climate Change's guidance¹² on the topic notes that for annual crops, the increase in biomass is equal to the loss in biomass from harvest and crop mortality, and thus there is no net accumulation of carbon in agricultural biomass. As more than 90 percent of the crop activity in Ontario is estimated to be annual crops, there is no substantive increase in agricultural biomass in Ontario. Additionally, agriculture does generate GHG emissions from nitrogen used for fertilizer, enteric fermentation and manure management associated with livestock, fuel use from agricultural equipment, and other activities. The GHG analysis does consider that an amount of carbon is sequestered in the soil of agricultural lands, but as this does not change annually, this source only shows as an increase or decrease in GHG emissions if the agricultural land is expanded or developed. Loss of this sequestration from the development of agricultural lands is accounted for in the "Land Use Change" emissions shown in Table 14 of the CCAP. TOP 2050 includes policies that address impacts to disadvantaged communities and access to healthy foods (see Policy SR-1.2) (see Section 2.2, *Summary of General Responses*, and Section 2.2.3, *Environmental Justice Policies*).

Regarding agrivoltaics, the Community Climate Action Plan (CCAP) supports the installation of solar panels and solar panels are permitted within the City, including Ontario Ranch.

Section 5.4, *Biological Resources*, evaluates potential impacts of TOP 2050 compared to the current TOP. As identified in Section 5.4, the Ontario Ranch supports little native vegetation as a result of long-standing agricultural use (see page 5.4-6). However, sensitive species are known to occur within Ontario; and therefore, to mitigate impacts to biological resources in the City, the City of Ontario has established a Biological Resources Habitat

¹² Intergovernmental Panel on Climate Change. 2019 Refinement to the 2006 IPCC Guidelines for National Greenhouse Gas Inventories, Volume 4: Agriculture, Forestry, and Other Land Use; Section 5.2.1.1 <https://www.ipcc-nggip.iges.or.jp/public/2019rf/vol4.html>

2. Response to Comments

Mitigation Fee for projects in the Ontario Ranch. The request to create habitat for endangered species will be forwarded to the decision-makers for their review and consideration.

The request to create a living history museum and educational facility for dairy in Ontario Ranch will be forwarded to the decision-makers for their review and consideration. As noted in response to Comment R36-2, the Great Park includes a Farm Hub.

The request to preserve Amy's Farm has been forwarded to decision-makers for their review and consideration.

- R36-4 Responses to Randy Bekendam's comments are addressed in response to Comments R36-2 through R36-3 above. The environmental impacts of the Proposed Project are addressed in the Draft SEIR, which is available for review on the City's website. The request to meet will be forwarded to decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R37 – Elizabeth Rocco (1 page)

-----Original Message-----

From: Liz's [REDACTED]
Sent: Thursday, June 23, 2022 8:01 AM
To: Thomas Grahn <T.Grahn@ontarioca.gov>
Subject: Ontario general plan

Hi, my name is Elizabeth Rocco. I have been a resident of Ontario for 24 years. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan.

On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR).

R37-1

These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.

This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes:

R37-2

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance.

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert.

R37-3

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP.

R37-4

5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations.

R37-5

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations.

R37-6

5. In order to be able to make sensible decisions that include the participation of Ontario community organization residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.

Please take these considerations seriously.

R37-7

Sincerely,

Elizabeth(Liz) Rocco

2. Response to Comments

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2. Response to Comments

R37. Response to Comments from Elizabeth Rocco, dated June 23, 2022.

- R37-1 Responses to Elizabeth Rocco's comments are addressed in response to Comments R37-2 through R37-6 below.
- R37-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R37-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R37-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R37-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R37-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R37-7 Responses to Elizabeth Rocco's comments are addressed in response to Comments R37-2 through R37-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R38 – Martha Lopez (2 pages)

From: Jose Lopez [REDACTED]
Sent: Thursday, June 23, 2022 9:13 AM
To: Thomas Grahn <TGrahn@ontarioca.gov>
Subject: Ontario Plan 2050

Hi, my name is Martha Lopez.

I have been a resident of Ontario for 28 years. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan.

On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR).

R38-1

These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.

This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes:

R38-2

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance.

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert.

R38-3

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP.

R38-4

2. Response to Comments

5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations.

R38-5

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations.

R38-6

5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.

Please take these considerations seriously.

R38-7

Sincerely,

Martha Lopez



2. Response to Comments

R38. Response to Comments from Martha Lopez, dated June 23, 2022.

- R38-1 Responses to Martha Lopez's comments are addressed in response to Comments R38-2 through R38-6 below.
- R38-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R38-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
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- R38-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R38-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R38-7 Responses to Martha Lopez's comments are addressed in response to Comments R38-2 through R38-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R39 – Karen and Guy Caparelli (2 pages)

From: Karen Caparelli [REDACTED]
Sent: Thursday, June 23, 2022 4:57 PM
To: Thomas Grahn <TGrahn@ontarioca.gov>
Subject: TOP2050 General Plan

Hi, my name is Karen Caparelli. I have been a resident of Ontario for 38 years. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan.

On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR).

R39-1

These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.

This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes:

R39-2

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance.

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert.

R39-3

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP.

R39-4

5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations.

R39-5

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations.

R39-6

5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.

R39-7

2. Response to Comments

Please take these considerations seriously.

Sincerely,

Karen and Guy Caparelli

R39-7
cont.

2. Response to Comments

R39. Response to Comments from Karen and Guy Caparelli, dated June 23, 2022.

- R39-1 Responses to Karen and Guy Caparelli's comments are addressed in response to Comments R39-2 through R39-6 below.
- R39-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R39-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R39-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R39-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R39-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R39-7 Responses to Karen and Guy Caparelli's comments are addressed in response to Comments R39-2 through R39-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R40 – Elias Luviano (2 pages)

From: Rosemary Ramos [REDACTED]
Sent: Thursday, June 23, 2022 5:06 PM
To: Thomas Grahn <TGrahn@ontarioca.gov>
Subject: Ontario 2050 plan

"Hi, my name is Elias Luviano. I have been a resident of Ontario for 57 years. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan.

On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR).

These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.

This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes:

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance.

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert.

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP.

5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations.

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations.

5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.

R40-1

R40-2

R40-3

R40-4

R40-5

R40-6

R40-7

2. Response to Comments

Please take these considerations Sincerely
Elias Luviano

R40-7
cont.

2. Response to Comments

R40. Response to Comments from Elias Luviano, dated June 23, 2022.

- R40-1 Responses to Elias Luviano's comments are addressed in response to Comments R40-2 through R40-6 below.
- R40-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R40-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R40-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R40-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R40-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R40-7 Responses to Elias Luviano's comments are addressed in response to Comments R40-2 through R40-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R41 – Nazareth Bracamontes (1 page)

-----Original Message-----

From: nazareth bracamontes [REDACTED]
Sent: Thursday, June 23, 2022 5:17 PM
To: Thomas Grahn <TGrahn@ontarioca.gov>
Subject:

Hi, my name is Nazareth Bracamontes. I have been a resident of Ontario for 12 years. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan.

On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR).

R41-1

These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.

This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes:

R41-2

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance.

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert.

R41-3

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP.

R41-4

5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations.

R41-5

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations.

R41-6

5. In order to be able to make sensible decisions that include the participation of Ontario community organization residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.

R41-7

Please take these considerations seriously.

Sincerely,

Nazareth Bracamontes

2. Response to Comments

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2. Response to Comments

R41. Response to Comments from Nazareth Bracamontes, dated June 23, 2022.

- R41-1 Responses to Nazareth Bracamontes' comments are addressed in response to Comments R41-2 through R41-6 below.
- R41-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R41-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R41-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R41-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R41-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R41-7 Responses to Nazareth Bracamontes' comments are addressed in response to Comments R41-2 through R41-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R42 – Estela Ballon (2 pages)

From: Estela Ballon [REDACTED]
Date: June 23, 2022 at 9:36:24 PM PDT
To: Thomas Grahn <TGrahn@ontarioca.gov>
Subject: opposition to the Planning Commission’s approval of the TOP2050 General Plan

Hi, my name is Estela Ballon. I have been a resident of Ontario for almost 20 years. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission’s approval of the TOP2050 General Plan.

On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR).

R42-1

These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.

This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes:

R42-2

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance.

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert.

R42-3

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD’s AQMP.

R42-4

2. Response to Comments

5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations.

R42-5

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations.

R42-6

5. In order to be able to make sensible decisions that include the participation of Ontario community organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents.

R42-7

Please take these considerations seriously.

Sincerely,
Estela Ballon

2. Response to Comments

R42. Response to Comments from Estela Ballon, dated June 23, 2022.

- R42-1 Responses to Estela Ballon's comments are addressed in response to Comments R42-2 through R42-6 below.
- R42-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R42-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R42-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R42-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R42-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R42-7 Responses to Estela Ballon's comments are addressed in response to Comments R42-2 through R42-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration.

2. Response to Comments

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2. Response to Comments

LETTER R43 – Rosemary Ramos (2 pages)

From: Rosemary Ramos [REDACTED]
Sent: Thursday, June 23, 2022 4:19 PM
To: Thomas Grahn <TGrahn@ontarioca.gov>
Subject: Ontario 2050 plan

"Hi, my name is Rosemary Ramos I have been a resident of Ontario for 56 years. This is a notification that I, along with a number of individuals, community leaders, community organizations, and environmental groups are opposed to the Planning Commission's approval of the TOP2050 General Plan.

On behalf of the residents and citizens of the Ontario community, I request that you take action to deny the approval of the TOP2050 General Plan. The General Plan proposes the rezoning of Agricultural land to Housing, Industrial and Commercial projects based on the findings of the Draft Supplemental Environmental Impact Report (SEIR). R43-1

These findings from the Draft SEIR examine the potential environmental impacts as a result of this project which include the following: Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities.

This project will have significant and unavoidable impacts pertaining to violations of laws on the Federal, State and local levels which includes: R43-2

5.1-1 TOP2050 would not alter scenic vistas - which is not true as many of our homes are now being blocked by large warehouse walls we can not see over or past any distance.

5.2-2 Would not conflict with existing zoning for agricultural use, or Williamson Act contract. This is also not true as All agricultural land was rezoned to industrial or housing in Violation of the Williamson Act, Prop 70 and in violation of 65302(h)(1)(A) Access to food; creating a food desert. R43-3

5.3-1 additional population growth & associated emissions would exceed South Coast AQMD's AQMP. R43-4

5.3-3 would generate long-term emissions in excess of South Coast AQMD's threshold criteria & cumulatively contribute to the South Coast Air Basin's nonattainment designations. R43-5

5.3-4 Operation of industrial and warehousing land uses would expose sensitive receptors to substantial toxic air contaminant concentrations. R43-6

5. In order to be able to make sensible decisions that include the participation of Ontario communi organizations, residents, and citizens, we call on you to support the delay of the approval of the TOP2050 General Plan. This is the most critical issue that the voters will address in the Ontario City Council Elections in November 2022. Your decision to delay the approval of the TOP2050 General Plan will be the first step towards protecting Ontario Air quality, biological resources, agricultural resources, cultural resources, Greenhouse gas emissions, noise, transportation, Tribal Cultural Resources and utilities, as well as, the health and quality of life of Ontario residents. R43-7

2. Response to Comments

Please take these considerations seriously.

Sincerely,

Rosemary Ramos
[REDACTED]

R43-7
cont.

2. Response to Comments

R43. Response to Comments from Rosemary Ramos, dated June 23, 2022.

- R43-1 Responses to Rosemary Ramos' comments are addressed in response to Comments R42-3 through R43-6 below.
- R43-2 See Section 2.2, *Summary of General Responses*, and Section 2.2.1, *Scenic Vistas*. Development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level. Additionally, future development under the Proposed Project would still be required to adhere to the City's Development Code. Finally, the Commenter's concerns regarding existing conditions do not constitute substantial evidence of a potential environmental impact of TOP 2050.
- R43-3 See Section 2.2, *Summary of General Responses*, Section 2.2.2, *Nonrenewal of Williamson Act Contracts*, and Section 2.2.3, *Environmental Justice Policies*. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of current Williamson Act contracts. However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. Further, the comment does not concern TOP 2050, but rather TOP 2010, which is not under consideration here. TOP 2010 was reviewed in the prior EIR and is properly treated as part of the environmental baseline in the SEIR (*Sierra Club v. City of Orange* (2008) 163 Cal.App.4th 523, 542). It should also be noted that environmental justice is not a CEQA topic, as social/economic issues are not environmental impacts under CEQA.
- R43-4 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R43-5 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Air quality impacts associated with the buildout of TOP 2050 were identified as a significant unavoidable impact of the Proposed Project.
- R43-6 See Section 2.2, *Summary of General Responses*, and Section 2.2.4, *Air Quality Impacts*. Cumulative health risk associated with buildout of TOP 2050 was identified as a significant unavoidable impact of the Proposed Project.
- R43-7 Responses to Rosemary Ramos' comments are addressed in response to Comments R43-2 through R43-6 above. The Commenter's opposition to TOP 2050 has been forwarded to the decision-makers for their review and consideration

2. Response to Comments

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3. Revisions to the Draft EIR

3.1 INTRODUCTION

This section contains revisions to the Draft SEIR based upon (1) additional or revised information required to prepare a response to a specific comment; (2) applicable updated information that was not available at the time of Draft SEIR publication; and/or (3) typographical errors. This section also includes additional mitigation measures to fully respond to commenter concerns as well as provide additional clarification to mitigation requirements included in the Draft SEIR.

None of the revisions to the Draft SEIR require recirculation of the document. Recirculation is only required when significant new information is added. Information is not significant unless the EIR is changed in a way that deprives the public of a meaningful opportunity to comment upon a substantial adverse environmental effect or a feasible way to mitigate or avoid such an effect. Recirculation is not required where the new information merely clarifies, amplifies, or makes insignificant modifications. (CEQA Guidelines Section 15088.5.) As explained below, none of the changes adds any new significant information and recirculation is not required.

Changes made to the Draft SEIR are identified here in ~~strikeout text~~ to indicate deletions and in double underlined text to signify additions.

3.2 DRAFT SEIR REVISIONS IN RESPONSE TO WRITTEN COMMENTS

The following text has been revised in response to comments received on the Draft SEIR.

Page 1-7, Chapter 1, *Executive Summary*. The following has been revised to reflect the correct acreage of the City.

The City comprises approximately 50 square miles (~~31,958~~32,022 acres), including the 8,200-acre Ontario Ranch in the southern part of the City—formerly known as the New Model Colony (NMC) and formerly the City's sphere of influence (SOI).

3. Revisions to the Draft EIR

Page 1-25, Chapter 1, *Executive Summary*, Section 1.7, *Areas of Controversy*. The following has been added to include discussion about loss of agricultural farmlands in response to Comments from Susan A. Philips, Professor of Environmental Analysis, Robert Redford Conservancy at Pitzer College; Randy Bekendam, Executive Director, Southern California Agricultural Land Foundation; Ana Gonzales, Director, Center for Community Action and Environmental Justice; Anthony Noriega, Director, District 5, LULAC de Inland Empire; and Kim Floyd, Conservation Chair, Sierra Club San Geronimo Chapter.

In accordance with Section 15123(b)(2) of the CEQA Guidelines, the EIR summary must identify areas of controversy known to the lead agency, including issues raised by agencies and the public. The City of Ontario has no knowledge of expressed opposition to the Proposed Project. Prior to preparation of the Supplemental DEIR, a public scoping meeting was held on August 5, 2021, to determine the concerns of responsible and trustee agencies and the community regarding the Proposed Project. The scoping meeting was held virtually and identified concerns to traffic, recreational access, and aircraft noise. In addition, NOP comment letters received during the review period are summarized in Chapter 2, *Introduction* (see Table 2-1, *NOP Comment Summary*). There has been growing opposition to the loss of agricultural land to industrial development within the City of Ontario. The City received 43 Comment Letters on the Draft SEIR expressing growing opposition to the loss of agriculture uses.

3. Revisions to the Draft EIR

Pages 1-28 and 1-29, Chapter 1, *Executive Summary*. Mitigation Measure 3-1 has been revised to correct a typographical error.

<p>Impact 5.3-2. Construction activities associated with future development that would be accommodated under TOP 2050 could generate short-term emissions in exceedance of the South Coast AQMD's threshold criteria.</p>	<p>Potentially significant</p>	<p>2010 Certified EIR</p> <p>3-1 <u>Prior to discretionary approval by the City of Ontario for development projects subject to CEQA (California Environmental Quality Act) review (i.e., nonexempt projects), project applicants shall prepare and submit a technical assessment evaluating potential project construction-related air quality impacts to the City of Ontario Planning Department for review and approval. The evaluation shall be prepared in conformance with South Coast Air Quality Management District (South Coast AQMD) methodology for assessing air quality impacts. If construction-related criteria air pollutants are determined to have the potential to exceed the South Coast AQMD-adopted thresholds of significance, the City of Ontario Building Department shall require that all new construction projects incorporate feasible mitigation measures to reduce air quality emissions. Potential measures shall be incorporated as conditions of approval for a project and may include:</u></p> <ul style="list-style-type: none"> • Require fugitive dust control measures that exceed South Coast Air Quality Management District's Rule 403, such as: <ul style="list-style-type: none"> ○ Requiring use of nontoxic soil stabilizers to reduce wind erosion. ○ Applying water every four hours to active soil disturbing activities. ○ Tarping and/or maintaining a minimum of 24 inches of freeboard on trucks hauling dirt, sand, soil, or other loose materials. • Using construction equipment rated by the United States Environmental Protection Agency as having Tier 3, Tier 4 interim or higher exhaust emission limits. • Ensuring construction equipment is properly serviced and maintained to the manufacturer's standards. • Limiting nonessential idling of construction equipment to no more than five consecutive minutes. • Using Super-Compliant VOC paints for coating of architectural surfaces whenever possible. A list of Super-Compliant architectural coating manufactures can be found on the South Coast Air Quality Management District's website at: http://www.aqmd.gov/prdas/brochures/Super-Compliant_AIM.pdf. <p><u>These identified measures shall be incorporated into all appropriate construction documents (e.g., construction management plans) submitted to the City and shall be verified by the City's Planning Department.</u></p>	<p>Significant and unavoidable</p>
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3. Revisions to the Draft EIR

Pages 1-32 and 1-33, Chapter 1, Executive Summary. Mitigation Measure 5-1 has been revised to provide additional detail on the City of Ontario procedures for evaluating historic resources.

<p>Impact 5.5-1. Implementation of TOP 2050 could impact a historic resource.</p>	<p>Potentially significant.</p>	<p>2010 Certified EIR</p> <p>5-1 Historic or potentially historic resources in the City shall be evaluated for historic significance through the City's tier system prior to the issuance of plan or development approvals in the Focus Areas. Pursuant to City's <u>Development Code (Chapter 7, Historic Preservation), mitigation measures for all Tier III Historic Resources shall include the following:</u></p> <ul style="list-style-type: none"> a) <u>Each historic resource shall be fully documented and cataloged pursuant to Historic American Building Survey/Historic American Engineering Record (HABS/HAER) standards, to provide a record of the resource, including, but not limited to: [i] the preparation of site plans, floor plans, exterior and interior elevations, and detail drawings of character defining features (such as moldings, stairs, etc.); and [ii] photographs of the resource, including the exterior, interior, and interior and exterior character defining features (such as moldings, light fixtures, trim patterns, etc.).</u> b) <u>A mitigation fee established pursuant to Section 7.01.025 (Historic Preservation Mitigation Fee) shall be paid to the City prior to the issuance of a demolition permit for Tier III historic resources. Fees for Tier I and II historic resources shall be determined during the Environmental Impact Report process. The fees established for Tier III will be used as a reference point for establishing fees for Tier I and II historic resources.</u> c) <u>A Certificate of Appropriateness shall not be issued for the demolition of an historic resource, either in whole or in part, until such time that a demolition permit application and a replacement structure has been approved by the City, and appropriate permits have been issued for its construction, unless: [i] a waiver is granted pursuant to Subsection H (Replacement Structure Waiver for Historic Resources Located within Industrial Zoning Districts) of this Section; [ii] a deferral of the replacement structure requirement is granted pursuant to Subsection G (Replacement Structure Deferral) of this Section; or [iii] demolition is required pursuant to Section 7.01.050 (Unsafe or Dangerous Conditions) of this Development Code.</u> d) <u>In an effort to preserve features and artifacts from historic resources, a determination whether items within or on the resource should be</u> 	<p>Significant and unavoidable</p>
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3. Revisions to the Draft EIR

		<p><u>salvaged must be made by the Planning Department and may include the local historical society prior to the issuance of the demolition permit. The applicant shall be responsible for the removal, relocation, storage, and donation of such items selected for salvaging. The applicant shall provide an inventory of salvaged items to the Planning Department, and shall include a list of each item name, description and dimension (as necessary), and the location of each item on a floor plan.</u></p>	
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Pages 1-55, 1-56, and 1-57, Chapter 1, *Executive Summary*. Mitigation Measure 12-4 has been revised to reflect the appropriate department.

<p>Impact 5.13-1. Construction activities associated with buildout of TOP 2050 would result in temporary noise increases at sensitive receptors during construction activities.</p>	<p>Potentially significant</p>	<p>2010 Certified EIR 12-4 Construction activities associated with new development that occurs near sensitive receptors shall be evaluated for potential noise impacts. Mitigation measures, such as installation of temporary sound barriers for adjacent construction activities that occur adjacent to occupied noise sensitive structures, equipping construction equipment with mufflers, and reducing non-essential idling of construction equipment to no more than five minutes, shall be incorporated into the construction operations to reduce construction-related noise to the extent feasible. <u>Construction contractors shall implement the following measures for construction activities conducted in the City of Ontario. Construction plans submitted to the City shall identify these measures on demolition, grading, and construction plans submitted to the City. The City of Ontario Planning and Building Departments shall verify that grading, demolition, and/or construction plans submitted include these notations prior to issuance of demolition, grading, and/or building permits.</u></p> <ul style="list-style-type: none"> • <u>Construction activity is limited to the hours: Between 7:00 AM and 6:00 PM Monday through Friday and 9:00 AM to 6:00 PM Saturdays and Sundays, as prescribed in Municipal Code Section 5-29.09.</u> • <u>During the entire active construction period, equipment and trucks used for project construction shall use the best-available noise control techniques (e.g., improved mufflers, equipment re-design, use of intake silencers, ducts, engine enclosures, and acoustically attenuating shields or shrouds), wherever feasible.</u> • <u>Impact tools (e.g., jack hammers and hoe rams) shall be hydraulically or electrically powered wherever possible. Where the use of pneumatic</u> 	<p>Significant and unavoidable</p>
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3. Revisions to the Draft EIR

		<p><u>tools is unavoidable, an exhaust muffler on the compressed air exhaust shall be used along with external noise jackets on the tools.</u></p> <ul style="list-style-type: none"> • <u>Stationary equipment, such as generators and air compressors shall be located as far as feasible from nearby noise-sensitive uses.</u> • <u>Stockpiling shall be located as far as feasible from nearby noise-sensitive receptors.</u> • <u>Construction traffic shall be limited, to the extent feasible, to approved haul routes established by the City's Engineering Department Planning and Building Departments.</u> • <u>At least 10 days prior to the start of construction activities, a sign shall be posted at the entrance(s) to the job site, clearly visible to the public, that includes permitted construction days and hours, as well as the telephone numbers of the City's and contractor's authorized representatives that are assigned to respond in the event of a noise or vibration complaint. If the authorized contractor's representative receives a complaint, he/she shall investigate, take appropriate corrective action, and report the action to the City.</u> • <u>Signs shall be posted at the job site entrance(s), within the on-site construction zones, and along queueing lanes (if any) to reinforce the prohibition of unnecessary engine idling. All other equipment shall be turned off if not in use for more than 5 minutes.</u> • <u>During the entire active construction period and to the extent feasible, the use of noise-producing signals, including horns, whistles, alarms, and bells, shall be for safety warning purposes only. The construction manager shall use smart back-up alarms, which automatically adjust the alarm level based on the background noise level or switch off back-up alarms and replace with human spotters in compliance with all safety requirements and laws.</u> • <u>Erect temporary noise barriers (at least as high as the exhaust of equipment and breaking line-of-sight between noise sources and sensitive receptors), as necessary and feasible, to maintain construction noise levels at or below the performance standard of 80 dBA L_{eq}. Barriers shall be constructed with a solid material that has a density of at least 1.5 pounds per square foot with no gaps from the ground to the top of the barrier and may be lined on the construction side with an acoustical blanket, curtain, or equivalent absorptive material.</u> 	
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3. Revisions to the Draft EIR

Page 3-1, Chapter 3, *Project Description*. The following has been revised to reflect the correct acreage of the City.

The City comprises approximately 50 square miles (~~31,958~~ 32,022 acres), including the 8,200-acre Ontario Ranch in the southern part of the City—formerly known as the New Model Colony (NMC) and formerly the City’s sphere of influence (SOI).

Page 3-18, Chapter 3, *Project Description*. Table 3-2, Land Use Designations in the City of Ontario, has been updated to include a statement specifying that Mixed Use Neighborhoods Activity Hub projects located outside of Ontario Ranch will be subject to Planned Unit Development (PUD) at the City’s discretion.

<p>Mixed Use Neighborhood Activity Hubs⁴ (MU-NH) 6th and Mountain 4th and Mountain Francis and Euclid Walnut and Euclid Riverside and Euclid</p>	<p>20.0 to 75.0 dwelling units per acre 1.0 FAR for retail and office Subject to Specific Plan³ <u>Projects located outside of Ontario Ranch will be subject to PUD at City's Discretion</u></p>	<p>Envisioned as a low-rise (3 to 5 stories), mixture of retail and residential uses that will create identity and place along the corridor and serve the surrounding residents.</p>
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Page 5.2-4, Section 5.2, *Agriculture and Forestry Resources*. The following minor technical revision was made.

Land uses surrounding the City mostly support ~~residential~~ industrial, commercial and ~~industrial~~ residential uses with minimal agricultural land parcels dispersed intermittently.

Page 5.3-45, Section 5.3, *Air Quality*. The following has been updated to reflect revisions to the TOP 2050 policy.

Implementation of the following TOP 2050 policies would reduce project-level localized impacts from industrial development:

- **ER-4.9: New Localized Air Pollution Sources Near Existing Sensitive Receptors.** We require new developments to conduct a Health Risk Assessment for land uses that generate more than 100 trucks per day or 40 trucks per day by trucks operating transportation refrigeration units (TRU's) within 1,000 feet ~~of homes, childcare facilities, schools, and hospitals~~ from sensitive land uses (California Health and Safety Code Section 42705.5(a)(5)). If the health risk assessment determines the new development poses health hazards that increase the incremental cancer risk above the threshold established by the South Coast Air Quality Management District (AQMD), we will only approve permits upon the condition that adequate mitigation measures are proposed and implemented for potential impacts on the sensitive uses around the site and along the route within Ontario taken by the trucks to and from freeways. We require new developments that must perform a health risk assessment to conduct additional public outreach by sending

3. Revisions to the Draft EIR

notifications in multiple languages to all residents living within 500 feet, and encourage hosting a public meeting.

Page 5.3-48, Section 5.3, *Air Quality*. The following has been corrected to reflect the appropriate TOP 2050 policy (highlighted).

- **ER-3.5: Fuel-Efficient and Alternative Energy Vehicles and Equipment.** We should require purchase and use vehicles and equipment that are fuel efficient and meet or surpass state emissions requirements and/or use renewable sources of energy.

Page 5.3-49, Section 5.3, *Air Quality*. The following has been updated to reflect revisions to the TOP 2050 policy (highlighted).

- **ER-4.9: New Localized Air Pollution Sources Near Existing Sensitive Receptors.** We require new developments to conduct a Health Risk Assessment for land uses that generate more than 100 trucks per day or 40 trucks per day by trucks operating transportation refrigeration units (TRU's) within 1,000 feet of homes, childcare facilities, schools, and hospitals from sensitive land uses (California Health and Safety Code Section 42705.5(a)(5)). If the health risk assessment determines the new development poses health hazards that increase the incremental cancer risk above the threshold established by the South Coast Air Quality Management District (AQMD), we will only approve permits upon the condition that adequate mitigation measures are proposed and implemented for potential impacts on the sensitive uses around the site and along the route within Ontario taken by the trucks to and from freeways. We require new developments that must perform a health risk assessment to conduct additional public outreach by sending notifications in multiple languages to all residents living within 500 feet, and encourage hosting a public meeting.

Page 5.3-52, Section 5.3, *Air Quality*. Mitigation Measure 3-1 has been revised to correct a typographical error.

- 3-1 Prior to discretionary approval by the City of Ontario for development projects subject to CEQA (California Environmental Quality Act) review (i.e., nonexempt projects), project applicants shall prepare and submit a technical assessment evaluating potential project construction-related air quality impacts to the City of Ontario Planning Department for review and approval. The evaluation shall be prepared in conformance with South Coast Air Quality Management District (South Coast AQMD) methodology for assessing air quality impacts. If construction-related criteria air pollutants are determined to have the potential to exceed the South Coast AQMD-adopted thresholds of significance, ~~The the~~ City of Ontario Building Department shall require that all new construction projects incorporate feasible mitigation measures to reduce air quality emissions. Potential measures shall be incorporated as conditions of approval for a project and may include:

3. Revisions to the Draft EIR

- Require fugitive dust control measures that exceed South Coast Air Quality Management District's Rule 403, such as:
 - Requiring use of nontoxic soil stabilizers to reduce wind erosion.
 - Applying water every four hours to active soil disturbing activities.
 - Tarping and/or maintaining a minimum of 24 inches of freeboard on trucks hauling dirt, sand, soil, or other loose materials.
- Using construction equipment rated by the United States Environmental Protection Agency as having ~~Tier 3~~ Tier 4 interim or higher exhaust emission limits.
- Ensuring construction equipment is properly serviced and maintained to the manufacturer's standards.
- Limiting nonessential idling of construction equipment to no more than five consecutive minutes.
- Using Super-Compliant VOC paints for coating of architectural surfaces whenever possible. A list of Super-Compliant architectural coating manufactures can be found on the South Coast Air Quality Management District's website at: http://www.aqmd.gov/prdas/brochures/Super-Compliant_AIM.pdf.

These identified measures shall be incorporated into all appropriate construction documents (e.g., construction management plans) submitted to the City and shall be verified by the City's Planning Department.

Pages 5.4-29 and 5.4-30, Chapter 4, *Biological Resources*. The following has been revised to remove extraneous information.

The settlement agreement for the City of Ontario Sphere of Influence General Plan Amendment governs mitigation for biological resources impacts in Ontario Ranch associated with potential impacts to the burrowing owl, the DSFLF, raptor foraging and wildlife habitat, loss of open space, actual and potential habitat and agricultural land, and sensitive species (listed and nonlisted). The terms of the settlement agreement were discussed in the "Local Regulations" section under "City of Ontario Sphere of Influence General Plan Amendment, Final EIR, and Settlement Agreement." ~~Per the conditions of the settlement agreement, the City will be soliciting RFPs to select a non-profit land trust and/or organization specializing in habitat conservation. The selected non-profit and/or organization will be responsible for the administration of the habitat mitigation fees and creation of a habitat program pursuant to the requirements of the Settlement Agreement between the City of Ontario, the Endangered Habitats League, and the Sierra Club. In partnership with the CDFW, the City~~

3. Revisions to the Draft EIR

~~will work with the selected non-profit and/or organization to maintain an interactive mapping and current inventory of the burrowing owl occurrences and in the selection of adequate lands for passive relocation.~~

Page 5.4-31, Chapter 4, *Biological Resources*. The following has been revised to remove extraneous information.

The settlement agreement for the City of Ontario Sphere of Influence General Plan Amendment governs mitigation for biological resources impacts in Ontario Ranch associated with potential impacts to the burrowing owl, the DSFLF, raptor foraging and wildlife habitat, loss of open space, actual and potential habitat and agricultural land, and sensitive species (listed and nonlisted). The terms of the settlement agreement were discussed in the “Local Regulations” section under “City of Ontario Sphere of Influence General Plan Amendment, Final EIR, and Settlement Agreement.” ~~Per the conditions of the settlement agreement, the City will be soliciting RFPs to select a non-profit land trust and/or organization specializing in habitat conservation. The selected non-profit and/or organization will be responsible for the administration of the habitat mitigation fees and creation of a habitat program pursuant to the requirements of the Settlement Agreement between the City of Ontario, the Endangered Habitats League, and the Sierra Club. In partnership with the CDFW, the City will work with the selected non-profit and/or organization to maintain an interactive mapping and current inventory of the burrowing owl occurrences and in the selection of adequate lands for passive relocation.~~

Page 5.4-32, Chapter 4, *Biological Resources*. The following has been revised to correct a typographical error.

The Cucamonga Creek and Deer Creek channels and portions of the Lower Deer Creek, Day Creek, Etiwanda Creek, and West Cucamonga Creek channels, are owned and maintained by San Bernardino County (Ontario 2010); ~~They~~ they are not subject to land use controls by the City of Ontario and would not be affected by TOP 2050.

Page 5.5-20, Chapter 5, *Cultural Resources*. The following has been revised to correct a typographical error.

However, long-term implementation of TOP 2050 would allow development and redevelopment, including grading, of sensitive areas, possible disturbing human remains, including those outside of formal cemeteries.

Page 5.5-22, Section 5.5, *Cultural Resources*. Mitigation Measure 5-1 has been revised to provide additional detail on the City of Ontario procedures for evaluating historic resources.

5-1 Historic or potentially historic resources in the City shall be evaluated for historic significance through the City’s tier system prior to the issuance of plan or development approvals ~~in the Focus Areas~~. Pursuant to City’s Development Code (Chapter 4, Permits, Actions, and Decisions, and Chapter 7, Historic Preservation), mitigation measures for all Tier III Historic Resources shall include the following:

3. Revisions to the Draft EIR

- a) Each historic resource shall be fully documented and cataloged pursuant to Historic American Building Survey/Historic American Engineering Record (HABS/HAER) standards, to provide a record of the resource, including, but not limited to: [i] the preparation of site plans, floor plans, exterior and interior elevations, and detail drawings of character defining features (such as moldings, stairs, etc.); and [ii] photographs of the resource, including the exterior, interior, and interior and exterior character defining features (such as moldings, light fixtures, trim patterns, etc.).
- b) A mitigation fee established pursuant to Section 7.01.030 (Historic Preservation Mitigation Fee) shall be paid to the City prior to the issuance of a demolition permit for Tier III historic resources. Fees for Tier I and II historic resources shall be determined during the Environmental Impact Report process. The fees established for Tier III will be used as a reference point for establishing fees for Tier I and II historic resources.
- c) A Certificate of Appropriateness shall not be issued for the demolition of an historic resource, either in whole or in part, until such time that a demolition permit application and a replacement structure has been approved by the City, and appropriate permits have been issued for its construction, unless: [i] a waiver is granted pursuant to Subsection H (Replacement Structure Waiver for Historic Resources Located within Industrial Zoning Districts) of Section 4.02.050; [ii] a deferral of the replacement structure requirement is granted pursuant to Subsection G (Replacement Structure Deferral) of Section 4.02.050; or [iii] demolition is required pursuant to Section 7.01.050 (Unsafe or Dangerous Conditions) of this Development Code.
- d) In an effort to preserve features and artifacts from historic resources, a determination whether items within or on the resource should be salvaged must be made by the Planning Department and may include the local historical society prior to the issuance of the demolition permit. The applicant shall be responsible for the removal, relocation, storage, and donation of such items selected for salvaging. The applicant shall provide an inventory of salvaged items to the Planning Department, and shall include a list of each item name, description, and dimension (as necessary), and the location of each item on a floor plan.

Page 5.6-11, Chapter 6, *Energy*. The following has been revised to reflect the appropriate TOP 2050 policy.

Policies ER-3.2 and ER-3.64 would require the best practices identified in green community rating systems to guide development in new communities and promote renewable energy sources for public- and private-sector development.

3. Revisions to the Draft EIR

Page 5.8-25, Chapter 8, *Greenhouse Gas Emissions*. The following has been revised to correct a typographical error.

- Community Design Element policies CD-2.7 would ensure that sustainability is considered in the design of new projects.
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Page 5.9-37, Chapter 9, *Hazards and Hazardous Materials*. The following has been revised to remove outdated information.

~~Pursuant to the current Development Code, Hazardous Waste Overlay Districts do not allow the placement of any facility that handles, manufactures, or transports hazardous waste, including household hazardous waste collection facilities, within 2,000 feet of a residential or institutional lot line (Ontario Development Code Sections 9-1.2815, 9-1.2830). As a result, TOP 2050 would not result in the placement of hazardous waste-generating facilities within a quarter mile of a school. Development associated with the proposed land use plan would follow the regulations set by the current Development Code.~~

Page 5.11-5, Chapter 11, *Land Use and Planning*. The following has been revised to reflect the appropriate term.

The mixed-use ~~overlays~~ designations would also bring entertainment, activity, and diversity to housing, retail, and workplace land uses in the City, which would help create attractive communities for local citizens and visitors.

Page 5.11-5, Chapter 11, *Land Use and Planning*. The following has been revised to include the appropriate Elements.

Additionally, the TOP 2050 Land Use and Housing Elements ~~has~~ specific policies that encourage neighborhood identity and preservation.

Page 5.11-10, Chapter 11, *Land Use and Planning*. The following has been revised to include the appropriate Elements.

The Airport Planning section of the TOP 2050 Land Use and Mobility Elements includes policies that would ensure airport planning compatibility and consistency.

Page 5.12-6, Chapter 12, *Mineral Resources*. The following has been revised to remove extraneous information.

Additionally, TOP 2050 includes a goal to protect ~~high-value habitat and farming and~~ mineral-resource-extraction activities that are compatible with adjacent development (see also Policy ER-5.5).

3. Revisions to the Draft EIR

Page 5.13-40, Chapter 13, *Noise*. Mitigation Measure 12-4 has been revised to reflect the appropriate department.

- 12-4 Construction activities associated with new development that occurs near sensitive receptors shall be evaluated for potential noise impacts. ~~Mitigation measures, such as installation of temporary sound barriers for adjacent construction activities that occur adjacent to occupied noise-sensitive structures, equipping construction equipment with mufflers, and reducing non-essential idling of construction equipment to no more than five minutes, shall be incorporated into the construction operations to reduce construction-related noise to the extent feasible. Construction contractors shall implement the following measures for construction activities in the City of Ontario. Construction plans submitted to the City shall identify these measures on demolition, grading, and construction plans. The City of Ontario Planning and Building Departments shall verify that grading, demolition, and/or construction plans submitted include these notations prior to issuance of demolition, grading, and/or building permits.~~
- Construction activity is limited to the hours between 7:00 am and 6:00 pm Monday through Friday and 9:00 am to 6:00 pm Saturdays and Sundays, as prescribed in Municipal Code Section 5-29.09.
 - During the entire active construction period, equipment and trucks used for project construction shall use the best-available noise control techniques wherever feasible (e.g., improved mufflers, equipment re-design, use of intake silencers, ducts, engine enclosures, and acoustically attenuating shields or shrouds).
 - Impact tools (e.g., jack hammers and hoe rams) shall be hydraulically or electrically powered wherever possible. Where the use of pneumatic tools is unavoidable, an exhaust muffler on the compressed air exhaust shall be used along with external noise jackets on the tools.
 - Stationary equipment such as generators and air compressors shall be located as far as feasible from nearby noise-sensitive uses.
 - Stockpiling shall be located as far as feasible from nearby noise-sensitive receptors.
 - Construction traffic shall be limited, to the extent feasible, to approved haul routes established by the City's Engineering Department ~~Planning and Building Departments~~.
 - At least 10 days prior to the start of construction activities, a sign shall be posted at the entrance(s) to the job site, clearly visible to the public, that includes permitted construction days and hours as well as the telephone numbers of the City's and contractor's authorized representatives that are assigned to respond in the event of a noise or vibration complaint. If the authorized contractor's representative receives a complaint, he/she shall investigate, take appropriate corrective action, and report the action to the City.

3. Revisions to the Draft EIR

- Signs shall be posted at the job site entrance(s), within the on-site construction zones, and along queueing lanes (if any) to reinforce the prohibition of unnecessary engine idling. All other equipment shall be turned off if not in use for more than 5 minutes.
- During the entire active construction period and to the extent feasible, the use of noise-producing signals, including horns, whistles, alarms, and bells, shall be for safety warning purposes only. The construction manager shall use smart back-up alarms, which automatically adjust the alarm level based on the background noise level or switch off back-up alarms and replace with human spotters in compliance with all safety requirements and laws.
- Erect temporary noise barriers (at least as high as the exhaust of equipment and breaking line-of-sight between noise sources and sensitive receptors), as necessary and feasible, to maintain construction noise levels at or below the performance standard of 80 dBA L_{eq} . Barriers shall be constructed with a solid material that has a density of at least 1.5 pounds per square foot with no gaps from the ground to the top of the barrier and may be lined on the construction side with an acoustical blanket, curtain, or equivalent absorptive material.

Page 5.15-3, Chapter 15, *Public Services*. The following has been added to include a clarifying statement regarding analysis of potential impacts of the development of future facilities.

While the construction of future facilities could result in potential environmental impacts, future environmental review would occur once specific locations have been determined. Without a definitive location for the development of future facilities, the analysis of potential impacts is too speculative to conduct. Future projects would be reviewed by the City and the OFD on an individual basis and would be required to comply with requirements in effect at the time building permits are issued, including the payment of development impact fees that contribute to funding for additional staffing, facilities, and equipment.

Page 5.15-3, Chapter 15, *Public Services*. The following has been added to include a clarifying statement regarding analysis of potential impacts of the development of future facilities.

It is possible that buildout of the Proposed Project would require additional facilities to support the OPD, the construction of which could result in potential environmental impacts. Such facilities would have to complete applicable environmental review under CEQA at that time, and locations and sizes of potential future facilities, if needed, is not known at this time. Without a definitive location for the development of future facilities, the analysis of potential impacts is too speculative to conduct. Future projects would also be reviewed by the City of Ontario on an individual basis and required to comply with regulations in effect at the time building permits are issued.

3. Revisions to the Draft EIR

Page 5.20-15, Section 5.20, *Wildfire*. The following has been revised to include the entirety of the TOP 2050 policy.

- **M-1.1: Roadway Design and Maintenance.** We require our roadways to: 1) Comply with federal, state, and local design and safety standards; 2) Meet the needs of multiple transportation modes and users; 3) Handle the capacity envisioned in the City of Ontario Master Plan of Streets and Highways; 4) Be maintained in accordance with best practices; 5) Be compatible with the streetscape and surrounding land uses; and 6) Promote the efficient flow of all modes of traffic through the implementation of intelligent transportation systems and travel demand management strategies.
-

Page 7-6, Chapter 7, *Alternatives to the Proposed Project*. The following has been revised to correct a typographical error.

As identified in Section 5.8, with implementation of the 2022 CCAP update, TOP 2050 would result in emissions below that of the current TOP.

3. Revisions to the Draft EIR

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Appendix A. Revised Draft SEIR Figures

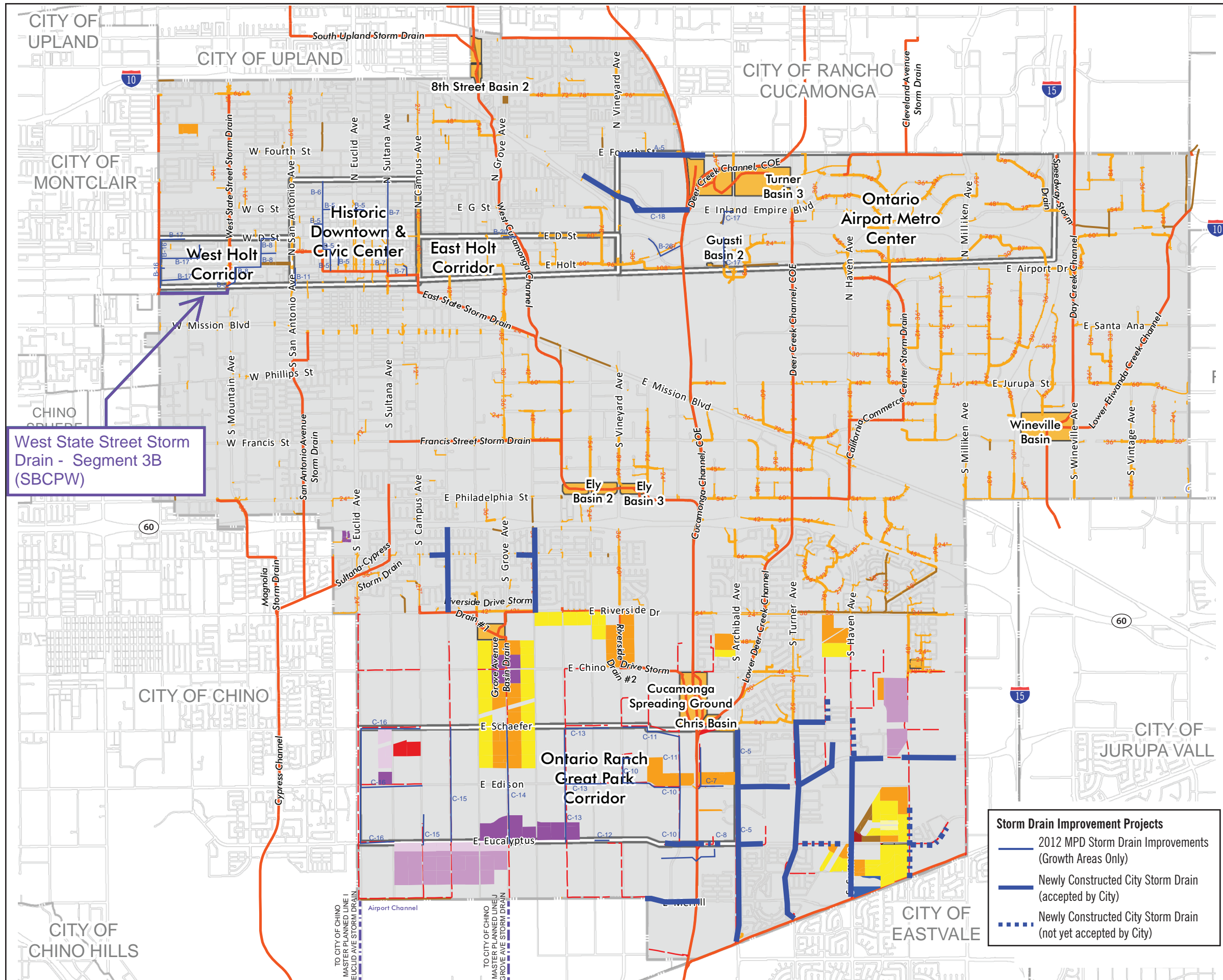
Appendix

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Figure 5.19-4


Proposed Storm Drain System

- Land Use Parcels**
Imperviousness Condition Change
- Residential**
- LDR Low Density Residential
 - LMDR Low Medium Density Residential
 - MDR Medium Density Residential
 - HDR High Density Residential
- Mixed-Use**
- MU Mixed Use
- Commercial**
- NC Neighborhood Commercial
 - GC General Commercial
- Employment**
- BP Business Park
 - IND Industrial
- Other**
- PS Public School
 - ROW Right of Way
- City Drainage Facilities**
- 12" - 42" Storm Drain Lines
 - 42" - 120" Storm Drain Lines
 - Detention Basins
 - Open Channel
 - Proposed Storm Channel
- Other Jurisdictional Drainage Facilities**
- Caltrans Storm Drain Lines
 - Private Storm Drain Lines
 - SB County Storm Drain Lines
 - SB County Flood Control Channel
 - Ontario City Boundary
 - Proposed Growth Areas



- Storm Drain Improvement Projects**
- 2012 MPD Storm Drain Improvements (Growth Areas Only)
 - Newly Constructed City Storm Drain (accepted by City)
 - Newly Constructed City Storm Drain (not yet accepted by City)


2 · 0 · 5 · 0
THE ONTARIO PLAN
 SUPPLEMENTAL EIR


 Source: Fuscoe 2022 Date: 6/28/2022

7. ALTERNATIVES

Figure 7-1
 Reduced Industrial
 Alternative Parcels



- Industrial Parcels (338 ac)
- Proposed TOP Areas of Change
- Ontario City Boundary
- County Boundary
- Rail Network
- Proposed Land Use**
- IND, Industrial

2 · 0 · 5 · 0

THE ONTARIO PLAN
 SUPPLEMENTAL EIR

0 2,500 5,000 10,000 FT

Source: The City of Ontario 2021 Date: 6/29/2022

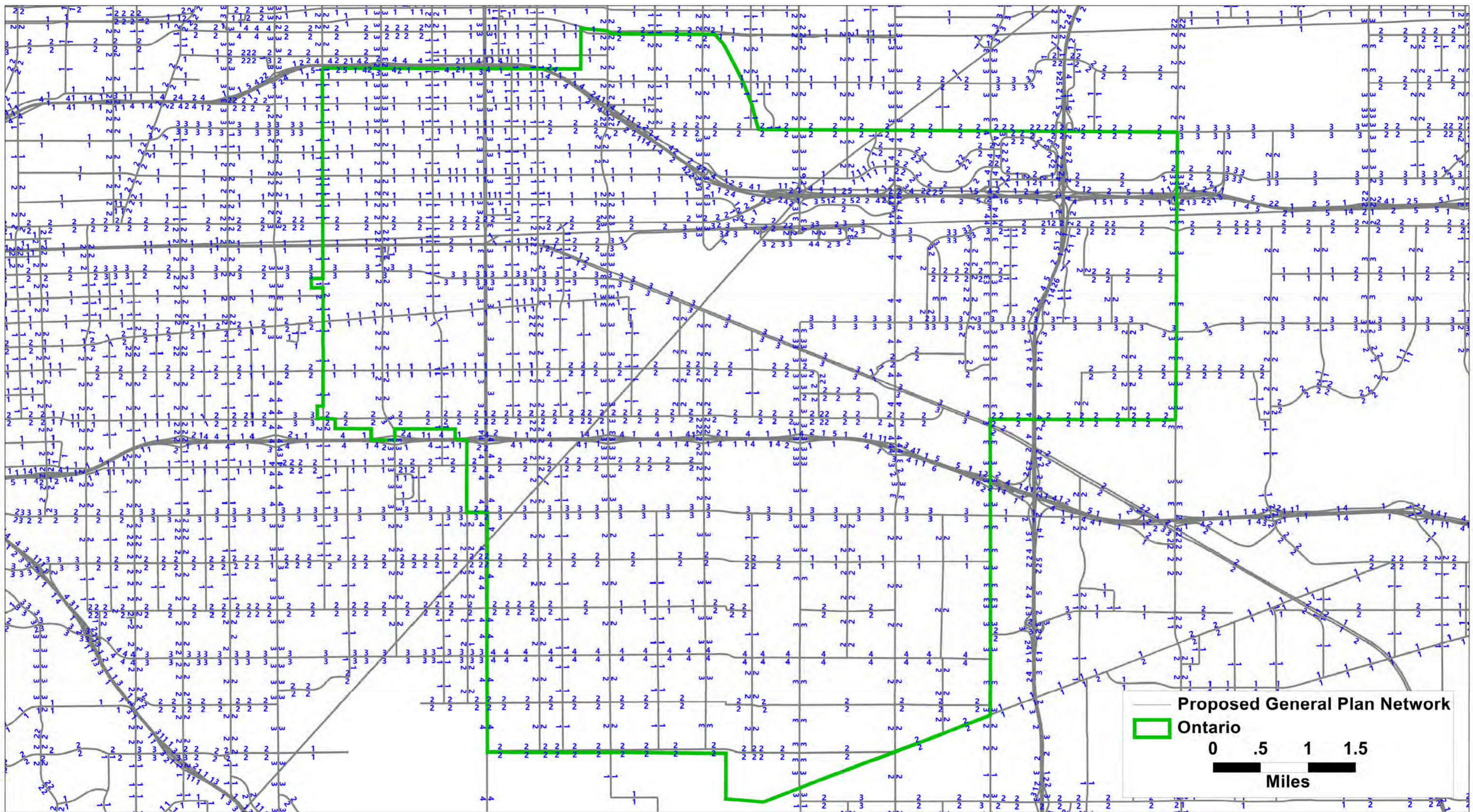


Figure 3
Proposed TOP 2050 Network

Appendix B. Environmental Justice Goals, Policies, and Actions

Appendix

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ATTACHMENT B:
CEQA FINDINGS OF FACTS
AND
STATEMENT OF OVERRIDING CONSIDERATIONS

A Component of The Ontario Plan 2050



Goals, Policies, and Actions on Environmental Justice and Climate Adaptation and Resiliency

Excerpts from The Ontario Plan 2050

Public Review Draft | July 2022

Please note that this is a collection of content pulled from The Ontario Plan 2050 intended to help the public and decision makers better understand the majority of technical updates proposed based on new state laws regarding environmental justice (SB 1000) and climate adaptation and resiliency (SB 379).



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CONTENTS

OVERVIEW	1
Introduction	1
Purpose.....	1
Notes/Caveats.....	2
Housing Element	2
Implementation Actions.....	2
Internal Consistency	2
ENVIRONMENTAL JUSTICE GOALS, POLICIES, AND ACTIONS	3
Enacting Legislation	3
Indicators	3
A Note on Content Not Shown	4
Additional Information.....	4
Land Use Element.....	4
Housing Element.....	6
Parks and Recreation Element	8
Environmental Resources Element.....	9
Community Economics Element	11
Safety Element.....	11
Mobility Element.....	13
Community Design Element.....	16
Social Resources Element.....	17
CLIMATE ADAPTATION RESILIENCY GOALS, POLICIES, AND ACTIONS.....	24
Enacting Legislation	24
Structure and Indicators.....	24
Safety Element	25
Flooding	25
Fire and Rescue and Related Services	27
Emergency Management	27
Energy Resiliency.....	29
Documents Incorporated by Reference.....	30
FIGURES	
ER-01 Environmental Justice Areas.....	20
M-02 Multipurpose Trails & Bikeways.....	21
M-03 Public Transit	22
M-04 Truck Routes	23
S-03 Flood Hazard Zones.....	31
S-04 Dam Inundation Zones.....	32
S-05 Fire Hazard Severity Zones.....	33
S-08 Critical Facilities	34
S-09 Evacuation Constrained Areas	35
S-10 Evacuation Routes.....	36

OVERVIEW

Introduction

The Ontario Plan (TOP) is the City's policy and implementation framework that guides the long-term growth and improvement of the Ontario community through six interrelated components of city governance: Vision, Governance Manual, Policy Plan, City Council Priorities, Implementation Plan, and Tracking and Feedback.

The Policy Plan serves as the City's General Plan (required by state law) that states long-term principles, goals, and policies for achieving Ontario's Vision through nine topics referred to as elements. The majority of new policy content being recommended through TOP 2050 relates to the topics of environmental justice and climate adaptation and resiliency. These two topics were introduced into state law through Senate Bills 1000 (environmental justice) and 379 (climate adaptation and resiliency), with the legislation requiring the City to address the topics either upon its next revision of two or more elements (trigger for environmental justice) or upon the next update of its safety element (trigger for climate adaptation and resiliency).

Furthermore, Senate Bill 1035 built off Senate Bill 379, requiring the safety element be updated every eight years upon the next housing element update. Accordingly, knowing that the City would be required to update its Housing Element in 2021, which would require the City to update its Safety Element, which in turn would require City to address the topics of environmental justice and climate adaptation and resiliency, the City embarked on a technical update of The Ontario Plan in its entirety.

This technical update is referred to as TOP 2050 and is intended to ensure the City complies with new state laws, reflects the latest in mobility investments and planning, brings long-term growth and fiscal projections into alignment with current economic conditions, and advanced the Tracking and Feedback system and Implementation Plan.

State laws allows the City to address any topic in an individual element or throughout multiple elements. The City has opted to address the topics of environmental justice and climate adaptation and resiliency throughout multiple elements of the Policy Plan to ensure the topics are presented alongside the multitude of issues and topics that affect our residents and resources. While this approach can be beneficial as it ensures the topics are not isolated and are understood within the most relevant context, those who want to quickly gather all of the content relevant to these topics must read the entirety of the Policy Plan.

Purpose

This document aggregates all of the content relevant to the topics of environmental justice and climate adaptation and resiliency from the Draft Policy Plan and Draft Implementation Plan. While some content from the currently adopted Policy Plan already address aspects of these new topics, the City augmented its Policy Plan and Implementation Plan to more fully address the relevant statutory requirements and to reflect public input obtained during outreach activities.

The City released a draft of the Policy Plan in May 2022 for public review and, following public input on the Draft Policy Plan and Supplemental Environmental Impact Report (SEIR), released a draft of The Implementation Plan in July 2022.

After adoption, the City intends to update its web-based format of The Ontario Plan to include a function that enables users to filter and search for content based on topics like environmental justice or climate adaptation and resiliency. Until that time, this document serves as a manual compendium of the relevant content.

Notes/Caveats

Housing Element

The City updated its Housing Element in March 2022 separately from the balance of TOP 2050 in order to comply with state housing law. Some adjustments are still being made to ensure the City receives certification from the California Department of Housing and Community Development, but such changes are minor and do not affect the content shown in this document. The Housing Element's goals, policies, and actions (written as programs) that remain largely as previously adopted in the 2013-2021 Housing Element are indicated to be existing, unchanged policies. Those goals, policies, or actions that were changed in a substantive manner or are entirely new as part of the 2022 adopted Housing Element are indicated to be modified or new.

Implementation Actions

The actions listed from the draft Implementation Plan reflect new activities that the City should conduct in order to implement the new aspects of the draft Policy Plan. The draft Implementation Plan does not include actions that the City already undertakes based on regulations, programs, or procedures that are already in place. Some implementation actions relate to more than one policy and/or goal. Additionally, not all goals or policies require new implementation actions. Some new goals or policies can be implemented by existing regulations, programs, or procedures, or are already address through other City planning documents. For example, the City adopted a Recreation and Parks Master Plan in 2021, which includes the following recommendations:

- Develop a resource allocation and cost recovery policy to ensure equitable use of City resources for recreation.
- Explore ways to include facilities within existing City owned properties, specifically facilities that may help increase Level of Service and mitigate park needs in park poor areas.
- Look at including pocket parks and mini parks specifically in park poor areas.

Internal Consistency

Should any inconsistencies or errors be present in this document compared to the draft Policy Plan and Implementation Plan documents, the latter documents should be considered the authoritative points of reference.

ENVIRONMENTAL JUSTICE PRINCIPLES, GOALS, POLICIES, ACTIONS, AND FIGURES

Enacting Legislation

Senate Bill 1000, the Planning for Healthy Communities Act, introduced legislation that was signed into law in 2016 and mandates that Ontario address the topic of environmental justice in its Policy Plan and Implementation Plan upon the creation and/or updating of two or more Policy Plan elements. Specifically, the new environmental justice goals, policies, and actions must do the following:

- Reduce the unique or compounded health risks in disadvantaged communities by reducing pollution exposure and promoting public improvements, public services, community amenities, food access, safe and sanitary homes, and physical activity.
- Promote civil engagement in the public decision-making process.
- Prioritize improvements and programs that address the needs of disadvantaged communities.

The City of Ontario has incorporated environmental justice goals and policies in all elements of its Policy Plan and associated actions in the Implementation Plan.

Indicators

The following content is presented by element, drawing principles, goals, and policies from the draft Policy Plan and actions/objectives from the draft Implementation Plan. All content is associated with the enumerated statutory requirements in parentheses, as well as indications whether the content is currently adopted, proposed for substantive modification, or proposed as entirely new. Appendix B of the Draft SEIR compares the Policy Plan goals and policies as currently adopted and as proposed. The list below provides the abbreviations associated with each set of indicators.

Environmental justice indicators:

- Reduce exposure to pollution (EP)
- Promotion of food access (FA)
- Promotion of physical activity (PA)
- Promotion of public facilities (PF)
- Promotion of safe and sanitary housing (H)
- Promotion of civil engagement (CE)

Status indicators:

- Currently adopted, no substantive changes (exist)
- Currently adopted, proposed for substantive modification (*mod*)
- Proposed as entirely new or proposed for such modifications as to essentially replace what is currently adopted (*new*)

A Note on Content Not Shown

Some policies from the Policy Plan are not included in this document because, while they may address environmental justice topics and help those who are disadvantaged, such policies are broad in nature and are often applied to anywhere and/or everywhere in Ontario. For example: “Policy M-3.5 Light Rail. We support extension of the Metro Rail Gold Line to Ontario, and will work to secure station locations at the proposed multimodal transit center.” This policy can help promote public facilities for those who are disadvantaged by expanding the service availability of light rail, but the policy itself is fairly broad and addresses facilities outside of the City’s designated environmental justice areas.

As another examples: “Policy ER-4.3 Greenhouse Gases (GHG) Emissions Reductions. We reduce GHG emissions in accordance with regional, state, and federal regulations.” This policy can help improve air quality for those living in environmental justice areas, but the implementation of this policy may just as likely improve air quality for anyone living in Ontario. However, this document does include some goals, policies, and actions that may be citywide in their application, provided they are related directly to environmental justice issues. Policy Plan goals may also be included if they help provide more context for the policies and actions.

Additional Information

The City published additional material related to environmental justice in Ontario on its website. This information was also presented (in English and Spanish) at various outreach events in 2021 and 2022.

- Environmental Justice story map ([English](#) | [Español](#))
- Environmental Justice presentation “Identifying the issues”
- Environmental Justice survey results

Land Use Element

Principles

- Land uses and development should be consistent with the Vision while protecting the quality of life in existing neighborhoods (EP, exist).

Goals and Policies

Goal LU-2 Compatibility between a wide range of uses and resultant urban patterns and forms. (EP, *mod*)

LU-2.1 Land Use Decisions. We minimize adverse impacts on adjacent properties when considering land use and zoning requests. (EP, exist)

LU-2.2 Buffers. We require new uses to provide mitigation or buffers between existing uses where potential adverse impacts could occur. Additional mitigation is

required when new uses could negatively impact environmental justice areas. (EP, *mod*)

LU-2.3 Hazardous Uses. We regulate the development of industrial and similar uses that use, store, produce, or transport toxic substances, air emissions, other pollutants, or hazardous materials. (EP, exist)

LU-2.7 Inter-jurisdictional Coordination. We maintain an ongoing liaison with ONT, Caltrans, Public Utilities Commission, the railroads, and other agencies to help minimize impacts and improve the operations and aesthetics of their facilities. (EP, exist)

LU-2.8 Transitional Areas. We require development in transitional areas to protect the quality of life of current residents. (EP, exist)

LU-2.9 Methane Gas Sites. We require sensitive land uses and new uses on former dairy farms or other methane-producing sites be designed to minimize health risks. (EP, exist)

LU-2.10 Sensitive Uses. We monitor and share information with the community about stationary and non-stationary emission sources. We encourage siting and design of facilities to minimize health and safety risks on existing and proposed sensitive uses, especially in environmental justice areas. (EP, *new*)

Goal LU-3 Staff, regulations and processes that support and allow flexible response to conditions and circumstances in order to achieve the Vision (EP, exist)

LU-3.3 Land Use Flexibility. We consider uses not typically permitted within a land use category if doing so improves livability, reduces vehicular trips, creates community gathering places and activity nodes, and helps create identity. (EP, exist)

Goal LU-5 Integrated airport systems and facilities that minimize negative impacts to the community and maximize economic benefits. (EP, exist)

LU-5.3 Airport Impacts. We work with agencies to maximize resources to mitigate the impacts and hazards related to airport operations – their homes. (EP, exist)

Actions

LU-2.1 Development standards. Review existing development and design standards and update as necessary to provide appropriate mitigation or buffers between existing uses, with a focus on additional buffering when new uses could negatively impact environmental justice areas. (EP, *new*)

See also Action ER-4.2 Citywide emissions monitoring.

Housing Element

Principles

- Preserving, maintaining, improving, and creating distinct neighborhoods and the housing stock within them protects property values and provides a desirable place to live (H, exist).

Goals and Policies

Goal H-1 **Stable neighborhoods of quality housing, ample community services and public facilities, well-maintained infrastructure, and public safety that foster a positive sense of identity.** (H, exist)

H-1.5 **Neighborhood Identity.** We strengthen neighborhood identity through creating parks and recreational outlets, sponsoring neighborhood events, and encouraging resident participation in the planning and improvement of their neighborhoods. (H, CE, exist)

Goal H-2 **Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.** (H, exist)

Policy H-2.2 **Historic Downtown.** We foster a vibrant historic downtown through facilitating a wide range of housing types and affordability levels for households of all ages, housing preferences, and income levels. (H, exist)

Goal H-4 **Increased opportunities for low- and moderate-income households and families to afford and maintain quality ownership and rental housing opportunities, including move-up opportunities. Inclusive communities, racial equity, fair housing choice, and access to opportunity.** (H, *mod*)

H-4.1 **Preservation of Affordable Apartments.** We strive to facilitate the preservation of the affordability of publicly assisted apartments for lower-income households through financial assistance, technical assistance, rehabilitation, and collaborative partnerships. (H, exist)

H-4.2 **Homeownership Opportunities.** We increase and expand homeownership rates for lower- and moderate-income households by offering financial assistance, low-interest loans, and educational resources, and by working in collaboration with partnerships. (H, exist)

H-4.3 **Rental Assistance.** We support the provision of rental assistance for individuals and families earning extremely low, very low, and low income with funding from the state and federal government. (H, exist)

H-4.4 **Mixed-income Housing.** We encourage the integration of affordable housing in the Ontario Ranch, Ontario Airport Metro Center Area, and existing neighborhoods. (H, exist)

- H-4.5 Collaborative Partnerships.** We support collaborative partnerships of nonprofit organizations, affordable housing developers, major employers, and for-profit developers to produce affordable housing. (H, exist)
- H-4.6 Fair Housing.** We further fair housing by prohibiting discrimination in the housing market, lifting barriers that restrict access to housing, and providing education, support, and enforcement services to address discriminatory practices. (H, *mod*)
- Goal H-5 A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age, or other status.** (H, exist)
- H-5.1 Senior Housing.** We support the development of accessible and affordable senior housing and provide financial assistance for seniors to maintain and improve their homes. (H, exist)
- H-5.2 Family Housing.** We support the development of larger rental apartments that are appropriate for families with children, including, as feasible, the provision of services, recreation, and other amenities. (H, exist)
- H-5.3 Disabled People.** We increase the supply of permanent, affordable, and accessible housing for people with disabilities, and provide assistance to allow them to maintain and improve their homes. (H, exist)
- H-5.4 Homeless People.** We partner with nonprofit partners to provide emergency shelters, transitional housing, permanent supportive housing, and supportive services for people who are homeless. (H, exist)
- H-5.5 Supportive Services.** We financially support organizations, as feasible, that provide support services that meet the needs of those with special needs and further the greatest level of independence. (H, exist)
- H-5.6 Partnerships.** We collaborate with nonprofit organizations, private developers, employers, government agencies, and other interested parties to develop affordable housing and provide support services. (H, exist)

Actions

(Note that actions listed as programs in the Housing Element, full text available in the Housing Element Technical Report):

- Program 6 Neighborhood Stabilization.** Implements the City's Neighborhood Preservation Strategy Plan, which identifies revitalization strategies in key neighborhoods in northwest Ontario. (H, *mod*)
- Program 11 Ontario Ranch.** Encourages development of affordable housing in future developments in Ontario Ranch, the high resource area in the city. (H, PF, *mod*)

Program 23 Public Housing. Commits to expanding the use of Housing Choice Vouchers (aka Section 8 Vouchers) in high and moderate resource areas. (H, PF, *mod*)

Program 24 Homeownership Program. Provide resources in multiple languages to reduce language barriers. (H, *mod*)

Program 27 Fair Housing Implementation. Provide resources in multiple languages to reduce language barriers. (H, CE, *mod*)

Parks and Recreation Element

Principles

- Well planned, managed and maintained parks provide opportunities for physical exercise and relaxation, which can lead to better health of members of the community (PF, PA, exist).

Goals and Policies

Goal PR-1 A system of safe and accessible parks that meets the needs of the community. (PA, PF, exist)

PR-1.1 Access to Parks. In all new residential development areas, we strive to provide a park and/or recreational facility within walking distance ($\frac{1}{4}$ mile) of every residence and prioritize the establishment of parks in environmental justice areas that do not have adequate access to parks. (PA, PF, *mod*)

PR-1.4 Joint-use Opportunities. In areas where there is a need but no City recreational facility, we explore joint-use opportunities. (e.g., school sites). (PA, PF, exist)

PR-1.7 Special Needs / Universal Design. We attempt to provide recreational opportunities at parks for people of all ages and abilities. (PA, PF, exist)

Goal PR-2 A range of recreational programs provided by public, private, and nonprofit organizations that meet the needs of the community's varied interests, age groups, and abilities. (PA, PF, exist)

PR-2.3 Community Involvement. We involve the local community in planning programs for neighborhood and community park facilities. (CE, exist)

PR-2.4 Access to Programs. We provide a range of recreational and physical exercise programs that are accessible to residents of all income levels throughout the community and prioritize establishing and maintaining equitable access for residents in environmental justice areas. (PA, PF, *mod*)

Actions

PR-1.1 Recreation and Parks Master Plan. Implement recommendations as outlined in the 2021 Master Plan, with a priority on addressing the needs in environmental justice areas. This includes recommendations such as new and/or expanded facilities, new

and expanded programs and services, community and agency partnerships, and fee structures.

Environmental Resources Element

Principles

- Every resident of Ontario should have the opportunity to live in a community that is healthy and safe (EP, PA, *new*).

Goals and Policies

Goal ER-1 A reliable and cost-effective system that permits the City to manage its diverse water resources needs. (EP, exist)

ER-1.5 **Water Resource Management.** Environmental justice areas are prioritized as we coordinate with local agencies to protect water quality, prevent pollution, address existing contamination, and remediate contaminated surface water and groundwater. (EP, *new*)

Goal ER-4 Improved indoor and outdoor air quality and reduced locally generated pollutant emissions. (EP, exist)

ER-4.2 **Sensitive Land Uses.** We prohibit the future siting of sensitive land uses within the distances defined by the California Air Resources Board for specific source categories, without sufficient mitigation. (EP, exist)

ER-4.4 **Indoor Air Quality.** We will comply with State Green Building Codes relative to indoor air quality. We seek funding to improve indoor air quality for households with poor indoor air quality, with priority for lower income households in environmental justice areas. (EP, *mod*)

ER-4.7 **Other Agency Collaboration.** We collaborate with other agencies within the South Coast Air Basin to improve regional air quality at the emission source, with a particular focus on sources that affect environmental justice areas in Ontario. (EP, *mod*)

ER-4.8 **Tree Planting.** We protect healthy trees within the City and plant new trees to increase carbon sequestration and help the regional/local air quality. We expand the tree canopy in environmental justice areas to enhance air quality and reduce the “heat island” effect. (EP, *mod*)

ER-4.9 **New Localized Air Pollution Sources Near Existing Sensitive Receptors.** We require new developments to conduct a Health Risk Assessment for land uses that generate more than 100 trucks per day or 40 trucks per day by trucks operating transportation refrigeration units (TRU's) within 1,000 feet from sensitive land uses (California Health and Safety Code § 42705.5(a)(5)). If the health risk assessment determines the new development poses health hazards that increase the

incremental cancer risk above the threshold established by the South Coast Air Quality Management District (AQMD), we will only approve permits upon the condition that adequate mitigation measures are proposed and implemented for potential impacts on the sensitive uses around the site and along the route within Ontario taken by the trucks to and from freeways. We require new developments that must perform a health risk assessment to conduct additional public outreach by sending notifications in multiple languages to all residents living within 500 feet, and encourage hosting a public meeting. (EP, *new*)

Actions

- ER-4.1 Citywide and regional air quality planning.** Review existing and monitor the development of new air monitoring and emissions reduction plans prepared by the South Coast Air Quality Management District. Gather and evaluate measures and strategies in such plans for their applicability to and feasibility for Ontario. (EP, *new*)
- ER-4.2 Citywide emissions monitoring.** Coordinate with the South Coast Air Quality Management District (SCAQMD) to monitor existing air measurements and recommend new air measurements and locations, with a specific focus on evaluating new measurements and locations in environmental justice areas. Coordinate with SCAQMD to provide monitoring information online to the public, including historical data and an explanation of trends, thresholds, and ongoing monitoring results. (EP, *new*)
- ER-4.3 Community air quality planning.** Engage with the South Coast Air Quality Management District and local stakeholders to evaluate the next steps in pursuing a priority community designation and/or community air protection program incentives for eligible environmental justice areas of the city, with focus on areas with unique needs and highest pollution burden as identified in the latest available CalEnviroScreen tool. If such a designation or incentives are not awarded or available, seek grant funds for activities such as local air quality monitoring.
- Also, explore ways to initiate data collection efforts for a community emissions reduction and/or community air monitoring plan, including the identification of information needed (new or updated), potential data sources and needed resources, and strategies to engage residents and collect information. (EP, *new*)
- ER-4.4 Health risk assessments.** Draft and adopt ordinance to implement new requirements (Policy ER-4.9) for health risk assessments when considering specific uses near existing sensitive receptors. (EP, *new*)
- ER-4.5 Trucks and cargo handling equipment.** Evaluate and implement strategies to reduce emissions associated with truck idling and cargo handling equipment near areas with existing and planned sensitive receptors, with a priority placed on facilities that have not yet finalized building permits and for those facilities in or adjacent to environmental justice areas. (EP, *new*)

- ER-4.6** **Healthy indoor options.** Explore options to incentivize or require the provision of indoor recreation space, particularly in environmental justice areas that experience high levels of exposure to air pollution. (PA, PF, *new*)
- ER-4.7** **Proactive engagement.** Collaborate with the South Coast Air Quality Management District and local stakeholders in environmental justice areas experiencing local air pollution issues to outline objectives and strategies for monitoring air pollution. Augment existing outreach programs to improve public awareness of state, regional and local agencies' roles, and resources to identify, monitor, and address air quality and other environmental hazards in the community. (CE, EP, *new*)

Community Economics Element

Principles

None.

Goals and Policies

- Goal CE-1** **A complete community that provides for all incomes and stages of life.** (PF, PA, exist)
- CE-1.6** **Diversity of Housing.** We collaborate with residents, housing providers, and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to encourage the development of housing supportive of our efforts to attract business in growing sectors of the community while being respectful of existing viable uses (H, *mod*).
- CE-1.12** **Circulation.** We continuously plan and improve public transit and non-vehicular circulation for the mobility of all, including those with limited or no access to private automobiles. (PF, PA, exist)

Actions

- CE-1.1** **Small Business Assistance.** Collaborate with the County Economic Development Agency and California District of the Small Business Administration to expand and promote resources and assistance for small businesses, with a specific focus on those in environmental justice areas. (PF, *new*)

See also actions associated with the Housing and Mobility Elements.

Safety Element

Principles

- It is the role of government to minimize exposure to natural and human-caused hazards. (EP, exist)

- A safe and healthy environment is necessary to build and maintain a sustainable, resilient, and prosperous Ontario. (EP, *mod*)

Goals and Policies

Goal S-1 **Minimized risk of injury, loss of life, property damage, and economic and social disruption caused by earthquake-induced and other geologic hazards.** (EP, exist)

S-1.4 **Seismically Vulnerable Structures.** We conform to state law regarding unreinforced masonry structures and coordinate with not-for-profits to facilitate seismic retrofits in environmental justice areas and for low-income households. (H, *mod*)

Goal S-3 **Reduced risk of death, injury, property damage and economic loss due to fires, accidents and normal everyday occurrences through prompt and capable emergency response.** (EP, exist)

S-3.5 **Emergency Notifications.** We maintain a public alert notification system that efficiently conveys information about imminent, developing, ongoing, and concluding emergency events to residents and visitors, working with network providers that translate information into other languages. (PF, *new*)

Goal S-4 **An environment where noise does not adversely affect the public's health, safety, and welfare.** (EP, exist)

S-4.7 **Rail Noise Mitigation.** We require residential and mixed-use development of vibration-sensitive uses in areas within 200 feet of rail to evaluate for indoor vibration levels and mitigate any exceedances of the Federal Transit Administration vibration-annoyance criteria. (H, *new*)

Goal S-6 **Reduced potential for hazardous materials exposure and contamination.** (EP, exist)

S-6.3 **Safer Alternatives.** We minimize our use of pesticides and other hazardous materials by choosing non-toxic alternatives that do not pose a threat to the environment, especially when it could affect public park facilities and open spaces. (EP, *mod*)

S-6.4 **Safe Storage and Maintenance Practices.** We require that the users of hazardous materials be adequately prepared to prevent and mitigate hazardous materials releases. (EP, exist)

S-6.5 **Location of Hazardous Material Facilities.** We regulate facilities that will be involved in the production, use, storage, or disposal of hazardous materials, pursuant to federal, state, county, and local regulations, so that impacts to the environment and sensitive land uses are mitigated. We prohibit new hazardous waste facilities in close proximity to sensitive land uses and environmental justice areas. (EP, *mod*)

S-6.6 Location of Sensitive Land Uses. We prohibit new sensitive land uses from locating within airport safety zones and near existing sites that use, store, or generate large quantities of hazardous materials. (EP, exist)

S-6.8 Mitigation and Remediation of Groundwater Contamination. We actively participate in local and regional efforts directed at both mitigating environmental exposure to contaminated groundwater and taking action to clean up contaminated groundwater once exposure occurs. (EP, exist)

Actions

S-1.1 Seismic Retrofits. Identify potential funding sources to facilitate seismic retrofits in environmental justice areas and for low-income households. (H, *mod*)

S-4.1 Vibration studies. Update development regulations to require vibration-sensitive uses in areas within 200 feet of rail to evaluate for indoor vibration levels and mitigate any exceedances of the Federal Transit Administration vibration-annoyance criteria. (H, *new*)

S-3.2 Public Outreach. Coordinate with network providers to ensure that residents and visitors, especially those in environmental justice communities, have access to emergency notifications. Information should be provided in multiple languages and formats appropriate for people with access and functional needs. (*new, numbering based on climate adaptation and resiliency*)

Mobility Element

Principles

- Access to convenient local and regional mobility options is essential to the City's growth and prosperity. (PF, exist)
- A comprehensive multimodal mobility system is vital to providing equitable access to jobs, schools, shopping, services, parks, and other key destination points for people of all abilities and incomes. (PF, *mod*)
- Well designed and maintained roadways, sidewalks, and bikeways are essential for the safe and efficient movement of goods and people. (PF, *mod*)

Goals and Policies

Goal M-1 A system of roadways that meets the mobility needs of a dynamic and prosperous Ontario. (PA, PF, exist)

M-1.4 Complete Streets. We work to provide a complete, balanced, context-aware, multimodal transportation network that meets the needs of all users of streets, roads, and highways, including motorists, pedestrians, bicyclists, children, persons with disabilities, seniors, movers of commercial goods, and users of public

transportation. We prioritize implementation of complete streets improvements in environmental justice areas to facilitate opportunities for residents to use active transportation systems. (PA, PF, *mod*)

Goal M-2 A system of trails and corridors that facilitate and encourage active modes of transportation. (PA, PF, *mod*)

M-2.1 **Active Transportation.** We maintain our Active Transportation Master Plan to create a comprehensive system of on- and off-street bikeways and pedestrian facilities that are safe, comfortable, and accessible and connect residential areas, businesses, schools, parks, and other key destination points. (PA, PF, *mod*)

M-2.2 **Bicycle System.** We provide off-street multipurpose trails and Class II bikeways as our preferred paths of travel and use the Class III for connectivity in constrained circumstances. When truck routes and bicycle facilities share a right-of-way, we prefer Class I or Class IV bicycle facilities. We require new development to include bicycle facilities, such as bicycle parking and secure storage areas. (PA, PF, *mod*)

M-2.3 **Pedestrian Walkways.** We require streets to include sidewalks and visible crosswalks at major intersections where necessary to promote safe and comfortable mobility between residential areas, businesses, schools, parks, recreation areas, and other key destination points. (PA, PF, *mod*)

M-2.4 **Network Opportunities.** We use public rights-of-way and easements such as, utility easements, levees, drainage corridors, road rights-of-way, medians, and other potential options to maintain and expand our bicycle and pedestrian network. In urban, mixed- use, and transit-oriented Place Types, we encourage the use of underutilized public and private spaces to expand our public realm and improve pedestrian and bicycle connectivity. (PA, PF, *mod*)

Goal M-3 A public transit system that is a viable alternative to automobile travel and meets basic transportation needs of the transit-dependent. (PF, exist)

M-3.1 **Transit Partners.** We maintain a proactive working partnership with transit providers to ensure that adequate public transit service is available, cost-efficient, and convenient, particularly for residents in environmental justice areas. (PF, *mod*)

M-3.6 **Metrolink Expansion.** We advocate expansion of Metrolink service to include the Downtown and the multimodal transit center. (PF, exist)

M-3.8 **Feeder Systems.** We work with regional transit agencies to secure convenient feeder service from the Metrolink station and the proposed multimodal transit center to employment centers in Ontario. (PF, exist)

M-3.11 **Transit and Community Facilities.** We require the future development of communitywide serving facilities to be sited in transit-ready areas that can be served and made accessible by public transit. Conversely, we plan (and coordinate with other transit agencies to plan) future transit routes to serve existing community facilities. (PF, exist)

Goal M-4 An efficient flow of goods through the City that maximizes economic benefits and minimizes negative impacts. (EP, PF, exist)

M-4.1 **Truck Routes.** We designate and maintain a network of City truck routes that provide for the safe and efficient transport of goods while minimizing negative impacts on local circulation and noise-sensitive land uses, as shown on Exhibit M-04, Truck Routes. We will minimize conflicts on truck routes through the design and implementation of buffers between travel lanes and pedestrian and bicycle facilities on designated truck routes. (PF, *mod*)

M-4.2 **Regional Planning.** We work with regional and subregional transportation agencies and adjacent cities to plan and implement goods movement strategies, including regional truck routes, plans and projects that improve mobility, support the efficient movement of goods, and minimize negative environmental impacts. (EP, *mod*)

M-4.4 **Environmental Considerations.** We support both local and regional efforts to reduce/eliminate the negative environmental impacts of goods movement through the planning and implementation of truck routing and the development of a plan to evaluate the future needs of clean fueling/recharging and electrified truck parking. (EP, *mod*)

Actions

M-2.1 **Priority Improvements.** Refine the City's Capital Improvement Program (CIP) and implementation recommendations of the Active Transportation Master Plan (ATMP) to elevate the priority of improvements proposed in (or serving) environmental justice areas. Continue to identify additional improvements that should be added to complete networks, remove barriers, and create buffers for pedestrians and bicyclists along truck routes, with priority given to those in environmental justice areas. (PF, PA, *mod*)

See also Action CD-2.2 Transit Stops and CD-2.3 Bus Rapid Transit.

M-3.1 **Expanded Bus and Light Rail Service.** Coordinate with Omnitrans and Metrolink to implement and update the agencies' strategic plans and long range transportation plans to prioritize improvements in and expansion of service in Ontario's environmental justice areas. In the next update of these plans, encourage each agency to increase the weighting of environmental justice factors in the prioritization of improvements and service expansion. (PF, *new*)

M-4.1 **Clean Energy Plan for Trucks and Cargo Handling Equipment.** Develop and implement a plan to evaluate the future needs of clean fueling/recharging and electrified truck parking and onsite cargo handling equipment. Promote the creation of centralized truck parking areas that provide clean energy refueling stations and other support facilities while ensuring adjacent residential neighborhoods are not negatively impacted by trucking activities. Target locations that serve both the northern and southern industrial parts of the city, and

coordinate to provide truck parking and charging areas in neighboring jurisdictions that serve regional truck travel. Require proposed truck-intensive uses to provide robust onsite clean energy fueling and/or electric charging stations for trucks and onsite cargo handling equipment. For existing and proposed uses, to contribute toward centralized parking solutions (for larger facilities, which will overlap with their compliance with the South Coast Air Quality Management District's Indirect Source Rule). (EP, *mod*)

Community Design Element

Principles

None.

Goals and Policies

Goal CD-1 A dynamic, progressive city containing distinct and complete places that foster a positive sense of identity and belonging among residents, visitors, and businesses. (H, PF, exist)

CD-1.3 **Existing Neighborhoods.** We require the existing character of viable residential and non-residential neighborhoods be preserved, protected, and enhanced. (EP, *mod*)

Goal CD-2 A high level of design quality resulting in neighborhoods, commercial areas, public spaces, parks, and streetscapes that are attractive, safe, functional, human-scale, and distinct. (PF, PA exist)

CD-2.5 **Streetscapes.** We design new and, when necessary, retrofit existing streets to improve walkability, bicycling and transit integration, strengthen connectivity, and enhance community identity through improvements to the public right-of-way such as sidewalks, street trees, parkways, curbs, street lighting and street furniture. (PF, PA, exist)

CD-2.16 **Transit Stops.** We require transit stops be conveniently located, well lit, safe, and clearly accessible to pedestrians, bicyclists, and people of all abilities. (PF, *mod*)

Goal CD-3 Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours. (PF, PA, exist)

CD-3.3 **Complete and Connected Network.** We require that pedestrian, vehicular, and bicycle circulation on both public and private property be coordinated to provide connections internally and externally to adjacent neighborhoods and properties (existing and planned) through a system of local roads and trails that promote walking and biking to nearby destinations (including existing and planned parks,

commercial areas, and transit stops) and are designed to maximize safety, comfort, and aesthetics. (PF, PA, *new*)

Goal CD-5 A sustained level of maintenance and improvement of properties, buildings, and infrastructure that protects the property values and encourages additional public and private investments. (H, PF, exist)

CD-5.4 **Neighborhood Involvement.** We encourage active community involvement to implement programs aimed at the beautification and improvement of neighborhoods. (CE, exist)

Actions

CD-2.1 **Objective Development and Design Standards.** Prepare objective development and design standards to help streamline the approval and production of new housing, while creating a more walkable, vibrant, and unified community. (H, *new*)

CD-2.2 **Transit Stops.** Coordinate with Omnitrans to implement and update the agency's Bus Stop Safety Improvement Plan, prioritizing improvements in Ontario's environmental justice areas. In the next update, encourage Omnitrans to increase the weighting of environmental justice factors in the evaluation, identification, and prioritization of improvements. (PF, PA, *new*)

CD-2.3 **Bus Rapid Transit.** Coordinate with Omnitrans on street cross section designs for BRT corridors to influence street designs to maximize walkability and bicycle access/safety of the transit riders. (PF, *new*)

Social Resources Element

Principles

- The health, wellbeing, education, and enrichment of its citizens are key components to making Ontario prosperous and a desirable place to live. (EP, PA, FA, exist)
- Access to healthcare, education, community services, and cultural resources encourages community engagement and healthy lifestyle choices. (PF, CE, exist)
- Coordination and collaboration with service providers is essential to a comprehensive system of high quality health, education, cultural activities, and community services in Ontario. (PF, CE, exist)
- A diverse range of community and cultural resources, programs, and facilities help make Ontario a more complete and prosperous community. (PF, exist)

Goals and Policies

Goal SR-1 Residents have access to information, services and goods that improve their health and wellbeing. (PF, CE, exist)

- SR-1.1 Partnering for Healthcare.** We work with healthcare providers, and local, regional, state, and federal agencies to attract and retain a diversity of affordable, quality healthcare and facilities for the entire community. (PF, exist)
- SR-1.2 Nutrition Choices.** We support the promotion of equitable access to affordable healthy food choices in the community, including community gardens, farmers markets, and cooking classes. (FA, *mod*)
- SR-1.3 Health Education.** We promote equitable access to health education, including disease prevention, mental health, nutrition, and physical fitness. (PA, FA, *mod*)
- SR-1.4 Physical Activity.** We encourage activities and community design that improve the physical fitness of our community members, with an emphasis on the provision of activities and facilities in environmental justice areas. (PA, PF, *mod*)
- Goal SR-2 A range of educational and training opportunities for residents and workers of all ages and abilities that improves their life choices and provides a skilled workforce for our businesses.** (CE, exist)
- SR-2.6 Language.** We promote broad outreach in languages used by the community for proposed projects that could negatively affect environmental justice areas. (CE, *new*)
- SR-2.7 Community Engagement.** We promote targeted outreach and education to historically underrepresented groups to encourage meaningful participation in decision-making process for projects whose outcomes will affect land use in environmental justice areas. (CE, *new*)
- Goal SR-4 City libraries that connect community members of all ages and abilities to a broad range of programs, communication, and informational resources.** (CE, exist)
- SR-4.5 Focal Points of the Community.** We design and program Ontario's libraries as focal points of community engagement, including public outreach and community events. (CE, exist)
- Goal SR-5 Local heritage, entertainment, and cultural experiences that enrich the lives of Ontario's residents, workers, and visitors and serve to attract residents and businesses to the City.** (PF, exist)
- SR-5.1 Provision of Entertainment and Culture.** We support equitable access to a range of entertainment and cultural experiences such as public art, exhibitions, and performances. (PF, *mod*)

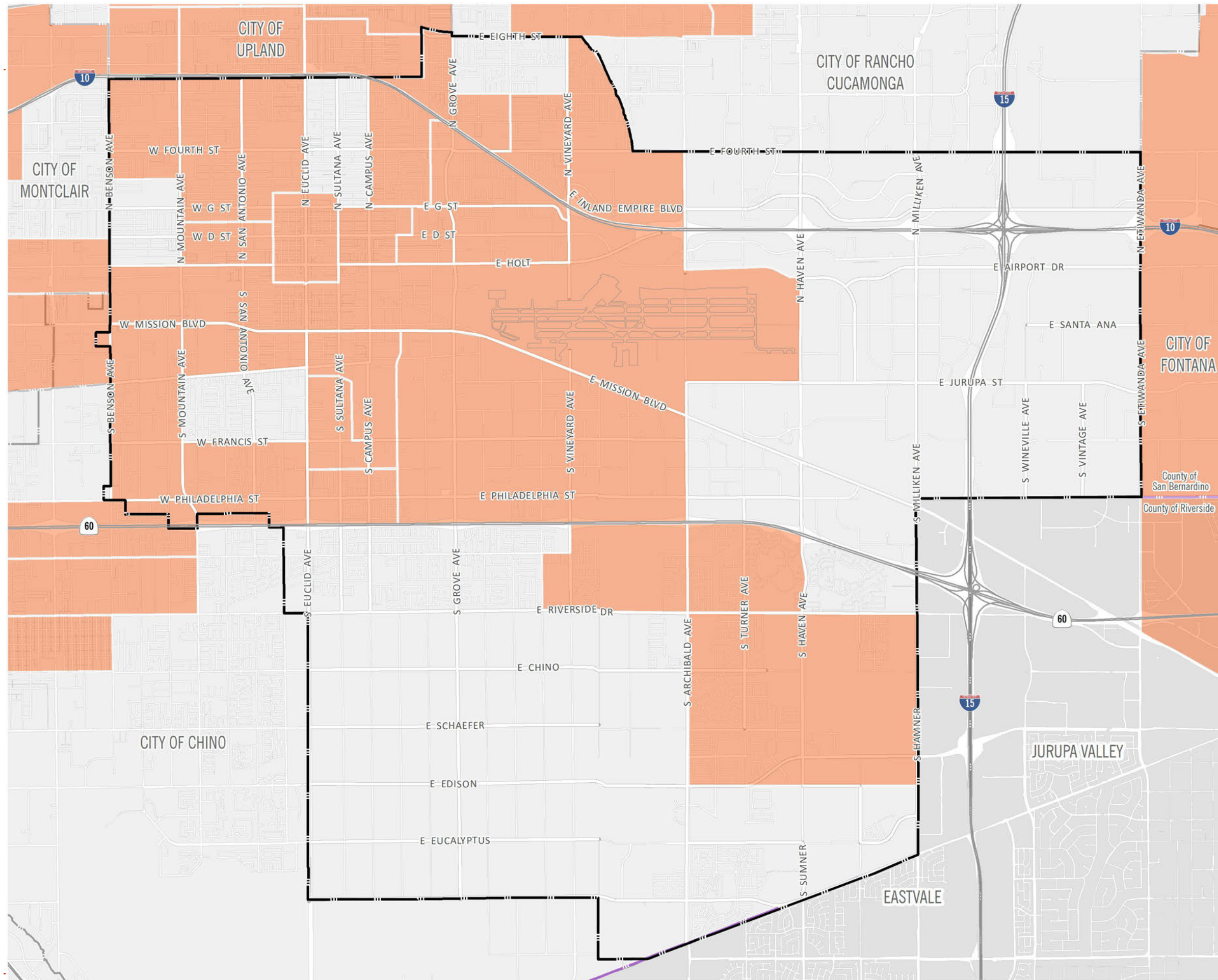
Actions

- SR-1.1 Healthy Ontario.** Continue to pursue programs, incentives, grants, and partnerships to fund and conduct the activities and achieve the objectives of the Healthy Ontario Initiative, including those on prevention and wellness, healthcare

access and utilization, education and lifelong learning, and safe and complete neighborhoods. Augment the initiative to prioritize activities and objectives to address neighborhoods in environmental justice areas. (FA, PF, PA, *mod*)

- SR-1.2 Collaboration.** Develop intentional, strategic partnerships with public, private, and nonprofit entities, such as the Community Healthy Improvement Association of Ontario (CHIA) and Center for Community Action and Environmental Justice (CCA EJ), to improve health outcomes by leveraging capacity, resources, and programs around mutually beneficial initiatives that promote health, equity, and civil engagement for residents, with a priority on those living in environmental justice area areas. (CE, PA, *new*)
- SR. 1.3 Resident Satisfaction Survey.** Augment the City’s resident satisfaction survey (conducted in English and Spanish) to address issues related to community health, pollution, parks, community engagement, and community services, with focused outreach for environment justice concerns and priority areas. Report findings of survey through the City’s various media platforms and utilize input to update the Implementation Plan and Policy Plan, as necessary. (PA, PF, *mod*)
- SR-1.4 Community Centers.** Continue to implement the City’s 2021 Recreation and Parks Master Plan and emphasize the creation of new community centers in environmental justice areas where residents are more than one-half mile from an existing community center. (PF, *mod*)
- SR-1.5 Expanded Representation.** Expand representation of residents concerned about environmental justice issues by extending invitations to such residents/groups (focused especially on those who also live in environmental justice areas within the City) to participate in civic government activities. (CE, *new*)

Figure ER-01
Environmental Justice Areas

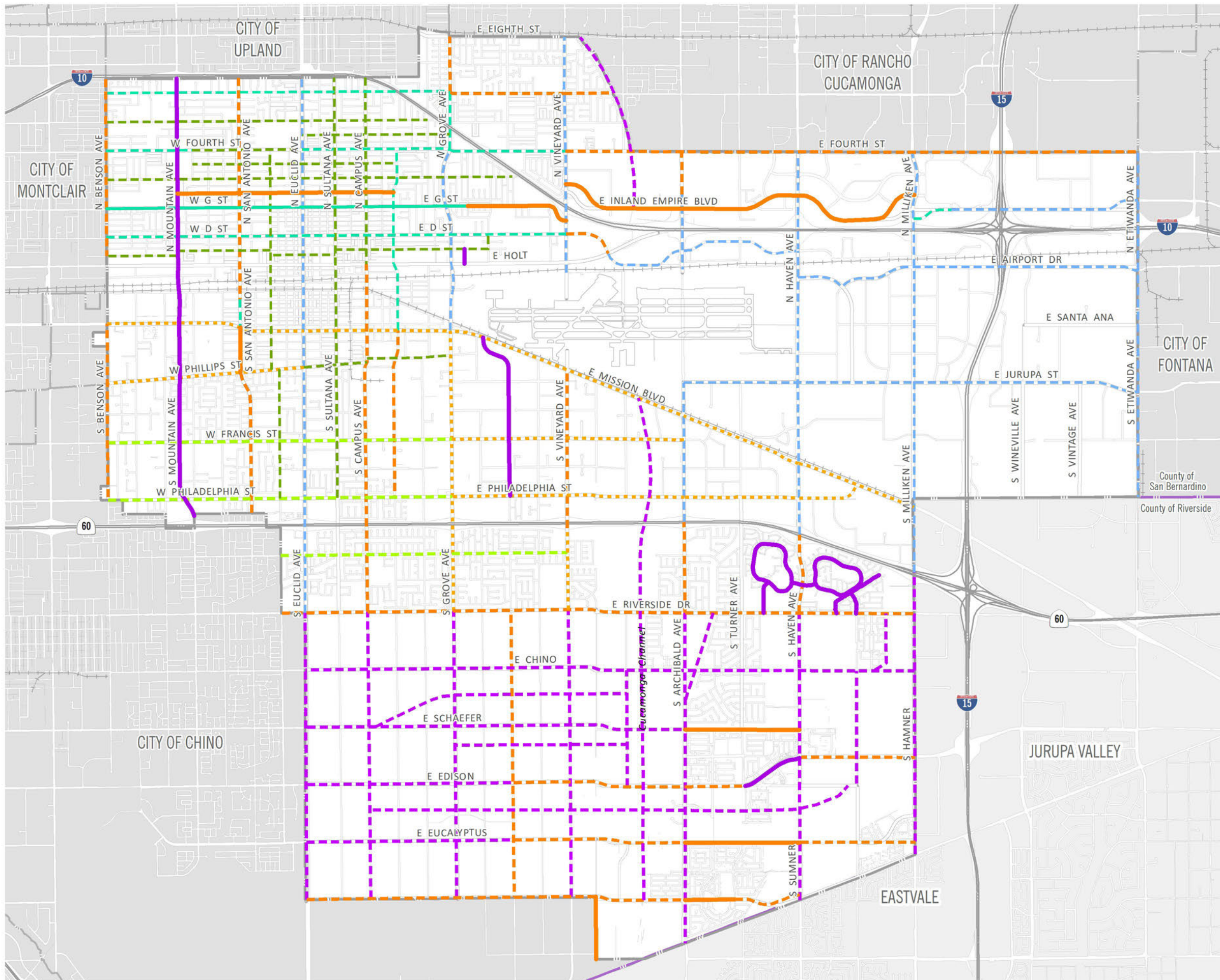


- Environmental Justice Community
- Ontario City Boundary
- County Boundary

2050
THE ONTARIO PLAN

0 2,500 5,000 10,000 FT

Source: CES4 2021 Date: 4/27/2022



MOBILITY

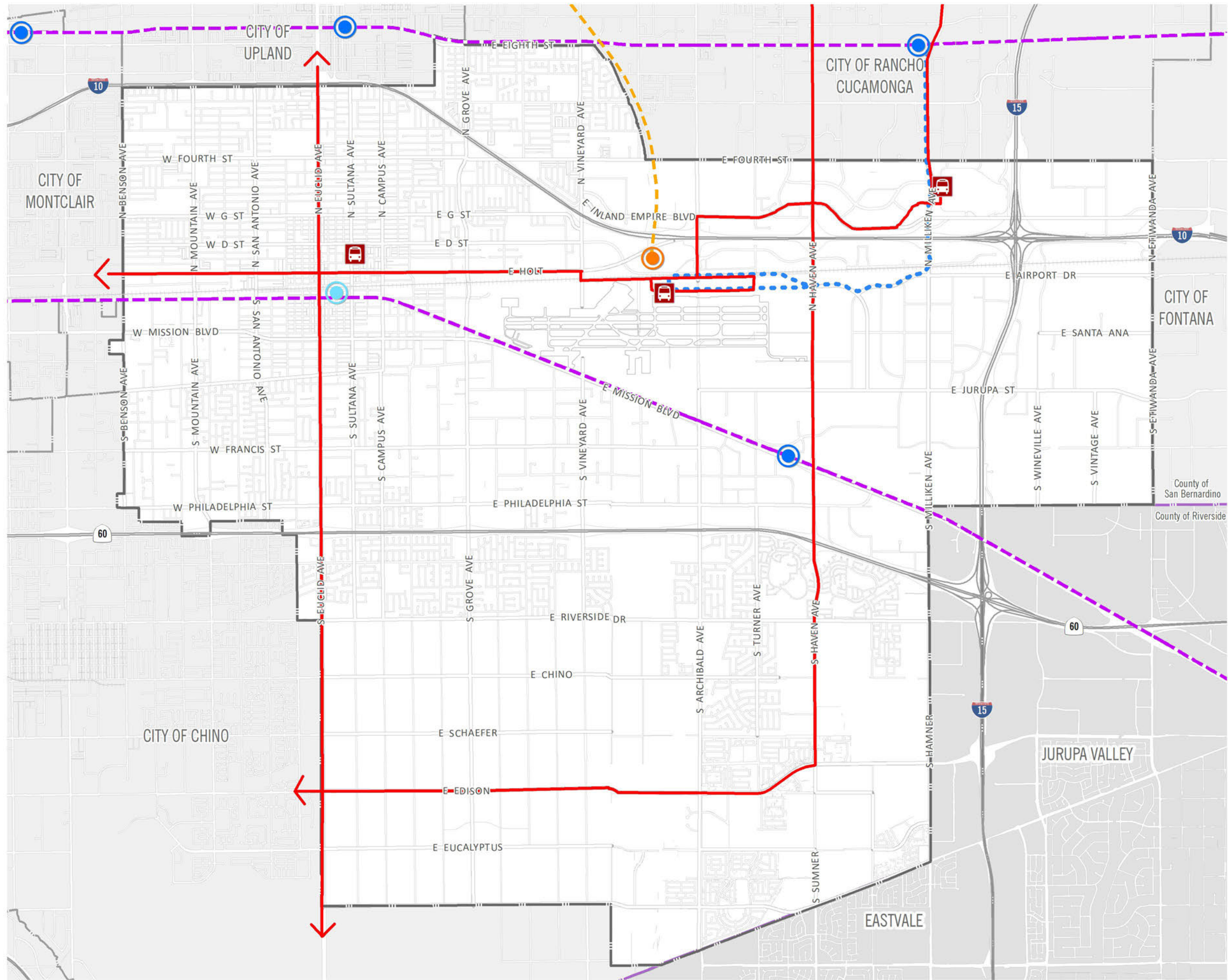
Figure M-02
 Multipurpose Trails & Bikeways

- Ontario City Boundary
- County Boundary
- Rail Network
- Existing Trail & Bike Network**
 - Multipurpose Trail (Class I)
 - Bike Lane (Class II)
 - Bike Route (Class III)
- Proposed Trail & Bike Network**
 - Multipurpose Trail (Class I)
 - Bike Lane (Class II)
 - Buffered Bike Lane (Class II)
 - Bike Lane (Class II)/Bike Route (Class III)
 - Bike Boulevard (Class III)
 - Bike Route (Class III)
 - Additional Studies Required

2050
THE ONTARIO PLAN

0 2,500 5,000 10,000 FT

Source: The City of Ontario 2022 Date: 4/27/2022



MOBILITY

Figure M-03
Public Transit

- Transit Centers**
- Existing Bus Transfer Center
 - Existing Metrolink Station
 - Amtrak Station/Proposed Metrolink Station
 - Future Multimodal Transportation Center
- Corridors & Lines**
- BRT Corridor
 - Metro Goldline Extension
 - Metrolink
 - Ontario Airport Loop
 - Railroad
- Boundaries**
- Ontario City Boundary
 - County Boundary

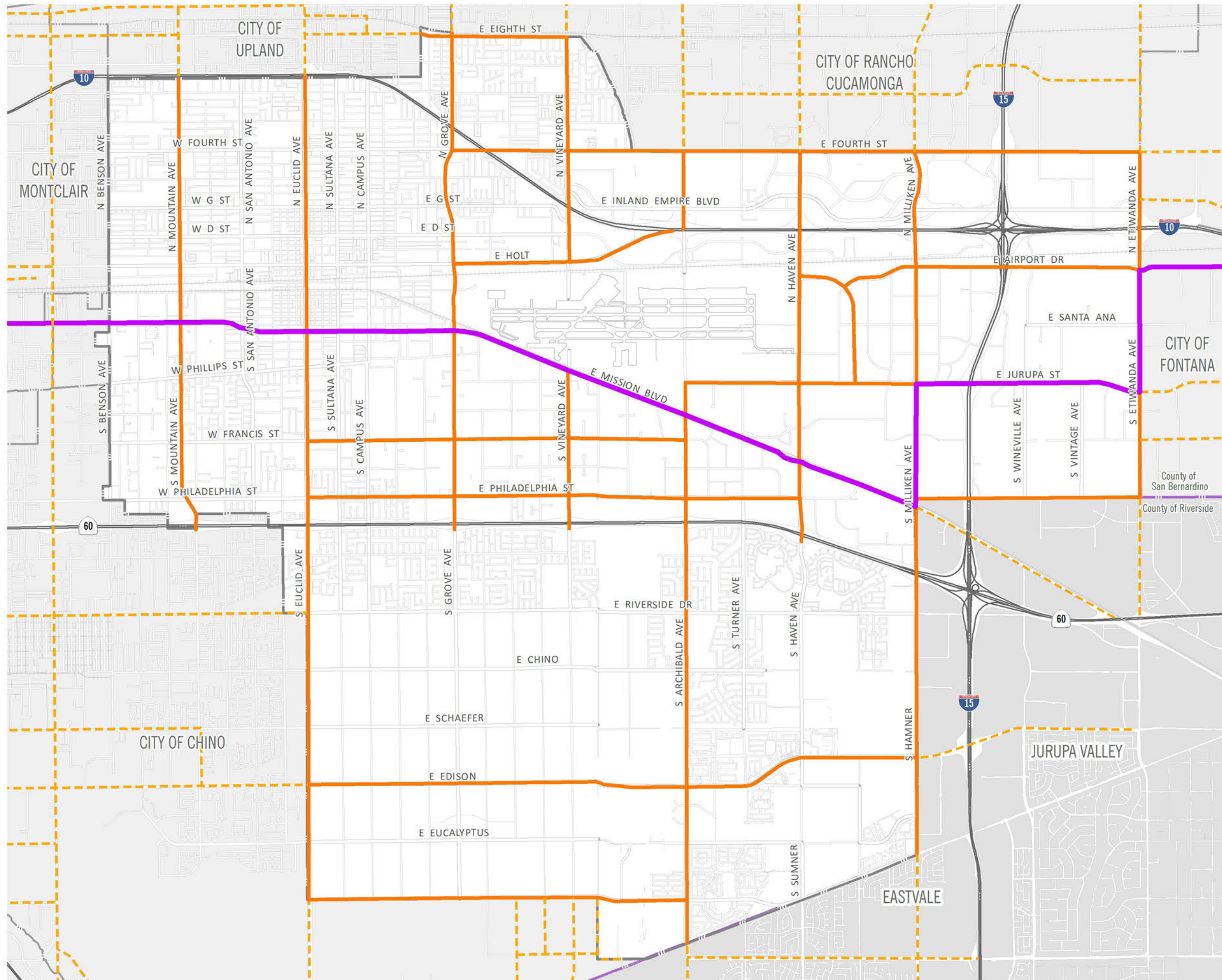
2050
THE ONTARIO PLAN

0 2,500 5,000 10,000 FT

Source: The City of Ontario 2020 Date: 4/29/2022

MOBILITY

Figure M-04
Truck Routes



- State DOT Network
- Truck Route
- Adjacent Agency Truck Route
- Ontario City Boundary
- County Boundary

2050
THE ONTARIO PLAN

0 2,500 5,000 10,000 FT

Source: The City of Ontario 2020 Date: 4/27/2022

CLIMATE ADAPTATION RESILIENCY PRINCIPLES, GOALS, POLICIES, ACTIONS, AND FIGURES

Enacting Legislation

SB 379 introduced legislation that was signed into law in 2015 and mandates requires Ontario to address climate adaptation and resiliency in its Policy Plan and Implementation Plan. Specifically, the City must include three main components:

- A vulnerability assessment that identifies the risks that climate change poses to the local jurisdiction and the geographic areas at risk from climate change impacts.
- A set of adaptation and resilience goals, policies, and objectives based on the information in the vulnerability assessment for the protection of the community.
- A set of feasible implementation measures designed to carry out the identified goals, policies, and objectives.

The goal of this requirement is to enable the community to prepare for, respond to, withstand, and recover from disruptions created or caused by climate change. The City of Ontario is enacting policies related to flooding, fire and rescue and related services, wind related hazards, emergency management, and energy resiliency.

The final draft Vulnerability Assessment and adopted Hazard Mitigation Plan are (or will be) incorporated into the Policy Plan by reference, with copies of each document available on the City website ([Vulnerability Assessment](#) and [Local Hazard Mitigation Plan](#)).

Structure and Indicators

The following content represents principles, goals, and policies from the draft Policy Plan Safety Element and actions/objectives from the draft Implementation Plan. The City opted to address climate adaptation and resiliency exclusively in the Safety Element. While there may be instances where other goals and policies in the Policy Plan may help reinforce or implement the Safety Element, no other goals or policies directly address climate adaptation and resiliency in a sufficient manner as to warrant inclusion in this document.

All content is labeled to indicate whether the content is currently adopted, proposed for substantive modification, or proposed as entirely new (see abbreviation list below). Appendix B of the Draft SEIR compares the Policy Plan goals and policies as currently adopted and as proposed.

Status indicators:

- Currently adopted, no substantive changes (exist)
- Currently adopted, proposed for substantive modification (*mod*)
- Proposed as entirely new or proposed for such modifications as to essentially replace what is currently adopted (*new*)

Safety Element

Principles

- It is the role of government to minimize exposure to natural and human-caused hazards. (exist)
- A safe and healthy environment is necessary to build and maintain a sustainable, resilient, and prosperous Ontario. (exist)
- Reduction in the loss of life, injury, private property damage, infrastructure damage, economic losses and social dislocation can be achieved through planning, preparedness, and response. (exist)
- Interdepartmental and inter-jurisdictional coordination and collaboration are necessary to be resilient to everyday emergencies and major disasters. (exist)

Flooding

Goals and Policies

Goal S-2 Minimize risk of injury, loss of life, property damage and economic and social disruption caused by flooding and inundation hazards. (exist)

S-2.1 Entitlement and Permitting Process. We require hydrological studies prepared by a state-certified engineer when new development is located in a 100-year or 500-year floodplain to assess the impact that the new development will have on the flooding potential of existing development down-gradient. (*mod*)

S-2.2 Floodplain Mapping. We require any new development partially or entirely in 100-year flood zones to provide detailed floodplain mapping for 100- and 200-year storm events as part of the development approval process. (*mod*)

S-2.3 Facilities that Use Hazardous Materials. We comply with state and federal law and do not permit facilities using, storing, or otherwise involved with substantial quantities of onsite hazardous materials to be located in the 100-year flood zone or 500-year flood zone unless all standards of elevation, floodproofing, and storage have been implemented to the satisfaction of the Building Department. (*mod*)

S-2.4 Prohibited Land Uses. We prohibit the development of new essential and critical facilities in the 100-year floodplain and discourage the development of new essential and critical facilities in the 500-year floodplain unless all standards of elevation and flood proofing demonstrate that a facility can be safe and operational during a flood event, implemented to the satisfaction of the Building Department. (*mod*)

S-2.5 Stormwater Management. We maintain the storm drain system to convey a 100-year storm, when feasible, and encourage environmental site design practices to

minimize flooding and increase groundwater recharge, including natural drainage, green infrastructure, and permeable ground surfaces. (*mod*)

- S-2.6 Use of Flood Control Facilities.** We encourage joint use of flood control facilities as open space or other types of recreational facilities. (*exist*)
- S-2.7 Collaboration Between Agencies.** Collaborate with the San Bernardino County Flood Control District and other state and federal agencies to maintain flood-control infrastructure to minimize flood damage. (*new*)

Actions

- S-2.1 Entitlement and Permitting.** Update the City's entitlement and permitting process to require hydrological studies that assess the 100-year and 500-year flood zones to assess the impact that the new development will have on the flooding potential of existing development down-gradient. Additionally update the entitlement and permitting process to require mapping for 200-year floodplains. (*mod*)
- S-2.2 Floodplain Mapping.** Seek grant funds to map the 200-year floodplain in Ontario outside of the entitlement process. (*mod*)
- S-2.3 Public Outreach.** Augment the City's public outreach efforts to disseminate information on flooding, flood control on private property, floodplains, and flood preparedness to the community through the City website, social media, and at City offices. (*mod*)
- S-2.4 Floodproofing Existing Buildings.** Seek grant funds to encourage property owners in 200-year (when mapped) or 500-year flood zones to floodproof existing structures. (*new*)
- S-2.5 Open space in 200-year floodplains.** Coordinate with developers to encourage passive recreation or parkland in 200-year floodplains or floodways (when mapped). (*new*)
- S-2.6 Green and Natural Infrastructure.** Coordinate with developers to incorporate the use of existing (or restoration of) natural features and ecosystem processes and low impact development techniques to increase permeable surfaces and promote sustainable management of open space and parks. This may include, but is not limited to, aquatic or terrestrial vegetated open space, systems and practices that use or mimic natural processes, and other engineered systems, to provide clean water, conserve ecosystem values and functions, promote drainage, and provide a wide array of benefits to people and wildlife. (*mod*)

Fire and Rescue and Related Services

Goals and Policies

- Goal S-3** **Reduced risk of death, injury, property damage and economic loss due to fires, accidents and normal everyday occurrences through prompt and capable emergency response.** (exist)
- S-3.3** **Fire and Emergency Medical Services.** We maintain sufficient fire stations, equipment and staffing to respond effectively to emergencies and meet the needs of the community and state requirements. (*mod*)
- S-3.5** **Emergency Notifications.** We maintain a public alert notification system that efficiently conveys information about imminent, developing, ongoing, and concluding emergency events to residents and visitors, working with network providers that translate information into other languages. (*new*)

Actions

- S-3.1** **Firefighting services and water supply.** Collaborate with regional water providers and surrounding fire protection agencies to ensure adequate water supply, equipment, and personnel for firefighting in the region given future projections regarding prolonged drought and the potential increase in the number and severity of wildfires in the surrounding area (that Ontario Fire Department fights through automatic and mutual aid agreements). (*mod*)
- S-3.2** **Public Outreach.** Coordinate with network providers to ensure that residents and visitors, especially those in environmental justice communities, have access to emergency notifications. Information should be provided in multiple languages and formats appropriate for people with access and functional needs. (*new*)

Emergency Management

Goals and Policies

- Goal S-8** **Disaster resilient, prepared community through effective emergency/disaster preparedness, response, mitigation, and recovery.** (exist)
- S-8.1** **State and Federal Mandates.** We maintain emergency management programs that meet the requirements of the State of California Standardized Emergency Management System (SEMS) and the National Incident Management System (NIMS). (exist)
- S-8.2** **Emergency Management Plans.** We maintain, update, and adopt the Emergency Operations Plan (EOP) and incorporate, by reference the City's Hazard Mitigation Plan (HMP). (exist)

- S-8.3 Emergency/Disaster Training Exercises.** We conduct training and exercises to prepare for and evaluate emergency/disaster response and recovery procedures. (exist)
- S-8.4 Interagency Collaboration.** We maintain partnerships, including automatic aid agreements, with fire protection, police and sheriff departments, and emergency management agencies in San Bernardino and Riverside County to strengthen emergency response. (*new*)
- S-8.5 Interdepartmental Coordination.** We utilize all City departments to help support emergency/disaster preparedness, response, mitigation, and recovery. (exist)
- S-8.6 Community Outreach.** We provide education to the community to promote personal, family, and community emergency preparedness to both natural and human-generated hazards. (exist)
- S-8.7 Extreme Heat and Air Quality.** We work to ensure that all community members are informed about and have access to community cooling centers and clean air centers during extreme heat events or wildfires, with a focus on serving environmental justice communities. We support the development of extreme heat emergency response policies and practices to address these critical health risks in the community. (*new*)
- S-8.8 Regional Partnerships for Climate Adaptation.** We partner with local governments in San Bernardino County, Riverside County, and Inland Southern California Climate Collaborative to develop regional climate change adaptation strategies and programs. (*new*)
- S-8.9 Backup Power in Critical Facilities.** We require backup power be maintained in critical facilities. We encourage backup power solutions that include renewable energy components. (*new*)

Actions

- S-8.1 Evacuation Assessment Plan.** Prior to or concurrently with the update of the City's Hazard Mitigation Plan, conduct an evacuation time assessment to comply with state law (enacted through Assembly Bill 747), accounting for natural and human-generated hazards, existing and proposed traffic evacuation volumes at buildout. (*new*)
- S-8.2 Evacuation Routes.** Review and update, as necessary, standards so that new development has at least two egress/ingress options, key signage that is visible to emergency responders during extreme weather events, signage that identifies evacuation routes, and adequate water supply for structural suppression. (*new*)
- S-8.3 Evacuation assistance program.** Coordinate with Omnitrans, Metrolink, and other transit providers to develop an evacuation assistance program for those with little mobility and those without a vehicle. (*new*)

- S-8.4 Extreme Heat Response Plan.** Develop an extreme heat response plan that includes establishment of community cooling centers and temperature triggers for when they will open, weatherization of City buildings, and cooling strategies for persons engaged in outdoor work and persons experiencing homelessness. (*new*)
- S-8.5 Resilience Hub and Cooling Centers.** Establish a network of equitably located resilience hubs and cooling centers throughout the City. Ensure that such facilities are located outside of areas at risk of hazards (to the extent possible), offer refuge from extreme heat and poor air quality, and are equipped with renewable energy generation, backup power, and backup water supplies. Such facilities should be in easily accessible locations and be available to all community members. (*new*)
- S-8.6 Cooling Transit Areas.** Coordinate with Omnitrans, Metrolink, and Amtrak to increase shading and heat-mitigating materials on pedestrian walkways, outdoor waiting areas, and transit stops/stations. (*new*)
- S-8.7 Critical Facilities.** Review procedures and update as necessary, to ensure that future critical facilities are located outside of hazards zones. If not feasible, ensure new critical facilities are designed to remain functional during hazardous events and identify funding sources to harden and improve existing facilities to remain functional during hazardous events. (*new*)
- S-8.8 Community Emergency Response Team (CERT).** Increase participation in CERT through program by expanding promotion efforts and course offerings, encouraging CERT participation for City employees, and exploring partnerships (e.g., partner with school districts to offer CERT training to high school students). (*new*)

Energy Resiliency

Goals and Policies

- Goal S-9 Incorporate energy efficient practices and renewable energy systems to improve air quality, comfort, and energy reliability during temporary power outages.** (*new*)
- S-9.1 Solar Energy.** We support and may incentivize the installation of residential and commercial solar panels and battery storage systems that can provide electricity during power outages. (*new*)
- S-9.2 Renewable Energy.** Renovate existing city-owned facilities and plan future facilities to include renewable energy generation capacity and battery storage as part of an effort to make public facilities and services greener and more resilient to power outages. (*new*)

S-9.3 Energy Efficiency Retrofits. We support and may incentivize retrofits to residential and commercial buildings that improve energy efficiency and insulation from extreme temperatures, giving priority towards low-income applicants. (*new*)

Actions

S-9.1 Energy independence. Coordinate with developers to encourage the integration of battery storage systems that can provide backup electrical service during temporary power outages. (*new*)

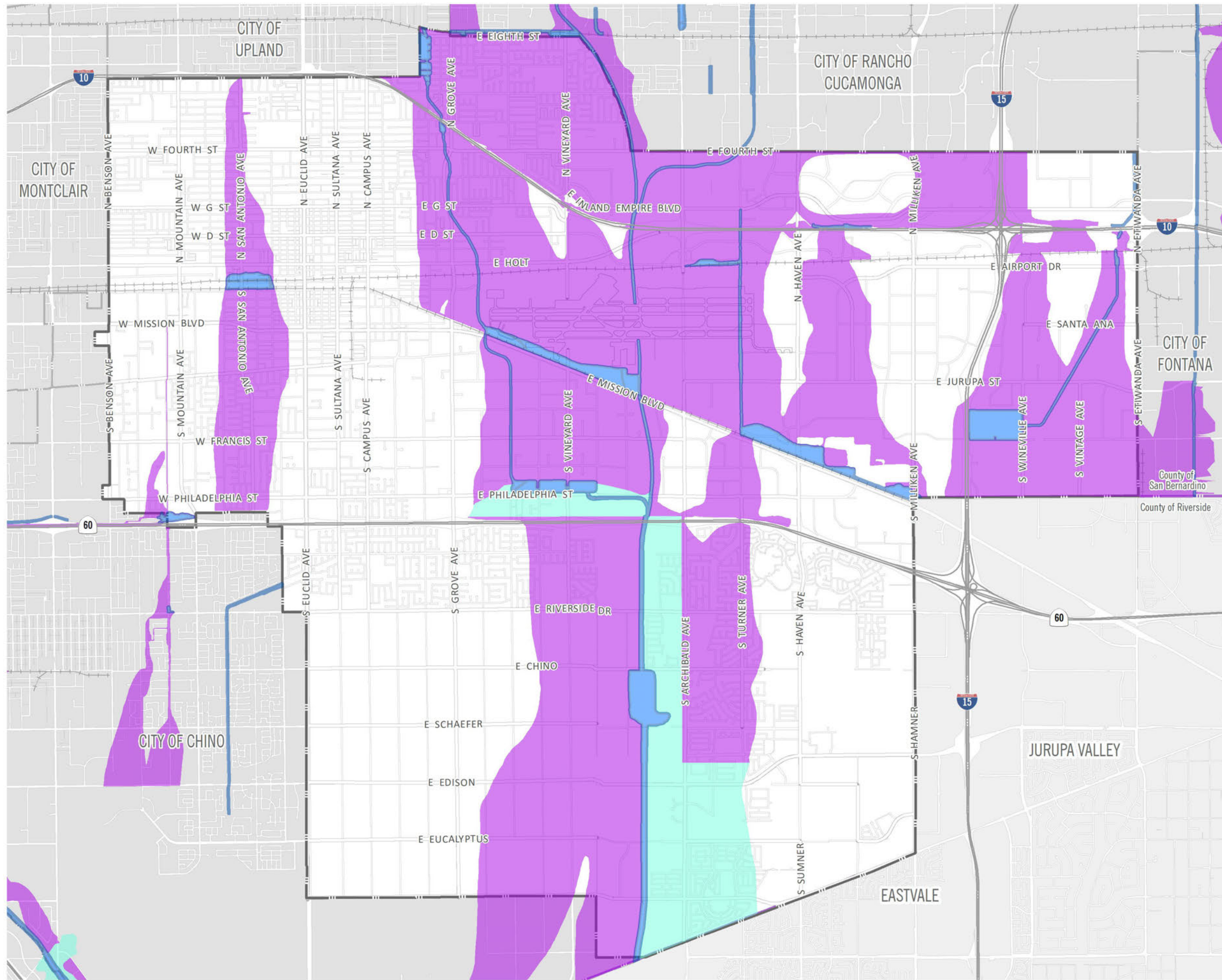
S-9.2 Renewable Energy. Renovate existing city-owned assets and design future city facilities to incorporate renewable energy generation systems, battery storage systems, and energy-efficient design and features, as feasible. (*new*)

S-9.3 Weatherization Program. Expand funding resources through partnerships and grant programs for low-income households and businesses to conduct weatherization and structural retrofits that will increase community resilience.

Documents Incorporated by Reference

[Local Hazard Mitigation Plan](#)

[Vulnerability Assessment Report](#)



SAFETY

Figure S-03
Flood Hazard Zones

- Ontario City Boundary
- County Boundary
- Rail Network

Flood Zone

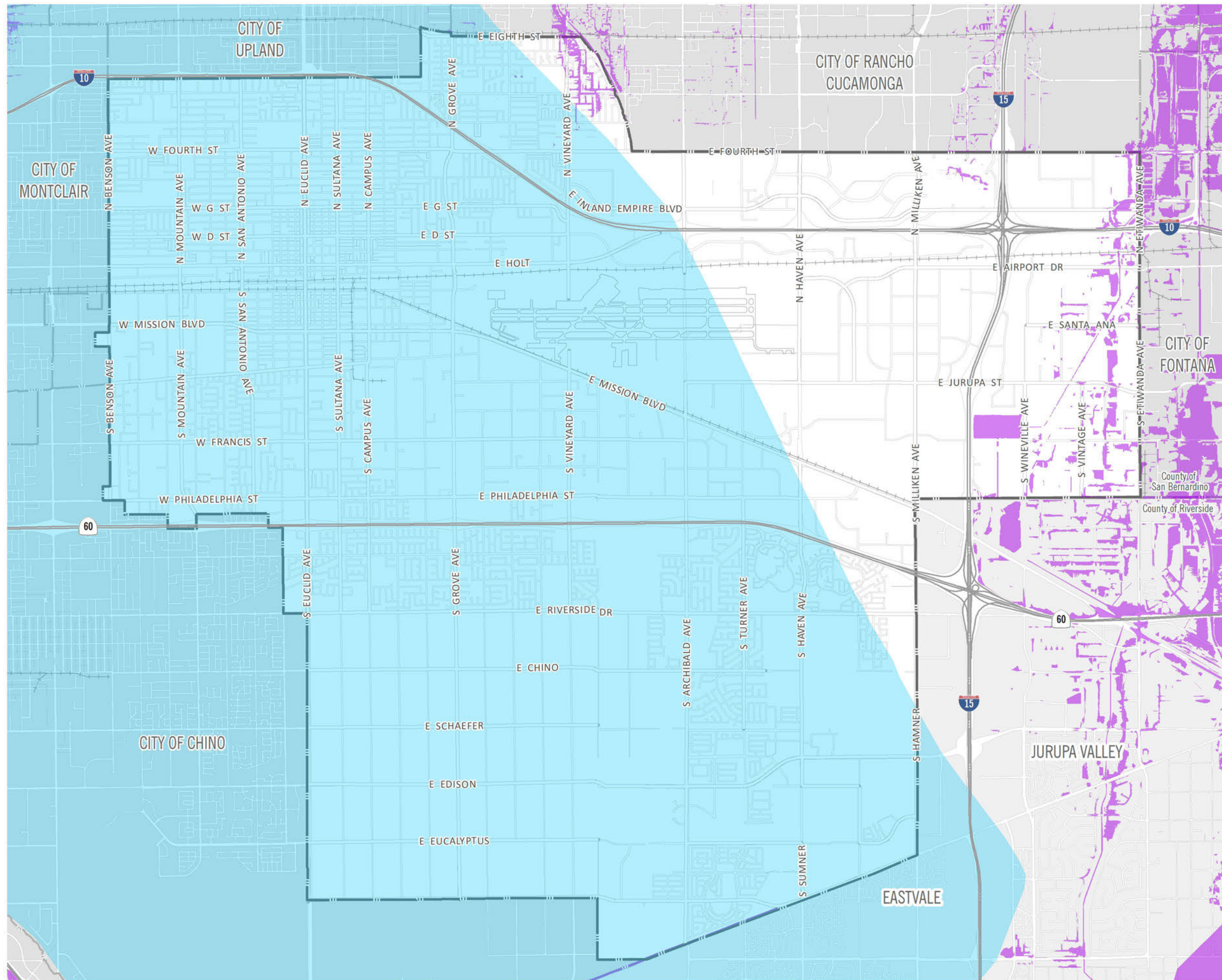
- FEMA 100-Year Floodplain
- FEMA 500-Year Floodplain
- Area with Reduced Flood Risk Due to Levee

2 · 0 · 5 · 0
THE ONTARIO PLAN

0 2,500 5,000 10,000 FT

Source: The City of Ontario 2021 Date: 4/27/2022

Figure S-04
Dam Inundation Zones

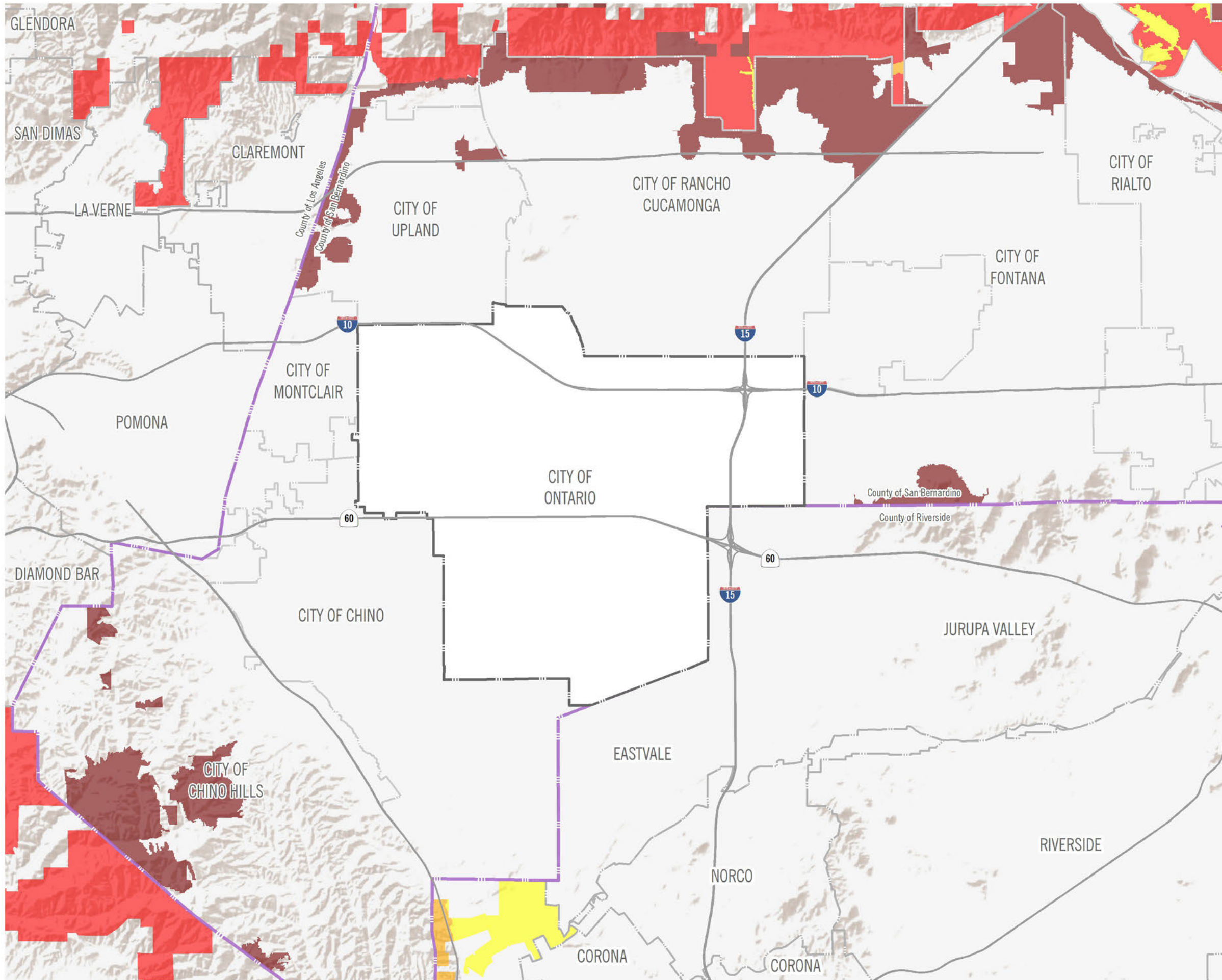


- Potential Inundation from San Antonio Dam
- Potential Inundation from Debris Basins
- Ontario City Boundary
- County Boundary
- Rail Network

2050
THE ONTARIO PLAN

0 2,500 5,000 10,000 FT

Source: DWR 2021 Date: 4/27/2022



SAFETY

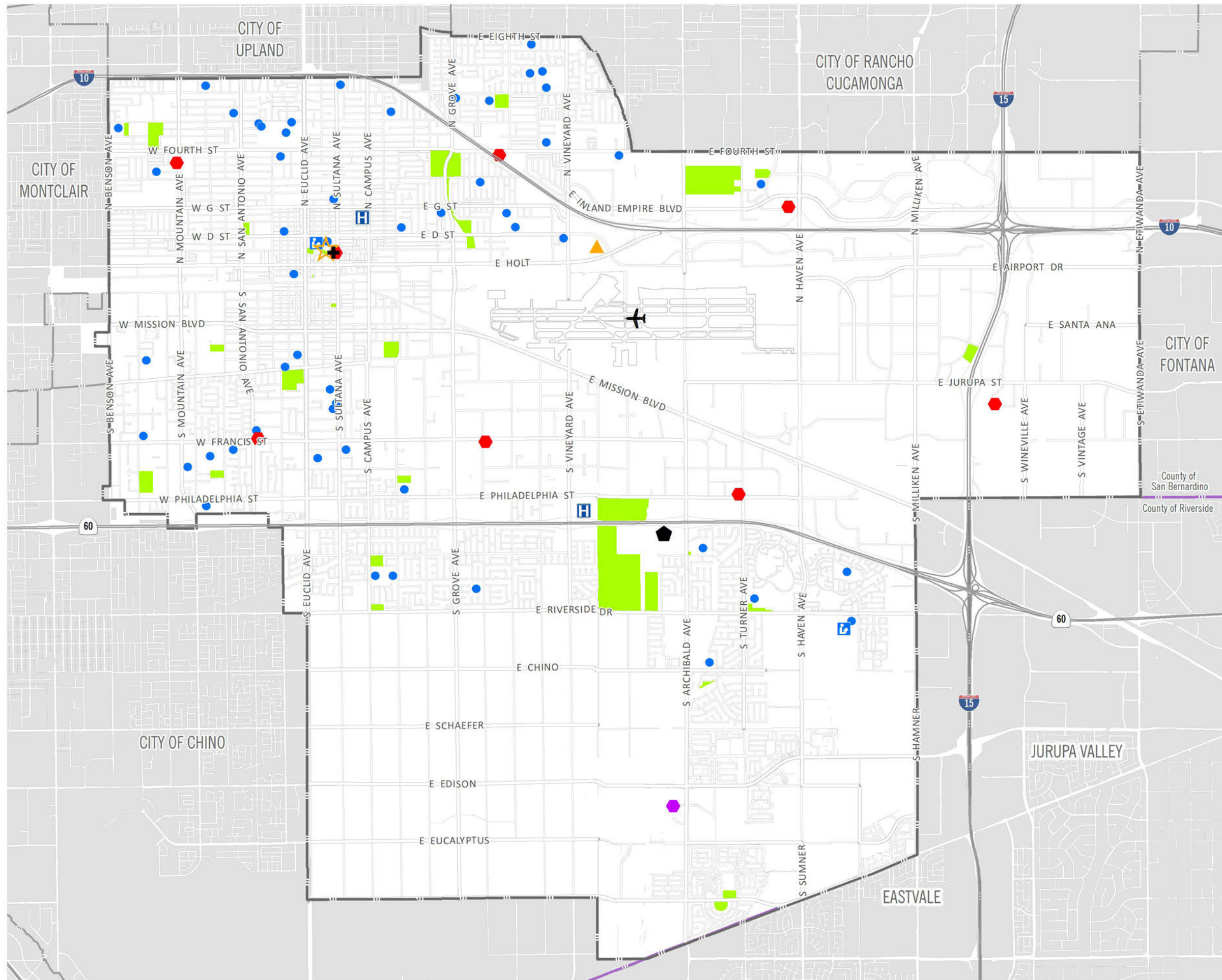
Figure S-05
Fire Hazard Severity Zones

- Ontario City Boundary
- County Boundary
- Fire Hazard Severity Zone in Local Responsibility Area
- Hazard Class
- Very High
- Fire Hazard Severity Zone in State Responsibility Area
- Hazard Class
- Very High
- High
- Moderate

2050
THE ONTARIO PLAN

0 2,500 5,000 10,000 FT

Source: CalFire 2007 Date: 4/27/2022



SAFETY

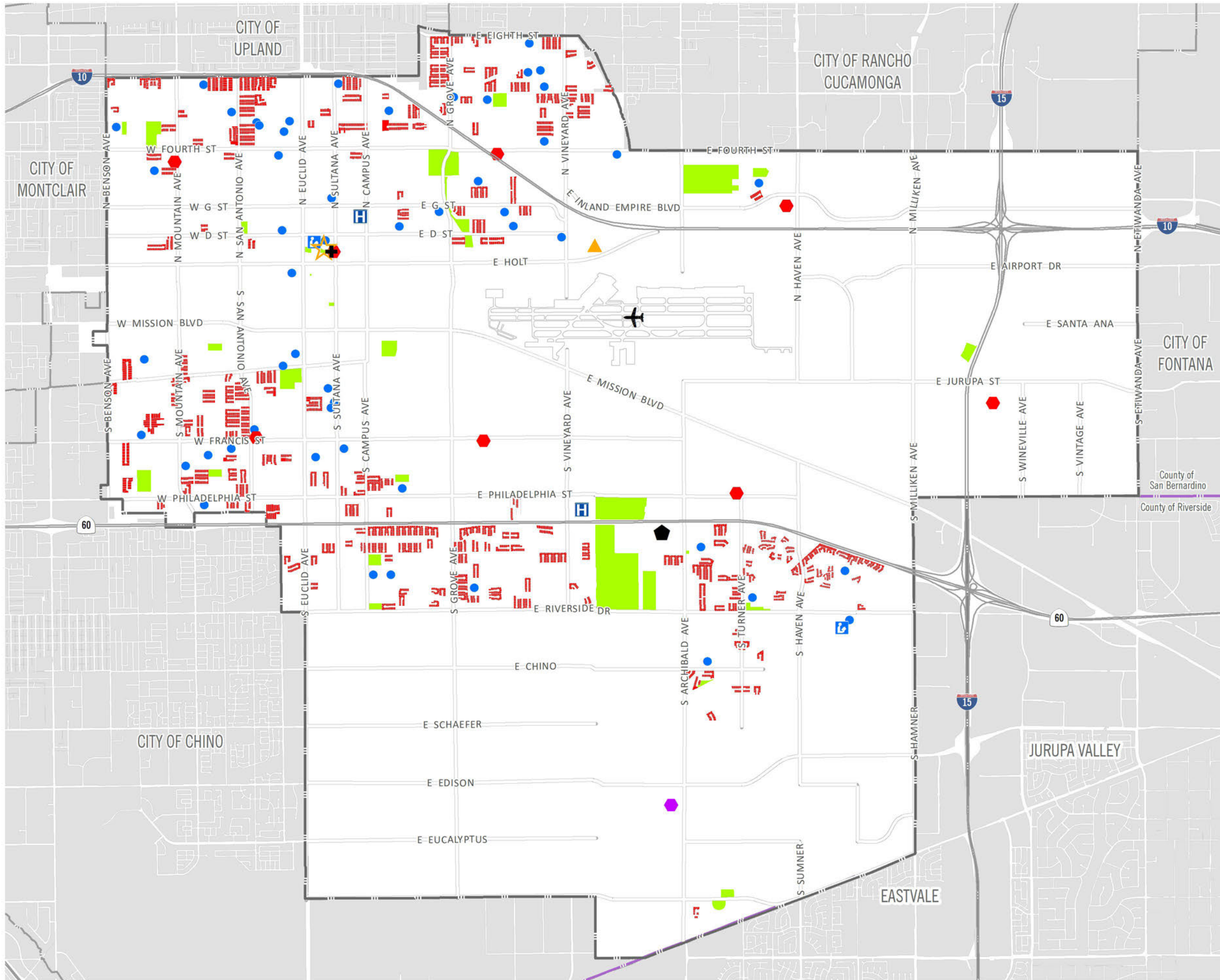
Figure S-08
Critical Facilities

- Ontario City Boundary
- County Boundary
- Critical Facilities**
- City Hall
- EOC
- Police Headquarters
- Convention Center
- Fire Station
- New Fire Station
- Airport
- Hospital
- Library
- Public & Private Schools
- Park

2050
THE ONTARIO PLAN

0 2,500 5,000 10,000 FT

Source: The City of Ontario 2021 Date: 4/28/2022



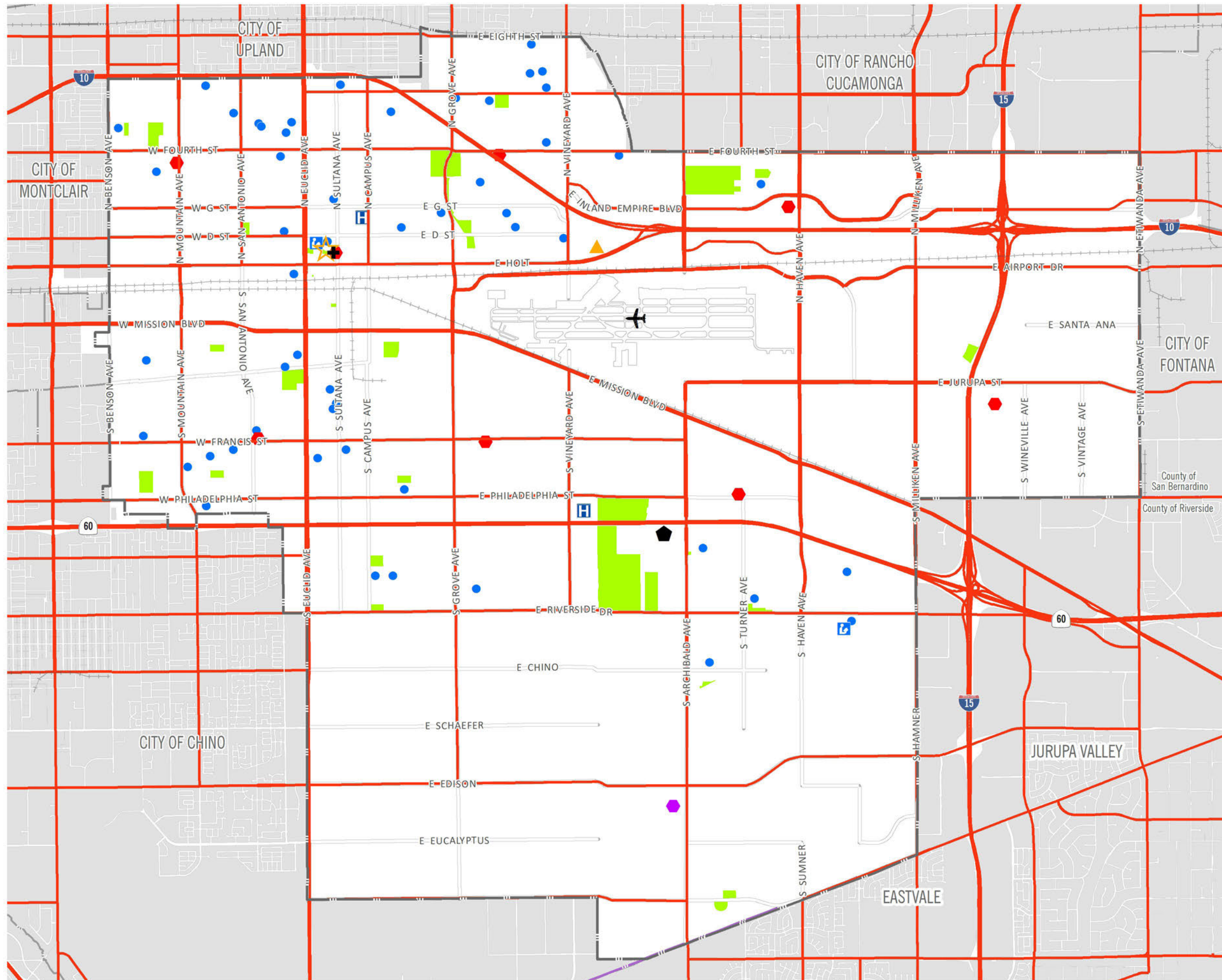
SAFETY

Figure S-09
Evacuation Constrained Areas

- Ontario City Boundary
 - County Boundary
 - Residential Parcel with Evacuation Constraint
- Critical Facilities
- City Hall
 - EOC
 - Police Headquarters
 - Convention Center
 - Fire Station
 - New Fire Station
 - Airport
 - Hospital
 - Library
 - Public & Private Schools
 - Park



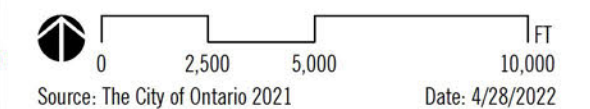
0 2,500 5,000 10,000 FT
 Source: The City of Ontario 2021 Date: 4/28/2022



SAFETY

Figure S-10
Evacuation Routes

- Ontario City Boundary
- County Boundary
- Evacuation Routes
- Critical Facilities**
- City Hall
- EOC
- Police Headquarters
- Convention Center
- Fire Station
- New Fire Station
- Airport
- Hospital
- Library
- Public & Private Schools
- Park
- Rail Network



**CEQA FINDINGS OF FACT AND
STATEMENT OF OVERRIDING CONSIDERATIONS
REGARDING THE
FINAL SUPPLEMENTAL ENVIRONMENTAL IMPACT REPORT
FOR THE ONTARIO PLAN 2050
SUPPLEMENTAL ENVIRONMENTAL IMPACT REPORT
STATE CLEARINGHOUSE NO. 2021070364**

Exhibit B

I. SUMMARY OF FINDINGS

The City Council hereby finds that it has been presented with the Supplemental Environmental Impact Report (SEIR), which it has reviewed and considered, and further finds that the SEIR is an accurate and objective statement that has been completed in full compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines. The City Council finds that the EIR reflects the independent judgment and analysis of the City. The City Council declares that no evidence of new significant impacts or any new information of “substantial importance” as defined by State CEQA Guidelines Section 15088.5, has been received by the City after circulation of the Draft SEIR that would require recirculation. Therefore, the City Council hereby certifies the SEIR based on the entirety of the record of proceedings.

II. PROCEDURAL COMPLIANCE WITH CALIFORNIA ENVIRONMENTAL QUALITY ACT

The City published a Draft SEIR on May 9, 2022. A Final SEIR was prepared in summer 2022 in compliance with CEQA requirements. The Final SEIR has been prepared in accordance with CEQA and the CEQA Guidelines, as amended. As authorized in State CEQA Guidelines Section 15084(d)(2), the City retained a consultant to assist with the preparation of the environmental documents. City staff from multiple departments, representing the Lead Agency, have directed, reviewed, and modified where appropriate all material prepared by the consultant. The Final SEIR reflects the City’s independent analysis and judgement. The key milestones associated with the preparation of the EIR are summarized below. As presented below, an extensive public involvement and agency notification effort was conducted to solicit input on the scope and content of the EIR and to solicit comments on the results of the environmental analysis presented in the Draft SEIR.

A. PUBLIC NOTIFICATION AND OUTREACH

In conformance with CEQA, the State CEQA Guidelines, and the City of Ontario CEQA Guidelines, the City of Ontario conducted an extensive environmental review of the Proposed Project.

- Completion of a Notice of Preparation (NOP) on July 20, 2021. The public review period was extended from July 20, 2021, to August 19, 2021. The NOP was published in the *Inland Valley Daily Bulletin* on July 6, 2021. The NOP was posted at the San Bernardino County Clerk’s office on July 19, 2020. Copies of the NOP were made available for public review at the City of Ontario and the City’s website.

- In accordance with Executive Order N-54-20, suspending open meeting requirements consistent with COVID-19 public health concerns, completion of the scoping process where the public was invited by the City to participate in a virtual scoping meeting held August 5, 2021, at 6:00 PM via Zoom. The notice of a public scoping meeting was included in the NOP.
- Preparation of a Draft SEIR, which was made available for a 45-day public review period beginning May 9, 2022, and ending June 23, 2022. The scope of the Draft SEIR was determined based on the CEQA Guidelines Appendix G Checklist, comments received in response to the NOP, and comments received at the scoping meeting conducted by the City of Ontario. Chapter 5, *Environmental Analysis*, of the Draft SEIR describes the issues identified for analysis in the Draft SEIR. The Notice of Availability (NOA) for the Draft SEIR was sent to interested persons and organizations, sent to the State Clearinghouse in Sacramento for distribution to public agencies, posted at the City of Ontario, and published in the *Inland Valley Daily Bulletin*. The NOA was posted at the San Bernardino County Clerk's office and published in the *Inland Valley Daily Bulletin* on May 9, 2022. Additionally, copies of the Draft SEIR were made available for review at the City Hall and Ovitt Family Community Library as well as on the City's website.
- Preparation of a Final SEIR, including the responses to comments to the Draft SEIR. The Final SEIR was released for a 10-day agency review period prior to certification of the Final SEIR.
- Public hearings on the Proposed Project, including a Planning Commission study session, hearing, and a City Council hearing.

In summary, the City conducted all required noticing and scoping for the Proposed Project in accordance with Section 15083 of the CEQA Guidelines, and conducted the public review for the EIR, which exceeded the requirements of Section 15087 of the CEQA Guidelines.

B. FINAL ENVIRONMENTAL IMPACT REPORT AND CITY COUNCIL PROCEEDINGS

The City prepared a Final SEIR, including Responses to Comments to the Draft SEIR. The Final SEIR/Response to Comments contains comments on the Draft SEIR, responses to those comments, revisions to the Draft SEIR, and appended documents. A total of 45 comment letters were received. Of the 45 comment letters, 2 letters were from public agencies and/or tribes, and 43 letters were from residents and/or organizations.

The Final SEIR found that prior to mitigation, implementation of the Proposed Project will result in potentially significant impacts to Cultural Resources, Geology and Soils, and Tribal Cultural Resources (TCRs). However, mitigation measures have been developed to avoid or reduce all of these impacts to levels considered less than significant. The Final SEIR also found that despite the implementation of recommended mitigation measures, impacts to Air Quality, Cultural Resources, Noise, and Transportation were significant and unavoidable. A Statement of Overriding Considerations was prepared for the Council's consideration.

Members of the public can view searchable agendas for scheduled City Council meetings and access agenda-related City information and services directly on the following website: <https://www.ontario.ca.gov/Agendas/CityCouncil>.

The Final SEIR document will be posted for viewing and download with the previously posted Draft SEIR prior to the City's consideration of the Final SEIR and project recommendations on the City's website.

A date for consideration of the Final SEIR and project recommendations at the City Council was set for the Proposed Project and notice of the meeting was provided consistent with the Brown Act (Government Code Sections 54950 et seq.). The City Council will take testimony on the Proposed Project and may continue on its calendar to a subsequent meeting date in its discretion.

C. RECORD OF PROCEEDINGS

For purposes of CEQA and these Findings, the Record of Proceedings for the Proposed Project consists of the following documents and other evidence, at a minimum:

- The NOP, NOA, and all other public notices issued by the City in conjunction with the Proposed Project.
- The Draft SEIR and Final SEIR for the Proposed Project.
- All written comments submitted by agencies or members of the public during the public review comment period on the Draft SEIR.
- All responses to written comments submitted by agencies or members of the public during the public review comment period on the Draft SEIR.
- All written and verbal public testimony presented during a noticed public hearing for the Proposed Project.
- The Mitigation Monitoring and Reporting Program.
- The Statement of Overriding Considerations.
- The reports and technical memoranda included or referenced in the Final SEIR.
- All documents, studies, EIRs, or other materials incorporated by reference in the Draft SEIR and Final SEIR.
- The Resolutions adopted by the City in connection with the Proposed Project, and all documents incorporated by reference therein, including comments received after the close of the comment period and responses thereto.
- Matters of common knowledge to the City, including but not limited to federal, state, and local laws and regulations.

- Any documents expressly cited in these Findings.
- Any other relevant materials required to be in the record of proceedings by Public Resources Code Section 21167.6(e).

D. CUSTODIAN AND LOCATION OF RECORDS

The documents and other materials that constitute the administrative record for the City’s actions related to the Proposed Project are at the City of Ontario’s City Hall – 303 East B Street, Ontario, CA 91764. The City Planning Department is the custodian of the administrative record for the project. Copies of these documents, which constitute the record of proceedings, are and at all relevant times have been and will be available upon request at the offices of the Planning Department. This information is provided in compliance with Public Resources Code Section 21081.6(a)(2) and Guidelines Section 15091(e).

E. PROJECT DESCRIPTION

The Proposed Project, The Ontario Plan 2050 (TOP 2050), is an update to The Ontario Plan (TOP or Approved Project) to guide the City’s development and conservation for the next 30 years through 2050. The Proposed Project is a focused effort, with particular emphasis on technical refinements to the Policy Plan to comply with state housing mandates; conform with new state laws related to community health, environmental justice, climate adaptation, resiliency, and mobility; bring long-term growth and fiscal projections into alignment with current economic conditions; and advance the Tracking and Feedback system and Implementation Plan.

TOP is the City’s policy and implementation framework that guides the long-term growth and improvement of the Ontario community through six interrelated components of city governance:

A **Vision** that provides a sense of purpose and mission for city governance and sets the tone for the other components of TOP. The Vision’s central theme is a sustained, community-wide prosperity that continuously adds value and yields benefits.

A **Governance Manual** that establishes a set of goals and policies to promote consistent City leadership based on the principles of regional leadership, transparency, long-term value, accountability, and inclusivity.

A **Policy Plan** that serves as the City’s legally required general plan and that states long-term goals, principles, and policies to achieve Ontario’s Vision through nine elements: land use, housing, mobility, safety, environmental resources, parks and recreation, community economics, community design, and social resources.

A list of **City Council Priorities** that shape the City’s ongoing annual budgeting process, with a focus on a variety of short- and long-term goals and objectives.

An **Implementation Plan** that identifies the actions needed to carry out TOP’s policies. This includes initiatives by the City such establishing consistent land use zoning and creating objective development

and design standards, as well as decisions on public and private development projects and City activity programs.

A **Tracking and Feedback** system that charts the City’s progress toward achieving the Policy Plan goals, providing data and analysis that enables decision makers to make strategic course corrections in response to changing circumstances and monitor ongoing operational effectiveness.

TOP 2050 is an update to TOP to guide the City’s development and conservation for the next 30 years through 2050. The Proposed Project is a focused effort, with particular emphasis on technical refinements to the Policy Plan to comply with state housing mandates; conform with new state laws related to community health, environmental justice, climate adaption, resiliency, and mobility; bring long-term growth and fiscal projections into alignment with current economic conditions; and advance the Tracking and Feedback system and Implementation Plan. TOP 2050 fulfills the mandatory Regional Housing Needs Assessment (RHNA) obligation. TOP 2050 brings long-term growth and fiscal projections into alignment with current economic conditions as well as property owner and stakeholder requests, all in support of the vision for Ontario.

Table 1, *Comparison of Approved TOP to TOP 2050*, provides a statistical summary of the buildout potential of TOP 2050 compared to existing conditions and to the buildout potential under the currently approved TOP. As shown in this table, TOP 2050 would increase population, dwelling units, and nonresidential buildings but would result in a small decrease in employment. The decrease in employment at buildout is largely because of automation in the industrial sector, with large warehousing and logistics buildings expected to create fewer new jobs through 2050 than a similarly sized industrial building was expected to create when the current TOP was adopted in 2010.

Table 1 Comparison of Approved TOP to TOP 2050

Scenario	Units	Population	Nonresidential Square Feet	Employment
Existing 2021 Conditions ¹	52,466	179,597	156,065,382	131,999
Approved TOP	104,163	357,957	260,399,271	313,067
Proposed TOP	129,562	410,492	261,491,779	296,002
Net Difference (Proposed TOP -Approved TOP)	25,399	52,535	1,092,508	-17,065

Note:

¹ See Chapter 4, *Environmental Setting*, of the Draft SEIR for a summary of existing conditions.

F. PROJECT OBJECTIVES

Objectives for The Ontario Plan (TOP) 2050 will aid decision-makers in their review of the project and associated environmental impacts:

1. Provide a technical update to the current TOP that updates the goals and policies to enhance public safety and livability, align with updated economic forecasts, and comply with new state laws while maintaining the foundation, vision, and objectives of the current TOP.

2. Provide a streamlined, user-friendly, web-based TOP that is easily accessible to the public.
3. Designate the distribution, location, balance, and extent of land uses, including residential, retail, employment, open space, and public uses.
4. Link Ontario's community design goals to a broader context that includes economic development, land use, housing, community health, infrastructure, and transportation.
5. Improve the balance between jobs and housing in the San Bernardino County subregion to reduce vehicle miles traveled and associated air quality impacts, consistent with regional policies on jobs-housing balance.
6. Provide employment and housing opportunities for the San Bernardino Council subregion, consistent with the goals of the Southern California Association of Governments' Sustainable Communities Program.
7. Provide for high-intensity mixed-use urban centers along the I-10 corridor and in the Ontario Ranch that reduce vehicle trips and incorporate smart growth principles.
8. Foster the development of pedestrian and transit-oriented environments that create lively, appealing, and safe pedestrian areas, active during both daytime and evening hours.
9. Maintain Ontario's distinct neighborhoods and districts to foster a positive sense of identity and belonging among residents and businesses.
10. Establish a framework for using and managing the city's natural resources sustainably.
11. Provide for the security and safe transportation of goods and hazardous materials and maintain disaster preparedness and response and recovery systems to reduce loss of life, injury, private property damage, infrastructure damage, economic losses, and social dislocation.
12. Enhance the capacity for the people, businesses, and public agencies that are in or serve Ontario to be resilient in cases of severe and/or prolonged weather conditions, natural disasters, and emergencies.
13. Prioritize the improvement of areas most impacted by environmental justice issues, and enable Ontario residents to enjoy equal access to public facilities, civic engagement opportunities, nutritious foods, and safe and healthy environments.
14. Correlate the mobility system with the future land use patterns and buildout levels of Ontario and with other transportation planning efforts by local, state, and federal authorities.
15. Address a range of mobility options in Ontario, including vehicular, trucking, freight and passenger rail, air, pedestrian, bicycle, and transit.

III. CALIFORNIA ENVIRONMENTAL QUALITY ACT FINDINGS

A. INTRODUCTION

CEQA requires that a number of written findings be made by the lead agency in connection with certification of an environmental impact report (EIR) prior to approval of the project pursuant to Sections 15091 and 15093 of the CEQA Guidelines and Section 21081 of the Public Resources Code. This document provides the findings required by CEQA. The potential environmental effects of The Ontario Plan 2050 (Proposed Project) have been analyzed in a Draft SEIR (State Clearinghouse [SCH] 2021070364) dated May 2021. A Final SEIR (Final SEIR) has also been prepared that incorporates the Draft SEIR and contains comments received on the Draft SEIR, responses to the individual comments, revisions to the Draft SEIR including any clarifications based on the comments and the responses to the comments, and the Mitigation Monitoring and Reporting Program (MMRP) for the Proposed Project. This document provides the findings required by CEQA for approval of the Proposed Project.

Statutory Requirements for Findings

The CEQA (Pub. Res. Code Section 21000 et seq.) and the State CEQA Guidelines (Guidelines) (14 Ca. Code Regs Section 15000 et seq.) promulgated thereunder, require the environmental impacts of a project be examined before a project is approved. Specifically, regarding findings, Guidelines Section 15091 provides:

- (a) No public agency shall approve or carry out a project for which an EIR has been certified which identifies one or more significant environmental effects of the project unless the public agency makes one or more written findings for each of those significant effects, accompanied by a brief explanation of the rationale for each finding. The possible findings are:
 - 1. Changes or alterations have been required in, or incorporated into, the project which avoid or substantially lessen the significant environmental effect as identified in the final EIR.
 - 2. Such changes or alterations are within the responsibility and jurisdiction of another public agency and not the agency making the finding. Such changes have been adopted by such other agency or can and should be adopted by such other agency.
 - 3. Specific economic, legal, social, technological, or other considerations, including provision of employment opportunities for highly trained workers, make infeasible the mitigation measures or project alternatives identified in the final EIR.
- (b) The findings required by subsection (a) shall be supported by substantial evidence in the record.

- (c) The finding in subdivision (a)(2) shall not be made if the agency making the finding has concurrent jurisdiction with another agency to deal with identified feasible mitigation measures or alternatives. The finding in subsection (a)(3) shall describe the specific reasons for rejecting identified mitigation measures and project alternatives.
- (d) When making the findings required in subdivision (a)(1), the agency shall also adopt a program for reporting on or monitoring the changes which it has either required in the project or made a condition of approval to avoid or substantially lessen significant environmental effects. These measures must be fully enforceable through permit conditions, agreements, or other measures.
- (e) The public agency shall specify the location and custodian of the documents or other material which constitute the record of the proceedings upon which its decision is based.
- (f) A statement made pursuant to Section 15093 does not substitute for the findings required by this section.

The “changes or alterations” referred to in Section 15091(a)(1) above, that are required in, or incorporated into, the project which mitigate or avoid the significant environmental effects of the project, may include a wide variety of measures or actions as set forth in Guidelines Section 15370, including:

- (a) Avoiding the impact altogether by not taking a certain action or parts of an action.
- (b) Minimizing impacts by limiting the degree or magnitude of the action and its implementation.
- (c) Rectifying the impact by repairing, rehabilitating, or restoring the impacted environment.
- (d) Reducing or eliminating the impact over time by preservation and maintenance operations during the life of the action.
- (e) Compensating for the impact by replacing or providing substitute resources or environments, including through permanent protection of such resources in the form of conservation easements.

As indicated above, Section 21002 requires an agency to “avoid or substantially lessen” significant adverse environmental impacts. Thus, mitigation measures that “substantially lessen” significant environmental impacts, even if not completely avoided, satisfy section 21002’s mandate. (*Laurel Hills Homeowners Assn. v. City Council* (1978) 83 Cal.App.3d 515, 521 [“CEQA does not mandate the choice of the environmentally best feasible project if through the imposition of feasible mitigation measures alone the appropriate public agency has reduced environmental damage from a project to an acceptable

level”]; *Las Virgenes Homeowners Fed., Inc. v. County of Los Angeles* (1986) 177 Cal. App. 3d 300, 309 [“[t]here is no requirement that adverse impacts of a project be avoided completely or reduced to a level of insignificance . . . if such would render the project unfeasible”].)

While CEQA requires that lead agencies adopt feasible mitigation measures or alternatives to substantially lessen or avoid significant environmental impacts, an agency need not adopt infeasible mitigation measures or alternatives. (Pub. Resources Code, Section 21002.1(c) [if “economic, social, or other conditions make it infeasible to mitigate one or more significant effects on the environment of a project, the project may nonetheless be carried out or approved at the discretion of a public agency”]; see also State CEQA Guidelines, Section 15126.6(a) [an “EIR is not required to consider alternatives which are infeasible”].) CEQA defines “feasible” to mean “capable of being accomplished in a successful manner within a reasonable period of time, taking into account economic, environmental, social, and technological factors.” (Pub. Resources Code, Section 21061.1.) The State CEQA Guidelines add “legal” considerations as another indicia of feasibility. (State CEQA Guidelines, Section 15364.) Project objectives also inform the determination of “feasibility.” (*Jones v. U.C. Regents* (2010) 183 Cal. App. 4th 818, 828-829.) “[F]easibility’ under CEQA encompasses ‘desirability’ to the extent that desirability is based on a reasonable balancing of the relevant economic, environmental, social, and technological factors.” (*City of Del Mar v. City of San Diego* (1982) 133 Cal.App.3d 401, 417; see also *Sequoia Hills Homeowners Assn. v. City of Oakland* (1993) 23 Cal.App.4th 704, 715.) “Broader considerations of policy thus come into play when the decision making body is considering actual feasibility[.]” (*Cal. Native Plant Soc’y v. City of Santa Cruz* (2009) 177 Cal.App.4th 957, 1000 (“Native Plant”)); see also Pub. Resources Code, Section 21081(a)(3) [“economic, legal, social, technological, or other considerations” may justify rejecting mitigation and alternatives as infeasible] (emphasis added).)

Environmental impacts that are less than significant do not require the imposition of mitigation measures. (*Leonoff v. Monterey County Board of Supervisors* (1990) 222 Cal.App.3d 1337, 1347.)

The California Supreme Court has stated, “[t]he wisdom of approving . . . any development project, a delicate task which requires a balancing of interests, is necessarily left to the sound discretion of the local officials and their constituents who are responsible for such decisions. The law as we interpret and apply it simply requires that those decisions be informed, and therefore balanced.” (*Citizens of Goleta Valley v. Board of Supervisors* (1990) 52 Cal.3d 553, 576.) In addition, perfection in a project or a project’s environmental alternatives is not required; rather, the requirement is that sufficient information be produced “to permit a reasonable choice of alternatives so far as environmental aspects are concerned.” Outside agencies (including courts) are not to “impose unreasonable extremes or to interject [themselves] within the area of discretion as to the choice of the action to be taken.” (*Residents Ad Hoc Stadium Com. v. Board of Trustees* (1979) 89 Cal.App.3d 274, 287.)

Findings

Having received, reviewed, and considered the EIR for the No. 2021070364, as well as other information in the record of proceedings on this matter, the City of Ontario Council adopts the following Findings (Findings) in its capacity as the legislative body for the City of Ontario (City), which is the CEQA Lead Agency. The Findings set forth the environmental and other bases for current and

subsequent discretionary actions to be undertaken by the City and responsible agencies for the implementation of the Proposed Project.

In addition, the City of Ontario City Council (City Council) hereby make findings pursuant to and in accordance with Section 21081 of the California Public Resources Code and State CEQA Guidelines Sections 15090 and 15091 and hereby certifies that:

- (1) Changes or alterations have been required in, or incorporated into, the project which mitigate or avoid the significant environmental effect as identified in the final EIR.
- (2) Such changes or alterations are within the responsibility and jurisdiction of another public agency and not the agency making the finding. Such changes have been adopted by such other agency or can and should be adopted by such other agency.
- (3) Specific economic, legal, social, technological, or other considerations, including provision of employment opportunities for highly trained workers, make infeasible the mitigation measures or project alternatives identified in the final EIR.

Project Environmental Report and Discretionary Actions

The Final SEIR addresses the direct, indirect, and cumulative environmental effects of construction and operation activities associated with the Proposed Project. The Final SEIR provides the environmental information necessary for the City to make a final decision on the requested discretionary actions for all phases of this project. The Final SEIR was also intended to support discretionary reviews and decisions by other responsible agencies.

Discretionary actions to be considered by the City may include, but are not limited to, the following:

- Approve the project; adopt the MMRP, finding that the MMRP is adequately designed to ensure compliance with the mitigation measures during project implementation; and determine that the significant adverse effects of the project either have been reduced to an acceptable level, or are outweighed by the specific overriding considerations of the project as outlined in the CEQA Findings of Fact, as set forth herein.
- Approve the Proposed Project and related discretionary actions.

Format

Section 15091 of the CEQA Guidelines requires that a Lead Agency make a finding for each significant effect for the project. This section summarizes the significant environmental impacts of the project, describes how these impacts are to be mitigated, and discusses various alternatives to the Proposed Project, which were developed in an effort to reduce the remaining significant environmental

impacts. All impacts are considered potentially significant prior to mitigation unless otherwise stated in the findings.

This remainder of this section is divided into the following subsections:

Section III B, Findings Regarding Environmental Impacts Not Requiring Mitigation, presents topical areas that would result in no impact or less than significant impacts in the Draft SEIR.

Section III C, Findings on Significant Environmental Impacts that can be Reduced to Less Than Significant, presents significant impacts of the Proposed Project that were identified in the Final SEIR, the mitigation measures identified in the MMRP, and the rationales for the findings.

Section III D, Significant Unavoidable Impacts that cannot be Mitigated to Below the Level of Significance, presents significant unavoidable impacts of the Proposed Project that were identified in the Final SEIR, the mitigation measures identified in the MMRP, and the rationales for the findings.

Section III E, Cumulative Impacts, presents the summary of cumulative impacts of the Proposed Project.

Section IV, Alternatives to the Proposed Project, presents alternatives to the project and evaluates them in relation to the findings set forth in Section 15091(a)(3) of the State CEQA Guidelines, which allows a public agency to approve a project that would result in one or more significant environmental effects if the project alternatives are found to be infeasible because of specific economic, social, or other considerations.

Section V, Additional CEQA Considerations, presents additional CEQA considerations including significant irreversible changes due to the Proposed Project and growth inducing impacts of the Proposed Project.

Section VI, Findings on Responses to Comments on the Draft SEIR and Revisions to the Final SEIR, presents the City's findings on the response to comments and revisions to Final SEIR, and decision on whether a recirculated Draft SEIR is necessary or not.

Section VII, Statement of Overriding Considerations, presents a description of the Proposed Project's significant and unavoidable adverse impacts and the justification for adopting a statement of overriding consideration.

Section VIII, Mitigation Monitoring Reporting Program, presents the Mitigation Monitoring and Reporting Program.

Section IX, Certification, identifies the requirements for certification of the SEIR.

B. FINDINGS REGARDING ENVIRONMENTAL IMPACTS NOT REQUIRING MITIGATION

Issues Deemed No Impact Or Less Than Significant Impact

Pursuant to CEQA Guidelines Section 15060(d) and 15063 that allow a lead agency to skip preparation of an Initial Study and begin work directly on the EIR process. As such, here, an NOP was issued without an accompanying Initial Study.

Findings on “No Impact” and “Less Than Significant Impacts”

Based on the environmental assessments in the Final SEIR, the City determined that the Proposed Project would have no impact or less than significant impacts, including direct, indirect, and cumulative impacts, for the environmental issues summarized below. The rationale for the conclusion that no significant impact would occur in each of the issue areas is based on the environmental evaluation in the listed topical EIR sections in Chapter 5 of the Draft SEIR, which include Environmental Setting, Environmental Impacts, Cumulative Impacts, and Mitigation Measures.

The EIR concluded that all or some of the impacts of the Proposed Project with respect to the following issues either will not be significant or will be reduced to below a level of significance by implementing project design features or existing plans, programs, and policies as detailed in Chapter 5 of the Draft SEIR. Those issues include the following topical areas in their entirety or portions thereof: Aesthetics, Agriculture and Forestry Resources, Air Quality, Biological Resources, Cultural Resources, Energy, Greenhouse Gas (GHG) Emissions, Hazards and Hazardous Materials, Hydrology and Water Quality, Land Use and Planning, Mineral Resources, Noise, Population and Housing, Public Services, Recreation, Transportation, Tribal Cultural Resources, Utilities and Service Systems, and Wildfire. CEQA Guidelines Section 15901 requires that an EIR may not be certified for a project which has one or more significant environmental effects unless one of three possible findings is made for each significance effect. Since the following environmental issue areas were determined to have no impact or a less than significant impact, no findings for these issues are required.

1. Aesthetics

Impact 5.1-1: Implementation of TOP 2050 would not substantially alter scenic vistas in Ontario. [Threshold AE-1]

Scenic vistas generally provide visual access or panoramic views to a large geographic area. Panoramic views are usually associated with vantage points over a section of urban or natural areas that provide a geographic orientation not commonly available. Examples of scenic or panoramic views might include an urban skyline, valley, mountain range, large open space, the ocean, or other bodies of water. The San Gabriel Mountains are the most prominent scenic vista in or around Ontario.

The 2010 Certified EIR concluded that the Approved Project would not substantially alter scenic vistas in the City, as proposed growth is primarily concentrated in undeveloped areas interspersed in existing residential areas. Land use change as a result of new development under the Approved Project would alter the visual appearance of the Ontario Ranch from rural agriculture to low and low-medium density

residential land uses and office/industrial mixed uses. However, the scale and design of the City would not deter views of the San Gabriel Mountains, which are the dominant scenic resource in the City of Ontario. Regulations such as the City's Municipal Code and policies as part of the Approved Project would ensure that increased development would not impact scenic vistas. Additionally, development within the low-lying areas of Ontario would not have the potential to alter scenic views provided by the backdrop of the San Gabriel Mountains as the peaks rise to 7,000 feet above mean sea level.

The Proposed Project would increase the number of housing units and population in comparison to the Approved Project, as shown in Table 3-4, *Comparison of Approved TOP to TOP 2050*, in Chapter 3, *Project Description*, of the Draft SEIR. TOP 2050 has minor changes in land use and buildout projections throughout the City, and the majority of changes are concentrated in four growth areas and the Ontario Ranch. Increased development under TOP 2050 would occur within the city limits and already urbanized areas of the City. As described in Chapter 3, *Project Description*, these land use changes are intended to improve growth areas by encouraging the use of alternative forms of transportation, promoting healthier communities through land use planning that encourages walking and biking, promoting vibrant communities, putting residents in proximity to resources (i.e., jobs, grocery stores, retail), and aligning growth with planned infrastructure improvements and regional transportation goals. In addition, Policy CD-1.5 would ensure that major north-south streets would be designed and redeveloped to feature views of the San Gabriel Mountains.

Accordingly, the Proposed Project would not result in significant impacts to scenic views in comparison with the Approved Project. Similarly, the scale and design of the City under TOP 2050 would not deter views of the San Gabriel Mountains. The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to scenic resources within a state scenic highway. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.1-5)

Impact 5.1-2: Implementation of TOP 2050 would not alter scenic resources within a State scenic highway. [Threshold AE-2]

The Euclid Corridor and the Mission Boulevard Corridor are the primary scenic corridors in Ontario. These are not State-designated scenic highways, and Ontario does not have any State scenic highways through or in the vicinity of the City. The closest designated State scenic highway is a portion of State Route 142 in Chino Hills, approximately five miles west of the Ontario city limit. As such, the Proposed Project would have no impact on State scenic highways.

The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have no direct, indirect, and cumulative impacts relating to scenic resources within a state scenic highway. Accordingly, no changes or alterations to the Proposed

Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.1-6)

Impact 5.1-3: Implementation of TOP 2050 would not conflict with zoning or other regulations governing scenic quality. [Threshold AE-3]

The 2010 Certified EIR concluded that while buildout in accordance with the Approved Project would alter the visual appearance of the City, it would not substantially degrade the existing visual character or quality of the site and its surroundings.

An “urbanized area,” as defined by CEQA Section 21071, is an incorporated city that either has a population of at least 100,000 persons, or if the population of that city and not more than two contiguous incorporated cities combined equals at least 100,000 persons. As described in Chapter 3, *Project Description*, of the Draft SEIR, the population of Ontario was approximately 179,597 as of 2021 (see also Table 4-1, *City of Ontario Existing Land Use*, in Chapter 4, *Environmental Setting*, of the Draft SEIR). Therefore, this impact analysis addresses whether, for an urbanized area, the Proposed Project would conflict with zoning or other regulations governing scenic quality.

TOP is also the primary planning document for the City of Ontario. As described in Chapter 3, *Project Description*, the Proposed Project is a focused effort intended to comply with State housing mandates; conform with new State laws on community health, environmental justice, climate adaptation, resiliency, and mobility; bring long-term growth and fiscal projections into alignment with current economic conditions; and advance the Implementation Plan and Tracking and Feedback system. The majority of updates created through the Proposed Project weave refinements throughout the existing structure of the Policy Plan.

TOP 2050 includes goals and policies to ensure that new development would be compatible with the existing community (Policy LU-2.6) and would be of quality design (Policies CD-2.1 through CD-2.9). Additionally, the Community Design Element includes policies to ensure that the urban environment is appealing (Policies CD-3.2, CD-3.3, and CD-3.5) and to preserve the historic neighborhood character (Policy CD-4.2). Adherence to the Land Use Element and Community Design Element policies described above would reduce visual impacts.

Additionally, future development under the Proposed Project would still be required to adhere to the City’s Development Code, which includes general development requirements for development density, screening and setback, signing, landscaping, lighting, height limitations, and other aspects related to the aesthetic of the City. Finally, as described in Chapter 1, *Development Code Enactment and General Provisions*, of the City’s Development Code, the Development Code is enacted to assist implementation of planning, zoning, development, subdivision, and environmental laws and the TOP and to achieve the proper arrangement of land uses envisioned in the TOP. Because it is the overriding planning document for the City, and because it is intended to improve consistency with existing regulations and conditions, the Proposed Project, as TOP 2050, would not have a significant impact with respect to being inconsistent with policies or regulations governing scenic quality. As such, the Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to existing visual character and quality of public views and to conflict with applicable zoning and other regulations governing scenic quality. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.1-7)

Impact 5.1-4: Buildout of the Proposed Project would generate additional light and glare, which would be minimized through adherence to the City of Ontario Development Code. [Threshold AE-4]

The 2010 Certified EIR concluded that development of the Approved Project would result in new sources of light or glare but with adherence to the design standards of the City of Ontario Development Code, impacts were less than significant.

New development would generate new sources of light and glare through increased urbanization and densification of the city, affecting day or nighttime views. Sources of light include nighttime street and building illumination, security lighting, nighttime traffic, and lighting associated with construction activities. Lighting introduced to undeveloped and open space areas has the potential to impact visual quality of the nighttime sky.

Like the Approved Project, TOP 2050 would result in additional sources of light or glare, especially in the Ontario Ranch area. However, the City of Ontario Development Code contains standards addressing lighting through its design policies. Adherence to the design standards of the City of Ontario Development Code would ensure that light and glare from new developments would be minimized and that significant impacts would not occur. Compared to the Approved Project, the Proposed Project does not introduce substantial new sources of light and glare, and impacts would be less than significant. Therefore, the Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to light and glare. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.1-7)

2. Agriculture and Forestry Resources

Impact 5.2-1: The Proposed Project would not convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance in Ontario to nonagricultural use. [Threshold AG-1]

The 2010 Certified EIR concluded that buildout of the Approved Project would result in significant and unavoidable impacts to Prime Farmland, Unique Farmland, and Farmland of Statewide Importance because it converted all of the then-existing land under these categories to residential, commercial, mixed-use, and industrial land uses.

With implementation of the Approved Project, the City of Ontario no longer has land designated for agricultural use. Existing agricultural uses are still allowed to persist as non-conforming uses (see Policies ER-5.3 and ER-5.4). Additionally, the Approved Project re-designated agricultural land to nonagricultural land uses provided that equivalent Important Farmland is preserved elsewhere, or funds associated with the 1988 Park Bond Act are returned. Consequently, buildout of the Approved Project would ultimately result in the conversion of all existing Important Farmland within the City to nonagricultural uses.

Because the City of Ontario's land use plan no longer designates agricultural land uses in the City, and the current TOP is the baseline for this SEIR, the Proposed Project would not, itself, plan for the conversion of Prime Farmland, Unique Farmland, and Farmland of Statewide Importance to nonagricultural uses. Therefore, the Proposed Project would have no impact on land zoned for the purpose of agricultural uses. The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have no direct, indirect, and cumulative impacts relating to conversion of Farmland to nonagricultural use. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.2.12)

Impact 5.2-2: The Proposed Project would not conflict with existing zoning for agricultural use or a Williamson Act contract. [Threshold AG-2]

The 2010 Certified EIR concluded that the Approved Project would have a significant and unavoidable impact on a Williamson Act contract because the Approved Project would result in loss of agricultural use. There are two main categories for agricultural land under the City's zoning code, including Residential-Agriculture and Specific Plan Agriculture Preserve. Rural residential land use and Residential-Agriculture zoning allow low density housing and estates with some minimal agriculture use such as the keeping of chickens or horses; however, this zoning designation was not intended for large-scale farming/agricultural operations. Additionally, areas of Ontario Ranch are zoned as Specific Plan Agriculture Preserve under the Approved Project. The Agricultural Overlay Zone (or the Right to Farm Ordinance) requires that each Specific Plan address the appropriate transition of the area from agricultural uses to urban uses and include provisions for buffering between the proposed uses to protect agricultural and urban uses.

At the time of approval of the Approved Project, a number of Williamson Act contracts were designated for nonrenewal by the landowners and set to expire between 2009 and 2017. As shown in Figure 5.2-2, *Williamson Act Lands*, in Section 5.2, *Agriculture and Forestry Resources*, of the Draft SEIR, some contracts have already expired since the Approved Project was adopted, but a number of contracts are still active for a total of 719 acres of Williamson Act contract land in the City.¹ Any land held in a Williamson Act contract would have to be filed for nonrenewal, and the contract would have to be allowed to expire before any development occurs on it. Buildout of the Proposed Project, like the Approved Project, would most likely require the cancellation or nonrenewal of these contracts.

¹ There are 18.78 acres set to expire in 2022, 275.52 acres set to expire in 2027, and 93.40 acres set to expire in 2028.

However, because buildout of the Approved Project would have resulted in the cancellation or nonrenewal of Williamson Act contracts, the Proposed Project would not result in further impacts to Williamson Act lands. As such, impacts from the Proposed Project in this respect would be less than significant. The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to conflict with existing zoning for agricultural use or a Williamson Act contract. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.2.13)

Impact 5.2-3: The Proposed Project would not conflict with existing zoning for forest land, timberland, or timberland zoned Timberland Production, or result in the loss of forest land or conversion of forest land to nonforest use. [Thresholds AG-3 and AG-4]

At the time of the 2010 Certified EIR, impacts to forest land or timberland were not included in the CEQA Guidelines Appendix G checklist. Therefore, the 2010 Certified EIR did not identify any significant impacts related to forest or timberlands.

There are no land use designations or zoning for forest land, timberland, or timberland zoned Timberland Production in the City of Ontario. Consequently, the Proposed Project would not conflict with existing zoning for forest land, timberland, or timberland zoned Timberland Production.

Native habitats and vegetation communities are virtually absent throughout Ontario. Present plants in the Original Model Colony (OMC) primarily include turf, weeds, nonnative grasses, and nonnative trees and plants for landscaping, which have limited biological resource value. Low and medium residential and industrial uses make up the majority of land uses in Ontario Ranch, and (nonnative) vegetation communities primarily include surface water areas, flood control channel areas, agricultural fields, and developed areas. Therefore, there is no land in Ontario that would be considered forest land. Consequently, implementation of the Proposed Project would not result in loss or conversion of timberland to nonforest uses, and there would be no impact.

The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have no direct, indirect, and cumulative impacts relating to conflict with existing zoning for, or cause rezoning of, forestland, timberland, or timberland zoned Timberland Production and to loss of forestland or conversion to nonforest use. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.2.13)

Impact 5.2-4: The Proposed Project would not involve other changes that would result in conversion of Farmland to nonagricultural use or conversion of forest land to nonforest use. [Threshold AG-5]

The 2010 Certified EIR determined that conversion of agricultural uses in the City may cause farms and agricultural land uses outside the City to be converted to nonagricultural uses because of the nuisances related to agriculture and because of development pressures. When nonagricultural land uses are placed near agricultural uses, the odors, noises, and other hazards related to agriculture conflict with the activities and the quality of life of the people living and working in the surrounding areas. The 2010 Certified EIR identified that even though future development projects under the Approved Project would require environmental review in accordance with CEQA, including assessment of potential agricultural resources impacts, the development of the land in accordance with the Approved Project would create significant impacts on surrounding agricultural resources by encouraging its conversion.

The 2010 Certified EIR identified significant and unavoidable impacts to agricultural resources because buildout would result in all agricultural areas being converted to nonagricultural uses, and because buildout of the Approved Project would lead to cancellation or expiration of Williamson Act contracts. As discussed above, this could affect areas outside of the City as well. However, because former agriculture areas within Ontario are now already designated for nonagricultural uses and the current TOP is the baseline for this SEIR, the Proposed Project would not conflict with agricultural uses and would not result in conversion of farmland to nonagricultural use. Therefore, the Proposed Project would not result in significant impacts in this regard.

There is no forest land in Ontario, and therefore the Proposed Project would not result in conversion of forest land to nonforest use.

Consequently, the Proposed Project would not involve other changes in the existing environment, which, due to their location or nature, could result in conversion of Farmland to nonagricultural use or conversion of forest land to non-forest use, and impacts would be less than significant. The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to other changes that could result in conversion of Farmland to nonagricultural use or conversion of forest land to nonforest use. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.2.14)

3. Air Quality²

Impact 5.3-5: The Proposed Project would not result in other emissions (such as those leading to odors) adversely affecting a substantial number of people. [Threshold AQ-4]

The Certified EIR did not identify any significant odor impacts associated with the Approved Project. Growth within the City under TOP 2050 could generate new sources of odors. Nuisance odors from land uses in the South Coast Air Basin (SoCAB) are regulated under South Coast Air Quality Management District (AQMD) Rule 402, Nuisance, which states:

A person shall not discharge from any source whatsoever such quantities of air contaminants or other material which cause injury, detriment, nuisance, or annoyance to any considerable number of persons or to the public, or which endanger the comfort, repose, health or safety of any such persons or the public, or which cause, or have a natural tendency to cause injury or damage to business or property. The provisions of this rule shall not apply to odors emanating from agricultural operations necessary for the growing of crops or the raising of fowl or animals.

Industrial Land Uses

Buildout permitted under the Approved Project and the Proposed Project could include new sources of odors, such as compost facilities, landfills, solid-waste transfer stations, fiberglass manufacturing facilities, paint/coating operations (e.g., auto body shops), asphalt batch manufacturing plants, chemical manufacturing, and food manufacturing facilities. Similar to the Approved Project, areas where these types of uses could be developed under TOP 2050 would be generally limited to the areas designated Industrial (see Figure 3-5, *Proposed Land Use Plan*, in Chapter 3, *Project Description*, of the Draft SEIR). Future environmental review would be required for these types of industrial projects, which would ensure that sensitive land uses are not exposed to objectionable odors. Industrial land uses associated with TOP 2050 also would be required to comply with South Coast AQMD Rule 402. Therefore, impacts from potential odors generated from industrial land uses associated with TOP 2050 are considered less than significant. The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Residential and Other Land Uses

Like the Approved Project, residential and other nonresidential, nonindustrial land uses that would be accommodated by TOP 2050 could result in the generation of odors such as exhaust from landscaping equipment and from cooking. Unlike industrial land uses, these are not considered potential generators of odor that could affect a substantial number of people. Nuisance odors are regulated under South Coast AQMD Rule 402, which requires abatement of any nuisance generating a verified odor complaint. Therefore, impacts from potential odors generated from residential and other nonresidential land uses associated with TOP 2050 are considered less than significant. The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

² Impacts 5.3-1, 5.3-2, 5.3-3, and 5.3-4 are addressed in Section E, *Significant and Unavoidable Significant Impacts that Cannot be Mitigated to Below the Level of Significance*.

Construction

Like the Approved Project, during construction activities of development projects that would be accommodated by TOP 2050, construction equipment exhaust and application of asphalt and architectural coatings would temporarily generate odors. Any construction-related odor emissions would be temporary and intermittent. Noxious odors would be confined to the immediate vicinity of the construction equipment in use. By the time such emissions reached any sensitive receptor sites, they would be diluted to well below any level of air quality concern. Short-term construction-related odors are expected to cease upon the drying or hardening of odor-producing materials. Therefore, impacts associated with construction-generated odors are considered less than significant. The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to other emissions, such as those leading to odors. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.3.47)

4. Biological Resources

Impact 5.4-1: Compliance with existing regulations would ensure that implementation of TOP 2050 would not adversely affect sensitive species. [Threshold B-1]

The 2010 Certified EIR found that development in accordance with the Approved Project could result in the loss of sensitive species. However, the Approved Project did not have substantial adverse impacts on sensitive animal species after compliance with the requirements of the California Endangered Species Act (CESA) and Federal Endangered Species Act (FESA), including United States Fish and Wildlife Service (USFWS) requirements regarding critical habitat; mitigation fees paid by projects in Ontario Ranch; and acquisition and management of habitat using those fees.

Implementation of TOP 2050 would not directly result in removal of vegetation or wildlife in the City because the General Plan does not confer entitlements for development. Development in accordance with TOP 2050 could result in habitat modification and removal. Such development could also result in the introduction of nonnative species of weeds, insects, and domestic animals that could adversely impact sensitive species. Development projects considered for approval under TOP 2050 would be required to undergo independent CEQA review. Such projects would be required to comply with the federal and California Endangered Species Acts.

The following sensitive species have been observed in Ontario, and suitable habitat for each of these species is present in the City: great egret (*Ardea alba*), great blue heron (*Ardea Herodias*), snowy egret (*Egretta thula*), sharp-shinned hawk (*Accipiter striatus*), tricolored blackbird (*Agelaius tricolor*), double-crested cormorant (*Phalacrocorax auritus*), Cooper's hawk (*Accipiter cooperi*), burrowing owl, loggerhead shrike (*Lanius ludovicianus*), and long-billed curlew (*Numenius americanus*). Several additional species have been observed for which the City has suitable foraging habitat but limited or no suitable nesting habitat: ferruginous hawk, mountain plover (*Charadrius montanus*), northern harrier (*Circus cyaneus*), white-tailed kite (*Elanus leucurus*), merlin (*Falco columbarius*), prairie falcon (*Falco mexicanus*), peregrine falcon, and

white-faced ibis. Several sensitive bat species are considered to have possible roosting opportunities in the City and are listed in Table 5.4-2, *Sensitive Wildlife Species Known or With Potential to Exist in the City of Ontario*, in Section 5.4, *Biological Resources*, of the Draft SEIR.

No sensitive plant species have been observed in Ontario, and the only such species that are considered potentially present in the City have a low potential due to lack of suitable habitat. Therefore, implementation of TOP 2050 would not have substantial adverse impacts on sensitive plant species.

Nearly the entire City is developed with urban and agricultural uses, and there is very little native habitat remaining. Vacant land in the City may have low habitat value, however, because much of it is barren ground and does not support vegetation, and because many areas of vacant land are small, surrounded by developed urban uses, and isolated from other vacant land. There is nonetheless a chance that some sensitive species occur in remnant or disturbed habitats, and focused surveys may be warranted for individual sites that are the subject of development proposals. The assessment of the need for focused surveys would be carried out on a project-by-project basis in accordance with existing federal, state, and local regulations. This would apply equally to the OMC and Ontario Ranch.

Most potential biological resources in the City are in Ontario Ranch because the rest of the City is almost entirely built out. Some of the parts of Ontario Ranch that were previously used as dairies have undergone surveys for Delhi Sands Flower-Loving Fly (DSFLF), and the USFWS has determined that the likelihood of occupancy by DSFLF in these areas is low enough that further surveys would not be required; however, project applicants would need to consult with the USFWS on a case-by-case basis to determine survey requirements.

Parts of the closed Milliken Waste Disposal Site in the OMC are considered suitable for preservation or enhancement as burrowing owl habitat.

The settlement agreement for the City of Ontario Sphere of Influence General Plan Amendment governs mitigation for biological resources impacts in Ontario Ranch associated with potential impacts to the burrowing owl, the DSFLF, raptor foraging and wildlife habitat, loss of open space, actual and potential habitat and agricultural land, and sensitive species (listed and nonlisted). The terms of the settlement agreement were discussed in the “Local Regulations” section under “City of Ontario Sphere of Influence General Plan Amendment, Final EIR, and Settlement Agreement.”

TOP 2050 includes policies to ensure that special-status species and habitat are protected through compliance with state and federal regulations (e.g., Policies ER-5.1 and ER-5.2). Projects under TOP 2050 that undergo independent CEQA review would be required to determine whether there is potential habitat on-site for sensitive species. If potential habitat were found on-site, focused surveys for those sensitive species potentially present would be required. If sensitive species were found, the project proponent would be required to consult with the CDFW regarding impacts to sensitive species and ensuing mitigation. Mitigation for impacts to sensitive species is often in the form of acquisition or restoration of habitat, on-site or off-site, at a ratio to the area of impacted land that would be determined by the CDFW or USFWS. For projects that are sited within critical habitat for a listed species and are proposed by federal agencies or involve federal permits or funding, the project proponent would be required under the FESA to consult with the USFWS regarding impacts and mitigation. Projects in Ontario Ranch would pay a mitigation fee that would be deposited into a trust

fund to be used for the acquisition, restoration, rehabilitation, and maintenance of lands deemed to have long-term conservation value.

TOP 2050 would have similar biological resources impacts as the current Approved TOP. This is because while the Proposed Project would increase land use intensity, TOP 2050 would not result in development of new, previously undeveloped areas of the City. Compliance with the requirements of the California and Federal Endangered Species Acts, including requirements of the USFWS regarding critical habitat; mitigation fees paid by projects in Ontario Ranch; and acquisition and management of habitat using those fees would reduce impacts on sensitive animal species from implementation of TOP 2050.

The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to special-status species compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to candidate, sensitive, or special status species. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.4.30)

Impact 5.4-2: Compliance with existing regulations would ensure that implementation of TOP 2050 would not have an adverse impact on riparian or sensitive habitats. [Threshold B-2]

Ontario is almost completely developed with urban and agricultural uses, with no large open areas of native habitat. Available open space consists of agricultural fields, parks and golf courses, and scattered vacant lots. The 2010 Certified EIR found that the Approved Project would not have substantial adverse impacts to surface water areas, or to riparian or aquatic vegetation in surface water areas or flood control channels. Detention basins would be designated Open Space–Non-recreation or Open Space–Parkland. Projects affecting riparian habitat would be required to mitigate potential impacts to riparian areas through the existing permitting process.

Surface water areas are assumed to contain sensitive natural communities if they support plants such as mulefat and willow, which also occur in sensitive communities listed in the California Natural Diversity Database as occurring in the region. Surface water areas in the City include detention basins and other man-made lakes, such as those in golf courses, as well as dairy manure water retention basins, irrigation ponds, and livestock watering ponds associated with agricultural uses in Ontario Ranch.

Detention basins would be designated Open Space–Non-recreation, except some of the basins in Cucamonga-Guasti Regional Park would be designated Open Space–Parkland. The basins would not be developed with other land uses.

Implementation of TOP 2050 would not result in direct vegetation removal in surface water areas in the City; however, projects approved pursuant to TOP 2050 could indirectly result in such removal. Projects that would result in impacts to surface water areas determined to be jurisdictional to the state would require CDFW approval pursuant to the Fish and Game Code (Section 1600 et. seq.) in the

form of Streambed Alteration Agreements. Such impacts would require mitigation, also subject to CDFW approval.

Policy ER-5.1 would support avoidance of adverse impacts to protected wetlands, waters of the United States, and waters of the State.

Compared to the Approved Project, TOP 2050 would have similar impacts to sensitive habitat because it would not result in development of new, previously undeveloped areas of the City even though it would result in an increase in land use intensity. Individual projects undergoing environmental review under CEQA would be required to determine whether there is potential habitat on-site for sensitive species. If sensitive species were found on-site, the project proponent would be required to consult with the CDFW regarding impacts to sensitive species and ensuing mitigation. Projects in Ontario Ranch would pay a mitigation fee that would be deposited into a trust fund to be used for the acquisition, restoration, rehabilitation, and maintenance of lands deemed to have long-term conservation value. In conclusion, projects affecting riparian habitat in the City would be required to mitigate potential impacts to riparian areas through the existing permitting process.

The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to riparian habitat or sensitive natural communities compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to riparian habitat or sensitive natural community. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.4.31)

Impact 5.4-3: Compliance with existing regulations would ensure that implementation of TOP 2050 would not have an adverse impact on jurisdictional waters. [Threshold B-3]

The 2010 Certified EIR found that the Approved Project would not have substantial adverse impacts on jurisdictional waters. Flood control channels and detention basins would be designated Open Space–Non-recreation or Open Space–Parkland. Projects that have the potential to result in impacts to waters of the state would be subject to approval by CDFW; United States Army Corps of Engineers (USACE); require a Section 404 permit under the Clean Water Act (CWA) or consultation with Environmental Protection Agency (EPA) for a Section 7 take permit, as applicable; and require mitigation in accordance with the applicable permits.

The Cucamonga Creek and Deer Creek channels and portions of the Lower Deer Creek, Day Creek, Etiwanda Creek, and West Cucamonga Creek channels, are owned and maintained by San Bernardino County; they are not subject to land use controls by the City of Ontario and would not be affected by TOP 2050. Remaining segments of the Lower Deer Creek, Day Creek, Etiwanda Creek, and West Cucamonga Creek channels in the City that are owned by the City of Ontario, would be designated Open Space–Non-recreation by TOP 2050 and would not be developed with other land uses. The Cucamonga, Ely, Wineville, and Chris detention basins are also owned and maintained by the County and would not be affected by TOP 2050.

Ontario Ranch contains dairy manure water retention basins, irrigation ponds, livestock watering, and man-made lakes. In addition, fields under cultivation or left fallow accumulate surface water in ponds or ditches. The CDFW may have jurisdiction over these water bodies, but they are not expected to come under USACE jurisdiction. Implementation of TOP 2050 would not result in direct impacts to waters of the State because TOP 2050 does not grant specific entitlements for development. Tributaries to any channels in the city, plus areas that are fed by surface waters, are considered waters of the State and are jurisdictional to CDFW. Projects resulting in impacts to waters of the State would be subject to approval by CDFW through Streambed Alteration Agreements and would require mitigation as determined by the CDFW for any consequent impacts.

Individual projects undergoing environmental review under CEQA would be required to determine whether there is potential habitat on-site for sensitive species. If sensitive species were found, the project proponent would be required to consult with the CDFW regarding impacts to sensitive species and ensuing mitigation. Projects in Ontario Ranch would pay a mitigation fee that would be deposited into a trust fund to be used for the acquisition, restoration, rehabilitation, and maintenance of lands deemed to have long-term conservation value.

Compared to the Approved Project, TOP 2050 would have similar impacts to jurisdictional waters. This is because the Proposed Project would result in an increase in land use intensity but would not result in development of new, previously undeveloped areas of the City. In conclusion, because projects that have the potential to result in impact to waters of the State would be subject to approval by CDFW and USACE, require a Section 404 permit under the CWA or consultation with the EPA for a Section 7 take permit, and mitigation would be required in accordance with the applicable permits, impacts to jurisdictional waters in the City associated with TOP 2050 would be less than significant.

The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to jurisdictional waters compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to state or federally protected wetlands. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.4.33)

Impact 5.4-4: Implementation of TOP 2050 would not adversely affect wildlife movement. [Threshold B-4]

The 2010 Certified EIR found that no regional wildlife movement corridors have been identified in the City. Therefore, the Approved Project did not result in substantial adverse effects to wildlife movement.

No regional wildlife movement corridors have been identified in the City, most of which is ill suited for the purposes of wildlife movement. The flood control channels and the Southern California Edison (SCE) corridors could serve as local corridors for movement within the City and between the San Gabriel Mountains to the north and the Prado Basin to the south. The segments of flood control channels in the City would be designated Open Space–Non-recreation under TOP 2050 and would

not be developed with other land uses. The SCE corridors would also be designated Open Space–Non-recreation. Therefore, implementation of TOP 2050 is not anticipated to substantially impair the use of flood control channels or SCE corridors in the City as wildlife movement corridors.

There are trees and shrubs scattered throughout the City that may be used for nesting or roosting by migrating birds. TOP 2050 would not grant specific entitlements for development; therefore, implementation of TOP 2050 would not directly impact vegetation that could be used by migrating birds. Such projects would be required to comply with the federal Migratory Bird Treaty Act. Therefore, TOP 2050 is not anticipated to have substantial adverse impacts to migratory birds. Furthermore, Policy ER-5.1 would encourage efforts to conserve flood control channels and transmission line corridors as wildlife movement corridors. Consequently, impacts would be less than significant.

Compared to the Approved Project, TOP 2050 would have similar impacts associated with wildlife movement corridors. Though the Proposed Project would increase land use intensity, it would not result in development of new, previously undeveloped areas of the City. Like the Approved Project, TOP 2050 would not result interfere with wildlife movement in a wildlife corridor.

The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to wildlife movement compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridor, and to the impediment of use of native wildlife nursery sites. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.4.33)

Impact 5.4-5: Development in accordance with TOP 2050 would require compliance with the requirements of the Delhi Sands Flower-Loving Fly Ontario Recovery Unit. [Thresholds B-5 and B-6]

The 2010 Certified EIR found that the Approved Project would not conflict with the requirements of the DSFLF Ontario Recovery Unit or critical habitat for the San Bernardino kangaroo rat. Since 2008, the critical habitat for the San Bernardino kangaroo rat is no longer in Ontario, so it is not evaluated in this discussion.

The Ontario Recovery Unit for the DSFLF includes 21.7 square miles of Ontario, mostly in the eastern and southwestern portions of the City, including portions of Ontario Ranch. Projects proposed within the Ontario Recovery Unit would be required to conduct focused surveys for DSFLF on the project site and consult with the USFWS regarding mitigation of impacts on any DSFLF found, pursuant to Section 7 of the FESA. In some of the parts of Ontario Ranch that were previously used as dairies, the USFWS has concluded from the findings of previous focused surveys that DSFLS is very unlikely to occur; therefore, no focused surveys for DSFLF areas are required in these areas. Projects proposed pursuant to TOP 2050 would need to ascertain requirements for focused surveys for DSFLF from the USFWS on a case-by-case basis.

There is one habitat conservation plan (HCP) in the City, a 19-acre area near the intersection of Greystone Drive and the eastern city boundary established to protect the DSFLF. The HCP area would remain designated Industrial under the Proposed Project. Any project proposed for development within this HCP pursuant to TOP 2050 would be required to consult with the USFWS regarding project impacts on DSFLF and mitigation of any such impacts. Therefore, TOP 2050 would comply with this HCP.

TOP 2050 would not grant specific entitlements for development and would not conflict with FESA requirements and USFWS regulations regarding critical habitat. Furthermore, Policy ER-5.1 of TOP 2050 would support efforts to conserve high-quality habitat for the DSFLF. Individual projects undergoing environmental review under CEQA would be required to determine whether there is a potential for habitat on-site for sensitive species. If sensitive species were found on-site, the project proponent would be required to consult with the CDFW regarding impacts to sensitive species and ensuing mitigation. Projects in Ontario Ranch would pay a mitigation fee that would be deposited into a trust fund to be used for the acquisition, restoration, rehabilitation, and maintenance of lands deemed to have long-term conservation value.

Compared to the Approved Project, TOP 2050 would have similar impacts regarding consistency with a habitat conservation plan. This is because the Proposed Project would increase land use intensity but would not result in development of new, previously undeveloped areas of the City. Like the Approved Project, TOP 2050 would not conflict with the DSFLF HCP.

The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to habitat conservation plan compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to conflict with local policies or ordinances protecting biological resources and to adopted habitat conservation plans or natural community conservation plans. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.4.34)

5. Cultural Resources³

Impact 5.5-3: Grading activities would not adversely impact human remains, if accidentally uncovered, because procedures are required under the Public Resources Code and California Health and Safety code. [Threshold C-3]

The 2010 Certified EIR found that grading activities in Ontario would comply with PRC Section 5079.98 so as not to disturb human remains.

There are known Native American gravesites and cemeteries in the City, including Bellevue Memorial Park on the north side of G Street, between Benson Avenue and Mountain Avenue. TOP 2050 in itself does not involve grading activities and would not directly disturb any human remains. However, long-

³ Impacts 5.5-1 is addressed in Section E, *Significant and Unavoidable Significant Impacts that Cannot be Mitigated to Below the Level of Significance*. Impact 5.5-2 is addressed in Section D, *Findings on Significant Environmental Impacts that Can be Reduced to a Less than Significant Level*.

term implementation of TOP 2050 would allow development and redevelopment, including grading, of sensitive areas, possibly disturbing human remains, including those outside of formal cemeteries.

California Health and Safety Code, Section 7050.5; CEQA Section 15064.5; and PRC Section 5097.98 mandate the process to be followed in the event of an accidental discovery of any human remains in a location other than a dedicated cemetery. Specifically, the California Health and Safety Code requires that if human remains are discovered on a project site, disturbance of the site shall remain halted until the coroner has conducted an investigation into the circumstances, manner, and cause of any death, and the recommendations concerning the treatment and disposition of the human remains have been made to the person responsible for the excavation, or to his or her authorized representative, in the manner provided in PRC Section 5097.98. If the coroner determines that the remains are not subject to his or her authority and if the coroner recognizes or has reason to believe the human remains to be those of a Native American, he or she shall contact, by telephone within 24 hours, the Native American Heritage Commission. Although soil-disturbing activities associated with development in accordance with TOP 2050 could result in the discovery of human remains, compliance with existing law would ensure that significant impacts to human remains would not occur.

The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to human remains compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to human remains. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.5.20)

6. Energy

Impact 5.6-1: TOP 2050 would not result in potentially significant environmental impacts due to wasteful, inefficient, or unnecessary consumption of energy resources during project construction or operation. [Threshold E-1]

The 2010 Certified EIR assessed the energy demand for electricity and gas services and concluded that the Approved Project would result in a less than significant impact to additional demand for electrical and gas services. The 2010 Certified EIR did not identify impacts associated with wasteful, inefficient, or unnecessary consumption of energy resources.

Short-Term Construction Impacts

Development projects constructed under the Proposed Project would create temporary demands for electricity. Natural gas is not generally required to power construction equipment, and therefore is not anticipated during construction phases. Electricity use would fluctuate according to the phase of construction. Additionally, it is anticipated that most electric-powered construction equipment would be hand tools (e.g., power drills, table saws, compressors) and lighting, which would result in minimal electricity usage during construction activities.

Development projects would also temporarily increase demands for energy associated with transportation. Transportation energy use depends on the type and number of trips, vehicle miles traveled (VMT), fuel efficiency of vehicles, and travel mode. Energy use during construction would come from the transport and use of construction equipment, delivery vehicles and haul trucks, and construction employee vehicles that would use diesel fuel or gasoline. The use of energy resources by these vehicles would fluctuate according to the phase of construction and would be temporary. It is anticipated that most off-road construction equipment, such as those used during demolition and grading, would be gas or diesel powered. In addition, all operation of construction equipment would cease upon completion of project construction.

Furthermore, the construction contractors would be required to minimize nonessential idling of construction equipment during construction in accordance with the California Code of Regulations Title 13, Article 4.8, Chapter 9, Section 2449. Such required practices would limit wasteful and unnecessary energy consumption. Moreover, future development projects accommodated under TOP 2050 would be similar to projects currently in development in Ontario. The types of land uses accommodated under TOP 2050 would also be similar to the land uses accommodated under the Approved Project. Thus, the construction processes for future development projects accommodated under the Proposed Project would be similar to the construction processes of current development projects and projects accommodated under the Approved Project.

TOP 2050 would not result in wasteful, inefficient, or unnecessary consumption of fuel use during construction. The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to that of the Approved Project.

Long-Term Impacts During Operation

Operation of new development projects accommodated under the Proposed Project would create additional demands for electricity and natural gas compared to existing conditions. Operational use of electricity and natural gas would include heating, cooling, and ventilation of buildings; water heating; operation of electrical systems; use of on-site equipment and appliances; and lighting.

Nontransportation Energy

Electrical service to the City is provided by SCE through connections to existing off-site electrical lines and new on-site infrastructure. As shown in Table 5.6-4, *Year 2050 Forecast Electricity Consumption*, in Section 5.6, *Energy*, of the Draft SEIR, by horizon year 2050, electricity use in the City would decrease by 32,244,780 kilowatt-hours per year, or approximately 9 percent, from existing conditions.

As shown in Table 5.6-5, *Year 2050 Forecast Natural Gas Consumption*, in Section 5.6, *Energy*, natural gas use under the Approved Project totals 100,249,150 therms annually. By 2050, natural gas use in the City would increase by 3,624,970 therms annually, or approximately 4 percent, from existing conditions.

While the electricity demand would decrease and natural gas demand would increase for the City compared to existing conditions, developments accommodated under the Proposed Project would be required to comply with the current and future updates to the Building Energy Efficiency Standards and CALGreen, which would contribute in reducing the energy demands shown in Tables 5.6-4 and

5.6-5. New and replacement buildings in compliance with these standards would generally have greater energy efficiency than existing buildings. It is anticipated that each update to the Building Energy Efficiency Standards and CALGreen would result in greater building energy efficiency and move closer to buildings achieving zero net energy.

In addition to the Building Energy Efficiency Standards and CALGreen, TOP 2050 includes the goals and policies to increase energy efficiency and reduce wasteful, inefficient use of energy resources. The Environmental Resources Element policies focus on coordinating with agencies to pursue energy-efficient goals and strategies, promoting energy-efficient development patterns and site designs, and expanding renewable energy strategies (Environmental Resources Element policies ER-3.2, ER-3.3, ER-3.4, and ER-3.6). Policies ER-3.2 and ER-3.4 would require the best practices identified in green community rating systems to guide development in new communities and promote renewable energy sources for public- and private-sector development. Policy ER-3.3 would require energy-efficient building and site design strategies for future development projects. Policy ER-3.4 would require all new and substantially renovated City buildings of 10,000 square feet and greater to achieve Leadership in Energy and Environmental Design (LEED) Silver Certification standard. Encouraging sustainable and energy-efficient building practices and using more renewable energy strategies will further reduce energy consumption in the City and move closer to achieving zero net energy.

Transportation Energy

The growth accommodated under TOP 2050 would consume transportation energy from the use of motor vehicles (e.g., gasoline, diesel, compressed natural gas, and electricity). Table 5.6-6, *Operation-Related Annual Fuel Usage*, in Section 5.6, *Energy*, shows the net change in VMT, fuel usage, and fuel efficiency of the Proposed Project compared to the Approved Project.

As shown in Table 5.6-6, when compared to the Approved Project, the Proposed Project would result in an overall increase in VMT and fuel usage for gasoline-, diesel-, compressed natural gas- and electricity-powered vehicles. Therefore, the Proposed Project would result in an increase in annual VMT and fuel usage for all vehicles, primarily due to the increase in projected population growth. Fuel efficiency will be the same as the Approved Project, and implementation of the Proposed Project would not result in less efficiency in transportation fuel usage.

The overall VMT shown in the table would be primarily attributable to the increase in population compared to the Approved Project. Although fuel efficiency between the Proposed Project and the Approved Project would be the same, the VMT per service personnel rate (VMT/SP) decreases under the Proposed Project (see Section 5.17, *Transportation*, Table 5.17-4, *VMT Comparison of TOP 2050 to the Approved TOP*, criterion 1, of the Draft SEIR). A decrease in VMT/SP indicates fewer vehicle trips and shorter trip distances with the growing service population, which accounts for residents and employees who lives and/or works in Ontario. This could be caused by multiple factors, such as better jobs-housing ratio, implementation of more public transit options in the City, and amenities closer to where residents live.

Although VMT associated with electric vehicles and thus electricity usage would increase under the with-project horizon year 2050 scenario when compared to existing Approved Project, it is also anticipated that electric vehicles will improve in energy efficiency. In conjunction with the regulatory

(i.e., Renewables Portfolio Standard [RPS], Senate Bill [SB] 350, and SB 100) and general trend toward increasing the supply and production of energy from renewable sources, it is anticipated that a greater share of electricity used to power electric vehicles will be from renewable sources in future years (e.g., individual photovoltaic systems, purchased electricity from a community choice aggregation, and/or purchased electricity from SCE that is generated from renewable sources).

In addition to regulatory compliance that would contribute to more fuel-efficient vehicles and less demand in fuels, the Proposed Project includes policies that will contribute to minimizing overall VMT, and thus fuel usage associated with the City. These proposed policies focus on minimizing VMT through land use and transportation planning efforts that work in combination. TOP 2050 includes Mobility Element policy M-3.3 and Land Use Element policies LU-1.2, LU-1.4, LU-1.5, and LU-1.6. These policies focus on situating residential development near commercial land uses to promote public transit use. Placing residential and nonresidential uses near each other to create self-sustaining communities and neighborhoods and offering mixed-used developments could result in shorter distances traveled between where people work and live and to amenities. The shorter distances reduce VMT by reducing the average vehicle trip distance traveled. It also encourages people to forego vehicle travel altogether and either bike, walk, or take public transportation, which would also contribute to minimizing VMT.

Furthermore, proposed policies under TOP 2050 include improving public transportation and active transit (e.g., biking and walking) infrastructure in the City (e.g., Mobility Element policies M-2.1, M-2.2, M-2.3, and M-2.4; Community Design Element policy CD-3.2). Improving the public transportation and active transit infrastructure in conjunction with creating more self-sustaining neighborhoods would encourage less travel by single-occupancy-passenger vehicle, which would further contribute to minimizing VMT. Moreover, TOP 2050 Environmental Resources Element policy ER-3.5 focuses on increasing the use of clean fuel and electric vehicles by purchasing more fuel-efficient alternative energy vehicles.

Summary

Overall, regulatory compliance (e.g., Building Energy Efficiency Standards, CALGreen, RPS, and Corporate Average Fuel Economy [CAFE] standards) will increase building energy efficiency and vehicle fuel efficiency and reduce building energy demand and transportation-related fuel usage. Additionally, the Proposed Project includes policies related to land use and transportation planning and design, energy efficiency, public and active transit, and renewable energy generation that will contribute to minimizing building and transportation-related energy demands overall and demands on nonrenewable sources of energy. Implementation of proposed policies under TOP 2050 and Community Climate Action Plan (CCAP) in conjunction with regulatory requirements would ensure that energy demand associated with growth under TOP 2050 would not be inefficient, wasteful, or unnecessary. Therefore, the Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to wasteful, inefficient, or unnecessary consumption of energy resources. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.6.13)

Impact 5.6-2: Implementation of TOP 2050 would not conflict with or obstruct a state or local plan for renewable energy or energy efficiency. [Threshold E-2]

The 2010 Certified EIR did not identify impacts related to consistency with plans for renewable energy or energy efficiency because this was not a threshold in the CEQA Guidelines Appendix G checklist at the time. Applicable plans relevant to the Proposed Project include the California RPS Program.

The state's electricity grid is transitioning to renewable energy under California's RPS Program. Renewable sources of electricity include wind, small hydropower, solar, geothermal, biomass, and biogas. In general, California has RPS requirements of 33 percent renewable energy by 2020 (SB X1-2), 40 percent by 2024 (SB 350), 50 percent by 2026 (SB 100), 60 percent by 2030 (SB 100), and 100 percent by 2045 (SB 100). SB 100 also establishes RPS requirements for publicly owned utilities that consist of 44 percent renewable energy by 2024, 52 percent by 2027, and 60 percent by 2030. The statewide RPS requirements do not directly apply to individual development projects, but to utilities and energy providers such as SCE, whose compliance with RPS requirements would contribute to the State of California objective of transitioning to renewable energy. The land uses accommodated under the Proposed Project would comply with the current and future iterations of the Building Energy Efficiency Standards and CALGreen.

Furthermore, TOP 2050 includes Environmental Resources Element policies ER-3.1, ER-3.2, ER-3.3, ER-3.4, ER-3.5, and ER-3.6 and Safety Element policies S-9.1, S-9.2, and S-9.3, which would support the statewide goal of transitioning the electricity grid to renewable sources and employ best practices regarding energy-saving standards. Therefore, implementation of TOP 2050 would not conflict with or obstruct implementation of California's RPS program. The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to conflict with or obstruction of a state or local plan for renewable energy or energy efficiency. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.6.14)

7. Geology and Soils⁴

Impact 5.7-1: Development of TOP 2050 would adhere to the California Building Code to ensure residents, employees, or visitors in Ontario would not be adversely affected by potential seismic-related hazards. [Threshold G-1]

The 2010 Certified EIR found that policies of the Approved Project, and state regulations would ensure that the potential impacts from seismic-related hazards would be less than significant.

⁴ Impact 5.7-6 is addressed in Section D, *Findings on Significant Environmental Impacts that Can be Reduced to a Less than Significant Level*.

Earthquakes

The Upper Santa Ana River Valley and vicinity contain a number of known earthquake faults, which are described above in Table 5.7-1, *Estimated Maximum Earthquake Magnitude and Associated Peak Ground Acceleration for Faults in and Near Ontario*, and shown on Figure 5.7-2, *Regional Faults and Fault Zones*, in Section 5.7, *Geology and Soils*, of the Draft SEIR. The City of Ontario is not within any Alquist-Priolo Earthquake Fault Zone. Of the faults listed, the southern section of the San Andreas Fault is estimated to be capable of generating the greatest magnitude earthquake, 8.0. The most intense peak horizontal ground acceleration that any of these faults is estimated to be capable of generating in Ontario is approximately 0.54 g by the Chino Fault, which passes approximately four miles from the southwestern City boundary. Projects considered for approval under TOP 2050 would be required to comply with seismic safety provisions of the California Building Code (CBC; Title 24, Part 2 of the California Code of Regulations). Such compliance would reduce hazards arising from ground shaking to less than significant.

Liquefaction

Based on the groundwater levels throughout the City being greater than 50 feet below ground surface, there is currently no potential for liquefaction.

Seismically Induced Settlement

The entire Ontario area is underlain by young, unconsolidated alluvial deposits and artificial fill that may be susceptible to seismically induced settlement (see Figure 5.7-1, *Geologic Map*, in Section 5.7, *Geology and Soils*, of the Draft SEIR). Implementation of TOP 2050 could indirectly increase the numbers of persons and structures in the City that could be subjected to earthquake-related hazards. Projects developed pursuant to TOP 2050 would be required to meet the most current seismic safety requirements in the CBC. Chapter 16 of the CBC contains requirements for design and construction of structures to resist loads, including earthquake loads. Chapter 18 contains requirements for excavation, grading, and fill; load-bearing values of soils; and foundations, footings, and piles. Compliance with those requirements would ensure that there would not be substantial impacts related to ground shaking, liquefaction, or seismic settlement. Furthermore, TOP 2050 includes the following policies regarding seismic-related hazards.

- **S-1.1: Implementation of Regulations and Standards.** We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.
- **S-1.2: Entitlement and Permitting Process.** We follow state guidelines and the California Building Code to determine when development proposals must conduct geotechnical and geological investigations.
- **S-1.3: Continual Update of Technical Information.** We maintain up-to-date California Geological Survey seismic hazard maps.

- **S-1.4: Seismically Vulnerable Structures.** We conform to state law regarding unreinforced masonry structures and coordinate with not-for-profits to facilitate seismic retrofits in environmental justice areas and for low-income households.

TOP 2050 would have similar seismic hazards as the current TOP. This is because while the Proposed Project would result in an increase in land use intensity, TOP 2050 would not result in development of new, previously undeveloped areas of the City. After compliance with the safety provisions of the CBC, implementation of TOP 2050 would have less-than-significant impacts from seismic hazards. The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to geology and soils compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to seismic-related hazards. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.7.19)

Impact 5.7-2: Implementation of TOP 2050 would not result in substantial soil erosion or the loss of topsoil. [Threshold G-2]

The 2010 Certified EIR found that policies of the Approved Project and state regulations would ensure that the potential impacts from erosion or the loss of topsoil would be less than significant.

Erosion

The young alluvial sediment and wind-blown sand underlying the City are generally granular, poorly consolidated, and very susceptible to erosion. Grading increases the potential for erosion by removing protective vegetation, changing natural drainage patterns, and constructing slopes. However, compliance with the CBC and review of grading plans for individual projects by the City Engineer would ensure no significant impacts would occur. In addition, construction activities on project sites larger than one acre are required to prepare a Stormwater Pollution Prevention Plan (SWPPP) that details Best Management Practices (BMPs) to reduce the potential for erosion during construction activities.

Furthermore, TOP 2050 includes the following policies regarding erosion and loss of topsoil:

- **ER-1.6: Urban Run-off Quantity.** We encourage the use of low impact development strategies, including green infrastructure, to intercept run-off, slow the discharge rate, increase infiltration, and ultimately reduce discharge volumes to traditional storm drain systems.
- **ER-1.7: Urban Run-Off Quality.** We require the control and management of urban runoff, consistent with Regional Water Quality Control Board regulations.
- **S-5.1: Dust Control Measures.** We require the implementation of Best Management Practices for dust control at all excavation and grading projects.
- **S-5.2: Grading in High Winds.** We prohibit excavation and grading during strong wind conditions, as defined by the Building Code.

TOP 2050 would have similar soil erosion as the current TOP. This is because while the Proposed Project would result in an increase in land use intensity, TOP 2050 would not result in development of new, previously undeveloped areas of the City. After compliance with the safety provisions of the CBC implementation of TOP 2050 would have less-than-significant impacts from soil erosion. The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to erosion and topsoil loss compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to erosion and loss of topsoil. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.7-20)

Impact 5.7-3: The City of Ontario would not exacerbate geologic hazards in the City, such as on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse. [Threshold G-3]

The 2010 Certified EIR found that policies of the Approved Project, and state regulations would ensure that the potential impacts from geology and soils hazards would be less than significant.

Ground Subsidence

The thick alluvial deposits comprising the Chino Subbasin may be susceptible to compaction, with resulting subsidence at the surface, in the event of rapid groundwater withdrawal. Surface subsidence of up to 2.5 feet and ground fissuring from groundwater extraction have been reported in Chino. Projects considered for approval under TOP 2050 could expose structures or persons to potentially significant hazards from ground subsidence. However, compliance with the CBC and review of grading plans for individual projects by the City Engineer would ensure no significant impacts would occur.

Compressible Soils

The young sediments underlying the City are generally dry and loose in the upper few feet, and therefore are susceptible to compression. Much of the Ontario Ranch has been intensively farmed and is especially susceptible to compression. Developments approved pursuant to TOP 2050 could expose persons or structures to potentially significant hazards from compressible soils. However, compliance with the CBC and review of grading plans for individual projects by the City Engineer would ensure no significant impacts would occur. Furthermore, TOP 2050 includes policies regarding geology and soils hazards.

TOP 2050 would have similar geological hazards as the current TOP. This is because while the Proposed Project would result in an increase in land use intensity, TOP 2050 would not result in development of new, previously undeveloped areas of the City. After compliance with the safety provisions of the CBC implementation of TOP 2050 would have less-than-significant impacts from geologic hazards. The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to geology and soils compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to geologic hazards. Accordingly, no changes or alterations to the Proposed Project

were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.7-21)

Impact 5.7-4: Development associated with TOP 2050 would not be located on expansive soil, as defined in Table 18-1B of the Uniform building Code (1994), creating substantial direct or indirect risks to life or property. [Threshold G-4]

The 2010 Certified EIR found that policies of the Approved Project, and state regulations would ensure that the potential impacts from geology and soils hazards would be less than significant.

Expansive Soils

Expansive soils are likely in the southern parts of the City, where there are silts, sandy silts, and silty clays. Near-surface soils in the northern and central parts of the City are primarily granular, that is, silty sand, sand, and gravel; such sediments are usually nonexpansive or have very low expansion potential. Projects in the southern part of the City under TOP 2050 could expose persons or structures to potentially significant hazards from expansive soils. However, compliance with the CBC and review of grading plans for individual projects by the City Engineer would ensure no significant impacts would occur. Additionally, TOP 2050 includes policies regarding geology and soils hazards.

TOP 2050 would have similar impacts from expansive soils as the current TOP. This is because while the Proposed Project would result in an increase in land use intensity, TOP 2050 would not result in development of new, previously undeveloped areas of the City. After compliance with the safety provisions of the CBC implementation of TOP 2050 would have less-than-significant impacts from expansive soils. The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to geology and soils compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to expansive soils. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.7-21)

Impact 5.7-5: Implementation of TOP 2050 would not result in use of septic tanks or alternative waste water disposal systems that would not be supported by soils in the City. [Threshold G-5]

The 2010 Certified EIR found that policies of the Approved Project, and state regulations would ensure that the potential impacts from geology and soils hazards would be less than significant. Wastewater from Ontario is treated at wastewater treatment facilities owned and operated by the Inland Empire Utilities Agency (IEUA). Use of septic tanks would be limited to existing septic tanks, and new septic tanks would be constricted to areas not in practical proximity to existing sewer mains, dependent on approval by the Santa Ana Regional Water Quality Control Board (RWQCB) on a case-by-case basis. The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to geology and soils compared to the Approved Project.

Finding.. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to septic tanks and alternative waste water disposal systems. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.7-22)

8. Greenhouse Gas Emissions

Impact 5.8-1: Implementation of TOP 2050 with the CCAP is projected to result in emissions below those of the Approved Project and meet the GHG reduction target established under SB 32 and Executive Order S-03-05 and progress toward the State’s carbon neutrality goal. [Threshold GHG-1]

The 2010 Certified EIR identified significant GHG emissions impacts associated with the Approved Project as a result of the magnitude of population and employment growth projected by SCAG and TOP. Development under the Proposed Project would contribute to global climate change through direct and indirect emissions of GHG from land uses within the city. A general plan does not directly result in development without additional approvals. Before any development can occur in the city, it must be analyzed for consistency with TOP 2050, zoning requirements, and other applicable local and State requirements; comply with the requirements of CEQA; and obtain all necessary clearances and permits.

Horizon Year 2050 Emissions Forecast

TOP 2050 is an update to TOP to guide the city’s development and conservation through 2050. The Proposed Project is a focused effort, with particular emphasis on conducting technical refinements to the Policy Plan to comply with state housing mandates; conform with new state laws related to community health, environmental justice, climate adaption, resiliency, and mobility; and bring long-term growth and fiscal projections into alignment with current economic conditions. The community GHG emissions inventory for TOP 2050 compared to TOP is shown in Table 5.8-7, *GHG Emission Forecast*, in Section 5.8, *Greenhouse Gas Emissions*, of the Draft SEIR. As shown in this table, the increase in residential units and population associated with the Approved Project compared to the Proposed Project results in an increase in residential building energy use and a slight increase in solid waste and water/wastewater generation compared to the current TOP. However, the GHG emissions efficiency of the Proposed Project, expressed in GHG emissions per service population, improves compared to the Approved Project. Overall, GHG emissions associated with the Proposed Project would be slightly higher compared to those of the Approved Project in the absence of local measures identified in the CCAP and would not meet the 2050 GHG target of 2.0 MTCO_{2e} per capita.

Table 5.8-7 includes reductions from state measures that have been adopted to reduce GHG emissions, including:

- The RPS requires increases in renewable electricity supplies.
- The Clean Car Standards require increased fuel efficiency of on-road vehicles and decreased carbon intensity of vehicle fuels.

- The updated Title 24 Building Energy Efficiency Standards require new buildings to achieve increased energy efficiency targets.
- The LCFS mandates reduced carbon intensity of fuels used in off-road equipment.
- The short-lived climate pollutants law (SB 1383) proposes a comprehensive strategy to reduce methane and other emissions of short-lived greenhouse gases through regulations on dairy operations and urban landfills, including higher diversion rates of food waste from landfills.

Local GHG Reduction Measures

The City of Ontario has been implementing the GHG reduction measures identified in the 2014 CCAP to reduce GHG emissions in the city.

To improve energy efficiency of municipal buildings and operations, the City launched the Smart Ontario initiative, which involves an energy audit, comprehensive upgrade of municipal utility infrastructure, and implementation of energy infrastructure improvements. As of March 2022, the City has retrofitted all citywide street lights with LED light fixtures and all interior and exterior light fixtures in city buildings (approximately 15,000); has replaced over 100 heating, ventilation, and air conditioning (HVAC) components in city facilities; and replaced 350 thermostats in all city buildings. The City has installed 1.8 megawatt solar photovoltaic systems at the Ontario Convention Center and the Ontario Police Department, generating 2,571,125 kilowatt-hours of energy in 2019. To reduce the GHG emissions of newly constructed city buildings, City policies support all new municipal buildings to be Leadership in Energy and Environmental Design (LEED) certified by the U.S. Green Building Council. In compliance with the California Building Standards Code of Title 24, in November 2019, the City passed an ordinance to amend the municipal code and adopt by reference the 2019 California Green Building Standards Code.

The City has also implemented numerous projects to reduce GHG emissions from the transportation sector, including the installation of 21 electric vehicle (EV) charging stations, adoption of an Active Transportation Master Plan, synchronization of 30 percent of traffic signals through the Traffic Management Center, and completion of pedestrian and bicycle infrastructure improvements through Safe Routes to School and Active Transportation Program grants. Future projects include a citywide e-scooter share program (launching in March 2023), the Multimodal Transportation Center (needs assessment completed in March 2022), and the West Valley Corridor Bus Rapid Transit, a zero-emission bus line (completion expected in 2024).

The Proposed Project includes implementation of the CCAP update. The 2022 update to the CCAP draws upon strategies from the 2014 CCAP and the San Bernadino GHG Reduction Plan, with new strategies to address current state regulations and local issues of concern. The CCAP identifies GHG emissions reductions targets for the City of Ontario that would ensure consistency with the State GHG reduction goals of EO S-03-05 and substantial progress toward the State's carbon neutrality goals under EO B-55-18. Table 5.8-8, *2022 CCAP GHG Reduction Measures*, in Section 5.8, *Greenhouse Gas Emissions*, of the Draft SEIR shows the GHG reduction measures and reductions associated with the local measures in the draft CCAP at buildout of TOP 2050 that would help achieve those reductions.

Table 5.8-9, *TOP 2050 GHG Emissions Reduction Target Analysis with the CCAP*, in Section 5.8, *Greenhouse Gas Emissions*, of the Draft SEIR shows that the City would achieve the GHG reduction targets for year 2050 with implementation of the CCAP.

Furthermore, TOP 2050 includes policies that would reduce GHG associated with development projects.

- Land Use Element policies LU-2.1 through LU-2.5 would regulate new development impacts on nearby sensitive land uses.
- Environmental Resources Element policies ER-1.1 through ER-1.8 would reduce GHG emissions from water use and wastewater generation.
 - Policies ER-2.1 through ER-2.3 would reduce GHG emissions from solid waste disposal.
 - Policies ER-3.1 through ER-3.6 would ensure that new development is energy efficient.
 - Policies ER-4.1 through ER-4.9 would reduce air pollution from new development.
- Community Design Element policy CD-2.7 would ensure that sustainability is considered in the design of new projects.
- Mobility Element policies M-1.4 (complete streets), M-3.1 through M-3.11 (transit), and M-2.1 through M-2.4 (bicycle and pedestrian) would reduce VMT.

With implementation of the CCAP, TOP 2050 would result in a decrease in emissions from the Approved Project (see Table 5.8-10, *GHG Emissions Forecast with CCAP Implementation*, in Section 5.8, *Greenhouse Gas Emissions*, of the Draft SEIR). Further, as shown in Table 5.8-8, with implementation of the CCAP, the city would achieve the EO S-03-05 GHG emissions reduction targets, resulting in an 80 percent decrease in GHG emissions in the city by 2050 from existing conditions, and would make substantial progress toward the State's carbon neutrality goals under EO B-55-18. Therefore, TOP 2050, which includes the CCAP, would reduce GHG emissions impacts compared to the current TOP. The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to the generation of greenhouse gas emissions that may have a significant impact on the environment. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.7-26)

Impact 5.8-2: Impact 5.8-2: Implementation of TOP 2050 would not conflict with an applicable plan, policy, or regulation of an agency adopted for the purpose of reducing GHG emissions. [Threshold GHG-2]

The 2010 Certified EIR identified that the Approved Project was consistent with statewide strategies adopted for the purpose of reducing GHG emissions. Applicable plans adopted for the purpose of

reducing GHG emissions include CARB's Scoping Plan and SCAG's Connect SoCal. A consistency analysis with these plans is presented below.

CARB Scoping Plan

The CARB Scoping Plan is applicable to state agencies but is not directly applicable to cities/counties and individual projects (i.e., the Scoping Plan does not require local jurisdictions to adopt its policies, programs, or regulations to reduce GHG emissions). However, new regulations adopted by the State agencies from the Scoping Plan result in GHG emissions reductions at the local level. So local jurisdictions benefit from reductions in transportation emissions rates, increases in water efficiency in the building and landscape codes, and other statewide actions that affect a local jurisdiction's emissions inventory from the top down. Statewide strategies to reduce GHG emissions include the LCFS and changes in the corporate average fuel economy standards.

Project GHG emissions shown in Table 5.8-8 includes reductions associated with statewide strategies that have been adopted since AB 32 and SB 32. Development projects accommodated under TOP 2050 are required to adhere to the programs and regulations identified by the Scoping Plan and implemented by state, regional, and local agencies to achieve the statewide GHG reduction goals of AB 32 and SB 32. Future development projects would be required to comply with these state GHG emissions reduction measures because they are statewide strategies. For example, new buildings associated with land uses accommodated by implementing TOP 2050 would be required to meet the CALGreen and Building Energy Efficiency Standards in effect at the time when applying for building permits. Furthermore, as discussed under the discussion for Impact 5.8-1, TOP 2050 includes goals, policies, and programs that would help reduce GHG emissions and therefore help achieve GHG reduction goals. Impacts associated with the Approved Project and Proposed Project are similar. Implementation of TOP 2050 would not obstruct implementation of the CARB Scoping Plan, and impacts would be less than significant. The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to that of the Approved Project.

SCAG's Connect SoCal

Connect SoCal is Southern California's regional transportation plan to achieve the passenger vehicle emissions reductions identified under SB 375. Connect SoCal was adopted in September 2020. Connect SoCal's "core vision" centers on maintaining and better managing the transportation network for moving people and goods while expanding mobility choices by locating housing, jobs, and transit closer together and increasing investment in transit and complete streets. Moreover, Connect SoCal identifies areas in the region that can house near-term and long-term growth and support a diverse economy and workforce. By integrating the Forecast Development Pattern with a suite of financially constrained transportation investments, Connect SoCal can reach the regional target of reducing GHGs from autos and light-duty trucks by 8 percent per capita by 2020, and 19 percent by 2035 (compared to 2005 levels) (SCAG 2020).

As demonstrated in Section 5.11, *Land Use and Planning*, and Section 5.17, *Transportation*, TOP 2050 would be consistent with the Connect SoCal goals. Mobility Element policies M-1.4 (complete streets), M-3.1 through M-3.11 (transit), and M-2.1 through M-2.4 (bicycle and pedestrian) would reduce VMT per service population consistent with the regional goals. Furthermore, as discussed in Section 5.14,

Population and Housing, implementation of the Proposed Project would improve and maintain the jobs-housing balance in the City. Thus, TOP 2050 would provide for residents to both live and work in the City instead of commuting to other areas, which would contribute to minimizing VMT and reducing VMT per service population. Therefore, TOP 2050 would not interfere with SCAG's ability to implement the regional strategies in Connect SoCal, and no impact would occur. The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to conflict with applicable plans, policies, or regulations adopted for the purpose of reducing the emissions of greenhouse gases. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.8-27)

9. Hazards and Hazardous Materials

Impact 5.9-1: Implementation of TOP 2050 would involve the transport, use, and/or disposal of hazardous materials, but existing regulations and TOP 2050 Policies would ensure no adverse impacts on the environment. [Thresholds H-1, H-2, and H-3]

The 2010 Certified EIR identified that implementation of the policies in the Safety Element in addition to existing regulations would ensure less than significant impacts from transport, use, and/or disposal of hazardous materials.

TOP 2050 involves the designation of commercial, industrial, and residential land uses in Ontario, as well as continued redevelopment and large amounts of infill development. Development associated with TOP 2050 would result in a concentration of commercial, hospitality, office, and industrial uses around ONT and numerous mixed-use projects throughout the City. Implementation of TOP 2050 would increase the number of businesses and residents in the City, thereby increasing the amount of hazardous materials being transported, stored, and manufactured, and the number of people exposed to these materials. Buildout in accordance with TOP 2050 would result in an increase in the frequency of transport, use, and disposal of hazardous materials associated with commercial and industrial growth in Ontario, especially in the Ontario Ranch and around ONT. Though businesses and users are required by federal, state, and local regulations to properly transport, use, and dispose of hazardous material, it is possible that upset or accidental conditions may arise that result in the release of hazardous materials into the environment.

The City also has a number of pipelines and electrical lines that run through it. A major high-pressure distribution pipeline, operated by Kinder Morgan, serves ONT with jet fuel. Although this pipeline is registered with the EPA as a large-quantity generator of hazardous materials, the number of tons of material it generates is not known at this time. This pipeline and others running throughout the City are monitored by pipeline operators who are responsible for the upkeep of pipelines and the authorization of excavations around pipeline locations. Buildout of TOP 2050 would increase the exposure of people and the environment to potential hazards related to pipeline or electrical line

rupture. As with all development in California, development in Ontario would be required to follow the procedural requirements of the Underground Service Alert of Southern California, or DigAlert.

The City of Ontario has around 127 facilities or sites that generate, transport, treat, store, and/or dispose of hazardous waste, as recorded by the national Resource Conservation Recovery Act Envirofacts Database. Tables 5.9-1, *Hazardous Materials Sites in the City*, and 5.9-2, *EPA Hazardous Waste Transporters in Ontario in 2021*, in Section 5.9, *Hazards and Hazardous Materials*, of the Draft SEIR show the known contaminated sites and facilities in Ontario based on the Comprehensive Environmental Response, Compensation, and Liability Information System and EnviroStor databases. An increase in the transport of hazardous waste from an increased demand for transport, use, and disposal within or outside the City could result in more accidents leading to the release of hazardous materials. An increase in the transport of hazardous materials as a result of the proposed project would be limited to areas along interstates and rail lines, where commercial and industrial uses would be concentrated. Some transport of hazardous materials may occur near small commercial pockets proposed throughout various areas of the City.

Furthermore, demolition activities that have the potential to expose construction workers and/or the public to asbestos-containing materials or lead-based paint will be conducted in accordance with applicable regulations, including, but not limited to: South Coast AQMD's Rule 1403; California Health and Safety Code (Section 39650 et seq.); California Code of Regulations (Title 8, Section 1529); California Occupational Safety and Health Administration regulations (California Code of Regulations, Title 8, Section 1529 [Asbestos] and Section 1532.1 [Lead]); and Code of Federal Regulations (Title 40, Part 61 [asbestos], Title 40, Part 763 [asbestos], and Title 29, Part 1926 [asbestos and lead]).

In conclusion, current federal and state regulations, City ordinances, and TOP 2050 policies would regulate the handling of hazardous substances to reduce potential releases; exposure; and risks of transporting, storing, treating, and disposing of hazardous materials and wastes. Compared to the Approved Project, TOP 2050 would have similar impacts because the Proposed Project would result in an increase in land use intensity rather than development of new, previously undeveloped areas of the City that would require substantial landform modification. Therefore, like the Approved Project, additional hazardous waste transport, use, and/or disposal that would occur upon the buildout of TOP 2050 would be less than significant with adherence to the existing regulations. The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to transport, use, and disposal of hazardous materials, to release of hazardous materials, and to emission and handling of hazardous materials, substances, or waste. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.9-37)

Impact 5.9-2: Land uses in Ontario are on a list of hazardous materials sites; however, existing regulations and Safety Element policies of TOP 2050 would ensure that development would not exacerbate existing hazards. [Threshold H-4]

The 2010 Certified EIR identified that implementation of the policies in the Safety Element in addition existing regulations ensured that development of the Approved Project would not exacerbate existing hazards associated with existing hazardous materials sites.

Development in accordance with TOP 2050 would involve redevelopment and reuse of some sites listed as hazardous materials sites on environmental databases.

The listings document the presence of hazardous materials on those sites but do not document hazardous releases. Redevelopment of these sites could potentially expose future residents and workers to hazards from known hazardous materials releases on and near the sites.

Site assessments for hazardous materials and remediation of hazardous materials releases would be required for redevelopment projects developed in accordance with TOP 2050 and the regulations and policies of the agency assigned to the site (i.e., Department of Toxic Substances Control, Water Quality Control Board, Certified Unified Program Agencies, EPA). There are several TOP policies that address development on and around known hazardous waste sites. These policies include:

- **LU-2.1: Land Use Decisions.** We minimize adverse impacts on adjacent properties when considering land use and zoning requests.
- **LU-2.3: Hazardous Uses.** We regulate the development of industrial and similar uses that use, store, produce, or transport toxic substances, air emissions, other pollutants, or hazardous materials.
- **LU-2.9: Methane Gas Sites.** We require sensitive land uses and new uses on former dairy farms or other methane-producing sites be designed to minimize health risks.
- **S-6.2: Response to Hazardous Materials Releases.** We respond to hazardous materials incidents and coordinate these services with other jurisdictions.
- **S-6.5: Location of Hazardous Material Facilities.** We regulate facilities that will be involved in the production, use, storage, or disposal of hazardous materials, pursuant to federal, state, county, and local regulations, so that impacts to the environment and sensitive land uses are mitigated. We prohibit new hazardous waste facilities in close proximity to sensitive land uses and environmental justice areas.
- **S-6.6: Location of Sensitive Land Uses.** We prohibit new sensitive land uses from locating within airport safety zones and near existing sites that use, store, or generate large quantities of hazardous materials.

- **S-6.8: Mitigation and Remediation of Groundwater Contamination.** We actively participate in local and regional efforts directed at both mitigating environmental exposure to contaminated groundwater and taking action to clean up contaminated groundwater once exposure occurs.
- **S-6.9: Remediation of Methane.** We require development to assess and mitigate the presence of methane, per regulatory standards and guidelines.

Compared to the Approved Project, TOP 2050 would have similar impacts because the Proposed Project would result in an increase in land use intensity rather than development of new, previously undeveloped areas of the City that would require substantial landform modification. Therefore, like the Approved Project, buildout of TOP 2050 would not expose people to substantial hazards from hazardous materials sites listed on environmental databases. The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to location on a site which is included on a list of hazard materials. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.9-39)

Impact 5.9-3: TOP 2050 is within the airport influence area of the Ontario International Airport and Chino Airport; however, land uses are consistent with the airport safety zones. [Threshold H-5]

Airport safety hazards include hazards posed to aircraft and hazards posed by aircraft to people and property on the ground. With proper land-use planning, aircraft safety risks can be reduced, primarily by avoiding incompatible land uses. The Federal Aviation Administration (FAA) and California Department of Transportation (Caltrans) Division of Aeronautics provide guidance for land use safety near airports. With adherence to these guidelines, high concentrations of people are not exposed to potential airplane accidents along runways or near airports while airplanes are departing and arriving. There are also guidelines on the placement of housing, schools, and other sensitive land uses near airports because of the noise pollution caused by airplanes. The 2010 Certified EIR identified that the Approved Project would have a less than significant impact associated with consistency with the ONT Airport Land Use Compatibility Plan (ALUCP) and the Caltrans 2011 California Airport Land Use Planning Handbook for Chino Airport. Ontario International Airport – Inter Agency Collaborative (ONT-IAC) made a determination of consistency for the Approved Project with the ALUCP, and no comments from Chino Airport were received.

Ontario International Airport

ONT has the capacity to provide regional air traffic for domestic and international commercial and cargo service, and the necessary support facilities for major and smaller airlines. It operates as a medium-hub, full-service airport serving major US cities and international cities with an average of 67 daily departures. The City of Ontario prepared an ALUCP for ONT in accordance with the Caltrans Division of Aeronautics’ California Airport Land Use Planning Handbook.

The Land Use Element of TOP 2050 states that all new developments surrounding ONT should be consistent with the adopted ALUCP and should meet standards and recommendations of Part 77 of the FAA, adopted through Ordinance 2758 in the Ontario Municipal Code. A consistency determination analysis for the ONT was prepared by the City and submitted to ONT-IAC and found that TOP 2050 is consistent with ALUCP for ONT. Therefore, like the Approved Project, TOP 2050 is consistent with the ALUCP for ONT because the general land use designations within the airport influence area are the same.

Chino Airport

The Chino Airport is predominantly a recreational airport. Because the airport is not planned for expansion and would remain primarily recreational, and only lower elevation buildings surround it and would continue to surround it upon project implementation, the Chino Airport poses no unique hazards. Buildout of TOP 2050 would involve development within the Chino Airport influence area. The proposed land uses include Medium Density Residential, Mixed Use, Business Park, Industrial, and Open Space–Recreation.

Projects accommodating TOP 2050 in this area would be required to meet the conditions of the Chino Airport Authority and the 2011 Caltrans Airport Land Use Planning Handbook, including those determining appropriate land uses, maximum population density, maximum site coverage, height restrictions, and required notification/disclosure areas based on the noise contours and runway protection, approach, and Part 77 zones of the adopted Chino Airport Master Plan. Additionally, implementation of TOP 2050 would result in a beneficial impact for land use compatibility near Chino Airport as a result of the change from residential and business park to warehouse/industrial land uses.

The Airport Planning section of TOP 2050 Land Use Element includes policies that would ensure airport planning compatibility and consistency. These policies include:

- **LU-5.1: Coordination with Airport Authorities.** We collaborate with FAA, Caltrans Division of Aeronautics, airport owners, neighboring jurisdictions, and other shareholders in the preparation, update, and maintenance of airport-related plans.
- **LU-5.2: Airport Planning Consistency.** We coordinate with airport authorities to ensure The Ontario Plan is consistent with state law, federal regulations, and/or adopted master plans, and airport land use compatibility plans for ONT and Chino Airport.
- **LU-5.3: Airport Impacts.** We work with agencies to maximize resources to mitigate the impacts and hazards related to airport operations – their homes.
- **LU-5.4: ONT Growth Forecast.** We support and promote an ONT that accommodates 30 million annual passengers and 1.6 million tons of cargo per year, as long as the impacts associated with that level of operations are planned for and mitigated.
- **LU-5.5: Airport Compatibility Planning for ONT.** We create and maintain the Airport Land Use Compatibility Plan for ONT.

- **LU-5.6: Alternative Process.** We fulfill our responsibilities and comply with state law with regard to the Alternative Process for proper airport land use compatibility planning.
- **LU-5.7: ALUCP Consistency with Land Use Regulations.** We comply with state law that requires general plans, specific plans, and all new development to be consistent with the policies and criteria set forth within an Airport Land Use Compatibility Plan for any public-use airport.
- **LU-5.8: Chino Airport.** We will support the creation and implementation of the Airport Land Use Compatibility Plan for Chino Airport.
- **M-5.2. Land Use Compatibility with Regional Transportation Facilities.** We work with ONT, railroads, Caltrans, SBCTA, and other transportation agencies to minimize impacts.

Consequently, TOP 2050 ensures compatibility with ONT and Chino Airport.

The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to safety hazard or excessive noise for people residing or working in the project area located within an airport land use plan. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.9-41)

Impact 5.9-4: Implementation of TOP 2050 would not impair implementation of or physically interfere with an adopted emergency response plan. [Threshold H-6]

The 2010 Certified EIR found that the Approved Project would not interfere with an emergency evacuation plan. According to the Vulnerability Analysis conducted as part of TOP 2050, the threat of flood is Ontario’s greatest hazard as large portions of the City are within the flood zone. The majority of the population growth associated with TOP 2050 would occur in Ontario Ranch. As identified in the City’s Roadway Classification map (see Figure 5.17-3, *Roadway Classifications*, in Section 5.17, *Transportation*, of the Draft SEIR), there is substantial improvements in transportation infrastructure planned to accommodate the increase in population in the City in the event of an emergency. A review of emergency access is included as part of the City’s Design Review process. According to the City’s 2018 Local Hazard Mitigation Plan (LHMP), interstate highways would serve as major emergency response and evacuation routes (see Figure 5.17-6, *Evacuation Routes*, in Section 5.17, *Transportation*, of the Draft SEIR). Additionally, the Ontario Fire Department (OFD) reviews development applications to ensure that adequate emergency accessibility is provided based on local and state guidance. The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts compared to the Approved Project.

Finding. Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to impairment of or interference with adopted emergency response plans or emergency evacuation plans. Accordingly, no changes or alterations to the Proposed Project were required to

avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.9-41)

Impact 5.9-5: TOP 2050 would not result exacerbate wildfire risks in Ontario. [Threshold H-7]

The 2010 Certified EIR found that the Approved Project would result in less than significant risks from wildfire hazards. The City is outside of the state responsibility area, and California Department of Forestry and Fire Protection (CAL FIRE) has determined that the City contains no areas subject to very high wildfire risk (see Figure 5.20-2, *Fire Hazard Severity Zones*, in Section 5.20, *Wildfire*, of the Draft SEIR). However, the City recognizes that even though fuel loading is light and fire risk comes primarily from urban fires, not wildfires, there is some risk related to wildfires.

There are many resources available to address wildland fires should they arise, including the CAL FIRE 2019 Strategic Fire Plan for California, the California Fire Code (CFC), County of San Bernardino Multi-jurisdiction Hazard Management Plan (MJHMP), the Ontario LHMP, and fire services from the OFD. With adherence to these building practices, development and infrastructure associated with TOP 2050 would not exacerbate risk or result in post-wildfire hazards (e.g., landslides, mudflows, and flooding).

In addition, the Safety Element contains the following policies to prevent wildfire hazards and support the community during wildfire events:

- **S-3.4: Special Team Services.** We maintain effective special rescue services.
- **S-3.6: Interagency Cooperation.** In order to back up and supplement our capabilities to respond to emergencies, we participate in the California Fire Rescue and Mutual Aid Plan.
- **S-3.8: Fire Prevention through Environmental Design.** We require new development to incorporate fire prevention consideration in the design of streetscapes, sites, open spaces, and buildings.
- **S-3.9: Resource Allocation.** We analyze fire data to evaluate the effectiveness of our fire prevention and reduction strategies and allocate resources accordingly.
- **S-8.3: Emergency/Disaster Training and Exercises.** We conduct training and exercises to prepare for and evaluate emergency/disaster response and recovery procedures.
- **S-8.5: Interdepartmental Coordination.** We utilize all City departments to help support emergency/disaster mitigation, preparedness, response, mitigation, and recovery.
- **CD-2.8: Safe Design.** We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintaining visibility and accessibility, and using lighting.

The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to that of the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to wildland fires. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.9-42)

10. Hydrology and Water Quality

Impact 5.10-1: The Proposed Project would not violate water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality. [Threshold HYD-1]

The 2010 Certified EIR identified less than significant impacts related to water quality from development activities associated with the Approved Project. Nearly the entire OMC is developed, and implementation of TOP 2050 would not substantially alter the amount of developed land in the OMC. Most of Ontario Ranch, however, is agricultural land which is designated for future urban use development by the current TOP and TOP 2050.

Construction

Clearing, grading, excavation, and construction activities associated with TOP 2050 have the potential to impact water quality through soil erosion and increasing the amount of silt and debris carried in runoff. Additionally, the use of construction materials, such as fuels, solvents, and paints, may present a risk to surface water quality. Finally, the refueling and parking of construction vehicles and other equipment on-site during construction may result in oil, grease, or related pollutant leaks and spills that may discharge into the storm drain system.

To minimize these potential impacts, future development associated with TOP 2050 would require compliance with the State Water Resources Control Board (SWRCB) Construction General Permit (CGP) Water Quality Order 2009-0009-DWQ as amended by Order No. 2010-0014-DWQ and 2012-006-DWQ. The SWRCB mandates that projects that disturb one or more acres of land must obtain coverage under the Statewide CGP. The CGP also requires that prior to the start of construction activities, the project applicant must file permit registration documents (PRDs) with the SWRCB, which includes a notice of intent, risk assessment, site map, annual fee, signed certification statement, and SWPPP.

A SWPPP requires the incorporation of BMPs to control sediment, erosion, and hazardous materials contamination of runoff during construction and prevent contaminants from reaching receiving water bodies. The construction contractor is always required to maintain a copy of the SWPPP at the site and implement all construction BMPs identified in the SWPPP during construction activities. Prior to the issuance of a grading permit, the project applicant is required to provide proof of filing of the PRDs with the SWRCB, which include preparation of SWPPP.

In addition, the City of Ontario requires that an erosion and sediment control plan be submitted prior to grading plan approval and the issuance of a grading permit. Implementation of the erosion control plan would address any potential erosion issues associated with proposed grading and site preparation activities associated with future buildout under TOP 2050.

Submittal of the PRDs and implementation of the SWPPP and the erosion control plan throughout the construction phase of the Proposed Project would address anticipated and expected pollutants of concern as a result of construction activities. The Proposed Project would comply with all applicable water quality standards and waste discharge requirements. As a result, water quality impacts associated with TOP 2050 construction activities would be less than significant.

Operation

Pollutants from the post-construction phases of projects include sediment, metals, nutrients, pesticides, and hydrocarbons. TOP 2050 includes policies ER-1.5, ER-1.6, and ER-1.7 direct the City to reduce pollutants in the City's stormwater system. Projects approved under TOP 2050 would be required to control pollutants in discharges of stormwater from postconstruction activities under National Pollutant Discharge Elimination System (NPDES) Permit No. CAS618036 through preparation of a Water Quality Management Plan (WQMP) identifying BMPs for prevention of stormwater pollution during the post-construction phase, including site-design, source-control, and/or treatment BMPs.

Site design BMPs are measuring for reducing or eliminating runoff—such as maximizing permeable areas and natural drainage systems such as swales and using stormwater detention and retention basins. Source control BMPs are designed to minimize the potential for pollutants to contact stormwater, which would limit the potential for water quality impacts downstream. Structural source control measures minimize stormwater pollution by such means as paving trash storage areas and fueling areas with impervious surfaces and grading such areas to redirect run-on. Nonstructural source control measures are intended to minimize stormwater pollution through such means as education of owners, tenants, and occupants; employee training; activity restrictions, including prohibiting the discharging of fertilizers, pesticides, or waste to streets or storm drains; and a spill contingency plan. Treatment control BMPs (single or in combination) remove pollutants of concern from on-site runoff. All treatment BMPs would be designed in accordance with the procedures and spreadsheets in the “San Bernardino County Technical Guidance Document for WQMPs.”

TOP 2050 would continue policies of the Approved Project to reduce pollutants from entering the City's stormwater system, and future development projects associated with TOP 2050 would be required to control pollutants in discharges of stormwater from post-construction activities through WQMP preparation and implementation. Therefore, water quality standards and waste discharge requirements would not be exceeded, and surface water and groundwater quality would not be degraded.

The Proposed Project would not result in a new or a substantial increase in the magnitude of impacts related to water quality associated with development activities compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to surface or groundwater quality. Accordingly, no changes or alterations to the

Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.10-21)

Impact 5.10-2: The Proposed Project would not substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin. [Threshold HYD-2]

The 2010 Certified EIR identified less than significant impacts related to groundwater recharge and supply for the Approved Project. Buildout of TOP 2050 is forecast to increase residential units by 25,399 and increase nonresidential square footage by 1,092,508 square feet compared to the Approved Project. Future development would increase the amount of impermeable surfaces in the City and reduce the amount of permeable surfaces available for groundwater recharge.

Nearly all of the OMC is developed with urban uses and not available for groundwater recharge. Therefore, most of the increase in impermeable surfaces in the City would result from development of the Ontario Ranch in accordance with land use designations in TOP 2050. Planned drainage improvements in Ontario Ranch would increase the capability of conveying stormwater to the county's existing regional storm drain system and minimize the potential for flooding to occur in City streets. Many of these projects have already been completed, as documented in Table 5.19-10, *Proposed Storm Drain Improvements*, in Section 5.19, *Utilities and Service Systems*, of the Draft SEIR. Projects considered for approval under the Approved Project and TOP 2050 would have to meet the following requirements for limiting impacts to groundwater recharge:

- BMPs for compliance with NPDES regulations, for instance, preservation of existing vegetation.
- Preparation of project-specific hydrology studies estimating project impacts on drainage, in accordance with procedures in the *San Bernardino County Technical Guidance Document for WQMPs* (2013).

TOP 2050 contains policies that would promote infiltration of runoff and groundwater recharge, including Policies ER-1.5 and ER-1.6. Policy ER-1.6 encourages use of Low Impact Development (LID) strategies to intercept runoff, slow the discharge rate, increase infiltration, and ultimately reduce discharge volumes to traditional storm drain systems. Potential LID strategies that could be implemented by development in the City include bioretention, dry wells, filter strips, grassed swales, infiltration trenches, inlet pollution removal devices, permeable pavement, permeable pavers, rain barrels and cisterns, soil amendments, tree box filters, vegetated buffers, and vegetated roofs.

In compliance with the Chino Basin Watermaster's Well Procedure for Developers, a well use/destruction plan and schedule for all existing private/agricultural wells shall be submitted to the City of Ontario for approval prior to the issuance of permits for any construction activity. If a private well is actively used for water supply, the developer shall submit a plan to abandon such well and connect users to the City's water system (residential to the domestic water system and agricultural to the recycled water system) when available. Wells shall be destroyed/abandoned per the California Water Resource Guidelines, which requires permitting from San Bernardino County Health Department. A copy of the permit and Form DWR 188 Well Completion Form shall be provided to

the City's Community Development Engineering Department and the Ontario Municipal Utilities Company (OMUC) Engineering Department prior to issuance of grading and/or building permits. If the developer proposes temporary use of an existing agricultural well for purposes other than agriculture, such as grading, dust control, etc., the developer shall make a formal request to the City for that use prior to issuance of permits for any construction activity. Upon approval, the developer shall enter into an agreement with the City and pay any applicable fees.

Future urban development in Ontario Ranch would be served by domestic water provided by the City. Approximately 46 percent of the City's water supply is groundwater pumped by the City from the Chino Groundwater Basin; groundwater pumping is managed by OMUC. so that domestic demands do not exceed the safe yield for the basin, consistent with the Chino Basin Watermaster's Optimum Basin Management Program, commonly called the "OBMP Peace Agreement". The City also recharges stormwater and recycled water into the Chino Groundwater Basin and therefore is entitled to groundwater recharge credits.

With the implementation of City policies that promote LID and infiltration for new development projects and compliance with the Chino Basin Watermaster's safe yield restrictions, the potential for the project to substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin is considered less than significant.

The Proposed Project would not result in a new or a substantial increase in magnitude of impacts that would impede sustainable groundwater management of the basin compared to the Approved Project. The Chino Groundwater Basin is adjudicated and is considered by Department of Water Resources to be a very low priority groundwater basin. Each water purveyor has an allotted amount of water that can be pumped from the basin so that the safe yield is not exceeded. The City has access to additional water supplies that can accommodate the proposed increase in growth with buildout of the TOP and would not interfere with sustainable management of the groundwater basin. Therefore, impacts would be less than significant.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to impediment of sustainable groundwater management of the basin. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.10-23)

Impact 5.10-3: The Proposed Project would increase impervious surfaces but would not substantially increase the rate or amount of surface runoff in a manner which would impact water quality or cause flooding. [Threshold HYD-3]

The 2010 Certified EIR identified less than significant impacts related to increased surface runoff for the Approved Project.

Erosion and Siltation

Similar to the Approved Project, future development associated with TOP 2050 would involve site improvements that require grading, excavation, and soil exposure during construction, with the

potential for erosion or siltation to occur. If not controlled, the transport of these materials to local waterways could temporarily increase suspended sediment concentrations and release pollutants attached to sediment particles. To minimize this impact, the project would be required to comply with the requirements in the State's CGP, including preparation of a notice of intent and SWPPP prior to the start of construction activities. The SWPPP would describe the BMPs to be implemented during the project's construction activities. The implementation of the BMPs during the construction phase would include the following measures to minimize erosion and siltation:

- Minimize disturbed areas of the site.
- Install on-site sediment basins to prevent off-site migration of erodible materials.
- Implement dust control measures, such as silt fences and regular watering of open areas.
- Stabilize construction entrances/exits.
- Install storm drain inlet protection measures.
- Install sediment control measures around the site, including silt fences or gravel bag barriers.

In addition, the City of Ontario requires preparation of an erosion and sediment control plan and implementation of BMPs to control erosion, debris, and construction-related pollutants. This would further reduce the potential for erosion and siltation during the construction phase.

For post-construction, projects approved under TOP 2050 would be required to control stormwater discharges under NPDES Permit No. CAS618036 through preparation of a WQMP identifying BMPs for reducing or eliminating runoff. Additionally, TOP 2050 policies ER-1.5, ER-1.6, and ER-1.7 direct the City to incorporate strategies to capture, slow, or treat run-off that would reduce the potential for erosion and siltation during the operational phase of future development projects.

Collectively, implementation of BMPs outlined in SWPPPs, erosion and sediment control plans, WQMPs, and TOP 2050 policies would address anticipated erosion and siltation impacts. Therefore, the project would not result in substantial erosion or siltation on- or off-site.

Flooding On- and Off-Site

Buildout of TOP 2050 is forecast to increase residential units by 25,399 and increase nonresidential square footage by 1,092,508 square feet, compared to the Approved Project. Future development would increase the amount of impermeable surfaces in the City, which could result in future on- and off-site flooding. Future development projects would implement BMPs outlined in SWPPPs to reduce flooding impacts due to runoff during construction and BMPs included in WQMPs to reduce the potential for post-construction flooding impacts. The City's standard conditions of approval for new development also require the preparation of hydrology studies and drainage analyses that document the peak runoff rates from the developed site and evaluate the capacity of the storm drain system to accept these flow rates. Additionally, TOP 2050 policies ER-1.6, ER-1.7, S-2.1, S-2.5 and S-2.6 direct the City to incorporate strategies to capture, slow, or treat run-off and to reduce the flooding potential down-gradient of new development. These policies would reduce the potential for on- and off-site

flooding during the operational phase of future development projects. Therefore, the project would not result in flooding on- or off-site.

Surface Runoff and Capacity of Storm Drain System

There are three major regional drainage channels that convey stormwater runoff from the City's storm drain system—San Antonio Channel, Cucamonga Channel, and Day Creek Channel. There are also several flood retentions and spreading basins in the City that are used to retain flood flows and recharge the Chino Groundwater Basin.

Projects considered for approval under TOP 2050 would be required to prepare project-specific hydrology and hydraulic studies as required by the City. The methodology for these studies is provided in the San Bernardino County Hydrology Manual, which describes the approach for estimating stormwater runoff and peak flow rates, for the 100-year storm event.

In compliance with the Municipal Separate Storm Sewer System (MS4) Permit and San Bernardino County Stormwater Program, new development projects would also be mandated to install stormwater treatment BMPs that retain the 2-year, 24-hour rainfall event. Furthermore, the City, under TOP 2050 policy ER-1.6, would encourage the use of LID strategies to intercept runoff, slow the discharge rate, increase infiltration, and ultimately reduce discharge volumes to traditional storm drain systems. The City, through TOP 2050 policy S-2.5, would maintain and improve the storm drain system to minimize flooding, thus reducing the impacts of any increases in surface water flows that enter the storm drainage systems. Because new development in the City would be required to prepare a hydrology study and drainage analysis in accordance with the San Bernardino County Hydrology Manual, no significant impacts would occur.

Flood Flows

On the current FIRM for Ontario, only small portions of the City adjacent to flood control channels, detention basins, and creeks are in the 100-year floodplain (see Figure 5.10-2, *Flood Hazard Zones*, in Section 5.10, *Hydrology and Water Quality*, of the Draft SEIR). The western portion of Ontario Ranch is labeled Zone D—undetermined flood hazard—and no hazard analysis has been completed for this area. Thus, implementation of the Approved Project and TOP 2050 could result in development in areas subject to flooding.

Under TOP 2050 policies, the City would take the following actions to reduce impacts of potential developments within 100-year flood zones:

- **S-2.1: Entitlement and Permitting Process.** We require hydrological studies prepared by a State-certified engineer when new development is located in a 100-year or 500-year floodplain to assess the impact that the new development will have on the flooding potential of existing development down-gradient.
- **S-2.2: Floodplain Mapping.** We require any new development partially or entirely in 100-year flood zones to provide detailed floodplain mapping for 100- and 200-year storm events as part of the development approval process.

- **S-2.3: Facilities that Use Hazardous Materials.** We comply with state and federal law and do not permit facilities using, storing, or otherwise involved with substantial quantities of onsite hazardous materials to be located in the 100-year flood zone or 500-year flood zone unless all standards of elevation, floodproofing, and storage have been implemented to the satisfaction of the Building Department.
- **S-2.4: Prohibited Land Uses.** We prohibit the development of new essential and critical facilities in the 100-year floodplain and discourage the development of new essential and critical facilities in the 500-year floodplain unless all standards of elevation and flood proofing demonstrate that a facility can be safe and operational during a flood event, implemented to the satisfaction of the Building Department.
- **S-2.5: Stormwater Management.** We maintain and improve the storm drain system to convey a 100-year storm, when feasible, and encourage environmental site design practices to minimize flooding and increase groundwater recharge, including natural drainage, green infrastructure, and permeable ground surfaces.

In addition to these policies, the Ontario Municipal Code, Chapter 13, *Flood Damage Prevention Program*, requires that a development permit be obtained prior to development in a special flood hazard area to ensure that the site is reasonably safe from flooding and flood hazards. The City requires that all new structures in a special flood hazard area have elevations above the base flood elevation. Therefore, with implementation of existing policies, the potential for the project to impede or redirect flood flows is considered less than significant.

The Proposed Project would not result in a new or a substantial increase in magnitude of impacts related to flood hazards compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to alteration or addition of impervious surfaces. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.10-26)

Impact 5.10-4: The Proposed Project would not exacerbate risk of flood hazards, tsunamis, or seiches or risk release of pollutants due to inundation. [Threshold HYD-4]

The 2010 Certified EIR identified less than significant impacts related to flood hazards, tsunamis, seiches, or dam inundation with the Approved Project. As shown on Figure 5.10-2, *Flood Hazard Zones*, in Section 5.10, *Hydrology and Water Quality*, of the Draft SEIR, only small portions of the City are in the 100-year floodplain, adjacent to flood control channels, detention basins, and creeks. Under TOP 2050 policies, the City would take the following actions to reduce impacts of potential developments within 100-year flood zones:

- **S-2.2: Floodplain Mapping.** We require any new development partially or entirely in 100-year flood zones to provide detailed floodplain mapping for 100- and 200-year storm events as part of the development approval process.

- **S-2.3: Facilities that Use Hazardous Materials.** We comply with state and federal law and do not permit facilities using, storing, or otherwise involved with substantial quantities of onsite hazardous materials to be located in the 100-year flood zone or 500-year flood zone unless all standards of elevation, floodproofing, and storage have been implemented to the satisfaction of the Building Department.

The western and southern portions of the City are in the dam inundation zone of San Antonio Dam (see Figure 5.10-3, *Dam Inundation Zones*, in Section 5.10, *Hydrology and Water Quality*, of the Draft SEIR). The dam is owned and operated by the USACE and functions as a flood control and debris dam for San Antonio Creek. Additionally, there are several debris basins in the surrounding areas that impact the northern and eastern parts of the City. The probability of dam failure is very low, and Ontario has never been impacted by a major dam failure. In addition, dam owners are required to maintain emergency action plans that include procedures for damage assessment and emergency warnings. An emergency action plan (EAP) identifies potential emergency conditions at a dam and specifies preplanned actions to help minimize property damage and loss of life should those conditions occur. EAPs contain procedures and information that instruct dam owners to issue early warning and notification messages to downstream emergency management authorities, such as the OFD. In addition, flooding would be minimal if any of the debris basins were to fail. Because the likelihood of catastrophic failure of the San Antonio Dam is very low and the City has EAP notification procedures, impacts of release of pollutants due to dam inundation are considered less than significant.

There are no large bodies of water that would result in a seiche during seismic activity. Additionally, the reservoirs/aboveground water tanks within the City are enclosed, thereby minimizing the possibility of a seiche. The project site is inland and approximately 30 miles from the ocean and is not at risk of flooding due to tsunamis.

Therefore, impacts associated with the release of pollutants due to inundation would be less than significant. The Proposed Project would not result in a new or a substantial increase in magnitude of impacts related to flood hazards compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to risk of release of pollutants due to project inundation. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.10-27)

Impact 5.10-5: The Proposed Project would not obstruct or conflict with the implementation of a water quality control plan or sustainable groundwater management plan. [Threshold HYD-5]

The 2010 Certified EIR identified less than significant impacts related to water quality or groundwater issues. The City's groundwater supplies are from the Chino Groundwater Basin, which is adjudicated and managed by the Chino Basin Watermaster. The Chino Basin is exempt from legislative requirements under the Sustainable Groundwater Management Act because it is an adjudicated basin and is not required to prepare a groundwater sustainability plan. Adjudicated basins have determined

the safe yield for the basin and have assigned individual pumping allocations to limit groundwater production to the safe yield.

Adherence to the State CGP, implementation of the SWPPP, and adherence to the City's Erosion and Sediment Control Plan requirements would ensure that surface and groundwater quality are not adversely impacted during construction. Projects approved under TOP 2050 would be required to comply with the Santa Ana River Basin Plan and to control pollutants in discharges of stormwater from postconstruction activities under NPDES Permit No. CAS618036 through preparation of a WQMP identifying BMPs for prevention of stormwater pollution during the post-construction phase, including site-design, source-control, and/or treatment BMPs. Therefore, the project would not obstruct or conflict with the RWQCB's Basin Plan or any groundwater management plan, and impacts would be less than significant.

The Proposed Project would not result in a new or a substantial increase in magnitude of impacts related to consistency with a water quality control plan or sustainable groundwater management plan.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to conflict with or obstruction of a water quality control plan or sustainable groundwater management plan. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.10-27)

11. Land Use and Planning

Impact 5.11-1: Project implementation would not divide an established community. [Threshold LU-1]

The 2010 Certified EIR found that the Approved Project's land use plan would not divide an established community.

Implementation of TOP 2050 would guide future growth within the City of Ontario. Table 3-1, *Approved TOP Buildout Projections*, and Table 3-4, *Comparison of Approved TOP to TOP 2050*, in Chapter 3, *Project Description*, of the Draft SEIR detail the buildout statistics associated with the current TOP and TOP 2050.

The changes in land use that would occur upon the implementation of TOP 2050 Land Use Plan would not result in the physical division of an established community. In the OMC, residential, commercial, and industrial land uses would remain similar to existing residential land uses. In Ontario Ranch, land use changes include residential to employment or mixed use, commercial to residential, and increased density residential, but would not divide established communities. Most of the agricultural land uses in Ontario Ranch are in decline and the establishment of new urban developments would create a sense of community. The mixed-use designations would also bring entertainment, activity, and diversity to housing, retail, and workplace land uses in the City, which would help create attractive communities for local citizens and visitors. The Land Use Element of TOP 2050 contains policies and programs that encourage the preservation or enhancement of the existing, primarily residential community through infill development, open space opportunities, and development of compatible uses that would

reduce the amount of conflict between contradicting land uses and enhance the existing character of Ontario.

Additionally, the TOP 2050 Land Use and Housing Elements have specific policies that encourage neighborhood identity and preservation. Some of these policies include, but are not limited to:

- **LU-1.1: Strategic Growth.** We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, foster the development of transit, and support the expansion of the active and multimodal transportation networks throughout the City.
- **LU-1.2: Sustainable Community Strategy.** We integrate state, regional, and local Sustainable Community/Smart Growth principles into the development and entitlement process.
- **LU-1.3: Adequate Capacity.** We require adequate infrastructure and services for all development.
- **LU-2.1: Land Use Decisions.** We minimize adverse impacts on adjacent properties when considering land use and zoning request.
- **LU-2.2: Buffers.** We require new uses to provide mitigation or buffers between existing uses where potential adverse impacts could occur. Additional mitigation is required when new uses could negatively impact environmental justice areas.
- **LU-2.6: Infrastructure Compatibility.** We require infrastructure to be aesthetically pleasing and in context with the community character.
- **LU-2.7: Inter-jurisdictional Coordination.** We maintain an ongoing liaison with ONT, Caltrans, Public Utilities Commission, the railroads, and other agencies to help minimize impacts and improve the operations and aesthetics of their facilities.
- **LU-2.10: Sensitive Uses.** We monitor and share information with the community about stationary and non-stationary emission sources. We encourage siting and design of facilities to minimize health and safety risks on existing and proposed sensitive uses, especially in environmental justice areas.
- **LU-2.11: Context-Aware Transitions and Connections.** We require new development projects and land-planning efforts to provide context-aware and appropriate transitions and connections between existing and planned neighborhoods, blocks, sites, and buildings.
- **LU-3.1: Development Standards.** We maintain clear development standards that allow flexibility to achieve our Vision and provide objective standards that ensure predictability and deliver the intended physical outcomes.
- **LU-3.3: Land Use Flexibility.** We consider uses not typically permitted within a land use category if doing so improves livability, reduces vehicular trips, creates community gathering places and activity nodes, and helps create identity.

- **LU-4.2: Interim Development.** We allow development in urban, mixed-use, and transit-oriented Place Types that is not immediately reflective of our ultimate Vision for the Place Type, provided it can be modified or replaced when circumstances are right to support development aligned with the Place Type Vision. We will not allow development that impedes, precludes, or compromises our ability to achieve our Vision.
- **LU-4.3: Infrastructure Timing.** We require that the necessary infrastructure and services be in place prior to or concurrently with development.
- **LU-4.4: Shared Infrastructure.** We encourage and facilitate the use of shared infrastructure (including shared or managed parking) in urban, mixed-use, and transit-oriented Place Types.
- **H-1.2: Neighborhood Conditions.** We direct efforts to improve the long-term sustainability of neighborhoods through comprehensive planning, provision of neighborhood amenities, rehabilitation and maintenance of housing, and community building efforts.
- **H-1.5: Neighborhood Identity.** We strengthen neighborhood identity through creating parks and recreational outlets, sponsoring neighborhood events, and encouraging resident participation in the planning and improvement of their neighborhoods.

Consequently, TOP 2050 would avoid conflicting land uses and would not divide an established community.

The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to the division of a community compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to physical division of an established community. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.11-6)

Impact 5.11-2: Project implementation would not conflict with applicable plans adopted for the purpose of avoiding or mitigating an environmental effect. [Threshold LU-2]

The 2010 Certified EIR found that buildout of the Approved Project would not conflict with applicable plans adopted for the purpose of avoiding or mitigating an environmental effect.

TOP 2050 is meant to be a framework for planning and development in Ontario for the next 30 or more years. As described in Section 5.14, *Population and Housing*, of the Draft SEIR, buildout of TOP 2050 Land Use Plan would provide sufficient dwelling units, population, and employment capacity to exceed SCAG’s projections for 2050.

The preparation of TOP 2050 and the City’s vision must be consistent with the policies and regulations of existing regional and local plans that are meant to prevent environmental impacts related to population growth and land use conflicts.

Consistency with SCAG's Regional Plans and Policies

The consistency of TOP 2050 with SCAG's Connect SoCal is shown in Table 5.11-1, *Consistency with SCAG Connect SoCal*, in Section 5.11, *Land Use and Planning*, of the Draft SEIR. Connect SoCal is a major advisory plan prepared by SCAG that addresses important regional issues like housing, traffic/transportation, water, and air/quality. Connect SoCal serves as an advisory document to local agencies in the Southern California region for their information and voluntary use for preparing local plans and handling local issues of regional significance. Connect SoCal is advisory only and cannot be used for intergovernmental review. TOP 2050 is consistent with the majority of Connect SoCal's goals (see Section 5.14, *Population and Housing*, of the Draft SEIR for a discussion on consistency with SCAG demographic projections). The elements of TOP 2050 are the Community Design (CD), Community Economics (CE), Environmental Resources (ER), Housing (H), Land Use (LU), Mobility (M), Parks and Recreation (PR), Safety (S), and Social Resources (SR) Elements. Policies from these elements are included in the consistency table.

As summarized in the table above, implementation of TOP 2050 would not result in significant land use impacts related to SCAG's Connect SoCal.

Airport Plans

Airport operations and their accompanying noise and safety hazards require careful land use planning on adjacent lands to ensure the safety of residents and passengers, and to protect Ontario businesses and property owners from the potential hazards that could be created by airport operations. The FAA and Caltrans Division of Aeronautics provide guidance for land use safety near airports. With adherence to these guidelines, high concentrations of people are not exposed to potential airplane accidents along runways or near airports while airplanes are departing and arriving. There are also guidelines on the placement of housing, schools, and other sensitive land uses near airports because of the noise pollution caused by airplanes.

Ontario International Airport

ONT has the capacity to provide regional air traffic for domestic and international commercial and cargo service, and the necessary support facilities for major and smaller airlines. It operates as a medium-hub, full-service airport serving major US cities and international cities with an average of 67 daily departures. The City of Ontario has prepared an ALUCP for ONT in accordance with the Caltrans Division of Aeronautics' California Airport Land Use Planning Handbook.

The Land Use Element of TOP 2050 states that all new developments surrounding ONT should be consistent with the adopted ALUCP and should meet standards and recommendations of Part 77 of the FAA, adopted through Ordinance 2758 in the Ontario Municipal Code. A consistency determination analysis for the ONT was prepared by the City, submitted to the ONT-IAC Technical Advisory Committee, and found that TOP 2050 is consistent with ALUCP for ONT.

Chino Airport

Chino Airport is operated by the San Bernardino County Department of Airports and is designated a reliever airport for ONT and San Bernardino International Airport. It operates on 1,100 acres and serves private, business, and corporate tenants and customers from the Inland Empire. The Chino

Airport Master Plan was implemented by San Bernardino County in 2003. Buildout of TOP would involve development within the Chino Airport influence area. Land uses within the Chino Airport Overlay include Medium Density Residential, Mixed Use, Business Park, Industrial, and Open Space – Recreation.

Projects accommodating TOP 2050 in this area would be required to meet the conditions of the Chino Airport Authority and the 2011 Caltrans Airport Land Use Planning Handbook, including those determining appropriate land uses, maximum population density, maximum site coverage, height restrictions, and required notification/disclosure areas based on the noise contours and runway protection, approach, and Part 77 zones of the FAA. Additionally, implementation of TOP 2050 would result in a beneficial impact for land use compatibility near Chino Airport as a result of the change from residential and business park to warehouse/industrial land uses.

The Airport Planning section of the TOP 2050 Land Use and Mobility Elements include policies that would ensure airport planning compatibility and consistency. These policies include:

- **LU-5.1: Coordination with Airport Authorities.** We collaborate with FAA, Caltrans Division of Aeronautics, airport owners, neighboring jurisdictions, and other shareholders in the preparation, update, and maintenance of airport-related plans.
- **LU-5.2: Airport Planning Consistency.** We coordinate with airport authorities to ensure The Ontario Plan is consistent with state law, federal regulations, and/or adopted master plans, and airport land use compatibility plans for ONT and Chino Airport.
- **LU-5.3: Airport Impacts.** We work with agencies to maximize resources to mitigate the impacts and hazards related to airport operations – their homes.
- **LU-5.4: ONT Growth Forecast.** We support and promote an ONT that accommodates 30 million annual passengers and 1.6 million tons of cargo per year, as long as the impacts associated with that level of operations are planned for and mitigated.
- **LU-5.5: Airport Compatibility Planning for ONT.** We create and maintain the Airport Land Use Compatibility Plan for ONT.
- **LU-5.6: Alternative Process.** We fulfill our responsibilities and comply with state law with regard to the Alternative Process for proper airport land use compatibility planning.
- **LU-5.7: ALUCP Consistency with Land Use Regulations.** We comply with state law that requires general plans, specific plans, and all new development be consistent with the policies and criteria set forth within an Airport Land Use Compatibility Plan for any public-use airport.
- **LU-5.8: Chino Airport.** We will support the creation and implementation of the Airport Land Use Compatibility Plan for Chino Airport.
- **M-5.2: Land Use Compatibility with Regional Transportation Facilities.** We work with ONT, railroads, Caltrans, SBCTA, and other transportation agencies to minimize impacts.

Therefore, TOP 2050 ensures compatibility with ONT and Chino Airport.

The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to the conflict of applicable plans compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to conflict with land use plans, policies, or regulations adopted for the purpose of avoiding or mitigating an environmental effect. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.11-11)

12. Mineral Resources

Impact 5.12-1: Project implementation would not result in the loss of availability of a known mineral resource. [Thresholds M-1 and M-2]

There are two areas in Ontario that are designated Mineral Resource Zone 2 (MRZ-2), where significant mineral resources are known or are likely. The remainder of the City is designated Mineral Resource Zone 3 (MRZ-3), where the significance of mineral deposits is unknown. Development in an MRZ-3 area would not result in significant impacts because mineral resources of statewide or local importance are not identified on the California Geological Survey's Production-Consumption maps.

Prior to permitting a use that would threaten the potential to extract minerals in an MRZ-2 area, the City of Ontario is required under the Surface Mining and Reclamation Act to prepare a statement specifying its reasons for permitting the proposed use and to consider the importance of these minerals to their market region as a whole and not just their importance to the City.

Mineral Resource Sectors D-3 and D-5 have been completely developed, as have portions of D-2. As shown in Figure 3-5, *Proposed Land Use Plan Map*, in Chapter 3, *Project Description*, of the Draft SEIR, the Proposed Project would not change the existing land use designations for these areas. Additionally, the parts of the City that are designated MRZ-2 but are outside of Mineral Resource Sectors are developed with urban uses and would continue to be designated for urban uses by TOP 2050. Areas designated MRZ-2 outside of Mineral Resource Sectors are not available for extraction of mineral resources, and the Proposed Project would not result in changes to the existing conditions of these areas. Therefore, the Proposed Project would not result in the loss of availability of a known mineral resource in Ontario, and impacts to mineral resources would be less than significant.

Additionally, TOP 2050 includes a goal to protect mineral-resource-extraction activities that are compatible with adjacent development (see also Policy ER-5.5). The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to availability of a known mineral resource and to the availability of locally-important mineral resource recovery site. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.12-6)

13. Noise⁵

Impact 5.13-2: Implementation of TOP 2050 would not result in long-term operation-related noise that would exceed established standards. [Threshold N-1]

Stationary Noise

The 2010 Certified EIR found TOP stationary-source noise impacts to be less than significant. TOP 2050 would also result in an increase in residential, commercial, industrial, and overall development and growth in Ontario. Primary stationary noise sources would be from landscaping, maintenance activities, air handline units (residential and commercial), and loading and unloading activities at commercial business parks and smaller retail stores. TOP 2050 would not result in new types of stationary noise sources than under the Approved Project. Furthermore, TOP 2050 includes Safety Element Policy S-4.1, Noise Mitigation, which utilizes the City's Noise Ordinance, building codes, and subdivision and development code regulations to reduce noise from future development projects. The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project. Impacts would be less than significant.

Transportation Noise

The 2010 Certified EIR found that traffic noise associated with the Approved Project would result in a substantial noise increase in the vicinity of noise-sensitive receptors that would exceed the City's noise standards; impacts were significant and unavoidable.

Table 5.13-10, *TOP 2050 Net Traffic Noise Level Increases*, in Section 5.13, *Noise*, of the Draft SEIR shows the ADT volumes for the Approved Project, the Proposed Project, and the net Community Noise Equivalent Level (CNEL) change along study roadway segments. A graphical representation of the future contours is shown on Figure 5.13-4, *Future Noise Contours*, in Section 5.13, *Noise*, of the Draft SEIR.

As shown in this table, traffic noise increases with implementation of the Proposed Project would be below the tiered thresholds. Traffic noise on State Street east of Vine Street, Sultana Avenue, and Campus Avenue is anticipated to increase by up to 3.3 A-weighted decibel (dBA) CNEL where the Approved Project ambient environment based on traffic noise modeling is between 63.7 and 67.2 dBA CNEL. However, these East State Street segments are parallel and adjacent to railroad tracks where ambient noise measurement LT-2 was conducted. The traffic noise model does not take into consideration other noise sources such as rail. Based on ambient noise monitoring, the existing noise environment at East State Street, east of Vine Street, Sultana Avenue, and Campus Avenue is 87 dBA CNEL (see Table 5.13-5, *Long-Term Noise Measurement Summary*, in Section 5.13, *Noise*, of the Draft SEIR). The traffic noise contribution would be negligible when compared to rail noise in this location. Therefore, the Proposed Project traffic noise level increase along this roadway segment would not result in the exceedance of the significance threshold.

⁵ Impacts 5.13-1, 5.13-3, and 5.13-4 are addressed in Section E, *Significant and Unavoidable Significant Impacts that Cannot be Mitigated to Below the Level of Significance*.

Additionally, TOP 2050 includes the following Safety Element Policies that would minimize traffic noise impacts:

- **S-4.2: Coordination with Transportation Authorities.** We collaborate with airport owners, FAA, Caltrans, SBCTA, SCAG, neighboring jurisdictions, and other transportation providers in the preparation and maintenance of, and updates to transportation-related plans to minimize noise impacts and provide appropriate mitigation measures.
- **S-4.4: Truck Traffic.** We manage truck traffic to minimize noise impacts on sensitive land uses.
- **S-4.5: Roadway Design.** We design streets and highways to minimize noise impacts.

The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project. Traffic noise impacts would be less than significant.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to long-term operation-related noise. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.13-32)

14. Population and Housing

Impact 5.14-1: TOP 2050 would directly result in population growth in the City of Ontario. [Threshold P-1]

The 2010 Certified EIR identified less than significant impacts associated with population and housing. One of the purposes of TOP 2050 is to adequately plan and accommodate future growth. Implementation of TOP 2050 accommodates population growth through land use designations, goals, and policies that provide a vision and guide growth in the City.

The proposed TOP includes minor changes in land use, with the majority of changes concentrated in four growth areas and the Ontario Ranch (defined as the area south of Riverside Drive and divided into the Ontario Ranch East and West by the Cucamonga Channel):

- Downtown Growth Area
- West Holt Growth Area
- East Holt Growth Area
- Ontario Airport Metro Center (OAMC)
- Ontario Ranch East
- Ontario Ranch West

Land use changes outside of these growth areas include converting shopping centers to mixed-use and increasing residential density in existing residential areas and religious properties. These land use changes are intended to improve growth areas by encouraging the use of alternative forms of transportation, promote healthier communities through land use planning that encourages walking and

biking, promote vibrant communities, put residents in proximity to resources (i.e., jobs, grocery stores, retail), and align growth with planned infrastructure improvements and regional transportation goals.

Table 5.14-8, *Buildout Comparison of Approved TOP to TOP 2050*, in Section 5.14, *Population and Housing*, of the Draft SEIR compares the buildout potential of TOP 2050 compared to the currently Approved Project. As shown in this table, TOP 2050 would increase population, dwelling units, and nonresidential buildings but would result in a small decrease in employment when compared to the Approved Project.

Table 5.14-9, *Buildout Comparison of Approved TOP and TOP 2050 to SCAG Projections*, in Section 5.14, *Population and Housing*, of the Draft SEIR compares the City's buildout projections for population, housing, and jobs to SCAG projections, and includes buildout projections under both the Approved Project and TOP 2050. SCAG projects the City to be jobs-rich, with a jobs-housing ratio of 2.2 in 2045. In comparison, the Proposed Land Use Plan under TOP 2050 would result in a slightly higher jobs-housing ratio of 2.3. TOP 2050 projections would represent a more balanced jobs-housing balance than the Approved Project, which would result in a jobs-housing ratio of 3.0. The City's jobs-housing ratio would therefore be more closely aligned to SCAG projections under TOP 2050 than under the Approved Project.

SCAG's Connect SoCal identifies several types of Priority Growth Areas in Ontario, including High-Quality Transit Areas, Transit Priority Areas, Neighborhood Mobility Areas, and Livable Corridors. TOP 2050 would promote growth consistent with these Priority Growth Areas, as proposed land use changes under TOP 2050 are intended to encourage walking and biking, put residents in proximity to resources, and align future growth in Ontario with planned infrastructure improvements and regional transportation goals. In addition, TOP 2050 includes several policies that promote strategic growth in support of sustainability goals.

- **LU-1.1: Strategic Growth.** We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, foster the development of transit, and support the expansion of the active and multimodal transportation networks throughout the City.
- **LU-1.2: Sustainable Community Strategy.** We integrate state, regional, and local Sustainable Community/Smart Growth principles into the development and entitlement process.
- **LU-1.3: Adequate Capacity.** We require adequate infrastructure and services for all development.
- **LU-1.5: Jobs-Housing Balance.** We coordinate land use, infrastructure, and transportation planning and analysis with the regional, county, and other local agencies to further regional and subregional goals for jobs-housing balance.
- **LU-4.3: Infrastructure Timing.** We require that the necessary infrastructure and services be in place prior to or concurrently with development.
- **H-2.1: Corridor Housing.** We revitalize transportation corridors by encouraging the production of higher density residential and mixed-uses that are architecturally, functionally, and aesthetically suited to corridors.

Although the increase in population, housing, and employment under TOP 2050 would exceed SCAG’s regional forecasts for the City of Ontario, TOP 2050 would improve the job-housing balance when compared to the Approved Project. Furthermore, TOP 2050 accommodates future growth by providing for infrastructure and associated public services to accommodate the projected growth of the City (see also Section 5.10, *Hydrology and Water Quality*, Section 5.15, *Public Services*, Section 5.17, *Transportation*, and Section 5.19, *Utilities and Service Systems*, of the Draft SEIR). Lastly, TOP 2050 is consistent with SCAG’s Connect SoCal (see Section 5.11, *Land Use and Planning*, of the Draft SEIR). Consequently, while buildout in accordance with the Proposed Land Use Plan would substantially increase both population and employment in the City, impacts would be less than significant.

The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to population growth. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.14-11)

Impact 5.14-2: Buildout of TOP 2050 would not displace people or housing and would not necessitate the construction of replacement housing. [Threshold P-2]

One of the purposes of TOP 2050 is to adequately plan and accommodate future growth through the distribution, location, balance, and extent of land uses. Implementation of TOP 2050 would accommodate population growth through land use designations, goals, and policies that provide a vision and guide growth in the City. Land use changes under the Proposed Land Use Plan would increase opportunities for housing in the City—for example, by converting shopping centers to mixed-use and increasing residential density in existing residential areas and religious properties. The Proposed Land Use Plan would provide land use designations for a variety of housing types and provide for additional residential opportunities throughout Ontario. TOP 2050 includes the following policies supporting an increase in the provision of housing and diversity of housing opportunities in the City:

- **H-2.4: Ontario Ranch.** We support a premier lifestyle community in the Ontario Ranch, distinguished by diverse housing, highest design quality, and cohesive and highly amenitized neighborhoods.
- **H-2.6: Infill Development.** We support the revitalization of neighborhoods through the construction of higher-density residential developments on underutilized residential and commercial sites.
- **ER-4.1: Land Use.** We reduce GHG and other local pollutant emissions through compact, mixed-use, and transit-oriented development and development that improves the regional jobs-housing balance.
- **CE-1.6: Diversity of Housing.** We collaborate with residents, housing providers, and the development community to provide housing opportunities for every stage of life; we plan for a

variety of housing types and price points to encourage the development of housing supportive of our efforts to attract business in growing sectors of the community while being respectful of existing viable uses.

The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to displacement of people and housing. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.14-12)

15. Public Services

Impact 5.15-1: The Ontario Fire Department would expand in response to the demand for fire protection facilities and personnel caused by the introduction of new structures, residents, and workers into the City's boundaries upon buildout of the Proposed Project. [Threshold FP-1]

The 2010 Certified EIR did not identify any significant impacts to fire service and facilities from implementation of the Approved Project. As shown in Table 3-4, *Comparison of Approved TOP to TOP 2050*, in Chapter 3, *Project Description*, of the Draft SEIR, the Proposed Project would result in an increase in development and population in comparison to the Approved Project, which would result in increased demand on fire protection services. Based on correspondence with the OFD, existing conditions would not be adequate to meet such increased demands from the Proposed Project or the Approved Project.

The OFD's recommendation is that three additional fire stations would be needed in the Ontario Ranch to meet projected needs while maintaining response times and meeting National Fire Protection Association recommendations for levels of service. Currently, the OFD has three potential focus areas in the Ontario Ranch for site acquisition that could support new fire stations, although these are not definitive at this time. While the construction of future facilities could result in potential environmental impacts, future environmental review would occur once specific locations have been determined. Without a definitive location for the development of future facilities, analysis of potential impacts is too speculative to conduct. Future projects would be reviewed by the City and the OFD on an individual basis and would be required to comply with requirements in effect at the time building permits are issued, including the payment of development impact fees that contribute to funding for additional staffing, facilities, and equipment. The Governance Manual of TOP 2050 is meant to bring collaboration between City departments, programs, and other involved agencies to achieve the City's development goals in phases, working within the budget and infrastructure constraints of the City. Following this process and similar to the Approved Project, sufficient revenue would be available for necessary service improvements to provide for adequate fire facilities, equipment, and personnel upon buildout of the Proposed Project, and impacts would be less than significant.

The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to fire protection services. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.15-3)

Impact 5.15-2: The Ontario Police Department would expand in response to the demand for police protection facilities and personnel caused by the introduction of new structures, residents, and workers into the City's boundaries upon buildout of the Proposed Project. [Threshold PP-1]

The 2010 Certified EIR identified less than significant impacts to police services and facilities. Buildout of the Proposed Project would result in increased population and development in comparison with the Approved Project, and would result in an increased demand on police protection services. According to correspondence with the Ontario Police Department (OPD) as part of this project, the OPD currently has enough staffing to meet current demands, but would require additional staffing as population increases to accommodate the Approved Project and Proposed Project. The OPD participates in mutual aid agreements with the San Bernardino County Sheriff and various jurisdictions surrounding Ontario to help participating jurisdictions when resources are inadequate to meet current service demands at a particular time.

Additionally, there are current plans to add a substation near the Entertainment District in Downtown Ontario, and the OPD anticipates needing to add a substation/multiuse facility in the Ontario Ranch. The development of these facilities would help to reduce impacts from increased population as part of the Proposed Project.

Future development under the Proposed Project would also be subject to development impact fees which pay for police services. Police services would receive adequate funding through the City's general fund to cover project needs, and the Governance Section of TOP 2050 would encourage collaboration between City departments, programs, and other involved agencies to achieve the City's development goals in phases that are within the fiscal and infrastructure limitations of the City. The police services required to cover the new development and population growth for Ontario would be assessed and acquired appropriately based on the needs of the City. It is possible that buildout of the Proposed Project would require additional facilities to support the OPD, the construction of which could result in potential environmental impacts. Such facilities would have to complete applicable environmental review under CEQA at that time, and locations and sizes of potential future facilities, if needed, is not known at this time. Without a definitive location for the development of future facilities, analysis of potential impacts is too speculative to conduct. Future projects would also be reviewed by the City of Ontario on an individual basis and required to comply with regulations in effect at the time building permits are issued. As with the Approved Project, the need for additional structures and personnel would be financed through the City's development impact fee program, and the impacts of the Proposed Project on police services would be less than significant.

Therefore, the Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to police protection services. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.15-7)

Impact 5.15-3: TOP 2050 would generate new students who would impact the school enrollment capacities of area schools, and construction of new schools and/or classroom facilities for additional students generated by buildout of the Proposed Project would be accommodated through assessment of school impact fees. [Threshold SS-1]

The 2010 Certified EIR identified impacts to school facilities and services as less than significant upon payment of SB 50 fees. As part of the development of this SEIR, the City reached out to the five school districts that serve residents of Ontario to obtain existing conditions information and information on potential impacts of the Proposed Project. Responses were received from four of the five school districts—Chaffey Joint Union High School District (CJUHSD), Chino Valley Unified School District (CVUSD), Mountain View School District (MVSD), and Ontario-Montclair School District (OMSD). Based on the responses received, CJUHSD schools have capacity to accommodate increased population projected as part of the Proposed Project. CVUSD and MVSD schools also have capacity to accommodate the Proposed Project. Current enrollment for all three of these school districts is below capacity, and the capacity of the schools in addition to any already planned construction projects would be able to accommodate the increased population of the Proposed Project.

The OMSD indicated that any increase in residential development will impact OMSD school facilities; however, further assessment would be needed to ensure accommodations for increased populations. While information provided by OMSD shows that most of its schools can accommodate the District’s projections for the next 10 years, some schools would not be able to accommodate projected increased capacity over the next 10 years. As such, it is possible that OMSD would need additional facilities by the horizon year of the Proposed Project, 2050; Cucamonga School District could be similarly impacted by increased student populations.

Each school district that serves the City of Ontario assesses its needs individually based on student generation rates from residential development, and charges development impact fees accordingly. Residential development in Ontario under the Proposed Project would require payments to corresponding school districts, which would go towards the construction of new facilities when and if they are needed. School districts determine their own development impact fees, often dependent on student generation rates for that district. These payments accommodate the need for new facilities based on the increase in student population in each district.

Developers would be required to pay the impact fees levied by each school district, set within the limits of SB 50. This funding program has been found by the Legislature to constitute “full and complete mitigation of the impacts” on the provision of adequate school facilities (Government Code Section 65995[h]). SB 50 establishes three potential limits for school districts, depending on the availability of new school construction funding from the state and the particular needs of the individual school districts. The school districts serving Ontario qualify for Level 1 fees, in which each district

justifies their development fees for each land use and cannot request payment of development fees for school facility construction exceeding the amount of the statutory fees expressed in Education Code Section 17620. If school districts conduct a school facility needs assessment and qualify for participation in the State Funding Program by the State Allocation Board, among meeting other requirements, they can be eligible for Level 2 and Level 3 Fees.

The majority of school districts within Ontario have existing capacity to accommodate the buildout and population increase of the Proposed Project. Although the increased demand on school facilities would have the potential to impact one or more of the school districts that serve Ontario, payment of impact fees in compliance with SB 50 would reduce the impacts to an acceptable level. Therefore, the Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to school services. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.15-14)

Impact 5.15-4: The Ontario library system would expand in response to the demand for library services and facilities and personnel caused by the introduction of new structures, residents, and workers into the City's boundaries upon buildout of the Proposed Project. [Threshold LS-1]

The 2010 Certified EIR identified less than significant impacts to library services and facilities. Based on the analysis for library services in the 2010 Certified EIR, the Approved Project would result in the Ontario library system not reaching its goal of 0.6 square feet of library facilities per capita. Based on information in the library's most recent Library Facility Master Plan, it currently still does not meet this standard. However, the Facility Master Plan does outline strategies for the library to expand services as population in Ontario continues to increase. While the Proposed Project projects to a horizon year of 2050, which is 15 years further than the Library Facility Master Plan currently projects to, the City's library system would continue to evaluate library needs based on facilities, staffing, and resources provided as population continues to increase. To allow more resources for the City's population, the Ontario library system also offers interlibrary exchanges with the Inland Library System as well as with participating libraries throughout the country.

TOP 2050 policies that reduce impacts of the Proposed Project on library services include the following:

- **SR-4.1: Community Needs.** We identify and monitor community needs for library services, technology, and facilities, and tailor them to effectively meet those needs.
- **SR-4.2: Interagency Coordination.** We leverage relationships with outside agencies, educational institutions, and neighboring jurisdictions to share the library resources to the benefit of Ontario residents.

- **SR-4.4: Coordination with other Community Services.** We coordinate library programs with other recreational and community programs and facilities.
- **SR-4.5: Focal Points of the Community.** We design and program Ontario's libraries as focal points of community engagement, including public outreach and community engagement.

Buildout of the Proposed Project would result in an increase in demand for library services in the City of Ontario based on an increase in population. New facilities, books, and personnel would be necessary to maintain and reach adequate levels of service. Environmental impacts could result from the construction of future facilities; however, the location and size of potential future facilities is currently unknown, and each project would have to complete applicable environmental review under CEQA when it is determined. Future projects would also be reviewed by the City of Ontario on an individual basis and would be required to comply with requirements in effect at the time building permits are issued (i.e., payment of development impact fees). Since adequate services would be provided and payment of development impact fees would offset the costs associated with library services, impacts on library services would be less than significant. The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to library services. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.15-18)

16. Recreation

Impact 5.16-1: Implementation of TOP 2050 would generate additional residents that would increase the use of existing park and recreational facilities, but park dedications and payment of in-lieu fees would ensure impacts are less than significant. [Threshold R-1]

The 2010 Certified EIR found that buildout of the Approved Project's Land Use Plan would generate additional residents, increasing the use of existing park and recreational facilities. However, it would not result in a significant impact, as development of park facilities would keep pace with the anticipated increase in population from buildout of the Approved Project.

Currently, the City of Ontario uses the established parkland standard of three acres per 1,000 residents but strives for five acres per 1,000 residents for parks in Ontario Ranch. The City has approximately 481 acres of parkland (see Table 5.16-1, *Recreational Facility Development Impact Fees*, in Section 5.16, *Recreation*, of the Draft SEIR). Based on a population of 179,597 (see Table 4-1 *City of Ontario Existing Land Use*, in Chapter 4, *Environmental Setting*, of the Draft SEIR), the City currently requires 539 acres of parkland.⁶

Buildout of TOP 2050 would generate additional residents in the City, most of whom would be concentrated in the southern portion of the City. Future growth in the City in accordance with buildout

⁶ (179,597 people / 1,000) x 3.0 acres per person = 538.79

of TOP 2050 would increase the demand for parks and increase existing park usage. The Quimby Act is a funding mechanism for parkland acquisition. Under this Act and pursuant to the City's Municipal Code, residential subdivisions must dedicate parkland or pay in lieu fees to enable the City to acquire a ratio of three acres of parkland per 1,000 residents. Based on this ratio and a projected buildout population of 357,957 by 2050, the current TOP would result in a demand of 1,074 acres of parkland.⁷ For the Proposed Project, which has projected buildout population of 410,492 by 2050, a total of 1,231 acres of parkland would be required at buildout.⁸ As a result, the Proposed Project would result in an increased demand of 158 acres of parkland compared to the Approved Project.

TOP 2050 addresses the need for recreation and parkland to preserve natural assets and environmentally sensitive lands. The Parks and Recreation Element contains relevant policies and programs to acquire additional parkland; integrate new park and recreation facilities with existing and future trails, bikeways, and easements; and conduct regular reviews and updates of the City's parks and trails plans to keep pace with demographic trends and recreational needs of Ontario's residents. The policies and regulations are intended to meet the TOP's standard parkland acreage ratio. However, the extent to which the City of Ontario can plan and implement parks, trails, and other recreational facilities is related to the availability of funding. TOP 2050 would designate approximately 900 acres for recreational uses under the Open Space – Recreation (OS-R) land use designation. The Proposed Land Use Plan includes designation of a community park, the Great Park, near Eucalyptus Avenue and a public park southwest of the intersection of Grove Avenue and Riverside Drive as OS-R.

Based on TOP 2050's future buildout projections, the 900 acres designated OS-R falls short of the City's existing Park Dedications and In-Lieu Fee Regulations for parkland acquisition by 331 acres.⁹ However, the City strives to have new development in Ontario Ranch provide an additional two acres per 1,000 residents for private parks in addition to the City's three acres per 1,000 residents for public parks. Ontario Ranch is estimated grow from the existing population of 22,286 to TOP 2050's population projection of 192,258, an increase of 169,972. This would mean an addition of approximately 340 acres of private parks.¹⁰ Additionally, there are at least 180 acres of regional recreational facilities, joint-use agreements with school districts, and private recreational opportunities providing services that cannot be accommodated by existing facilities. Prado Regional Park also provides approximately 2,000 acres that would offset recreational demands.

TOP 2050 provides land use opportunities for public parks to be developed in line with future development. The proposed Parks and Recreation Element contains relevant goals, policies, and programs that support a regular review of the City's parks and trails plans to keep pace with demographic trends and recreational needs of Ontario's residents (see Policies PR-1.1 through PR-1.16). In addition, under TOP 2050's Parks and Recreation Element, Policy PR-1.5 strives to provide five acres of parkland per 1,000 residents, and Policy PR-1.6 provides a minimum of two acres of developed private park space per 1,000 residents in addition to the three acres per 1,000 persons standard.

⁷ $(357,957 \text{ people} / 1,000) \times 3.0 \text{ acres per person} = 1,073.87$

⁸ $(410,492 \text{ people} / 1,000) \times 3.0 \text{ acres per person} = 1,231.48$

⁹ TOP 2050 parkland demand of 1,231.48 acres – 900 acres of parkland = 331.48

¹⁰ $(169,972 \text{ people} / 1,000) \times 2.0 \text{ acres per person} = 339.94$

As a result, development of park facilities would keep pace with the anticipated increase in population from buildout of TOP 2050.

The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to the use of existing park and recreational facilities compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to substantial physical deterioration of recreational facilities. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.16-14)

Impact 5.16-2: Project implementation would result in environmental impacts to provide new and/or expanded recreational facilities but would not result in a significant impact. [Threshold R-2]

The 2010 Certified EIR found that implementation of the Approved Project would result in environmental impacts from the provision of new and/or expanded recreational facilities, but impacts would not be considered significant.

TOP 2050 guides growth in development within the City and is not a development project. The Proposed Project includes expansion of the equestrian and hiking trails and improved bikeways throughout the City. The City has 481 acres of parkland, and buildout of TOP 2050 would provide 900 acres. Including the Great Park, TOP 2050 would result in an additional 419 acres of park facilities. As a result of these planned park facilities, TOP 2050 may result in the construction of new or expansion of existing recreational facilities in Ontario. The majority of these facilities would be in Ontario Ranch, including the Great Park. Development and implementation of the Great Park may have an adverse physical effect on the environment, such as lighting, biological resources, noise, traffic, etc. However, it is speculative to determine the location of proposed park facilities in the City and impacts arising from development of individual park projects. Existing federal, state, and local regulations as well as goals, policies, and actions in TOP 2050 would mitigate potential adverse impacts to the environment that may result from buildout of TOP 2050, including expansion of parks, recreational facilities, and multiuse trails. Furthermore, subsequent environmental review would be required for development of park projects under the Proposed Land Use Plan. Consequently, TOP 2050 would not result in significant impacts in this regard.

The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to the use of existing park and recreational facilities compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to construction or expansion of recreational facilities. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.16-14)

17. Transportation¹¹

Impact 5.17-1: The Proposed Project would not conflict with a program, plan, ordinance, or policy addressing the circulation system, including transit, roadway, bicycle, and pedestrian facilities. [Threshold T-1]

The 2010 Certified EIR found that the recommended circulation plan of the Approved Project would comply with adopted policies, plans, and programs for alternative transportation. TOP 2050 includes additional goals and policies to address alternative transportation systems. Section 5.11, *Land Use and Planning*, of the Draft SEIR includes a consistency analysis with SCAG's Connect SoCal.

Transit

The Public Transit Plan (see Figure 5.17-2, *Public Transit*, in Section 5.17, *Transportation*, of the Draft SEIR) is focused on providing efficient connectivity and integration via coordinated bus transfer centers and multimodal terminals. Elements identified include collaborating with regional transit agencies to provide for more extensive and frequent basic local bus service, higher-speed bus rapid transit corridors for longer trips, more Metrolink trains in all directions, convenient transfer centers, and future land use patterns that are more suitable for transit users.

Additional alternative transportation elements include support for rail or high-speed rail systems and supporting feeder and distribution systems to move people to and from the rail stations.

TOP 2050 includes the following policies to encourage and provide access to the regional transit network.

- **M-3.1. Transit Partners.** We maintain a proactive working partnership with transit providers to ensure that adequate public transit service is available, cost-efficient, and convenient, particularly for residents in environmental justice areas.
- **M-3.2. Alternative Transit Facilities at New Development.** We require new development adjacent to an existing or planned transit stop to contribute to the creation of transit facilities, such as bus shelters, transit bays and turnouts, and bicycle facilities, such as secure storage areas.
- **M-3.3. Transit-Oriented Development.** We may provide additional development-related incentives to those inherent in the Land Use Plan for projects that promote transit use and reduce vehicle miles traveled.
- **M-3.4. Bus Rapid Transit (BRT) Corridors.** We work with regional transit agencies to implement BRT service and reduce vehicle miles traveled by targeting destinations and corridors with the highest number of potential riders.
- **M-3.5. Light Rail.** We support the extension of the Metro Rail Gold Line to Ontario, and will work to secure station locations at the proposed multimodal transit center.

¹¹ Impact 5.17-2 is addressed in Section E, *Significant and Unavoidable Significant Impacts that Cannot be Mitigated to Below the Level of Significance*.

- **M-3.6. Metrolink Expansion.** We advocate expansion of Metrolink service to include the Downtown and the multimodal transit center.
- **M-3.7. High-Speed Rail.** We encourage the development of high-speed rail systems that would enhance regional mobility in Southern California and serve the City of Ontario.
- **M-3.8. Feeder Systems.** We work with regional transit agencies to secure convenient feeder service from the Metrolink station and the proposed multimodal transit center to employment centers in Ontario.
- **M-3.9. Ontario Airport Metro Center Circulator.** We will explore the development of a convenient mobility system, including but not limited to shuttle service, people mover, and shared car system, for the Ontario Airport Metro Center.
- **M-3.10. Multimodal Transportation Center.** We intend to ensure the development of a multimodal transportation center near ONT airport to serve as a transit hub with amenities for transit riders, pedestrians, and bicyclists transitioning to local buses, BRT, the Gold Line, high-speed rail, the proposed Ontario Airport Metro Center Circulator, and other future transit modes. We support locations for the multimodal transportation center that are north of ONT airport, between Vineyard Avenue and Interstate 15.
- **M-3.11. Transit and Community Facilities.** We require the future development of community-wide serving facilities to be sited in transit-ready areas that can be served and made accessible by public transit. Conversely, we plan (and coordinate with other transit agencies to plan) future transit routes to serve existing community facilities.

Therefore, implementation of TOP 2050 would not interfere with or obstruct the implementation and usage of transit systems.

Nonmotorized Transportation

TOP 2050 would create a comprehensive system of on- and off-street bikeways that connect residential areas, businesses, schools, parks, and other destination points (see Figure 5.17-4, *Multipurpose Trails & Bikeways*, in Section 5.17, *Transportation*, of the Draft SEIR). The recommended strategies and approaches for transit and nonmotorized transportation would expand alternative transportation options in Ontario (see TOP 2050 Mobility Element). The City's goal is to provide an off-street multipurpose pedestrian and bicycle trail system, a Class II on-street striped bicycle system, and a Class III on-street signed bicycle system. The Class III bikeways would be used to connect multipurpose trails and Class II bikeways. In addition, development of mixed-use areas would provide more walkable communities and would require infrastructure improvements that encourage both walking and bicycle trips. Furthermore, mixed-use developments would reduce the distance traveled between services and amenities, and higher density areas would better utilize public transit and nonmotorized transportation due to the critical mass required to make these viable options for people. Overall, integrating these two approaches to transit and nonmotorized transportation in conjunction with the development of mixed-use areas would contribute to reducing VMT in Ontario.

TOP 2050 includes the following policies to enhance connectivity to the City's nonmotorized transportation network:

- **M-1.4. Complete Streets.** We work to provide a complete, balanced, context-aware, multimodal transportation network that meets the needs of all users of streets, roads, and highways, including motorists, pedestrians, bicyclists, children, persons with disabilities, seniors, movers of commercial goods, and users of public transportation. We prioritize implementation of complete streets improvements in environmental justice areas to facilitate opportunities for residents to use active transportation systems.
- **M-2.1. Active Transportation.** We maintain our Active Transportation Master Plan to create a comprehensive system of on- and off-street bikeways and pedestrian facilities that are safe, comfortable, and accessible and connect residential areas, businesses, schools, parks, and other key destination points.
- **M-2.2. Bicycle System.** We provide off-street multipurpose trails and Class II bikeways as our preferred paths of travel and use the Class III for connectivity in constrained circumstances. When truck routes and bicycle facilities share a right-of-way we prefer Class I or Class IV bicycle facilities. We require new development to include bicycle facilities, such as bicycle parking and secure storage areas.
- **M-2.3. Pedestrian Walkways.** We require streets to include sidewalks and visible crosswalks at major intersections where necessary to promote safe and comfortable mobility between residential areas, businesses, schools, parks, recreation areas, and other key destination points.
- **M-2.4. Network Opportunities.** We use public rights-of-way and easements, such as utility easements, levees, drainage corridors, road rights-of-way, medians, and other potential options to maintain and expand our bicycle and pedestrian network. In urban, mixed-use, and transit-oriented Place Types, we encourage the use of underutilized public and private spaces to expand our public realm and improve pedestrian and bicycle connectivity.

Therefore, implementation of TOP 2050 would not interfere with or obstruct the implementation and usage of nonmotorized transportation.

Connect SoCal

TOP 2050 is consistent with Connect SoCal, as shown in Section 5.11, *Land Use and Planning*, of the Draft SEIR. The goals of the Regional Transportation Plan/Sustainable Communities Strategy (RTP/SCS) focus on transit, transportation and mobility, and protection of the environment and health of residents.

TOP 2050 proposes the following changes to the Mobility Element that have the potential to affect infrastructure projects identified in the RTP:

- **RTP ID 4A04220.** *Widen Schaefer Ave from Euclid Avenue to Haven Avenue from zero to four lanes.* TOP 2050 would eliminate the bridge connection on Schaefer Avenue between Ontario Avenue and Archibald Avenue. A VMT forecast with and without removal of the Schaefer Bridge was

conducted with the Boundary Method to determine whether this change would increase or decrease VMT. The results indicated that VMT in Ontario would decrease by 8,729 VMT per weekday within the city limits. This indicates that removing this project would result in a benefit to VMT in the City.

- **RTP ID 4160063.** *Widen State Street from Bon View Avenue to Grove Avenue from two to four lanes.* TOP 2050 would retain State Street as a two-lane facility.
- **RTP ID 4A07327.** *Widen four-lane bridge on Holt Boulevard over Cucamonga Creek to six lanes.* TOP 2050 would retain Holt Boulevard as a four-lane facility east of Vineyard Avenue.
- **RTP ID 2002160-20150201.** *Widen Grove Avenue between Fourth Street and State Street/Airport Drive from four to six lanes.* TOP 2050 would retain Grove Avenue as a four-lane facility north of State Street.

The Proposed Project would not result in a new or a substantial increase in magnitude of impacts related to consistency with transit, bicycle, and pedestrian plans compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to conflict with a program, plan, ordinance, or policy addressing the circulation system. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.17-23)

Impact 5.17-3: The Mobility Element adequately addresses potentially hazardous conditions (sharp curves, etc.), potential conflicting uses, and emergency access. [Thresholds T-3 and T-4]

The 2010 Certified EIR found that circulation improvements under the recommended circulation plan would be designed to adequately address potential hazardous conditions, potential conflicting uses, and emergency access.

The majority of the population growth associated with TOP 2050 would occur in Ontario Ranch. As identified in the City's Roadway Classification map (see Figure 5.17-3, *Roadway Classification*, in Section 5.17, *Transportation*, of the Draft SEIR), there is substantial improvements in transportation infrastructure planned to accommodate the increase in population in the City in the event of an emergency. The City has adopted roadway classification standards in Policy M-1.1 that include roadway design standards as part of TOP 2050, precluding the construction of any unsafe features.

- **M-1.1. Roadway Design and Maintenance.** We require our roadways to: 1) Comply with federal, state, and local design and safety standards; 2) Meet the needs of multiple transportation modes and users; 3) Handle the capacity envisioned in the City of Ontario Master Plan of Streets and Highways; 4) Be maintained in accordance with best practices; 5) Be compatible with the streetscape and surrounding land uses; and 6) Promote the efficient flow of all modes of traffic through the implementation of intelligent transportation systems and travel demand implementation strategies.

Additionally, a review of emergency access is included as part of the City's Design Review process. According to the City's 2018 LHMP, interstate highways would serve as major emergency response and evacuation routes (see Figure 5.17-5, *Evacuation Routes*). Additionally, the OFD reviews development applications to ensure that adequate emergency accessibility is provided based on local and state guidance.

The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to transportation hazards and emergency access compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to increased hazards due to a geometric design feature or incompatible uses and to inadequate emergency access. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.17-26)

18. Tribal Cultural Resources¹²

19. Utilities and Service Systems

Impact 5.19-1: Project-generated wastewater could be adequately treated by the wastewater service provider for the project and would not require the construction of new wastewater treatment facilities or the expansion of existing facilities or exceed wastewater treatment requirements of the Regional Water Quality Control Board. [Thresholds U-1, U-2 (part), and U-5]

The 2010 Certified EIR concluded that the Approved Project would generate additional wastewater, which would be adequately treated in accordance with the Santa Ana RWQCB and California Department of Public Health requirements.

The Proposed Project would result in an overall increase in the number of residential dwellings and nonresidential square footage compared to the Approved Project. The breakdown for the increases in wastewater flows is provided in Table 5.19-3, *Projected Wastewater Flow Rates*, in Section 5.19, *Utilities and Service Systems*, of the Draft SEIR.

Sewer Infrastructure

The Proposed Project would have the potential to increase sewer flows by 2.55 mgd within the City and by 166 mgd in the growth areas. The largest increase in wastewater flow rates would be in the OAMC at 18 percent; the second highest increase would be in the Ontario Ranch Great Park Corridor at 10 percent.

There are four proposed capital improvement projects in two of the growth areas:

¹² Impact 5.18-1 is addressed in Section D, *Findings on Significant Environmental Impacts that Can be Reduced to a Less than Significant Level*.

- **East Holt Corridor.** One project at the intersection of Holt Boulevard and Grove Avenue consists of replacing existing pipe with 12-inch and 15-inch pipe. The other project is construction of a new 21-inch sewer main to divert flow from Vineyard Avenue.
- **Ontario Airport Metro Center.** One project south of I-10 and west of Archibald Avenue would involve upsizing 8-inch diameter pipes to 12-inch pipes. The other project is along Old Guasti Road between Turner Avenue and Archibald Avenue and would upsize 8-inch pipes to 12-inch diameter pipes.

For the Ontario Ranch, sewer flows are anticipated to increase by 10 percent when comparing the current TOP to the proposed TOP. The City has confirmed that the sewer infrastructure in this area has been sized to accommodate sewer flows associated with the TOP 2050. Therefore, no adverse impacts on sewer infrastructure is anticipated in this area.

Additionally, the TOP 2050 has policies in place to require improvements to sewer infrastructure as part of new development and redevelopment projects and has processes in place to ensure that any sewer improvement projects are implemented prior to or during new development (LU-1.3, LU-4.3, and ER-1.8). The City also has the discretion to require additional sewer capacity studies for new development and redevelopment and is currently performing site-specific studies of certain areas of the sewer system. Based on the results of these studies, the City can require development fees to fund infrastructure improvements that are required for the proposed new developments.

The City regularly updates its Sewer Master Plan and capital improvement program (CIP) and has a process to assess local sewer impacts on a project-by-project basis. The draft 2020 Sewer Master Plan serves as an infrastructure planning tool to make decisions as to when CIP projects are warranted. The OMUC regularly provides and prioritizes sewer projects for inclusion in the latest CIP, which includes a budget for wastewater infrastructure improvements over a five-year planning horizon.

In summary, the City's wastewater collection system is adequate to convey the additional 2.55 mgd that would occur with implementation of the Proposed Project. The City has indicated that the sewer infrastructure will be able to accommodate sewer flows associated with the TOP 2050. A description of proposed regional and City improvements is provided in Tables 5.19-1, *IEUA Capital Projects*, and 5.19-2, *2021 Draft Sewer Master Plan: Recommended Capacity Improvement Projects*, in Section 5.19, *Utilities and Service Systems*, of the Draft SEIR. With funding from sewer connection/usage fees and the CIP budget, the City would continue to expand and improve the sewer infrastructure to accommodate new development and future growth. Therefore, there would be no significant impacts on wastewater infrastructure.

Wastewater Treatment Capacity

With respect to wastewater treatment, IEUA has two facilities that serve the City of Ontario: RP-1 and RP-5. The current combined capacity of these two facilities is 60.3 mgd and would increase to 66.5 mgd once the expansion project that is currently under construction at RP-5 is completed. IEUA can route flows to either of the two facilities as needed. In 2020, RP-1 treated an average wastewater flow of 25 mgd, and RP-2 treated an average wastewater flow of 8 mgd. Therefore, these two wastewater treatment plants (WWTPs) have a current combined treatment rate of 33 mgd. Current and future

WWTP capacities are summarized in Table 5.19-4, *IEUA WWTP Flow Rates and Capacities*, in Section 5.19, *Utilities and Service Systems*, of the Draft SEIR.

The excess capacity for the two facilities is 27.3 mgd under current conditions. After the expansion project at RP-5 is complete in 2025, excess capacity would increase to 33.5 mgd. There are additional plans to increase the treatment capacity of RP-5 to 30 mgd by 2040; the combined treatment capacity of both WWTPs would be 74 mgd.

The 2021 wastewater flow rate for Ontario is estimated to be 10.4 mgd, and the wastewater flow rate for the TOP 2050 buildout is estimated to be 37.1 mgd. The additional flow with implementation of the Proposed Project would be 26.7 mgd (37.1 – 10.4). Since the excess capacity of the two WWTPs in 2025 is 33.5 mgd, the additional flow rate from the Proposed Project of 26.7 mgd would not exceed the capacity of the wastewater treatment providers.

In addition, IEUA has seen a decrease in the volume of sewage flows of approximately 10 percent since 2013, even as the population has increased. This is a result of a decrease in indoor water consumption with the installation of more efficient plumbing fixtures and compliance with CALGreen Building Standards Code for new developments. IEUA projects a significant increase in the growth of its service area in the next ten years, with 40 percent of the growth resulting from new development in Ontario. The projected increase in population growth rates and corresponding increase in wastewater flows have been accounted for in IEUA's capital improvement projects, with expansions of both RP-1 and RP-5 scheduled for completion by 2035.

IEUA also assesses monthly wastewater sewer fees and one-time sewer connection fees to provide funds for future upgrades and expansion of its infrastructure and WWTPs. In addition, IEUA continually updates its Wastewater Facilities Master Plans for RP-1 and RP-5 and includes plans for expansion of these facilities to meet the growth within the service area through year 2060.

IEUA uses an average factor of 270 gpd per equivalent dwelling unit (EDU) in its projection of wastewater flows. Recent flow measurements indicate that with water conservation efforts and compliance with the CALGreen Building Standards Code for new construction, actual flow rates are now around 200 gpd/EDU. Therefore, even with future increases in population and wastewater flows in the service area, IEUA could continue to provide wastewater treatment to all its customers.

The quality of wastewater is overseen by two agencies: the Santa Ana RWQCB and the California Department of Public Health. The Santa Ana RWQCB has regional permitting authority over water quality issues, and the California Department of Public Health oversees standards and health concerns. Title 22 of the California Code of Regulations provides the regulatory setting for drinking water quality in California and is followed by these agencies when they assess water quality. The wastewater treated in IEUA's regional plants meets or exceeds the standards for recycled water quality set by Title 22. RP-1 and RP-5 would continue to meet the water quality standards of the Santa Ana RWQCB and the California Department of Public Health as well as the wastewater discharge limitations in the RWQCB NPDES permit. Therefore, there would be no significant impacts on wastewater collection and treatment.

The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to project-generated wastewater, to construction and/or expansion of wastewater treatment facilities, and to wastewater treatment requirements. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.19-11)

Impact 5.19-2: Water supply and delivery systems are adequate to meet project requirements. [Thresholds U-2 (part) and U-4]

The 2010 Certified EIR concluded that the Approved Project would have less than significant impacts associated with water storage and/or supply.

Water Infrastructure

The Proposed Project would result in an overall increase in the number of residential dwelling units and nonresidential square footage compared to the Approved Project. The breakdown for the increase in water demand is provided in Table 5.19-9, *Projected Water Demand Rates*, in Section 5.19, *Utilities and Service Systems*, of the Draft SEIR. Full implementation of TOP 2050 has the potential to increase water demand by 1.8 mgd in the growth areas and by 3 mgd in the City overall. This accounts for an increase of approximately 6 percent from the current TOP to TOP 2050. The growth areas represent about 60 percent of the proposed increased in water demand. The largest increase in water demand is in the OAMC.

To accommodate needed infrastructure expansion and improvements, the City has prepared a Water Master Plan (WMP) and CIP. It also requires development impact fees and has construction requirements based on a completed evaluation of existing and projected water demands. The potable water demand factors in the WMP are conservative and are used for sizing water pipes to convey average and peak daily flow rates. Therefore, they do not reflect the State's requirements to reduce residential indoor water demand to 55 gallons/person/day by 2025 and 50 gallons/person/day after 2030. An extensive list of planned capital improvement projects is provided in the WMP.

Because the planned development in the City for the current TOP and TOP 2050 would result in an increase in demand for potable and recycled water, the City and the IEUA have made plans for infrastructure expansion and improvement. As part of the land development approval process, the City determines a project's fair-share costs and connection fees. Through the use of connection fees and agreements, the City maintains and expands its water distribution system as necessary and is able to ensure that new developments pay their fair-share costs. The City has the discretion to require water capacity studies associated with new development and redevelopment and currently requires site-specific studies to determine a project's impact throughout the water system. Therefore, impacts related to infrastructure expansion and improvement caused by the implementation of TOP 2050 would be less than significant.

Water Demand

Total water demand associated with the Proposed Project would be 78,128 acre-feet per year (afy), which is an increase of approximately 6 percent compared to the City's 2020 Urban Water Management Plan (UWMP) calculated water demand of 73,688 afy for the years 2040 and 2045. The 2020 UWMP considered the buildout projections for the current TOP; therefore, the increase of 6 percent is the same as comparing the current TOP to TOP 2050.

The water demand factors used in the 2020 UWMP are conservative because they are based on the City's water demand factors from the WMP, which don't consider declining per capita water use in future years, the City's continuing conservation efforts, and the increased use of recycled water. Recent State laws, changes in the building code, and water service costs are anticipated to substantially lower water demand rates in the future:

- SB 606 and AB 1668 establish indoor water use standards (55 gallons/person/day until 2025, 50 gallons/ person/day after 2030), outdoor water standards, and water loss standards that water suppliers must meet by 2025.
- New construction is subject to the latest CALGreen building code, which typically results in a 20 percent reduction in indoor water use.
- SB 407 requires all buildings in California to meet current plumbing fixture standards within this decade, which will require retrofitting of existing homes and businesses.
- Increases in water service costs, which will provide an incentive for additional water-saving practices and the use of recycled water, which is less costly, for nonpotable uses.

Applying these more conservative water demand factors by implementing a future water demand factor of 50 gallon/person/day for all residential uses and a 20 percent reduction in nonresidential uses (compliance with CALGreen and new building code requirements), the calculated water demand for the Proposed Project buildout would be approximately 60,000 afy, which is well below the UWMP estimate of 73,688 afy for the years 2040 and 2045. The calculations to support the reduced water demand are provided in Appendix G.

In addition, when a proposed project triggers the criteria for preparing a water supply assessment, such as a residential project with more than 500 dwelling units, the project must demonstrate that adequate supplies of water are available to meet the demand of the new development. Also, the mitigation measures from the 2010 Certified EIR have been incorporated into the City's policies and municipal code.

TOP 2050 policies LU-1.3 and LU-4.3 ensure that the infrastructure and services for all development are adequate and that the necessary infrastructure and services are in place prior to or concurrent with development. The goal of Policy ER-1.1 is to increase local water supplies to reduce dependence on imported water. Policy ER-1.2 states the water supply and quality should match the appropriate use and Policy ER-1.3 requires conservative strategies that reduce water usage. Policy ER-1.4 requires that water supply and demands be balanced and ER-1.5 relates to water quality protection, pollution

prevention, and existing contamination and remediation. Policy S-3.7 requires monitoring the water supply system to ensure that there are adequate supplies for firefighting needs.

The 2020 UWMP states that there are sufficient water supplies through 2045 to meet projected demands in normal years, single dry years, and multiple dry years through 2045. Although the Proposed Project at buildout is estimated to generate a 6 percent increase in water demand using conservative water demand factors, new State requirements as listed above and City policies and code requirements would result in enhanced water efficiency and conservation. Applying these measures to the Proposed Project water demand estimates would result in total water demand below the projections in the 2020 UWMP for year 2045. Therefore, there would be sufficient water supplies to meet the demand for TOP 2050 buildout.

The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to water supply and delivery systems. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.19-31)

20. Wildfire

Impact 5.20-1: The Mobility Element adequately addresses emergency access. [Threshold W-1]

The 2010 Certified EIR found that circulation improvements under the recommended circulation plan would be designed to adequately address emergency access.

The majority of the population growth associated with TOP 2050 would occur in Ontario Ranch. As identified in the City's Roadway Classification map (see Figure 5.17-3, *Roadway Classification*, in Section 5.17, *Transportation*, of the Draft SEIR), there is substantial improvements in transportation infrastructure planned to accommodate the increase in population in the City in the event of an emergency. The City has adopted roadway classification standards in Policy M-1.1 that include roadway design standards as part of TOP 2050, precluding the construction of any unsafe features.

- **M-1.1: Roadway Design and Maintenance.** We require our roadways to: 1) Comply with federal, state, and local design and safety standards; 2) Meet the needs of multiple transportation modes and users; 3) Handle the capacity envisioned in the City of Ontario Master Plan of Streets and Highways; 4) Be maintained in accordance with best practices; 5) Be compatible with the streetscape and surrounding land uses; and 6) Promote the efficient flow of all modes of traffic through the implementation of intelligent transportation systems and travel demand management strategies.

Additionally, a review of emergency access is included as part of the City's Design Review process. According to the City's 2018 LHMP, interstate highways would serve as major emergency response

and evacuation routes. Additionally, the OFD reviews development applications to ensure that adequate emergency accessibility is provided based on local and state guidance.

The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to transportation hazards and emergency access compared to the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to impairment of an adopted emergency response plan or emergency evacuation plan. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.20-15)

Impact 5.20-2: TOP 2050 would not result exacerbate wildfire risks or expose people or structures to significant risks that may occur following a wildfire (e.g., landslides, mudflows, and flooding). [Thresholds W-2, W-3, and W-4]

The 2010 Certified EIR found that the Approved Project would result in less-than-significant risks from wildfire hazards. The City is outside of the State Responsibility Area, and CAL FIRE has determined that the City contains no areas subject to very high wildfire risk. However, the City recognizes that even though fuel loading is light in Ontario and fire risk comes primarily from urban fires, not wildfires, there is some risk related to wildfires.

There are many resources available to address wildland fires should they arise—CAL FIRE’s *2019 Strategic Fire Plan for California*, the CFC, County of San Bernardino MJHMP, City of Ontario LHMP, and fire services from the OFD. With adherence to these building practices, development and infrastructure associated with TOP 2050 would not exacerbate risk or result in post-wildfire hazards (e.g., landslides, mudflows, and flooding).

In addition, the TOP 2050 contains the following policies to prevent wildfire hazards and support the community during wildfire events:

- **S-3.4: Special Team Services.** We maintain effective special rescue services.
- **S-3.6: Interagency Cooperation.** In order to back up and supplement our capabilities to respond to emergencies, we participate in the California Fire Rescue and Mutual Aid Plan.
- **S-3.8: Fire Prevention through Environmental Design.** We require new development to incorporate fire prevention considerations in the design of streetscapes, sites, open spaces, and buildings.
- **S-3.9: Resource Allocation.** We analyze fire data to evaluate the effectiveness of our fire prevention and reduction strategies and allocate resources accordingly.
- **S-8.3: Emergency/Disaster Training and Exercises.** We conduct training and exercises to prepare for and evaluate emergency/disaster response and recovery procedures.
- **S-8.5: Interdepartmental Coordination.** We utilize all City departments to help support emergency/disaster mitigation, preparedness, response, mitigation, and recovery.

- **CD-2.8: Safe Design.** We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintaining visibility and accessibility, and using lighting.

The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to that of the Approved Project.

Finding. The Proposed Project would have a less than significant direct, indirect, and cumulative impacts relating to wildfire risks. Accordingly, no changes or alterations to the Proposed Project were required to avoid or substantially lessen any significant environmental impacts under those thresholds. (Draft SEIR pg. 5.20-16)

C. FINDINGS ON SIGNIFICANT ENVIRONMENTAL IMPACTS THAT CAN BE REDUCED TO A LESS THAN SIGNIFICANT LEVEL

The following summary describes impacts of the Proposed Project that, without mitigation, would result in significant adverse impacts. The City Council hereby finds that Mitigation Measures have been identified in the EIR and these Findings that will avoid or substantially lessen the following potentially significant environmental impacts to a less than significant level. Upon implementation of the mitigation measures provided in the EIR, these impacts would be considered less than significant.

1. Cultural Resources

Impact 5.5-2: Implementation of TOP 2050 could impact archaeological resources. [Threshold C-2]

The 2010 Certified EIR found that buildout of the Approved Project could impact archeological resources or paleontological resources. However, the 2010 Certified EIR identified that existing federal, state, and local regulations in addition to Mitigation Measure 5-2 would reduce impacts to archeological resources to less than significant.

As previously discussed, the records review at the South Central Coastal Information Center identified 17 archeological resources in the City. Based on the results of the research, there is potential archaeological sensitivity throughout the City.

Adoption of TOP 2050 would not directly affect archaeological resources. TOP 2050 is a regulatory document that sets the framework for future growth and development but does not result in development in and of itself. However, long-term implementation of TOP 2050 land use plan could allow development (e.g., new development, infill development, redevelopment, and revitalization/restoration), including grading, of known and unknown sensitive areas. Grading and construction activities of undeveloped areas or redevelopment that requires more intensive soil excavation than in the past could potentially cause the disturbance of archeological resources. Therefore, future development that would be accommodated by TOP 2050 could potentially unearth previously unrecorded resources.

Existing federal, state, and local regulations address the provisions of studies to identify archaeological resources, review application for projects that would potentially involve land disturbance, provide a project-level standard condition of approval that addresses unanticipated archeological discoveries and enforces requirements to develop specific mitigation measures if resources are encountered during any development activity. The Historic Preservation section of TOP 2050 Community Design Element addresses the management of artifacts through Policy CD-4.1, Cultural Resources Management, and the collaboration and promotion of public involvement in preservation through Policies CD-4.2, Collaboration with Property Owners and Developers; CD-4.6, Promotion of Public Involvement in Preservation; and CD-4.7, Public Outreach.

Archaeological sites are also protected by a wide variety of state policies and regulations under the California Public Resources Code. Cultural resources are recognized as nonrenewable and therefore receive protection under the California Public Resources Code and CEQA. Review and protection of archaeological resources are afforded by CEQA for individual development projects accommodating TOP 2050, subject to discretionary actions that are implemented in accordance with the land use plan of TOP 2050. According to CEQA, the lead agency is required to determine whether a development project may have a significant effect on archaeological resources (PRC Section 21083.2). If the lead agency determines that the project may have a significant effect on unique archaeological resources, the project-level CEQA document being prepared for the development project is required to address and mitigate the impacts of those resources.

The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to archeological resources compared to the Approved Project.

Mitigation Measure

The following mitigation measure was included in the Draft SEIR and the Final SEIR in an abundance of caution, and is applicable to the Proposed Project. The measure as provided includes any revisions incorporated in the Final SEIR.

MM 5-2 In areas of documented or inferred from evident archaeological and/or paleontological resource presence, City staff shall require applicants for development permits to provide studies to document the presence/absence of such resources. On properties where resources are identified, such studies shall provide a detailed mitigation plan, including a monitoring program and recovery and/or in situ preservation plan, based on the recommendations of a qualified cultural preservation expert. The mitigation plan shall include the following requirements:

- a) Archaeologists and/or paleontologist shall be retained for the project and will be on call during grading and other significant ground-disturbing activities.
- b) Should any cultural resources be discovered, no further grading shall occur in the area of the discovery until the Planning Director or designee is satisfied that adequate provisions are in place to protect these resources.

- c) Unanticipated discoveries shall be evaluated for significance by a San Bernardino County Certified Professional Archaeologist/Paleontologist. If significance criteria are met, then the project shall be required to perform data recovery, professional identification, radiocarbon dates, and other special studies; submit materials to a museum for permanent curation; and provide a comprehensive final report including a catalog with museum numbers.

Finding

Changes or alterations have been required in, or incorporated into, the Proposed Project that avoid or substantially lessen the significant environmental effect as identified in the Draft SEIR. These changes are identified in the form of the mitigation measure above. The City of Ontario hereby finds that implementation of the mitigation measure is feasible, and the measure is therefore adopted.

Rationale for Finding

Mitigation Measure 5-2 would require preservation and curation of archeological resources if uncovered during development. Mitigation Measure 5-2 would reduce potential impacts to archeological resources to a level that is less than significant. Impact 5.5-2 would be less than significant with mitigation. (Draft SEIR pg. 5.5-23)

2. Geology and Soils

Impact 5.7-6: Implementation of TOP 2050 could directly or indirectly destroy a unique paleontological resource. [Threshold G-6]

The 2010 Certified EIR found that buildout of the Approved Project would not result in impacts to paleontological resources with mitigation.

Geologic Features

The geologic units exposed at the surface in Ontario consist of sediments less than 11,000 years old (Holocene) deposited either by water or wind. Such geology is common throughout the City and region and is not considered unique.

Paleontological Resources

Ontario is underlain by deposits of Quaternary and upper-Pleistocene sediments deposited during Pliocene and early Pleistocene time. Quaternary Older Alluvial sediments may contain significant, nonrenewable, paleontological resources and are therefore considered to have high sensitivity. Older Pleistocene alluvial sediments can yield fossil remains, often found at depths of 10 feet or more below existing ground surface. As previously discussed, for the Approved TOP, the San Bernardino County Museum, Division of Geological Sciences, conducted the paleontological records search and found one previously known paleontological resource locality recorded by the Regional Paleontologic Locality Inventory, a computer database with positional and contextual data for more than 3,000 fossil localities throughout California and the southwestern United States. This review found one paleontological locality for the City area (SBCM 5.1.8). This locality yielded the remains of a mammoth

from approximately 20 feet below the ground surface. As a result, the possibility of finding additional paleontological resources within City boundaries is moderate to high at depths of 10 feet or more below ground surface.

Long-term implementation of TOP 2050 would allow development (e.g., new development, infill development, redevelopment, and revitalization/restoration), including grading, of known and unknown sensitive areas. Grading and construction activities of undeveloped areas or redevelopment that requires more intensive soil excavation than in the past could potentially cause the disturbance of paleontological resources. Therefore, future development that would be accommodated by TOP 2050 could potentially unearth previously unrecorded resources.

Existing federal, state, and local regulations address the provisions of studies to identify paleontological resources, review applications for projects that would potentially involve land disturbance, provide a project-level standard condition of approval that addresses unanticipated paleontological discoveries, and enforces requirements to develop specific mitigation measures if resources are encountered during any development activity. The Historic Preservation section of the Community Design Element contains policies that address the management of artifacts (see Policy CD-4.1) and the collaboration, promotion of public involvement in preservation, and public outreach (see Policies CD-4.2, CD-4.6, and CD-4.7) of cultural resources.

Paleontological resources are recognized as nonrenewable; and therefore, receive protection under the California Public Resources Code (Section 21083.2) and CEQA. Review and protection of paleontological resources are also afforded by CEQA for individual development projects that would be accommodated by TOP 2050, subject to discretionary actions that are implemented in accordance with the land use plan of TOP 2050. Compared to the Approved Project, TOP 2050 would have similar impacts because the Proposed Project would result in an increase in land use intensity rather than development of new, previously undeveloped areas of the City which would require substantial landform modification. However, the potential to uncover undiscovered archeological and paleontological resources is high. Therefore, like the Approved Project, paleontological resources impacts of TOP 2050 would be potentially significant.

Mitigation Measure

The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to paleontological resources compared to the Approved Project. The following mitigation measure was included in the Draft SEIR and the Final SEIR in an abundance of caution, and is applicable to the Proposed Project. The measure as provided includes any revisions incorporated in the Final SEIR.

MM 5-2 In areas of documented or inferred from evident archaeological and/or paleontological resource presence, City staff shall require applicants for development permits to provide studies to document the presence/absence of such resources. On properties where resources are identified, such studies shall provide a detailed mitigation plan, including a monitoring program and recovery and/or in situ preservation plan, based on the recommendations of a qualified cultural preservation expert. The mitigation plan shall include the following requirements:

- a) Archaeologists and/or paleontologist shall be retained for the project and will be on call during grading and other significant ground-disturbing activities.
- b) Should any cultural resources be discovered, no further grading shall occur in the area of the discovery until the Planning Director or designee is satisfied that adequate provisions are in place to protect these resources.
- c) Unanticipated discoveries shall be evaluated for significance by a San Bernardino County Certified Professional Archaeologist/Paleontologist. If significance criteria are met, then the project shall be required to perform data recovery, professional identification, radiocarbon dates, and other special studies; submit materials to a museum for permanent curation; and provide a comprehensive final report including a catalog with museum numbers.

Finding

Changes or alterations have been required in, or incorporated into, the Proposed Project that avoid or substantially lessen the significant environmental effect as identified in the Draft SEIR. These changes are identified in the form of the mitigation measure above. The City of Ontario hereby finds that implementation of the mitigation measure is feasible, and the measure is therefore adopted.

Rationale for Finding

Mitigation Measure 5-2 requires that in the event of an unanticipated discovery of paleontological resources during grading and excavation of the site, a qualified paleontologist would assess the find and develop a course of action to preserve the find. Therefore, Mitigation Measure 5-2 would reduce potential impacts to paleontological resources to a level that is less than significant. (Draft SEIR pg. 5.7-25)

3. Tribal Cultural Resources

Impact 5.18-1: Tribal cultural resources could be adversely impacted by grading activities associated with the Proposed Project. [Threshold TRC-1]

The 2010 Certified EIR found that under the Approved Project, impacts to prehistoric archeological resources, which include TCRs, would be less than significant with mitigation.

Conducting consultation early in the CEQA process allows tribal governments, public lead agencies, and project proponents to discuss the level of environmental review, identify and address potential adverse impacts to TCRs, and reduce the potential for delay and conflict in the environmental review process.

Sacred Lands File Search and Consultation

The City requested a local government tribal consultation list from the Native American Heritage Commission (NAHC) on June 9, 2021. The tribal consultation list was requested in accordance with SB 18 and AB 52 requirements. The NAHC responded on June 22, 2021, and provided a list of tribes

for the City to contact regarding potential consultation. The NAHC also notified the City that the result of the Sacred Lands File check conducted through the NAHC was negative. The City sent initial notification letters to California Native American tribes and tribal contacts on July 2, 2021, via certified mail.

SB 18 and AB 52 Consultation

In accordance with AB 52 and SB 18 requirements, the City sent invitation letters to the Native American contacts provided by the NAHC on July 2, 2021, formally inviting tribes to consult with the City on the general plan update. The intent of consultations is to provide an opportunity for interested Native American contacts to work with the City during the project planning process to identify and protect TCRs. Response letters were received from the following tribes (see also Appendix L, *Tribal Consultation Responses*, of the Draft SEIR).

- **Agua Caliente Band of Cahuilla Indians.** The Agua Caliente Band of Cahuilla Indians responded on July 9 and August 3 that the City is not within the tribe's traditional use area, and it therefore defers to other tribes in the area.
- **Gabrieleño Band of Mission Indians–Kizh Nation.** Gabrieleño Band of Mission Indians–Kizh Nation responded on July 6 stating that the tribal government concurs with the updated plan. However, in the event of future construction or any ground disturbance, the tribal government would like to consult with the lead agency.
- **Quechan Tribe of the Fort Yuma Reservation.** Quechan Tribe of the Fort Yuma Reservation responded on July 9 and July 21 stating that the City is not within the tribe's traditional use area, and it therefore defers to other tribes in the area.
- **San Manuel Band of Mission Indians.** San Manuel Band of Mission Indians responded on July 13 and August 2 stating that TOP 2050 may impact projects in Serrano ancestral territory, and therefore is of interest to the tribe. The tribe requested additional information concerning whether the general plan updates would include any plans for museums, cultural centers, or interpretive sites. The tribe sees no conflicts with the zoning changes; however, when specific projects are planned and implemented, it might have comments and/or request formal consultation with the lead agency pursuant to CEQA (as amended, 2015) and PRC 21080.3.1.

TOP 2050 a regulatory document that sets the framework for future growth and development in the City and does not result in development in and of itself. However, future development as a result of the implementation of TOP 2050 could include grading in portions of the City with sensitivity to TCRs. Though the Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to TCRs compared to the Approved Project, grading and construction activities in undeveloped areas or redevelopment that requires deeper soil excavation than in the past could potentially disturb TCRs. Therefore, future development could potentially unearth previously unknown/unrecorded TCRs.

Mitigation Measure

The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to tribal cultural resources compared to the Approved Project. The following mitigation measures were included in the Draft SEIR and the Final SEIR in an abundance of caution, and is applicable to the Proposed Project. The measure as provided includes any revisions incorporated in the Final SEIR. Mitigation Measure (MM) indicates existing mitigation measures from the Approved Project that are also applicable to the Proposed Project, while Tribal Cultural Resource (TRC) indicates a new mitigation measure included as part of the Proposed Project.

MM 5-3 Upon receipt of an application for a proposed project subject to CEQA and within the City's jurisdiction, the City's representative shall consult with the relevant tribe(s)' representative(s) to determine if the proposed project is within a culturally sensitive area to the tribe. If sufficient evidence is provided to reasonably ascertain that the site is within a [tribal] culturally sensitive area, an archaeologist shall prepare a cultural resources assessment. The findings of the cultural resources assessment shall be incorporated into the CEQA documentation. A copy of the report shall be forwarded to the tribe(s). If mitigation is recommended in the CEQA document, the procedure described in Mitigation Measure 5-4 shall be followed.

MM 5-4 Prior to the issuance of grading permits for a proposed project for which the CEQA document defines cultural resource mitigation for potential tribal resources, the project applicant shall contact the designated tribe(s) to notify them of the grading, excavation, and monitoring program. The applicant shall coordinate with the City of Ontario and the tribal representative(s) to develop mitigation measures that address the designation, responsibilities, and participation of tribal monitors during grading, excavation, and ground-disturbing activities; scheduling; terms of compensation; and treatment and final disposition of any cultural resources, sacred sites, and human remains discovered on the site. The City of Ontario shall be the final arbiter of the conditions for projects within the City's jurisdiction.

TCR-1 **Tribal Cultural Resources Monitoring.** The project archaeologist, in consultation with interested tribes, the developer, and the City of Ontario, shall develop an archaeological monitoring plan (AMP) to address the details, timing, and responsibility of archaeological and cultural activities that will occur on the project site. Details in the AMP shall include:

1. Project-related ground disturbance (including, but not limited to, brush clearing, grading, trenching, etc.) and development scheduling;
2. The development of a rotating or simultaneous schedule in coordination with the developer and the project archeologist for designated Native American Tribal Monitors from the consulting tribes during grading, excavation and ground disturbing activities on the site: including the scheduling, safety requirements, duties, scope of work, and Native American Tribal Monitors' authority to stop and redirect grading activities in coordination with all project archaeologists (if

the tribes cannot come to an agreement on the rotating or simultaneous schedule of tribal monitoring, the Native American Heritage Commission shall designate the schedule for the onsite Native American Tribal Monitor for the proposed project);

3. The protocols and stipulations that the developer, City, Tribes, and project archaeologist will follow in the event of inadvertent cultural resources discoveries, including any newly discovered cultural resource deposits that shall be subject to a cultural resources evaluation.

At least 30 days prior to application for a grading permit and before any brush clearance, grading, excavation, and/or ground disturbing activities on the site, the developer shall retain a tribal cultural monitor to monitor all ground-disturbing activities in an effort to identify any unknown archaeological resources.

Pursuant to the AMP, a tribal monitor from the consulting tribe shall be present during the initial grading activities. If tribal resources are found during grubbing activities, the tribal monitoring shall be present during site grading activities.

TCR-2

Treatment and Disposition of Cultural Resources. In the event that Native American cultural resources are inadvertently discovered during the course of any ground-disturbing activities, including but not limited to brush clearance, grading, trenching, etc., for the proposed project, the following procedures will be carried out for treatment and disposition of the discoveries:

1. Temporary Curation and Storage: During the course of construction, all discovered resources shall be temporarily curated in a secure location on-site or at the offices of the project archaeologist. The removal of any artifacts from the project site will need to be thoroughly inventoried with tribal monitor oversight of the process;
2. Treatment and Final Disposition: The landowner(s) shall relinquish ownership of all cultural resources, including sacred items, burial goods, and all archaeological artifacts and nonhuman remains as part of the required mitigation for impacts to cultural resources. The applicant shall relinquish the artifacts through one or more of the following methods and provide the City of Ontario with evidence of same:
 - a. Accommodate the process for on-site reburial of the discovered items with the consulting Native American tribes or bands. This shall include measures and provisions to protect the future reburial area from any future impacts. Reburial shall not occur until all cataloging, basic analysis, other analyses as recommended by the project archaeologist and approved by consulting tribes, and basic recordation have been completed; all documentation should be at a level of standard professional practice to allow the writing of a report of professional quality;

- b. A curation agreement with an appropriate qualified repository in San Bernardino County that meets federal standards per 36 CFR Part 79, and therefore the resource would be professionally curated and made available to other archaeologists/researchers for further study. The collections and associated records shall be transferred, including title, to an appropriate curation facility in San Bernardino County, to be accompanied by payment of the fees necessary for permanent curation;
- c. For purposes of conflict resolution, if more than one Native American tribe or band is involved with the project and cannot come to an agreement as to the disposition of cultural materials, materials shall be curated at the San Bernardino County Museum by default;
- d. At the completion of grading, excavation, and ground-disturbing activities on the site, a Phase IV Monitoring Report shall be submitted to the City documenting monitoring activities conducted by the project archaeologist and Native Tribal Monitors within 60 days of completion of grading. This report shall document the impacts to the known resources on the property; describe how each mitigation measure was fulfilled; document the type of cultural resources recovered and the disposition of such resources; provide evidence of the required cultural sensitivity training for the construction staff held during the required pregrade meeting; and, in a confidential appendix, include the daily/weekly monitoring notes from the archaeologist. All reports produced will be submitted to the City, County Museum, and consulting tribes.

Finding

Changes or alterations have been required in, or incorporated into, the Proposed Project that avoid or substantially lessen the significant environmental effect as identified in the Draft SEIR. These changes are identified in the form of the mitigation measure above. The City of Ontario hereby finds that implementation of the mitigation measure is feasible, and the measure is therefore adopted.

Rationale for Finding

Mitigation Measures 5-3 and 5-4 and new Mitigation Measures TCR-1 and TCR-2 would reduce potential impacts associated with TCRs to a level that is less than significant by monitoring, treatment, and final deposition of resources in coordinate with the Tribes. Therefore, no significant unavoidable adverse impacts relating to TCRs remain. (Draft SEIR pg. 5.18-11)

D. SIGNIFICANT AND UNAVOIDABLE SIGNIFICANT IMPACTS THAT CANNOT BE MITIGATED TO BELOW THE LEVEL OF SIGNIFICANCE

The following summary describes the unavoidable adverse impact of the Proposed Project where either mitigation measures were found to be infeasible, or the mitigation measures are under the control of another lead agency. The following impact would remain significant and unavoidable.

1. Air Quality

Impact 5.3-1: The additional population growth forecast for TOP 2050 and the associated emissions would exceed the assumptions of the South Coast AQMD's AQMP. [Threshold AQ-1]

The 2010 Certified EIR identified that TOP had the potential to conflict with the South Coast AQMD Air Quality Management Plan (AQMP). The following describes potential air quality impacts of consistency with the AQMP from implementation of TOP 2050 compared to TOP.

The South Coast AQMD is directly responsible for reducing emissions from area, stationary, and mobile sources in the SoCAB to achieve the National and California Ambient Air Quality Standards (AAQS) and has responded to this requirement by preparing an AQMP. Since the 2010 EIR was certified, the South Coast AQMD Governing Board adopted the 2016 AQMP, which is a regional and multiagency effort (South Coast AQMD, CARB, SCAG, and EPA). In addition, South Coast AQMD will release the 2022 AQMP this year.

A consistency determination with the AQMP plays an important role in local agency project review by linking local planning and individual projects to the AQMP. It fulfills the CEQA goal of informing decision makers of the environmental efforts of the project under consideration early enough to ensure that air quality concerns are fully addressed. It also provides the local agency with ongoing information as to whether they are contributing to the clean air goals in the AQMP.

The two principal criteria for conformance with an AQMP are:

1. Whether the project would exceed the assumptions in the AQMP.
2. Whether the project would result in an increase in the frequency or severity of existing air quality violations, cause or contribute to new violations, or delay timeline attainment of air quality standards.

SCAG is South Coast AQMD's partner in the preparation of the AQMP, providing the latest economic and demographic forecasts and developing transportation measures. Regional population, housing, and employment projects developed by SCAG are based, in part, on general plan land use designations. These projections form the foundation for the emissions inventory of the AQMP.

Criterion 1

Table 5.3-9, *Comparison of Population and Employment Forecast*, in Section 5.3, *Air Quality*, of the Draft SEIR compares the population and employment growth forecast under TOP 2050 to the Approved

Project. The table shows that TOP 2050 would result in more VMT as a result of an increase in population; however, VMT per service population would decrease from the Approved Project. As a result, TOP 2050 provides a more efficient land use plan that reduces VMT per resident and employee. Therefore, the Proposed Project would be consistent with the AQMP under the first criterion.

Criterion 2

The SoCAB is designated nonattainment for ozone (O₃) and fine inhalable particulate matter (PM_{2.5}) under the California and National AAQS,¹³ nonattainment for nitrogen dioxide (NO₂) along SR-60 under the California AAQS,¹⁴ nonattainment for coarse inhalable particulate matter (PM₁₀) under the California AAQS, and nonattainment for lead (Los Angeles County only) under the National AAQS. Because TOP 2050 involves long-term growth associated with buildout of the City, cumulative emissions generated from operation of individual development projects would exceed the South Coast AQMD regional and localized thresholds. Consequently, emissions generated by development projects in addition to existing sources in the City are considered to cumulatively contribute to the nonattainment designations of the SoCAB. Buildout of the proposed land use plan associated with TOP 2050 could contribute to an increase in frequency or severity of air quality violations and delay attainment of the AAQS or interim emission reductions in the AQMP, and emissions generated from buildout would result in a significant air quality impact. Therefore, like the Approved Project, the Proposed Project would be inconsistent with the AQMP. The Proposed Project would result in a substantial increase in volatile organic compounds (VOC) compared to the Approved Project. Therefore, TOP 2050 would result in a substantial increase in magnitude of impacts compared to the Proposed Project.

Summary

Buildout of TOP 2050 would be consistent with the AQMP under the first criteria. However, air pollutant emissions associated with buildout of TOP 2050 would cumulatively contribute to the nonattainment designations in the SoCAB. Therefore, like the Approved Project, TOP 2050 would be inconsistent with the AQMP. Additionally, because of the substantial increase in population and associated VOC emission, the Proposed Project would result in a substantial increase in magnitude of impacts compared to the Approved Project.

Mitigation Measure

The following mitigation measure was included in the Draft SEIR and the Final SEIR; and in an abundance of caution, is applicable to the Proposed Project. The measure as provided include any revisions incorporated in the Final SEIR. Mitigation Measure (MM) indicates existing mitigation

¹³ The SoCAB is pending a resignation request from nonattainment to attainment for the 24-hour federal PM_{2.5} standards. The 2021 PM_{2.5} Redesignation Request and Maintenance Plan demonstrates that the South Coast meets the requirements of the CAA to allow the EPA to redesignate the SoCAB to attainment for the 65 µg/m³ and 35 µg/m³ 24-hour PM_{2.5} standards. CARB will submit the 2021 PM_{2.5} Redesignation Request to the EPA as a revision to the California SIP.

¹⁴ On February 21, 2019, CARB's board approved the separation of the area that runs along State Route 60 corridor through portions of Riverside, San Bernardino, and Los Angeles counties from the remainder of the SoCAB for State nonattainment designation purposes. The board designated this corridor as nonattainment. The remainder of the SoCAB remains in attainment for NO₂. CARB is proposing to redesignate SR-60 Near-Road Portion of San Bernardino, Riverside, and Los Angeles Counties in the SoCAB as attainment for NO₂ at the February 24, 2022, board hearing.

measures from the Approved Project that are applicable to the Proposed Project, while Air Quality (AQ) indicates a new mitigation measure included as part of the Proposed Project.

MM 3-2 The City of Ontario shall evaluate new development proposals within the City and require all developments to include access or linkages to alternative modes of transportation, such as transit stops, bike paths, and/or pedestrian paths (e.g. sidewalks).

AQ-1 Prior to discretionary approval by the City of Ontario for development projects subject to CEQA (California Environmental Quality Act) review (i.e., nonexempt projects), project applicants shall prepare and submit a technical assessment evaluating potential project operation-phase-related air quality impacts to the City of Ontario Planning Department for review and approval. The evaluation shall be prepared in conformance with South Coast Air Quality Management District (South Coast AQMD) methodology in assessing air quality impacts. If operation-related air pollutants are determined to have the potential to exceed the South Coast AQMD-adopted thresholds of significance, the City of Ontario Planning Department shall require that applicants for new development projects incorporate mitigation measures to reduce air pollutant emissions during operational activities. The identified measures shall be included as part of the conditions of approval. Possible mitigation measures to reduce long-term emissions could include, but are not limited to the following:

- For site-specific development that requires refrigerated vehicles, the construction documents shall demonstrate an adequate number of electrical service connections at loading docks for plug-in of the anticipated number of refrigerated trailers to reduce idling time and emissions.
- Applicants for manufacturing and light industrial uses shall consider energy storage and combined heat and power in appropriate applications to optimize renewable energy generation systems and avoid peak energy use.
- Site-specific developments with truck delivery and loading areas and truck parking spaces shall include signage as a reminder to limit idling of vehicles while parked for loading/unloading in accordance with California Air Resources Board Rule 2845 (13 CCR Chapter 10 sec. 2485).
- Provide changing/shower facilities as specified in Section A5.106.4.3 of CALGreen (Nonresidential Voluntary Measures).
- Provide bicycle parking facilities per Section A4.106.9 of CALGreen (Residential Voluntary Measures).
- Provide preferential parking spaces for low-emitting, fuel-efficient, and carpool/van vehicles per Section A5.106.5.1 of CALGreen (Nonresidential Voluntary Measures).

- Provide facilities to support electric charging stations per Section A5.106.5.3 and Section A5.106.8.2 of CALGreen (Nonresidential Voluntary Measures; Residential Voluntary Measures).
- Applicant-provided appliances shall be Energy Star-certified appliances or appliances of equivalent energy efficiency (e.g., dishwashers, refrigerators, clothes washers, and dryers). Installation of Energy Star-certified or equivalent appliances shall be verified by the City during plan check.

Finding

Changes or alterations have been required in, or incorporated into, the Proposed Project that avoid or substantially lessen the significant environmental effect as identified in the Draft SEIR. These changes are identified in the form of the mitigation measure above. The City of Ontario hereby finds that implementation of the mitigation measure is feasible, and the measure is therefore adopted.

The City finds that there are no other mitigation measures that are feasible, taking into consideration specific economic, legal, social, technological or other factors, that would mitigate this impact to a less-than-significant level, and further, that specific economic, legal, social, technological, or other considerations, including considerations for the provision of employment opportunities for highly trained workers, make infeasible the alternatives identified in the SEIR, as discussed in Section IV of these Findings (Public Resources Code Section 21081(a)(1), (3); Guidelines Section 15091(a)(1), (3)). As described in the Statement of Overriding Considerations, the City has determined that this impact is acceptable because specific overriding economic, legal, social, technological, or other benefits, including regionwide or statewide environmental benefits, of the proposed project outweigh its significant effects on the environment.

Impact 5.3-2: Construction activities associated with future development that would be accommodated under TOP 2050 could generate short-term emissions in exceedance of the South Coast AQMD's threshold criteria. [Thresholds AQ-2 and AQ-3]

The 2010 Certified EIR identified that, due to the scale of development activity associated under the Approved Project, the short-term emissions would likely exceed the South Coast AQMD regional significance thresholds.

Construction activities under TOP 2050 would also temporarily increase PM₁₀, PM_{2.5}, VOC, NO_x (nitrous oxides), SO_x (sulfur oxides), and CO (carbon monoxide) regional emissions in the SoCAB. The primary source of NO_x, CO, and SO_x emissions is the operation of construction equipment. The primary sources of particulate matter (PM₁₀ and PM_{2.5}) emissions are activities that disturb the soil, such as grading and excavation, road construction, and building demolition and construction. The primary sources of VOC emissions are the application of architectural coating and off-gas emissions associated with asphalt paving.

Construction activities associated with TOP 2050 would occur over the buildout horizon of the plan, causing short-term emissions of criteria air pollutants. However, information regarding specific

development projects, soil types, and the locations of receptors would be needed in order to quantify the level of impact associated with construction activity. Due to the scale of development activity associated with buildout of TOP 2050, emissions would likely exceed the South Coast AQMD regional significance thresholds. In accordance with the South Coast AQMD methodology, emissions that exceed the regional significance thresholds would cumulatively contribute to the nonattainment designations of the SoCAB.

Air quality emissions related to construction must be addressed on a project-by-project basis. For TOP 2050, which is a broad-based policy plan, it is not possible to determine whether the scale and phasing of individual projects would exceed the South Coast AQMD's short-term regional or localized construction emissions thresholds. In addition to regulatory measures—e.g., South Coast AQMD Rule 403 for fugitive dust control, Rule 1113 for architectural coatings, and CARB's Airborne Toxic Control Measures—mitigation imposed at the project level may include extension of construction schedules and/or use of special equipment.

Furthermore, TOP 2050 includes Land Use Element Policy LU-2.1, Land Use Decisions, which requires new development to minimize impacts on adjacent properties and would reduce construction emissions associated with development projects. Safety Element Policy S-5.1, Dust Control Measures, requires the implementation of BMPs for dust control at all excavation and grading projects, and Policy S-5.2, Grading in High Winds, prohibits excavation and grading during strong wind conditions.

While individual projects accommodated under TOP 2050 may not exceed the South Coast AQMD regional significance thresholds, the likely scale and extent of construction activities associated with TOP 2050 would likely continue to exceed the relevant South Coast AQMD thresholds for some projects. Compared to the Approved Project, TOP 2050 would have similar impacts because the Proposed Project would result in an increase in land use intensity rather than development of new, previously undeveloped areas of the City which would require substantial landform modification. Therefore, like the Approved Project, construction-related regional air quality impacts of developments that would be accommodated by TOP 2050 would be potentially significant.

Mitigation Measure

The following mitigation measure was included in the Draft SEIR and the Final SEIR; and in an abundance of caution, is applicable to the Proposed Project. The measure as provided include any revisions incorporated in the Final SEIR.

MM 3-1 Prior to discretionary approval by the City of Ontario for development projects subject to CEQA (California Environmental Quality Act) review (i.e., nonexempt projects), project applicants shall prepare and submit a technical assessment evaluating potential project construction-related air quality impacts to the City of Ontario Planning Department for review and approval. The evaluation shall be prepared in conformance with South Coast Air Quality Management District (South Coast AQMD) methodology for assessing air quality impacts. If construction-related criteria air pollutants are determined to have the potential to exceed the South Coast AQMD-adopted thresholds of significance, the City of Ontario Building Department shall require feasible mitigation measures to reduce air quality emissions. Potential

measures shall be incorporated as conditions of approval for a project and may include:

- Require fugitive dust control measures that exceed South Coast Air Quality Management District's Rule 403, such as:
 - Requiring use of nontoxic soil stabilizers to reduce wind erosion.
 - Applying water every four hours to active soil disturbing activities.
 - Tarping and/or maintaining a minimum of 24 inches of freeboard on trucks hauling dirt, sand, soil, or other loose materials.
- Using construction equipment rated by the United States Environmental Protection Agency as having Tier 4 interim or higher exhaust emission limits.
- Ensuring construction equipment is properly serviced and maintained to the manufacturer's standards.
- Limiting nonessential idling of construction equipment to no more than five consecutive minutes.
- Using Super-Compliant VOC paints for coating of architectural surfaces whenever possible. A list of Super-Compliant architectural coating manufactures can be found on the South Coast Air Quality Management District's website at: http://www.aqmd.gov/prdas/brochures/Super-Compliant_AIM.pdf.

These identified measures shall be incorporated into all appropriate construction documents (e.g., construction management plans) submitted to the City and shall be verified by the City's Planning Department.

Finding

Changes or alterations have been required in, or incorporated into, the Proposed Project that avoid or substantially lessen the significant environmental effect as identified in the Draft SEIR. These changes are identified in the form of the mitigation measure above. The City of Ontario hereby finds that implementation of the mitigation measure is feasible, and the measure is therefore adopted.

The City finds that there are no other mitigation measures that are feasible, taking into consideration specific economic, legal, social, technological or other factors, that would mitigate this impact to a less-than-significant level, and further, that specific economic, legal, social, technological, or other considerations, including considerations for the provision of employment opportunities for highly trained workers, make infeasible the alternatives identified in the SEIR, as discussed in Section IV of these Findings (Public Resources Code Section 21081(a)(1), (3); Guidelines Section 15091(a)(1), (3)). As described in the Statement of Overriding Considerations, the City has determined that this impact is acceptable because specific overriding economic, legal, social, technological, or other benefits,

including regionwide or statewide environmental benefits, of the proposed project outweigh its significant effects on the environment.

Impact 5.3-3: Implementation of TOP 2050 would generate additional, long-term emissions in exceedance of South Coast AQMD's threshold criteria and cumulatively contribute to the South Coast Air Basin's nonattainment designations. [Threshold AQ-2]

The 2010 Certified EIR identified that the Approved Project would generate long-term emissions that would exceed the daily South Coast AQMD thresholds for all criteria pollutants and cumulatively contribute to the nonattainment designations in the SoCAB for O₃ and particulate matter (PM₁₀ and PM_{2.5}) under the National and California AAQS.

TOP 2050 guides growth and development in the City by designating allowed land uses by parcel and through implementation of its goals and policies. New development would increase air pollutant emissions in the City and contribute to the overall emissions in the SoCAB. TOP 2050 sets up the framework for growth and development and does not directly result in development. Before development can occur, it must be analyzed for conformance with the general plan, zoning requirements, and other applicable local and State requirements; comply with the requirements of CEQA; and obtain all necessary clearances and permits.

TOP 2050 Criteria Air Pollutant Emissions Forecast

The emissions inventory for the City under TOP 2050 and Approved TOP is shown in Table 5.3-10, *City of Ontario Regional Criteria Air Pollutant Emissions Forecast*, in Section 5.3, *Air Quality*, of the Draft SEIR. As shown in the table, buildout of TOP 2050 would generate an increase in long-term emissions that exceed the daily South Coast AQMD thresholds for VOC. Emissions of NO_x, CO, PM₁₀, and PM_{2.5} would slightly decrease compared to the Approved Project. Emissions of VOC are a precursor to O₃. The increase in VOC emissions compared to the Approved Project is a result of the increase in residential uses, which result in a greater increase in consumer product use in the City. Emissions of VOC that exceed the South Coast AQMD regional significance thresholds would contribute to the O₃ nonattainment designation of the SoCAB.

Furthermore, TOP 2050 includes policies that would reduce operational emissions associated with development projects. Land Use Element policies LU-2.1 through LU-2.5 would regulate new development impacts on nearby sensitive land uses. Environmental Resources Element policies ER-3.1 through ER-3.6 would ensure that new development is energy efficient. Community Design Element policy CD-2.7 would ensure that sustainability is considered in the design of new projects. Environmental Resources policies ER-4.1 through ER-4.9 would reduce air pollution from new development. Mobility Element policies M-1.4 (complete streets), M-1.6 (VMT), M-2.1 through M-2.4 (bicycle and pedestrian), and M-3.1 through M-3.11 (transit) would reduce VMT.

The 2010 Certified EIR identified significant impacts associated with VOC, NO_x, CO, SO₂ (sulfur dioxide), PM₁₀, and PM_{2.5}. Despite the additional policies in Top 2050, because VOC emissions would be substantially greater, TOP 2050 is considered to result in an increase in magnitude of impacts for VOC compared to the Approved Project.

Mitigation Measure

The following mitigation measure was included in the Draft SEIR and the Final SEIR; and in an abundance of caution, is applicable to the Proposed Project. The measure as provided include any revisions incorporated in the Final SEIR. Mitigation Measure (MM) indicates existing mitigation measures from the Approved Project that are applicable to the Proposed Project, while Air Quality (AQ) indicates a new mitigation measure included as part of the Proposed Project.

MM 3-2 The City of Ontario shall evaluate new development proposals within the City and require all developments to include access or linkages to alternative modes of transportation, such as transit stops, bike paths, and/or pedestrian paths (e.g. sidewalks).

AQ-1 Prior to discretionary approval by the City of Ontario for development projects subject to CEQA (California Environmental Quality Act) review (i.e., nonexempt projects), project applicants shall prepare and submit a technical assessment evaluating potential project operation-phase-related air quality impacts to the City of Ontario Planning Department for review and approval. The evaluation shall be prepared in conformance with South Coast Air Quality Management District (South Coast AQMD) methodology in assessing air quality impacts. If operation-related air pollutants are determined to have the potential to exceed the South Coast AQMD–adopted thresholds of significance, the City of Ontario Planning Department shall require that applicants for new development projects incorporate mitigation measures to reduce air pollutant emissions during operational activities. The identified measures shall be included as part of the conditions of approval. Possible mitigation measures to reduce long-term emissions could include, but are not limited to the following:

- For site-specific development that requires refrigerated vehicles, the construction documents shall demonstrate an adequate number of electrical service connections at loading docks for plug-in of the anticipated number of refrigerated trailers to reduce idling time and emissions.
- Applicants for manufacturing and light industrial uses shall consider energy storage and combined heat and power in appropriate applications to optimize renewable energy generation systems and avoid peak energy use.
- Site-specific developments with truck delivery and loading areas and truck parking spaces shall include signage as a reminder to limit idling of vehicles while parked for loading/unloading in accordance with California Air Resources Board Rule 2845 (13 CCR Chapter 10 sec. 2485).
- Provide changing/shower facilities as specified in Section A5.106.4.3 of CALGreen (Nonresidential Voluntary Measures).
- Provide bicycle parking facilities per Section A4.106.9 of CALGreen (Residential Voluntary Measures).

- Provide preferential parking spaces for low-emitting, fuel-efficient, and carpool/van vehicles per Section A5.106.5.1 of CALGreen (Nonresidential Voluntary Measures).
- Provide facilities to support electric charging stations per Section A5.106.5.3 and Section A5.106.8.2 of CALGreen (Nonresidential Voluntary Measures; Residential Voluntary Measures).
- Applicant-provided appliances shall be Energy Star-certified appliances or appliances of equivalent energy efficiency (e.g., dishwashers, refrigerators, clothes washers, and dryers). Installation of Energy Star-certified or equivalent appliances shall be verified by the City during plan check.

Finding

Changes or alterations have been required in, or incorporated into, the Proposed Project that avoid or substantially lessen the significant environmental effect as identified in the Draft SEIR. These changes are identified in the form of the mitigation measure above. The City of Ontario hereby finds that implementation of the mitigation measure is feasible, and the measure is therefore adopted.

The City finds that there are no other mitigation measures that are feasible, taking into consideration specific economic, legal, social, technological or other factors, that would mitigate this impact to a less-than-significant level, and further, that specific economic, legal, social, technological, or other considerations, including considerations for the provision of employment opportunities for highly trained workers, make infeasible the alternatives identified in the SEIR, as discussed in Section IV of these Findings (Public Resources Code Section 21081(a)(1), (3); Guidelines Section 15091(a)(1), (3)). As described in the Statement of Overriding Considerations, the City has determined that this impact is acceptable because specific overriding economic, legal, social, technological, or other benefits, including regionwide or statewide environmental benefits, of the proposed project outweigh its significant effects on the environment.

Impact 5.3-4: Operation of industrial and warehousing land uses accommodated under TOP 2050 could expose sensitive receptors to substantial toxic air contaminant concentrations. [Threshold AQ-3]

The 2010 Certified EIR identified that various industrial and commercial development would occur under the Approved Project, but that individual projects would be required to comply with South Coast AQMD Rule 402 to prevent occurrence of and avoid creation of a public nuisance.

Development and operation of new land uses accommodated under TOP 2050 proposed land use plan could generate new sources of localized criteria air pollutant and toxic air contaminants (TACs) in the City from area/stationary sources and mobile sources.

CO Hotspots

Areas of vehicle congestion have the potential to create pockets of CO called hotspots. In 2007, the SoCAB was designated in attainment for CO under both the California AAQS and National AAQS.

The CO hotspot analysis conducted for the attainment by South Coast AQMD did not predict a violation of CO standards at the busiest intersections in Los Angeles during the peak morning and afternoon periods.¹⁵ As identified in South Coast AQMD's 2003 AQMP and the 1992 Federal Attainment Plan for Carbon Monoxide (1992 CO Plan), peak carbon monoxide concentrations in the SoCAB in previous years, prior to redesignation, were a result of unusual meteorological and topographical conditions and not of congestion at a particular intersection.

Under existing and future vehicle emission rates, a project would have to increase traffic volumes at a single intersection by more than 44,000 vehicles per hour—or 24,000 vehicles per hour where vertical and/or horizontal air does not mix—in order to generate a significant CO impact. Implementation of TOP 2050 under horizon year conditions would not result in hourly traffic increases of this magnitude. This net increase would be below the screening criteria. Thus, implementation of TOP 2050 would not produce the volume of traffic required to generate a CO hotspot, and CO hotspots impacts would be less than significant. The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to that of the Approved Project.

Permitted Stationary Sources

Various industrial and commercial processes (e.g., manufacturing, dry cleaning) allowed under the proposed land use plan would be expected to release TACs. Industrial land uses, such as chemical processing facilities, chrome-plating facilities, dry cleaners, and gasoline-dispensing facilities, have the potential to be substantial stationary sources that would require a permit from South Coast AQMD. Emissions of TACs would be controlled by South Coast AQMD through permitting and would be subject to further study and health risk assessment prior to the issuance of any necessary air quality permits under South Coast AQMD Rule 1401, which would ensure less than significant impacts. Additionally, though implementation of TOP 2050 may result in projects that emit TACs throughout the City, the incremental impact of the Proposed Project is the same as the Approved Project. As a result, the Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Warehouse/Industrial Land Uses

New warehousing operations could generate substantial diesel particulate matter (DPM) emissions from off-road equipment use and truck idling. Some warehousing and industrial facilities may also use transport refrigeration units for cold storage. New land uses in the City under TOP 2050 that use trucks, including trucks with transportation refrigeration units, could generate an increase in DPM that would contribute to cancer and noncancer health risk in the SoCAB. These types of facilities could also generate particulate matter (PM₁₀ and PM_{2.5}) that could cause an exceedance or contribute to the continuing exceedance of the federal and state AAQS. These new land uses could be near existing sensitive receptors. In addition, trucks would travel on regional transportation routes through the SoCAB, contributing to near-roadway diesel particulate matter concentrations.

¹⁵ The four intersections were: Long Beach Boulevard and Imperial Highway; Wilshire Boulevard and Veteran Avenue; Sunset Boulevard and Highland Avenue; and La Cienega Boulevard and Century Boulevard. The busiest intersection evaluated (Wilshire and Veteran) had a daily traffic volume of approximately 100,000 vehicles per day with LOS E in the morning peak hour and LOS F in the evening peak hour.

Implementation of the following TOP 2050 policies would reduce project-level localized impacts from industrial development:

- **ER-4.9: New Localized Air Pollution Sources Near Existing Sensitive Receptors.** We require new developments to conduct a Health Risk Assessment for land uses that generate more than 100 trucks per day or 40 trucks per day by trucks operating transportation refrigeration units (TRU's) within 1,000 feet from sensitive land uses (California Health and Safety Code Section 42705.5(a)(5)). If the health risk assessment determines the new development poses health hazards that increase the incremental cancer risk above the threshold established by the South Coast Air Quality Management District (AQMD), we will only approve permits upon the condition that adequate mitigation measures are proposed and implemented for potential impacts on the sensitive uses around the site and along the route within Ontario taken by the tracks to and from freeways. We require new developments that must perform a health risk assessment to conduct additional public outreach by sending notifications in multiple languages to all residents living within 500 feet, and encourage hosting a public meeting.

Though individual projects would be required to have less than significant impacts, cumulative development in the City would result in an increase in DPM concentrations and could increase the environmental burden on sensitive populations, including environmental justice communities, in the SoCAB.

Regional emissions are divided into two major source categories: stationary and mobile sources. TOP 2050 provides a land use plan that designates land uses for employment-generating uses, including Business Park and Industrial. The Business Park and Industrial land use categories cover a wide variety of potential uses. As a long-range planning document, TOP 2050 lacks sufficient detail on specific development projects that would potentially be developed in the future; therefore, it is not possible to determine what types of TACs would be generated on an individual site. Because the exact nature of the future industrial uses is not known, the quantity of TACs generated by the Proposed Project is also unknown. Furthermore, for warehouse development projects, cancer risk is predominately associated with diesel-powered cargo handling equipment rather than onsite truck idling. There is insufficient information available at this level of analysis to conduct a reasonable or scientifically valid analysis of DPM associated with onsite diesel-powered cargo handling equipment and trucks, or other sources of TACs. Thus, for programmatic, general-plan-level assessments, it is not feasible to conduct regional dispersion modeling to determine the incremental contribution of risks associated with land use changes.

Specific development projects in the City that have the potential to generate potentially significant risks associated with the release of TACs are required to undergo an analysis of their potential health risks associated with TACs based upon the specific details of each individual project (see Policy ER-4.9). Overall, because there are no specific development projects identified or approved under the Proposed Project and the location and exact nature of future development projects are unknown, determining health risk at this time is considered speculative pursuant to Section 15145 of the CEQA Guidelines.

Individual development projects would be required to achieve the incremental risk thresholds established by South Coast AQMD established through Policy ER 4-9. However, implementation of

TOP 2050 would generate TACs that could contribute to elevated levels in the air basin. This effect is more substantial with the Proposed Project compared to the Approved Project because of the increase in industrial land use allowed under the Proposed Project. While individual projects would achieve the project-level risk threshold of 10 per million, they would nonetheless contribute to the higher levels of cancer risk in the SoCAB; and therefore, result in a cumulatively considerable impact.

Finding

The City finds that there are no other mitigation measures that are feasible, taking into consideration specific economic, legal, social, technological or other factors, that would mitigate this impact to a less-than-significant level, and further, that specific economic, legal, social, technological, or other considerations, including considerations for the provision of employment opportunities for highly trained workers, make infeasible the alternatives identified in the SEIR, as discussed in Section IV of these Findings (Public Resources Code Section 21081(a)(1), (3); Guidelines Section 15091(a)(1), (3)). As described in the Statement of Overriding Considerations, the City has determined that this impact is acceptable because specific overriding economic, legal, social, technological, or other benefits, including regionwide or statewide environmental benefits, of the proposed project outweigh its significant effects on the environment.

2. Cultural Resources

Impact 5.5-1: Implementation of TOP 2050 could impact a historic resource. [Threshold C-1]

The 2010 Certified EIR found that policies of the Approved Project, state and federal regulations, and the City's historic preservation ordinance would ensure that historical resources classified as Tier I or Tier II would not be impacted on a programmatic level. Implementation of the Approved Project's land use plan could threaten historic resources classified as Tier III, especially within growth focus areas, and impacts would remain significant and unavoidable after Mitigation Measure 5-1.

Historic resources in the City include historic districts (designated, proposed, and potential), historic landmarks or points of historical interest, and other buildings, structures, objects, and sites that appear eligible for listing on the National, California, or local historic registers. Ontario has eight historic districts, and four proposed and five potential historic districts are deemed eligible for listing (see Figure 5.5-1, *Historic Districts*, in Section 5.5, *Cultural Resources*, of the Draft SEIR). The City's Register of Historic Resources shows 1,957 historic resources, 99 of which are designated Historic Landmark properties. The majority of the historic structures are in residential areas of the City's historic districts.

TOP 2050 is a regulatory document that sets the framework for future growth and development of the City and does not directly result in development. All development or redevelopment projects must be analyzed for conformance with TOP 2050, zoning requirements, and other applicable local and state requirements; comply with the requirements of CEQA; and obtain all necessary clearances and permits. Thus, adoption of TOP 2050 in itself would not lead to demolition or material alteration of any of these historic resources. Identified historic structures and sites that are potentially eligible for future historic resources listing may be vulnerable to development accommodating TOP 2050. In addition,

other buildings or structures that could meet the National Register criteria upon reaching 50 years of age might be impacted by development or redevelopment activity under TOP 2050.

Known or future historic sites or resources listed in the national, California, or local registers would be protected through local ordinances, TOP 2050 policies, and state and federal regulations restricting alteration, relocation, and demolition of historical resources. Sensitive historical resources of local interest are protected under Chapter 4 and Chapter 7 of the Ontario Development Code. Policy CD-4.1, Cultural Resource Management, and Policy CD-4.2, Collaboration with Property Owners and Developers, of TOP 2050's Community Design Element would require the City to update and maintain an inventory of historic sites, buildings, and other resources and work with property owners and developers to implement strategies and best practices that preserve the character of the City's historic buildings, streetscape, and neighborhoods. Policy H-1.4, Historic Preservation, of TOP 2050's Housing Element would support the preservation of enhancement of residential structures, properties, street designs, lot configurations, and other reminders of Ontario's past that are considered local historical or cultural resources. Compliance with TOP 2050 policies and state and federal regulations would ensure that development would not result in adverse impacts to identified historic and cultural resources.

At the time a development project is proposed adjacent to or near a known or potential historic structure or resource, the project-level CEQA document of the development project would need to identify any impacts, direct or indirect, that the project could have on the identified historic structure or resource. The CEQA Guidelines require a project that will have potentially adverse impacts on historical resources to conform to the Secretary of the Interior's Standards for the Treatment of Historic Properties.

Chapter 4 of the City's Development Code contains significance criteria and procedures for the designation of historic resources such as Historic Landmarks, Historic Districts, Architectural Conservation Areas, and Automatic Designations; however, not all properties on the City's list of historic resources have been evaluated for significance. To provide a greater level of certainty regarding the City's preservation goals, the ordinance includes a tier system with standard criteria and procedures for evaluating the significance of historic or potentially historic resources threatened by major modifications or demolition.

The Development Code establishes criteria for Tier I, Tier II or Tier III properties, with Tier I and II being of the highest value. The tier system identifies resources that have the highest preservation value in terms of their architectural and/or historical contribution to the City and method to evaluate the significance of their loss in the case of major modification or demolition. The tier system also includes minimum mitigation measures and a mitigation fee structure for each tier. Tier I consists of properties that should not be demolished or significantly altered under any circumstances, regardless of their designation status. Tier II consists of properties where demolition of these properties should be avoided. Given this strong policy of the City and the programmatic nature of TOP 2050 and this EIR, is it not reasonably foreseeable at this time that any projects would be proposed and approved by the City that would 1) require the demolition of Tier II resources, and 2) for which a project alternative avoiding demolition would not be available for adoption instead of the proposed project. Thus, on a programmatic level, implementation of TOP 2050 would not result in significant impacts to Tier II

resources. Tier III consists of all properties that are Designated Historic Landmarks, are contributing structures in Designated Historic Districts, or are Eligible Historical Resources, as defined by the Development Code. Demolition of these properties should be avoided where possible, but may be appropriate under certain circumstances. If demolition occurs, the City requires historic resources to be documented and historic features to be salvaged, and requires a demolition mitigation fee. Therefore, the Development Code does not provide a high level of protection for Tier III resources. As a result, historical resources categorized under the ordinance as Tier III could potentially be impacted with implementation of TOP 2050.

Compared to the Approved Project, TOP 2050 would have similar impact associated with historic resources. The Proposed Project would result in an increase in land use intensity compared to the Approved Project but would not result in development in areas of the City that were not planned for development under the Current TOP. Therefore, like the Approved Project, historical resources categorized under the Development Code as Tier III could potentially be impacted with implementation of the Proposed Land Use Plan and would be potentially significant.

Mitigation Measure

The Proposed Project would not result in new impacts or a substantial increase in the magnitude of impacts to historic resources compared to the Approved Project. The following mitigation measure was included in the Draft SEIR and the Final SEIR; and in an abundance of caution, is applicable to the Proposed Project. The measure as provided include any revisions incorporated in the Final SEIR.

MM 5-1 Historic or potentially historic resources in the City shall be evaluated for historic significance through the City's tier system prior to the issuance of plan or development approvals. Pursuant to City's Development Code (Chapter 4, Permits, Actions, and Decisions, and Chapter 7, Historic Preservation), mitigation measures for all Tier III Historic Resources shall include the following:

- a) Each historic resource shall be fully documented and cataloged pursuant to Historic American Building Survey/Historic American Engineering Record (HABS/HAER) standards, to provide a record of the resource, including, but not limited to: [i] the preparation of site plans, floor plans, exterior and interior elevations, and detail drawings of character defining features (such as moldings, stairs, etc.); and [ii] photographs of the resource, including the exterior, interior, and interior and exterior character defining features (such as moldings, light fixtures, trim patterns, etc.).
- b) A mitigation fee established pursuant to Section 7.01.030 (Historic Preservation Mitigation Fee) shall be paid to the City prior to the issuance of a demolition permit for Tier III historic resources. Fees for Tier I and II historic resources shall be determined during the Environmental Impact Report process. The fees established for Tier III will be used as a reference point for establishing fees for Tier I and II historic resources.

- c) A Certificate of Appropriateness shall not be issued for the demolition of an historic resource, either in whole or in part, until such time that a demolition permit application and a replacement structure has been approved by the City, and appropriate permits have been issued for its construction, unless: [i] a waiver is granted pursuant to Subsection H (Replacement Structure Waiver for Historic Resources Located within Industrial Zoning Districts) of Section 4.02.050; [ii] a deferral of the replacement structure requirement is granted pursuant to Subsection G (Replacement Structure Deferral) of Section 4.02.050; or [iii] demolition is required pursuant to Section 7.01.050 (Unsafe or Dangerous Conditions) of this Development Code.
- d) In an effort to preserve features and artifacts from historic resources, a determination whether items within or on the resource should be salvaged must be made by the Planning Department and may include the local historical society prior to the issuance of the demolition permit. The applicant shall be responsible for the removal, relocation, storage, and donation of such items selected for salvaging. The applicant shall provide an inventory of salvaged items to the Planning Department, and shall include a list of each item name, description, and dimension (as necessary), and the location of each item on a floor plan.

Finding

Changes or alterations have been required in, or incorporated into, the Proposed Project that avoid or substantially lessen the significant environmental effect as identified in the Draft SEIR. These changes are identified in the form of the mitigation measure above. The City of Ontario hereby finds that implementation of the mitigation measure is feasible, and the measure is therefore adopted.

The City finds that there are no other mitigation measures that are feasible, taking into consideration specific economic, legal, social, technological or other factors, that would mitigate this impact to a less-than-significant level, and further, that specific economic, legal, social, technological, or other considerations, including considerations for the provision of employment opportunities for highly trained workers, make infeasible the alternatives identified in the SEIR, as discussed in Section IV of these Findings (Public Resources Code Section 21081(a)(1), (3); Guidelines Section 15091(a)(1), (3)). As described in the Statement of Overriding Considerations, the City has determined that this impact is acceptable because specific overriding economic, legal, social, technological, or other benefits, including regionwide or statewide environmental benefits, of the proposed project outweigh its significant effects on the environment.

3. Noise

Impact 5.13-1: Construction activities associated with buildout of TOP 2050 would result in temporary noise increases at sensitive receptors during construction activities. [Threshold N-1]

The 2010 Certified EIR identified that TOP buildout could result in individual construction developments near noise sensitive receptors and expose receptors to prolong periods of construction

activity. Mitigation Measure 12-4 was identified to reduce construction noise to the extent feasible. However, construction noise impacts of the Approved Project were significant and unavoidable in the 2010 Certified EIR.

TOP 2050 is an update to TOP and focuses on technical updates to the Policy Plan to comply with state housing mandates and conform with new state laws related to community health, environmental justice, climate adaptation, resiliency, and mobility.

Two types of short-term noise impacts could occur during construction. First, the transport of workers and movement of materials to and from the site could incrementally increase noise levels along local access roads. This amount of construction traffic is typically small in relation to the total daily traffic volumes on those roadway segments. The second type of short-term noise impact is related to demolition, site preparation, grading, and/or physical construction.

The Proposed Project would result in an increase in land use intensity rather than development of new, previously undeveloped areas of the City that would require substantial landform modification. While specific project level data for individual developments for TOP 2050 (such as construction equipment, duration, and phasing) are not available, construction could generate noise levels in excess of 80 dBA equivalent continuous noise level (L_{eq}) and generate noise disturbances for prolonged periods of time at noise-sensitive receptors. Safety Element Policy S-4.1, Noise Mitigation, would help minimize the construction noise impacts through enforcement of the City's Noise Ordinance. This includes Municipal Code Chapter 29, Section 5-29.09, which limits construction, remodeling, digging, grading, demolition, or any other related building activity to between the hours of 7:00 am and 6:00 pm, Monday through Friday, and 9:00 am to 6:00 pm on weekends. The Proposed Project would not result in new or a substantial increase in the magnitude of impacts compared to the Approved Project. Nevertheless, construction-related noise impacts from the Proposed Project would be potentially significant.

Mitigation Measure

The following mitigation measure was included in the Draft SEIR and the Final SEIR; and in an abundance of caution, is applicable to the Proposed Project. The measure as provided include any revisions incorporated in the Final SEIR.

MM 12-4 Construction activities associated with new development that occurs near sensitive receptors shall be evaluated for potential noise impacts. Construction contractors shall implement the following measures for construction activities in the City of Ontario. Construction plans submitted to the City shall identify these measures on demolition, grading, and construction plans. The City of Ontario Planning and Building Departments shall verify that grading, demolition, and/or construction plans submitted include these notations prior to issuance of demolition, grading, and/or building permits.

- Construction activity is limited to the hours between 7:00 am and 6:00 pm Monday through Friday and 9:00 am to 6:00 pm Saturdays and Sundays, as prescribed in Municipal Code Section 5-29.09.

- During the entire active construction period, equipment and trucks used for project construction shall use the best-available noise control techniques wherever feasible (e.g., improved mufflers, equipment re-design, use of intake silencers, ducts, engine enclosures, and acoustically attenuating shields or shrouds).
- Impact tools (e.g., jack hammers and hoe rams) shall be hydraulically or electrically powered wherever possible. Where the use of pneumatic tools is unavoidable, an exhaust muffler on the compressed air exhaust shall be used along with external noise jackets on the tools.
- Stationary equipment such as generators and air compressors shall be located as far as feasible from nearby noise-sensitive uses.
- Stockpiling shall be located as far as feasible from nearby noise-sensitive receptors.
- Construction traffic shall be limited, to the extent feasible, to approved haul routes established by the City's Engineering Department.
- At least 10 days prior to the start of construction activities, a sign shall be posted at the entrance(s) to the job site, clearly visible to the public, that includes permitted construction days and hours as well as the telephone numbers of the City's and contractor's authorized representatives that are assigned to respond in the event of a noise or vibration complaint. If the authorized contractor's representative receives a complaint, he/she shall investigate, take appropriate corrective action, and report the action to the City.
- Signs shall be posted at the job site entrance(s), within the on-site construction zones, and along queuing lanes (if any) to reinforce the prohibition of unnecessary engine idling. All other equipment shall be turned off if not in use for more than 5 minutes.
- During the entire active construction period and to the extent feasible, the use of noise-producing signals, including horns, whistles, alarms, and bells, shall be for safety warning purposes only. The construction manager shall use smart back-up alarms, which automatically adjust the alarm level based on the background noise level or switch off back-up alarms and replace with human spotters in compliance with all safety requirements and laws.
- Erect temporary noise barriers (at least as high as the exhaust of equipment and breaking line-of-sight between noise sources and sensitive receptors), as necessary and feasible, to maintain construction noise levels at or below the performance standard of 80 dBA L_{eq} . Barriers shall be constructed with a solid material that has a density of at least 1.5 pounds per square foot with no gaps from the ground

to the top of the barrier and may be lined on the construction side with an acoustical blanket, curtain, or equivalent absorptive material.

Finding

Changes or alterations have been required in, or incorporated into, the Proposed Project that avoid or substantially lessen the significant environmental effect as identified in the Draft SEIR. These changes are identified in the form of the mitigation measure above. The City of Ontario hereby finds that implementation of the mitigation measure is feasible, and the measure is therefore adopted.

The City finds that there are no other mitigation measures that are feasible, taking into consideration specific economic, legal, social, technological or other factors, that would mitigate this impact to a less-than-significant level, and further, that specific economic, legal, social, technological, or other considerations, including considerations for the provision of employment opportunities for highly trained workers, make infeasible the alternatives identified in the SEIR, as discussed in Section IV of these Findings (Public Resources Code Section 21081(a)(1), (3); Guidelines Section 15091(a)(1), (3)). As described in the Statement of Overriding Considerations, the City has determined that this impact is acceptable because specific overriding economic, legal, social, technological, or other benefits, including regionwide or statewide environmental benefits, of the proposed project outweigh its significant effects on the environment.

Impact 5.13-3: Development in accordance with TOP 2050 could create groundborne vibration and groundborne noise during construction activities in excess of established standards. [Threshold N-2]

Construction Vibration

The 2010 Certified EIR identified that vibration generated during construction activities would be a significant impact despite implementation of Mitigation Measure 12-2.

Construction activity at projects within TOP 2050 plan area would generate varying degrees of ground vibration, depending on the construction procedures and equipment. Operation of construction equipment generates vibrations that spread through the ground and diminish with distance from the source. The effect on buildings in the vicinity of the construction site varies depending on soil type, ground strata, and receptor building construction. The results from vibration can range from no perceptible effects at the lowest vibration levels, to low rumbling sounds and perceptible vibrations at moderate levels, to slight structural damage at the highest levels. Vibration from construction activities rarely reaches the levels that can damage structures but can achieve the audible and perceptible ranges in buildings close to the construction site. Table 5.13-11, *Vibration Levels for Construction Equipment*, in Section 5.13, *Noise*, of the Draft SEIR lists reference vibration levels for construction equipment at a distance of 25 feet.

Like the Approved Project, the Proposed Project would have similar impacts because specific project-level data for individual developments for TOP 2050 (such as construction equipment) are not available, and construction could generate excessive vibration levels at sensitive receptor locations.

Vibration-related noise impacts from the Proposed Project that would accommodate buildout of TOP 2050 would be potentially significant.

Operational Vibration

The 2010 Certified EIR found that potential impacts from on-road vehicles would not generate more than 0.8 in/sec peak particle velocity at vibration-sensitive receptors; therefore, impacts associated with the Approved Project were identified as less than significant.

Caltrans has studied the effects of propagation of vehicle vibration on sensitive land uses and notes that heavy trucks and buses generate the highest earth-borne vibrations of normal traffic. Caltrans further notes that the highest traffic-generated vibrations are along freeways and state routes. Its study finds that

... vibrations measured on freeway shoulders (five meters from the centerline of the nearest lane) have never exceeded 0.08 inches per second, with the worst combinations of heavy trucks and poor roadway conditions (while such trucks were moving at freeway speeds). This level coincides with the maximum recommended safe level for ruins and ancient monuments (and historic buildings).

The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to that of the Approved Project. Therefore, like the Approved Project, the Proposed Project impacts would be less than significant.

Commercial and industrial operations would generate varying degrees of ground vibration, depending on the operational procedures and equipment. The 2010 Certified EIR found that the majority of industrial uses would not be immediately adjacent to vibration-sensitive uses, the use of heavy equipment associated with industrial activities would occur indoors, and no significant vibration impacts would occur from vibration generated by industrial uses. Like the Approved Project, the Proposed Project's commercial and industrial operations would not generate significant vibration impacts, and vibration from industrial and commercial operations would be less than significant.

The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project.

Mitigation Measure

The following mitigation measure was included in the Draft SEIR and the Final SEIR; and in an abundance of caution, is applicable to the Proposed Project. The measure as provided include any revisions incorporated in the Final SEIR.

MM 12-2 Prior to issuance of a building permit, individual projects that involve vibration-intensive construction activities, such as pile drivers, jack hammers, and vibratory rollers near sensitive receptors shall be evaluated for potential vibration impacts. For construction within 135 feet of fragile structures, such as historical resources, within 100 feet of nonengineered timber and masonry buildings (e.g., most residential buildings), or within 75 feet of engineered concrete and masonry (no plaster); or a vibratory roller within 25 feet of any structure, the project applicant shall prepare a

noise and vibration analysis to assess and mitigate potential noise and vibration impacts related to these activities. This noise and vibration analysis shall be conducted by a qualified and experienced acoustical consultant or engineer. The vibration levels shall not exceed Federal Transit Administration (FTA) architectural damage thresholds (e.g., 0.12 inches per second [in/sec] peak particle velocity [PPV] for fragile or historical resources, 0.2 in/sec PPV for nonengineered timber and masonry buildings, and 0.3 in/sec PPV for engineered concrete and masonry). If vibration levels would exceed this threshold, alternative uses shall be used, such as drilling piles as opposed to pile driving and static rollers as opposed to vibratory rollers. If necessary, construction vibration monitoring shall be conducted to ensure vibration thresholds are not exceeded.

Finding

Changes or alterations have been required in, or incorporated into, the Proposed Project that avoid or substantially lessen the significant environmental effect as identified in the Draft SEIR. These changes are identified in the form of the mitigation measure above. The City of Ontario hereby finds that implementation of the mitigation measure is feasible, and the measure is therefore adopted.

The City finds that there are no other mitigation measures that are feasible, taking into consideration specific economic, legal, social, technological or other factors, that would mitigate this impact to a less-than-significant level, and further, that specific economic, legal, social, technological, or other considerations, including considerations for the provision of employment opportunities for highly trained workers, make infeasible the alternatives identified in the SEIR, as discussed in Section IV of these Findings (Public Resources Code Section 21081(a)(1), (3); Guidelines Section 15091(a)(1), (3)). As described in the Statement of Overriding Considerations, the City has determined that this impact is acceptable because specific overriding economic, legal, social, technological, or other benefits, including regionwide or statewide environmental benefits, of the proposed project outweigh its significant effects on the environment.

Impact 5.13-4: Implementation of TOP 2050 could expose noise sensitive uses to excessive noise levels from the Ontario International Airport. [Threshold N-3]

The 2010 Certified EIR identified that airport noise impacts of the Approved Project were significant and unavoidable despite implementation of Mitigation Measure 12-1.

Future noise contours were developed based on data provided by the City of Ontario. Figure 5.13-3, *Airport Noise Contours*, in Section 5.13, *Noise*, of the Draft SEIR, show the ONT noise contours identified in the ALUCP. The Chino Airport noise contours do not extend into the City.

The City of Ontario's noise and land use compatibility standards considers a noise environment up to 60 dBA CNEL to be "clearly acceptable" for residential uses. Residential uses in exterior noise environments of up to 65 dBA CNEL are "normally acceptable." Normally acceptable conditions would require an acoustical report for major new residential construction. CBC Part 2, Volume 1,

Chapter 12, Section 1206.4, Allowable Interior Noise Levels, requires that interior noise levels attributable to exterior sources not exceed 45 dBA in any habitable room.

The 2010 Certified EIR found that residents and other sensitive receptors in the noise contour would be exposed to excessive noise levels from airport operations, and consequently indoor and exterior noise environments would be exposed to elevated noise levels from aircraft overflights. Safety Element Policy S-4.6, Airport Noise Compatibility, would minimize impacts. Policy S-4.6 states that information from the ALUCPs shall be utilized to prevent the construction of new noise-sensitive land uses within airport noise impact zones. The Proposed Project would not result in new or a substantial increase in magnitude of impacts compared to the Approved Project. However, impacts would remain potentially significant, and future sensitive uses within an airport 65 dBA CNEL or more contour would be required to conduct a noise assessment and mitigate, as feasible, to achieve an interior noise level 45 dBA CNEL in any habitable room.

Additionally, TOP 2050 includes policies that help minimize airport noise impacts:

- **S-4.2: Coordination with Transportation Authorities.** We collaborate with airport owners, FAA, Caltrans, SBCTA, SCAG, neighboring jurisdictions, and other transportation providers in the preparation and maintenance of, and updates to transportation-related plans to minimize noise impacts and provide appropriate mitigation measures.
- **S-4.3: Airport Noise Mitigation.** We aggressively pursue funding and utilize programs to reduce the effects of aircraft noise in impacted areas of our community.

Mitigation Measure

The following mitigation measure was included in the Draft SEIR and the Final SEIR; and in an abundance of caution, is applicable to the Proposed Project. The measure as provided include any revisions incorporated in the Final SEIR.

MM 12-1 Prior to the issuance of building permits for any project that involves a noise-sensitive use within the 65 dBA CNEL contour of the Ontario International Airport, the project property owner/developers shall retain an acoustical engineer to conduct an acoustic analysis and identify, where appropriate, site design features and/or required building acoustical improvements (e.g., sound transmission class rated windows, doors, and attic baffling), to ensure compliance with the City's Noise Compatibility Criteria and the California State Building Code and California Noise Insulation Standards (Titles 24 and 21 of the California Code of Regulations).

Finding

Changes or alterations have been required in, or incorporated into, the Proposed Project that avoid or substantially lessen the significant environmental effect as identified in the Draft SEIR. These changes are identified in the form of the mitigation measure above. The City of Ontario hereby finds that implementation of the mitigation measure is feasible, and the measure is therefore adopted.

The City finds that there are no other mitigation measures that are feasible, taking into consideration specific economic, legal, social, technological or other factors, that would mitigate this impact to a less-than-significant level, and further, that specific economic, legal, social, technological, or other considerations, including considerations for the provision of employment opportunities for highly trained workers, make infeasible the alternatives identified in the SEIR, as discussed in Section IV of these Findings (Public Resources Code Section 21081(a)(1), (3); Guidelines Section 15091(a)(1), (3)). As described in the Statement of Overriding Considerations, the City has determined that this impact is acceptable because specific overriding economic, legal, social, technological, or other benefits, including regionwide or statewide environmental benefits, of the proposed project outweigh its significant effects on the environment.

4. Transportation

Impact 5.17-2: The Proposed Project would generate a substantial increase in total VMT compared to the Approved Project. [Threshold T-2]

Table 5.17-4, *VMT Comparison of TOP 2050 to the Approved TOP*, in Section 5.17, *Transportation*, of the Draft SEIR shows that Boundary VMT is higher under the Proposed Project than the Approved Project within the Ontario city boundary (VMT Threshold 2); however, the origin-destination method VMT/SP is lower under the Proposed Project, indicating more efficient mix of land uses (VMT Threshold a). Since there would be a net increase in total citywide Boundary VMT from Approved TOP to TOP 2050, the Proposed Project is anticipated to result in a significant transportation impact related to VMT. This is primarily due to the increase in population accommodated by the Proposed Project.

TOP 2050 includes goals and policies to offset VMT impacts by creating greater access to transit and enhanced alternative transportation modes. In addition, the following policies encourage reduced VMT through land use planning and design.

- **LU-1.1: Strategic Growth.** We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, foster the development of transit, and support the expansion of the active and multimodal transportation networks throughout the City.
- **LU-1.2: Sustainable Community Strategy.** We integrate state, regional, and local Sustainable Community/Smart Growth principles into the development and entitlement process.
- **LU-1.4: Multimodal Mobility.** We require development and urban design, where appropriate, that reduces reliance on the automobile and capitalizes on active transportation, transit, electric vehicles, and multimodal transportation opportunities.
- **CE-1.12: Circulation.** We continuously plan and improve public transit and non-vehicular circulation for the mobility of all, including those with limited or no access to private automobiles.
- **M-1.2: Mitigation of Impacts.** We require development to mitigate its traffic impacts.

- **M-1.6: Reduce Vehicle Miles Traveled.** We will strive to reduce VMT through a combination of land use, transportation projects, travel demand management strategies, and other trip reduction measures in coordination with development projects and public capital improvement projects.
- **CD-2.5: Streetscapes.** We design new and, when necessary, retrofit existing streets to improve walkability, bicycling and transit integration, strengthen connectivity, and enhance community identity through improvements to the public right-of-way such as sidewalks, street trees, parkways, curbs, street lighting and street furniture.
- **CD-2.6: Connectivity.** We promote development of street patterns, multimodal networks, and connected public spaces that create and unify neighborhoods, rather than divide them, and create cohesive and continuous corridors, rather than independent “islands” through the following means: 1) Local streets that provide access both between subdivisions and within neighborhoods and discourage through traffic; 2) A local street system that is logical and understandable for the user. A grid system is preferred to avoid circuitous and confusing travel paths between internal neighborhood areas and adjacent arterials and provide adequate emergency and evacuation access; and 3) Pedestrian and bicycle networks that provide convenient access to neighborhoods and nearby destinations, such as schools, parks, other public spaces, commercial areas, and transit stops.
- **CD-2.16: Transit Stops.** We require transit stops be conveniently located, well lit, safe, and accessible to pedestrians, bicyclists, and people of all abilities.
- **CD-3.2: Comfortable, Human-Scale Public Realm.** We require that public spaces, including streets, parks, and plazas on both public and private property be designed to maximize safety, comfort, and aesthetics and connect to the citywide pedestrian, vehicular, and bicycle networks.
- **CD-3.3: Complete and Connected Network.** We require that pedestrian, vehicular, and bicycle circulation on both public and private property be coordinated to provide connections internally and externally to adjacent neighborhoods and properties (existing and planned) through a system of local roads and trails that promote walking and biking to nearby destinations (including existing and planned parks, commercial areas, and transit stops) and designed to maximize safety, comfort, and aesthetics.
- **CD-3.5: Active Frontages.** We create lively pedestrian streetscapes by requiring primary building, business, and residential entrances, outdoor dining, and storefronts be located on ground floors adjacent to sidewalks or public spaces and designed to maximize safety, comfort, aesthetics, and the intended functionality (as defined by the Place Type).

Even with the additional goals, policies, and actions related to VMT reduction identified as part of TOP 2050, which are not reflected in the traffic modeling, the Proposed Project is not anticipated to reduce the impact of increased total VMT. Therefore, VMT impacts of TOP 2050 would result in a new significant impact compared to the Approved Project.

Mitigation Measure

The following mitigation measure was included in the Draft SEIR and the Final SEIR; and in an abundance of caution, is applicable to the Proposed Project. The measure as provided include any revisions incorporated in the Final SEIR.

T-1 Prior to approval of discretionary projects subject to VMT reduction analysis, applicants shall demonstrate compliance with the City's VMT Guidelines for CEQA assessment of VMT impacts. For projects with VMT per Service Population exceeding the County's significance threshold, a mitigation plan shall be developed and implemented. Mitigation should consist of Transportation Demand Management (TDM) measures analyzed under a VMT-reduction methodology consistent with the California Air Pollution Control Officers Association's (CAPCOA) *Final Handbook for Analyzing Greenhouse Gas Emission Reductions, Assessing Climate Vulnerabilities, and Advancing Health and Equity* (2021) and approved by the City of Ontario (if applicable). Examples of measures include but are not limited to:

- Pedestrian Network Improvements: constructing new sidewalks and/or improving damaged or substandard sidewalks that connect to a larger pedestrian network.
- Construct or Improve Bike Facilities: constructing new or enhancing a single existing Class I, II or IV bike facility that connects to a larger bike network.
- Construct or Improve Bike Boulevards: implementing a Class III bike boulevard on a local or collector street that is one travel lane in each direction, has a design speed of 25 mph or less and a design volume of 5,000 ADT or less.
- Expand Bikeway Networks: constructing a network of interconnected new Class I, II, or IV bike facilities.
- Provide End of Trip Bicycle Facilities: constructing facilities that support cyclists such as bike parking, lockers, and showers.
- Implement Transit-Supportive Roadway Treatments: funding infrastructure improvements such as traffic signal modifications and roadway signing and striping that are dedicated to improving transit travel times and reliability.
- Transit Passes: providing discounted or free transit fare to a specific geographic area, population group, or to the general public.
- Vanpool Program: providing groups of 5 to 15 people with direct shuttle service between their workplace and residence.
- Carshare Program (conventional or EV): providing access to a shared fleet of on-demand vehicles for short-term use/rental. Best practice is to discount carshare

membership and provide priority parking for carshare vehicles to encourage use of the service.

- Bikeshare Program (conventional or EV): providing access to a shared fleet of on-demand bicycles for short-term use/rental. Best practice is to discount bikeshare membership and dedicate bikeshare parking to encourage use of the service.
- Rideshare Program: providing access to and encouraging the use of a ridesharing platform or service. This could be an app, website, or other service that provides ride-matching coordination services.
- Community-Based Travel Planning (CBTP): CBTP is a residential based approach to outreach, performed by trained advisors, that provides households within a targeted geographic area with customized information, incentives, and support to encourage the use of transportation alternatives in place of single occupancy vehicles.
- Commute Trip Reduction (CTR) Program: CTR programs can be mandatory or voluntary, and involve providing information, coordination, services, infrastructure, and/or incentives for alternative modes such as ridesharing, vanpool, transit passes, and cycling.

Finding

Changes or alterations have been required in, or incorporated into, the Proposed Project that avoid or substantially lessen the significant environmental effect as identified in the Draft SEIR. These changes are identified in the form of the mitigation measure above. The City of Ontario hereby finds that implementation of the mitigation measure is feasible, and the measure is therefore adopted.

The City finds that there are no other mitigation measures that are feasible, taking into consideration specific economic, legal, social, technological or other factors, that would mitigate this impact to a less-than-significant level, and further, that specific economic, legal, social, technological, or other considerations, including considerations for the provision of employment opportunities for highly trained workers, make infeasible the alternatives identified in the SEIR, as discussed in Section IV of these Findings (Public Resources Code Section 21081(a)(1), (3); Guidelines Section 15091(a)(1), (3)). As described in the Statement of Overriding Considerations, the City has determined that this impact is acceptable because specific overriding economic, legal, social, technological, or other benefits, including regionwide or statewide environmental benefits, of the proposed project outweigh its significant effects on the environment.

E. CUMULATIVE IMPACTS

Regarding the Project's potential to result in cumulative impacts, the City hereby finds as follows:

1. Aesthetics

Cumulative impacts related to aesthetics would be contiguous with the city boundaries. Cumulative projects in Ontario would have the potential to result in a cumulative impact to aesthetic resources if, in combination, they would result in the removal or substantial adverse change of one or more features that contribute to the valued visual character or image of a neighborhood, community, State scenic highway, or localized area, such as a designated landmark, historic resource, trees, or rock outcropping.

Scenic Vistas and Scenic Resources. Growth within the City of Ontario could affect scenic vistas and specific scenic resources. However, because growth allowed under the Proposed Project would be subject to goals, policies, and regulations that reduce impacts of the TOP 2050 on scenic resources to a less than significant level, the Proposed Project's contribution to impacts would not be cumulatively considerable. Cumulative impacts of TOP 2050 related to scenic vistas and scenic resources are therefore considered less than significant.

Visual Character and Quality. Growth anticipated in Ontario would fundamentally alter visual character and quality in some areas of the City, including Ontario Ranch. However, because development allowed under the Proposed Project would be subject to goals, policies, and regulations that reduce impacts of TOP 2050 on visual resources and character to a less than significant level, the Proposed Project's contribution to cumulative impacts would not be cumulatively considerable. Cumulative impacts of the Proposed Project related to visual character and quality are therefore considered less than significant.

Light and Glare. The construction and operation of cumulative projects located in Ontario would have the potential to result in new sources of light and glare from new development and redevelopment that requires night lighting—such as security lighting in commercial areas—or is constructed with materials that would result in glare, such as expanses of glass on office buildings. Impacts from glare are generally localized and not cumulative in nature; therefore, a significant cumulative impact related to glare would not occur. Additionally, because development allowed under the Proposed Project would be subject to the Development Code, which contains standards addressing lighting, and would reduce impacts of TOP 2050 related to light and glare to a less than significant level, the Proposed Project's contribution to cumulative impacts would not be cumulatively considerable.

2. Agriculture and Forestry Resources

The area considered for cumulative impacts to agriculture and forestry resources is San Bernardino County.

Mapped Important Farmland, Williamson Act Contracts, and Agricultural Zoning. Implementation of the Proposed Project would not result in agricultural resource impacts that would combine with impacts in San Bernardino County to result in significant, cumulative impacts. Impacts would be less than significant.

Forest Resources. The City of Ontario does not have any forest resources or timberland; therefore, no significant cumulative impact to forest resources would occur, and impacts would not be cumulatively considerable.

3. Air Quality

The cumulative setting for air quality is the SoCAB. In accordance with the South Coast AQMD methodology, any project that produces a significant project-level regional air quality impact in an area that is in nonattainment contributes to the cumulative impact. Cumulative projects include new development and general growth within the SoCAB. The SoCAB is nonattainment for ozone, PM₁₀, and PM_{2.5}. Due to the extent of the area potentially impacted from cumulative project emissions, South Coast AQMD consider a project cumulatively significant when project-related emissions exceed the regional emissions thresholds. As identified in Impact 5.3-2 (operation) and Impact 5.3-3 (construction), implementation of the Proposed Project would cumulatively contribute to the nonattainment designations of the air basins, and cumulative impacts are significant.

Construction. The SoCAB are designated nonattainment for O₃, PM_{2.5}, PM₁₀, and lead (SoCAB: Los Angeles County only) under the California and/or National AAQS. Construction of cumulative projects would further degrade the regional and local air quality. Air quality would be temporarily impacted during construction activities. Implementation of mitigation measures for related projects would reduce cumulative impacts. Mitigation Measure 3-1 would reduce impacts associated with consistency with the South Coast AQMD. However, project-related construction emissions could still potentially exceed the South Coast AQMD significance thresholds on a project and cumulative basis. Consequently, the Proposed Project's contribution to cumulative air quality impacts would be cumulatively considerable and would therefore be significant.

Operation. For operational air quality emissions, any project that does not exceed or can be mitigated to less than the daily regional threshold values is not considered by South Coast AQMD to be a substantial source of air pollution and does not add significantly to a cumulative impact. Mitigation Measures 3-2 and new Mitigation Measure AQ-1 would reduce impacts associated with consistency with the South Coast AQMD. Nevertheless, operation of the Proposed Project would result in emissions in excess of the South Coast AQMD regional emissions thresholds for long-term operation. Additionally, development under TOP 2050 would generate TACs that could contribute to elevated levels of risk. Therefore, the Proposed Project's air pollutant emissions would be cumulatively considerable and therefore significant.

4. Biological Resources

The analysis presented in this section, by the nature of TOP 2050, provides a cumulative assessment of biological impacts within the City. TOP 2050 policies would minimize potential cumulative impacts to biological resources, as identified above. Coordination with resource agencies would reduce potential cumulative impacts to biological resources by prioritizing areas for conservation and maintaining communication among jurisdictions. With implementation of existing regulations and TOP 2050 policies, impacts to biological resources would be less than significant and would not be cumulatively considerable.

5. Cultural Resources

Historical Resources. The area considered for cumulative impacts is the City of Ontario. Projects in the City could destroy or otherwise diminish the historical significance of historical resources. Mitigation

Measure 5-1 would require historic or potentially historic resources to be evaluated for historic significance through the City's Development Code tier system. Major modification or demolition of Tier III resources may be appropriate under certain circumstances. If demolition occurs, the City requires historic resources to be documented and historic features to be salvaged, and requires a demolition mitigation fee. Therefore, the ordinance does not provide a high level of protection for Tier III historic resources. As described above, historical resources categorized under Chapter 4 of the City's Development Code as Tier III could potentially be impacted with implementation of TOP 2050 and would be potentially significant; and therefore, cumulatively considerable.

Archaeological Resources. The area considered for cumulative impacts is the City of Ontario. Projects in the City would disturb soil and thus could damage archaeological resources. Projects in the City of Ontario and would comply with federal and state regulations governing the treatment of archeological resources. Mitigation Measure 5-2 would ensure that impacts to archeological resources are less than significant and would be less than cumulatively considerable.

6. Energy

The area considered for cumulative impacts to electricity and natural gas supplies and facilities is SCE and Southern California Gas Company (SoCalGas) service areas. Other projects in the SCE and SoCalGas service area would be subject to existing regulations, including the CBC which requires new buildings increase energy efficiency. TOP 2050 includes policies to reduce energy use and the CCAP includes measures to align with the state's goals for carbon neutrality. Cumulative impacts would be less than significant, and impacts would not be cumulatively considerable.

7. Geology and Soils

Geological Hazards. Geology and soils impacts related to the Proposed Project would be specific to the sites of each development or redevelopment project under TOP 2050. Compliance with applicable state and local building regulations would be required of all development projects. Site-specific geologic hazards would be addressed by the geotechnical report required for each development project. The geologic investigation would identify the specific geologic and seismic characteristics on a site and provide guidelines for engineering design and construction to maintain the structural integrity of proposed structures and infrastructure. Therefore, compliance with applicable state and local building regulations and standard engineering practices related to seismic and geologic hazard reduction would prevent significant cumulative adverse impacts associated with geologic and seismic hazards. Impacts of the Proposed Project on geology and soils would not be cumulatively considerable.

Paleontological Resources. The area considered for cumulative impacts for paleontological resources is the City of Ontario. Projects in the City of Ontario and would comply with federal and state regulations governing the treatment of paleontological resources. Mitigation Measure 5-2 would ensure that impacts to paleontological resources are less than significant and would be less than cumulatively considerable.

8. Greenhouse Gas Emissions

Project-related GHG emissions are not confined to a particular air basin but are dispersed worldwide. Therefore, impacts identified under Impact 5.8-1 and Impact 5.8-2 are not project-specific impacts to global warming, but the Proposed Project's contribution to this cumulative impact. As discussed above, the City would experience a reduction in GHG emissions from existing conditions despite the anticipated population and employment growth. In addition, with implementation of the CCAP, the Proposed Project would achieve the state's GHG emissions efficiency target without implementation of additional local GHG reduction measures. Goals and policies in TOP 2050 and actions in the CCAP would minimize GHG emissions generated by the residential and nonresidential land uses in the City. Consequently, the Proposed Project's cumulative contribution to global climate change impacts is less than cumulatively considerable.

9. Hazards and Hazardous Materials

Hazardous Materials. The cumulative setting for hazardous materials is the City of Ontario. Impacts arising from hazardous materials and hazardous materials releases are site-specific and generally do not combine to cause cumulative impacts. Therefore, hazards and hazardous materials impacts are less than significant and would not be cumulatively considerable.

Airport Hazards. The areas considered for cumulative airport-related hazards impacts are the airport influence areas of ONT and Chino Airport. Development proposed within the airport influence area of ONT and Chino Airport would be required to be evaluated under the ALUCP for ONT and the 2011 Handbook for Chino Airport to ensure that the projects proposed within such zones would comply with land use regulations for the respective safety zones set forth by the affected agencies. Cumulative impacts would be less than significant after compliance with such regulations, and impacts of TOP 2050 would not be cumulatively considerable.

Fire Hazards. The areas considered for cumulative impacts related to wildfires are fire hazard severity zones in the City. Projects within wildfire hazards zones are required to comply with regulations governing development in such zones, including CBC Chapter 7A, CFC Chapter 49, and California Public Resources Code Sections 4291 et seq. TOP 2050 policies regarding wildfire would also reduce cumulative impacts. Wildfire impacts of TOP 2050 would not be cumulatively considerable.

Emergency Response and Evacuation. According to the City's 2018 Local Hazard Mitigation Plan, interstate highways would serve as major emergency response and evacuation routes. Additionally, the Ontario Fire Department reviews development applications to ensure that adequate emergency accessibility is provided based on local and state guidance. Review of emergency access is also included as part of the City's Design Review process. Therefore, impacts to emergency response and evacuation are less than significant; and therefore, less than cumulatively considerable.

10. Hydrology and Water Quality

Cumulative impacts to hydrology, drainage, flooding, and water quality are considered for the Chino Creek subwatershed and the Middle Santa Ana River subwatershed, which are part of the larger Santa Ana River Watershed.

Development in the City of Ontario and other projects in these watersheds would increase impervious areas, thus increasing runoff and flows into storm drainage systems. Within San Bernardino County, other projects would be required to prepare hydrology and hydraulic studies in accordance with the County Hydrology Manual and analyze stormwater flows that result from the 100-year storm event to ensure that the capacities of the storm drain systems are not exceeded. Additionally, other projects would be required to comply with MS4 permits applicable in those watersheds. The Santa Ana RWQCB MS4 permit applies to portions of three counties in the Santa Ana Basin. Other projects compliance with the requirements of the Santa Ana RWQCB MS4 permit, the San Bernardino County Stormwater Program, and San Bernardino County Hydrology Manual guidance would reduce cumulative impacts to hydrology and drainage to less than significant and would not be cumulatively considerable.

If projects in the watersheds are within 100-year flood zones, they would be mandated to comply with National Flood Insurance Program requirements. Cumulative impacts to hydrology, drainage, and flooding would be less than significant, and impacts of TOP 2050 would not be cumulatively considerable.

Cumulative projects could generate pollutants that would contaminate stormwater. Compliance with the MS4 permit includes implementation of site design and source control BMPs that reduce the potential for pollutants to enter runoff and treatment control BMPs that remove pollutants from stormwater. Cumulative water quality impacts would be less than significant after compliance with such permits, and impacts would not be cumulatively considerable.

11. Land Use and Planning

Cumulative projects in the City would have the potential to result in a cumulative impact if they would, in combination, conflict with existing land use plans, policies, and regulations adopted for the purpose of avoiding or mitigating an environmental impact. Projects in the City would utilize TOP 2050 and regional planning documents such as SCAG's Connect SoCal during planning, to the extent that they are applicable. Cumulative projects would be required to comply with TOP 2050 or they would not be approved without a general plan amendment. As discussed above, implementation of TOP 2050 would not conflict with existing land use plans, policies, or regulations. Therefore, TOP 2050 would not contribute to a significant cumulative impact.

12. Mineral Resources

The area considered for cumulative impacts to mineral resources is the P-C regions overlapping the City of Ontario and extending into other counties within the Greater Los Angeles Sand and Gravel Resources Area: the Claremont-Upland P-C region extends into Los Angeles County; the Orange County-Temescal Region extends into Orange County and Western Riverside County, and the San Bernardino P-C region encompasses San Bernardino and most western Riverside County. Other projects in the referenced areas would likely be proposed within MRZ-2 and MRZ-3 areas. Development of such projects could cause loss of availability of known mineral resources valuable to the region. Other projects would be subject to independent CEQA review, including analysis of impacts to MRZ areas and mining sites. Implementation of all feasible mitigation measures would be required to reduce any significant impacts identified. As identified above, the Proposed Project would

not impact mineral resources of statewide, regional, or local value. In addition policies of TOP 2050 would minimize cumulative impacts. Therefore, TOP 2050 would not contribute to a significant cumulative impact.

13. Noise

The above analysis of TOP 2050 addresses cumulative impacts with regard to operational and construction noise as well as groundborne noise and vibration in the City. TOP 2050 proposes the long-term buildout and operation of many different uses. Although multiple simultaneous nearby noise sources may, in combination, result in higher overall noise levels, this effect is captured and accounted for by the community noise level metrics that form the basis of the standards of significance for noise analysis. To specifically estimate the Proposed Project's contribution to traffic noise, existing noise levels were compared to those projected with completion of TOP 2050. As demonstrated above, TOP 2050's contribution to increases in ambient noise levels results in a significant impact.

The area considered for cumulative impacts for construction noise and vibration is the City of Ontario. Construction activities may occur simultaneously and in close proximity to noise-sensitive receptors, resulting in significant impacts. Mitigation Measure 12-2 would reduce potential impacts associated with construction vibration from individual development projects to the extent feasible. Mitigation Measure 12-4 would reduce potential impacts associated with construction from individual development projects to the extent feasible. However, due to the potential for proximity of construction activities to sensitive uses, the number of construction projects occurring simultaneously, and the potential duration of construction activities, impacts could be significant. Since details of individual development projects in the City are currently unknown, it cannot be determined whether Mitigation Measure 12-2 and 12-4, would reduce potentially significant impacts to less than significant levels. TOP 2050 would; therefore, contribute to cumulatively considerable construction-related noise, and the cumulative impact would be significant and unavoidable.

14. Population and Housing

The area considered for cumulative impacts is the SCAG region. As described above, although the increase in population, housing, and employment under TOP 2050 would exceed SCAG's regional forecasts for the City of Ontario, TOP 2050 would improve the job-housing balance when compared to the Approved Project. SCAG identifies several Priority Growth Areas in Ontario, including High-Quality Transit Areas, Transit Priority Areas, Neighborhood Mobility Areas, and Livable Corridors. TOP 2050 would promote growth consistent with these Priority Growth Areas, as proposed land use changes under TOP 2050 are intended to encourage walking and biking, put residents in proximity to resources, and align future growth in Ontario with planned infrastructure improvements and regional transportation goals. Therefore, implementation of TOP 2050 would not contribute to a significant cumulative population and housing impact.

15. Public Services

Fire Protection. The area of cumulative effect for fire protection is the City of Ontario. As described above, OFD would be required to meet the increased demand for population and employment growth over the buildout of TOP 2050. Development or expansion of fire stations, equipment, and personnel

would be subject to TOP 2050 policies designed to protect environmental resources and would also be subject to environmental review and impact mitigation per CEQA. Cumulative impacts associated with development of new stations are therefore determined to result in less than significant impacts.

Police Protection. The area of cumulative effect for police services is the City of Ontario. As described above, OPD would be required to meet the increased demand for population and employment growth over the buildout of TOP 2050. Development or expansion of police stations, equipment, and personnel would be subject to TOP 2050 policies designed to protect environmental resources and would also be subject to environmental review and impact mitigation per CEQA. Cumulative impacts associated with development of new police stations are therefore determined to result in less than significant impacts.

School Services. The area considered for cumulative analysis is the service areas of school districts serving the City. Cumulative development projects that involve residential development would increase the public-school population in the region and require the construction or expansion of school facilities so that adequate service ratios are maintained. This increase in student population would require the construction or expansion of school facilities, which could result in adverse environmental impacts. As discussed above, under state law, development projects are required to pay established school impact fees in accordance with SB 50 at the time of building permit issuance. The funding program established by SB 50 has been found by the Legislature to constitute “full and complete mitigation of the impacts of any legislative or adjudicative act...on the provision of adequate school facilities” (Government Code Section 65995[h]). The fees authorized for collection under SB 50 are conclusively deemed full and adequate mitigation of impacts on school district facilities. Furthermore, cumulative school projects require discretionary actions and would be required to demonstrate compliance with CEQA prior to project approval. TOP 2050 would not combine with areawide growth to result in cumulatively considerable impacts to school services. This impact would be less than significant.

Library Services. The area considered for cumulative analysis is the service area of the Ontario library system. Cumulative development projects that involve residential development would increase the population in the region and require the construction or expansion of library facilities so that adequate service ratios are maintained. This increase in population would require the construction or expansion of library facilities, which could result in adverse environmental impacts. New and/or expanded libraries in the City would be subject to TOP 2050 policies protecting the environment, and new or expanded libraries would be subject to environmental review and mitigation pursuant to CEQA. Impacts would be less than significant, and therefore, less than cumulatively considerable.

16. Recreation

The area considered for cumulative impacts for recreation is the City of Ontario. As described above, TOP 2050 provides land use opportunities for public parks to be developed in line with future development. The proposed Parks and Recreation Element contains relevant goals, policies, and programs that support a regular review of the City’s parks and trails plans to keep pace with demographic trends and recreational needs of Ontario’s residents (see Policies PR-1.1 through PR-1.16). In addition, under TOP 2050’s Parks and Recreation Element, Policy PR-1.5 strives to provide five acres of parkland per 1,000 residents, and Policy PR-1.6 provides a minimum of two acres of

developed private park space per 1,000 residents in addition to the three acres per 1,000 persons standard. As a result, development of park facilities would keep pace with the anticipated increase in population from buildout of TOP 2050. Therefore, impacts are less than significant and would not be cumulatively considerable.

17. Transportation

The cumulative effect for transportation impacts is the SBCTA region. Cumulative traffic impacts consider the impacts of future growth and development in the SBCTA region. As identified above, TOP 2050 would result in a significant cumulative impact for VMT as a result of a substantial increase in population within the City. Mitigation Measure T-1 would reduce potential impacts for future development projects to the extent feasible. Future development projects consistent with TOP 2050 would need to consider transportation demand management (TDM) measures consistent with those identified in the Mobility Element. TDM techniques include incentives to use transit; incentives to form carpools rather than drive alone; and making home, work, and shopping closer together to shorten travel distances. VMT impacts under the Proposed Project would remain. Therefore, VMT impacts of TOP 2050 are cumulatively considerable.

The Proposed Project is consistent with adopted policies, plans, or programs regarding public transit, bicycle, and pedestrian facilities, and the performance and safety of such facilities, and would not combine with other area projects to result in significant impacts to such facilities. Impacts associated with alternative transportation polices are less than significant.

According to the City's 2018 Local Hazard Mitigation Plan, interstate highways would serve as major emergency response and evacuation routes. Additionally, the Ontario Fire Department reviews development applications to ensure that adequate emergency accessibility is provided based on local and state guidance. Review of emergency access is also included as part of the City's Design Review process. Therefore, impacts to emergency response and evacuation are less than significant; and therefore, less than cumulatively considerable.

18. Tribal Cultural Resources

The area considered for analysis of cumulative impacts for TCR is the traditional tribal territories of the Tribes. Projects in Ontario and within the traditional tribal territories would involve ground disturbance and thus could damage TCR. Other lead agencies would consult independently with Native American tribes regarding TCRs pursuant to AB 52 and/or SB 18. Other projects would comply with state and federal laws and regulations protecting TCRs and would implement feasible mitigation measures for significant impacts identified. Therefore, cumulative impacts would be less than significant.

19. Utilities and Service Systems

Wastewater Treatment and Collection. The area considered for cumulative impacts for wastewater is the IEUA service area. Cumulative projects in the IEUA service area could cause significant impacts if they either exceeded wastewater treatment requirements of RWQCBs or generated wastewater exceeding the combined capacities of wastewater treatment plants. Cumulative development within

the IEUA service area could result in the need for new and/or expanded the wastewater treatment plants. However, as stated previously, IEUA has experienced a decrease in the volume of sewage flow of approximately 10 percent over the last 20 years, due to a decrease in indoor water consumption with new development compliance with California Green Building Standards Code and water conservation efforts. The IEUA anticipates a significant increase in the growth of its service area in the next ten years, with 40 percent of the growth resulting from new development in Ontario. The IEUA develops 10-year forecasts and specifies capital improvements that will be implemented to meet the increase in demand. The ultimate capacity for wastewater flows to the IEUA WWTPs is 80 mgd by 2060. Also, future development within the service area would be required to comply with all applicable regulations and ordinances issued by IEUA. Wastewater from cumulative projects is assumed in the SSMPs prepared by IEUA and the cities that send wastewater to the IEUA WWTPs. The IEUA and the cities within its service area plan for increased demand with future development. Therefore, with continued compliance with local and regional regulations and the requirements of TOP 2050, cumulative impacts would be less than significant, and would not be cumulatively considerable.

Water Supply and Distribution. The area considered for cumulative impacts for water supply and treatment is the IEUA and City of Ontario service areas. The IEUA and the OMUC obtain groundwater from the Chino Groundwater Basin, which is adjudicated and managed by the Chino Basin Watermaster, imported water from MWD, purchased water from San Antonio Water Company, and recycled water from IEUA. The IEUA and City's 2020 UWMPs state that there are sufficient water supplies through 2045 to meet projected demands in normal years, single dry years, and multiple dry years. Although the Proposed Project at buildout is estimated to generate a 6 percent increase in water demand using conservative water demand factors, new State requirements and City policies and code requirements would result in enhanced water efficiency and conservation would result in total water demand below the projections in the 2020 UWMP for year 2045. With the implementation of SB X7-7 and State, regional, and local water conservation ordinances, all new development would be required to conserve water use and implement water efficiency measures. In addition, pursuant to SB 610 and SB 221, water supply assessments would be prepared for large development projects prior to the approval of each project to ensure adequate water supply for new development. Overall, cumulative water demands would neither exceed planned levels of supply nor require building new water treatment facilities or expanding existing facilities beyond what is currently planned. In addition, future development would be required to pay connection fees, which would offset the costs of system maintenance and capital upgrades to support the new development in the service areas. Therefore, cumulative impacts would be less than significant and would not be cumulatively considerable.

Storm Drainage Systems. The area considered for cumulative impacts is the Chino Creek and the Middle Santa Ana River subwatershed. Other projects in this area would increase impervious areas, thus increasing runoff and flows into the storm drain systems. Within San Bernardino county, other projects would also be required to prepare hydrology and hydraulic studies in accordance with the County Hydrology Manual and analyze stormwater flows that result from the 100-year storm event to ensure that the capacities of the storm drain systems are not exceeded. Additionally, other project would be required to comply with the MS4 permits applicable to those watersheds. The Santa Ana RWQCB MS4 permit applies to portions of three counties in the Santa Ana Basin. Most projects would meet criteria in the MS4 permits that require low-impact development and on-site stormwater bioretention

facilities that would reduce the amount of runoff entering public storm drain systems. Cumulative impacts would be less than significant and would not be cumulatively considerable.

Solid Waste. The area considered for cumulative impacts to solid waste disposal includes all the cities and counties that dispose of their solid waste in Badlands Sanitary Landfill or El Sobrante Landfill. These landfills currently have a combined excess daily capacity of 7,046 tons/day and have a remaining landfill capacity of 15,750,000 cubic yards for Badlands Sanitary Landfill and 144,000,000 cubic yards for El Sobrante Landfill. Both landfills have closure dates beyond 2050. In addition, state and local regulations and ordinances regarding the recycling of construction debris and organic wastes will further reduce the amount of solid waste transported to these landfills in the future. Therefore, with continued compliance with the applicable regulations, in combination with reasonably foreseeable future development, cumulative impacts would be less than significant, and project impacts would not be cumulatively considerable.

20. Wildfire

Fire Hazards. The areas considered for cumulative impacts related to wildfires are fire hazard severity zones in the City. Projects within wildfire hazards zones are required to comply with regulations governing development in such zones, including CBC Chapter 7A, CFC Chapter 49, and California Public Resources Code Sections 4291 et seq. TOP 2050 policies regarding wildfire would also reduce cumulative impacts. Wildfire impacts of TOP 2050 would not be cumulatively considerable.

Emergency Response and Evacuation. According to the City's 2018 Local Hazard Mitigation Plan, interstate highways would serve as major emergency response and evacuation routes. Additionally, the Ontario Fire Department reviews development applications to ensure that adequate emergency accessibility is provided based on local and state guidance. Review of emergency access is also included as part of the City's Design Review process. Therefore, impacts associated with evacuation are less than significant and would not be cumulatively considerable.

IV. ALTERNATIVES TO THE PROPOSED PROJECT

An EIR must briefly describe the rationale for selection and rejection of alternatives. The lead agency may make an initial determination as to which alternatives are feasible, and therefore, merit in-depth consideration, and which ones are infeasible.

Section 15126.6 of the State CEQA Guidelines requires an EIR to describe a range of reasonable alternatives to the Project, or to the location of the Project, which could feasibly achieve most of its basic objectives, but would avoid or substantially lessen any of the significant effects identified in the EIR analysis. An EIR is not required to consider every conceivable alternative to a proposed project. Rather, an EIR must consider a reasonable range of alternatives that are potentially feasible; an EIR is not required to consider alternatives that are infeasible. In addition, an EIR should evaluate the comparative merits of the alternatives. Therefore, this section sets forth the potential alternatives to the Project analyzed in the EIR and evaluates them in light of the objectives of the Project, as required by CEQA.

Key provisions of the State CEQA Guidelines relating to the alternatives' analysis (Section 15126.6 et seq.) are summarized below:

- [T]he discussion of alternatives shall focus on alternatives to the Project or its location which are capable of avoiding or substantially lessening any significant effects of the Project, even if these alternatives would impede to some degree the attainment of the Project objectives or would be more costly.” (CEQA Guidelines Section 15126.6[b]).
- “The specific alternative of ‘no project’ shall also be evaluated along with its impact.” (CEQA Guidelines Section 15126.6[e][1])
- “The no project analysis shall discuss the existing conditions at the time the notice of preparation is published, or if no notice of preparation is published, at the time environmental analysis is commenced, as well as what would be reasonably expected to occur in the foreseeable future if the project were not approved, based on current plans and consistent with available infrastructure and community services. If the environmentally superior alternative is the ‘no project’ alternative, the EIR shall also identify an environmentally superior alternative among the other alternatives.” (CEQA Guidelines Section 15126.6[e][2])
- “The range of alternatives required in an EIR is governed by a ‘rule of reason’ that requires the EIR to set forth only those alternatives necessary to permit a reasoned choice. The alternatives shall be limited to ones that would avoid or substantially lessen any of the significant effects of the project.” (CEQA Guidelines Section 15126.6[f])
- “Among the factors that may be taken into account when addressing the feasibility of alternatives are site suitability, economic viability, availability of infrastructure, general plan consistency, other plans or regulatory limitations, jurisdictional boundaries..., and whether the proponent can reasonably acquire, control or otherwise have access to the alternative site (or the site is already owned by the proponent)” (CEQA Guidelines Section 15126.6[f][1]).
- “Only locations that would avoid or substantially lessen any of the significant effects of the project need be considered for inclusion in the EIR.” (CEQA Guidelines Section 15126.6[f][2][A])
- “An EIR need not consider an alternative whose effect cannot be reasonably ascertained and whose implementation is remote and speculative.” (CEQA Guidelines Section 15126.6[f][3])

A. RATIONALE FOR SELECTING POTENTIALLY FEASIBLE ALTERNATIVES

The alternatives must include a no-project alternative and a range of reasonable alternatives to the Project if those reasonable alternatives would attain most of the Project objectives while substantially lessening the potentially significant Project impacts. The range of alternatives discussed in an EIR is governed by a “rule of reason,” which the State CEQA Guidelines Section 15126.6(f)(3) defines as:

“ . . . set[ting] forth only those alternatives necessary to permit a reasoned choice. The alternatives shall be limited to ones that would avoid or substantially lessen any of the significant effects of the project. Of those alternatives, the EIR need examine in detail

only the ones that the lead agency determines could feasibly attain most of the basic objectives of the project. The range of feasible alternatives shall be selected and discussed in a manner to foster meaningful public participation and informed decision-making.”

Among the factors that may be taken into account when addressing the feasibility of alternatives (as described in the State CEQA Guidelines Section 15126.6(f)(1)) are environmental impacts, site suitability, economic viability, availability of infrastructure, general plan consistency, other plans or regulatory limitations, jurisdictional boundaries, and whether the Project proponent could reasonably acquire, control, or otherwise have access to an alternative site. An EIR need not consider an alternative whose effects could not be reasonably identified, and whose implementation is remote or speculative.

For purposes of this analysis, the Project alternatives are evaluated to determine the extent to which they attain the basic Project objectives, while significantly lessening any significant effects of the Project.

B. ALTERNATIVES CONSIDERED AND REJECTED DURING THE SCOPING/PROJECT PLANNING PROCESS

The following is a discussion of the alternatives considered during the scoping and planning process and the reasons why they were not selected for detailed analysis in the SEIR.

1. Alternative Development Areas

CEQA requires that the discussion of alternatives focus on alternatives to the project or its location that are capable of avoiding or substantially lessening any significant effects of the project. The key question and first step in the analysis is whether any of the significant effects of the project would be avoided or substantially lessened by putting the project in another location. Only locations that would avoid or substantially lessen any of the significant effects of the project need be considered for inclusion in the EIR (CEQA Guidelines Section 15126[5][B][1]). The City does not have land use authority outside of the City’s boundaries. Therefore, an alternative development area would be infeasible and was not analyzed.

Finding

The City finds that there are no alternative development areas for the Proposed Project as the City does not have jurisdiction over land uses outside of the City’s boundaries. As described in these Findings of Fact, the Proposed Project would result in less than significant impacts, or impacts that can be mitigated to less than significant. For significant and unavoidable impacts, the City has determined that these impacts are acceptable because specific overriding economic, legal, social, technological, or other benefits, including regionwide or statewide environmental benefits, of the proposed project outweigh its significant effects on the environment, as described in the Statement of Overriding Considerations.

2. Reduction of Housing Units

California Public Resources Code Section 21159.26 states that “a Lead or Responsible Agency shall not reduce the proposed number of housing units as a mitigation measure or alternative to lessen a particular significant effect on the environment if that agency determines that there is another feasible, specific mitigation measure or alternative that would provide a comparable lessening of the significant effect” (see also CEQA Guidelines Section 15041 [c], 15092[c], 15096[g][2]). TOP 2050 would result in significant air quality and VMT impacts associated with the increase in housing units of the Proposed Project compared to the Approved Project. While a reduction in housing units would reduce VMT and VOC emissions from consumer product use, this alternative is considered and rejected because the increase in housing units under TOP 2050 is consistent with the RHNA allocation for Ontario.

Finding

The City finds that although a reduction of housing units under the Proposed Project would reduce VMT and VOC emissions, it would not be consistent with the RHNA allocation for Ontario. As described in these Findings of Fact, the Proposed Project would result in less than significant impacts, or impacts that can be mitigated to less than significant. For significant and unavoidable impacts, the City has determined that these impacts are acceptable because specific overriding economic, legal, social, technological, or other benefits, including regionwide or statewide environmental benefits, of the proposed project outweigh its significant effects on the environment, as described in the Statement of Overriding Considerations.

C. ALTERNATIVES SELECTED FOR FURTHER ANALYSIS

The following alternatives were determined to represent a reasonable range of alternatives with the potential to feasibly attain most of the basic objectives of the project but avoid or substantially lessen any of the significant effects of the project. Table 7-2, *Summary of Impacts of Alternatives Compared to the Proposed Project*, in Chapter 7, *Alternatives to the Proposed Project*, of the Draft SEIR, identifies how each of the alternatives selected for further analysis compare to the Proposed Project. Table 7-3, *Ability of Each Alternative to Meet the Project Objectives*, in Chapter 7, *Alternatives to the Proposed Project*, of the Draft SEIR, provides a summary of the ability of the alternatives to achieve the project objectives.

1. No Project Alternative

In the No Project/Current TOP Alternative, TOP 2050 would not be implemented by the City. The current TOP would remain in effect. Buildout statistics for TOP 2050 and the current TOP are compared in Table 7-1, *Buildout Statistical Summary*, in Chapter 7, *Alternatives to the Proposed Project*, of the Draft SEIR. In addition, this alternative would not include the policy updates for environmental justice, climate vulnerability, adaptation, resiliency, complete streets, and the CCAP.

Impacts of the No Project/Current TOP alternative would be similar for aesthetics, agriculture and forestry resources, air quality, biological resources, cultural resources, energy, geology and soils, hydrology and water quality, mineral resources, noise, tribal cultural resources, and wildfire. This alternative would eliminate the Proposed Project’s VMT impact on transportation and lessen impacts associated with public services, recreation, and utilities and service systems. This alternative would

slightly increase population and housing impacts; and would increase impacts related to hazards and hazardous materials (airport safety), GHG emissions, and land use and planning (airport land use compatibility, resulting in a significant unavoidable impact).

Finding

The No Project Alternative would not implement the proposed TOP 2050 policies, which are designed to further enhance the project objectives, compared to the existing TOP. While this alternative would eliminate the Proposed Project's VMT impact on transportation and lessen impacts associated with public services, recreation, and utilities and service systems, this alternative would slightly increase population and housing impacts; and would increase impacts related to hazards and hazardous materials (airport safety), GHG emissions, and land use and planning (airport land use compatibility). As a result, specific economic, legal, social, technological, or other considerations, including provision of employment opportunities for highly trained workers, make infeasible this project alternative for the reasons identified in the Final SEIR.

2. Reduced Industrial Alternative

TOP 2050 includes 338 additional acres zoned Industrial compared to the current TOP. Compared to the Approved Project, the Proposed Plan is adding capacity for 25,399 residential units and 1,092,508 square feet of nonresidential square footage. This includes additional capacity for 5,189,888 square feet of Business Park (BP) and 8,099,949 square feet of Industrial (IND), and reduced capacity for 8,990,532 square feet of Mixed Use (MU) and 3,206,797 square feet of Commercial and Hospitality (NC, GC, OC, and HOS). To eliminate impacts associated with an increase in diesel trucks, VMT from trucks (which have a greater trip length), and associated DPM, this alternative would eliminate approximately 8.1 million square feet of industrial development in the City, resulting in 4,405 fewer warehouse jobs compared to the Proposed Project.

Impacts of the Reduced Industrial alternative would be similar for aesthetics, agriculture and forestry resources, biological resources, cultural resources, energy, geology and soils, hazards and hazardous materials, hydrology and water quality, land use and planning, mineral resources, population and housing, public services, tribal cultural resources, and wildfire. This alternative would reduce the Proposed Project's air quality, GHG, noise, and utilities and service systems impacts. This alternative would reduce but would not eliminate the Proposed Project's significant transportation (VMT) impact.

Finding

The Reduced Industrial Alternative would meet all of the project objectives to the same extent as the Proposed Project. While this alternative would lessen the Proposed Project's air quality, GHG, noise, and utilities and service systems impacts, this alternative would also reduce but would not eliminate the Proposed Project's significant transportation (VMT) impact. As a result, specific economic, legal, social, technological, or other considerations, including provision of employment opportunities for highly trained workers, make infeasible this project alternative for the reasons identified in the Final SEIR.

D. ENVIRONMENTALLY SUPERIOR ALTERNATIVE

An EIR must identify an “environmentally superior” alternative and where the No Project Alternative is identified as environmentally superior, the EIR is then required to identify as environmentally superior an alternative from among the others evaluated. Based on the analysis contained within the Draft SEIR, the Reduced Industrial Alternative has been identified as the “environmentally superior” to the Proposed Project.

The Reduced Industrial Alternative has been identified as the environmentally superior alternative because it would result in reduced impacts related to air quality, greenhouse gas emissions, noise, transportation, and utilities and service systems due to the reduction in square footage of industrial development. However, significant and unavoidable impacts related to cultural resources would continue to occur from implementation of this alternative. Impacts related to aesthetics, agriculture and forestry resources, biological resources, energy, geology and soils, hazardous and hazardous materials, hydrology and water quality, land use and planning, mineral resources, population and housing, public services, recreation, tribal cultural resources, and wildfire would be similar to the Proposed Project.

CEQA does not require the lead agency (the City of Ontario) to choose the environmentally superior alternative. Instead, CEQA requires the City to consider environmentally superior alternatives, weigh those considerations against the environmental impacts of the Proposed Project, and make findings that the benefits of those considerations outweigh the harm. “Among the factors that may be used to eliminate alternatives from detailed consideration in an EIR are: (i) failure to meet most of the basic project objectives, (ii) infeasibility, or (iii) inability to avoid significant environmental impacts” (CEQA Guidelines Section 15126.6[c]).

The City Council rejects the Reduced Industrial Alternative on the following grounds, each of which individually provides sufficient justification for rejection of this alternative: (1) this alternative does not avoid the Project’s significant and unavoidable impacts on the environment; and (2) this alternative meets the Project objectives to a lesser extent than the proposed Project. Therefore, the Reduced Industrial Alternative is eliminated from further consideration.

V. ADDITIONAL CEQA CONSIDERATIONS

A. SIGNIFICANT IRREVERSIBLE CHANGES DUE TO THE PROJECT

Section 15126.2(c) of the State CEQA Guidelines requires that an EIR describe any significant irreversible environmental changes that would be caused by the proposed project should it be implemented. Specifically, the State CEQA Guidelines state:

“Uses of nonrenewable resources during the initial and continued phases of the project may be irreversible since a large commitment of such resources makes removal or nonuse thereafter unlikely. Primary impacts and, particularly, secondary impacts (such as highways improvement which provides access to a previously inaccessible area) generally commit future generations to similar uses. Also, irreversible damage can result from environmental accidents associated with the

project. Irretrievable commitments of resources should be evaluated to assure that such current consumption is justified.”

Generally, a project would result in significant irreversible environmental changes if:

- The primary and secondary impacts would generally commit future generations to similar uses;
- The project would involve a large commitment of nonrenewable resources;
- The project would involve uses in which irreversible damage could result from any potential environmental accidents associated with the project; or
- The proposed irretrievable commitments of nonrenewable resources are not justified (e.g., the project involves the wasteful use of energy).

In the case of TOP 2050, implementation would cause the following significant irreversible changes:

- Implementation of the proposed project would include construction activities that would entail the commitment of nonrenewable and/or slowly renewable energy resources; human resources; and natural resources such as lumber and other forest products, sand and gravel, asphalt, steel, copper, lead, other metals, water, and fossil fuels. Future developments in accordance with the proposed project would require the use of natural gas and electricity, fossil fuels, and water. The commitment of resources required for the construction and operation of the proposed project would limit the availability of such resources for future generations or for other uses during the life of the project.
- An increased commitment of social services and public maintenance services (e.g., police, fire, schools, libraries, and sewer and water services) would also be required. The energy and social service commitments would be long-term obligations in view of the low likelihood of returning the land to its original condition once it has been developed.
- Population growth related to the Proposed Project compared to the Approved Project would increase vehicle miles traveled and volatile organic compound emissions associated with consumer product use. The Proposed Project would cumulatively contribute to the South Coast Air Basin’s nonattainment designation for ozone.

B. GROWTH-INDUCING IMPACTS OF THE PROPOSED PROJECT

Pursuant to Section 15126(d) and 15126.2(d) of the State CEQA Guidelines, this section is provided to examine ways in which the Project could foster economic or population growth, or the construction of additional housing, either directly or indirectly, in the surrounding environment. To address this issue, potential growth-inducing effects will be examined through analysis of the following questions:

- Would this project remove obstacles to growth, e.g., through the construction or extension of major infrastructure facilities that do not presently exist in the project area, or through changes in existing regulations pertaining to land development?

- Would this project result in the need to expand one or more public services to maintain desired levels of service?
- Would this project encourage or facilitate economic effects that could result in other activities that could significantly affect the environment?
- Would approval of this project involve some precedent-setting action that could encourage and facilitate other activities that could significantly affect the environment?

Please note that growth-inducing effects are not to be construed as necessarily beneficial, detrimental, or of little significance to the environment. This issue is presented to provide additional information on ways in which TOP 2050 could contribute to significant changes in the environment, beyond the direct consequences of developing the land use concept examined in the preceding sections of this SEIR.

1. Would this project remove obstacles to growth, e.g., through the construction or extension of major infrastructure facilities that do not presently exist in the project area, or through changes in existing regulations pertaining to land development?

Approval and implementation of TOP 2050 would not remove obstacles to growth. Development in the City is guided by TOP. Changes to the Approved Project are identified in Section 3.4.2.3, *Areas of Change*, and would not result in changes to existing regulations that would remove barriers to growth in the City. Portions of the City are already served by infrastructure. Like the current TOP, implementation of TOP 2050 would allow for development of currently undeveloped land and redevelopment of existing land uses. This would induce construction of infrastructure extensions and improvements, such as roadways, storms drains, water pipes, solid waste collection systems, and energy/communication extensions toward undeveloped areas of the City. In addition, the Proposed Project would increase demand for electricity and natural gas that could require expansion of energy infrastructure, as provided by SCE and the SoCalGas. Impacts to existing utilities and service systems and potential needs for future improvements are discussed further in Section 5.19, *Utilities and Service Systems*.

TOP 2050 accommodates the additional population growth required to accommodate the SCAG's RHNA. Buildout of the proposed project may require additional firefighting and police personnel and construction of new and/or expanded facilities to improve response times, if necessary. Buildout may also require future construction of new and/or expanded schools in the school districts that serve Ontario (e.g., Chaffey Joint Union High School District, Chino Valley Unified School District, Cucamonga School District, Ontario-Montclair School District, and Mountain View School District). Impacts from the proposed project on public services facilities are discussed in detail in Section 5.15, *Public Services*.

2. Would this project result in the need to expand one or more public services to maintain desired levels of service?

As stated above, like the Approved Project buildout, the Proposed Project may require additional fire and police services, school facilities, and library space to maintain desired levels of service. This would include expanding existing facilities; acquiring land to construct new stations, schools, and libraries;

and adequately equipping and staffing new facilities. Section 5.15, *Public Services*, analyzes the impacts of the Proposed Project on existing public services in more detail.

3. Would this project encourage or facilitate economic effects that could result in other activities that could significantly affect the environment?

Like the Approved Project, implementation of the Proposed Project would not encourage or facilitate economic effects that could result in other activities that could significantly affect the environment. Impacts of job-generating land uses and employment pursuant to TOP 2050 are analyzed throughout Chapter 5 of this SEIR. No additional impacts would occur.

4. Would approval of this project involve some precedent-setting action that could encourage and facilitate other activities that could significantly affect the environment?

Cities and counties in California periodically update their general plans pursuant to California Government Code Sections 65300 et seq. Thus, approval of TOP 2050 would not set a precedent that could encourage and facilitate other activities that could significantly affect the environment.

VI. FINDINGS ON RESPONSES TO COMMENTS ON THE DRAFT SEIR AND REVISIONS TO THE FINAL SEIR

The Final SEIR contains response to comments, clarifications, revisions, and corrections to the Draft SEIR. The focus of the response to comments is on the disposition of significant environmental issues as raised in the comments, as specified by State CEQA Guidelines Section 15088(b). The City provided written responses to each comment made by a public agency, as set forth in Section 2 of the Final SEIR, pursuant to State CEQA Guidelines Section 15088(b), and revisions and corrections to the Draft SEIR are found in Section 3 of the Final SEIR.

City staff has reviewed this material and determined that none of this material constitutes the type of significant new information that requires recirculation of the Draft SEIR for further public comment under CEQA Guidelines Section 15088.5. None of this new material indicates that the project will result in a significant new environmental impact not previously disclosed in the Draft SEIR. Additionally, none of this material indicates that there would be a substantial increase in the severity of a previously identified environmental impact that will not be mitigated, or that there would be any of the other circumstances requiring recirculation described in Section 15088.5 of the CEQA Guidelines.

VII. STATEMENT OF OVERRIDING CONSIDERATIONS

CEQA requires decision makers to balance the benefits of the Proposed Project against its unavoidable environmental risks when determining whether to approve the project. If the benefits of the project outweigh the unavoidable adverse effects, those effects may be considered “acceptable” (State CEQA Guidelines Section 15093[a]). CEQA requires the agency to support, in writing, the specific reasons for considering a project acceptable when significant impacts are infeasible to mitigate. Such reasons must be based on substantial evidence in the FEIR or elsewhere in the administrative

record (State CEQA Guidelines Section 15093 [b]). The agency's statement is referred to as a Statement of Overriding Considerations.

The following provides a description of the project's significant and unavoidable adverse impact and the justification for adopting a statement of overriding considerations.

A. SIGNIFICANT AND UNAVOIDABLE IMPACTS

Although most potential project impacts have been substantially avoided or mitigated, as described above, there remains nine project impacts for which complete mitigation is not feasible. The EIR identified the following significant unavoidable adverse impacts of the project, which would continue to be applicable upon implementation of the Proposed Project:

1. Air Quality

- **Impact 5.3-1:** TOP 2050 would be inconsistent with the South Coast AQMD AQMP because buildout under the plan would cumulatively contribute to the nonattainment designations of the SoCAB. Incorporation of Mitigation Measures 3-2 and AQ-1 into future development projects for the operation phase would reduce criteria air pollutant emissions associated with buildout of TOP 2050. Additionally, goals and policies in TOP 2050 would promote increased capacity for alternative transportation modes. However, due to the magnitude of residential units that would be developed under TOP 2050 to accommodate the RHNA, compared to the Approved Project, no additional mitigation measures are available that would reduce impacts below South Coast AQMD thresholds. Similar to the Approved Project, Impact 5.3-1 would remain *significant and unavoidable*.
- **Impact 5.3-2:** Buildout in accordance with TOP 2050 would generate short-term emissions that would exceed South Coast AQMD's regional significance thresholds and cumulatively contribute to the nonattainment designations of the SoCAB. Mitigation Measure 3-1 and the goals and policies of TOP 2050 would reduce construction-related air pollutant emissions to the extent feasible. Construction emissions associated with the Proposed Project would be similar to the Approved Project, because the Proposed Project would result in an increase in land use intensity rather than development of new, previously undeveloped areas of the City that would require substantial landform modification. However, individual projects accommodated under TOP 2050 may exceed the South Coast AQMD regional significance thresholds. Therefore, like the Approved Project, construction-related regional air quality impacts of developments that would be accommodated by TOP 2050 under Impact 5.3-2 would remain *significant and unavoidable*.
- **Impact 5.3-3:** Buildout in accordance with TOP 2050 would generate long-term emissions that would exceed South Coast AQMD's regional significance thresholds and cumulatively contribute to the nonattainment designations of the SoCAB. Mitigation Measure 3-2 and AQ-1, in addition to the goals and policies of TOP 2050, would reduce air pollutant emissions to the extent feasible. The measures and policies covering topics such as expansion of the pedestrian and bicycle networks, promotion of public and active transit, and support to increase building energy efficiency and energy conservation would also reduce criteria air pollutants within the City. However, Impact 5.3-3 would remain *significant and unavoidable* due to the increase in VOCs

from residential development associated with TOP 2050 compared to that of the Approved Project.

- **Impact 5.3-4:** Buildout of TOP 2050 and the Approved Project could expose sensitive receptors to substantial concentrations of Toxic Air Contaminants (TACs). Buildout could result in new sources of criteria air pollutant emissions and/or TACs near existing or planned sensitive receptors. Review of development projects by South Coast AQMD for permitted sources of air toxics (e.g., industrial facilities, dry cleaners, and gasoline dispensing facilities) would ensure that health risks are minimized. Policy ER-4.9, New Localized Air Pollution Sources Near Existing Sensitive Receptors, would ensure mobile sources of TACs not covered under South Coast AQMD permits are considered during subsequent project-level environmental review by the City of Ontario. Individual development projects would be required to achieve the incremental risk thresholds established by South Coast AQMD, and TACs would be less than significant. However, implementation of TOP 2050 would generate TACs that could contribute to elevated levels in the air basin. This effect is more substantial with the Proposed Project compared to the Approved Project because of the increase in industrial land use allowed under the Proposed Project. While individual projects would achieve the project-level risk threshold of 10 per million, they would nonetheless contribute to the higher levels of cancer risk in the SoCAB; and therefore, result in a cumulatively considerable impact. Therefore, similar to the Approved Project, the Proposed Project's cumulative contribution to health risk is *significant and unavoidable*.

2. Cultural Resources

- **Impact 5.5-1:** Mitigation Measure 5-1 would require historic or potentially historic resources to be evaluated for historic significance through the City's Development Code tier system. Major modification or demolition of Tier III resources may be appropriate under certain circumstances. If demolition occurs, the City requires historic resources to be documented and historic features to be salvaged, and requires a demolition mitigation fee. Therefore, the ordinance does not provide a high level of protection for Tier III historic resources. Similar to the Approved Project, Impact 5.5-1 would remain *significant and unavoidable*.

3. Noise

- **Impact 5.13-1:** Mitigation Measure 12-4 would reduce potential impacts associated with construction from individual development projects to the extent feasible. However, due to the potential for proximity of construction activities to sensitive uses, the number of construction projects occurring simultaneously, and the potential duration of construction activities, Impact 5.13-1 could still result in a temporary substantial increase in noise levels above ambient conditions and exceedance of the 80 dBA L_{eq} threshold. Therefore, similar to the Approved Project, project and cumulative impacts of the Proposed Project would remain **significant and unavoidable**. It should be noted that the identification of this program-level impact does not preclude the finding of less-than-significant impacts for subsequent projects analyzed at the project level.
- **Impact 5.13-3:** Mitigation Measure 12-2 would reduce potential impacts associated with construction vibration from individual development projects to the extent feasible. However, due

to the potential for proximity of construction activities to sensitive uses, the number of construction projects occurring simultaneously, and the potential duration of construction activities, Impact 5.13-3 could be significant. Therefore, similar to the Approved Project, project and cumulative impacts associated with the Proposed Project would remain **significant and unavoidable**. It should be noted that the identification of this program-level impact does not preclude the finding of less-than-significant impacts for subsequent projects analyzed at the project level.

- **Impact 5.13-4:** With the implementation of Mitigation Measure 12-1, impacts to future sensitive receptors from excessive airport-related noise would be reduced to interior noise levels of 45 dBA CNEL or less. While interior noise levels are required to achieve the interior noise limits of Title 24 and Title 25, exterior noise levels may continue to exceed the noise compatibility criteria for the City. Consequently, airport noise compatibility impacts of the Proposed Project would remain **significant and unavoidable**, similar to the Approved Project.

4. Transportation

- **Impact 5.17-2:** Total VMT would increase under the Proposed Project compared to the Approved Project, primarily as a result of the increase in residential land use in the City. Mitigation Measure T-1 would reduce potential impacts for future development projects to the extent feasible. Future development projects consistent with TOP 2050 would need to consider transportation demand management (TDM) measures consistent with those identified in the Mobility Element. TDM techniques include incentives to use transit; incentives to form carpools rather than drive alone; and making home, work, and shopping closer together to shorten travel distances. VMT impacts under the Proposed Project would remain. Impact 5.17-2 would be **significant and unavoidable**.

B. PROJECT BENEFITS IN SUPPORT OF THE STATEMENT OF OVERRIDING CONSIDERATIONS

The following section describes the benefits of the Proposed Project that outweigh the project's unavoidable adverse effects and provides specific reasons for considering the project acceptable even though the FEIR has indicated that there will be nine significant project impacts if the mitigation measures for these impacts cannot be implemented. Accordingly, this Statement of Overriding Considerations regarding potentially significant adverse environmental impacts resulting from the Proposed Project, as set forth below, has been prepared. Pursuant to CEQA Guidelines Section 15093(c), the Statement of Overriding Considerations will be included in the record of the project approval and will also be noted in the Notice of Determination. Each of the benefits identified below provides a separate and independent basis for overriding the significant environmental effects of the Proposed Project.

Having reduced the potential effects of the Proposed Project through all feasible mitigation measures as described previously herein, and balancing the benefits of the Proposed Project against its potential unavoidable adverse impacts on air quality, cultural resources, noise, and transportation if the mitigation measures for these impacts cannot be implemented, the City finds that the following legal

requirements and benefits of the Proposed Project individually and collectively outweigh the potentially significant unavoidable adverse impacts for the following reasons:

1. Implements the Objectives Established for the Proposed Project

The proposed project would provide goals and policies that would facilitate and achieve the project objectives:

- Provide a technical update to the current TOP that updates the goals and policies to enhance public safety and livability, align with updated economic forecasts, and comply with new state laws while maintaining the foundation, vision, and objectives of the current TOP.
- Provide a streamlined, user-friendly, web-based TOP that is easily accessible to the public.
- Designate the distribution, location, balance, and extent of land uses, including residential, retail, employment, open space, and public uses.
- Link Ontario's community design goals to a broader context that includes economic development, land use, housing, community health, infrastructure, and transportation.
- Improve the balance between jobs and housing in the San Bernardino County subregion to reduce vehicle miles traveled and associated air quality impacts, consistent with regional policies on jobs-housing balance.
- Provide employment and housing opportunities for the San Bernardino Council of Governments subregion, consistent with the goals of the Southern California Association of Governments Sustainable Communities Program.
- Provide for high-intensity mixed-use urban centers along the I-10 corridor and citywide bus rapid transit system that reduce vehicle trips and incorporate smart growth principles.
- Foster the development of pedestrian and transit-oriented environments that create lively, appealing, and safe pedestrian areas, active during both daytime and evening hours.
- Maintain Ontario's distinct neighborhoods and districts to foster a positive sense of identity and belonging among residents and businesses.
- Establish a framework for using and managing the city's natural resources sustainably.
- Provide for the security and safe transportation of goods and hazardous materials, and maintain disaster preparedness and response and recovery systems to reduce loss of life, injury, private property damage, infrastructure damage, economic losses, and social dislocation.
- Enhance the capacity for the people, businesses, and public agencies that are in or serve Ontario to be resilient in cases of severe and/or prolonged weather conditions, natural disasters, and emergencies.

- Prioritize the improvement of areas most impacted by environmental justice issues, and enable Ontario residents to enjoy equal access to public facilities, civic engagement opportunities, nutritious foods, and safe and healthy environments.
- Correlate the mobility system with the future land use patterns and buildout levels of Ontario and with other transportation planning efforts by local, state, and federal authorities.
- Address a range of mobility options in Ontario, including vehicular, trucking, freight and passenger rail, air, pedestrian, bicycle, and transit.

2. Provides High-Density Housing that Helps Achieve the City’s Regional Housing Needs

The Proposed Project would introduce an additional 77,096 housing units in the City by 2050 compared to existing conditions, or 25,399 units compared to full buildout of the Approved Project. To make meaningful reforms to the housing crisis in California, the state Department of Housing and Community Development (HCD) recently declared that cities and counties in Southern California will have to plan for the construction of 1.3 million new homes in the next decade. The Southern California Association of Governments (SCAG) distributed the increased targets to jurisdictions based on factors such as jobs, households, and affordability that were considered in the City’s 2021-2029 Housing Element Update. For cities and counties that do not perform, the state can withhold state transportation revenue generated from Senate Bill 1 (2017). The Proposed Project includes refinements to the Policy Plan to comply with state housing mandates and accommodate the mandatory Regional Housing Needs Assessment (RHNA) allocation. TOP 2050 brings long-term growth and fiscal projections into alignment with current economic conditions as well as property owner and stakeholder requests, to support the vision for Ontario. TOP 2050 includes map changes to convert areas reserved for strictly commercial and office uses to mixed-use or residential areas that are better positioned to respond to the current and future economic climate and are positioned to help the City to meet its RHNA requirements. TOP 2050 also intensifies residential uses on key sites that align higher-density housing with regional plans for high-quality transportation routes and help to meet the City’s RHNA obligation.

3. Provides Employment Opportunities and Promotes the City’s Economic Vision

- TOP 2050 includes a Community Economics Element to protect investments in the City that improve the quality of life for Ontario’s residents, employees, and visitors. The Proposed Project brings long-term growth and fiscal projections into alignment with current economic conditions. The Proposed Project would provide employment opportunities for the economic activities envisioned for the City of Ontario. Buildout of TOP 2050 would allow development of approximately one million additional square feet of nonresidential buildings compared to the Approved Project. The Proposed Project would better align land uses, infrastructure plans, and job opportunities with the current and forecasted market conditions, ensuring that future development is more likely to occur during the buildout timeframe and decrease the likelihood of sites remaining perpetually vacant. Additionally, while the Proposed Project would represent a decrease of 17,065 total jobs compared the Approved Project, the Proposed Project still supports opportunities for generating an additional 164,003 jobs to the City’s existing 2021 employment of

131,999. The Proposed Project's employment projections of TOP 2050 would exceed SCAG's current regional forecasts for the City Ontario. The Community Economics Element includes policies that provide a complete community:

- **CE-1.1 Jobs-Housing Balance.** We pursue improvement to the Inland Empire's balance between jobs and housing by promoting job growth that reduces the regional economy's reliance on out-commuting.
- **CE-1.2 Jobs and Workforce Skills.** We use our economic development resources to:
 1. attract jobs suited for the skills and education of current and future City residents;
 2. work with regional partners to provide opportunities for the labor force to improve its skills and education; and
 3. attract businesses that increase Ontario's stake and participation in growing sectors of the regional and global economy.
- **CE-1.3 Regional Approach to Workforce Development.** We work with our partners to provide workforce training and development services throughout the region, recognizing that Ontario employers rely on workers living outside of the City.
- **CE-1.4 Business Retention and Expansion.** We continuously improve two-way communication with the Ontario business community and emphasize customer service to existing businesses as part of our competitive advantage.
- **CE-1.5 Business Attraction.** We proactively attract new and expanding businesses to Ontario in order to increase the City's share of growing sectors of the regional and global economy.

4. Included Goals and Policies for Community Health, Environmental Justice, Climate Adaptation and Resiliency, and Mobility.

The Proposed Project would conform with new state laws related to community health, environmental justice, climate adaption, resiliency, and mobility.

As part of the technical background work for TOP 2050 a Vulnerability Assessment was conducted that analyzed how climate-related hazards may harm the community. This work is a requirement of California Government Code Section 65302(g)(4) as amended by Senate Bill (SB) 379. The Vulnerability Assessment Report followed the recommended process in the *California Adaptation Planning Guide*, which is the State's guidance for how local communities should conduct climate adaptation planning efforts, including vulnerability assessments. The Vulnerability Assessment Report presents the local and regional impacts created by climate change hazards and the ability of Ontario's populations and community to resist these hazards, to assess which aspects of the community are most vulnerable to climate change. TOP 2050 Safety Element includes goals and policies that were informed by the CCVA. Goals of TOP 2050 Safety Element include:

- **Goal S-1:** Minimized risk of injury, loss of life, property damage, and economic and social disruption caused by earthquake-induced and other geologic hazards.
- **Goal S-2:** Minimized risk of injury, loss of life, property damage and economic and social disruption caused by flooding and inundation hazards.
- **Goal S-3:** Reduced risk of death, injury, property damage and economic loss due to fires, accidents and normal everyday occurrences through prompt and capable emergency response.
- **Goal S-5:** Minimize the risk of injury, property damage, and economic loss resulting from windstorms and wind-related hazards.
- **Goal S-7:** Residential neighborhoods, commercial areas, and industrial districts that are kept safe through a multi-faceted approach of prevention, suppression, and community involvement in public safety.
- **Goal S-8:** Disaster resilient, prepared community through effective emergency/disaster preparedness, response, mitigation, and recovery.
- **Goal S-9:** Incorporate energy efficient practices and renewable energy systems to improve air quality, comfort, and energy reliability during temporary power outages.

TOP 2050 also conform with the requirements of Senate Bill 1000, the Planning for Health Communities Act (2016). The Vision, principles, goals, and policies throughout the Policy Plan reinforce the City's commitment to enabling all persons to enjoy equal access to healthy environments, healthy foods, parks and recreational facilities, and civic engagement opportunities. The Environmental Resources Element maps environmental justice areas. TOP 2050 weaves environmental justice (EJ) goals and policies throughout the Policy Plan. Some policies explicitly address residents living in the physical environment of environmental justice areas while others guide decisions, investments, and priorities that seek to improve issues and conditions that will benefit people within and around environmental justice areas. A sample of policies are listed below.

- **LU-2.2 Buffers.** We require new uses to provide mitigation or buffers between existing uses where potential adverse impacts could occur. Additional mitigation is required when new uses could negatively impact environmental justice areas.
- **LU-2.10 Sensitive Uses.** We monitor and share information with the community about stationary and non-stationary emission sources. We encourage siting and design of facilities to minimize health and safety risks on existing and proposed sensitive uses, especially in environmental justice areas.
- **ER-1.5 Water Resource Management.** Environmental justice areas are prioritized as we coordinate with local agencies to protect water quality, prevent pollution, address existing contamination, and remediate contaminated surface water and groundwater.

- **ER-4.4 Indoor Air Quality.** We will comply with State Green Building Codes relative to indoor air quality. We seek funding to improve indoor air quality for households with poor indoor air quality, with priority for lower income households in environmental justice areas.
- **ER-4.7 Other Agency Collaboration.** We collaborate with other agencies within the South Coast Air Basin to improve regional air quality at the emission source, with a particular focus on sources that affect environmental justice areas in Ontario.
- **ER-4.8 Tree Planting.** We protect healthy trees within the City and plant new trees to increase carbon sequestration and help the regional/local air quality. We expand the tree canopy in environmental justice areas to enhance air quality and reduce the “heat island” effect.
- **ER-4.9 New Localized Air Pollution Sources Near Existing Sensitive Receptors.** We require new developments to conduct a Health Risk Assessment for land uses that generate more than 100 trucks per day or 40 trucks per day by trucks operating transportation refrigeration units (TRU's) within 1,000 feet from sensitive land uses (California Health and Safety Code § 42705.5(a)(5)). If the health risk assessment determines the new development poses health hazards that increase the incremental cancer risk above the threshold established by the South Coast Air Quality Management District (AQMD), we will only approve permits upon the condition that adequate mitigation measures are proposed and implemented for potential impacts on the sensitive uses around the site and along the route within Ontario taken by the trucks to and from freeways. We require new developments that must perform a health risk assessment to conduct additional public outreach by sending notifications in multiple languages to all residents living within 500 feet, and encourage hosting a public meeting.
- **SR-1.2 Nutrition Choices.** We support the promotion of equitable access to affordable healthy food choices in the community, including community gardens, farmers markets, and cooking classes.
- **SR-1.3 Health Education.** We promote equitable access to health education, including disease prevention, mental health, nutrition, and physical fitness.
- **SR-1.4 Physical Activity.** We encourage activities and community design that improve the physical fitness of our community members, with an emphasis on the provision of activities and facilities in environmental justice areas.
- **SR-2.6 Language.** We promote broad outreach in languages used by the community for proposed projects that could negatively affect environmental justice areas.
- **SR-2.7 Community Engagement.** We promote targeted outreach and education to historically underrepresented groups to encourage meaningful participation in decision-making process for projects whose outcomes will affect land use in environmental justice areas.
- **PR-1.1 Access to Parks.** In all new residential development areas, we strive to provide a park and/or recreational facility within walking distance (1/4 mile) of every residence and prioritize the establishment of parks in environmental justice areas that do not have adequate access to parks.

- **PR-2.4 Access to Programs.** We provide a range of recreational and physical exercise programs that are accessible to residents of all income levels throughout the community and prioritize establishing and maintaining equitable access for residents in environmental justice areas.
- **S-1.4 Seismically Vulnerable Structures.** We conform to state law regarding unreinforced masonry structures and coordinate with not-for-profits to facilitate seismic retrofits in environmental justice areas and for low-income households.
- **S-6.5 Location of Hazardous Material Facilities.** We regulate facilities that will be involved in the production, use, storage, or disposal of hazardous materials, pursuant to federal, state, county, and local regulations, so that impacts to the environment and sensitive land uses are mitigated. We prohibit new hazardous waste facilities in close proximity to sensitive land uses and environmental justice areas.
- **S-8.7 Extreme Heat and Air Quality.** We work to ensure that all community members are informed about and have access to community cooling centers and clean air centers during extreme heat events or wildfires, with a focus on serving environmental justice communities. We support the development of extreme heat emergency response policies and practices to address these critical health risks in the community.
- **M-3.1 Transit Partners.** We maintain a proactive working partnership with transit providers to ensure that adequate public transit service is available, cost-efficient, and convenient, particularly for residents in environmental justice areas:

Land use changes in growth areas are intended to improve growth areas by encouraging the use of alternative forms of transportation and promoting healthier communities through land use planning that encourages walking and biking, promotes vibrant communities, puts residents in proximity to resources (i.e., jobs, grocery stores, retail), and aligns growth with planned infrastructure improvements and regional transportation goals.

5. Includes Goals, Policies, and an Update to the Community Climate Action Plan to reduce GHG emissions in the City.

The Proposed Project includes goals, policies, and an update to the Community Climate Action Plan (CCAP) to reduce GHG emissions in the City. The update to the CCAP identifies GHG emissions reduction targets to achieve the GHG reduction goals of the City of Ontario consistent with Senate Bill 32, Executive Order S-03-05, and substantial progress toward the State’s carbon neutrality goals of Executive Order B-55-18. The 2022 update to the CCAP would result in beneficial impacts to GHG emissions and co-benefits for air quality.

Various elements of TOP 2050 contain goals and policies to reduce GHG emissions in the City, including:

- **Goal ER-3:** Cost-effective and reliable energy system sustained through a combination of low impact buildings, site and neighborhood energy conservation, and diverse sources of energy generation that collectively helps to minimize the region’s carbon footprint.

- **ER-3.1 Conservation Strategy.** We require conservation as the first strategy to be employed to meet applicable energy-saving standards.
- **ER-3.2 Green Development – Communities.** We encourage the use of the LEED Neighborhood Development rating system, or similar mechanism, to guide the planning and development of all new communities.
- **ER-3.3 Building and Site Design.** We require new construction to incorporate energy efficient building and site design strategies, which could include appropriate solar orientation, maximum use of natural daylight, passive solar, and natural ventilation.
- **ER-3.4 Green Development – Public Buildings.** We require all new and substantially renovated City buildings in excess of 10,000 square feet achieve a LEED Silver Certification standard, as determined by the U.S. Green Building Council.
- **ER-3.5 Fuel-Efficient and Alternative Energy Vehicles and Equipment.** We require purchase and use vehicles and equipment that are fuel efficient and meet or surpass state emissions requirements and/or use renewable sources of energy.
- **ER-3.6 Generation- Renewable Sources.** We promote the use of renewable energy sources (e.g., solar, wind, biomass) in public and private sector development.
- **Goal ER-4: Improved indoor and outdoor air quality and reduced locally generated pollutant emissions.**
 - **ER-4.1 Land Use.** We reduce GHG and other local pollutant emissions through compact, mixed use, and transit-oriented development and development that improves the regional jobs-housing balance.
 - **ER-4.3 Greenhouse Gases (GHG) Emissions Reductions.** We will reduce GHG emissions in accordance with regional, state, and federal regulations.
 - **ER-4.4 Indoor Air Quality.** We will comply with State Green Building Codes relative to indoor air quality. We seek funding to improve indoor air quality for households with poor indoor air quality, with priority for lower income households in environmental justice areas.
 - **ER-4.5 Transportation.** We promote mass transit and non-motorized mobility options (walking, biking) to reduce air pollutant emissions.
 - **ER-4.6 Particulate Matter.** We support efforts to reduce particulate matter to meet State and Federal Clean Air Standards.
 - **ER-4.7 Other Agency Collaboration.** We collaborate with other agencies within the South Coast Air Basin to improve regional air quality at the emission source, with a particular focus on sources that affect environmental justice areas in Ontario.

- **ER-4.8 Tree Planting.** We protect healthy trees within the City and plant new trees to increase carbon sequestration and help the regional/local air quality. We expand the tree canopy in environmental justice areas to enhance air quality and reduce the “heat island” effect.
- **Goal S-9:** Incorporate energy efficient practices and renewable energy systems to improve air quality, comfort, and energy reliability during temporary power outages.
 - **S-9.1: Solar Energy.** We support and may incentivize the installation of residential and commercial solar panels and battery storage systems that can provide electricity during power outages.
 - **S-9.2: Renewable Energy.** Renovate existing city-owned facilities and plan future facilities to include renewable energy generation capacity and battery storage as part of an effort to make public facilities and services greener and more resilient to power outages.
 - **S-9.3: Energy Efficiency Retrofits.** We support and may incentivize retrofits to residential and commercial buildings that improve energy efficiency and insulation from extreme temperatures, giving priority towards low-income applicants.
- **Goal M-2:** A system of trails and corridors that facilitate and encourage active modes of transportation.
 - **M-2.1: Active Transportation.** We maintain our Active Transportation Master Plan to create a comprehensive system of on- and off-street bikeways and pedestrian facilities that are safe, comfortable, and accessible and connect residential areas, businesses, schools, parks, and other key destination points.
 - **M-2.2: Bicycle System.** We provide off-street multipurpose trails and Class II bikeways as our preferred paths of travel and use the Class III for connectivity in constrained circumstances. When truck routes and bicycle facilities share a right-of-way, we prefer Class I or Class IV bicycle facilities. We require new development to include bicycle facilities, such as bicycle parking and secure storage areas.
 - **M-2.3: Pedestrian Walkways.** We require streets to include sidewalks and visible crosswalks at major intersections where necessary to promote safe and comfortable mobility between residential areas, businesses, schools, parks, recreation areas, and other key destination points.
 - **M-2.4: Network Opportunities.** We use public rights-of-way and easements such as, utility easements, levees, drainage corridors, road rights-of-way, medians, and other potential options to maintain and expand our bicycle and pedestrian network. In urban, mixed- use, and transit-oriented Place Types, we encourage the use of underutilized public and private spaces to expand our public realm and improve pedestrian and bicycle connectivity.
- **Goal M-3:** A public transit system that is a viable alternative to automobile travel and meets basic transportation needs of the transit-dependent.

- **M-3.1 Transit Partners.** We maintain a proactive working partnership with transit providers to ensure that adequate public transit service is available, cost-efficient, and convenient, particularly for residents in environmental justice areas.
- **M-3.2 Alternative Transit Facilities at New Development.** We require new development adjacent to an existing or planned transit stop to contribute to the creation of transit facilities, such as bus shelters, transit bays and turnouts, and bicycle facilities, such as secure storage areas.
- **M-3.3 Transit-Oriented Development.** We may provide additional development-related incentives to those inherent in the Land Use Plan for projects that promote transit use and reduce vehicle miles traveled.
- **M-3.4 Bus Rapid Transit (BRT) Corridors.** We work with regional transit agencies to implement BRT service and reduce vehicle miles traveled by targeting destinations and corridors with the highest number of potential riders.
- **M-3.5 Light Rail.** We support extension of the Metro Rail Gold Line to Ontario, and will work to secure station locations at the proposed multimodal transit center.
- **M-3.6 Metrolink Expansion.** We advocate expansion of Metrolink service to include the Downtown and the multimodal transit center.
- **M-3.7 High Speed Rail.** We encourage the development of high-speed rail systems that would enhance regional mobility in Southern California and serve the City of Ontario.
- **M-3.8 Feeder Systems.** We work with regional transit agencies to secure convenient feeder service from the Metrolink station and the proposed multimodal transit center to employment centers in Ontario.
- **M-3.9 Ontario Airport Metro Center Circulator.** We will explore development of a convenient mobility system, including but not limited to shuttle service, people mover, and shared car system, for the Ontario Airport Metro Center.
- **M-3.10 Multimodal Transportation Center.** We intend to ensure the development of a multimodal transportation center near ONT airport to serve as a transit hub with amenities for transit riders, pedestrians, and bicyclists transitioning to local buses, BRT, the Gold Line, high-speed rail, the proposed Ontario Airport Metro Center Circulator, and other future transit modes. We support locations for the multimodal transportation center that are north of ONT airport, between Vineyard Avenue and Interstate 15.
- **M-3.11 Transit and Community Facilities.** We require the future development of community-wide serving facilities to be sited in transit-ready areas that can be served and made accessible by public transit. Conversely, we plan (and coordinate with other transit agencies to plan) future transit routes to serve existing community facilities.

The City of Ontario prepared the 2022 CCAP to update the community's strategic path to reducing GHG emissions beyond 2020, consistent with State requirements and TOP 2050. Specifically, this CCAP does the following:

- Identifies and updates sources of GHG emissions within the City of Ontario's municipal boundaries for the calendar year of 2019 and estimates how these emissions may change over time.
- Identifies GHG reduction targets.
- Provides strategies in various sectors to meet or exceed the state targets of reducing emissions 40 percent below 1990 levels by 2030 and 80 percent below 1990 levels by 2050, consistent with the direction of the State of California via Assembly Bill (AB) 32, Governor's Executive Order S-03-05, and California Public Resources Code Section 21083.3.
- Provides substantial evidence that the emission reductions estimated in the CCAP are feasible, with supporting technical detail.
- Provides an implementation program and discusses the various outcomes of reduction efforts and how these reduction efforts can be implemented.
- Serves as the programmatic tiering document for review of the climate change impacts of projects under CEQA.

6. Consistency with the Regional Goals in the RTP/SCS

SCAG's Connect SoCal was adopted in September 2020. The RTP/SCS identifies that land use strategies that focus on new housing and job growth in areas served by high quality transit and other opportunity areas would be consistent with a land use development pattern that supports and complements the proposed transportation network. The overarching strategy in Connect SoCal RTP/SCS is to provide for a plan that allows the southern California region to grow in more compact communities in existing urban areas; provide neighborhoods with efficient and plentiful public transit and abundant and safe opportunities to walk, bike, and pursue other forms of active transportation; and preserve more of the region's remaining natural lands.

SCAG's Connect SoCal identifies several types of Priority Growth Areas in Ontario, including High-Quality Transit Areas, Transit Priority Areas, Neighborhood Mobility Areas, and Livable Corridors. TOP 2050 would promote growth consistent with these Priority Growth Areas, as proposed land use changes under TOP 2050 are intended to encourage walking and biking, put residents in proximity to resources, and align future growth in Ontario with planned infrastructure improvements and regional transportation goals. In addition, TOP 2050 includes several policies that promote strategic growth in support of sustainability goals.

- **LU-1.1 Strategic Growth.** We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, foster the development of transit, and support the expansion of the active and multimodal transportation networks throughout the City.

- **LU-1.2 Sustainable Community Strategy.** We integrate state, regional, and local Sustainable Community/Smart Growth principles into the development and entitlement process.
- **LU-1.3 Adequate Capacity.** We require adequate infrastructure and services for all development.
- **LU-1.5 Jobs-Housing Balance.** We coordinate land use, infrastructure, and transportation planning and analysis with the regional, county, and other local agencies to further regional and subregional goals for jobs-housing balance.
- **LU-4.3 Infrastructure Timing.** We require that the necessary infrastructure and services be in place prior to or concurrently with development.
- **H-2.1 Corridor Housing.** We revitalize transportation corridors by encouraging the production of higher density residential and mixed-uses that are architecturally, functionally, and aesthetically suited to corridors.

The goals and policies of TOP 2050 is consistent with the overall objectives of SCAG’s RTP/SCS, which include maximizing mobility; ensuring safe, sustainable and reliable travel; encouraging active transportation; encouraging energy efficiency; and encouraging land use growth that facilitate transit and non-motorized transportation.

7. Consistency with the Ontario International Airport (ONT) and Chino Airport ALUC

Airport Land Use Commission (ALUC) review is required for adoptions of, or amendments to a General Plan or Specific Plan; zoning ordinance; Master Plan for public use airports; and heliports within the airport influence area (Public Utilities Code Sections 21676(b), 21676(c), 21664.5, and 21661.5).

The Land Use Element of TOP 2050 states that all new developments surrounding ONT should be consistent with the adopted Airport Land Use Compatibility Plan (ALUCP) and should meet standards and recommendations of Part 77 of the FAA, adopted through Ordinance 2758 in the Ontario Municipal Code. A consistency determination analysis for the ONT was prepared by the City, submitted to the ONT-IAC Technical Advisory Committee, and found that TOP 2050 is consistent with ALUCP for ONT.

Buildout of TOP would involve development within the Chino Airport influence area. Land uses within the Chino Airport Overlay include Medium Density Residential, Mixed Use, Business Park, Industrial, and Open Space – Recreation. Projects accommodating TOP 2050 in this area would be required to meet the conditions of the Chino Airport Authority and the 2011 Caltrans Airport Land Use Planning Handbook, including those determining appropriate land uses, maximum population density, maximum site coverage, height restrictions, and required notification/disclosure areas based on the noise contours and runway protection, approach, and Part 77 zones of the FAA. Additionally, implementation of TOP 2050 would result in a beneficial impact for land use compatibility near Chino Airport as a result of the change from residential and business park to warehouse/industrial land uses.

The Airport Planning section of the TOP 2050 Land Use Element includes policies that would ensure airport planning compatibility and consistency. These policies include:

- **LU-5.1 Coordination with Airport Authorities.** We collaborate with FAA, Caltrans Division of Aeronautics, airport owners, neighboring jurisdictions, and other shareholders in the preparation, update, and maintenance of airport-related plans.
- **LU-5.2 Airport Planning Consistency.** We coordinate with airport authorities to ensure The Ontario Plan is consistent with state law, federal regulations, and/or adopted master plans, and airport land use compatibility plans for ONT and Chino Airport.
- **LU-5.3 Airport Impacts.** We work with agencies to maximize resources to mitigate the impacts and hazards related to airport operations – their homes.
- **LU-5.4 ONT Growth Forecast.** We support and promote an ONT that accommodates 30 million annual passengers and 1.6 million tons of cargo per year, as long as the impacts associated with that level of operations are planned for and mitigated.
- **LU-5.5 Airport Compatibility Planning for ONT.** We create and maintain the Airport Land Use Compatibility Plan for ONT.
- **LU-5.6 Alternative Process.** We fulfill our responsibilities and comply with state law with regard to the Alternative Process for proper airport land use compatibility planning.
- **LU-5.7 ALUCP Consistency with Land Use Regulations.** We comply with state law that requires general plans, specific plans, and all new development be consistent with the policies and criteria set forth within an Airport Land Use Compatibility Plan for any public-use airport.
- **LU-5.8 Chino Airport.** We will support the creation and implementation of the Airport Land Use Compatibility Plan for Chino Airport.
- **M-5.2 Land Use Compatibility with Regional Transportation Facilities.** We work with ONT, railroads, Caltrans, SBCTA, and other transportation agencies to minimize impacts.

Therefore, TOP 2050 ensures compatibility with ONT and Chino Airport.

8. Improves the City’s Jobs-housing balance.

The Proposed Project would also improve the City’s jobs-housing balance. TOP 2050 projections would represent a more balanced jobs-housing balance (2.3 jobs-housing ratio) than the Approved Project (3.0 jobs-housing ratio). The City’s jobs-housing ratio would also be more closely aligned to SCAG projections under TOP 2050 than under the Approved Project.

9. Other Considerations.

There are unavoidable, significant impacts in the following categories: Air Quality, Cultural Resources, Noise, and Transportation.

- If the City does not update TOP 2050, there would still be significant impacts relating to air quality and construction noise. Even without any growth in the City, which is not a realistic scenario, the significant impacts relating to air quality emissions will occur simply due to regional growth.
- Impacts relating to construction noise and vibration are temporary in nature.

C. Conclusion

The City Council hereby declares that, pursuant to the State CEQA Guidelines section 15093, the City Council has balanced the benefits of the Proposed Project against any unavoidable environmental impacts in determining whether to approve the Proposed Project. Pursuant to the State CEQA Guidelines, if the benefits of the Proposed Project outweigh the Proposed Project's unavoidable adverse environmental impacts, those impacts may be considered "acceptable."

Having reduced the adverse significant environmental effect of the Proposed Project to the extent feasible by adopting the Mitigation Measures contained in the SEIR, the Mitigation Monitoring and Reporting Program (MMRP), and this Resolution, having considered the entire administrative record on the Proposed Project, and having weighed the benefits of the Proposed Project against its unavoidable adverse impact after mitigation, the City Council has determined that each of the following social, economic and environmental benefits of the Proposed Project separately and individually outweigh the Proposed Project's potential unavoidable adverse impacts and render those potential adverse environmental impacts acceptable based upon the following overriding considerations: The Proposed Project will:

- A. Provide a technical update to the current TOP that updates the goals and policies to enhance public safety and livability, align with updated economic forecasts, and comply with new state laws while maintaining the foundation, vision, and objectives of the current TOP.
- B. Provide a streamlined, user-friendly, web-based TOP that is easily accessible to the public.
- C. Designate the distribution, location, balance, and extent of land uses, including residential, retail, employment, open space, and public uses.
- D. Link Ontario's community design goals to a broader context that includes economic development, land use, housing, community health, infrastructure, and transportation.
- E. Improve the balance between jobs and housing in the San Bernardino County subregion to reduce vehicle miles traveled and associated air quality impacts, consistent with regional policies on jobs-housing balance.
- F. Provide employment and housing opportunities for the San Bernardino County subregion, consistent with the goals of the Southern California Association of Governments' Sustainable Communities Program.
- G. Provide for high-intensity mixed-use urban centers along the I-10 corridor and in the Ontario Ranch that reduce vehicle trips and incorporate smart growth principles.

- H. Foster the development of pedestrian and transit-oriented environments that create lively, appealing, and safe pedestrian areas, active during both daytime and evening hours.
- I. Maintain Ontario’s distinct neighborhoods and districts to foster a positive sense of identity and belonging among residents and businesses.
- J. Establish a framework for using and managing the city’s natural resources sustainably.
- K. Provide for the security and safe transportation of goods and hazardous materials and maintain disaster preparedness and response and recovery systems to reduce loss of life, injury, private property damage, infrastructure damage, economic losses, and social dislocation.
- L. Enhance the capacity for the people, businesses, and public agencies that are in or serve Ontario to be resilient in cases of severe and/or prolonged weather conditions, natural disasters, and emergencies.
- M. Prioritize the improvement of areas most impacted by environmental justice issues, and enable Ontario residents to enjoy equal access to public facilities, civic engagement opportunities, nutritious foods, and safe and healthy environments.
- N. Correlate the mobility system with the future land use patterns and buildout levels of Ontario and with other transportation planning efforts by local, state, and federal authorities.
- O. Address a range of mobility options in Ontario, including vehicular, trucking, freight and passenger rail, air, pedestrian, bicycle, and transit.

The City Council hereby declares that the foregoing benefits provided to the public through the approval and implementation of the Proposed Project outweigh the identified significant adverse environmental impact of the Proposed Project that cannot be mitigated. The City Council finds that each of the Proposed Project benefits separately and individually outweighs all of the unavoidable adverse environmental effects identified in the SEIR and therefore finds those impacts to be acceptable.

VIII. MITIGATION MONITORING AND REPORTING PROGRAM

Pursuant to Public Resources Code section 21081.6, the City Council hereby adopts the Mitigation Monitoring and Reporting Program (“MMRP”) attached as Exhibit “C.” Implementation of the mitigation measures contained in the MMRP is hereby made a condition of approval of the Project. In the event of any inconsistencies between the mitigation measures set for herein and the MMRP, the MMRP shall control.

IX. CERTIFICATION

The City Council finds that it has been presented with the SEIR, which it has reviewed and considered, and further finds that the SEIR is an accurate and objective statement that has been completed in full compliance with CEQA, the State CEQA Guidelines and the City's Local CEQA Guidelines and that the SEIR reflects the independent judgment and analysis of the City Council.

The City Council declares that no evidence of new significant impacts as defined by State CEQA Guidelines section 15088.5 has been received by the City Council after circulation of the Draft SEIR which would require recirculation.

Therefore, the City Council hereby certifies the SEIR based on the entirety of the record of proceedings.

ATTACHMENT C:

MITIGATION MONITORING AND REPORTING PROGRAM

A Component of The Ontario Plan 2050

July 2022 | Mitigation Monitoring and Reporting Program
State Clearinghouse No. 2021070364

The Ontario Plan 2050

for City of Ontario

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Table of Contents

Section	Page
1. INTRODUCTION	1
1.1 PURPOSE OF MITIGATION MONITORING AND REPORTING PROGRAM.....	1
1.2 PROJECT LOCATION.....	1
1.3 PROJECT DESCRIPTION.....	2
1.4 ENVIRONMENTAL IMPACTS.....	3
2. MITIGATION MONITORING REQUIREMENTS	5
2.1 CATEGORIZED MITIGATION MEASURES/MATRIX.....	5
3. REPORT PREPARATION.....	23
3.1 LIST OF PREPARERS.....	23

Table of Contents

List of Tables

Table		Page
Table 1	Comparison of Approved TOP to TOP 2050	3
Table 2	Mitigation Monitoring Requirements	7

1. Introduction

1.1 PURPOSE OF MITIGATION MONITORING AND REPORTING PROGRAM

This Mitigation Monitoring and Reporting Program (MMRP) has been developed to provide a vehicle by which to monitor mitigation measures and conditions of approval outlined in the Draft Supplemental Environmental Impact Report (Draft SEIR), State Clearinghouse No. 2021070364 for The Ontario Plan 2050 (TOP 2050 or Proposed Project). The MMRP has been prepared in conformance with Section 21081.6 of the Public Resources Code and City of Ontario Monitoring Requirements. Section 21081.6 states:

- (a) When making findings required by paragraph (1) of subdivision (a) of Section 21081 or when adopting a mitigated negative declaration pursuant to paragraph (2) of subdivision (c) of Section 21080, the following requirements shall apply:
 - (1) The public agency shall adopt a reporting or monitoring program for the changes made to the project or conditions of project approval, adopted in order to mitigate or avoid significant effects on the environment. The reporting or monitoring program shall be designed to ensure compliance during project implementation. For those changes which have been required or incorporated into the project at the request of a responsible agency or a public agency having jurisdiction by law over natural resources affected by the project, that agency shall, if so requested by the lead or responsible agency, prepare and submit a proposed reporting or monitoring program.
 - (2) The lead agency shall specify the location and custodian of the documents or other material which constitute the record of proceedings upon which its decision is based.

1.2 PROJECT LOCATION

The City of Ontario is in the southwestern portion of San Bernardino County and is surrounded by the cities of Chino and Montclair and unincorporated San Bernardino County to the west; the cities of Upland and Rancho Cucamonga to the north; the City of Fontana and unincorporated San Bernardino County to the east; and the cities of Eastvale and Jurupa Valley to the south. The City is in the central part of the Upper Santa Ana River Valley, bounded by the San Gabriel Mountains to the north; the Chino Hills, Puente Hills, and San Jose Hills to the west; the Santa Ana River to the south; and Lytle Creek Wash on the east.

The City comprises approximately 50 square miles (32,022 acres), including the 8,200-acre Ontario Ranch in the southern part of the City—formerly known as the New Model Colony and formerly the City's sphere of influence. The northern, more urbanized part of the City is known as the Original Model Colony in reference to the City's founding as a model colony for cities in terms of layout and infrastructure. Generally, the City is bounded by Benson Avenue and Euclid Avenue on the west; Interstate 10 (I-10), 8th Street, and 4th Street on

1. Introduction

the north; Etiwanda Avenue and Hammer Avenue on the east; and Merrill Avenue and the San Bernardino County/Riverside County boundary on the south. Regional circulation to and through the City is provided by I-10 and State Route (SR) 60, east to west, and by I-15 and SR-83 (Euclid Avenue), north to south. The City is also home to the Ontario International Airport and proximate to Chino Airport.

1.3 PROJECT DESCRIPTION

The Proposed Project, The Ontario Plan (TOP) 2050, is an update to The Ontario Plan (TOP or Approved Project) to guide the City's development and conservation for the next 30 years through 2050. The Proposed Project is a focused effort, with particular emphasis on technical refinements to the Policy Plan to comply with state housing mandates; conform with new state laws related to community health, environmental justice, climate adaptation, resiliency, and mobility; bring long-term growth and fiscal projections into alignment with current economic conditions; and advance the Tracking and Feedback system and Implementation Plan.

TOP is the City's policy and implementation framework that guides the long-term growth and improvement of the Ontario community through six interrelated components of city governance:

A **Vision** that provides a sense of purpose and mission for city governance and sets the tone for the other components of TOP. The Vision's central theme is a sustained, community-wide prosperity that continuously adds value and yields benefits.

A **Governance Manual** that establishes a set of goals and policies to promote consistent City leadership based on the principles of regional leadership, transparency, long-term value, accountability, and inclusivity.

A **Policy Plan** that serves as the City's legally required general plan and that states long-term goals, principles, and policies to achieve Ontario's Vision through nine elements: land use, housing, mobility, safety, environmental resources, parks and recreation, community economics, community design, and social resources.

A list of **City Council Priorities** that shape the City's ongoing annual budgeting process, with a focus on a variety of short- and long-term goals and objectives.

An **Implementation Plan** that identifies the actions needed to carry out TOP's policies. This includes initiatives by the City such as establishing consistent land use zoning and creating objective development and design standards, as well as decisions on public and private development projects and City activity programs.

A **Tracking and Feedback** system that charts the City's progress toward achieving the Policy Plan goals, providing data and analysis that enables decision makers to make strategic course corrections in response to changing circumstances and monitor ongoing operational effectiveness.

TOP 2050 is an update to TOP to guide the City's development and conservation for the next 30 years through 2050. The Proposed Project is a focused effort, with particular emphasis on technical refinements to the Policy Plan to comply with state housing mandates; conform with new state laws related to community health, environmental justice, climate adaption, resiliency, and mobility; bring long-term growth and fiscal projections into alignment with current economic conditions; and advance the Tracking and Feedback system and Implementation Plan. TOP 2050 fulfills the mandatory Regional Housing Needs Assessment (RHNA)

1. Introduction

obligation. TOP 2050 brings long-term growth and fiscal projections into alignment with current economic conditions as well as property owner and stakeholder requests, all in support of the vision for Ontario.

Table 1, *Comparison of Approved TOP to TOP 2050*, provides a statistical summary of the buildout potential of TOP 2050 compared to existing conditions and to the buildout potential under the currently approved TOP. As shown in this table, TOP 2050 would increase population, dwelling units, and nonresidential buildings but would result in a small decrease in employment. The decrease in employment at buildout is largely because of automation in the industrial sector, with large warehousing and logistics buildings expected to create fewer new jobs through 2050 than a similarly sized industrial building was expected to create when the current TOP was adopted in 2010.

Table 1 Comparison of Approved TOP to TOP 2050

Scenario	Units	Population	Nonresidential Square Feet	Employment
Existing 2021 Conditions ¹	52,466	179,597	156,065,382	131,999
Approved TOP	104,163	357,957	260,399,271	313,067
Proposed TOP	129,562	410,492	261,491,779	296,002
Net Difference (Proposed TOP -Approved TOP)	25,399	52,535	1,092,508	-17,065

Note:

¹ See Chapter 4, *Environmental Setting*, for a summary of existing conditions.

1.4 ENVIRONMENTAL IMPACTS

1.4.1 Impacts Considered No Impact or Less Than Significant

The EIR identified various thresholds from the CEQA Guidelines among a number of environmental categories that would not be significantly impacts by the proposed project in Chapter 5, *Environmental Analysis*, and Chapter 8, *Impacts Found Not to Be Significant*, and therefore, did not require mitigation. Impacts to the following environmental resources were found to be less than significant or no impact:

- Aesthetics
- Agriculture and Forestry Resources
- Biological Resources
- Energy
- Greenhouse Gas Emissions
- Hydrology and Water Quality
- Land Use and Planning
- Mineral Resources
- Population and Housing
- Public Services
- Recreation
- Utilities and Service Systems
- Wildlife

1. Introduction

1.4.2 Potentially Significant Adverse Impacts That Can Be Mitigated, Avoided, or Substantially Lessened

The EIR concluded that the proposed project could result in one or more potentially significant impacts in the following topic areas:

- Cultural Resources (Archeological Resources)
- Geology and Soils (Paleontological Resources)
- Tribal Cultural Resources

However, the EIR also found that these impacts would be reduced, avoided, or substantially lessened through the implementation of mitigation measures, which are listed in Table 2, *Mitigation Monitoring Requirements*.

1.4.3 Unavoidable Significant Adverse Impacts

The following impact would remain significant and unavoidable after implementation of required mitigation, as identified in the EIR:

- Air Quality (AQMP Consistency, Regional Construction Emissions, Regional Operation Emissions, and Cumulative Health Risk)
- Cultural Resources (Historic Resources)
- Noise (Construction Noise, Construction Vibration, and Airport Land Use Compatibility)
- Transportation (Vehicle Miles Traveled)

2. Mitigation Monitoring Requirements

2.1 CATEGORIZED MITIGATION MEASURES/MATRIX

Project-specific mitigation measures (MM) have been categorized in matrix format, as shown in Table 2, *Mitigation Monitoring Requirements*. The matrix identifies the environmental factor, specific mitigation measures, schedule, and responsible monitor. The mitigation matrix will serve as the basis for scheduling the implementation of, and compliance with, all mitigation measures.

2. Mitigation Monitoring Requirements

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2. Mitigation Monitoring Requirements

Table 2 Mitigation Monitoring Requirements

Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)	
AESTHETICS					
No Mitigation Measures are applicable.					
AGRICULTURAL AND FORESTRY RESOURCES					
No Mitigation Measures are applicable.					
AIR QUALITY					
MM 3-1	<p>Prior to discretionary approval by the City of Ontario for development projects subject to CEQA (California Environmental Quality Act) review (i.e., nonexempt projects), project applicants shall prepare and submit a technical assessment evaluating potential project construction-related air quality impacts to the City of Ontario Planning Department for review and approval. The evaluation shall be prepared in conformance with South Coast Air Quality Management District (South Coast AQMD) methodology for assessing air quality impacts. If construction-related criteria air pollutants are determined to have the potential to exceed the South Coast AQMD-adopted thresholds of significance, the City of Ontario Building Department shall require feasible mitigation measures to reduce air quality emissions. Potential measures shall be incorporated as conditions of approval for a project and may include:</p> <ul style="list-style-type: none"> • Require fugitive dust control measures that exceed South Coast Air Quality Management District’s Rule 403, such as: <ul style="list-style-type: none"> – Requiring use of nontoxic soil stabilizers to reduce wind erosion. – Applying water every four hours to active soil disturbing activities. – Tarping and/or maintaining a minimum of 24 inches of freeboard on trucks hauling dirt, sand, soil, or other loose materials. • Using construction equipment rated by the United States Environmental Protection Agency as having Tier 4 interim or higher exhaust emission limits. • Ensuring construction equipment is properly serviced and maintained to the manufacturer’s standards. 	City of Ontario Planning Department in coordination with Landowner/Project Applicant	Prior to individual project approvals and during construction activities	City of Ontario Planning/Building Department	

2. Mitigation Monitoring Requirements

Table 2 Mitigation Monitoring Requirements

	Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
	<ul style="list-style-type: none"> • Limiting nonessential idling of construction equipment to no more than five consecutive minutes. • Using Super-Compliant VOC paints for coating of architectural surfaces whenever possible. A list of Super-Compliant architectural coating manufactures can be found on the South Coast Air Quality Management District's website at: http://www.aqmd.gov/prdas/brochures/Super-Compliant_AIM.pdf. <p>These identified measures shall be incorporated into all appropriate construction documents (e.g., construction management plans) submitted to the City and shall be verified by the City's Planning Department.</p>				
MM 3-2	The City of Ontario shall evaluate new development proposals within the City and require all developments to include access or linkages to alternative modes of transportation, such as transit stops, bike paths, and/or pedestrian paths (e.g. sidewalks).	City of Ontario Planning/Engineering Departments in coordination with the Landowner/Project Applicant	Prior to individual project approvals	City of Ontario Planning Department	
AQ-1	Prior to discretionary approval by the City of Ontario for development projects subject to CEQA (California Environmental Quality Act) review (i.e., nonexempt projects), project applicants shall prepare and submit a technical assessment evaluating potential project operation-phase-related air quality impacts to the City of Ontario Planning Department for review and approval. The evaluation shall be prepared in conformance with South Coast Air Quality Management District (South Coast AQMD) methodology in assessing air quality impacts. If operation-related air pollutants are determined to have the potential to exceed the South Coast AQMD-adopted thresholds of significance, the City of Ontario Planning Department shall require that applicants for new development projects incorporate mitigation measures to reduce air pollutant emissions during operational activities. The identified measures shall be included as part of the conditions of approval. Possible	City of Ontario Planning Department in coordination with the Landowner/Project Applicant	Prior to individual project approvals	City of Ontario Planning Department	

2. Mitigation Monitoring Requirements

Table 2 Mitigation Monitoring Requirements

Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>mitigation measures to reduce long-term emissions could include, but are not limited to the following:</p> <ul style="list-style-type: none"> • For site-specific development that requires refrigerated vehicles, the construction documents shall demonstrate an adequate number of electrical service connections at loading docks for plug-in of the anticipated number of refrigerated trailers to reduce idling time and emissions. • Applicants for manufacturing and light industrial uses shall consider energy storage and combined heat and power in appropriate applications to optimize renewable energy generation systems and avoid peak energy use. • Site-specific developments with truck delivery and loading areas and truck parking spaces shall include signage as a reminder to limit idling of vehicles while parked for loading/unloading in accordance with California Air Resources Board Rule 2845 (13 CCR Chapter 10 sec. 2485). • Provide changing/shower facilities as specified in Section A5.106.4.3 of CALGreen (Nonresidential Voluntary Measures). • Provide bicycle parking facilities per Section A4.106.9 of CALGreen (Residential Voluntary Measures). • Provide preferential parking spaces for low-emitting, fuel-efficient, and carpool/van vehicles per Section A5.106.5.1 of CALGreen (Nonresidential Voluntary Measures). • Provide facilities to support electric charging stations per Section A5.106.5.3 and Section A5.106.8.2 of CALGreen (Nonresidential Voluntary Measures; Residential Voluntary Measures). • Applicant-provided appliances shall be Energy Star–certified appliances or appliances of equivalent energy efficiency (e.g., dishwashers, refrigerators, clothes washers, and dryers). Installation of Energy Star–certified or equivalent appliances shall be verified by the City during plan check. 				

BIOLOGICAL RESOURCES

No Mitigation Measures are applicable.

2. Mitigation Monitoring Requirements

Table 2 Mitigation Monitoring Requirements

Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
CULTURAL AND PALEONTOLOGICAL RESOURCES				
<p>MM 5-1</p> <p>Historic or potentially historic resources in the City shall be evaluated for historic significance through the City's tier system prior to the issuance of plan or development approval. Pursuant to City's Development Code (Chapter 4, Permit, Actions, and Decisions, and Chapter 7, Historic Preservation), mitigation measures for all Tier III Historic Resources shall include the following:</p> <ul style="list-style-type: none"> a) Each historic resource shall be fully documented and cataloged pursuant to Historic American Building Survey/Historic American Engineering Record (HABS/HAER) standards, to provide a record of the resource, including, but not limited to: [i] the preparation of site plans, floor plans, exterior and interior elevations, and detail drawings of character defining features (such as moldings, stairs, etc.); and [ii] photographs of the resource, including the exterior, interior, and interior and exterior character defining features (such as moldings, light fixtures, trim patterns, etc.). b) A mitigation fee established pursuant to Section 7.01.030 (Historic Preservation Mitigation Fee) shall be paid to the City prior to the issuance of a demolition permit for Tier III historic resources. Fees for Tier I and II historic resources shall be determined during the Environmental Impact Report process. The fees established for Tier III will be used as a reference point for establishing fees for Tier I and II historic resources. c) A Certificate of Appropriateness shall not be issued for the demolition of an historic resource, either in whole or in part, until such time that a demolition permit application and a replacement structure has been approved by the City, and appropriate permits have been issued for its construction, unless: [i] a waiver is granted pursuant to Subsection H (Replacement Structure Waiver for Historic Resources Located within Industrial Zoning Districts) of Section 4.02.050; [ii] a deferral of the replacement structure requirement is granted pursuant to Subsection G (Replacement Structure Deferral) of Section 4.02.050; or [iii] demolition is required 	<p>City of Ontario Planning Department</p>	<p>Prior to individual project approvals</p>	<p>City of Ontario Planning Department</p>	

2. Mitigation Monitoring Requirements

Table 2 Mitigation Monitoring Requirements

	Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
	<p>pursuant to Section 7.01.050 (Unsafe or Dangerous Conditions) of this Development Code.</p> <p>d) In an effort to preserve features and artifacts from historic resources, a determination whether items within or on the resource should be salvaged must be made by the Planning Department and may include the local historical society prior to the issuance of the demolition permit. The applicant shall be responsible for the removal, relocation, storage, and donation of such items selected for salvaging. The applicant shall provide an inventory of salvaged items to the Planning Department, and shall include a list of each item name, description, and dimension (as necessary), and the location of each item on a floor plan.</p>				
MM 5-2	<p>In areas of documented or inferred from evident archaeological and/or paleontological resource presence, City staff shall require applicants for development permits to provide studies to document the presence/absence of such resources. On properties where resources are identified, such studies shall provide a detailed mitigation plan, including a monitoring program and recovery and/or in situ preservation plan, based on the recommendations of a qualified cultural preservation expert. The mitigation plan shall include the following requirements:</p> <p>a) Archaeologists and/or paleontologist shall be retained for the project and will be on call during grading and other significant ground-disturbing activities.</p> <p>b) Should any cultural resources be discovered, no further grading shall occur in the area of the discovery until the Planning Director or designee is satisfied that adequate provisions are in place to protect these resources.</p> <p>c) Unanticipated discoveries shall be evaluated for significance by a San Bernardino County Certified Professional Archaeologist/Paleontologist. If significance criteria are met, then the project shall be required to perform data recovery, professional identification, radiocarbon dates, and other special studies; submit materials to a museum for permanent curation;</p>	City of Ontario Planning Department in coordination with Landowner/Project Applicant	Prior to individual project approvals and during ground disturbing activities	City of Ontario Planning Department	

2. Mitigation Monitoring Requirements

Table 2 Mitigation Monitoring Requirements

Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
and provide a comprehensive final report including catalog with museum numbers.				
ENERGY				
No Mitigation Measures are applicable.				
GEOLOGY AND SOILS				
See MM 5-2				
GREENHOUSE GAS EMISSIONS				
No Mitigation Measures are applicable.				
HAZARDS AND HAZARDOUS MATERIALS				
No Mitigation Measures are applicable.				
HYDROLOGY AND WATER QUALITY				
No Mitigation Measures are applicable.				
LAND USE AND PLANNING				
No Mitigation Measures are applicable.				
MINERAL RESOURCES				
No Mitigation Measures are applicable.				
NOISE				
MM 12-1	Prior to the issuance of building permits for any project that involves a noise-sensitive use within the 65 dBA CNEL contour of the Ontario International Airport, the project property owner/developers shall retain an acoustical engineer to conduct an acoustic analysis and identify, where appropriate, site design and/or required building acoustical improvements (e.g., sound transmission class rated windows, doors, and attic baffling), to ensure compliance with the City's Noise Compatibility Criteria and the California State Building Code and California Noise Insulation Standards (Titles 24 and 21 of the California Code of Regulations).	City of Ontario Planning/Building Department in coordination with Landowner/Project Applicant	Prior to individual project approvals	City of Ontario Planning/Building Department

2. Mitigation Monitoring Requirements

Table 2 Mitigation Monitoring Requirements

	Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
MM 12-2	<p>Prior to issuance of a building permit, individual projects that involve vibration-intensive construction activities, such as pile drivers, jack hammers, and vibratory rollers occurring near sensitive receptors shall be evaluated for potential vibration impacts. For construction within 135 feet of fragile structures, such as historical resources, within 100 feet of nonengineered timber and masonry buildings (e.g., most residential buildings), or within 75 feet of engineered concrete and masonry (no plaster); or a vibratory roller within 25 feet of any structure, the project applicant shall prepare a noise and vibration analysis to assess and mitigate potential noise and vibration impacts related to these activities. This noise and vibration analysis shall be conducted by a qualified and experienced acoustical consultant or engineer. The vibration levels shall not exceed Federal Transit Administration (FTA) architectural damage thresholds (e.g., 0.12 inches per second [in/sec] peak particle velocity [PPV] for fragile or historical resources, 0.2 in/sec PPV for nonengineered timber and masonry buildings, and 0.3 in/sec PPV for engineered concrete and masonry). If vibration levels would exceed this threshold, alternative uses shall be used, such as drilling piles as opposed to pile driving and static rollers as opposed to vibratory rollers. If necessary, construction vibration monitoring shall be conducted to ensure vibration thresholds are not exceeded.</p>	<p>City of Ontario Planning/Building Department in coordination with Landowner/Project Applicant's construction contractor</p>	<p>Prior to and during construction</p>	<p>City of Ontario Building Department</p>	
MM 12-4	<p>Construction activities associated with new development that occurs near sensitive receptors shall be evaluated for potential noise impacts. Construction contractors shall implement the following measures for construction activities in the City of Ontario. Construction plans submitted to the City shall identify these measures on demolition, grading, and construction plans. The City of Ontario Planning and Building Departments shall verify that grading, demolition, and/or construction plans submitted include these notations prior to issuance of demolition, grading, and/or building permits.</p> <ul style="list-style-type: none"> • Construction activity is limited to the hours between 7:00 am and 6:00 pm Monday through Friday and 9:00 am to 6:00 pm 	<p>City of Ontario Planning/Building Department in coordination with Landowner/Project Applicant's construction contractor</p>	<p>During construction</p>	<p>City of Ontario Building Department</p>	

2. Mitigation Monitoring Requirements

Table 2 Mitigation Monitoring Requirements

Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>Saturdays and Sundays, as prescribed in Municipal Code Section 5-29.09.</p> <ul style="list-style-type: none"> • During the entire active construction period, equipment and trucks used for project construction shall use the best-available noise control techniques wherever feasible (e.g., improved mufflers, equipment re-design, use of intake silencers, ducts, engine enclosures, and acoustically attenuating shields or shrouds). • Impact tools (e.g., jack hammers and hoe rams) shall be hydraulically or electrically powered wherever possible. Where the use of pneumatic tools is unavoidable, an exhaust muffler on the compressed air exhaust shall be used along with external noise jackets on the tools. • Stationary equipment such as generators and air compressors shall be located as far as feasible from nearby noise-sensitive uses. • Stockpiling shall be located as far as feasible from nearby noise-sensitive receptors. • Construction traffic shall be limited, to the extent feasible, to approved haul routes established by the City's Engineering Department. • At least 10 days prior to the start of construction activities, a sign shall be posted at the entrance(s) to the job site, clearly visible to the public, that includes permitted construction days and hours as well as the telephone numbers of the City's and contractor's authorized representatives that are assigned to respond in the event of a noise or vibration complaint. If the authorized contractor's representative receives a complaint, he/she shall investigate, take appropriate corrective action, and report the action to the City. • Signs shall be posted at the job site entrance(s), within the on-site construction zones, and along queueing lanes (if any) to reinforce the prohibition of unnecessary engine idling. All other equipment shall be turned off if not in use for more than 5 minutes. 				

2. Mitigation Monitoring Requirements

Table 2 Mitigation Monitoring Requirements

Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)	
<ul style="list-style-type: none"> During the entire active construction period and to the extent feasible, the use of noise-producing signals, including horns, whistles, alarms, and bells, shall be for safety warning purposes only. The construction manager shall use smart back-up alarms, which automatically adjust the alarm level based on the background noise level or switch off back-up alarms and replace with human spotters in compliance with all safety requirements and laws. Erect temporary noise barriers (at least as high as the exhaust of equipment and breaking line-of-sight between noise sources and sensitive receptors), as necessary and feasible, to maintain construction noise levels at or below the performance standard of 80 dBA L_{eq}. Barriers shall be constructed with a solid material that has a density of at least 1.5 pounds per square foot with no gaps from the ground to the top of the barrier and may be lined on the construction side with an acoustical blanket, curtain, or equivalent absorptive material. 					
POPULATION AND HOUSING					
No Mitigation Measures are applicable.					
PUBLIC SERVICES					
No Mitigation Measures are applicable.					
RECREATION					
No Mitigation Measures are applicable.					
TRANSPORTATION					
T-1	Prior to approval of discretionary projects subject to VMT reduction analysis, applicants shall demonstrate compliance with the City's VMT Guidelines for CEQA assessment of VMT impacts. For projects with VMT per Service Population exceeding the County's significance threshold, a mitigation plan shall be developed and implemented. Mitigation should consist of Transportation Demand Management (TDM) measures analyzed under a VMT-reduction methodology consistent with the California Air Pollution Control Officers Association's (CAPCOA) <i>Final Handbook for Analyzing</i>	City of Ontario Planning/Engineering Departments in coordination with Landowner/Project Applicant	Prior to individual project approvals	City of Ontario Planning Department	

2. Mitigation Monitoring Requirements

Table 2 Mitigation Monitoring Requirements

Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p><i>Greenhouse Gas Emission Reductions, Assessing Climate Vulnerabilities, and Advancing Health and Equity (2021)</i> and approved by the City of Ontario (if applicable). Examples of measures include but are not limited to:</p> <ul style="list-style-type: none"> • Pedestrian Network Improvements: constructing new sidewalks and/or improving damaged or substandard sidewalks that connect to a larger pedestrian network. • Construct or Improve Bike Facilities: constructing new or enhancing a single existing Class I, II or IV bike facility that connects to a larger bike network. • Construct or Improve Bike Boulevards: implementing a Class III bike boulevard on a local or collector street that is one travel lane in each direction, has a design speed of 25 mph or less and a design volume of 5,000 ADT or less. • Expand Bikeway Networks: constructing a network of interconnected new Class I, II, or IV bike facilities. • Provide End of Trip Bicycle Facilities: constructing facilities that support cyclists such as bike parking, lockers, and showers. • Implement Transit-Supportive Roadway Treatments: funding infrastructure improvements such as traffic signal modifications and roadway signing and striping that are dedicated to improving transit travel times and reliability. • Transit Passes: providing discounted or free transit fare to a specific geographic area, population group, or to the general public. • Vanpool Program: providing groups of 5 to 15 people with direct shuttle service between their workplace and residence. • Carshare Program (conventional or EV): providing access to a shared fleet of on-demand vehicles for short-term use/rental. Best practice is to discount carshare membership and provide priority parking for carshare vehicles to encourage use of the service. • Bikeshare Program (conventional or EV): providing access to a shared fleet of on-demand bicycles for short-term use/rental. 				

2. Mitigation Monitoring Requirements

Table 2 Mitigation Monitoring Requirements

Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)	
<p>Best practice is to discount bikeshare membership and dedicate bikeshare parking to encourage use of the service.</p> <ul style="list-style-type: none"> • Rideshare Program: providing access to and encouraging the use of a ridesharing platform or service. This could be an app, website, or other service that provides ride-matching coordination services. • Community-Based Travel Planning (CBTP): CBTP is a residential based approach to outreach, performed by trained advisors, that provides households within a targeted geographic area with customized information, incentives, and support to encourage the use of transportation alternatives in place of single occupancy vehicles. • Commute Trip Reduction (CTR) Program: CTR programs can be mandatory or voluntary, and involve providing information, coordination, services, infrastructure, and/or incentives for alternative modes such as ridesharing, vanpool, transit passes, and cycling. 					
TRIBAL CULTURAL RESOURCES					
MM 5-3	<p>Upon receipt of an application for a proposed project subject to CEQA and within the City's jurisdiction, the City's representative shall consult with the relevant tribe(s)' representative(s) to determine if the proposed project is within a culturally sensitive area to the tribe. If sufficient evidence is provided to reasonably ascertain that the site is within a [tribal] culturally sensitive area, an archaeologist shall prepare then a cultural resources assessment. The findings of the cultural resources assessment shall be incorporated into the CEQA documentation. A copy of the report shall be forwarded to the tribe(s). If mitigation is recommended in the CEQA document, the procedure described in Mitigation Measure 5-4 shall be followed.</p>	City of Ontario Planning Department	Prior to individual project approvals	City of Ontario Planning Department	
MM 5-4	<p>Prior to the issuance of grading permits for a proposed project for which the CEQA document defines cultural resource mitigation for potential tribal resources, the project applicant shall contact the designated tribe(s) to notify them of the grading, excavation, and</p>	City of Ontario Planning Department in coordination with	Prior to issuance of grading permit(s)	City of Ontario Planning Department	

2. Mitigation Monitoring Requirements

Table 2 Mitigation Monitoring Requirements

	Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
	monitoring program. The applicant shall coordinate with the City of Ontario and the tribal representative(s) to develop mitigation measures that address the designation, responsibilities, and participation of tribal monitors during grading, excavation, and ground-disturbing activities; scheduling; terms of compensation; and treatment and final disposition of any cultural resources, sacred sites, and human remains discovered on the site. The City of Ontario shall be the final arbiter of the conditions for projects within the City's jurisdiction.	Landowner/Project Applicant			
TRC-1	<p>Tribal Cultural Resources Monitoring. The project archaeologist, in consultation with interested tribes, the developer, and the City of Ontario, shall develop an archaeological monitoring plan (AMP) to address the details, timing, and responsibility of archaeological and cultural activities that will occur on the project site. Details in the AMP shall include:</p> <ol style="list-style-type: none"> 1. Project-related ground disturbance (including, but not limited to, brush clearing, grading, trenching, etc.) and development scheduling; 2. The development of a rotating or simultaneous schedule in coordination with the developer and the project archeologist for designated Native American Tribal Monitors from the consulting tribes during grading, excavation and ground disturbing activities on the site: including the scheduling, safety requirements, duties, scope of work, and Native American Tribal Monitors' authority to stop and redirect grading activities in coordination with all project archaeologists (if the tribes cannot come to an agreement on the rotating or simultaneous schedule of tribal monitoring, the Native American Heritage Commission shall designate the schedule for the onsite Native American Tribal Monitor for the proposed project); 3. The protocols and stipulations that the developer, City, Tribes, and project archaeologist will follow in the event of inadvertent cultural resources discoveries, including any newly discovered cultural resource deposits that shall be subject to a cultural resources evaluation. 	City of Ontario Planning Department in coordination with Landowner/Project Applicant's archeologist and interested tribes	Prior to and during construction	City of Ontario Planning Department	

2. Mitigation Monitoring Requirements

Table 2 Mitigation Monitoring Requirements

	Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
	<p>At least 30 days prior to application for a grading permit and before any brush clearance, grading, excavation, and/or ground disturbing activities on the site, the developer shall retain a tribal cultural monitor to monitor all ground-disturbing activities in an effort to identify any unknown archaeological resources.</p> <p>Pursuant to the AMP, a tribal monitor from the consulting tribe shall be present during the initial grading activities. If tribal resources are found during grubbing activities, the tribal monitoring shall be present during site grading activities.</p>				
TRC-2	<p>Treatment and Disposition of Cultural Resources. In the event that Native American cultural resources are inadvertently discovered during the course of any ground-disturbing activities, including but not limited to brush clearance, grading, trenching, etc., for the proposed project, the following procedures will be carried out for treatment and disposition of the discoveries:</p> <ol style="list-style-type: none"> 1. Temporary Curation and Storage: During the course of construction, all discovered resources shall be temporarily curated in a secure location on-site or at the offices of the project archaeologist. The removal of any artifacts from the project site will need to be thoroughly inventoried with tribal monitor oversight of the process; 2. Treatment and Final Disposition: The landowner(s) shall relinquish ownership of all cultural resources, including sacred items, burial goods, and all archaeological artifacts and nonhuman remains as part of the required mitigation for impacts to cultural resources. The applicant shall relinquish the artifacts through one or more of the following methods and provide the City of Ontario with evidence of same: <ol style="list-style-type: none"> a. Accommodate the process for on-site reburial of the discovered items with the consulting Native American tribes or bands. This shall include measures and provisions to protect the future reburial area from any future impacts. Reburial shall not occur until all cataloging, basic analysis, other analyses as recommended by the project archaeologist and approved by consulting tribes, and basic recordation have been completed; 	<p>City of Ontario Planning Department in coordination with Landowner/Project Applicant's archeologist and interested tribes</p>	<p>During construction</p>	<p>City of Ontario Planning Department</p>	

2. Mitigation Monitoring Requirements

Table 2 Mitigation Monitoring Requirements

Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>all documentation should be at a level of standard professional practice to allow the writing of a report of professional quality;</p> <p>b. A curation agreement with an appropriate qualified repository in San Bernardino County that meets federal standards per 36 CFR Part 79, and therefore the resource would be professionally curated and made available to other archaeologists/researchers for further study. The collections and associated records shall be transferred, including title, to an appropriate curation facility in San Bernardino County, to be accompanied by payment of the fees necessary for permanent curation;</p> <p>c. For purposes of conflict resolution, if more than one Native American tribe or band is involved with the project and cannot come to an agreement as to the disposition of cultural materials, materials shall be curated at the San Bernardino County Museum by default;</p> <p>d. At the completion of grading, excavation, and ground-disturbing activities on the site, a Phase IV Monitoring Report shall be submitted to the City documenting monitoring activities conducted by the project archaeologist and Native Tribal Monitors within 60 days of completion of grading. This report shall document the impacts to the known resources on the property; describe how each mitigation measure was fulfilled; document the type of cultural resources recovered and the disposition of such resources; provide evidence of the required cultural sensitivity training for the construction staff held during the required pregrade meeting; and, in a confidential appendix, include the daily/weekly monitoring notes from the archaeologist. All reports produced will be submitted to the City, County Museum, and consulting tribes.</p>				

2. Mitigation Monitoring Requirements

Table 2 Mitigation Monitoring Requirements

Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
UTILITIES AND SERVICE SYSTEMS				
No Mitigation Measures are applicable.				
WILDFIRE				
No Mitigation Measures are applicable.				
Note: MM indicates existing mitigation measures from the Approved Project, while others (AQ, T, and TRC) indicate new mitigation measures included as part of the Proposed Project.				

2. Mitigation Monitoring Requirements

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3. Report Preparation

3.1 LIST OF PREPARERS

City of Ontario

Rudy Zeledon, Planning Director

Kimberly Ruddins, AICP, Sustainability Program Manager

Thomas Grahn, Senior Planner

PlaceWorks

Nicole Vermilion, Principal

Vivian Kha, Project Planner

3. Report Preparation

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RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING THE CITY COUNCIL APPROVE FILE NO PGPA20-002 PART B, AN AMENDMENT TO THE ONTARIO PLAN GOVERNANCE MANUAL, A POLICY COMPONENT OF THE ONTARIO PLAN SUPPORTING A BUSINESS PLAN APPROACH TO GOVERNANCE AND APPROVING THE ONTARIO PLAN VISION.

WHEREAS, the City of Ontario Certified the Environmental Impact Report prepared for The Ontario Plan and associated Statement of Overriding Considerations, and issued Resolution No. 2010-003; and

WHEREAS, the City of Ontario adopted The Ontario Plan on January 27, 2010, and issued Resolution No. 2010-004; and

WHEREAS, the City of Ontario adopted The Ontario Plan Governance Manual, a Policy component of The Ontario Plan, and issued Resolution No. 2010-005; and

WHEREAS, the Vision is a component of The Ontario Plan. The Vision was designed to guide the City towards a sustained, community-wide prosperity which continuously adds value and yields benefits. The Vision includes four points of vision:

- 1) A **Dynamic Balance** that enables our community to confront the continued dynamic growth of the region and technological change with confidence and a sense of opportunity;
- 2) A **Prosperous Economy** that sustains the perception and reality of prosperity across our entire community that positively impacts all the people of Ontario and is broadly – though not uniformly – shared;
- 3) **Distinctive Development** that integrates our varied and diverse focal points, districts, centers, corridors, and neighborhoods to provide a feeling of coherence without sacrificing uniqueness;
- 4) **Recognized Leadership** in local governance that stimulates excellence and serves to unify the people of Ontario in support of best practices in conducting public endeavors; and

WHEREAS, the City of Ontario is incorporated under State law as a general law city with a Council-Manager form of government; and

WHEREAS, the Governance Manual and the Policy Plan (General Plan) are the two stand-alone policy components of The Ontario Plan (TOP); and

WHEREAS, the Governance Manual provides policies to support two of the four Points of Vision: Prosperous Economy and Recognized Leadership; and

WHEREAS, the Governance Manual is an integrated component within TOP; and

WHEREAS, the Governance Manual, together with the Policy Plan provides the policy framework for the City's comprehensive long-term municipal business plan; and

WHEREAS, the Governance Manual is consistent with, and implements, the Vision through the following high-level guiding principles:

- 1) A clear Vision and Policy Plan which is communicated to City leaders, staff and the public, serves as the foundation for informed decision making;
- 2) Stable, predictable, timely and effective governance is essential to achieving the Ontario Vision;
- 3) The Ontario Vision should guide community decisions;
- 4) Our job is to create, maintain and grow economic value and we do our job by providing infrastructure and services;
- 5) Regular investment is needed to maintain the community's public and private built environment;
- 6) Ontario is in competition with other cities for this investment;
- 7) The better we do our job, the more investment we will attract and the more revenue it will generate for reinvestment, creating a self-sustaining cycle; and

WHEREAS, the Governance Manual contains five (5) sections: 1) Decision-Making, 2) Communication, 3) Implementation, 4) External Factors, and 5) Municipal Finance; and

WHEREAS, together, these sections:

- 1) Provide guidance for ongoing City leadership;
- 2) Describe how the City will operate in a business-like way;
- 3) Require the decision-making system to always add value;
- 4) Integrate TOP into the City's daily operations;

- 5) Guide the budget process (the most tangible expression of the City's commitments) towards the Vision;
- 6) Provide for effective administration of TOP; and

WHEREAS, TOP 2050 – Policy Plan Supplemental Environmental Impact Report (SEIR) was reviewed, studied, and found to comply with the California Environmental Quality Act (CEQA); and

WHEREAS, the City of Ontario made The Ontario Plan 2050 website (www.Ontarioplan.org/TOP2050) including the Vision and Governance Manual, available for public review beginning May 9, 2022; and

WHEREAS, the City of Ontario conducted a virtual open house on June 14, 2021 to gain input from impacted property owners; and

WHEREAS, the City of Ontario conducted a virtual meeting with Community Health Improvement Association on June 24, 2021, to gain input from members of the public; and

WHEREAS, the City of Ontario conducted a virtual Community Workshop #1 on June 28, 2021, to gain input from members of the public; and

WHEREAS, the City of Ontario conducted a virtual Community Workshop #2 on July 29, 2021, to gain input from members of the public; and

WHEREAS, the City of Ontario conducted outreach events at Concerts in the Park on August 25, 2021 and August 26, 2021, to gain input from members of the public and participate in the Environmental Justice Survey; and

WHEREAS, the City of Ontario conducted an outreach event at Taste of Ontario on March 19, 2022, to provide an update to the TOP 2050 and gain input from members of the public; and

WHEREAS, the City of Ontario conducted a series of virtual Open House/Office Hours on March 31, 2022, April 28, 2022, and May 26, 2022, to provide brief overview of TOP 2050 and provide the public with an informal format to ask questions and share insights; and

WHEREAS, the City of Ontario conducted a series of outreach meeting with the Ontario's Teen Action Committee on June 6, 2022, June 15, 2022, and June 16, 2022, providing an overview of the TOP 2050, climate adaptation and resiliency, and the importance of civic participation; and

WHEREAS, the City of Ontario conducted an in-person open house located inside City Hall on June 30, 2022, to provide an overview of the TOP 2050 Update SEIR, Policy Plan, and Community Climate Action Plan, and gain input from members of the public; and

WHEREAS, the Planning Commission is responsible for reviewing updates to the Policy Plan (General Plan) and Map and forwarding a recommendation to the City Council on the adoption of the General Plan and Map; and

WHEREAS, on July 26, 2022, the Planning Commission of the City of Ontario conducted a duly noticed public hearing on the General Plan Update at which time all persons wishing to testify in connection with the General Plan Update were heard; and

WHEREAS, all other legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, BE IT RESOLVED BY THE PLANNING COMMISSION OF THE CITY OF ONTARIO:

SECTION 1: Environmental Determination and Findings. Based on the entire record before the Planning Commission and all written and oral evidence presented to the Planning Commission, the Planning Commission recommends that the City Council finds TOP complies with CEQA (Public Res. Code, § 2100 et seq.), the State CEQA Guidelines (14 CCR § 1500 et seq.), and the City's CEQA Guidelines.

SECTION 2: Planning Commission Action. Based on the entire record before the Planning Commission, all written and oral evidence presented, and the findings made in this Resolution, the Planning Commission recommends the City Council adopts TOP Vision and Governance Manual as set forth in Attachment "A" (Vision) and Attachment "B" (Governance Manual).

SECTION 3: Concluding Facts and Reasons. As the recommending body for the Project, the Planning Commission finds that The Ontario Plan Governance Manual promotes the goals and objectives of the City of Ontario and hereby concludes as follows:

- 1) The Governance Manual is a long-term, vision-driven policy component of The Ontario Plan; and
- 2) The Governance Manual contains guiding principles that support a business plan approach to governance; and
- 3) The Governance Manual supports consistent City leadership sustained through time and succession; and

- 4) The Governance Manual promotes the principles of regional leadership, transparency, long-term value, accountability, and inclusivity; and
- 5) The Governance Manual is a compatible, integrated, and internally consistent component of The Ontario Plan.

SECTION 4: *Recommendation.* Based on the entire record before the Planning Commission and all written and oral evidence presented, the Planning Commission finds TOP Vision promotes the goals and objectives of the City of Ontario and the Governance Manual incorporates TOP Vision.

SECTION 5: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 6: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26th day of July 2022, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Jim Willoughby
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on July 26, 2022, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

Vision
A Component of The Ontario Plan 2050



City of Ontario Vision

A Component of The Ontario Plan 2050

Draft | May 2022

(as amended through _____)



Prepared By: PlaceWorks

3 MacArthur Place, Suite 1100

Santa Ana, CA 92707

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TABLE OF CONTENTS

INTRODUCTION..... 1
 A Premier Community by Design 1

DYNAMIC STABILITY 2

PROSPEROUS ECONOMY 2

DISTINCTIVE DEVELOPMENT 3
 Commercial and Residential Development 3
 Development Quality 3
 Mobility 4
 Infrastructure 4
 Public Safety 4
 Leisure and Cultural Activities 4
 Health and Education..... 5
 Resource Conservation and Enhancement..... 5

RECOGNIZED LEADERSHIP 5

INTRODUCTION

The central theme that motivates The Ontario Vision, as adopted by the City Council is:

A sustained, community-wide prosperity which continuously adds value and yields benefits.

It is important to note that adding value in this sense encompasses a range of applications, including adding value to prosperity as well as to people, making Ontario truly a *premier community* of choice. Adding value is expressed in land, revenues, infrastructure, businesses, as well as in education, employment, health, community services, recreation, etc. Adding value comprehensively is what the Ontario Vision is based on and is a keystone of The Ontario Plan. The essence of the central theme can be explained as:

A Premier Community by Design

Everything the City does and every action the City takes should be done with this simple yet comprehensive theme in mind, from the design quality of our built environment to the intent of designing our socio-economic programs, to the way in which we govern as a community.

In discussing a Vision that would endure for the lifetime of The Ontario Plan (30 years or more), the City Council recognized that there are four components that serve as the basic building blocks that set the foundation for a unified and prosperous community. These foundational blocks must be expressed and widely accepted throughout the Plan's lifetime. They are:

- A **Dynamic Balance** that enables our community to confront the continued dynamic growth of the region and technological change with confidence and a sense of opportunity.
- A **Prosperous Economy** that sustains the perception and reality of prosperity across our entire community that positively impacts all the people of Ontario and is broadly – though not uniformly – shared.
- **Distinctive Development** that integrates our varied and diverse focal points, districts, centers, corridors, and neighborhoods to provide a feeling of coherence without sacrificing uniqueness.
- **Recognized Leadership** in local governance that stimulates excellence and serves to unify the people of Ontario in support of best practices in conducting public endeavors.

Building on these foundational blocks, our Vision is supported and amplified by more specific ideas we call “Points of Vision.” Our Points of Vision are stated in the present tense to signify that we are now in the future and the Vision is real; it has been achieved. This required our community leaders who crafted the Vision to “mentally” relocate themselves a couple of decades into the future and describe the conditions they sought for our City. An easy way to think of these points is to mentally insert before each statement, the words “We have . . .” or “We are . . .” (as appropriate). In this way you as the reader can project yourself into the future and view the Ontario that was envisioned by our leaders.

One more thing needs to be said. The people of Ontario own this Vision. The use of the active “we” and “our” used throughout indicates that it is the community speaking, not just some anonymous technical writer. We strive to be the *premier community* in the Inland Empire.

DYNAMIC STABILITY

An ongoing commitment to maintaining balance among the competing priorities we face as evidenced by the way we allocate public resources and funds through a rigorous goal-setting and budgeting process.

A community that is consistently seeking—and thoughtfully applying—new ways of doing things better.

A community that readily recognizes and capitalizes on new opportunities and intervenes in problems before they have a chance to escalate.

Consistency—year-in and year-out—in setting priorities according to their importance over the long-term rather than focusing only on short-term gains.

Planning systems and personnel that exemplify the very best in professional practices, based on a stable Vision and constantly evolving Ontario Plan.

An established City culture that reflects an ongoing commitment to The Ontario Plan, and the Plan’s active use in a process of continuous improvement, informed decision making and a respect for the inherited benefits derived from past leadership.

The determination to use The Ontario Plan to enable us to leverage quality development and ensure qualities we want and reject those we do not.

An appreciation for the “personality and charm” of this community, preserving important characteristics and values even as growth and change occur, all the while retaining a distinctive local feel where people love to be.

Total commitment to coordinated implementation of the Ontario Vision by all City departments.

PROSPEROUS ECONOMY

A highly diverse economic base that capitalized on early regional centers, strategic corridor locations and international markets as the impetus for the extensive financial and technical centers that now exist.

Strengthened resiliency of Ontario’s economic base alongside the fostering of vibrant mixed-use opportunities throughout the City.

Sustained economic viability and leadership in the region that reflects a highly trained and diverse workforce and that confirms Ontario as an investment destination of choice.

A world-class air facility that is a focal point and magnet for dynamic, multi-faceted metropolitan commercial, cultural and tourist centers serving the economic interests of Ontario and the Southern California region.

Prudent public ownership and timely disposition of strategic properties where public ownership can be demonstrated to reinforce market forces in achieving the City's economic development and revitalization goals.

Innovative and highly productive industrial areas that set the standards in the region for efficient land use, environmental management, and workforce employment opportunities.

DISTINCTIVE DEVELOPMENT

Commercial and Residential Development

Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

A system of vibrant retail, town, and neighborhood centers responsive to market demands and woven into the fabric of the community.

A reputation for being good for business to work with while still satisfying broader community interests.

A demonstrated ability to attract housing in pursuit of our acknowledged responsibility to balance housing with the job growth that drives our quality of life.

One of the most comprehensive and diverse housing stocks in the region that offers broad choices for its diverse workforce and their families, ranging from entry level housing to executive level development; from semi-rural to highly urban.

Distinctive and well maintained neighborhoods that offer exceptional variety in lifestyles, with convenient access to schools, recreation and cultural facilities, places of worship, places of employment and shopping.

A contemporary arrangement of neighborhoods south of Riverside Drive unified with the older set of neighborhoods to the north, with care taken to preserve and enhance what is successful and selectively allowing intensification and revitalization where desired and appropriate.

Development Quality

A community that is so well maintained and litter-free that its properties uniformly convey a sense of prosperity that is readily apparent and a symbol of community pride.

Unique and authentic districts, centers, and corridors of varied scales that establish Ontario as one of the most vital and diverse locales in Southern California.

Superior quality and design of the built environment and open spaces through careful attention to detail at every scale, including public and private spaces and structures.

Mobility

A true multi-modal transportation system and user oriented management and techniques that facilitate an exceptional degree of movement and connectivity for people and goods to, from and within Ontario.

Comprehensive transit, pedestrian, and bicycle networks that provide connections to parks, open space amenities, schools, and other key destination points for our local citizens.

A track record of identifying and dealing effectively with “choke points” in our transportation system to prevent those constraints from limiting our mobility and economic potential.

Seamless transitions and connections between different transportation modes and between sectors of the City.

Infrastructure

Infrastructure systems that are properly sized to support approved land uses and their occupancy and are maintained in a timely fashion through funding by user groups.

A comprehensive water supply system that provides affordable and dependable volume and quality of water despite unprecedented pressures on the region’s water sources.

A multi-faceted, environmentally responsible energy management system that provides cost-effective power, even during high-demand periods.

Strategically located City owned properties and facilities that serve the City’s civic and economic interests, including public buildings, recreation, transportation, and other infrastructure.

Public Safety

A level of security and public safety that exceeds conventional standards, generates a sense of security throughout the community and establishes Ontario as one of the safest medium-sized cities in the nation.

A comprehensive network of safe routes between residential neighborhoods, schools, parks, community centers and cultural facilities.

Leisure and Cultural Activities

Recreation spaces, facilities and programs that offer ample recreation and cultural involvement by a diverse population.

Strong historic districts and cultural resources matching interests of a population with diverse cultural backgrounds, as well as offering a range of arts and entertainment venues of international significance.

Health and Education

A recognized healthy city, defined by public programs, advanced medical facilities (including a state-of-the-art hospital), engaged community based organizations, and land use/urban design features that foster healthy living.

An outstanding educational system that enables quality education for youth, offers lifelong learning for adults, provides for a variety of academic and vocational pathways from which to choose, and serves the needs of the business community, based on a strong community culture that highly values education.

Resource Conservation and Enhancement

Reduced air pollutants generated from Ontario on a regionally proportional basis, resulting in improved air quality in Ontario and the region.

Land preserved for the open space and recreation needs of the community and stewardship of natural resources.

A broad, integrated approach to resource allocation that will enable future generations to enjoy a quality of life at the same or higher level than the current generation.

RECOGNIZED LEADERSHIP

A City government that is renowned for applying contemporary business practices for those functions where private sector approaches are applicable.

A community that is widely recognized as a creative and effective partner and a leader within the Southern California region and the Inland Empire.

A community that continues to step into the future ahead of everyone else.

A pivotal force in identifying and implementing regional transportation system improvements and technologies that not only serve Ontario but provide critical regional access as well.

Continuance of effective governance and state-of-the-art practices that enable the municipal corporation to consistently provide exceptional levels of service to the people of Ontario at a relatively low per capita net cost.

A fine-tuned set of ordinances and regulations that implement and do not inadvertently impede our Vision, along with a widely emulated training program for officials and staff to ensure that these tools are understood and used effectively.

Strategic employment of leading new technology that has been rigorously evaluated and found to be cost-effective.

Creation and refinement of visualizations and analytics to clarify and market our Vision worldwide, based on one of the strongest data bases available to local government anywhere.

ATTACHMENT B:

Governance
A Component of The Ontario Plan 2050



City of Ontario

Governance Manual

A Component of The Ontario Plan 2050

Draft | May 2022

(as amended through _____)



Prepared By: PlaceWorks

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TABLE OF CONTENTS

INTRODUCTION..... 1

- Context 1
- Ontario Approach..... 1
- Purpose..... 1
- Principles..... 2

G1. DECISION MAKING 3

- Goal 3
- Policies 3
- Supporting Documents 3

G2. COMMUNICATION 6

- Goal 6
- Policies 6
- Supporting Documents 6

G3. IMPLEMENTATION 9

- Goal 9
- Policies 9

G4. EXTERNAL FACTORS 10

- Goal 10
- Policies 10
- Supporting Documents 10

G5. MUNICIPAL FINANCE..... 14

- Goal 14
- Policies 14

INTRODUCTION

The Governance Manual institutionalizes an approach to governance that leads to The Ontario Plan serving as a business plan for the City. The Manual includes a set of high-level governance principles with long-term value as well as Vision-driven goals and broad policies. Together, the goals and policies support consistent City leadership sustained through time and succession. The Governance Manual promotes the principles of regional leadership, transparency, long-term value, accountability, and inclusivity.

Context

The City of Ontario is incorporated under State law as a general law city with a Council-Manager form of government. The Mayor and Council are directly elected by the voters as their representatives. The City Council sets policy through the adoption of minute actions, resolutions and ordinances and determines the allocation of City resources through the adoption of the City Budget. The Mayor and Council hire a professional administrator, the City Manager, to hire staff, advise them, implement their policies, and run the day-to-day operation of the City. The Mayor and Council acting as a body, not as individuals, provide direction to the City Manager and staff in duly noticed public meetings.

Ontario Approach

The City of Ontario has a very definite approach to governance. It begins with the realization that the City is a corporation. It may be a municipal corporation, but it is a corporation nonetheless. The City operates using a corporate paradigm.

The Mayor and City Council are the elected board of directors. They are responsible for making policy. They hire a CEO, the City Manager, to advise them, implement their policies and run the day to day operations of the City.

We believe that the job of the City is to create, maintain and grow economic value. We do this through the provision of infrastructure and services. Everyone choosing to buy a home, shop, open a business or develop in our community has invested in our community. They are our shareholders. Their return on investment is the maintenance and growth of both quantitative (monetary) and qualitative (quality of life) value-added. ([Link to Complete Community](#))

Every action of the City and every interaction with current and potential shareholders is an opportunity to retain or capture their investment.

Purpose

The Governance Manual:

- Provides guidance for ongoing City leadership.
- Describes how the City will operate in a business-like way.

- Requires the decision-making system to always add value.
- Integrates The Ontario Plan into the City's daily operations.
- Guides the budget process (the most tangible expression of the City's commitments) towards the Vision.
- Provides for effective administration of The Ontario Plan.

Principles

We believe in:

- A clear Vision and Policy Plan which are communicated to City leaders, staff and the public, and serve as the foundation for informed decision making.
- Stable, predictable, timely and effective governance is essential to achieving the Ontario Vision.
- The Ontario Vision should guide community decisions.
- Our job is to create, maintain and grow economic value, and we do our job by providing infrastructure and services.
- Regular investment is needed to maintain the community's public and private built environments.
- Ontario is in competition with other cities for this investment.
- The better we do our job, the more investment we will attract and the more revenue it will generate for reinvestment, creating a self-sustaining cycle.

G1. DECISION MAKING

Ontario has succeeded in maintaining its leadership role through a system of decision-making that is based on communication, collaboration, and a clear understanding of and respect for roles within a Council-Manager form of government. The City Manager meets with City Council Members on a regular basis to ensure that Council has the information they need for decision-making. Key Management Staff members submit their best professional advice and recommendations through reports and budgetary requests, generated through broad staff collaboration, to the City Manager who transmits them to Council after determining consistency with Council's policy direction. The Ontario Plan (which encompasses the Vision, City Council Priorities, Ontario Governance Manual, Policy Plan, Implementation Program, and Tracking and Feedback), together with the Annual Budget, helps decision makers at every level to make informed choices that move Ontario toward its long-term Vision. In other words, the Plan is integrated into City operations at every level.

Goal

G1. Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.

Policies

G 1-1. Consistency with Policies. We require that staff recommendations to the City Council be consistent with adopted [City Council Priorities](#) (Goals and Objectives) and [The Policy Plan](#).

G 1-2. Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision.

G 1-3. Maintenance and Replacement Costs. We shall incorporate maintenance and replacement costs in the pricing of new programs and facilities as well as recording such deferred costs as debits against City revenues.

Supporting Documents

[The following section is a standalone document that is separate from the Governance Manual but is provided as a link on a website and included below for additional context.]

Interpreting Intent and Applying Policies: Decision-Making

The Ontario Plan deals with the reality that the Vision must be achieved in increments over a long period of time. Many factors applicable to its achievement will change during future decades. Consequently, it is important that decisions be weighed in terms of both short and long-range benefits. Tradeoffs will sometimes be required between these two perspectives. As City leadership transitions over time, the record of past decisions and their rationale will become increasingly valuable in helping to stay the course and still exercise reasonable flexibility. Therefore, management of the City's decision-making process requires diligent documentation

in a form that is readily retrievable. Some of the decision factors involved in this documentation include:

1. **Qualities.** Decision-making in Ontario must be viewed in the context of an institutional mind-set. In this case, that mind-set includes several critical qualities. They include:
 - **Imagination.** The Vision is ambitious. Imaginative thinking and approaches will be needed to achieve it.
 - **Agility.** Sometimes events move very quickly and in surprising directions. The City is determined to remain agile in adjusting to these situations, requiring a combination of wisdom grounded in experience and intelligence grounded in inventiveness.
 - **Innovation.** Often solutions are not ready-made. We live in a time of aggressive innovation, and municipal decision-making is capable of more far-reaching results if it benefits from innovative thinking.
2. **Documentation.** Integrating The Ontario Plan into day-to-day decision-making and its documentation becomes a powerful device for achieving the Vision. Decision-making must be documented at multiple levels (e.g., counter staff, department heads, city management, City Council, etc.) and incrementally over time. We are doing this for transparency so stakeholders will understand how and where decisions are made.
3. **Information.** What information was considered relevant in support of a decision? This is necessary to assess the degree to which that information is or is no longer relevant at a later date.
4. **Fiscal Considerations.** Virtually all of the City's decisions will have fiscal implications, and they must be considered in decision-making. Larger impacts and longer-term impacts will be carefully analyzed and documented. Included among these factors are:
 - Potential return on investment;
 - Net amount of value added;
 - Indebtedness;
 - Capital costs and one-time revenue offsets versus net costs for ongoing operation, maintenance, and reserves; and
 - Land use net revenues (where applicable).
5. **Indicators.** Indicators applicable to decision options include qualitative as well as quantitative measures. Not everything of importance can be "counted." But it should be possible to describe options qualitatively with sufficient clarity to distinguish among them. In any case, these tools are available to enable more informed decision-making as well as to create records of value to subsequent decision-makers.

6. Feedback. The City's system of Evaluation and Feedback will draw heavily on the documentation associated with decision-making. This involves more than data documentation. It also includes consistency determination with appropriate goals and policies.
7. Project Steps. An illustrative table containing project evaluation steps was prepared to demonstrate how The Ontario Plan works as a tool for decision-making. It offers a guided process for determining how responsive to the Plan a proposed project is. It does not attempt to impose a rigid procedure but, rather, seeks to provide a checklist by which a project may be evaluated.

G2. COMMUNICATION

Ontario's disciplined and ambitious approach to communication is key to sustaining its role as a regional leader and achieving the Ontario Vision. Communication is not the same as information: communication involves sending and receiving; information may be only one-way. Thus, Ontario's communication culture is one that focuses on the end-user and values accuracy, timeliness, credibility, clarity, and the capacity to exchange ideas. The intent of this culture is to enable the people and the decision-makers of Ontario to have the information they need to understand what is happening in their City, why it is happening, and what it means for the quality of their lives. It should also enable them to decide what to do in response to what the information tells them.

Goal

G2. Thoroughly informed and connected leaders, staff, public and shareholders.

Policies

G2-1. Quality. We require a professional standard of quality, no matter what medium is involved, to convey the sense and reality of accuracy, validity and honesty that befits this City.

G2-2. Accessibility. Our communications shall be designed to be easily accessible to end-users, including those with special needs such as hearing or visually impaired.

G2-3. Format. Our communications shall be presented in whatever formats are best able to meet the needs of the end-users.

G2-4. Coordination. We require coordination of communication activities and products among City departments so that coherent and internally consistent information is delivered to end-users.

G2-5. Coherence. We require a level of communication in which the information is understandable to the intended audience(s).

G2-6. Connection. We require communications to include a means by which the end-users may provide feedback to the City or pursue further clarification.

Supporting Documents

[The following section is a standalone document that is separate from the Governance Manual but is provided as a link on a website and included below for additional context.]

Interpreting Intent and Applying Policies: Communication

The City continues to make a considerable investment in information assets generated within City government as well as externally. It makes information available to not only City officials, but also many other interested stakeholders. Ontario's pride in its civic identity and Vision comes

across in all information the City communicates. Anything the City produces or responds to through its communication network is expected to manifest this pride. This commitment to professional quality written, oral, and visual communication is crucial as Ontario adapts to the constantly changing ways in which information is processed by end-users. It makes information available in appealing and useful ways regardless of the medium of delivery. The Ontario Plan website is a prime example of how Ontario capitalizes on changes in communication technology and trends in order to improve delivery of service to its citizens and maintain a competitive advantage.

It is important to recognize, as the City does, that there is a vast difference between communication and information. Communication is two-way and therefore requires an appreciation and understanding for the needs of all parties to the communication system. Ease of feedback and mutual respect among end-users and information generators are essential characteristics to be sustained. In the absence of this reciprocal nature, communication is at best merely information.

Here are several considerations:

1. **End-User.** The first thing that Ontario's communication culture calls for on the part of any author is an understanding of the audience. That is the basic "need definition" to be used in framing what is to be conveyed, the best format(s) to use, and the most effective means of interaction between the City and the end-user. This may suggest more than one version of the material at hand if there are multiple audiences that do not share common levels of information need.
2. **Transparency.** Communication should enlighten government processes, such as City decision-making, and be understood by those who will be affected by it.
3. **Medium.** Communications are not just a transfer of information; they are a source of intelligence. So the design of communications must be based on knowing what understanding and insights are desired. This has an important impact on the type of medium to be used. For example, graphic presentations may communicate much more effectively on certain topics than written form, enabling the end-user to grasp immediately the direct information and its relationships as well.
4. **Limitations of Information.** The degree of accuracy and availability of information, including any conditional limitations, should be explained. It is often more important to understand what one is not being told on a certain subject than what is being stated--and why. If the information is not known or not shared or its accuracy limited, that needs to be stated and explained. Care needs to be taken by those generating communications to understand and make known the degree of accuracy reflected in what is produced.
5. **Privacy.** It is essential to be constantly aware of privacy laws and legal limitations on what should be publicly accessible or not. The City rigorously complies with such laws. That principle needs to be broadly understood and respected throughout the City.

6. **Support.** Even though contemporary communication media offer significant increases in efficiency over past technology, they still require adequate support. This takes several forms: 1) funding through the budget process; 2) competent and currently knowledgeable staffing; 3) cost-effective equipment and systems; and 4) positive leadership and reinforcement.
7. **Accountability.** Many City departments, the City Council, City advisory bodies and numerous other public and quasi-public agencies produce information and process responses to it. In all cases, it is essential that accountability for quality, timeliness and usability, be maintained. This is the means of improving the level of communication serving the community. While there is no direct public control over the communication activities of private sector parties and governmental organizations other than the City of Ontario, the City expects these parties to also maintain a sense of responsibility and accountability in their communications.
8. **Identity.** Ontario seeks, in its Vision, to be a prosperous community and a regional leader. Therefore, all communications initiated by the City are expected to maintain a consistent image so that the end-user immediately recognizes the source and has a sense of confidence in the information.

These concepts go beyond principles, rules, and procedures. They reflect and demand a commitment to a governance culture in which a vital communication mentality prevails.

G3. IMPLEMENTATION

Realization of The Ontario Plan and its Vision will not happen by accident. Intentional actions are required. Actions occur at two levels. The first and most comprehensive carries out the policies of the Plan: initiatives, programs, organizational management, resource management, education and influence of other entities that actively move the City toward its destiny. The second level is the administration of the Plan: applying the Plan's policies to the City's decisions so that development projects, budgets, public agency projects, City regulations and similar activities respond to Plan policies appropriately and do their share toward achieving the Ontario Vision. Collectively, these actions are called implementation.

Goal

G3. Continuous progress toward achievement of the Ontario Vision through sustained initiatives and diligent administration by the City and others as specified in The Ontario Plan.

Policies

G 3-1. City Council Priorities. We shall set priorities and direct timely actions for implementing The Ontario Plan through periodic adoption of [City Council Priorities](#) (Goals and Objectives).

G 3-2. Tools. We shall create and maintain state-of-the-art systems, procedures and technology that provide maximum leverage from City resources in implementing The Ontario Plan.

G 3-3. Organization. We shall maintain a municipal organization under City Council direction that offers best professional practices and integrates The Ontario Plan and Vision into its operations.

G3-4. Administration. We shall operate a cost-effective system for administration of The Ontario Plan as proposed development plans, projects, and programs are considered by the City.

G3-5. Interagency Cooperation. We shall collaborate with other initiatives and programs of other agencies such as local, county, regional, state and federal governments and non-profit agencies to the maximum extent possible toward implementing The Ontario Plan.

G3-6. Monitoring Development and City Master Plans. We monitor development to ensure that it is consistent with City Master Plans (e.g., Water, Parks, Energy, Climate Action Plan, etc.) and The Ontario Plan. The Policy Plan in particular will incorporate changes to Master Plans as they are updated periodically.

G4. EXTERNAL FACTORS

In order to achieve its Vision, the City of Ontario benefits from the dynamic environment of which it is a part, but over which it does not have direct control. The context in which the City operates is remarkably complex and will become more so. The City chooses to prepare for this reality by addressing external factors before they've had their impact, rather than taking a reactive approach. The key is an attitude of awareness, consciousness, and intent—all aimed at establishing an optimum posture regarding external factors as the future evolves.

Goal

G4. Constant diligence in dealing with external factors impacting Ontario in a manner that contributes to the Ontario Vision.

Policies

G4-1. Legal Mandates. We shall comply with legal mandates upon the City so that they are satisfied and make maximum contribution to the Ontario Vision.

G4-2. Public and Private Partnerships. We shall engage in public and private partnerships where they offer opportunities for achieving the Ontario Vision beyond what could be achieved independently.

G4-3. Awareness. We shall actively maintain awareness of the external forces impacting Ontario so that their positive impacts can be optimized, and negative impacts minimized.

G4-4. Proactive Approach. We shall seek to influence external factors impacting Ontario sooner rather than later.

G4-5. Multiple Projections. We shall maintain a range of future projections instead of a single forecast as a means of understanding and managing the forces of change and promote the same approach by other governments.

G4-6. Interagency Cooperation. We shall participate with regional governments, surrounding cities and other agencies in devising optimum strategies for addressing external factors of mutual interest.

Supporting Documents

[The following section is a standalone document that is separate from the Governance Manual but is provided as a link on a website and included below for additional context.]

Interpreting Intent and Applying Policies: External Factors

External factors are forces acting on the City but not controlled by it. They include laws, regulations, decisions, activities, initiatives, and influences originating with numerous sources, including, but not limited to:

- State and federal governments,
- Courts,
- Quasi-governmental and non-governmental organizations, including nonprofit organizations,
- Special districts,
- Utility providers,
- Intergovernmental associations,
- Professional associations,
- Focused interest groups,
- Transportation system initiatives,
- The business community, especially development interests,
- Investors,
- Neighborhood and homeowner associations,
- Financial markets,
- Economic trends,
- Social changes, including demographic trends, and
- Environmental conditions and responses to them.

As encompassing as this list appears, it is not complete. But it illustrates the impressive array of interests and influences that are brought to bear on a single community, whether that is acknowledged or not. The essence of this component of The Ontario Plan is to deal with these forces intentionally rather than reactively. In many cases, there is little or nothing the City can do to influence, modify or blunt the impacts from these sources. Still, in these instances the City can understand and prepare for what is coming its way.

It is important to understand that this is not a new perspective for the City of Ontario. It has a track record of intergovernmental involvement, creative partnerships and scanning the forces that impact its options. Moreover, the City has developed a careful strategy for getting the most out of these forces in terms of what benefits the community. In this regard, it has been unusually effective. The thrust of this component of The Ontario Plan is to continue and build upon that foundation so that the City can sustain its awareness, consciousness, and intent to make the most of opportunities it encounters or can create.

The key ingredient that shapes The Ontario Plan is a thoughtful and practical Ontario Vision that motivates everything the City does. It is understandable that, by definition, external factors are driven by their own agenda—one that may or may not involve conscious consideration of

Ontario's priorities. Given that, the intent is that Ontario itself will inject that consideration into its influence or response to any external factor it must confront.

Clearly, it would be possible, given the rapid change and complex interactions of forces in our larger environment—regionally, nationally, and internationally—to be overwhelmed by these forces. That would accomplish nothing. And that is the reason that a major aspect of The Ontario Plan entails a conscious effort to stay in touch with its context by focusing on those forces that have the greatest potential to advance Ontario's future or, conversely, to detract from it. That insight then puts the City in the position to take timely action to achieve the best possible outcome.

It should be made clear that nothing in this Section is intended to suggest a reluctance on the part of the City to comply with the law nor to decline participation in thoughtful and mutually beneficial negotiations with any interested parties. Rather, it is to announce that Ontario has a Vision about which it is very serious and anyone engaging the City for whatever reason needs to be aware of that. Hopefully, because the City chooses to be so forthright about this aspect of its future, this posture will enjoy the respect of others.

Regarding general forces of change—economic, political, environmental, social, and demographic—the situation is a bit different. These are factors that operate on a scale of their own making and do not typically respond (at least, quickly) to governmental positions. Some are actually worldwide in scale, the understanding of which is evolving even as we watch. Uncertainty is rampant. The interactions stimulated by many of these conditions are simply beyond comprehension. So, what is an aspiring City in the midst of all of this to do?

The answer is to understand first and foremost what it seeks. Then it knows what tack to take in influencing or adapting to the dynamic forces of change. It will not always be right. Mistakes will be made. Totally unpredictable changes will occur, or trends will completely reverse without warning. All of these uncertainties are part of the puzzle.

Within what often appears to be chaos, those who prevail are those who keep their heads and are clear about their interests. That is why Ontario Governance contains an intentional Section devoted to “keeping its head” no matter what may confront it between now and realization of the Ontario Vision.

None of this suggests a unilateral, inelastic course of action. Rather, it suggests maintaining an informed position, knowing what matters most and retaining the capacity to shift priorities where that makes sense. It entails openness to mutually beneficial collaboration, but with clear visibility of what is on the table. It is the exact opposite of intractability.

Finally, with respect to the business of forecasts and projections into the future, Ontario leans strongly on the use of scenario modeling rather than inflexible extensions of the past into the future. There are several reasons for this approach:

1. The data base underpinning the City's scenario-building capability is robust and will become more so in the future. This allows for generating implications of multiple changes, individually and collectively, that offer a useful understanding of how different choices perform.

2. Projections often become less accurate over time and obscure the factors that leverage change. Understanding those factors and their influence is more valuable than whether or not a particular anticipated growth number has or has not been achieved.
3. Ontario's future growth opportunities are tied to its geographic context. As such, GIS-based scenario building approaches are eminently suitable.
4. The Ontario Plan is a continuously improving plan and requires dynamic tools for its optimization.

G5. MUNICIPAL FINANCE

No single action of a city government more meaningfully states its values and priorities than the budget by which it allocates City resources and reflects the Vision and City Council's goals and priorities. The system of municipal finance includes:

Annual Budgeting Process. The entire municipal finance system culminates in the preparation, adoption, and implementation of the annual budget. The City intends its budget document to establish policy, guide operations, plan for future financial circumstances, and communicate to the City's residents, businesses, and investors.

Capital Improvement Programming. To better manage large-scale investments, the City adopts an annual Capital Improvement Program that schedules engineering and design, land acquisition, and construction for large projects over the subsequent five years.

Cash Management. The process of managing cash inflows and outflows for proper collection, disbursement, and investment of cash. This involves deposits, reconciliation, and adherence to the City's Cash Handling Policy.

Public Trust. The municipal finance system rests on a foundation of public confidence in the City's ability to fairly and effectively manage municipal finances over the long-term. The City preserves the public trust with clear and concise communications and transparent and accountable financial decision-making.

Fund Balance and Reserves. The City seeks to maintain certain levels of funds in reserve for unforeseen cash flow disruptions and for specific purposes, such as post-retirement health care expenses. Reserves differ from "rainy day" funds, which are more like savings for periods of overall economic stagnation or retrenchment.

Revenue and Expenditure Forecasting. Key to effective budgeting, the City conservatively forecasts future revenues and expenditure needs and constantly tracks those assumptions.

Risk Management. The risk management program includes special funds for reserves to cover potential losses, risk control programs and activities, and self-insurance pools.

Goal

Goal G5. An easily understood, transparent, and accountable municipal finance system that implements The Ontario Plan, safeguards the City's assets, and maintains the public's trust and confidence.

Policies

G5-1. Departmental Budgeting Requests. We require each department to submit an annual budget request free from reliance on one-time revenues (except for specific grant funded projects) and unsustainable revenue and deficit spending.

G5-2. Department Budget Limitation. We will not commit to spending more than a conservative projection of recurring revenues for the City as a whole.

G5-3. Limitations on Fund Transfers. The City Manager may transfer budgeted amounts between departments within any fund; however, we require the City Council approve any revisions that alter the total expenditures of any individual fund.

G5-4. Cost Allocation Plans. We account for costs for administration, employment benefits, risk management, and support services through special funds, including reimbursements from special projects, funds, and contracts based on an annual cost allocation plan.

G5-5. Investment Objective. We require safety of principal as the foremost investment objective; investment decisions shall seek to minimize net capital losses on a portfolio basis.

G5-6. Investment Maturities and Available Resources. We will mitigate market risk by limiting the weighted average maturity of the fund consistent with the approved investment policy; the City's fund will be structured to ensure that the City's projected expenditure requirements for the next six months can be met with a combination of anticipated revenues, maturing securities, principal and interest payments and liquid instruments.

G5-7. Fund Balance Target. We expect the annual budget to maintain a minimum unreserved general fund balance in accordance with policies established and from time to time revised by City Council resolution; we maintain this unreserved fund balance to deal with unanticipated disruptions to cash flow and do not otherwise use this reserve.

G5-8. Deferred Maintenance. We acknowledge current and cumulative deferred maintenance, if any, in the annual budget document.

G5-9. Comprehensive Debt Strategy. We pursue a strategy to avoid or minimize bonded indebtedness against the General Fund and to finance capital improvement projects on a pay-as-you-go basis and through special funds, or, if warranted by magnitude and/or lifespan, through applicable special financing districts and mechanisms.

G5-10. General Fund Purpose. We pursue a strategy to allocate general fund resources toward the City's provision of basic municipal services.

G5-11. Transparency. We expect our City's budget and annual financial report documents to maintain the highest standards for clarity and content.

G5-12. Investment Strategies. We utilize the Treasurer's Investment Policy to establish strategies that guide how the City invests short-term cash flow and invests longer-term reserves.

G5-13. Financial Services Administration. We require the Financial Services Administration review and analyze City Council staff reports to determine potential fiscal impacts.

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING THE CITY COUNCIL APPROVE AN AMENDMENT, FOR FILE NO. PGPA20-002 PART C, CONSISTING OF A TECHNICAL UPDATE TO THE ONTARIO POLICY PLAN (GENERAL PLAN).

WHEREAS, the City of Ontario Certified the Environmental Impact Report prepared for The Ontario Plan and associated Statement of Overriding Considerations, and issued Resolution No. 2010-003; and

WHEREAS, the City of Ontario adopted the Policy Plan (General Plan) as part of the component framework for The Ontario Plan on January 27, 2010, and issued Resolution No. 2010-004; and

WHEREAS, the City of Ontario has been operating under The Ontario Plan Policy Plan (General Plan) adopted by City Council on January 27, 2010, Resolution No. 2010-004, along with their respective Policy Plan (General Plan) text and maps; and

WHEREAS, Section 65300 of the California Government Code of the State of California authorizes cities to prepare and update long-range comprehensive guides known as general plans; and

WHEREAS, the City of Ontario's Technical Update to the Policy Plan complies with Section 65300 in that it meets the state mandate of a general plan; and

WHEREAS, the Policy Plan is an integrated component within The Ontario Plan, the City's comprehensive long-term municipal business plan; and

WHEREAS, during the development of the Policy Plan, the City participated in Southern California Association of Governments (SCAG) regional planning programs including Regional Housing Needs Assessment (RHNA), and Regional Transportation Plan Sustainable Community Strategy (RTP/SCS); and

WHEREAS, the Policy Plan advances regional planning policies; and

WHEREAS, Section 65361 limits the amendment of a mandatory element of the Policy Plan to not more than four (4) times per year; and

WHEREAS, the Policy Plan contains nine elements: 1) Land Use, 2) Housing, 3) Parks and Recreation, 4) Environmental Resources, 5) Community Economics, 6) Safety, 7) Mobility, 8) Community Design, and 9) Social Resources; and

WHEREAS, the Land Use, Housing, Parks and Recreation, Environmental Resources, Safety and Mobility elements are mandatory elements of the Policy Plan per Section 65302; and

WHEREAS, the Community Economics, Community Design, and Social Resources elements are optional elements of the Policy Plan per Section 65302; and

WHEREAS, the Climate Adaptation and Resiliency, and Environmental Justice elements are woven throughout the policy plan; and

WHEREAS, the City of Ontario has determined that the existing General Plan required revision to bring the document into conformance with state law and to make all the elements internally consistent; and

WHEREAS, the City of Ontario has prepared a technical update to the Policy Plan (General Plan), to replace the current Policy Plan (General Plan) adopted in 2010, with exception to the Housing Element that was approved by the City Council on March 1, 2022 and pending the California Department of Housing and Community Developments (HCD's) review and certification; and

WHEREAS, The Ontario Plan 2050 – Policy Plan Supplemental Environmental Impact Report (SEIR) was reviewed, studied, and found to comply with the California Environmental Quality Act (CEQA); and

WHEREAS, the City of Ontario made The Ontario Plan 2050 website (www.Ontarioplan.org/TOP2050) including the Policy Plan, available for public review on May 9, 2022; and

WHEREAS, the City of Ontario conducted a virtual open house on June 14, 2021 to gain input from impacted property owners; and

WHEREAS, the City of Ontario conducted a virtual meeting with Community Health Improvement Association on June 24, 2021, to gain input from members of the public; and

WHEREAS, the City of Ontario conducted a virtual Community Workshop #1 on June 28, 2021, to gain input from members of the public; and

WHEREAS, the City of Ontario conducted a virtual Community Workshop #2 on July 29, 2021, to gain input from members of the public; and

WHEREAS, the City of Ontario conducted outreach events at Concerts in the Park on August 25, 2021 and August 26, 2021, to gain input from members of the public and participate in the Environmental Justice Survey; and

WHEREAS, the City of Ontario conducted an outreach event at Taste of Ontario on March 19, 2022, to provide an update to the TOP 2050 and gain input from members of the public; and

WHEREAS, the City of Ontario conducted a series of virtual Open House/Office Hours on March 31, 2022, April 28, 2022, and May 26, 2022, to provide brief overview of TOP 2050 and provide the public with an informal format to ask questions and share insights; and

WHEREAS, the City of Ontario conducted a series of outreach meeting with the Ontario's Teen Action Committee on June 6, 2022, June 15, 2022, and June 16, 2022, providing an overview of the TOP 2050, climate adaptation and resiliency, and the importance of civic participation; and

WHEREAS, the City of Ontario conducted an in-person open house located inside City Hall on June 30, 2022, to provide an overview of the TOP 2050 Update SEIR, Policy Plan, and Community Climate Action Plan, and gain input from members of the public; and

WHEREAS, the Planning Commission is responsible for reviewing updates to the Policy Plan (General Plan) and Map and forwarding a recommendation to the City Council on the adoption of the General Plan and Map; and

WHEREAS, on July 26, 2022, the Planning Commission of the City of Ontario conducted a duly noticed public hearing on the General Plan Update at which time all persons wishing to testify in connection with the General Plan Update were heard; and

WHEREAS, all other legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, BE IT RESOLVED BY THE PLANNING COMMISSION OF THE CITY OF ONTARIO:

SECTION 1: Environmental Determination and Findings. Based on the entire record before the Planning Commission and all written and oral evidence presented to the Planning Commission, the Planning Commission recommends that the City Council finds that the environmental effects of the General Plan Update (The Ontario Plan – Policy Plan) have been analyzed and discussed in the TOP 2050 Update SEIR in compliance with CEQA. The Planning Commission further recommends that the City Council find that the SEIR was prepared in compliance with CEQA, that the City complied with CEQA's procedural and substantive requirements, and that the SEIR is an accurate and objective statement that fully reflects the independent judgement of the City.

SECTION 2: Planning Commission Action. Based on the entire record before the Planning Commission, all written and oral evidence presented, and the findings made in this Resolution, the Planning Commission recommends that the City Council amend the Policy Plan (General Plan) as set forth in Attachment “A” (The Ontario Plan 2050– Policy Plan).

SECTION 3: Recommendation. Based on the entire record before the Planning Commission and all written and oral evidence presented, the Planning Commission recommends the City Council finds the General Plan Update (The Ontario Plan – Policy Plan) promotes the goals and objectives of the City of Ontario and leaves the General Plan Update a long-term, compatible, integrated, and internally consistent statement of policies for the following reasons:

A. **Land Use.** The General Plan Update (The Ontario Plan – Policy Plan) shows how the City of Ontario will develop, use and arrange land which is critical to achieving the Ontario Vision. Land is a finite and valuable resource. How it is used is a key factor in the City's economic future. As stewards of the land, the City must plan for uses and development that adds value to the community, in terms of function, design and fiscal return. Ontario is steadfast in its Vision, but the Policy Plan is flexible enough to accommodate the evolution of development types. Ontario desires to have distinct neighborhoods and activity centers; diversity of residential, employment, retail, entertainment, and community and recreational services; and a world-class airport which are connected through a unified mobility system. The Land Use Element reflects Ontario's Vision to be a complete community.

B. **Housing.** The General Plan Update (The Ontario Plan – Policy Plan) is integrated and compatible with the Housing Element in that it provides for development consistent with the existing and projected housing needs of the community. The General Plan Update (The Ontario Plan – Policy Plan) will maintain a supply of developable residential land adequate to accommodate the amount and type of projected household and job growth. The General Plan Update (The Ontario Plan – Policy Plan) will also ensure that residential sites are served by adequate infrastructure and services and with a range of housing types to accommodate a variety of incomes and lifestyles including the City's fair share of regional housing need. The General Plan Update (The Ontario Plan – Policy Plan) will also provide housing opportunities for groups with special needs and for all people regardless of race, religion, gender, marital status, ancestry or national origin.

C. **Parks & Recreation.** The General Plan Update (The Ontario Plan – Policy Plan) integrates parks and recreational spaces which are critical physical, social and psychological element of a complete community. Beyond merely providing recreational opportunities, carefully sited and well-designed parks serve a multitude of functions for the community: creating neighborhood identity, serving as focal points for gathering and celebration, and increasing and protecting environmental resources. Moreover, parks act

as the city's "breathing spaces", where people experience relief as they go about their daily routines.

D. Environmental Resources. Ontario's environmental resources are an integral part of our maturing City and are valued for the benefits they provide. Appropriately managed, they reduce health risks for residents and workers, lower energy and water costs, safeguard our water assets, and protect our environment. The systems that convey and treat water and waste, generate and distribute energy, promote beneficial air quality, and protect natural resources are essential to Ontario's health and prosperity. The General Plan Update (The Ontario Plan – Policy Plan) will manage resources comprehensively, utilizing natural and man-made systems that are both environmentally and economically sustainable.

E. Community Economics. The City's role is to create, maintain and grow economic value. Community shareholders, including property owners, tenants, families and businesses have invested in Ontario and should expect a return from those investments, including financial and qualitative returns, such as: quality of life, job opportunities, potential for business growth, sense of safety, healthy environment, responsive government, amenities and so much more - all of which is part of the Vision of being a "complete community". Their investments, in turn, generate returns for the community. A business investing in expansion creates new job opportunities for residents. A developer investing in new housing attracts new consumers for Ontario's retailers. These investments add municipal revenues, which the City reinvests in community facilities, public improvements, and services that increase shareholder returns. Through the Community Economics Element policies, The General Plan Update (The Ontario Plan – Policy Plan) commits itself to protecting these investments. Generating better and higher returns gives Ontario a competitive advantage to attract even more investment. This new investment and reinvestment creates a self-sustaining cycle of prosperity. The General Plan Update (The Ontario Plan – Policy Plan) is integrated and compatible with the Community Economics Element.

F. Safety. The General Plan Update (The Ontario Plan – Policy Plan) is integrated and compatible with the Safety Element in that it provides an appropriate land use distribution and orientation which protects the community from unreasonable risks associated with seismic, geologic, flood, wind, and wildfire hazards.

G. Mobility. Ontario is strategically located within a regional transportation network that includes an international airport with passenger and air cargo operations, three freeways, three freight rail lines, commuter and passenger rail services, public transit and a local network of streets and multi-purpose trails. This network provides multi-modal transportation options for those traveling within, to or through the City. This robust system creates unique opportunities for Ontario as a regional jobs hub and a complete community. Ontario's Vision to concentrate growth in key locations will allow the City to capitalize on this transportation system. Ontario's Vision is that there will be more mobility

options as the City and the region grow. Personal vehicles will continue to provide individual mobility and flexibility for travel, though fuel systems will change, and guidance technology will become much more sophisticated. Bus travel will be a convenient and reliable option. The commuter rail system will provide service all day throughout the region and high-speed rail may be developed for longer-distance trips. Air travel will be linked through a future multi-modal station in close proximity to ONT airport where access to many modes of transit will be available. Freight transportation via trucks, rail and air cargo will provide efficient movement of goods to the City, region and beyond while minimizing negative impacts. The mobility element will be coordinated with future land use patterns and levels of buildout. Access and connectivity to mobility options will be integrated into neighborhoods, centers, corridors and districts. The placement of housing, jobs and amenities in closer proximity to each other and design strategies focused on the pedestrian will make walking a desirable alternative and a connected regional system of multi-purpose trails will enable safe and convenient non-motorized travel. The General Plan Update (The Ontario Plan – Policy Plan) is integrated and compatible with the Mobility Element.

H. Community Design. All members of the community—residents, business owners, workers, visitors and investors— need places and spaces that are both functional and appealing. Quality design of the built environment can: protect existing investments; achieve sustainable environments; create safe and pleasant places where people want to live, work and recreate; and add value to the community. The General Plan Update (The Ontario Plan – Policy Plan) is integrated and compatible with the Community Design Element.

I. Social Resources. Access to healthcare, nutrition, and healthy environments, quality education, diverse community services and facilities and cultural opportunities are essential to a prosperous Ontario. All are critical to individual and community well-being and Ontario’s ability to attract residents and investors. However, direct control over delivery of healthcare, education, social services and cultural opportunities is in the hands of outside organizations. Therefore, the City must leverage the efforts of service providers within the region. The General Plan Update (The Ontario Plan – Policy Plan) is integrated and compatible with the Social Resources Element.

SECTION 4: Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East “B” Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 5: Certification to Adoption. The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26th day of July 2022, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Jim Willoughby
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

Planning Commission Resolution
File No. PGPA20-002
July 26, 2022
Page 8

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. ____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on July 26, 2022, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

***Policy Plan
A Component of The Ontario Plan 2050***



City of Ontario Policy Plan

A Component of The Ontario Plan 2050

Public Review Draft | May 2022

Please note that this is a hard copy version of what is intended to be published in a web-based format.
There are notations and placeholders for links throughout that are not active in this draft.



Prepared By: PlaceWorks

3 MacArthur Place, Suite 1100

Santa Ana, CA 92707

714.966.9220

TABLE OF CONTENTS

INTRODUCTION..... 1

 Purpose..... 1

LAND USE ELEMENT 2

 Introduction 2

 Purpose..... 2

 Principles..... 2

 Goals & Policies..... 3

 LU-1 Balance..... 3

 LU-2 Compatibility 4

 LU-3 Flexibility..... 5

 LU-4 Phased Growth..... 6

 LU-5 Airport Planning 6

 Exhibits: Tables and Figures..... 8

HOUSING ELEMENT 25

 Introduction 25

 Purpose..... 25

 Principles..... 25

 Goals & Policies..... 25

 H-1 Neighborhoods & Housing..... 25

 H-2 Housing Supply & Diversity..... 26

 H-3 Governmental Regulations 27

 H-4 Housing Assistance..... 28

 H-5 Special Needs..... 29

 Exhibits: Documents Incorporated by Reference 31

PARKS AND RECREATION ELEMENT..... 32

 Introduction 32

 Purpose..... 32

 Principles..... 32

 Goals & Policies..... 32

 PR-1 Planning & Design..... 32

 PR-2 Programming 34

 Exhibits: Figures..... 35

ENVIRONMENTAL RESOURCES ELEMENT 38

 Introduction 38

 Purpose..... 38

 Principles..... 38

 Goals & Policies..... 39

 ER-1 Water & Wastewater 39

 ER-2 Solid Waste & Recycling..... 40

 ER-3 Energy 41

 ER-4 Air Quality..... 42

 ER-5 Biological, Mineral & Agricultural Resources 43

 Exhibits: Figures..... 45

City of Ontario Policy Plan

Table of Contents

COMMUNITY ECONOMICS ELEMENT	47
Introduction	47
Purpose	47
Principles	47
Goals & Policies	48
CE-1 Complete Community	48
CE-2 Placemaking	49
CE-3 Fiscal Decision-making	50
SAFETY ELEMENT.....	52
Introduction	52
Purpose	52
Principles	52
Goals & Policies	53
S-1 Seismic & Geologic Hazards	53
S-2 Flood Hazards.....	53
S-3 Fire & Rescue Hazards.....	54
S-4 Noise Hazards.....	55
S-5 Wind-Related Hazards	56
S-6 Hazardous Materials & Waste.....	57
S-7 Law Enforcement.....	58
S-8 Emergency Management	58
S-9 Energy Resiliency	60
Exhibits: Figures	61
Exhibits: Documents Incorporated by Reference	74
MOBILITY ELEMENT	75
Introduction	75
Purpose	75
Principles	76
Goals & Policies	76
M-1 Roadway System.....	76
M-2 Active Transportation	77
M-3 Public Transit.....	78
M-4 Goods Movement.....	80
M-5 Regional Transportation	81
Exhibits: Figures	82
COMMUNITY DESIGN ELEMENT	87
Introduction	87
Purpose	87
Principles	87
Goals & Policies	88
CD-1 Image & Identity	88
CD-2 Design Quality	89
CD-3 Urban, Mixed Use, and Transit-Oriented Place Types	92
CD-4 Historic Preservation.....	93
CD-5 Protection of Investment.....	94
Exhibits: Tables and Figures.....	95

City of Ontario Policy Plan
Table of Contents

SOCIAL RESOURCES ELEMENT..... 117

- Purpose..... 117
- Principles..... 117
- Goals & Policies..... 117
 - SR-1 Health 117
 - SR-2 Education 118
 - SR-3 Community & Leisure Activities 119
 - SR-4 Library 120
 - SR-5 Entertainment & Culture 120

TABLES

- LU-02 Land Use Designations Summary Table 10
- LU-03 Future Buildout Table 18
- LU-05 Noise Level Exposure and Land Use Compatibility Guidelines 24
- CD-02 Place Type Categories 97

FIGURES

- LU-01 Official Land Use Plan..... 9
- LU-04 Mixed Use Areas 21
- LU-05 Overlay Areas 22
- LU-04 Airport Safety Zones & Influence Areas..... 23
- PR-01 Park & Recreational Facilities 36
- PR-02 Equestrian Trails 37
- ER-01 Environmental Justice Areas 46
- S-01 Regional Faults & Fault Zones 62
- S-03 Flood Hazard Zones 64
- S-04 Dam Inundation Zones 65
- S-05 Fire Hazard Severity Zones 66
- S-06a Future Traffic Noise Contours 67
- S-06b Future Rail Noise Contours..... 68
- S-06c Airport Noise Contours 69
- S-07 Hazardous Materials Cleanup Sites 70
- S-08 Critical Facilities..... 71
- S-09 Evacuation Constrained Areas..... 72
- S-10 Evacuation Routes 73
- M-01 Roadway Classifications 83
- M-02 Multipurpose Trails & Bikeways..... 84
- M-03 Public Transit..... 85
- M-04 Truck Routes..... 86
- CD-01 Place Types 96
- CD-03 Downtown District Place Type 101
- CD-04 Airport Metro / Entertainment District Place Type 103
- CD-05 Guasti District Place Type..... 105

City of Ontario Policy Plan

Table of Contents

CD-06 South Euclid District Place Type 107
CD-07 South Employment District Place Type..... 109
CD-08 Corridor Place Types..... 111
CD-09 Neighborhood Center Place Types..... 114

DOCUMENTS INCORPORATED BY REFERENCE

Housing Element Technical Report..... 31
Local Hazard Mitigation Plan 74
Vulnerability Assessment Report 74

APPENDICES

APPENDIX A. IMPLEMENTATION ACTIONS RELATED TO ENVIRONMENTAL JUSTICE AND CLIMATE ADAPTATION AND RESILIENCY

APPENDIX B. GLOSSARY OF TERMS AND ABBREVIATIONS

INTRODUCTION

The Policy Plan states long-term goals, principles, and policies for achieving Ontario’s Vision. It guides growth and development to achieve optimum results from the City’s physical, economic, environmental, and human resources. The Policy Plan serves as the City’s General Plan, which is mandated by state law. General Plans are required to include eight topics (Land Use, Housing, Circulation, Noise, Safety, Open Space and Conservation, and Environmental Justice). The organization of these eight topics, and any optional topics, are determined by the local jurisdiction. The General Plan is required to be comprehensive in nature and internally consistent.

Ontario’s General Plan is made up of nine elements: Land Use, Housing, Mobility, Safety (including Noise), Environmental Resources (including Conservation), Parks and Recreation (including Open Space), Community Economics, Community Design, and Social Resources. Environmental Justice is woven throughout the Policy Plan.

While State law requires Housing and Safety Elements to be updated every eight years, the State Office of Planning and Research recommends that local jurisdictions comprehensively update their General Plan every 10-15 years. Ontario’s last comprehensive General Plan update was adopted in 2010. The Housing Element was updated in 2022.

In addition to acting as to the City’s General Plan, the Policy Plan incorporates the goals and policies necessary for the City to achieve its full Vision beyond what state law mandates.

The Policy Plan is intended to be a long-term policy document. It includes abiding principles for each element, goals (what we want to achieve) and policies (how we accomplish these goals). The short-term actions needed to implement the Policy Plan are housed in a separate Implementation Plan. The portion of the Implementation Plan related to Housing, Environmental Justice, and Safety Element regulations are part of the Policy Plan as required by state law; all other portions of the Implementation Plan are not and do not require an amendment to the Policy Plan to be updated.

Purpose

The Policy Plan:

- Takes a fresh look at the changing circumstances within the City and the region since the previous Policy Plan was adopted.
- Establishes a strong, yet flexible policy base that moves the City towards its Vision.
- Incorporates the current ([link](#)) City Council Priorities (goals and objectives) into this policy document which will help guide future Council efforts when the plan is updated or revised.
- Complies with State requirements for local general plans.
- Imbeds the City’s governance ([link](#)) approach into how it operates and makes decisions.

LAND USE ELEMENT

Introduction

How we develop, use and arrange our land is critical to achieving the Ontario Vision. Land is a finite and valuable resource. How we use it becomes a key factor in the City's economic future. As stewards of the land, the City must plan for uses and development that adds value to the community in terms of function, design, and fiscal return. We are steadfast in our Vision, but the Policy Plan is flexible enough to accommodate the evolution of development types.

We desire Ontario to have distinct neighborhoods and activity centers, corridors, and districts; diversity of residential, employment, retail, entertainment, community, and recreational services; and a world-class airport which are connected through a unified mobility system. The Land Use Element reflects Ontario's Vision to be a complete community.

Purpose

The Land Use Element:

- Designates the distribution, location, and balance of land uses.
- Describes the desired build-out of Ontario
- Describes building intensity standards for each land use.
- Communicates population density.
- Ensures compatibility between land uses.

Principles

The City believes:

- Development that supports the Vision will strengthen the City's economy.
- New growth will enhance the quality of life in the entire community, including our existing neighborhoods.
- Land uses and development should be consistent with the Vision while protecting the quality of life in existing neighborhoods.
- Our development review process should incorporate flexible standards to achieve our Vision.
- The way land is used and developed will determine our revenue base and expenditures.

Goals & Policies

LU-1 Balance

Ontario is poised to experience extraordinary growth. Achieving this growth in a manner that is consistent with our Vision requires a delicate balancing act. For instance, the City must balance the benefits and impacts of being a regional urban and transportation hub with the livability of our neighborhoods.

As an active participant in regional planning, Ontario contributes to the regional jobs-housing balance by providing significant office, industrial and retail employment opportunities while focusing this growth strategically to minimize development impacts on established neighborhoods.

Goal LU-1 A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.

Policies

- LU-1.1 Strategic Growth.** We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, foster the development of transit, and support the expansion of the active and multimodal transportation networks throughout the City. (Link to Mobility Element Policies M2-M-2.1, M3-M-3.3; Community Design Element *Image and Identity* Section; Community Design Policies CD-3.1, CD-3.3, CD-3.6)
- LU-1.2 Sustainable Community Strategy.** We integrate state, regional, and local Sustainable Community/Smart Growth principles into the development and entitlement process.
- LU-1.3 Adequate Capacity.** We require adequate infrastructure and services for all development.
- LU-1.4 Multimodal Mobility.** We require development and urban design, where appropriate, that reduces reliance on the automobile and capitalizes on active transportation, transit, electric vehicles, and multimodal transportation opportunities (Link to Mobility Element Policies M-2.1, M-3.3; Community Design Element Policy CD-2.6, CD-3.1, CD-3.3, CD3.6).
- LU-1.5 Jobs-Housing Balance.** We coordinate land use, infrastructure, and transportation planning and analysis with regional, county, and other local agencies to further regional and subregional goals for jobs-housing balance. (Link to Community Economics Element Policy CE-1.1; Mobility Element Policy M-1.6)

- LU-1.6 Complete Community.** We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers, and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Link to Community Economics Element *Complete Community* Section; Community Design Element *Urban, Mixed Use, and Transit-oriented Place Types* Section)
- LU-1.7 Revenues and Costs.** We require future amendments to our Land Use Plan to be accompanied by analyses of fiscal impacts. (Link to Community Economics Policy CE-3.2)

LU-2 Compatibility

The Land Use Plan provides for a wide range of land uses, including residential, industrial, office, and commercial. Some land use designations and the uses permitted within them are compatible, while others have the potential to create adverse impacts. The City utilizes land use regulations, site planning, and design controls to achieve compatibility.

Goal LU-2 Compatibility between a wide range of uses and resultant urban patterns and forms.

Policies

- LU-2.1 Land Use Decisions.** We minimize adverse impacts on adjacent properties when considering land use and zoning requests.
- LU-2.2 Buffers.** We require new uses to provide mitigation or buffers between existing uses where potential adverse impacts could occur. Additional mitigation is required when new uses could negatively impact environmental justice areas. (Link to Community Design Element)
- LU-2.3 Hazardous Uses.** We regulate the development of industrial and similar uses that use, store, produce, or transport toxic substances, air emissions, other pollutants, or hazardous materials. (Link to Safety Element *Hazardous Materials & Waste* Section, including Policies S-6.4 and S-6.5)
- LU-2.4 Regulation of Nuisances.** We regulate the location, concentration, and operation of potential nuisances.
- LU-2.5 Regulation of Uses.** We regulate the location, concentration, and operation of uses that have impacts on surrounding land uses.
- LU-2.6 Infrastructure Compatibility.** We require infrastructure to be aesthetically pleasing and in context with the community character.
- LU-2.7 Inter-jurisdictional Coordination.** We maintain an ongoing liaison with ONT, Caltrans, Public Utilities Commission, the railroads, and other agencies to help minimize impacts and improve the operations and aesthetics of their facilities.

- LU-2.8** **Transitional Areas.** We require development in transitional areas to protect the quality of life of current residents.
- LU-2.9** **Methane Gas Sites.** We require sensitive land uses and new uses on former dairy farms or other methane-producing sites be designed to minimize health risks.
- LU-2.10** **Sensitive Uses.** We monitor and share information with the community about stationary and non-stationary emission sources. We encourage siting and design of facilities to minimize health and safety risks on existing and proposed sensitive uses, especially in environmental justice areas.
- LU-2.11** **Context-Aware Transitions and Connections.** We require new development projects and land-planning efforts to provide context-aware and appropriate transitions and connections between existing and planned neighborhoods, blocks, sites, and buildings. (Link to Community Design Element Policies CD-1.1, CD-1.3, CD-3.4)

LU-3 Flexibility

Our Policy Plan and implementing regulations are designed to focus growth in key areas; however, they cannot address every situation. In order to take advantage of opportunities or remove impediments to achieving our Vision of a complete community, we need the ability to quickly respond to changing market conditions and innovative development proposals. This is accomplished through our planning incentive program and flexible application of development standards.

Goal LU-3 **Staff, regulations and processes that support and allow flexible response to conditions and circumstances in order to achieve the Vision.**

Policies

- LU-3.1** **Development Standards.** We maintain clear development standards which allow flexibility to achieve our Vision and provide objective standards that ensure predictability and deliver the intended physical outcomes. (Link to Community Design Element *Design Quality* and *Urban, Mixed Use, and Transit-oriented Place Types* Sections)
- LU-3.2** **Design Incentives.** We offer design incentives to help projects achieve the Vision. (Link to Community Design Element)
- LU-3.3** **Land Use Flexibility.** We consider uses not typically permitted within a land use category if doing so improves livability, reduces vehicular trips, creates community gathering places and activity nodes, and helps create identity.

LU-4 Phased Growth

As we progress, we will face development decisions that require us to balance short and long-term costs and benefits. In some cases, conditions necessary for development of the type and scale that fully realizes our Vision may be years in the future. But opportunities that can add immediate value may present themselves. We will allow interim development in our growth areas, provided it can be modified or replaced when circumstances for development more reflective of our Vision are right. We will not allow development that impedes, precludes, or compromises our ability to achieve our Vision.

Goal LU-4 Development that provides short-term value only when the opportunity to achieve our Vision can be preserved.

Policies

LU-4.1 **Commitment to Vision.** We are committed to achieving our Vision but realize that it may take time and several interim steps to get there.

LU-4.2 **Interim Development.** We allow development in urban, mixed use, and transit-oriented Place Types that is not immediately reflective of our ultimate Vision for the Place Type, provided it can be modified or replaced when circumstances are right to support development aligned with the Place Type Vision. We will not allow development that impedes, precludes, or compromises our ability to achieve our Vision. (Link to Community Economics Element Policy CE-2.3; Community Design Element Policy CD-1.2; Community Design Element *Urban, Mixed Use, and Transit-oriented Place Types* section).

LU-4.3 **Infrastructure Timing.** We require that the necessary infrastructure and services be in place prior to or concurrently with development.

LU-4.4 **Shared Infrastructure.** We encourage and facilitate the use of shared infrastructure (including shared or managed parking) in urban, mixed use, and transit-oriented Place Types. (Link to Community Design Element Policy CD-3.6)

LU-5 Airport Planning

Airports play a significant role in regional and local economies providing critical services such as business travel, tourism, and emergency response. The City benefits from having two airports in close proximity, Ontario International Airport (ONT) and Chino Airport, and is responsible for implementing measures to ensure their orderly expansion while protecting the public from excessive noise and safety hazards. ONT and Chino Airport are part of a national aviation transportation system that is overseen by federal, state and local governments.

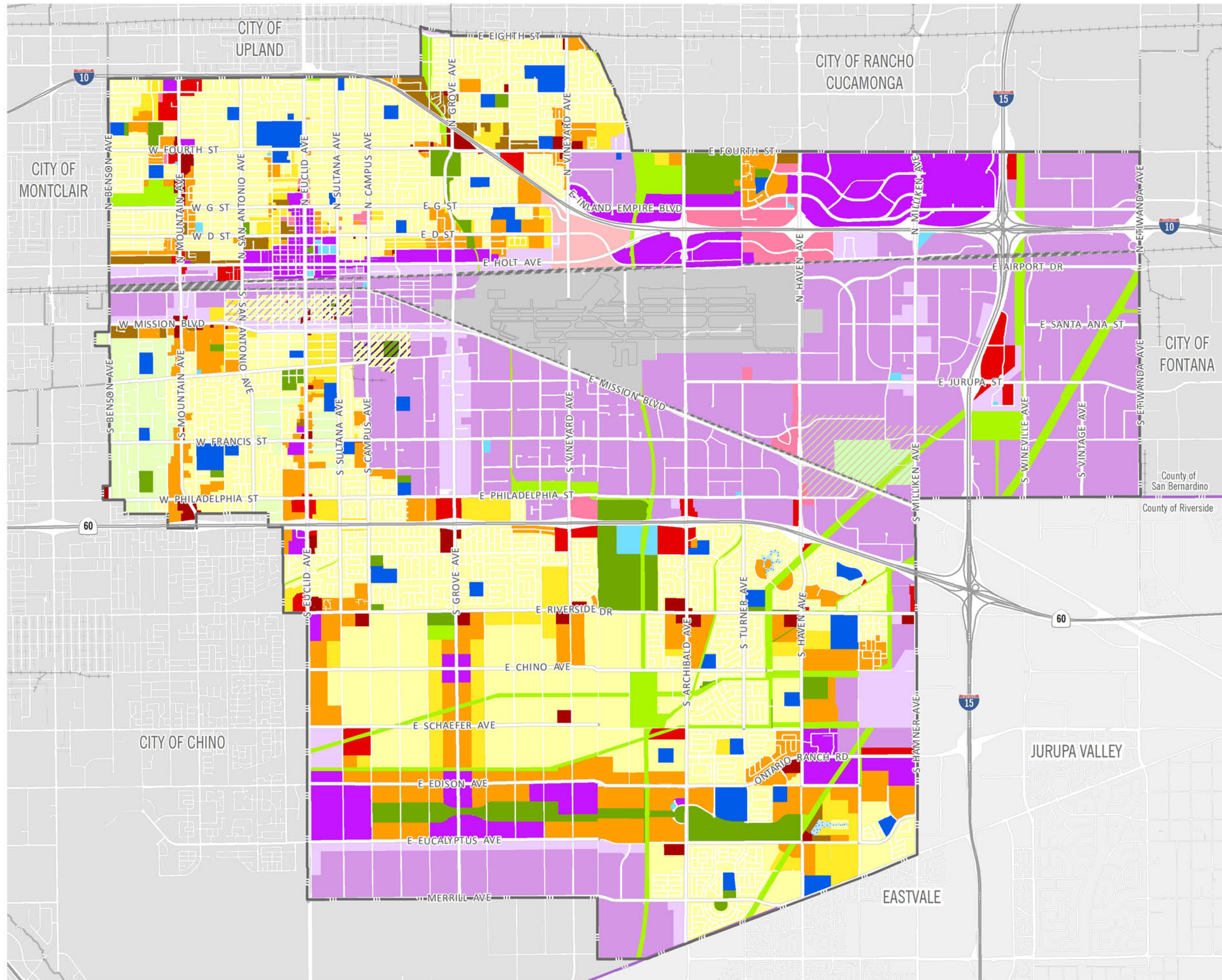
ONT is a medium-hub commercial airport. Centrally located within the City, ONT is the single most prominent land use in Ontario. Chino Airport, a general aviation airport, is located outside the City along the southern boundary. This section addresses the impacts of the airports.

Goal LU5 **Integrated airport systems and facilities that minimize negative impacts to the community and maximize economic benefits.**

Policies

- LU-5.1** **Coordination with Airport Authorities.** We collaborate with FAA, Caltrans Division of Aeronautics, airport owners, neighboring jurisdictions, and other shareholders in the preparation, update, and maintenance of airport-related plans.
- LU-5.2** **Airport Planning Consistency.** We coordinate with airport authorities to ensure The Ontario Plan is consistent with state law, federal regulations, and/or adopted master plans, and airport land use compatibility plans for ONT and Chino Airport.
- LU-5.3** **Airport Impacts.** We work with agencies to maximize resources to mitigate the impacts and hazards related to airport operations – their homes.
- LU-5.4** **ONT Growth Forecast.** We support and promote an ONT that accommodates 30 million annual passengers and 1.6 million tons of cargo per year, as long as the impacts associated with that level of operations are planned for and mitigated.
- LU-5.5** **Airport Compatibility Planning for ONT.** We create and maintain the Airport Land Use Compatibility Plan for ONT.
- LU-5.6** **Alternative Process.** We fulfill our responsibilities and comply with state law with regard to the Alternative Process for proper airport land use compatibility planning.
- LU-5.7** **ALUCP Consistency with Land Use Regulations.** We comply with state law that requires general plans, specific plans, and all new development to be consistent with the policies and criteria set forth within an Airport Land Use Compatibility Plan for any public-use airport.
- LU-5.8** **Chino Airport.** We will support the creation and implementation of the Airport Land Use Compatibility Plan for Chino Airport.

Exhibits: Tables and Figures



LAND USE
 Figure LU-01
 Official Land Use Plan

- Ontario City Boundary
- County Boundary
- Rail Network
- Overlay Zones**
- Business Park
- Industrial
- Landfill Impact Area
- Proposed Land Use**
- Residential**
- RR Rural Residential
- LDR Low Density Residential
- LMDR Low Medium Density Residential
- MDR Medium Density Residential
- HDR High Density Residential
- Mixed-Use**
- MU Mixed Use
- Commercial**
- NC Neighborhood Commercial
- GC General Commercial
- OC Office Commercial
- HOS Hospitality
- Employment**
- BP Business Park
- IND Industrial
- Other**
- OS-NR Open Space - Non-Rec
- OS-R Open Space - Recreation
- OS-W Open Space - Water
- PF Public Facility
- PS Public School
- ARPT Airport
- LF Landfill
- Rail

2 · 0 · 5 · 0
THE ONTARIO PLAN

0 2,500 5,000 10,000 FT
 Source: The City of Ontario 2022 Date: 5/9/22

LU-02 Land Use Designations Summary Table

Land Use Designation	Residential Density & Non-Residential Intensity	Intention
<p>Residential – A wide range of housing densities and products to meet the demand of current and future residents with varying lifestyles. In addition to the residential uses described below, other uses such as schools, parks, childcare facilities, utilities, live-work units, and other public/institutional uses that are determined to be compatible with, oriented towards the needs of residential neighborhoods they serve, and those that help enhance community may also be allowed. When calculating the number of units permitted, the existing parcel size, before required dedication, shall be used. For developments that 1) encompass multiple properties and/or districts in established specific plans, 2) contain more than one land use designation, and 3) are located South of Riverside Drive, the maximum number of units permitted for the development may be spread over the entire site thereby allowing the blending of residential densities.</p>		
Rural	0 to 2.0 dwelling units per acre	Single-family detached residences, typically in an estate setting.
Low Density ¹	2.1 to 5.0 dwelling units per acre	Single-family detached residences.
Low-Medium Density ¹	5.1 to 11.0 dwelling units per acre	Single/multi-family attached and detached residences, including small lot subdivisions, townhouses, and courtyard homes.
Medium Density ¹	11.1 to 25.0 ^{2,3} dwelling units per acre	Single/multi-family attached and detached residences including townhouses, stacked flats, courtyard homes, and small lot single-family subdivisions.
High Density ¹	25.1 to 45.0 dwelling units per acre	Multi-family dwellings including stacked flats and mid-rise and high-rise residential complexes.
<p>Retail/Service – A full spectrum of retail, service, professional, office, medical, tourist-related, and entertainment uses at a range of intensities to respond to market demand and the character of the surrounding environment. In addition to the retail/service uses described below, other uses such as parks, childcare facilities, live-work units, utilities, and other public/institutional uses that are determined to be compatible with, oriented towards the needs of the surrounding neighborhood, and those that help enhance community may also be allowed.</p>		
Neighborhood Commercial ¹	0.40 FAR	Local serving retail, personal service, office, and dining uses, typically located within a predominantly residential neighborhood.
General Commercial ¹	0.40 FAR	Local and regional serving retail, personal service, entertainment, dining, office, tourist-serving, and related commercial uses.
Office/Commercial ¹	0.75 FAR	An intense mixture of professional office, supported by regional serving retail, service, tourist-serving, entertainment, dining, and supporting service uses that capitalize on strategic locations in Ontario. This designation also allows for professional offices such as financial, legal, insurance, medical, and other similar uses in a neighborhood setting and/or as adaptive reuse.

LU-02 Land Use Designations Summary Table

Land Use Designation	Residential Density & Non-Residential Intensity	Intention
Hospitality ¹	1.00 FAR	Regional serving tourist-serving, retail, entertainment, and service uses such as convention centers, hotels/motels, and restaurants.
<p>Employment – An array of employment uses, such as manufacturing, distribution, research and development, and office, at a range of intensities to meet the demand of current and future market conditions. In addition to the employment uses described below, other uses such as parks, live-work units, utilities, and other public/institutional uses that are determined to be compatible with and oriented towards the surrounding community may also be allowed.</p>		
Business Park ¹	0.60 FAR	Employee-intensive office uses including corporate offices, technology centers, research and development, “clean” industry, light manufacturing, and supporting retail within a business park setting.
Industrial ¹	0.55 FAR	Variety of light industrial uses, including warehousing / distribution, assembly, light manufacturing, research and development, storage, repair facilities, and supporting retail and professional office uses. This designation also accommodates activities that could potentially generate impacts, such as noise, dust, and other nuisances. If office uses and/or multiple tenant uses are developed on parcels fronting on the Milliken, Haven, and Archibald corridors, a FAR of 0.60 may be used.
Other		
Open Space - Non-Recreation ¹	Not applicable	Open space that includes utility easements, and drainage channels. We desire to realize multiple uses from these open spaces, such as trails, greenways, joint-use recreational amenities, landscaped parkways/medians, parking lots, and nurseries.
Open Space - Parkland ¹	Not applicable	Recreational facilities, such as tot-lots, parks, golf courses, and sports complexes and joint-use facilities with schools, utilities, and drainage facilities.
Public Facility ¹	Not applicable	Public facilities including civic centers, governmental institutions, police and fire stations, transportation facilities, museums, and public libraries.
Public School ¹	Not applicable	Public schools (K-12) and universities.
Airport	Not applicable	Airport, including terminals, parking, service commercial, distribution, hangers, repair, and warehousing.
Landfill	Not applicable	Allows for the use, operation, and reclamation of the Milliken Landfill. If the site is reclaimed, the City will

LU-02 Land Use Designations Summary Table

Land Use Designation	Residential Density & Non-Residential Intensity	Intention
		consider a host of uses including a transit station and multimodal transfer station.
<p>Mixed Use – An intense mixture of uses that, when concentrated, create focal points for community activity and identity and facilitate walking, biking, and the use of transit. The Mixed Use land use category accommodates a horizontal and/or vertical mixture of retail, service, office, restaurant, entertainment, cultural, and residential uses.</p> <ul style="list-style-type: none"> Density, intensity and intended character varies by area, as generally described below, and defined in detail under the applicable Place Type in the Community Design Element. The densities and intensities of the Mixed Use designation represent the intended level of anticipated development; however, individual projects may vary depending upon an approved master plan, such as a specific plan, or planned unit development. The maximum amount of development in each Mixed Use area shall be limited by the Future Buildout Projections detailed in Table LU-03. Further direction regarding land use distributions, densities and intensities within some of these areas are provided by specific plans as noted below. Most Mixed Use areas are the focal point of the City's Place Type designations illustrated on Figure CD-01 and noted below. Place Types are defined in the Community Design Element, and the vision for the intended outcome and character of each area is established and illustrated in the Place Type Figures noted below. 		
<p>Mixed Use - Downtown¹ (MU-Downtown)</p>	<ul style="list-style-type: none"> 25.0 to 75.0 dwelling units per acre 2.0 FAR for retail and office uses <p><i>Subject to PUD at City's discretion</i></p>	<p>Envisioned as an intensive vertical and horizontal mixture of retail, office, and residential uses in a pedestrian friendly atmosphere. The historic character is enhanced. The most intensive uses are envisioned along Euclid and Holt Avenues. [Link to Community Design Element <i>Urban, Mixed Use, and Transit-oriented Place Types</i> Section, Community Design Element <i>Exhibits CD-01, CD-02, CD-03 and CD-08</i>]</p>
<p>Mixed Use - Holt Blvd¹ (MU-Holt)</p>	<ul style="list-style-type: none"> 14.0² to 40.0 dwelling units per acre 2.0 FAR for office uses 1.0 FAR for retail uses <p><i>Subject to PUD at City's discretion</i></p>	<p>This area is envisioned as a low-rise (3-5 stories) intensification of the Holt Corridor. The intent is to create identity and place along the Holt Corridor, connect the corridor to Downtown, and connect the Downtown to the Ontario Airport Metro Center. [Link to Community Design Element <i>Urban, Mixed Use, and Transit-oriented Place Types</i> Section, Community Design Element <i>Exhibits CD-01, CD-02, CD-08 and CD-09</i>]</p>
<p>Mixed Use - Meredith¹ (MU-Meredith)</p>	<ul style="list-style-type: none"> 14.0 to 125.0 dwelling units per acre 3.0 FAR for office and retail uses 	<p>This area is envisioned as a mixture of mid-rise buildings, regional-serving retail and office centers, and stand-alone high density residential projects. [Link to Community Design Element <i>Urban, Mixed Use, and Transit-oriented Place Types</i> Section, Community Design Element <i>Exhibits CD-01, CD-02, and CD-08</i>]</p>

LU-02 Land Use Designations Summary Table

Land Use Designation	Residential Density & Non-Residential Intensity	Intention
<p>Mixed Use - Multimodal¹ (MU-Multimodal)</p>	<ul style="list-style-type: none"> • 20.0 to 80.0 dwelling units per acre • 1.0 FAR for office and retail uses 	<p>The Multimodal Mixed Use Area is under consideration for our future multimodal transit station that links rail, regional, local, and Airport transit. Intensive office, retail, and residential uses are envisioned to be integrated with the transit station, which is expected to be within the area or in close proximity. The transit center is envisioned as an iconic, convenient, and intuitively designed multimodal transportation center that serves the City of Ontario and the region at-large. [Link to Community Design Element <i>Urban, Mixed Use, and Transit-oriented Place Types</i> Section, Community Design Element <i>Exhibits CD-01, CD-02, and CD-05</i>]</p>
<p>Mixed Use - Inland Empire Corridor¹ (MU-Inland Empire)</p>	<ul style="list-style-type: none"> • 14.0 to 30.0 dwelling units per acre • 2.0 FAR for office uses • 1.0 FAR for retail uses 	<p>Located along Inland Empire Boulevard, this area is primarily residential with a retail center at the corner of Inland Empire Boulevard and Archibald. Commercial uses that relate to the park are envisioned for the area West of the regional park. [Link to Community Design Element <i>Urban, Mixed Use, and Transit-oriented Place Types</i> Section, Community Design Element <i>Exhibits CD-01, CD-02, and CD-08</i>]</p>
<p>Mixed Use - Guasti¹ (MU-Guasti)</p>	<ul style="list-style-type: none"> • 25.0 to 65.0 dwelling units per acre • 1.0 FAR for office and retail uses 	<p>This site includes the Guasti Winery, which is on the National Register of Historic Places. This area is envisioned as a mixture of high-quality office, lodging, retail and residential uses that incorporate the Guasti Winery. More intensive office and commercial uses are envisioned along I-10 while office, commercial, and lodging uses are envisioned in and around the historic structures; the southern portion of the area is being considered as potential site for the multimodal transit center. There is an approved Specific Plan on this site that may require amendment to align with TOP. [Link to Community Design Element <i>Urban, Mixed Use, and Transit-oriented Place Types</i> Section, Community Design Element <i>Exhibits CD-01, CD-02, and CD-05</i>]</p>
<p>Mixed Use - Ontario Center¹ (MU-Ontario Center)</p>	<ul style="list-style-type: none"> • 20.0 to 125.0 dwelling units per acre • 2.0 FAR for office uses • 1.0 FAR for retail uses 	<p>This area is one of the most intensive developments in Ontario and is characterized by low-rise (3-5 stories) and mid-rise (5-10 stories), mixed use buildings, iconic architecture, and regionally significant uses, such as the City owned arena and other cultural and entertainment uses. This area accommodates a vertical and horizontal mixture of entertainment, retail, office, and residential uses in an active, pedestrian oriented atmosphere. In this</p>

LU-02 Land Use Designations Summary Table

Land Use Designation	Residential Density & Non-Residential Intensity	Intention
		<p>area, the Haven Corridor is envisioned as an elegant, landscaped boulevard lined multi-story office uses near the I-10 and mixed and residential uses closer to the City's northern boundary along 4th Street. There is an approved Specific Plan on this site that may require amendment to align with TOP. [Link to Community Design Element <i>Urban, Mixed Use, and Transit-oriented Place Types</i> Section, Community Design Element <i>Exhibits CD-01, CD-02, and CD-04</i>]</p>
<p>Mixed Use - Ontario Mills¹ (MU-Ontario Mills)</p>	<ul style="list-style-type: none"> • 25.0 to 85.0 dwelling units per acre • 1.5 FAR for office uses • 1.0 FAR for retail uses 	<p>This area will continue to be our regional retail center. We envision intensification of the area to include additional retail and entertainment, office, and multi-family (3-5 story) residential uses. New development is envisioned to occur along the interior loop road and the perimeter of the area. There is an approved Specific Plan on this site that may require amendment to align with TOP. [Link to Community Design Element <i>Urban, Mixed Use, and Transit-oriented Place Types</i> Section, Community Design Element <i>Exhibits CD-01, CD-02, and CD-04</i>]</p>
<p>Mixed Use - Rich Haven (MU-Rich Haven)</p>	<ul style="list-style-type: none"> • 14.0 to 50.0 dwelling units per acre • 0.7 FAR for office and retail uses <p><i>Subject to approved Specific Plan</i></p>	<p>The Rich-Haven Mixed Use Area is within the Rich-Haven Specific Plan. This area is envisioned as a low-rise (3-5 stories), primarily horizontal mixture of retail, office, medical, and residential uses. The greatest level of intensity is envisioned along Ontario Ranch Road and Hamner Avenue.</p>
<p>Mixed Use - Great Park¹ (MU-Great Park)</p>	<ul style="list-style-type: none"> • 14.0 to 65.0 dwelling units per acre² • 1.5 FAR for office uses • 1.0 FAR for retail uses <p><i>Subject to Specific Plan³</i></p>	<p>The Great Park Mixed Use Areas are envisioned as the southwestern—activity centers for citizens of Ontario. These areas accommodate a vertical and horizontal mixture of commercial, office, entertainment, and residential uses all connecting to the Great Park with a pedestrian oriented atmosphere. It is envisioned that the major roads through these Mixed Use areas are couplets, which are a series of one-way streets that disperse traffic and allow reduced street widths, maximize the sense of community, and emphasize pedestrian accessibility. These Mixed Use areas are envisioned as low-rise (3-5 stories) with some mid-rise (5-10 stories) near the intersection of Euclid and Edison/Ontario Ranch Road. [Link to Community Design Element <i>Urban, Mixed Use, and Transit-oriented Place Types</i> Section, Community Design Element <i>Exhibits CD-01, CD-02, CD-06, CD-09</i>]</p>

LU-02 Land Use Designations Summary Table

Land Use Designation	Residential Density & Non-Residential Intensity	Intention
Mixed Use - Grove ¹ (MU-Grove)	<ul style="list-style-type: none"> • 14.0² to 65.0 dwelling units per acre • 1.5 FAR for office uses • 1.0 FAR for retail uses <i>Subject to Specific Plan³; subject to PUD at City's discretion</i>	Envisioned as a low-rise (3-5 stories), mixture of retail and residential uses that will create identity and place along the corridor and serve the surrounding residents. [Link to Community Design Element <i>Urban, Mixed Use, and Transit-oriented Place Types</i> Section, Community Design Element Exhibits CD-01, CD-02, and CD-09]
Mixed Use - Eucalyptus / Chino Airport Overlay ¹ (MU-EU)	<ul style="list-style-type: none"> • 25.0 to 45.0 dwelling units per acre (outside of airport safety zone) • 2.0 FAR for office and vertically-mixed uses • 0.60 FAR for business park and retail uses <i>Subject to Specific Plan³</i>	Envisioned to primarily accommodate employee-intensive office, entertainment facilities, live/work, and supporting retail uses in a campus environment designed to leverage proximity to the park and maintain compatibility with surrounding residential areas. Stand-alone and mixed use residential is permitted outside of the Chino Airport safety zone, primarily centered on Grove. Business park uses, such as research and development and “clean” industrial are also permitted provided they do not involve the frequent use of trucks (Class 4 or higher) as part of its primary activities. [Link to Community Design Element <i>Urban, Mixed Use, and Transit-oriented Place Types</i> Section, Community Design Element Exhibits CD-01, CD-02, and CD-07]
Mixed Use - Parkside ¹ (MU-PS)	<ul style="list-style-type: none"> • 25.0 to 45.0 dwelling units per acre • 1.0 FAR for retail uses <i>Subject to approved Specific Plan</i>	Envisioned as a low-rise (3-5 stories), mixture of retail and residential uses that will create identity and place and serve the surrounding residents. [Link to Community Design Element <i>Urban, Mixed Use, and Transit-oriented Place Types</i> Section, Community Design Element Exhibits CD-01, CD-02, and CD-09]
Mixed Use - Neighborhood Activity Hubs ¹ (MU-NH)	<ul style="list-style-type: none"> • 20.0 to 75.0 dwelling units per acre • 1.0 FAR for retail and office <i>Subject to Specific Plan³; projects outside of Ontario Ranch subject to PUD at City's discretion</i>	Envisioned as a low-rise (3-5 stories), mixture of retail and residential uses that will create identity and place along the corridor and serve the surrounding residents. [Link to Community Design Element <i>Urban, Mixed Use, and Transit-oriented Place Types</i> Section, Community Design Element Exhibits CD-01, CD-02, and CD-09]
Overlays – An overlay is intended to reflect a particular characteristic of an area and is applied “over” an underlying land use designation to provide guidance above and beyond the underlying land use designation.		
Business Park Transitional Areas	Per the underlying designation unless a non-residential use is developed in which case the density and use requirements of the Business Park land use designations shall apply.	This area is within existing and future noise and safety impact zones of Ontario International Airport. This overlay allows residential uses to transition to a Business Park land use if an entire block can be recycled to a Business Park use and the block is contiguous to another non-residential block. In these cases, the City shall be

LU-02 Land Use Designations Summary Table

Land Use Designation	Residential Density & Non-Residential Intensity	Intention
		responsible for the necessary amendments to the Policy Plan Map and Development Code.
Industrial Transitional Areas	Per the underlying designation unless a non-residential use is developed in which case the density and use requirements of the Industrial land use designations shall apply.	This area is within existing and future noise and safety impact zones of Ontario International Airport. This overlay allows residential uses to transition to an industrial land use if an entire block can be recycled to an Industrial use and the block is contiguous to another non-residential block. In these cases, the City shall be responsible for the necessary amendments to the Policy Plan Map and Development Code.
ONT Airport Influence Area	Varies	An area in which current or future airport-related noise, overflight, safety, or airspace protection factors may significantly affect land uses or necessitate restriction on those uses. Refer to the Airport Land Use Compatibility Plan for Ontario International Airport.
Chino Airport Influence Area	Varies	An area in which current or future airport-related noise, overflight, safety, or airspace protection factors may significantly affect land uses or necessitate restriction on those Uses. Refer to the Ontario Development Code for Chino Airport land use policies and criteria for development.
Landfill Impact Area	Varies	Lands immediately surrounding the Milliken Landfill may be contaminated or have other landfill-related hazards that may limit allowable uses, as well as site design. Development in this area requires the submission of a detailed environmental analysis.
Plan Required Overlay – Denotes areas where specific plans or planned unit development plans are required prior to development, with final determination at the discretion of the City. See adopted specific plans and planned unit development in the City on the City’s website.		
Ontario Airport Metro Center	Per approved individual specific plans	Envisioned as the most intensive area outside of downtown Los Angeles with a vertical and horizontal mixture of regional-serving retail, office, restaurant, entertainment, cultural, and residential uses in low to mid-rise buildings (3-10 stories). See adopted specific plans for more detail.
Ontario Ranch	Per approved individual specific plans ³	Envisioned as a mixture of residential neighborhoods integrated with areas of high intensity (3-10 stories) with a mixture of employment, retail, service, entertainment, cultural, and residential uses united by a network of greenways/trails, open spaces, amenities, and infrastructure. All development to be oriented toward or designed to leverage the “Great Park,” a linear open space amenity

LU-02 Land Use Designations Summary Table

Land Use Designation	Residential Density & Non-Residential Intensity	Intention
		containing active and passive recreational features, gardens, water features, and cultural facilities. Additional direction may be provided through the application of place types and specific plans.
Downtown	Per approved planned unit development (PUD may be waived at City's discretion) ³	Envisioned as an intensive vertical and horizontal mixture of retail, office, and residential uses in a pedestrian friendly atmosphere. The historic character is enhanced. The most intensive uses are envisioned along Euclid and Holt Avenues. See the Downtown District Plan in the City's development code for more detail.

Notes:

1. Some parcels with this designation may fall within a Place Type, which characterizes the vision and urban design intent within a specified area. If any portion of a parcel is within a Place Type boundary, as shown in Figure CD-01, Place Types in the Community Design Element, that parcel is subject to Goal CD-3, and related policies. Projects must demonstrate that they are consistent with the vision and policy intent for the applicable Place Type as defined in Exhibits CD-02 through CD-09. Link to Community Design Element *Urban, Mixed-Use, and Transit-oriented Place Types* Section.
2. Parcels designated as MDR within the affordable housing overlay zoning district allow a maximum density of 30 dwelling units per acre if the project includes 25 percent of units affordable to lower incomes, consistent with Tier 2 requirements of the overlay zone.
3. All parcels within the affordable housing overlay zoning district have a minimum density of 20 dwelling units per acre. All parcels within the affordable housing overlay zoning district are exempt from the specific plan requirement if there is no existing specific plan and the project includes 20 percent of units affordable to lower incomes, consistent with Tier 1 requirements of the overlay zone.

LU-03 Future Buildout Table

Land Use	Acres ²	Assumed Density/Intensity ³	Units	Pop ⁴	Non-Residential Square Feet	Jobs ⁵
Residential						
Rural	529	2.0 du/ac	1,057	3,863	-	-
Low Density ^{6,7}	6,509	5.9 du/ac (north of Riverside Dr)	36,173	132,167	-	-
		4.5 du/ac (south of Riverside Dr) ⁷				
Low-Medium Density ⁶	889	8.5 du/ac	7,197	26,363	-	-
Medium Density ⁷	2,237	18.0 du/ac (north of Riverside Dr)	45,469	147,233	-	-
		22.0 du/ac (south of Riverside Dr) ⁸				
High Density	206	25.0 du/ac	5,299	13,577	-	-
Subtotal	10,370		95,495	323,203	-	-
Mixed Use						
• Downtown	128	<ul style="list-style-type: none"> 60% of the area at 35 du/ac 40% of the area at 0.80 FAR for office and retail 	2,678	6,862	1,777,586	3,973
• East Holt Boulevard ⁸	65	<ul style="list-style-type: none"> 75% of the area at 30 du/ac 25% of area at 0.80 FAR retail 	1,473	3,773	570,156	475
• West Holt Boulevard ⁸	1	<ul style="list-style-type: none"> 75% of the area at 30 du/ac 25% of area at 0.80 FAR retail 	33	84	12,678	11
• Meredith	91	<ul style="list-style-type: none"> 50% of the area at 40 du/ac 50% at 0.35 FAR for retail uses 	1,815	4,651	691,939	577
• Multimodal Transit Center	73	<ul style="list-style-type: none"> 15% of the area at 60 du/ac 60% of the area at 1.0 FAR office and retail 25% of the area at 0.70 FAR Lodging 	653	1,673	2,449,557	5,993
• Inland Empire Corridor	37	<ul style="list-style-type: none"> 82% of the area at 10 du/ac 18% of area t 0.35 FAR retail 	300	769	100,455	84
• Guasti	86	<ul style="list-style-type: none"> 30% of the area at 30 du/ac 15% of area at 1.0 FAR retail 30% of area at 0.70 FAR office 25% of area at 0.70 FAR Lodging 	777	1,991	2,012,077	3,848
• Ontario Center ⁸ <i>(south of Concourse & east of Haven)</i>	186	<ul style="list-style-type: none"> 40% of area at 40 du/ac 20% of area at 1.0 FAR office 30% of area at 0.70 FAR retail 	3,729	9,553	3,329,507	7,327
• Ontario Center ⁸ <i>(north of Concourse & west of Haven)</i>	169	<ul style="list-style-type: none"> 75% of area at 40 du/ac 10% of area at 1.00 FAR office 15% of area at 0.80 FAR retail 	5,076	13,006	1,621,435	3,417
• Ontario Mills	249	<ul style="list-style-type: none"> 40% of area at 40 du/ac 5% of area at 0.75 FAR office 55% of area at 0.50 FAR retail 	3,990	10,223	3,394,666	3,971
• Rich-Haven ²	154	<ul style="list-style-type: none"> 30% of area at 35 du/ac 70% of area at 0.70 FAR office and retail 	3,311	6,621	6,729,889	17,188
• Parkside	3	<ul style="list-style-type: none"> 50% of the area at 35 du/ac 50% of area at 0.80 FAR retail 	52	132	51,440	129

LU-03 Future Buildout Table

Land Use	Acres ²	Assumed Density/Intensity ³	Units	Pop ⁴	Non-Residential Square Feet	Jobs ⁵
• Eucalyptus/ Chino Airport ²	105	<ul style="list-style-type: none"> • 40% of the area at 35 du/ac • 10% of area at 0.80 FAR retail • 20% of area at 1.00 FAR office • 30% of area at 0.60 FAR business park (industrial) 	1,471	3,769	2,059,760	4,008
• Grove ²	36	<ul style="list-style-type: none"> • 65% of the area at 35 du/ac • 25% of area at 0.80 FAR retail • 10% of area at 0.70 FAR Office 	821	2,104	385,285	630
• Great Park ²	305	<ul style="list-style-type: none"> • 30% of area at 25 du/ac • 30% of area at 0.35 FAR for office • 40% of area at 0.30 FAR for retail uses 	7,470	19,141	2,789,181	4,930
• Neighborhood Activity Hub ⁸ (Mountain Village)	8	<ul style="list-style-type: none"> • 50% of the area at 35 du/ac • 50% of area at 0.80 FAR retail 	137	350	136,070	340
• Neighborhood Activity Hub ⁸ (Mountain & Fourth)	7	<ul style="list-style-type: none"> • 70% of the area at 50 du/ac • 30% of area at 0.80 FAR retail 	251	643	75,008	188
• Neighborhood Activity Hub ⁸ (Euclid & Francis)	13	<ul style="list-style-type: none"> • 50% of the area at 30 du/ac • 50% of area at 0.80 FAR retail 	190	487	220,912	552
• Neighborhood Activity Hub ⁸ (Euclid & Walnut)	16	<ul style="list-style-type: none"> • 75% of the area at 30 du/ac • 25% of area at 0.80 FAR retail 	369	945	142,840	357
• Neighborhood Activity Hub ^{2,8} (Euclid & Riverside)	15	<ul style="list-style-type: none"> • 75% of the area at 35 du/ac • 25% of area at 0.80 FAR retail 	394	1,009	130,662	327
Subtotal	1,748	-	34,067	87,289	22,340,301	41,508
Retail/Service						
Neighborhood Commercial ⁶	248	0.30 FAR	-	-	3,238,367	8,832
General Commercial	385	0.30 FAR	-	-	5,033,395	5,605
Office/Commercial	310	0.75 FAR	-	-	-	-
Hospitality	143	1.00 FAR	-	-	9,981,163	27,902
Subtotal	1,085		-	-	24,583,993	51,057
Employment						
Business Park	1,149	0.50 FAR	-	-	25,028,936	43,251
Industrial	7,664	0.55 FAR	-	-	183,596,786	148,483
Subtotal	8,812		-	-	208,625,721	191,734
Other						
Open Space–Non-Recreation	1,200	Not applicable	-	-	-	-

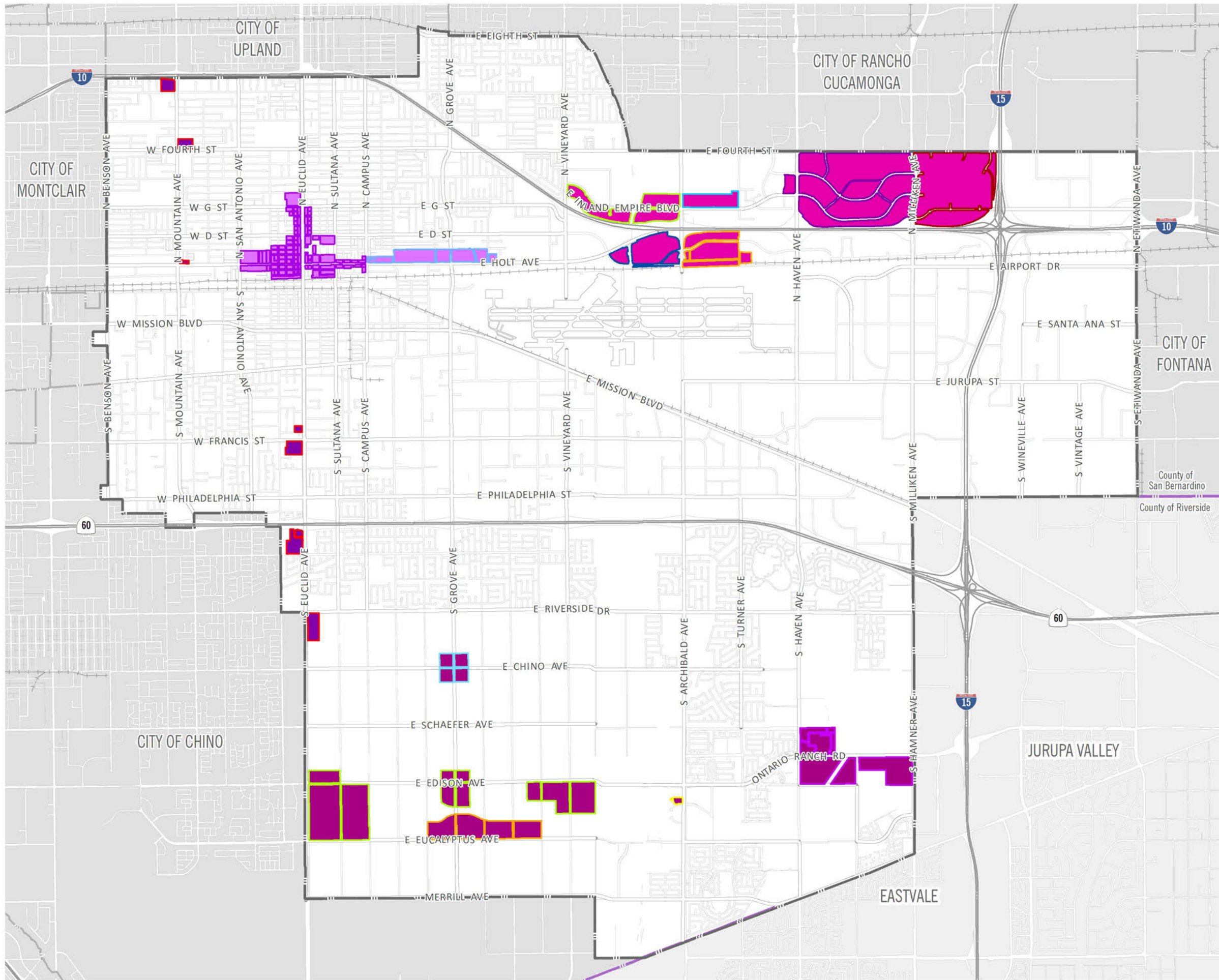
LU-03 Future Buildout Table

Land Use	Acres ²	Assumed Density/Intensity ³	Units	Pop ⁴	Non-Residential Square Feet	Jobs ⁵
Open Space–Parkland ⁶	913	Not applicable	-	-	-	-
Open Space–Water	17	Not applicable	-	-	-	-
Public Facility	90	Not applicable	-	-	-	-
Public School	614	Not applicable	-	-	-	-
Ontario International Airport	1,423	Not applicable	-	-	-	-
Landfill	137	Not applicable	-	-	-	-
Railroad	250	Not applicable	-	-	-	-
Roadways	5,364	Not applicable	-	-	-	-
<i>Subtotal</i>	10,007	-	-	-	-	-
Total	32,022	-	129,562	410,492	261,491,779	296,002

Notes

- Historically, citywide buildout levels do not achieve the maximum allowable density/intensity on every parcel and are, on average, lower than allowed by the Policy Plan. Accordingly, the buildout projections in this Policy Plan do not assume buildout at the maximum density or intensity and instead are adjusted downward. There are a few notable exceptions to this. The assumed density for Low-Density Residential north of Riverside Drive is estimated above the allowed density to account for the continued use of areas that were built at densities above the current allowance, and to account for growth in these areas due to accessory dwelling unit (ADU) development and lot splits. Some areas were also entitled at greater densities due to the use of gross acreage rather than net acreage. These areas, identified in note 7 utilized customized growth assumptions to ensure that the future projection was equal to or larger than the number of existing units in built areas, and equal to or larger than existing entitlements in undeveloped or partially developed areas.
- Acres are based on the City's Parcel data as of January 2021.
- Assumed Density/Intensity includes both residential density, expressed as units per acre, and non-residential intensity, expressed as floor area ratio (FAR), which is the amount of building square feet in relation to the size of the lot.
- Projections of population by residential designation are based on projected occupancy rates and a persons-per-household factor that varies by housing type.
- The factors used to generate the number of employees by land use category were derived by comparing 2018 employment data published by the U.S. Census as part of the Longitudinal Employer-Household Dynamics (LEHD) dataset to existing building square footage by use type, as recorded in the City's building permit database.
- Acreages and corresponding buildout estimates for these designations do not reflect underlying land uses within the Business Park and Industrial Overlays. Estimates within overlay areas are included within the corresponding Business Park and Industrial categories.
- To ensure future projections are equal to or larger than existing conditions in built out areas as of August 2021, and equal to or larger than existing entitlements (approved before August 2021) in undeveloped or partially developed areas, buildout estimates for the following projects, assumed realistic densities as follows: Countryside Specific Plan (Low Density Residential: 6.18 du/ac), Creekside Specific Plan (Low Density Residential: 7.47 du/ac, Medium Density Residential: 15.81 du/ac), Subarea 29 Specific Plan (Low Density Residential: 7.03).
- In certain mixed use categories, different buildout assumptions were employed on different sites. Variations in assumptions are included in multiple rows within this table.

Last Amended: [Placeholder for TOP 2050 Policy Plan Adoption Date]



LAND USE

Figure LU-04
Mixed Use Areas

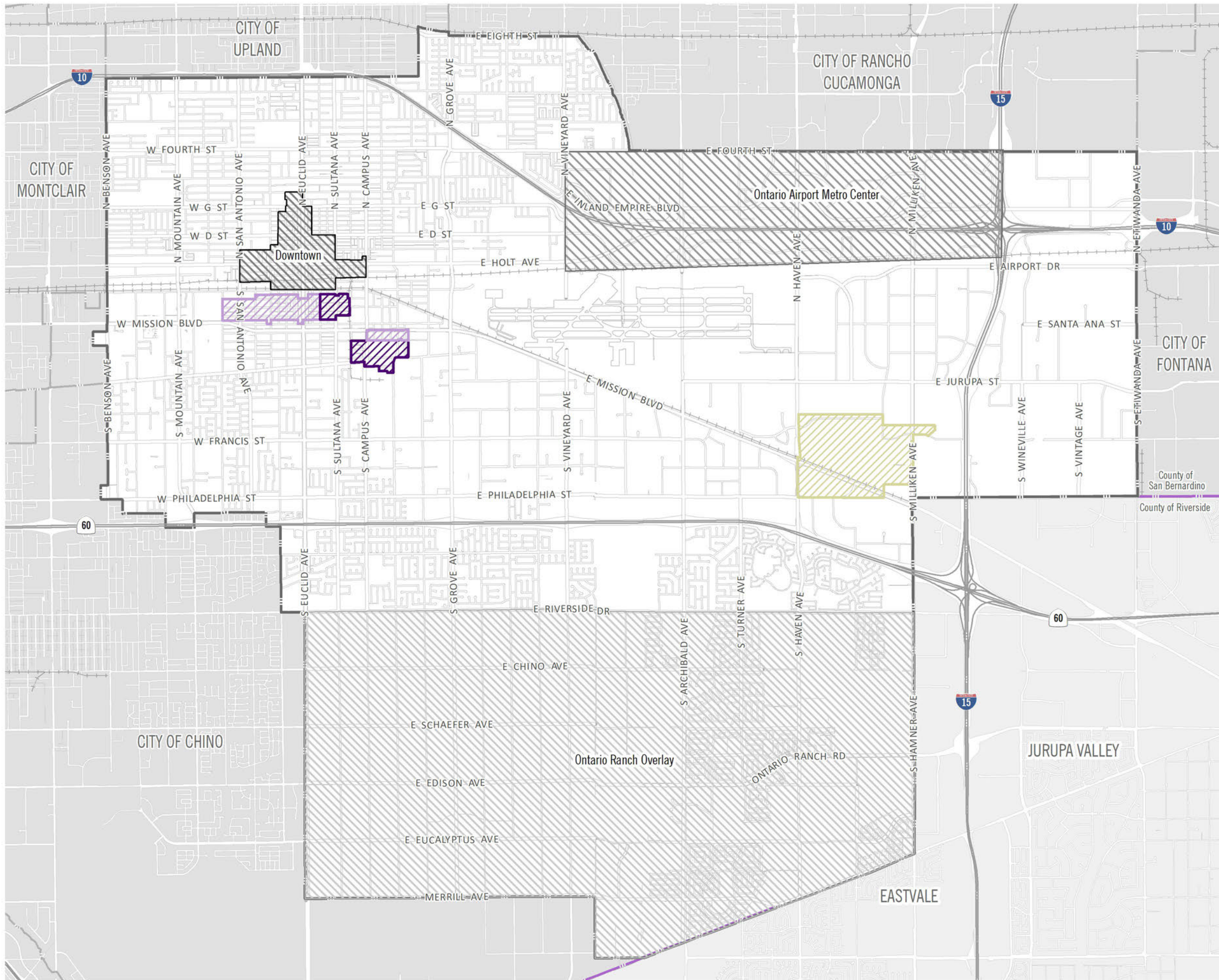
Mixed Use Categories

- Mixed Use - Downtown
- Mixed Use - Holt Blvd
- Mixed Use - Meredith
- Mixed Use - Multimodal
- Mixed Use - Inland Empire Corridor
- Mixed Use - Guasti
- Mixed Use - Ontario Center
- Mixed Use - Ontario Mills
- Mixed Use - Rich Haven
- Mixed Use - Great Park
- Mixed Use - Grove
- Mixed Use - Eucalyptus / Chino Airport Overlay
- Mixed Use - Parkside
- Mixed Use - Neighborhood Activity Hub
- Ontario City Boundary
- County Boundary
- Rail Network

2050
THE ONTARIO PLAN

0 2,500 5,000 10,000 FT

Source: The City of Ontario 2022 Date: 5/9/22



LAND USE

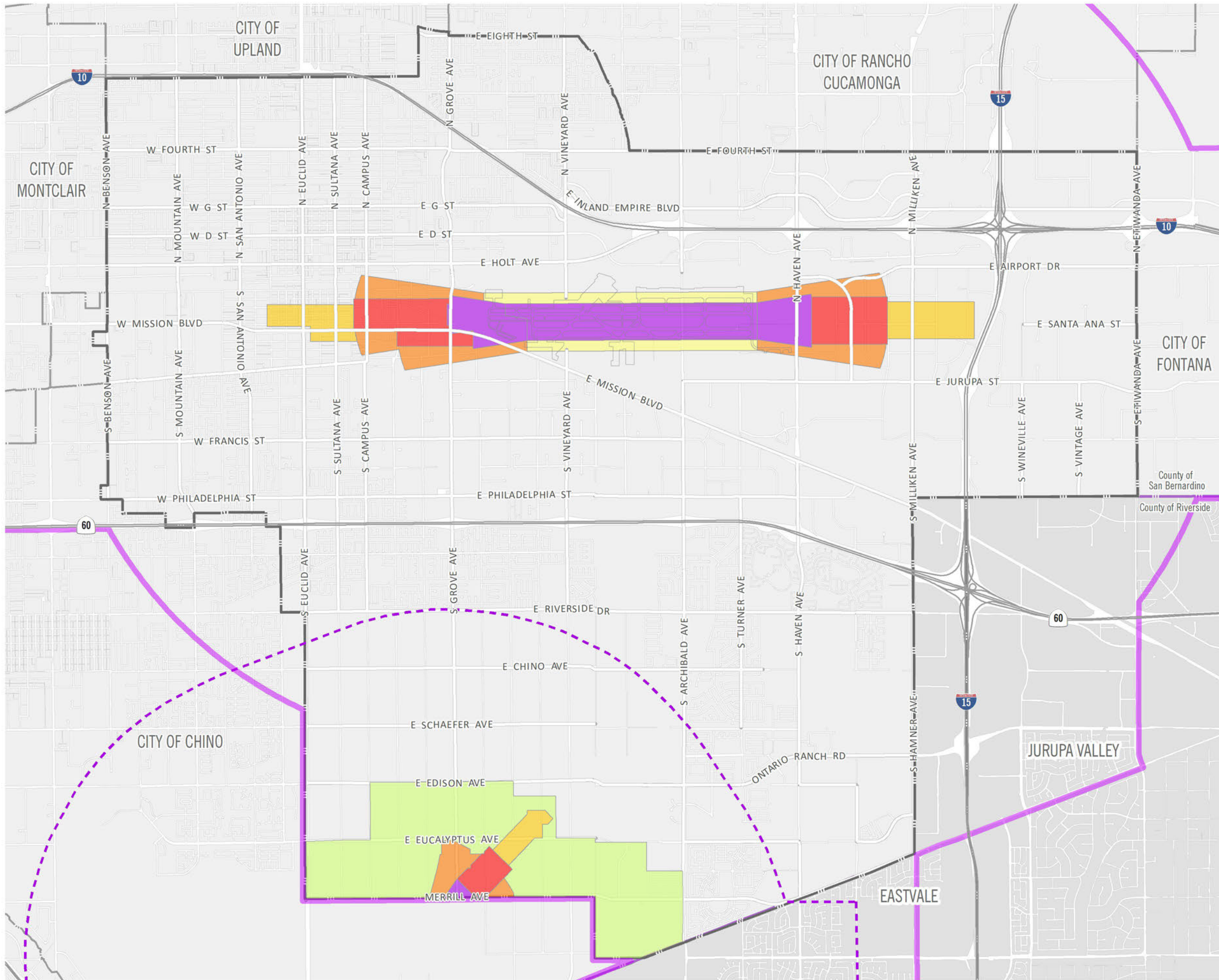
Figure LU-05
Overlay Areas

- Ontario City Boundary
- County Boundary
- Rail Network
- Overlays**
- Business Park
- Industrial
- Landfill Impact Area
- Plan-Required Overlays**
- Downtown District Overlay
- Ontario Airport Metro Center Overlay
- Ontario Ranch Overlay

2050
THE ONTARIO PLAN

0 2,500 5,000 10,000 FT

Source: The City of Ontario 2022 Date: 5/9/22



LAND USE

Figure LU-06
 Airport Safety Zones
 & Influence Areas

- Ontario City Boundary
- County Boundary
- Ontario International Airport Safety**
 - ZONE-1
 - ZONE-2
 - ZONE-3
 - ZONE-4
 - ZONE-5
- Chino Airport Safety Zones**
 - Safety Zone 1
 - Safety Zone 2
 - Safety Zone 3
 - Safety Zone 4
 - Safety Zone 6
- Airport Influence Areas**
 - Airport Influence Area (2020)
 - Chino Airport Influence Area (2009)

2050
THE ONTARIO PLAN

0 2,500 5,000 10,000 FT

Source: City of Ontario, 2019 and 2022 Date: 5/9/22

LU-05 Noise Level Exposure and Land Use Compatibility Guidelines

Land Use Categories		Community Noise Equivalent Level (CNEL)					
Category	Land Use	55	60	65	70	75	80
Residential / Lodging	Single Family / Duplex	Green	Green	Yellow	Orange	Red	Red
	Multi-Family	Green	Green	Yellow	Orange	Orange	Red
	Mobile Homes	Green	Green	Yellow	Red	Red	Red
	Hotel/Motels	Green	Green	Green	Yellow	Orange	Orange
Public / Institutional	Schools/Hospitals	Green	Green	Yellow	Orange	Red	Red
	Churches/ Libraries	Green	Green	Yellow	Orange	Red	Red
	Auditoriums/Concert Halls	Green	Yellow	Orange	Orange	Red	Red
Commercial	Offices	Green	Green	Green	Yellow	Yellow	Orange
	Retail	Green	Green	Green	Green	Yellow	Orange
Industrial	Manufacturing	Green	Green	Green	Green	Yellow	Orange
	Warehousing	Green	Green	Green	Green	Yellow	Yellow
Recreational/ Open Space	Parks/Playgrounds	Green	Green	Green	Yellow	Orange	Red
	Golf Courses/ Riding Stables	Green	Green	Green	Yellow	Orange	Red
	Outdoor Spectator Sports	Green	Green	Yellow	Orange	Orange	Red
	Outdoor Music Shells/ Amphitheaters	Yellow	Yellow	Orange	Red	Red	Red
	Livestock/Wildlife Preserves	Green	Green	Green	Green	Orange	Red
	Crop Agriculture	Green	Green	Green	Green	Green	Green

Legend

	Clearly Acceptable:	No special noise insulation is required, assuming buildings of normal conventional construction.
	Normally Acceptable:	Acoustical reports will be required for major new residential construction. Conventional construction with closed windows and fresh air supply systems of air conditioning will normally suffice.
	Normally Unacceptable:	New construction should be discouraged. Noise/aviation easements are required for all new construction. If new construction does proceed, a detailed analysis of noise reduction requirements must be made, and necessary noise insulation features included.
	Clearly Unacceptable:	No new construction should be permitted.

Note: For noise compatibility criteria and contours for Ontario International Airport refer to the adopted ALUCP for ONT.

HOUSING ELEMENT

Introduction

The State of California recognizes the importance of housing and therefore legislates requirements for local jurisdictions to contribute to solutions to meet their local and regional housing needs. All communities across California are therefore required to prepare a Housing Element every eight years to address their local housing needs and a share of the region's need for housing.

Purpose

An adequate supply of quality and affordable housing is fundamental to the economic and social well-being of Ontario. The Housing Element is required to address the production, preservation, and improvement of housing in the community. Among its most important functions, the Housing Element analyzes existing and future housing needs; addresses constraints to meeting local housing needs; identifies land, financial, and administrative resources for housing; sets forth goals and policies to meet community housing needs; and establishes housing programs and an implementation plan.

Principles

The City believes:

- A range of housing for all income levels is essential to a complete community.
- The City's housing stock should match the type and price needed by current and future residents and workforce, including those with special needs.
- Preserving, maintaining, improving, and creating distinct neighborhoods and the housing stock within them protects property values and provides a desirable place to live.
- Affordable, quality housing helps attract and retain a qualified workforce and supports a prosperous local economy.

Goals & Policies

H-1 Neighborhoods & Housing

Ontario's neighborhoods determine our quality of life and reflect the value we place in our community. Neighborhoods differ in lot sizes, housing types, history, purpose, and environment. Whether rural residential, suburban, historic, or urban, Ontario's neighborhoods should provide a nurturing environment for all residents to enjoy their lives. Residential neighborhoods should

provide quality housing, ample parks and recreational opportunities, tree-lined streets and sidewalks for walking, safety and security, and public facilities and services.

As an established community, Ontario is committed to improving its older neighborhoods. This may be achieved through redevelopment, housing rehabilitation, code enforcement, and neighborhood improvement projects. Ontario will facilitate the development of new neighborhoods consistent with their unique purpose, such as Ontario Ranch (areas south of Riverside Drive), Ontario Airport Metro Center, and other areas. Taken together, Ontario is committed to creating and strengthening neighborhoods to promote a high quality of life for residents.

Goal H-1 Stable neighborhoods of quality housing, ample community services and public facilities, well-maintained infrastructure, and public safety that foster a positive sense of identity.

Policies

H-1.1 Housing Rehabilitation. We support the rehabilitation, maintenance, and improvement of single-family, multiple-family, and mobile homes through code compliance, removal of blight where necessary, and provision of rehabilitation assistance where feasible.

H-1.2 Neighborhood Conditions. We direct efforts to improve the long-term sustainability of neighborhoods through comprehensive planning, provision of neighborhood amenities, rehabilitation and maintenance of housing, and community building efforts.

H-1.3 Community Amenities. We shall provide adequate public services, infrastructure, open space, parking and traffic management, pedestrian, bicycle, and equestrian routes and public safety for neighborhoods consistent with City master plans and neighborhood plans.

H-1.4 Historical Preservation. We support the preservation and enhancement of residential structures, properties, street designs, lot configurations, and other reminders of Ontario's past that are considered to be local historical or cultural resources.

H-1.5 Neighborhood Identity. We strengthen neighborhood identity through creating parks and recreational outlets, sponsoring neighborhood events, and encouraging resident participation in the planning and improvement of their neighborhoods.

H-2 Housing Supply & Diversity

Bolstered by its International Airport, burgeoning employment sector, the Ontario Ranch, and unparalleled transportation access, Ontario aspires to be the premier community of the Inland Empire. Housing diversity is critical to achieving this goal. Ontario is committed to ensuring the provision of the widest range of housing choices for the varied lifestyles of its residents and future

workforce. This includes single-family and multiple-family housing, mixed- and multi-use housing, senior housing, live-work units, and other types of housing opportunities.

Housing production is to be encouraged in a responsible manner that furthers citywide and neighborhood goals. New housing will be creatively designed, sustainable and accessible. Residential and mixed-use growth is strategically directed to the Downtown, corridors, Ontario Airport Metro Center Area, Ontario Ranch, and other areas. By encouraging an adequate supply and diversity of housing, Ontario will accommodate its changing housing needs, support economic prosperity, foster an inclusive community, and become the premier community of the Inland Empire.

Goal H-2 **Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.**

Policies

- H-2.1** **Corridor Housing.** We revitalize transportation corridors by encouraging the production of higher density residential and mixed-uses that are architecturally, functionally, and aesthetically suited to corridors.
- H-2.2** **Historic Downtown.** We foster a vibrant historic downtown through facilitating a wide range of housing types and affordability levels for households of all ages, housing preferences, and income levels.
- H-2.3** **Ontario Airport Metro Center.** We foster a vibrant, urban, intense, and highly amenitized community in the Ontario Airport Metro Center Area through a mix of residential, entertainment, retail, and office-oriented uses.
- H-2.4** **Ontario Ranch.** We support a premier lifestyle community in the Ontario Ranch, distinguished by diverse housing, highest design quality, and cohesive and highly amenitized neighborhoods.
- H-2.5** **Housing Design.** We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices, and other best practices.
- H-2.6** **Infill Development.** We support the revitalization of neighborhoods through the construction of higher-density residential developments on underutilized residential and commercial sites.

H-3 Governmental Regulations

The City is committed to facilitating and encouraging the production, maintenance and improvement of housing in a responsible manner. However, various factors may limit the City's ability to address its housing needs, such as governmental regulations or environmental considerations. Market factors may also affect the feasibility of building housing or the affordability of housing in the community. Moreover, housing goals may at times conflict with

the need to promote other important City goals, including open space or the provision of jobs for the region.

Whereas City land use policy and municipal codes provide a regulatory framework for addressing housing, existing regulations cannot address every situation. In order to facilitate the type of development desired and to realize the greatest community benefits, the City's regulatory framework must be flexible and incentive based. The development review process must be time sensitive, predictable, and thorough. The review process must support long-term community benefits, rather than just short-term gain. Finally, the regulatory framework must contain a broad range of incentives to stimulate desired development and private investment and realize the community features that improve quality of life.

Goal H-3 A City regulatory environment that balances the need for creativity and excellence in residential design, flexibility and predictability in the project approval process, and the provision of an adequate supply and prices of housing.

Policies

- H-3.1 Incentives.** We maintain incentive programs that can be offered to projects that provide benefits to the community such as exceptional design quality, economic advantages, environmental sustainability, or other benefits that would otherwise be unrealized.
- H-3.2 Flexible Standards.** We allow flexibility in the application of residential and mixed-use development standards in order to gain benefits such as exceptional design quality, economic advantages, sustainability, or other benefits that would otherwise be unrealized.
- H-3.3 Development Review.** We maintain a residential development review process that provides certainty and transparency for project stakeholders and the public, yet allows for the appropriate review to facilitate quality housing development.
- H-3.4 Financial Incentives.** We consider financial incentives to facilitate and encourage the production, rehabilitation or improvement of housing, or provision of services where such activity furthers housing and community-wide goals.

H-4 Housing Assistance

Ontario recognizes the importance of an adequate supply of affordable housing and its importance to the quality of life of residents. Residential developments in the Ontario Ranch and Ontario Airport Metro Center area will provide quality housing opportunities to attract and retain Ontario's workforce and support Citywide economic development goals. Lower- and moderate-income residents will require homeownership and rental assistance to secure and maintain housing.

Housing prices and rents in Ontario and across the region continue to lead to lower homeownership rates, longer commutes, increased traffic congestion, higher cost burdens, and overcrowding in neighborhoods. Working with partners and the state and federal governments,

the City of Ontario is committed to providing a range of housing types and prices affordable to all economic segments of the City and assisting residents and the workforce to secure and maintain housing that is affordable and appropriate to their needs.

Goal H-4 Increased opportunities for low- and moderate-income households and families to afford and maintain quality ownership and rental housing opportunities, including move-up opportunities. Inclusive communities, racial equity, fair housing choice, and access to opportunity.

Policies

- H-4.1 Preservation of Affordable Apartments.** We strive to facilitate the preservation of the affordability of publicly assisted apartments for lower-income households through financial assistance, technical assistance, rehabilitation, and collaborative partnerships.
- H-4.2 Homeownership Opportunities.** We increase and expand homeownership rates for lower- and moderate-income households by offering financial assistance, low-interest loans, and educational resources, and by working in collaboration with partnerships.
- H-4.3 Rental Assistance.** We support the provision of rental assistance for individuals and families earning extremely low, very low, and low income with funding from the state and federal government.
- H-4.4 Mixed-income Housing.** We encourage the integration of affordable housing in the Ontario Ranch, Ontario Airport Metro Center Area, and existing neighborhoods.
- H-4.5 Collaborative Partnerships.** We support collaborative partnerships of nonprofit organizations, affordable housing developers, major employers, and for-profit developers to produce affordable housing.
- H-4.6 Fair Housing.** We further fair housing by prohibiting discrimination in the housing market, lifting barriers that restrict access to housing, and providing education, support, and enforcement services to address discriminatory practices.

H-5 Special Needs

The City of Ontario is home to a large number of people with special housing needs. These special needs may be related to occupation, income, family characteristics, disability, veteran status or other characteristics. Special needs groups include, but are not limited to, seniors, large families with children, people with disabilities, single-parent families, college students, veterans and people who are homeless. Though each group is markedly different, they share the challenge of finding suitable and affordable housing.

Ontario aspires to be the premier community in the Inland Empire. As such, the city's population will become increasingly diverse, with people of many cultures, backgrounds, family types, ages, and experiences. The housing needs of Ontario's residents will be equally diverse. Recognizing

the contributions of this diversity to the community, Ontario has the opportunity to demonstrate leadership in addressing the housing and support needs of all residents. Ontario is thus committed to creating a community that allows people to live in the City for their entire life, regardless of their special needs.

Goal H-5 **A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age, or other status.**

Policies

H-5.1 **Senior Housing.** We support the development of accessible and affordable senior housing and provide financial assistance for seniors to maintain and improve their homes.

H-5.2 **Family Housing.** We support the development of larger rental apartments that are appropriate for families with children, including, as feasible, the provision of services, recreation, and other amenities.

H-5.3 **Disabled People.** We increase the supply of permanent, affordable, and accessible housing for people with disabilities, and provide assistance to allow them to maintain and improve their homes.

H-5.4 **Homeless People.** We partner with nonprofit partners to provide emergency shelters, transitional housing, permanent supportive housing, and supportive services for people who are homeless.

H-5.5 **Supportive Services.** We financially support organizations, as feasible, that provide support services that meet the needs of those with special needs and further the greatest level of independence.

H-5.6 **Partnerships.** We collaborate with nonprofit organizations, private developers, employers, government agencies, and other interested parties to develop affordable housing and provide support services.

Exhibits: Documents Incorporated by Reference

Housing Element Technical Report

PARKS AND RECREATION ELEMENT

Introduction

Parks and recreational spaces represent a critical physical, social and psychological element of a complete community. Beyond merely providing recreational opportunities, carefully sited and well-designed parks serve a multitude of functions for the community: creating neighborhood identity, serving as focal points for gathering and celebration, and increasing and protecting environmental resources. Moreover, parks act as the city's "breathing spaces," where people experience relief from the urban environment as they go about their daily routines.

Purpose

The Parks and Recreation Element:

- Establishes goals for the Ontario park and recreation system and recreation programs.
- Underscores the vital role parks and recreation programs play in achieving economic development, land use, housing, community health, infrastructure, and transportation objectives.

Principles

The City believes:

- Parks promote community engagement, economic investment, and quality aesthetic design.
- Parks should be distributed throughout the City.
- Well planned, managed and maintained parks provide opportunities for physical exercise and relaxation, which can lead to better health of members of the community.
- Programs and activities enrich the lives of members of the community.

Goals & Policies

PR-1 Planning & Design

Ontario's Park system will establish park sites that serve the City's population and employment locations. These parks will provide active, organized uses such as sports, local gathering spaces, recreational facilities, passive use areas, and signature destinations with notable attractions. They will be planned and designed to integrate with their surroundings.

Goal PR-1 A system of safe and accessible parks that meets the needs of the community.

Policies

- PR-1.1 Access to Parks.** In all new residential development areas, we strive to provide a park and/or recreational facility within walking distance (¼ mile) of every residence and prioritize the establishment of parks in environmental justice areas that do not have adequate access to parks.
- PR-1.2 Adjacency to Schools.** We examine locating parks adjacent to school sites to promote joint-use opportunities.
- PR-1.3 Funding.** We shall seek outside, one-time sources of funding for capital improvements and reserve ongoing City funds primarily for operations and maintenance.
- PR-1.4 Joint-use Opportunities.** In areas where there is a need but no City recreational facility, we explore joint-use opportunities. (e.g., school sites).
- PR-1.5 Acreage Standard.** We strive to provide 5 acres of parkland (public and private) per 1,000 residents.
- PR-1.6 Private Parks.** We expect development to provide a minimum of 2 acres of developed private park space per 1,000 residents.
- PR-1.7 Special Needs/Universal Design.** We attempt to provide recreational opportunities at parks for people of all ages and abilities.
- PR-1.8 Renovation.** We examine renovating existing facilities prior to building replacement facilities.
- PR-1.9 Phased Development.** We require parks be built in new communities before a significant proportion of residents move in.
- PR-1.10 Master Plans for Individual Park Facilities.** We require an individual park master plan for parks in excess of 10 acres.
- PR-1.11 Environmental Function of Parks.** We require new parks to meet environmental management objectives.
- PR-1.12 Trails.** We promote connections between parks and local trails including those managed by other public agencies.
- PR-1.13 Equestrian Trails.** We require the design, construction, and maintenance of equestrian trails in Rural Residential designated areas.
- PR-1.14 Multi-family Residential Developments.** We require that new multi-family residential developments of five or more units provide recreational facilities or open space, in addition to paying adopted impact fees.

PR-1.15 Trail Connectivity. We strengthen and improve equestrian, bike, and multipurpose trail connections within the City and work to improve trail connections into adjacent jurisdictions.

PR-1.16 Equestrian Master Plan. We use Homer Briggs Park as the primary focal point for the development of a Master Plan of Equestrian Trails in the Rural Residential area.

PR-2 Programming

Programs maximize the use of facilities, enrich the lives of residents, and strengthen social networks. Ontario's park and recreational facilities should be programmed to provide a range of recreational, cultural and educational opportunities that meet the needs of the entire community. As needs continue to evolve, we will respond by involving community members and organizations in programming, fostering a sense of ownership and increasing park usage, thereby creating a safer park environment.

Goal PR-2 A range of recreational programs provided by public, private, and nonprofit organizations that meet the needs of the community's varied interests, age groups, and abilities.

Policies

PR-2.1 Participation. We program park facilities to maximize utilization and participation, while considering park size, location, and population served.

PR-2.2 Needs Assessment. We track the needs and priorities for recreational programming and look for ways to meet demand.

PR-2.3 Community Involvement. We involve the local community in planning programs for neighborhood and community park facilities.

PR-2.4 Access to Programs. We provide a range of recreational and physical exercise programs that are accessible to residents of all income levels throughout the community and prioritize establishing and maintaining equitable access for residents in environmental justice areas.

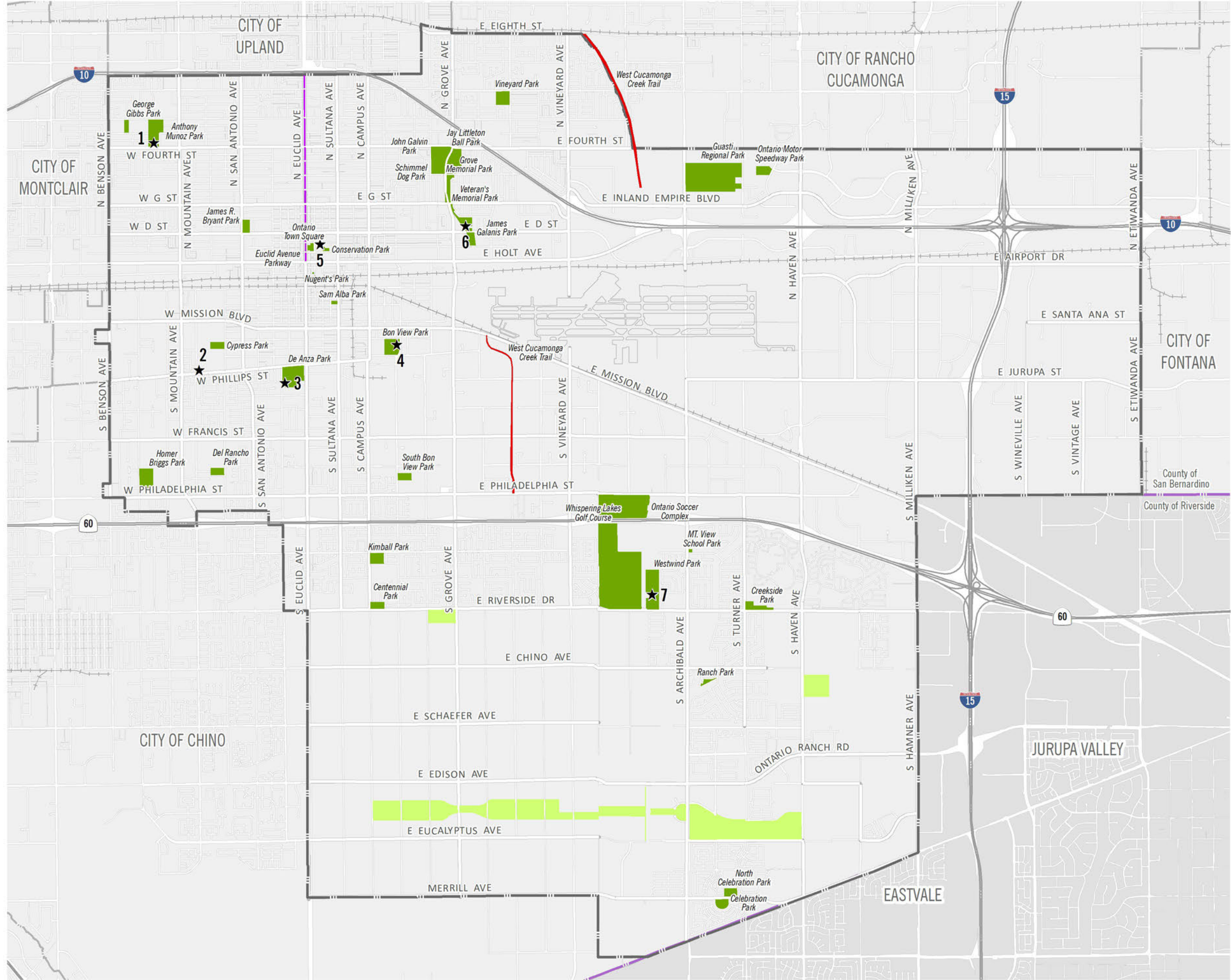
PR-2.5 Partnerships. We partner with local and regional agencies, nonprofit organizations, and the private sector to provide a comprehensive range of recreational programs.

PR-2.6 Crime Deterrents. We promote and participate in recreational programming as part of our crime prevention effort.

Exhibits: Figures

PARKS & RECREATION

Figure PR-01
Park & Recreational Facilities



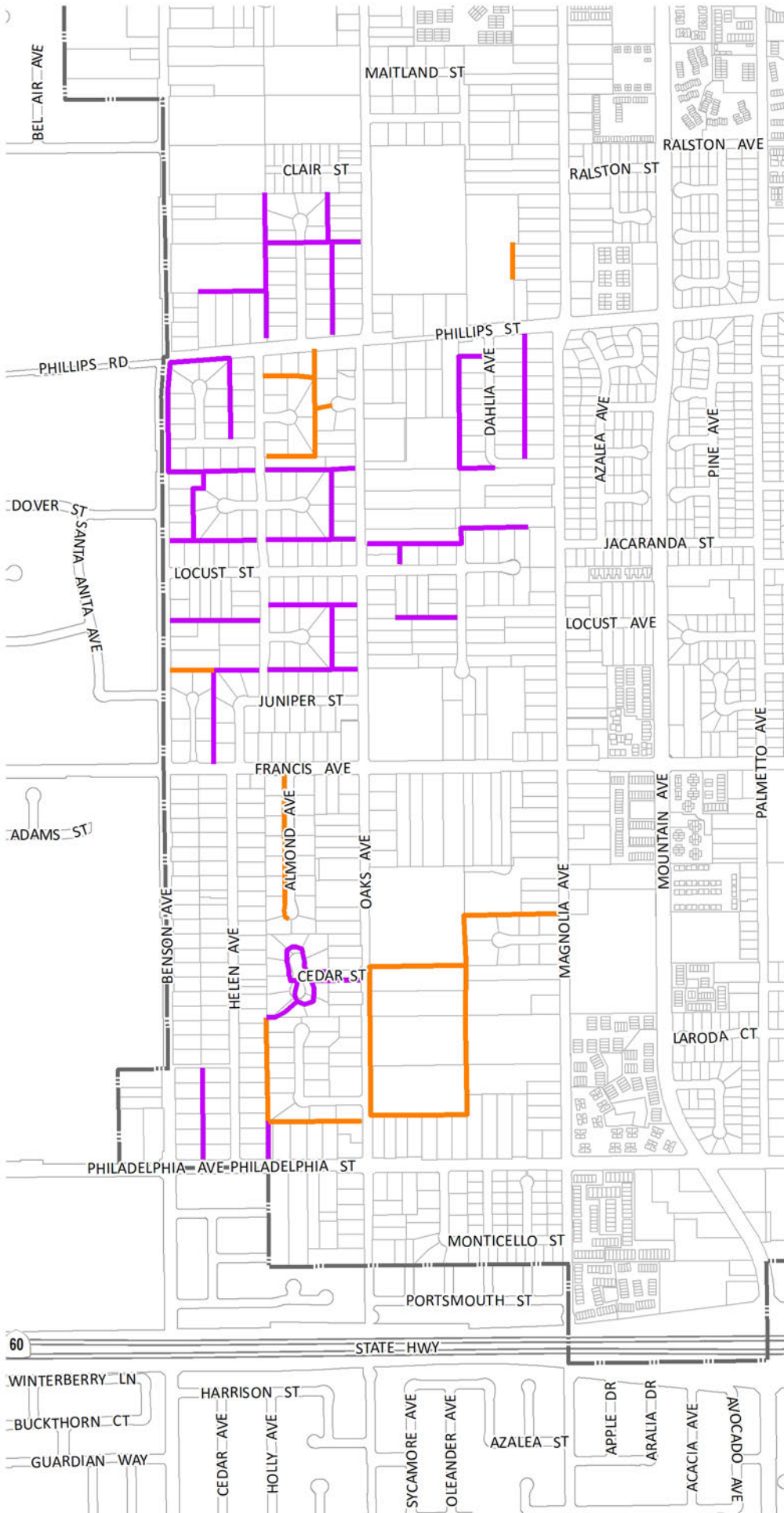
- Existing Park
 - Existing Parkway
 - Existing Trail
 - Proposed Public Park
 - Ontario City Boundary
 - County Boundary
 - Rail Network
 - ★ Community Center
1. Anthony Munoz Community Center
 2. Armstrong Community Center
 3. De Anza Community & Teen Center
 4. Dorothy A Quesada Community Center
 5. Ontario Senior Center
 6. Veterans Memorial Community Center
 7. Westwind Community Center

2050
THE ONTARIO PLAN

0 2,500 5,000 10,000 FT

Source: The City of Ontario 2021 Date: 5/9/22

Figure PR-02 Equestrian Trails



- City Equestrian Trail
- Private Equestrian Trail
- Ontario City Boundary

20050
THE ONTARIO PLAN

0 400 800 1,600 FT

Source: The City of Ontario 2022 Date: 5/9/22

ENVIRONMENTAL RESOURCES ELEMENT

Introduction

Ontario's environmental resources are an integral part of our maturing City and are valued for the benefits they provide. Appropriately managed, they reduce health risks for residents and workers, lower energy and water costs, safeguard our water assets, and protect our environment. The systems that convey and treat water and waste, generate and distribute energy, promote beneficial air quality, and protect natural resources are essential to Ontario's health and prosperity. Ontario will manage resources comprehensively, utilizing natural and man-made systems that are both environmentally and economically sustainable.

Meeting growth demands in Ontario, however, requires careful treatment of our land, water, air and energy – not as disposable goods but as essential commodities and capital assets.

Particular focus must be given to improving the conditions and opportunities for those residents that face the highest risk of exposure to pollution and are also burdened by socioeconomic and health issues. These residents are considered to be disadvantaged and live in environmental justice areas. Our Vision and Principles throughout the Policy Plan reinforce the City's commitment to enabling all persons to enjoy equal access to healthy environments, healthy foods, parks and recreational facilities, and civic engagement opportunities. Although this Element includes a number of policies and a map that addresses the topic of environmental justice, the City has opted for an "environmental justice in all policies" approach to ensure the topic is present alongside the multitude of issues and topics that affect our residents and resources.

Purpose

The Environmental Resources Element:

- Defines the ethic to guide management of the City's environmental resources.
- Establishes goals for Environmental Infrastructure.
- Maps environmental justice areas.
- Establishes policies that support system integration, resource conservation and regeneration, energy independence, environmental justice, and healthy communities.

Principles

The City believes:

- Quality design of the physical environment includes vigilant stewardship of the City's environmental resources.

- Efficient backbone infrastructure systems should be multifunctional, strategically sited, sustainably designed, and integrated into the urban fabric.
- Commitment to the development and maintenance of our environmental infrastructure ensures community prosperity.
- Environmental infrastructure is a critical public investment.
- Every resident of Ontario should have the opportunity to live in a community that is healthy and safe.
- High quality environmental resources are integral building blocks of the community.
- Ecosystems improve public health and contribute significantly to the City's overall economic vitality.
- In order to protect our environmental resources, we must make wise decisions regarding the use of these resources.
- Protecting environmental resources is the responsibility of individuals, communities, the region, and the world.

Goals & Policies

ER-1 Water & Wastewater

Given constrained supplies in Southern California, water is a precious commodity that is critical to the City's ability to achieve its Vision.

Ontario gets water from four sources: groundwater, imported water, recycled water and local precipitation. Ontario's potable water supply comes predominantly from a combination of groundwater and imported sources. Recycled water is available for non-potable purposes in portions of the City. Some urban run-off is captured and used for recharging the aquifer, but most travels through the City during storm events via natural and man-made systems to Prado Dam. Protection of ground and surface water quality is important to the continued availability of the resource. Wholesale recycled water is available through the Inland Empire Utilities Agency (IEUA) for distribution by the City.

The City maintains a wastewater collection system and contracts with the IEUA for wastewater treatment. IEUA also operates an industrial non-reclaimable wastewater system, which runs through Ontario and is available to our industries.

Ontario recognizes that all water sources are not of equal quality, but all water has value and the water used should match the water quality required for the use. Recycled water, for example, should be used for landscape irrigation. The City's approach to water resources is to conserve when possible, reclaim and reuse where feasible, and ensure "the right water for the right use."

Goal ER-1 A reliable and cost-effective system that permits the City to manage its diverse water resources and needs.

Policies

ER-1.1 Local Water Supply. We increase local water supplies to reduce our dependence on imported water. New and redevelopment projects are aligned with our available water supply and/or to enhance our available water supply.

ER-1.2 Matching Supply to Use. We match water supply and quality to the appropriate use.

ER-1.3 Conservation and Sustainable Water Supply. We work with regional water providers and users to conserve water and ensure sustainable local water supplies as more frequent droughts reduce long term local and regional water availability.

ER-1.4 Supply-Demand Balance. We require that available water supply and demands be balanced.

ER-1.5 Water Resource Management. Environmental justice areas are prioritized as we coordinate with local agencies to protect water quality, prevent pollution, address existing contamination, and remediate contaminated surface water and groundwater.

ER-1.6 Urban Run-off Quantity. We encourage the use of low impact development strategies, including green infrastructure, to intercept run-off, slow the discharge rate, increase infiltration, and ultimately reduce discharge volumes to traditional storm drain systems.

ER-1.7 Urban Run-off Quality. We require the control and management of urban run-off, consistent with Regional Water Quality Control Board regulations.

ER-1.8 Wastewater Management. We require the management of wastewater discharge and collection consistent with waste discharge requirements adopted by the Regional Water Quality Control Board.

ER-2 Solid Waste & Recycling

Ontario provides solid waste collection and disposal services. For solid waste materials that cannot be reduced at the source, the City must take steps to maximize recycling and composting to ensure efficient collection and safe, environmentally sound disposal. If not recycled or composted, solid waste takes up landfill space that is very limited. Waste materials cause environmental impacts at each stage of their life cycle. There are environmental and economic impacts associated with the manufacture, transport, sale, disposal, and recycling of solid waste.

The treatment and disposal of solid waste traditionally creates significant unusable by-products and energy demands. The first approach to reduce the City's waste stream should be to reduce the generation of waste and find ways waste can be reused and recycled. New technologies and approaches can be used to reduce these impacts through recycling of construction, consumer,

green, and liquid waste and utilizing these waste products to generate renewable energy that reduce impacts on landfills. There is value in reducing the City's waste stream to as low as possible. Waste by-products that are channeled into a revenue stream reduce the demand upon facilities and reduce the use of non-renewable materials. The City cannot achieve its growth vision without decreasing waste by-products and freeing up capacity in landfills. Natural ecosystems efficiently breakdown waste by-products and are often more efficient, cost-effective and environmentally sensitive than man-made systems. While natural systems may be the optimal choice, the City recognizes the need to integrate both natural and manmade systems and processes to manage waste.

Goal ER-2 A cost effective, integrated waste management system that meets or exceeds state and federal recycling and waste diversion mandates.

Policies

ER-2.1 **Waste Diversion.** We shall meet or exceed AB 939 requirements.

ER-2.2 **Hazardous and Electronic Wastes.** We prohibit the disposal of hazardous and electronic waste into the municipal waste stream pursuant to state law.

ER-2.3 **Purchase Products Made from Recycled Materials.** We purchase recycled-content products where it is cost effective.

ER-3 Energy

Local conditions such as land use patterns, transportation options, and individual consumer consumption decisions impact energy use within the City. How we build neighborhoods, and construct and operate the buildings within them, can reduce per capita energy demand, increase conservation opportunities, and incorporate alternative energy sources. Finding ways to reduce the energy demands and encourage the development of renewable energy resources within the City can not only reduce our reliance upon non-renewable energy resources but can also make the City more ecologically and economically sustainable.

Goal ER-3 Cost-effective and reliable energy system sustained through a combination of low impact buildings, site and neighborhood energy conservation, and diverse sources of energy generation that collectively helps to minimize the region's carbon footprint.

Policies

ER-3.1 **Conservation Strategy.** We require conservation as the first strategy to be employed to meet applicable energy-saving standards.

ER-3.2 **Green Development- Communities.** We encourage the use of the LEED Neighborhood Development rating system, or similar mechanism, to guide the planning and development of all new communities.

- ER-3.3 Building and Site Design.** We require new construction to incorporate energy efficient building and site design strategies, which could include appropriate solar orientation, maximum use of natural daylight, passive solar, and natural ventilation.
- ER-3.4 Green Development- Public Buildings.** We require all new and substantially renovated City buildings in excess of 10,000 square feet achieve a LEED Silver Certification standard, as determined by the U.S. Green Building Council.
- ER-3.5 Fuel-Efficient and Alternative Energy Vehicles and Equipment.** We require purchase and use vehicles and equipment that are fuel efficient and meet or surpass state emissions requirements and/or use renewable sources of energy.
- ER-3.6 Generation- Renewable Sources.** We promote the use of renewable energy sources (e.g., solar, wind, biomass) in public and private sector development.

ER-4 Air Quality

Even though air quality is a regional issue, Ontario has the opportunity to demonstrate its leadership by striving for the healthiest air quality possible and reducing pollution sources within the City. Better indoor and outdoor air quality for Ontario and the air basin will improve the quality of life of residents, workers and visitors, decrease health care costs and make Ontario more prosperous by making the City a more desirable place to be. Most of the air pollution within Ontario is a result of the City's location within an inland valley where the predominant wind pattern flows from Orange and Los Angeles counties and backs up against the San Gabriel and San Bernardino Mountains.

The City is home to major transportation goods movement infrastructure – Ontario International Airport, major interstate highways, and three railway lines – which are major sources of air pollution. This combination of geographical factors and pollution sources contribute significantly to the region's failure to meet several federal and state standards.

The City of Ontario expects to double its population by 2050. We intend to develop strategies to minimize the air quality impacts of this growth. Of particular concern are the City's future impacts on Global Warming which is caused by an accumulation of Greenhouse Gases (GHG).

Goal ER-4 Improved indoor and outdoor air quality and reduced locally generated pollutant emissions.

Policies

- ER-4.1 Land Use.** We reduce GHG and other local pollutant emissions through compact, mixed use, and transit-oriented development and development that improves the regional jobs-housing balance.
- ER-4.2 Sensitive Land Uses.** We prohibit the future siting of sensitive land uses within the distances defined by the California Air Resources Board for specific source categories, without sufficient mitigation.

- ER-4.3 Greenhouse Gases (GHG) Emissions Reductions.** We will reduce GHG emissions in accordance with regional, state, and federal regulations.
- ER-4.4 Indoor Air Quality.** We will comply with State Green Building Codes relative to indoor air quality. We seek funding to improve indoor air quality for households with poor indoor air quality, with priority for lower income households in environmental justice areas.
- ER-4.5 Transportation.** We promote mass transit and non-motorized mobility options (walking, biking) to reduce air pollutant emissions.
- ER-4.6 Particulate Matter.** We support efforts to reduce particulate matter to meet State and Federal Clean Air Standards.
- ER-4.7 Other Agency Collaboration.** We collaborate with other agencies within the South Coast Air Basin to improve regional air quality at the emission source, with a particular focus on sources that affect environmental justice areas in Ontario.
- ER-4.8 Tree Planting.** We protect healthy trees within the City and plant new trees to increase carbon sequestration and help the regional/local air quality. We expand the tree canopy in environmental justice areas to enhance air quality and reduce the “heat island” effect.
- ER-4.9 New Localized Air Pollution Sources Near Existing Sensitive Receptors.** We require new developments to conduct a Health Risk Assessment for land uses that generate more than 100 trucks per day or 40 trucks per day by trucks operating transportation refrigeration units (TRU's) within 1,000 feet from sensitive land uses (California Health and Safety Code § 42705.5(a)(5)). If the health risk assessment determines the new development poses health hazards that increase the incremental cancer risk above the threshold established by the South Coast Air Quality Management District (AQMD), we will only approve permits upon the condition that adequate mitigation measures are proposed and implemented for potential impacts on the sensitive uses around the site and along the route within Ontario taken by the trucks to and from freeways. We require new developments that must perform a health risk assessment to conduct additional public outreach by sending notifications in multiple languages to all residents living within 500 feet, and encourage hosting a public meeting.

ER-5 Biological, Mineral & Agricultural Resources

Developed as the “Model Irrigation Colony,” Ontario has a rich agricultural heritage. The northern portion of the City was farmed with grapes, citrus, olives and other fruit tree crops. The southern portion of the City has been used predominantly for dairy farms for over half a century. Other types of agricultural uses include cultivated crops, fallow fields and plant nurseries. Until the mid-1990s, southern Ontario was part of the San Bernardino County Dairy Preserve. Some of the City’s dairy preserve properties are still under Williamson Act contracts. The City of Ontario adopted a right to farm ordinance which recognizes the right of agricultural operations to continue. However, increased environmental regulations are causing existing dairies to relocate

out of the region, resulting in a continued decline in the long-term viability of agricultural operations in southern Ontario.

Rare and/or endangered species that have the potential to occur in Ontario include the Delhi Sands Flower, the Loving Fly, and the San Bernardino Kangaroo Rat. Existing habitat for these species is of poor quality and/or is limited to isolated pockets. As the City further develops, there may be opportunities to integrate suitable habitat for sensitive species into new developments and/or participate in regional efforts for the conservation of high-quality habitat, thereby expanding and creating new habitat corridors.

There are no permitted mining operations in the City. According to the Department of Conservation, significant mineral resources within Ontario are limited to construction aggregate. Areas identified with potential mineral resources have been developed with urban uses and are not suitable for mineral resource extraction.

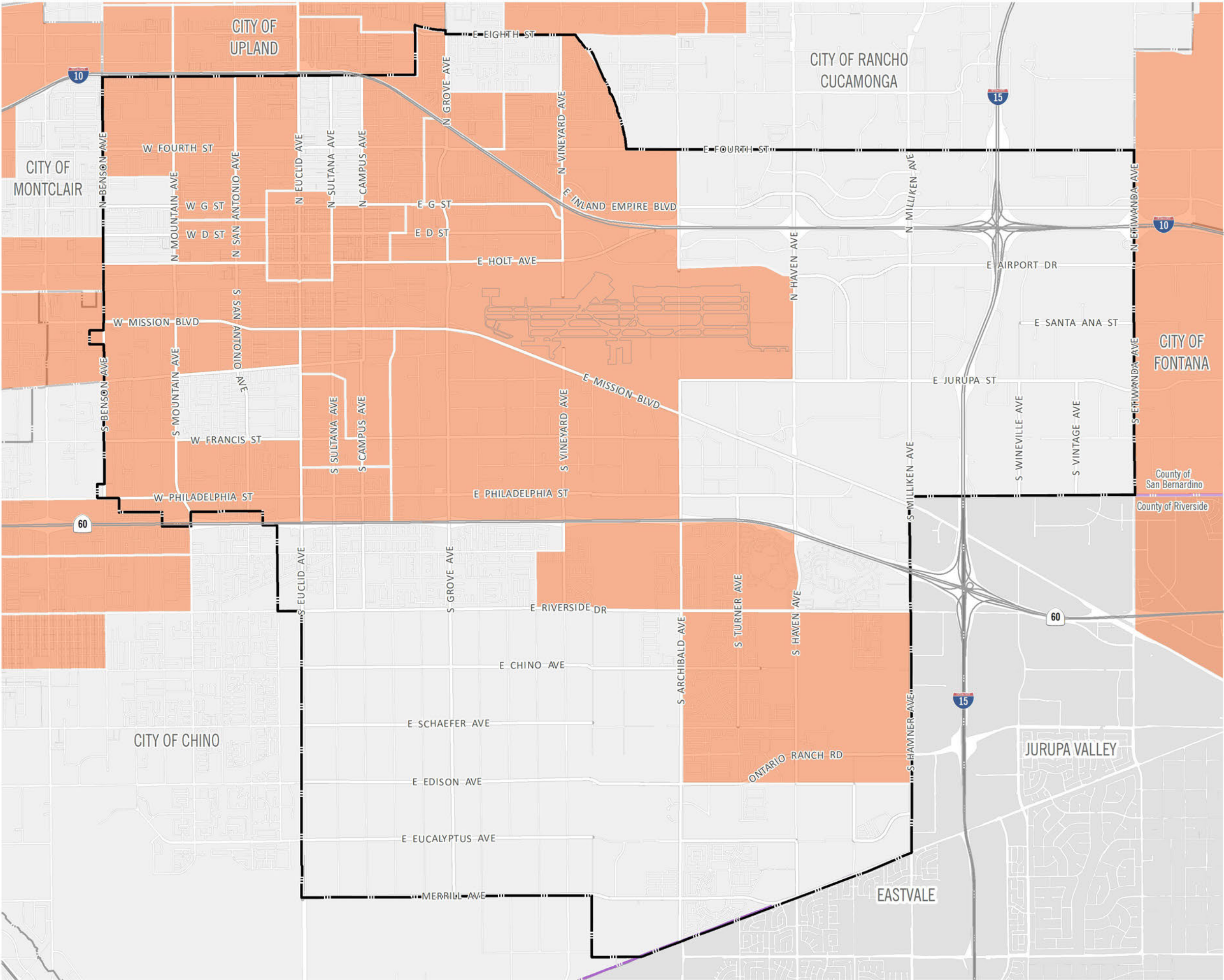
Goal ER-5 Protected high value habitat and farming and mineral resource extraction activities that are compatible with adjacent development.

Policies

- ER-5.1 Habitat Conservation Areas.** We support the protection of biological resources through the establishment, restoration, and conservation of high-quality habitat areas.
- ER-5.2 Entitlement and Permitting Process.** We comply with state and federal regulations regarding protected species.
- ER-5.3 Right to Farm.** We support the right of existing farms to continue their operations within the Ontario Ranch.
- ER-5.4 Transition of Farms.** We protect both existing farms and sensitive uses around them as agricultural areas transition to urban uses.
- ER-5.5 Mining Operations.** We prohibit future mining operations where the resource extraction activities are incompatible with existing or proposed adjacent land uses.

Exhibits: Figures

Figure ER-01
Environmental Justice Areas



- Environmental Justice Community
- Ontario City Boundary
- County Boundary

2050
THE ONTARIO PLAN

0 2,500 5,000 10,000 FT

Source: CES4 2021 Date: 5/9/22

COMMUNITY ECONOMICS ELEMENT

Introduction

The City's role is to create, maintain and grow economic value. Community shareholders, including property owners, tenants, families, and businesses have invested in Ontario and should expect a return from those investments, including financial and qualitative returns, such as: quality of life, job opportunities, potential for business growth, sense of safety, healthy environment, responsive government, amenities, and so much more – all of which is part of the Vision of being a “complete community.”

Their investments, in turn, generate returns for the community. A business investing in expansion creates new job opportunities for residents. A developer investing in new housing attracts new consumers for Ontario's retailers. These investments add municipal revenues, which the City reinvests in community facilities, public improvements, and services that improve the quality of life for Ontario residents, employees, and visitors.

Through the Community Economics Element policies, the City commits itself to protecting these investments. Generating better and higher returns gives Ontario a competitive advantage to attract even more investment. This new investment and reinvestment creates a self-sustaining cycle of prosperity.

Purpose

The Community Economics Element:

- Articulates our approach to developing and maintaining the community's economy and its relationship to the City's fiscal health.
- Creates a framework to attract investment in Ontario.
- Establishes our policies for economic development.

Principles

The City believes:

- A complete community will be better positioned to compete and attract investment.
- The wise and principled management of the public's finances is a primary responsibility of our City government.
- Actions taken by the City should take into consideration immediate and long term economic and fiscal implications.
- Unique, high-quality places differentiate Ontario and thereby attract additional investment.

Goals & Policies

CE-1 Complete Community

The concept of creating, maintaining, and growing economic value within the City underlies the Ontario Vision. While economic value is often measured in monetary terms – property values, taxable sales, and equity growth, we require decision-making that also reflects qualitative value. We express this qualitative value as the concept of “complete community.” A complete community provides housing, jobs, education, shopping and services, culture, and recreation for people at all ages and socioeconomic levels. Most importantly, a complete community provides opportunities for those residents and businesses willing to improve themselves and create wealth and invest in the community.

This section establishes policies that incorporate this concept into land use planning, regulations, City governance, and operations.

Goal CE-1 A complete community that provides for all incomes and stages of life.

Policies

CE-1.1 Jobs-Housing Balance. We pursue improvement to the Inland Empire’s balance between jobs and housing by promoting job growth that reduces the regional economy’s reliance on out-commuting.

CE-1.2 Jobs and Workforce Skills. We use our economic development resources to:

1. attract jobs suited for the skills and education of current and future City residents;
2. work with regional partners to provide opportunities for the labor force to improve its skills and education; and
3. attract businesses that increase Ontario’s stake and participation in growing sectors of the regional and global economy.

(Link to Social Resources Policy SR-2.2)

CE-1.3 Regional Approach to Workforce Development. We work with our partners to provide workforce training and development services throughout the region, recognizing that Ontario employers rely on workers living outside of the City.

CE-1.4 Business Retention and Expansion. We continuously improve two-way communication with the Ontario business community and emphasize customer service to existing businesses as part of our competitive advantage.

CE-1.5 Business Attraction. We proactively attract new and expanding businesses to Ontario in order to increase the City’s share of growing sectors of the regional and global economy.

- CE-1.6 Diversity of Housing.** We collaborate with residents, housing providers, and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to encourage the development of housing supportive of our efforts to attract business in growing sectors of the community while being respectful of existing viable uses.
- CE-1.7 Retail Goods and Services.** We seek to ensure a mix of retail businesses that provide the full continuum of goods and services for the community.
- CE-1.8 Regional Attraction.** We encourage the development and programming of regional, cultural, and entertainment destinations in Ontario.
- CE-1.9 Regional Leadership.** We provide leadership for public, quasi-public, and private-sector partners that help Ontario and its residents and businesses realize our goals and achieve our Vision.
- CE-1.10 Life-Long Education.** We work with our partners who provide life-long learning to ensure that our residents and workforce have access to education at all stages of life.
- CE-1.11 Socioeconomic Trends.** We continuously monitor, plan for, and respond to changing socioeconomic trends.
- CE-1.12 Circulation.** We continuously plan and improve public transit and non-vehicular circulation for the mobility of all, including those with limited or no access to private automobiles. (Link to Mobility Element *Public Transit* Section)
- CE-1.13 Safety and Security.** We invest in public safety and communicate our successes because the perception and reality of safety and security are necessary prerequisites for private investment and economic growth.

CE-2 Placemaking

The City's land use plans and policies seek to create distinctive and high-quality places. Such places add value and thereby attract additional investment. The City of Ontario focuses on creating a range of places where people want to live, work, and visit. This section establishes policies that acknowledge the value of place in the decision-making process.

Goal CE-2 A City of distinctive neighborhoods, districts, corridors, and centers where people choose to be.

Policies

- CE-2.1 Development Projects.** We require new development and redevelopment to create unique, high-quality places that add value to the community. (Link to Community Design Element)

- CE-2.2 Development Review.** We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional, and sustainable places that will compete well with their competition within the region.
- CE-2.3 Interim Development.** We require interim development that does not reflect the long-term Vision, be limited in scale of development so that the investment can be sufficiently amortized to make Vision-compatible redevelopment financially feasible.
- CE-2.4 Protection of Investment.** We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.
- CE-2.5 Private Maintenance.** We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values. (Link to Community Design Element Policy CD-5.1)
- CE-2.6 Public Maintenance.** We require the establishment and operation of maintenance districts or other vehicles to fund the long-term operation and maintenance of the public realm whether on private land, in rights-of-way, or on publicly-owned property. (Link to Community Design Element Policy CD-5.1)

CE-3 Fiscal Decision-making

Every Municipal decision affects the revenues and expenditures of city government. New zoning regulations, for example, can necessitate staff time to document non-conformance created by new standards. Street widening can encourage speeding, requiring increased patrolling and enforcement.

Good decision-making requires that fiscal impacts be reasonably projected and be part of the deliberative process. The City desires to continuously improve its understanding of the direct and indirect fiscal impacts of its decisions and include them in its decision-making process.

This section establishes policies that minimize negative fiscal impacts and incorporate long-term fiscal thinking into decisions.

Goal CE-3 Decision-making deliberations that incorporate the full short-term and long-term economic and fiscal implications of proposed City Council actions.

Policies

- CE-3.1 Fiscal Impact Disclosure.** We require requests for City Council action to disclose the full fiscal impacts, including direct and indirect costs.
- CE-3.2 General Plan Amendments.** We require those proposing General Plan amendments to disclose reasonably foreseeable impacts through a fiscal analysis.

- CE-3.3 Long-Term Funding Disclosure.** We require those requesting City support or funding for projects or programs to disclose if and how they can be continued without further City support.
- CE-3.4 Improving Fiscal Decision-Making.** We periodically assess the accuracy of projections for staff time and City resources and use the assessment results to improve our fiscal decision-making process.
- CE-3.5 Sustainable Development.** We recognize impacts to municipal finances as an element of sustainability, and we require claims of sustainability to assess fiscal impacts.
- CE-3.6 Fully Funded Liability.** We require long-term liabilities, such as retiree medical benefits, employee accrued leave balances, and self-insured liability claims to be fully funded to ensure sound, long-term fiscal health.
- CE-3.7 Programmatically Balanced Budget.** We require that the annual budget include appropriations allocated in a manner to meet the goal of the programmatically balanced budget.
- CE-3.8 Budget Margins.** We require that the adopted budget for revenue and expenditures reflect sufficient budget margins to minimize negative impacts to City services due to economic uncertainties.
- CE-3.9 Complete Comparative Context.** We require that our annual budget process provide the complete comparative context for proposed new and increased funding so decision makers can fully understand the trade-offs among budget choices.

SAFETY ELEMENT

Introduction

The City is committed to protecting life, property, and commerce from disruptions and loss associated from human-caused and natural hazards, disasters, and other threats to public health and safety. Natural hazards that could impact Ontario include earthquakes, flooding, fire, wind, and climate-related hazards. Man-made hazards include hazardous materials, noise, and crime. A clear management framework for emergency services is essential to adequately plan for and respond to these hazards.

Purpose

The Safety Element:

- Identifies potential hazards.
- Provides background on the history of hazards and the likelihood of future changes to these hazards.
- Provides policies that increase resilience of residents, businesses, workers, and visitors.
- Provides policies to reduce the level of property loss due to a potential disaster.
- Provides a framework for emergency management.

Principles

The City believes:

- It is the role of government to minimize exposure to natural and human-caused hazards.
- A safe and healthy environment is necessary to build and maintain a sustainable, resilient, and prosperous Ontario.
- Reduction in the loss of life, injury, private property damage, infrastructure damage, economic losses and social dislocation can be achieved through planning, preparedness, and response.
- Interdepartmental and inter-jurisdictional coordination and collaboration are necessary to be resilient to everyday emergencies and major disasters.

Goals & Policies

S-1 Seismic & Geologic Hazards

Ontario is susceptible to earthquakes, liquefaction, and subsidence caused by rapid withdrawal of groundwater. For the City to thrive and continue to attract investment, residents, business owners, and investors need assurance that the City is prepared for and will effectively respond to seismic and geologic hazards.

Goal S-1 **Minimized risk of injury, loss of life, property damage, and economic and social disruption caused by earthquake-induced and other geologic hazards.**

Policies

- S-1.1** **Implementation of Regulations and Standards.** We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.
- S-1.2** **Entitlement and Permitting Process.** We follow state guidelines and the California Building Code to determine when development proposals must conduct geotechnical and geological investigations.
- S-1.3** **Continual Update of Technical Information.** We maintain up-to-date California Geological Survey seismic hazard maps.
- S-1.4** **Seismically Vulnerable Structures.** We conform to state law regarding unreinforced masonry structures and coordinate with not-for-profits to facilitate seismic retrofits in environmental justice areas and for low-income households.

S-2 Flood Hazards

Flood hazards to the area can be classified as flooding down natural channels and flooding due to capacity constraints of the storm drain system. A 100-year flood or larger event is anticipated to result in extensive property damage and displacement of hundreds of households. Catastrophic failure of any water retaining structure due to storm induced flood or dam failure inundation, has the potential to cause considerable damage in Ontario.

Goal S-2 **Minimized risk of injury, loss of life, property damage and economic and social disruption caused by flooding and inundation hazards.**

Policies

- S-2.1** **Entitlement and Permitting Process.** We require hydrological studies prepared by a state-certified engineer when new development is located in a 100-year or 500-year floodplain to assess the impact that the new development will have on the flooding potential of existing development down-gradient.

- S-2.2 Floodplain Mapping.** We require any new development partially or entirely in 100-year flood zones to provide detailed floodplain mapping for 100- and 200-year storm events as part of the development approval process.
- S-2.3 Facilities that Use Hazardous Materials.** We comply with state and federal law and do not permit facilities using, storing, or otherwise involved with substantial quantities of onsite hazardous materials to be located in the 100-year flood zone or 500-year flood zone unless all standards of elevation, floodproofing, and storage have been implemented to the satisfaction of the Building Department.
- S-2.4 Prohibited Land Uses.** We prohibit the development of new essential and critical facilities in the 100-year floodplain and discourage the development of new essential and critical facilities in the 500-year floodplain unless all standards of elevation and flood proofing demonstrate that a facility can be safe and operational during a flood event, implemented to the satisfaction of the Building Department.
- S-2.5 Stormwater Management.** We maintain the storm drain system to convey a 100-year storm, when feasible, and encourage environmental site design practices to minimize flooding and increase groundwater recharge, including natural drainage, green infrastructure, and permeable ground surfaces. (Link to Environmental Resources Element)
- S-2.6 Use of Flood Control Facilities.** We encourage joint use of flood control facilities as open space or other types of recreational facilities.
- S-2.7 Collaboration Between Agencies.** Collaborate with the San Bernardino County Flood Control District and other state and federal agencies to maintain flood-control infrastructure to minimize flood damage.

S-3 Fire & Rescue Hazards

The City of Ontario seeks to reduce the threat of fire hazards to life, property and economic viability by providing fire, rescue, emergency medical, and specialty emergency response services. Due to the local topography and nearby Cajon Pass, Santa Ana Winds by far pose the greatest fire hazard to the City. The Santa Ana winds pose a continual fire conflagration hazard to any dense area of the City, with an increased risk to older portions of Ontario. Ontario's commercial and industrial facilities increase the possibility of fires involving hazardous materials, which could affect nearby residential areas. Ontario is also surrounded and bisected by major transportation networks and pipeline transfer systems which add further risk.

- Goal S-3** **Reduced risk of death, injury, property damage and economic loss due to fires, accidents and normal everyday occurrences through prompt and capable emergency response.**

Policies

- S-3.1 Prevention Services.** We proactively mitigate or reduce the negative effects of fire, hazardous materials release, and structural collapse by implementing the regularly adopted California Fire Code and California Building Code.
- S-3.2 Community Outreach.** We provide education to local schools and community groups to promote personal and public safety.
- S-3.3 Fire and Emergency Medical Services.** We maintain sufficient fire stations, equipment and staffing to respond effectively to emergencies and meet the needs of the community and state requirements.
- S-3.4 Special Team Services.** We maintain effective special rescue services.
- S-3.5 Emergency Notifications.** We maintain a public alert notification system that efficiently conveys information about imminent, developing, ongoing, and concluding emergency events to residents and visitors, working with network providers that translate information into other languages.
- S-3.6 Interagency Cooperation.** In order to back up and supplement our capabilities to respond to emergencies, we participate in the California Fire Rescue and Mutual Aid Plan.
- S-3.7 Water Supply and System Redundancy.** We monitor our water system to manage and ensure adequate firefighting water supplies.
- S-3.8 Fire Prevention through Environmental Design.** We require new development to incorporate fire prevention consideration in the design of streetscapes, sites, open spaces, and buildings. (Link to Community Design Element)
- S-3.9 Resource Allocation.** We analyze fire data to evaluate the effectiveness of our fire prevention and reduction strategies and allocate resources accordingly.

S-4 Noise Hazards

Physical health, psychological wellbeing, social cohesion, property values and economic productivity can all be affected by excessive amounts of noise. Ontario has many mobile and stationary sources of noise, impacts from them must be considered in development decisions.

Goal S-4 An environment where noise does not adversely affect the public's health, safety, and welfare.

Policies

- S-4.1 Noise Mitigation.** We utilize the City's Noise Ordinance, building codes, and subdivision and development codes to mitigate noise impacts.

- S-4.2 Coordination with Transportation Authorities.** We collaborate with airport owners, FAA, Caltrans, SBCTA, SCAG, neighboring jurisdictions, and other transportation providers in the preparation and maintenance of, and updates to transportation-related plans to minimize noise impacts and provide appropriate mitigation measures.
- S-4.3 Airport Noise Mitigation.** We aggressively pursue funding and utilize programs to reduce effects of aircraft noise in impacted areas of our community.
- S-4.4 Truck Traffic.** We manage truck traffic to minimize noise impacts on sensitive land uses.
- S-4.5 Roadway Design.** We design streets and highways to minimize noise impacts.
- S-4.6 Airport Noise Compatibility.** We utilize information from Airport Land Use Compatibility Plans to prevent the construction of new noise-sensitive land uses within airport noise impact zones.
- S-4.7 Rail Noise Mitigation.** We require residential and mixed use development of vibration-sensitive uses in areas within 200 feet of rail to evaluate for indoor vibration levels and mitigate any exceedances of the Federal Transit Administration vibration-annoyance criteria.

S-5 Wind-Related Hazards

Severe windstorms can pose a significant risk to property and life in the region by creating conditions that disrupt essential systems such as public utilities, telecommunications, and transportation routes. High winds, including Santa Ana winds, can cause damage to homes, businesses, landscaping, public property, and utilities, as well as pose threats to public safety through the accelerated spread of a fire. The alluvial sand that underlies the majority of Ontario is very susceptible to erosion, and strong winds cause this sand to impact property, air quality, and visibility.

Goal S-5 Minimize the risk of injury, property damage, and economic loss resulting from windstorms and wind-related hazards.

Policies

- S-5.1 Dust Control Measures.** We require the implementation of Best Management Practices for dust control at all excavation and grading projects.
- S-5.2 Grading in High Winds.** We prohibit excavation and grading during strong wind conditions, as defined by the Building Code.
- S-5.3 Public Safety Power Shutoffs.** We coordinate with utility companies to minimize service interruptions, such as Public Safety Power Shutoffs, before, during, and after windstorms and wind-related hazards.

S-6 Hazardous Materials & Waste

Ontario's role as a transportation hub and manufacturing center is key to the City's economy. However, these uses make the city susceptible to spills of toxic materials and vulnerable to the byproducts generated in industrial areas. Earthquakes, fires, floods, and strong winds all increase the potential of a hazardous materials release, contaminating the land, air, and water. Of particular concern is exposure to airborne pollutants and groundwater contamination.

Goal S-6 Reduced potential for hazardous materials exposure and contamination.

Policies

- S-6.1 Disclosure and Notification.** We enforce disclosure laws that require all users, producers, and transporters of hazardous materials and wastes to clearly identify the materials that they store, use, or transport.
- S-6.2 Response to Hazardous Materials Releases.** We respond to hazardous materials incidents and coordinate these services with other jurisdictions.
- S-6.3 Safer Alternatives.** We minimize our use of pesticides and other hazardous materials by choosing non-toxic alternatives that do not pose a threat to the environment, especially when it could affect public park facilities and open spaces.
- S-6.4 Safe Storage and Maintenance Practices.** We require that the users of hazardous materials be adequately prepared to prevent and mitigate hazardous materials releases.
- S-6.5 Location of Hazardous Material Facilities.** We regulate facilities that will be involved in the production, use, storage, or disposal of hazardous materials, pursuant to federal, state, county, and local regulations, so that impacts to the environment and sensitive land uses are mitigated. We prohibit new hazardous waste facilities in close proximity to sensitive land uses and environmental justice areas.
- S-6.6 Location of Sensitive Land Uses.** We prohibit new sensitive land uses from locating within airport safety zones and near existing sites that use, store, or generate large quantities of hazardous materials. ([Link to Land Use Element](#))
- S-6.7 Household Hazardous Waste.** We support the proper disposal of household hazardous substances.
- S-6.8 Mitigation and Remediation of Groundwater Contamination.** We actively participate in local and regional efforts directed at both mitigating environmental exposure to contaminated groundwater and taking action to clean up contaminated groundwater once exposure occurs.
- S-6.9 Remediation of Methane.** We require development to assess and mitigate the presence of methane, per regulatory standards and guidelines.

S-7 Law Enforcement

Maintaining health and safety in Ontario's residential neighborhoods and commercial and industrial districts contributes to the City's prosperity. Ontario achieves this through a proactive approach that includes prevention strategies that engage the community.

Goal S-7 Residential neighborhoods, commercial areas, and industrial districts that are kept safe through a multi-faceted approach of prevention, suppression, and community involvement in public safety.

Policies

- S-7.1 Police Unit Response.** We respond to calls for service in a timely manner.
- S-7.2 Community Oriented Problem Solving (C.O.P.S.).** We support and maintain the mission of COPS to identify and resolve community problems.
- S-7.3 Prevention Services.** We provide crime prevention programs targeted to youth, parents, seniors, businesses, and neighborhoods.
- S-7.4 Crime Prevention through Environmental Design (CPTED).** We require new development to incorporate CPTED in the design of streetscapes, sites, open spaces, and buildings.
- S-7.5 Interdepartmental Coordination.** We utilize all City departments to help reduce crime and promote public safety.
- S-7.6 Partnerships.** We partner with other local, state, and federal law enforcement agencies and private security providers to enhance public safety services in Ontario.
- S-7.7 Resource Allocation.** We analyze crime data to evaluate the effectiveness of crime prevention and reduction strategies and allocate resources accordingly.
- S-7.8 Social Services.** We support behavioral health and social services as part of the public safety solution.

S-8 Emergency Management

Ontario is susceptible to a variety of natural and human-caused hazards including earthquakes, floods, fires, windstorms, and hazardous materials release. While evacuation routes are located throughout the city, some neighborhoods may have evacuation constraints. Ontario seeks to reduce risks and increase resilience by building collaborative preparedness, response, and recovery programs.

Goal S-8 Disaster resilient, prepared community through effective emergency/disaster preparedness, response, mitigation, and recovery.

Policies

- S-8.1 State and Federal Mandates.** We maintain emergency management programs that meet the requirements of the State of California Standardized Emergency Management System (SEMS) and the National Incident Management System (NIMS).
- S-8.2 Emergency Management Plans.** We maintain, update, and adopt the Emergency Operations Plan (EOP) and incorporate, by reference the City’s Hazard Mitigation Plan (HMP).
- S-8.3 Emergency/Disaster Training Exercises.** We conduct training and exercises to prepare for and evaluate emergency/disaster response and recovery procedures.
- S-8.4 Interagency Collaboration.** We maintain partnerships, including automatic aid agreements, with fire protection, police and sheriff departments, and emergency management agencies in San Bernardino and Riverside County to strengthen emergency response.
- S-8.5 Interdepartmental Coordination.** We utilize all City departments to help support emergency/disaster preparedness, response, mitigation, and recovery.
- S-8.6 Community Outreach.** We provide education to the community to promote personal, family, and community emergency preparedness to both natural and human-generated hazards.
- S-8.7 Extreme Heat and Air Quality.** We work to ensure that all community members are informed about and have access to community cooling centers and clean air centers during extreme heat events or wildfires, with a focus on serving environmental justice communities. We support the development of extreme heat emergency response policies and practices to address these critical health risks in the community.
- S-8.8 Regional Partnerships for Climate Adaptation.** We partner with local governments in San Bernardino County, Riverside County, and Inland Southern California Climate Collaborative to develop regional climate change adaptation strategies and programs.
- S-8.9 Backup Power in Critical Facilities.** We require backup power be maintained in critical facilities. We encourage backup power solutions that include renewable energy components.

S-9 Energy Resiliency

Maintaining an efficient and resilient energy supply for Ontario's residents and businesses helps ensure continuity of economic drivers and city operations. Ontario attains energy resiliency through creating local, renewable energy and battery storage systems, constructing new buildings and retrofitting existing ones to use less electricity and natural gas.

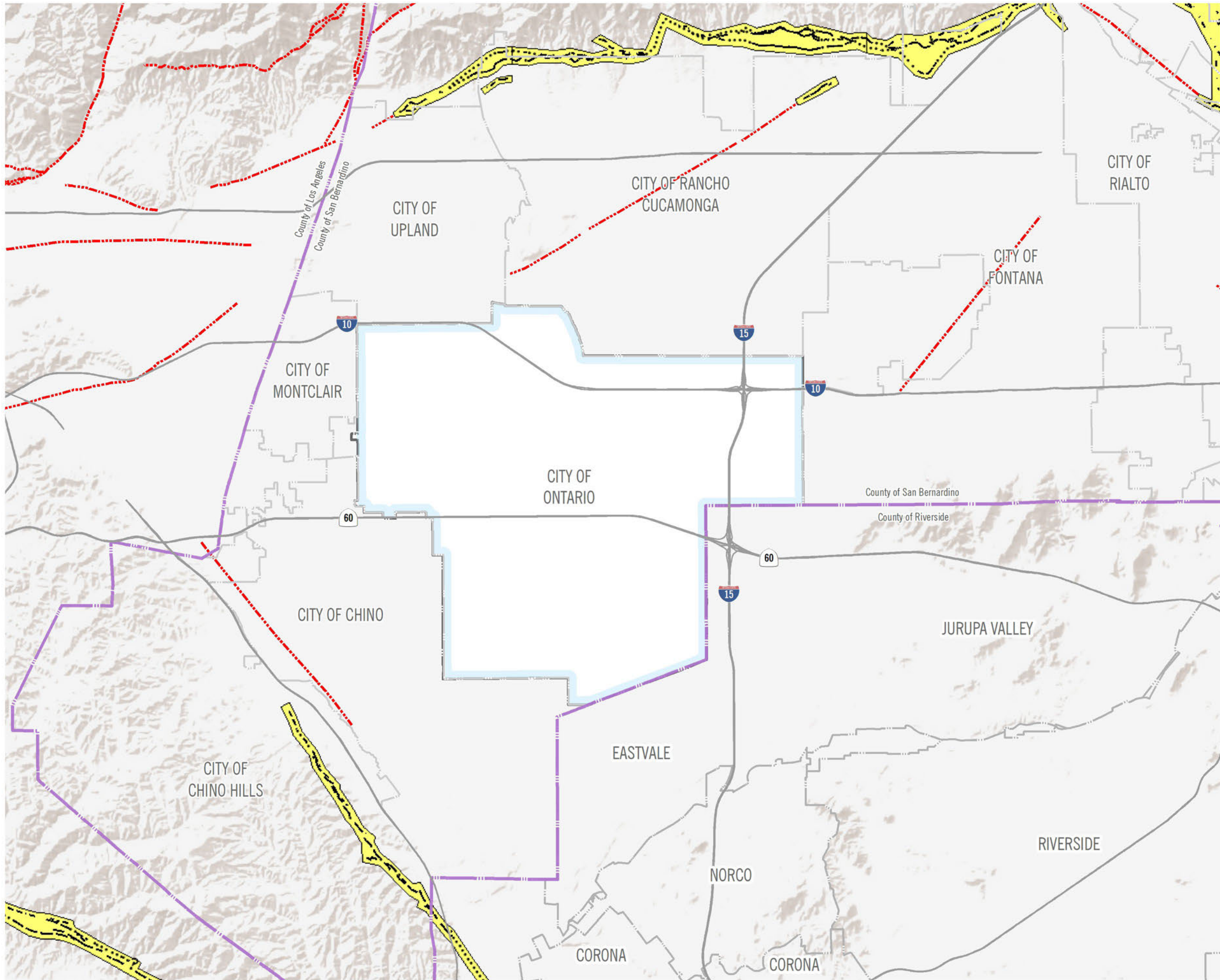
Goal S-9 **Incorporate energy efficient practices and renewable energy systems to improve air quality, comfort, and energy reliability during temporary power outages.**

S-9.1 **Solar Energy.** We support and may incentivize the installation of residential and commercial solar panels and battery storage systems that can provide electricity during power outages.

S-9.2 **Renewable Energy.** Renovate existing city-owned facilities and plan future facilities to include renewable energy generation capacity and battery storage as part of an effort to make public facilities and services greener and more resilient to power outages.







S-9.3 **Energy Efficiency Retrofits.** We support and may incentivize retrofits to residential and commercial buildings that improve energy efficiency and insulation from extreme temperatures, giving priority towards low-income applicants.


Exhibits: Figures



SAFETY

Figure S-01
Regional Faults & Fault Zones

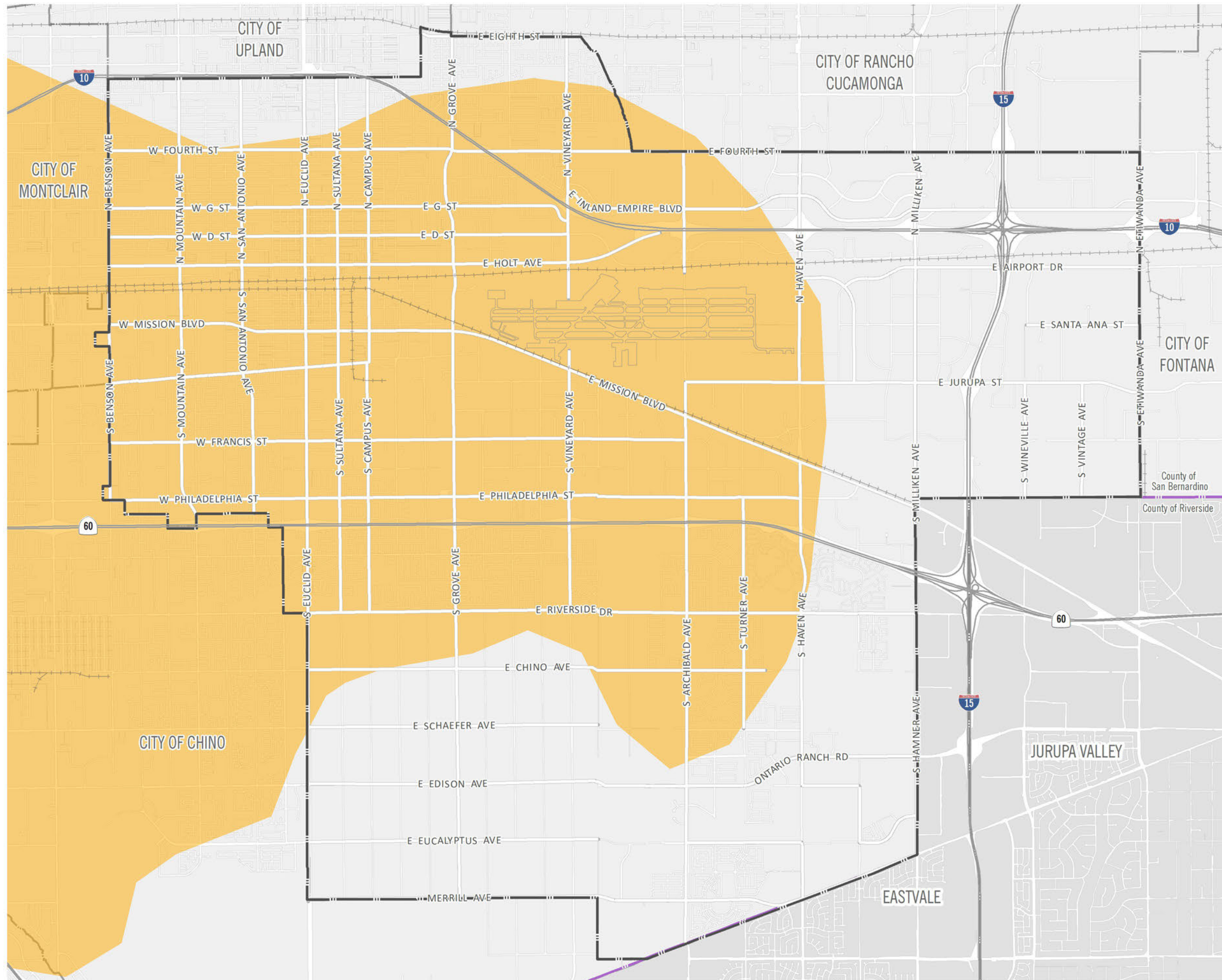
-  Ontario City Boundary
-  County Boundary
- Alquist Priolo Fault Traces
 -  Inferred
 -  Concealed
-  Alquist Priolo Fault Zones
-  Regional Faults (USGS)



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THE ONTARIO PLAN





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Source: USGS 2021 Date: 5/9/22



SAFETY

Figure S-02
Subsidence Zones

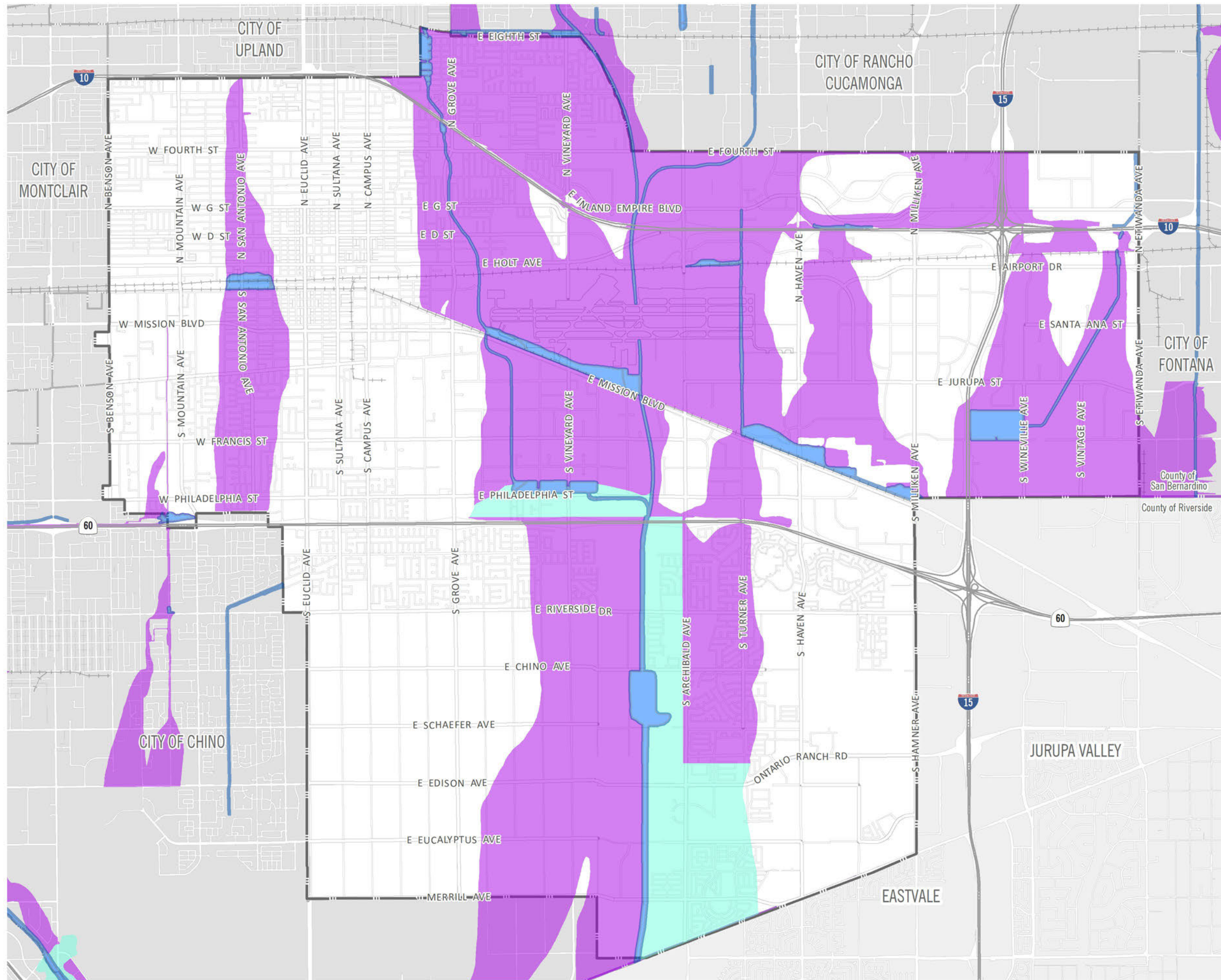
-  Ontario City Boundary
-  County Boundary
-  Subsidence Due to Groundwater Pumping
-  Rail Network



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Source: The City of Ontario 2021 Date: 5/9/22



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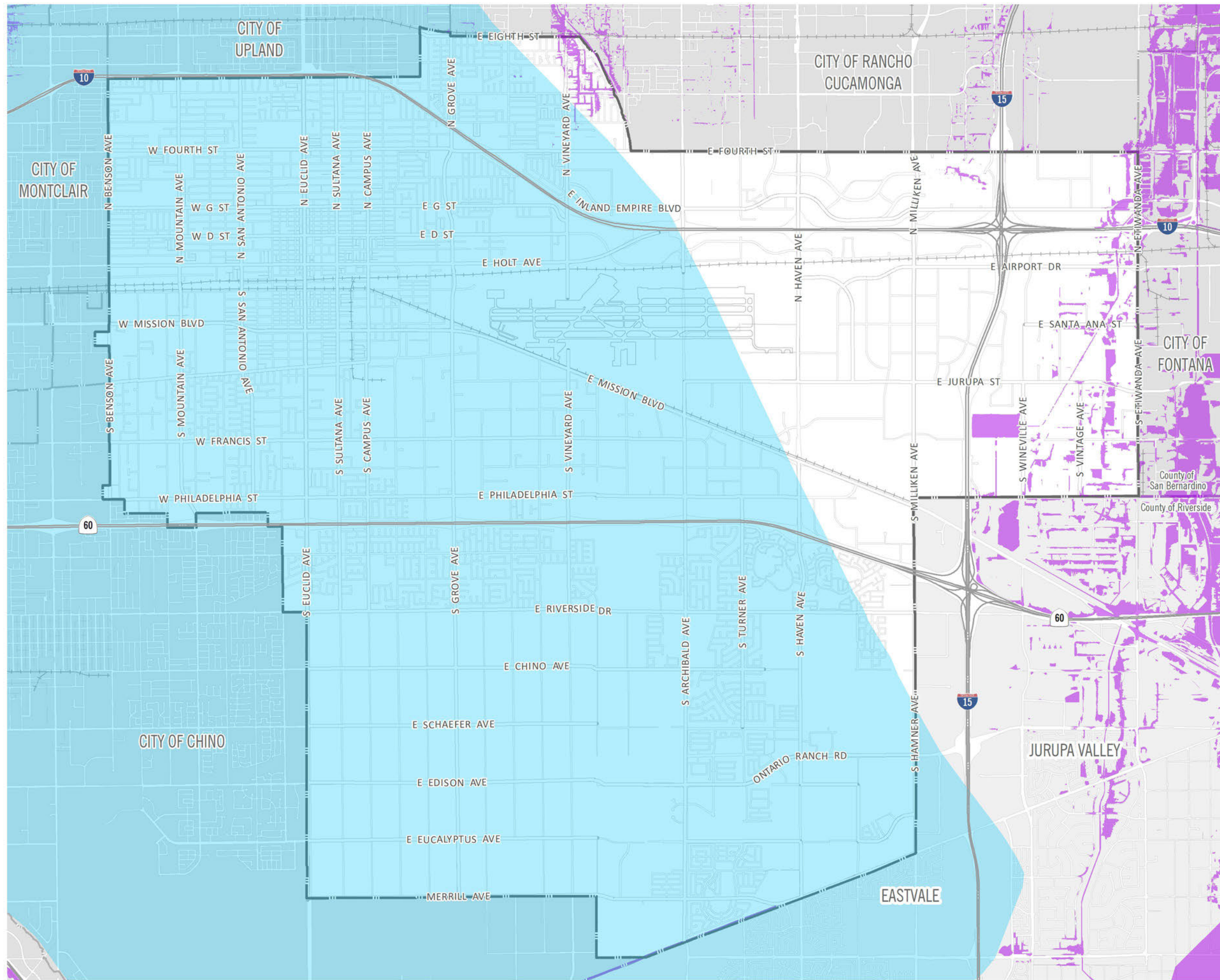
Figure S-03
Flood Hazard Zones

- Ontario City Boundary
- County Boundary
- Rail Network
- Flood Zone**
 - FEMA 100-Year Floodplain
 - FEMA 500-Year Floodplain
 - Area with Reduced Flood Risk Due to Levee



Source: The City of Ontario 2021 Date: 5/9/22

Figure S-04
Dam Inundation Zones

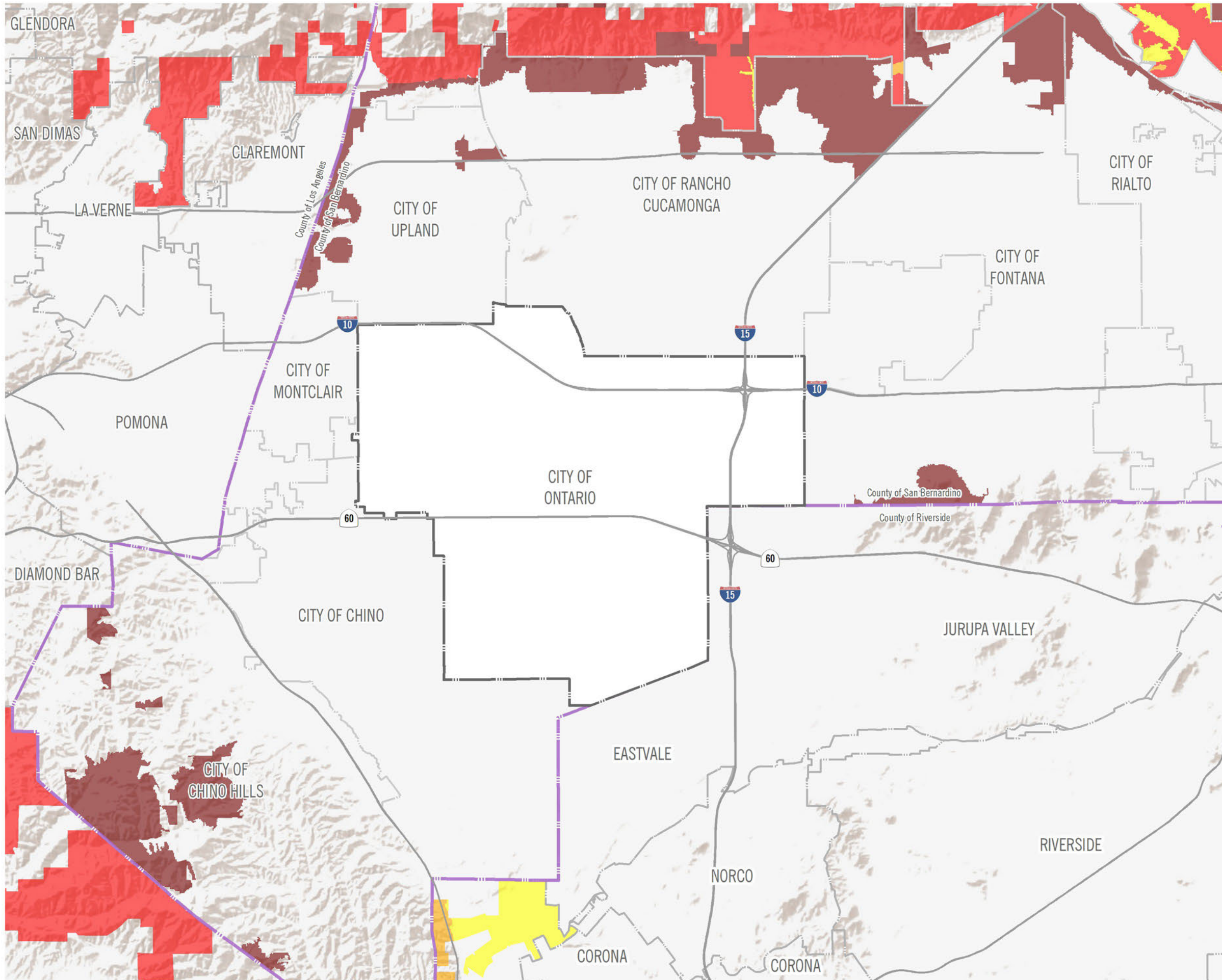


- Potential Inundation from San Antonio Dam
- Potential Inundation from Debris Basins
- Ontario City Boundary
- County Boundary
- Rail Network

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THE ONTARIO PLAN

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Source: DWR 2021 Date: 5/9/22



SAFETY

Figure S-05
Fire Hazard Severity Zones

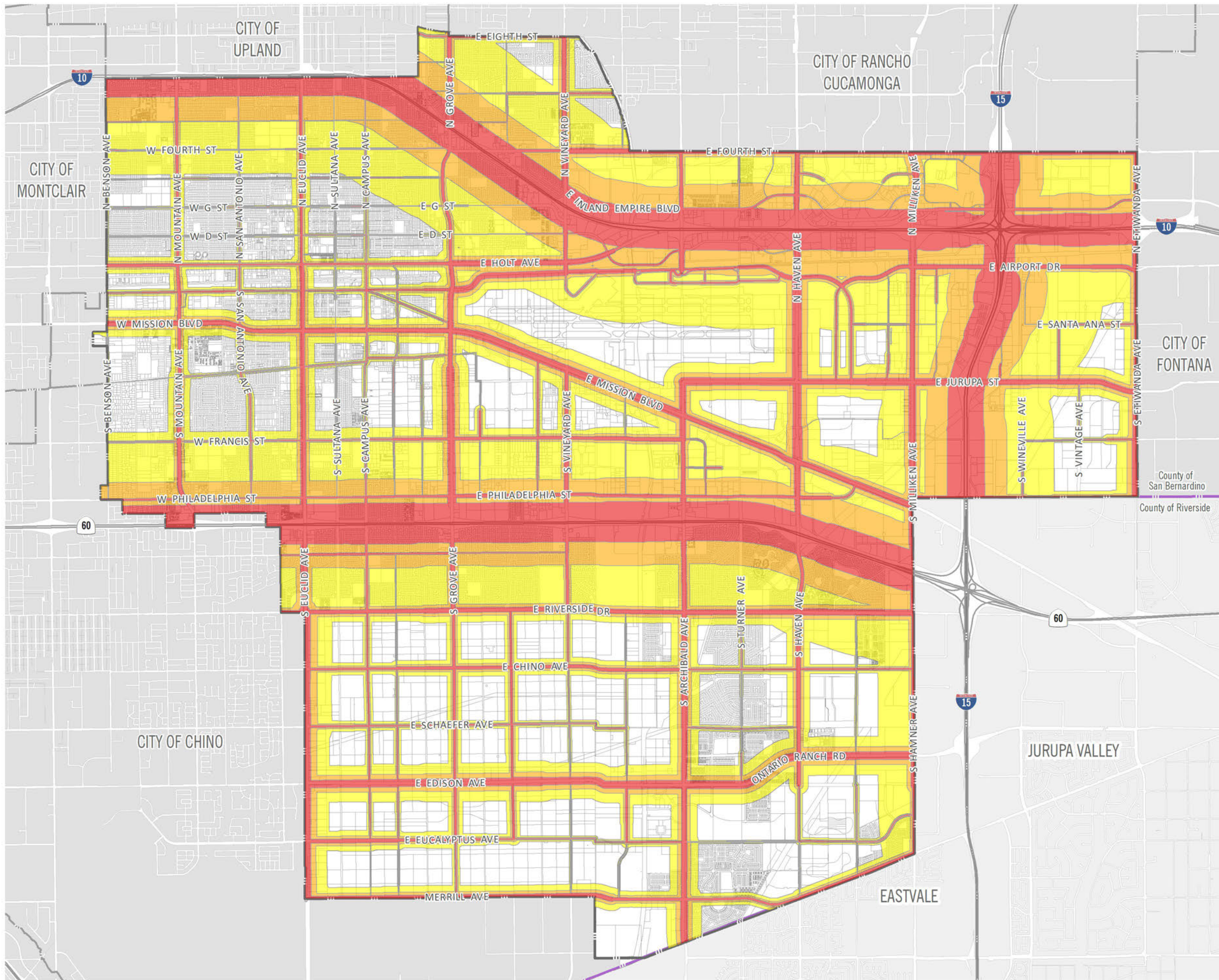
- Ontario City Boundary
- County Boundary
- Fire Hazard Severity Zone in Local Responsibility Area
- Hazard Class
 - Very High
- Fire Hazard Severity Zone in State Responsibility Area
- Hazard Class
 - Very High
 - High
 - Moderate

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THE ONTARIO PLAN

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Source: CalFire 2007

Date: 5/9/22



NOISE

Figure S-06a
Future Traffic Noise Contours

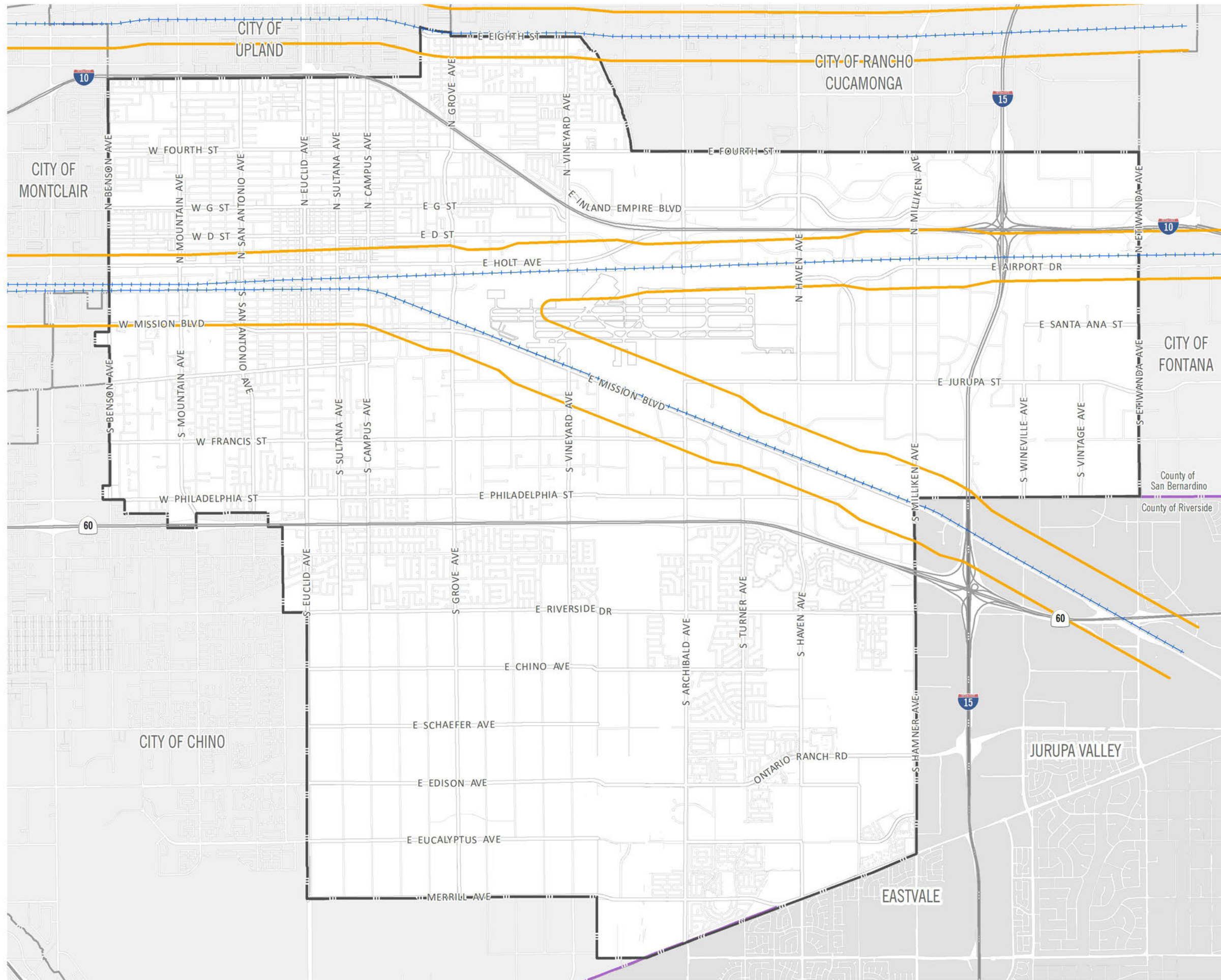
Future Noise Contours

- 60-65 dBA CNEL
- 65-70 dBA CNEL
- 70+ dBA CNEL
- Ontario City Boundary
- County Boundary

2050
THE ONTARIO PLAN

0 2,500 5,000 10,000 FT


Source: Fehr & Peers 2021; PlaceWorks 2021 Date: 5/9/22



SAFETY

Figure S-06b
 Rail Noise Contour

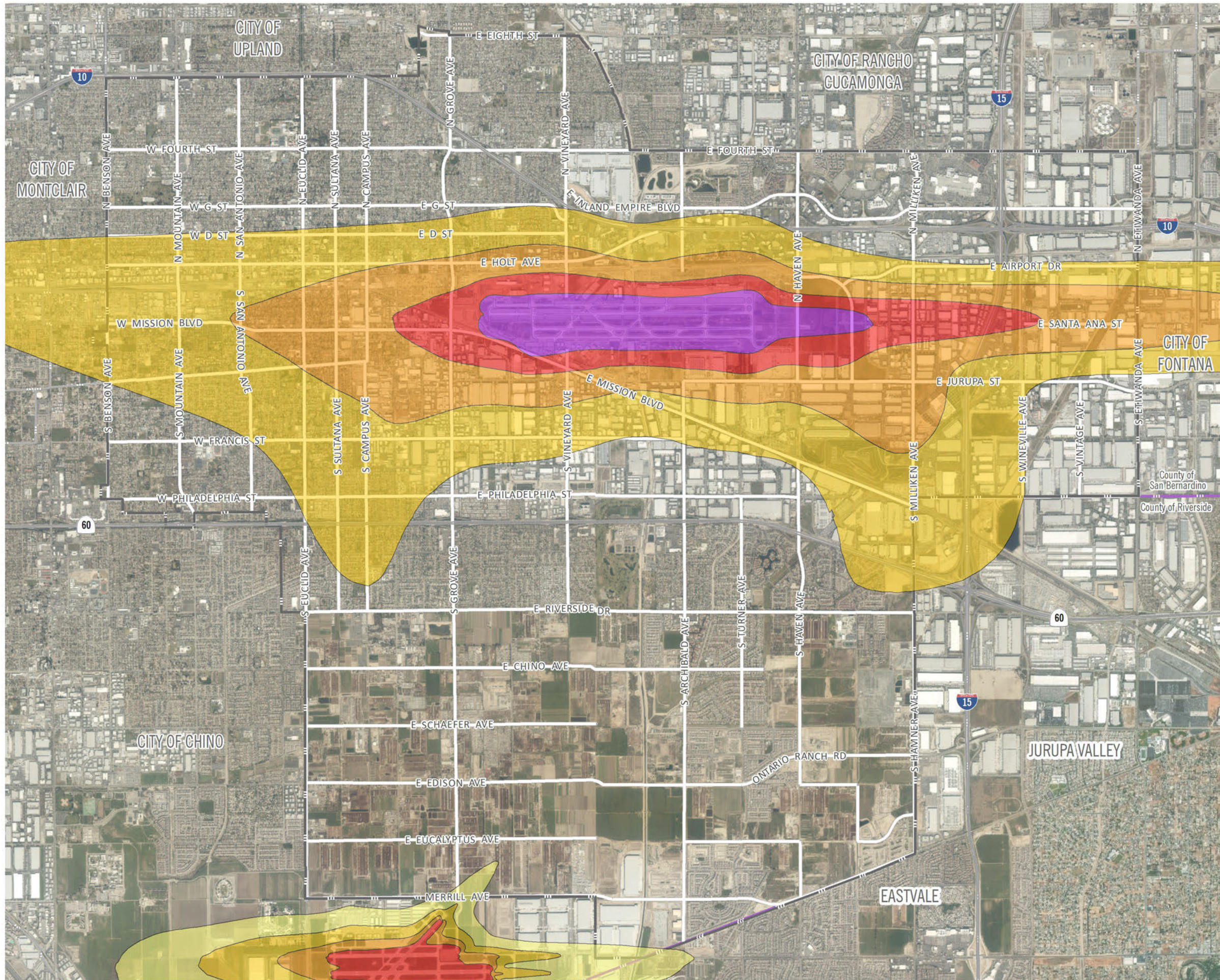
-  Rail Network
-  Future 65 dBA CNEL
-  Ontario City Boundary
-  County Boundary



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THE ONTARIO PLAN

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Source: The City of Ontario 2009 Date: 5/9/22



SAFETY

Figure S-06c
Airport Noise Contours

- Ontario City Boundary
- County Boundary
- Ontario Airport Noise Contours**
 - 60-65 dBA CNEL
 - 65-70 dBA CNEL
 - 70-75 dBA CNEL
 - 75+ dBA CNEL
- Chino Airport Noise Contours**
 - 55-60 dBA CNEL
 - 60-65 dBA CNEL
 - 65-70 dBA CNEL
 - 70+ dBA CNEL

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THE ONTARIO PLAN

Source: The City of Ontario 2020, 2022;
Riverside County ALUCP 2008 Date: 5/9/22

SAFETY

Figure S-07
Hazardous Material Cleanup Sites



- Ontario City Boundary
- County Boundary
- Rail Network

Hazardous Material Cleanup Sites (Open Cases)

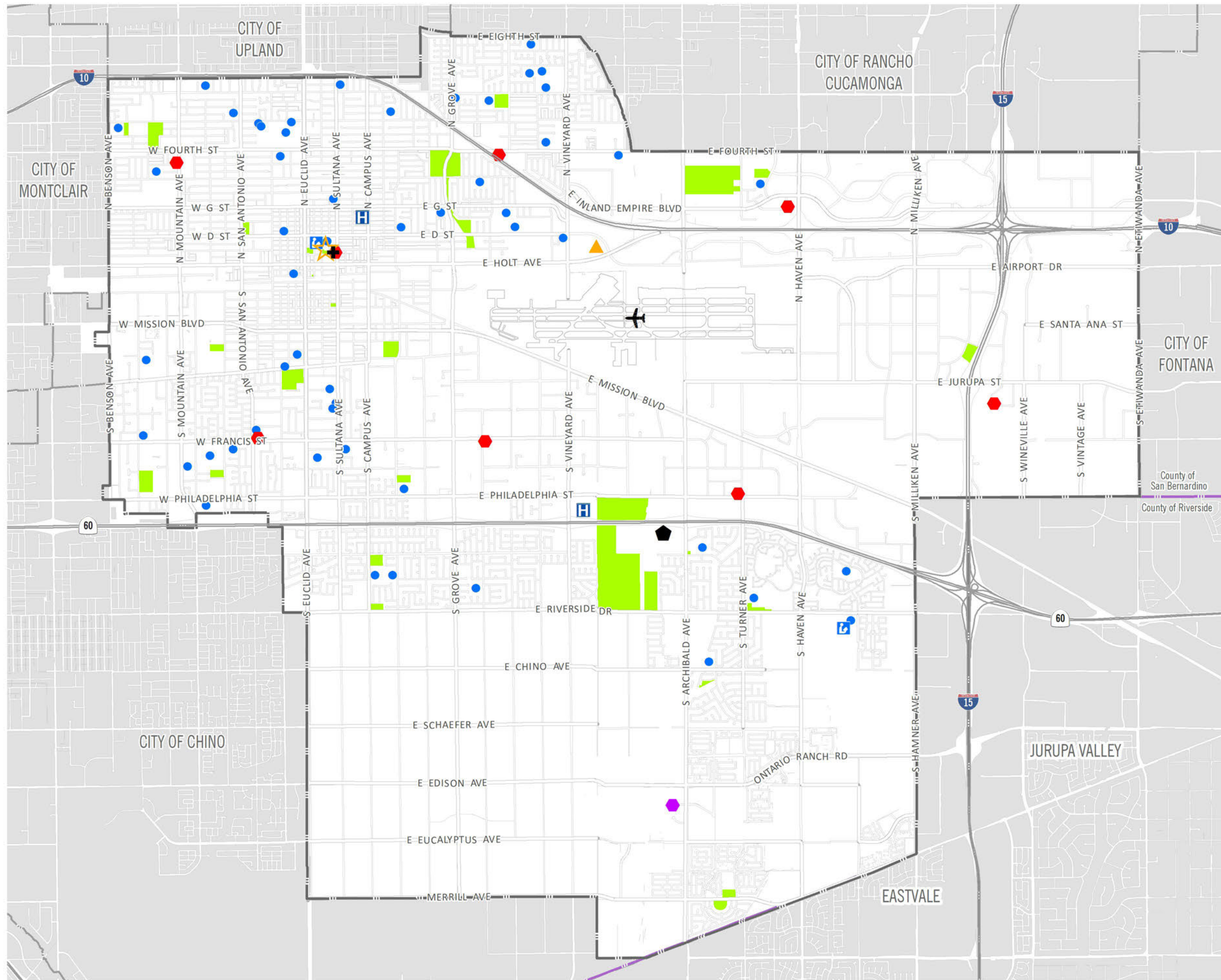
- 1, Alger Manufacturing Company Inc.
- 2, General Electric – Flat Iron
- 3, General Electric Co – Jet Engine Test Cell Facility
- 4, Ontario International Airport Joint Investigation
- 5, South Archibald TCE Plume
- 6, Sunshine Cleaners
- 7, 1425 South Vineyard Avenue
- 8, Aluminum – Art Plating Company Inc.
- 9, American Metals Recycling
- 10, Danco
- 11, Mission Cleaners
- 12, Ontario Plaza

Cleanup sites include those where soil or groundwater has been affected or is suspected to be affected by a chemical release from past or present land uses (referred to as “environmental cases”) and are identified on federal, state, and local regulatory agency lists. These lists are developed to document and record disturbance activities on identified sites. The status of each environmental case varies and can be either active (with ongoing investigations or remediation), closed (remediation or clean-up completed and approved by the regulatory agency), or unresolved (usually indicating that efforts toward remediation have stalled or been suspended).

This exhibit shows cleanup sites with open cases in Ontario which include sites with an active or unresolved status.



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 Source: Envirostar 2021, GeoTracker 2021 Date: 5/9/22



SAFETY

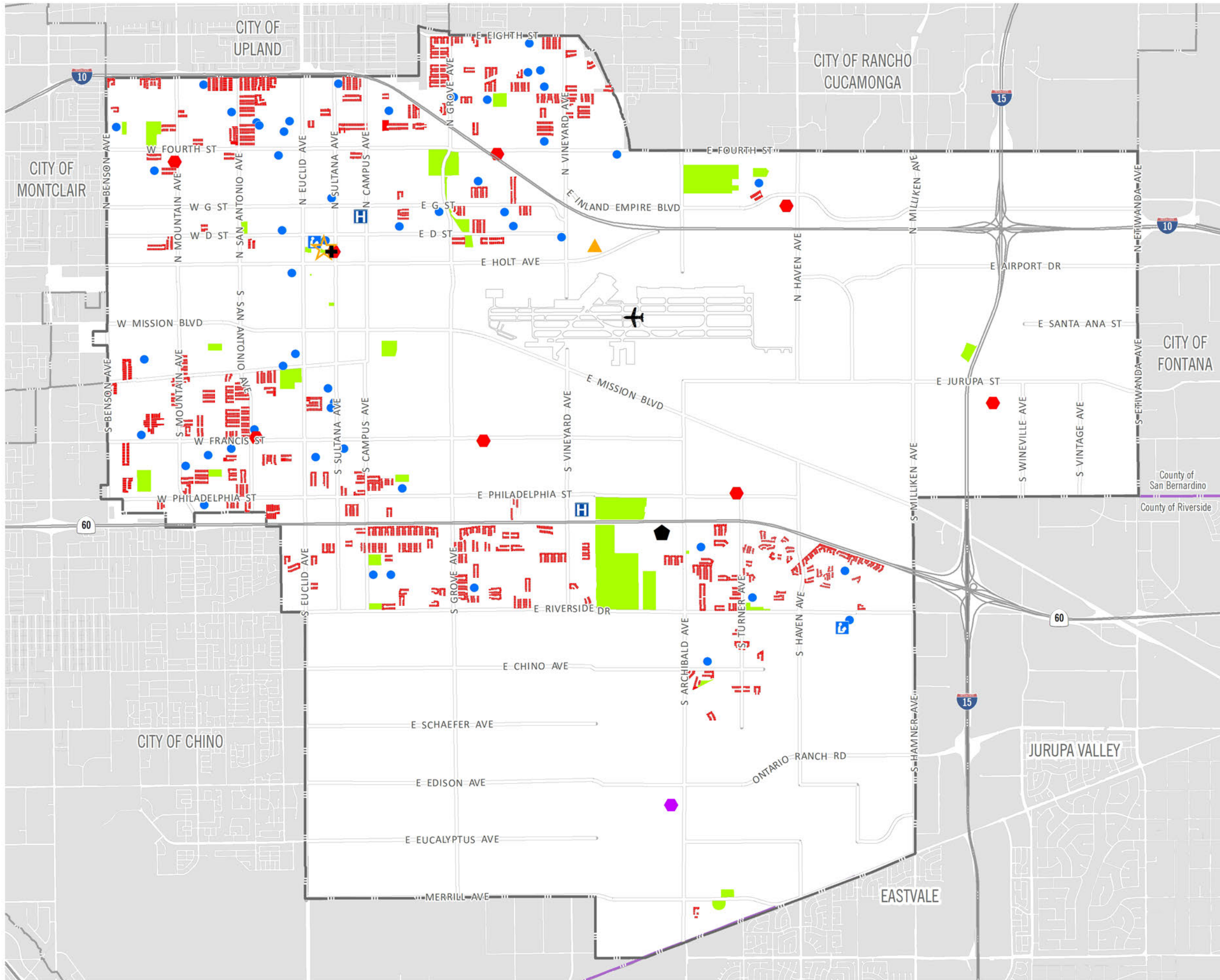
Figure S-08
Critical Facilities

- Ontario City Boundary
- County Boundary
- Critical Facilities**
- City Hall
- EOC
- Police Headquarters
- Convention Center
- Fire Station
- New Fire Station
- Airport
- Hospital
- Library
- Public & Private Schools
- Park

2050
THE ONTARIO PLAN

0 2,500 5,000 10,000 FT

Source: The City of Ontario 2021 Date: 5/9/22



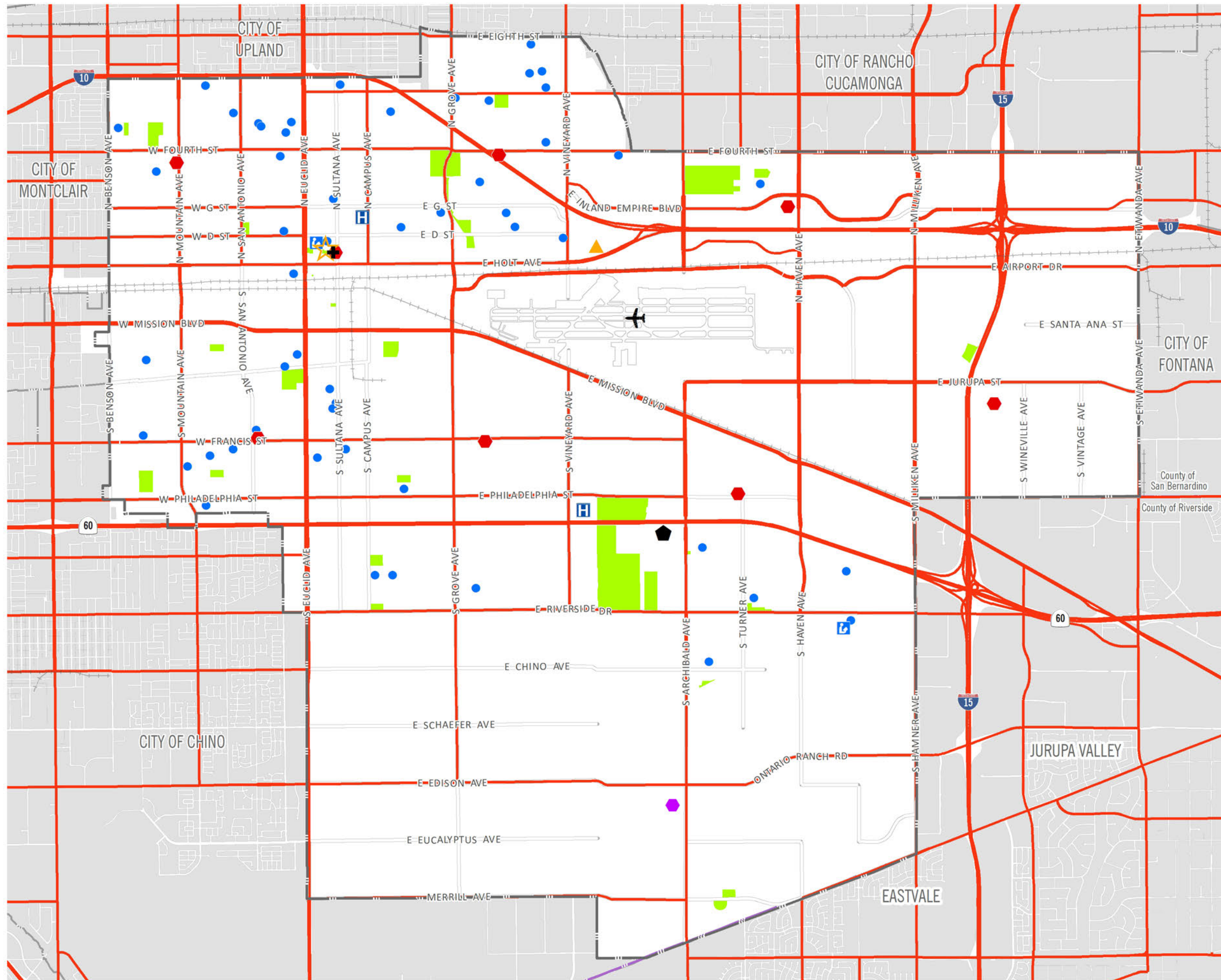
SAFETY

Figure S-09
Evacuation Constrained Areas

- Ontario City Boundary
- County Boundary
- Residential Parcel with Evacuation Constraint
- Critical Facilities**
- City Hall
- EOC
- Police Headquarters
- Convention Center
- Fire Station
- New Fire Station
- Airport
- Hospital
- Library
- Public & Private Schools
- Park



0 2,500 5,000 10,000 FT
 Source: The City of Ontario 2021 Date: 5/9/22



SAFETY

Figure S-10
Evacuation Routes

- County Boundary
- Ontario City Boundary
- Rail Network
- Evacuation Routes
- Critical Facilities**
- City Hall
- EOC
- Police Headquarters
- Convention Center
- Fire Station
- New Fire Station
- Airport
- Hospital
- Library
- Public & Private Schools
- Park



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 Source: The City of Ontario 2021 Date: 5/9/22

Exhibits: Documents Incorporated by Reference

Local Hazard Mitigation Plan
Vulnerability Assessment Report

MOBILITY ELEMENT

Introduction

Ontario is strategically located within a regional transportation network that includes an international airport with passenger and air cargo operations, three freeways, three freight rail lines, commuter and passenger rail services, public transit and a local network of streets and multi-purpose trails. This network provides multimodal transportation options for those traveling within, to or through the City. This robust system creates unique opportunities for Ontario as a regional jobs hub and a complete community. Ontario's Vision to concentrate growth in key locations will allow the City to capitalize on this transportation system.

Ontario's Vision is that there will be more mobility options as the City and the region grow. Personal vehicles will continue to provide individual mobility and flexibility for travel, though fuel systems will change, and guidance technology will become much more sophisticated. Bus travel will be a convenient and reliable option. The commuter rail system will provide service all day throughout the region and high-speed rail may be developed for longer-distance trips. Air travel will be linked through a future multimodal station in close proximity to ONT airport where access to many modes of transit will be available. Freight transportation via trucks, rail and air cargo will provide efficient movement of goods to the City, region, and beyond while minimizing negative impacts.

The mobility system will be coordinated with future land use patterns and levels of buildout. Access and connectivity to mobility options will be integrated into neighborhoods, center, corridors and districts. The placement of housing, jobs and amenities in closer proximity to each other and design strategies focused on the pedestrian and a variety of multimodal options will make walking and other forms of active transportation a desirable alternative to driving.

Purpose

The Mobility Element:

- Provides overall guidance for the City's responsibility to satisfy the local and subregional mobility needs of our residents, visitors, and businesses while maintaining the quality of life outlined in the Vision.
- Coordinates the mobility system with future land use patterns and levels of buildout.
- Addresses access and connectivity among the various neighborhoods, centers, corridors, and districts.
- Addresses the range of mobility options, including vehicular, trucking, freight and passenger rail, air, pedestrian, bicycle, other modes of active transportation, and transit.

Principles

The City believes:

- Access to convenient local and regional mobility options is essential to the City's growth and prosperity.
- A comprehensive multimodal mobility system is vital to providing equitable access to jobs, schools, shopping, services, parks, and other key destination points for people of all abilities and incomes.
- Transportation systems should reflect the context and desired character of the surrounding land uses.
- Well designed and maintained roadways, sidewalks, and bikeways are essential for the safe and efficient movement of goods and people.
- Transportation routes and their rights-of-way should be planned and preserved based upon projected travel demands.

Goals & Policies

M-1 Roadway System

Ontario's roadway system must meet multiple goals. It must provide convenient access and be safe, free flowing, visually appealing, multifunctional and in context with its surroundings. The roadway system should be designed to provide the necessary capacity to accommodate the traffic generated from the future buildout of the Land Use Plan while maintaining feasible Level of Service standards. The street rights-of-way also need to accommodate pedestrians, bicyclists, landscaping, traffic control devices, and infrastructure in a manner that is safe and aesthetically pleasing. Exhibit M-01, Roadway Classifications shows the hierarchy of our roadway system, consistent with the guidelines of the Federal Highway Administration.

Goal M-1 **A system of roadways that meets the mobility needs of a dynamic and prosperous Ontario.**

Policies

M-1.1 **Roadway Design and Maintenance.** We require our roadways to:

1. Comply with federal, state, and local design and safety standards;
2. Meet the needs of multiple transportation modes and users;
3. Handle the capacity envisioned in the City of Ontario Master Plan of Streets and Highways;
4. Be maintained in accordance with best practices;

5. Be compatible with the streetscape and surrounding land uses; and
6. Promote the efficient flow of all modes of traffic through the implementation of intelligent transportation systems and travel demand management strategies.

(Link to Community Design Element Policies CD-2.5, CD-2.6, CD-2.16, CD-3.3)

M-1.2 Mitigation of Impacts. We require development to mitigate its traffic impacts.

M-1.3 Agency Coordination on Roadway Improvements. We work with Caltrans, SBCTA, and others to identify, fund, and implement needed improvements to roadways when necessary. We work with neighboring jurisdictions to promote regional connectivity, access, and meet operational level of service standards at the City limits.

M-1.4 Complete Streets. We work to provide a complete, balanced, context-aware, multimodal transportation network that meets the needs of all users of streets, roads, and highways, including motorists, pedestrians, bicyclists, children, persons with disabilities, seniors, movers of commercial goods, and users of public transportation. We prioritize implementation of complete streets improvements in environmental justice areas to facilitate opportunities for residents to use active transportation systems.

M-1.5 Level of Service. Maintain a peak hour Level of Service (LOS) E or better at all intersections. Maintain Level of Service D or better on arterial streets in the City. Develop and maintain a list of locations where LOS E or LOS F are considered acceptable and would be exempt from this level of service policy. Considerations for LOS exemption include being restricted by environmental constraints, lacking available right-of-way, deterring an increase in VMT, or degrading other modes of travel (such as bicycle or pedestrian infrastructure).

M-1.6 Reduce Vehicle Miles Traveled. We will strive to reduce VMT through a combination of land use, transportation projects, travel demand management strategies, and other trip reduction measures in coordination with development projects and public capital improvement projects.

M-2 Active Transportation

In Ontario, active modes of transportation, such as walking and bicycling, promote a healthy lifestyle, improve air quality and traffic congestion by reducing the number of vehicles on the road, help to provide equitable access for people of different abilities and incomes to all areas of the City, and play a key role in creating vibrant neighborhoods, centers, corridors, and districts. Promoting development patterns and a mix of land uses that encourage active transportation combined with requiring thoughtful design of our trails, rights-of-way, buildings, and public realm to create a safe, comfortable, and convenient experience is integral to establishing a robust active transportation network.

Goal M-2 A system of trails and corridors that facilitate and encourage active modes of transportation.

Policies

- M-2.1 Active Transportation.** We maintain our Active Transportation Master Plan to create a comprehensive system of on- and off-street bikeways and pedestrian facilities that are safe, comfortable, and accessible and connect residential areas, businesses, schools, parks, and other key destination points.
- M-2.2 Bicycle System.** We provide off-street multipurpose trails and Class II bikeways as our preferred paths of travel and use the Class III for connectivity in constrained circumstances. When truck routes and bicycle facilities share a right-of-way, we prefer Class I or Class IV bicycle facilities. We require new development to include bicycle facilities, such as bicycle parking and secure storage areas.
- M-2.3 Pedestrian Walkways.** We require streets to include sidewalks and visible crosswalks at major intersections where necessary to promote safe and comfortable mobility between residential areas, businesses, schools, parks, recreation areas, and other key destination points. (Link to Community Design Policy CD-3.3)
- M-2.4 Network Opportunities.** We use public rights-of-way and easements such as, utility easements, levees, drainage corridors, road rights-of-way, medians, and other potential options to maintain and expand our bicycle and pedestrian network. In urban, mixed-use, and transit-oriented Place Types, we encourage the use of underutilized public and private spaces to expand our public realm and improve pedestrian and bicycle connectivity. (Link to Community Design Element *Urban, Mixed Use, and Transit-oriented Place Types* Section)

M-3 Public Transit

Public transportation plays an important role in providing an equitable and comprehensive transportation system and is essential to achieving the Vision. It provides an alternative mode of transportation for motorists and a primary mode for the transit dependent. Though the development and operation of most public transit services and facilities are outside the City's authority, the City actively promotes transit through sound land planning, urban design, and active participation in regional transportation agencies.

Future features of the public transit system in Ontario could include a new multimodal transit center, more extensive and frequent local bus service, higher-speed bus rapid transit corridors for longer trips, more Metrolink trains that connect to other regional hubs, convenient transfer centers, light and high-speed rail connections, improved feeder systems, connections to the City's active transportation network, and future land use patterns that are designed to promote transit use.

Goal M-3 A public transit system that is a viable alternative to automobile travel and meets basic transportation needs of the transit-dependent.

Policies

- M-3.1 Transit Partners.** We maintain a proactive working partnership with transit providers to ensure that adequate public transit service is available, cost-efficient, and convenient, particularly for residents in environmental justice areas.
- M-3.2 Alternative Transit Facilities at New Development.** We require new development adjacent to an existing or planned transit stop to contribute to the creation of transit facilities, such as bus shelters, transit bays and turnouts, and bicycle facilities, such as secure storage areas.
- M-3.3 Transit-Oriented Development.** We may provide additional development-related incentives to those inherent in the Land Use Plan for projects that promote transit use and reduce vehicle miles traveled.
- M-3.4 Bus Rapid Transit (BRT) Corridors.** We work with regional transit agencies to implement BRT service and reduce vehicle miles traveled by targeting destinations and corridors with the highest number of potential riders.
- M-3.5 Light Rail.** We support extension of the Metro Rail Gold Line to Ontario, and will work to secure station locations at the proposed multimodal transit center.
- M-3.6 Metrolink Expansion.** We advocate expansion of Metrolink service to include the Downtown and the multimodal transit center.
- M-3.7 High Speed Rail.** We encourage the development of high-speed rail systems that would enhance regional mobility in Southern California and serve the City of Ontario.
- M-3.8 Feeder Systems.** We work with regional transit agencies to secure convenient feeder service from the Metrolink station and the proposed multimodal transit center to employment centers in Ontario.
- M-3.9 Ontario Airport Metro Center Circulator.** We will explore development of a convenient mobility system, including but not limited to shuttle service, people mover, and shared car system, for the Ontario Airport Metro Center.
- M-3.10 Multimodal Transportation Center.** We intend to ensure the development of a multimodal transportation center near ONT airport to serve as a transit hub with amenities for transit riders, pedestrians, and bicyclists transitioning to local buses, BRT, the Gold Line, high-speed rail, the proposed Ontario Airport Metro Center Circulator, and other future transit modes. We support locations for the multimodal transportation center that are north of ONT airport, between Vineyard Avenue and Interstate 15.
- M-3.11 Transit and Community Facilities.** We require the future development of community-wide serving facilities to be sited in transit-ready areas that can be served and made accessible by public transit. Conversely, we plan (and coordinate

with other transit agencies to plan) future transit routes to serve existing community facilities.

M-4 Goods Movement

Goods movement in Ontario is provided via trains, trucks, and airplanes. The movement of goods to and through Ontario is critical to our economy, enabling growth of industrial and transportation-related businesses in the City and region. However, planes, trucks, and trains generate noise and air pollutants and contribute to traffic congestion along flight paths and corridors and at railroad grade crossings. To mitigate these impacts, we are committed to building grade-separated rail crossings, designing and locating industrial and warehousing land uses, and developing truck routes to protect our neighborhoods from truck traffic.

Goal M-4 An efficient flow of goods through the City that maximizes economic benefits and minimizes negative impacts.

Policies

- M-4.1** **Truck Routes.** We designate and maintain a network of City truck routes that provide for the safe and efficient transport of goods while minimizing negative impacts on local circulation and noise-sensitive land uses, as shown on Exhibit M-04, Truck Routes. We will minimize conflicts on truck routes through the design and implementation of buffers between travel lanes and pedestrian and bicycle facilities on designated truck routes.
- M-4.2** **Regional Participation.** We work with regional and subregional transportation agencies and adjacent cities to plan and implement goods movement strategies, including regional truck routes, plans and projects that improve mobility, support the efficient movement of goods, and minimize negative environmental impacts. (Link to Environmental Resources Policy ER-4.3)
- M-4.3** **Railroad Grade Separations.** We eliminate at-grade rail crossings identified on Exhibit M-01, Roadway Classifications.
- M-4.4** **Environmental Considerations.** We support both local and regional efforts to reduce/eliminate the negative environmental impacts of goods movement through the planning and implementation of truck routing and the development of a plan to evaluate the future needs of clean fueling/recharging and electrified truck parking.
- M-4.5** **Air Cargo.** We support and promote a ONT airport that accommodates 1.6 million tons of cargo per year, as long as the impacts associated with that level of operations are planned for and mitigated.

M-5 Regional Transportation

The transportation system serving Ontario, which includes three freeways, three rail mainlines and an international airport, provides our city with unparalleled regional access and is a primary reason for our tremendous potential. However, regional transportation forecasts project more congested freeways and local streets, which could limit economic development and affect quality of life. To maximize our potential and achieve our Vision, the City needs to be an active participant and leader in identifying and developing solutions to these issues through cooperative regional and subregional planning efforts.

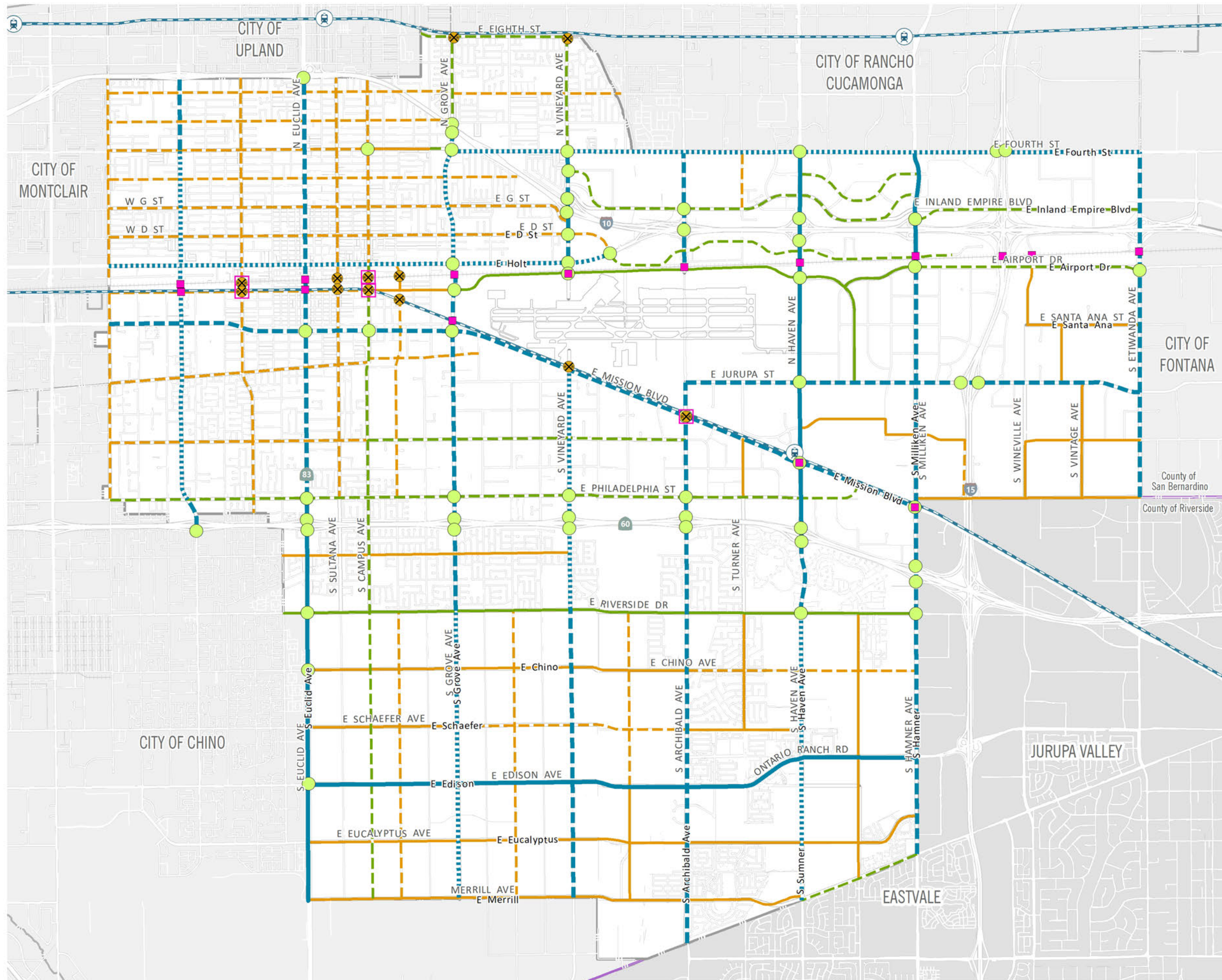
Traditional solutions won't be enough to solve projected future regional traffic congestion and new approaches may be needed. The City should take a leadership role at the federal, state, regional, and subregional levels to pursue the most promising strategies.

Goal M-5 **A proactive leadership role in helping identify and facilitate implementation of strategies that address regional transportation challenges.**

Policies

- M-5.1** **Regional Leadership.** We maintain a leadership role to help identify and implement potential solutions to long-term regional transportation problems.
- M-5.2** **Land Use Compatibility with Regional Transportation Facilities.** We work with ONT, railroads, Caltrans, SBCTA, and other transportation agencies to minimize impacts.

Exhibits: Figures



MOBILITY

Figure M-01
Roadway Classifications

- Principal Arterial**
 - 8 Lanes
 - 6 Lanes
 - 4 Lanes
- Minor Arterial**
 - 6 Lanes
 - 4 Lanes
- Collector**
 - 4 Lanes
 - 2 Lanes
- Enhanced Intersections
- Rail Crossings**
 - Existing At-Grade Crossing
 - Existing Grade-Separated Crossing
 - Future Grade-Separated Crossing
- Metrolink
- Metrolink Station
- Railroad
- Ontario City Boundary
- County Boundary

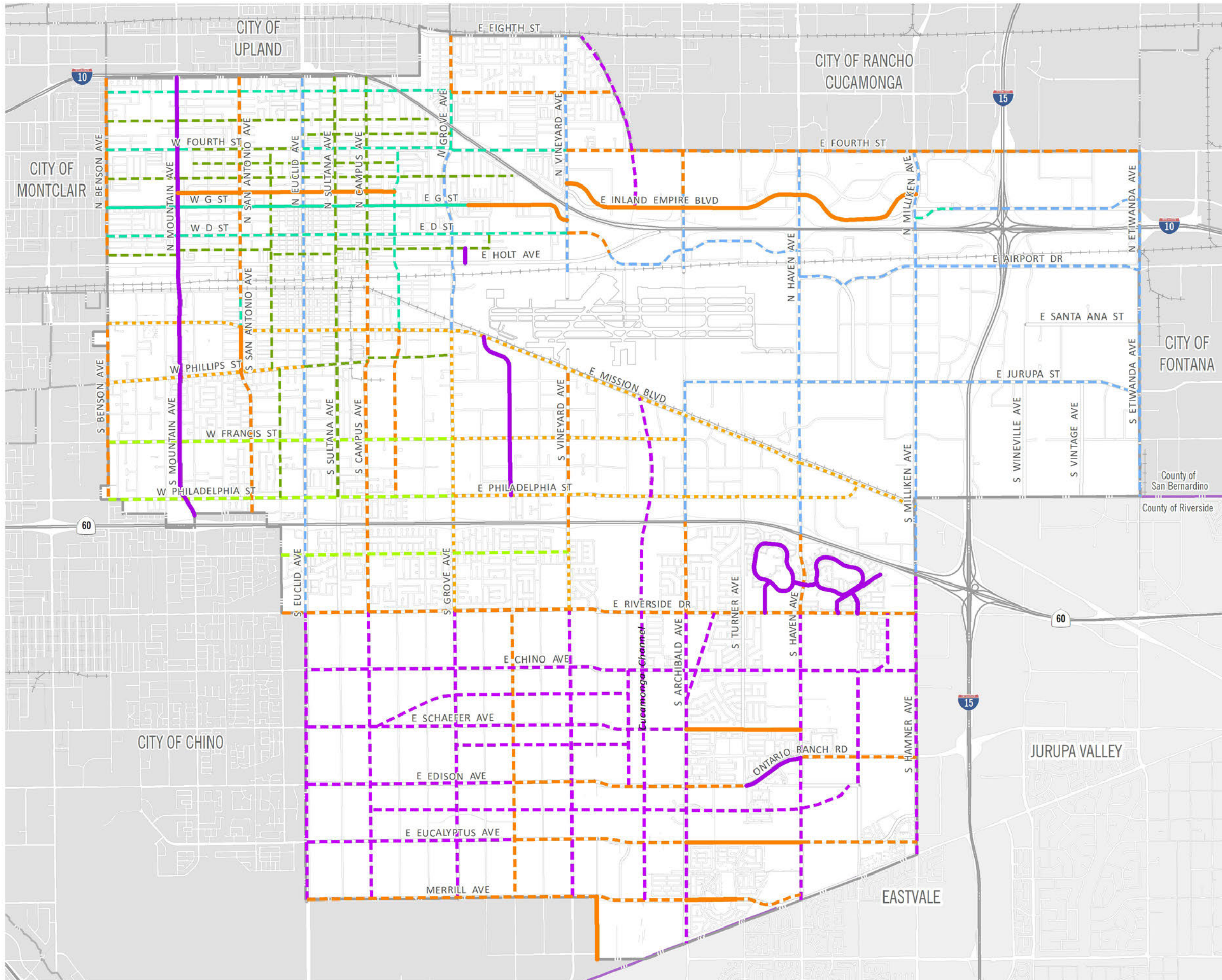
2050
THE ONTARIO PLAN

0 2,500 5,000 10,000 FT

Source: The City of Ontario 2021 Date: 5/9/22

MOBILITY

Figure M-02
 Multipurpose Trails & Bikeways



- Ontario City Boundary
- County Boundary
- Rail Network
- Existing Trail & Bike Network**
 - Multipurpose Trail (Class I)
 - Bike Lane (Class II)
 - Bike Route (Class III)
- Proposed Trail & Bike Network**
 - Multipurpose Trail (Class I)
 - Bike Lane (Class II)
 - Buffered Bike Lane (Class II)
 - Bike Lane (Class II)/Bike Route (Class III)
 - Bike Boulevard (Class III)
 - Bike Route (Class III)
 - Additional Studies Required

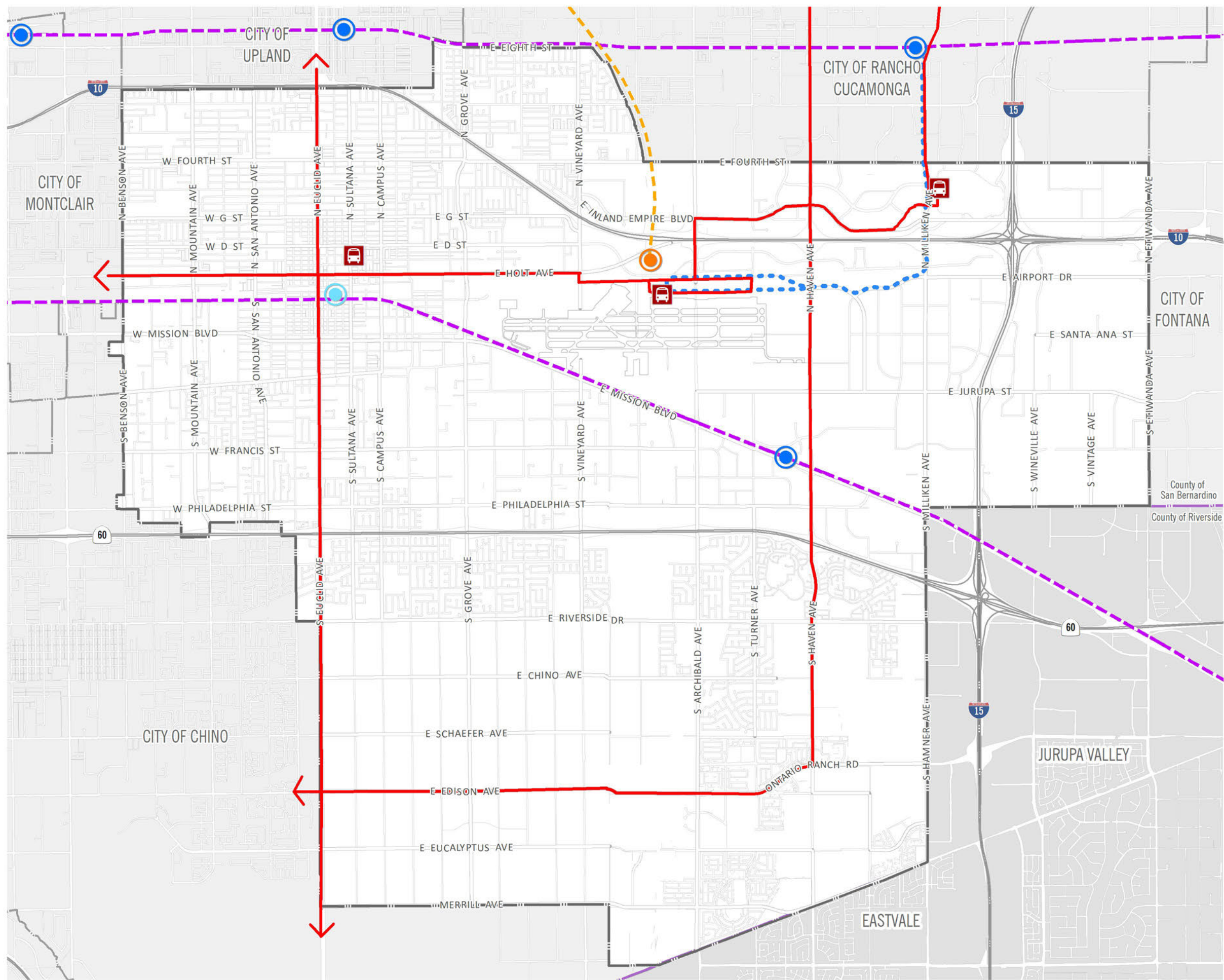
2050
THE ONTARIO PLAN

0 2,500 5,000 10,000 FT

Source: The City of Ontario 2022 Date: 5/9/22

MOBILITY

Figure M-03
Public Transit



Transit Centers

- Existing Bus Transfer Center
- Existing Metrolink Station
- Amtrak Station/Proposed Metrolink Station
- Future Multimodal Transportation Center
- BRT Corridor
- Metro Goldline Extension
- Metrolink
- Ontario Airport Loop
- Railroad
- Ontario City Boundary
- County Boundary

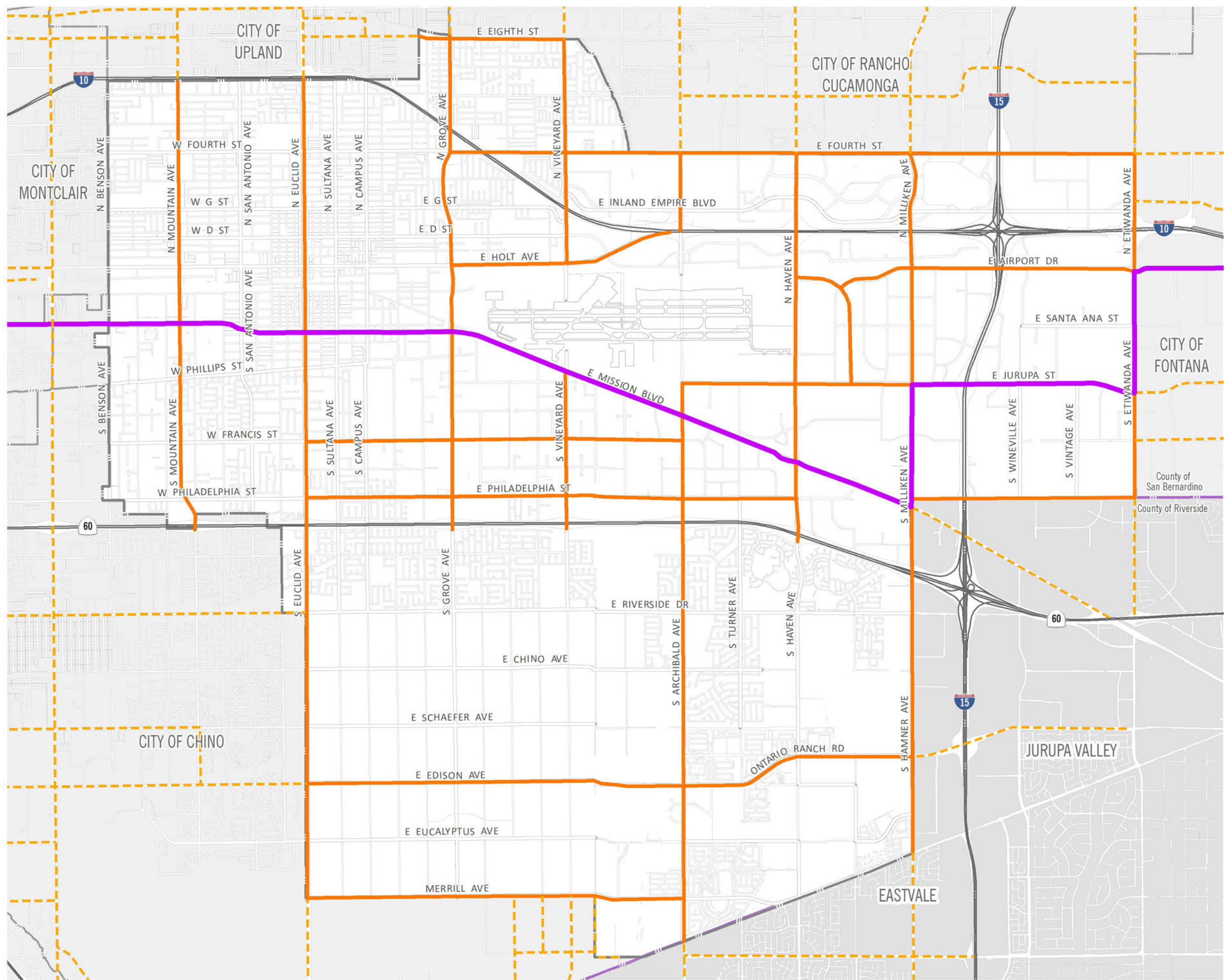
2050
THE ONTARIO PLAN

0 2,500 5,000 10,000 FT

Source: The City of Ontario 2020 Date: 5/9/22

MOBILITY

Figure M-04
Truck Routes



- State DOT Network
- Truck Route
- Adjacent Agency Truck Route
- Ontario City Boundary
- County Boundary

2050
THE ONTARIO PLAN

0 2,500 5,000 10,000 FT

Source: The City of Ontario 2020 Date: 5/9/22

COMMUNITY DESIGN ELEMENT

Introduction

All members of the community – residents, business owners, workers, visitors, and investors – need places and spaces that are both functional and appealing. Quality design of the built environment can: protect existing investments; achieve sustainable environments; create safe, comfortable, and pleasant places where people want to live, work and recreate; and add value to the community.

Purpose

The Community Design Element:

- Distinguishes Ontario as home to a unique, highly aesthetic built environment that fosters enjoyment, financial benefit, and wellbeing for the entire community.
- Articulates design qualities that will create locally and regionally significant places.
- Utilizes community design to help achieve the Vision in the areas of economic development, land use, housing, community health, infrastructure, and transportation.

Principles

The City believes:

- Quality design of buildings, streets, public spaces, City gateways, and open spaces is vital to prosperity and makes Ontario a place where people want to be.
- Strategically located urban districts that are designed around transportation systems help define Ontario's regional identity.
- Ontario's unique history and heritage – expressed in its streets, landscaping and buildings – help define the community's identity.
- Well maintained property and infrastructure are required to protect and encourage community investment.
- A diverse mix of residential and commercial neighborhoods, centers, corridors, and districts is vital to achieving the Vision.

Goals & Policies

CD-1 Image & Identity

Ontario's Vision is to be the urban and employment center for the Inland Empire. Due to its strategic regional location, capacity for growth, and potential for intensification and urbanization of select areas, Ontario is uniquely situated to fulfill this role. However, thoughtful design choices will be required to ensure the new urban patterns and forms create distinct and complete places that complement Ontario's historic development and respects the distinct identities of the City's established viable neighborhoods.

The City's physical form will set it apart from suburban communities in the Inland Empire by establishing new urban districts, like the Ontario Airport Metro Center, which will serve as regional employment, entertainment, and commercial hubs that are supported by a robust residential population. The planning and design of these districts should enhance the City's image and identity within the region, state, and nation.

Existing viable neighborhoods also distinguish Ontario as a premier community with a breadth of different places for people to live, work, and play. As the City evolves, it is committed to preserving and protecting these neighborhoods, while finding ways to enhance and embrace the special character of each.

Ontario's transportation and view corridors also play an integral role in characterizing the City's image and identity. The City is traversed by three freeways, three rail lines, one state highway, and it is home to an international airport. For many visitors, the primary image of Ontario is shaped by what is seen from these transportation systems. The experience of traveling along transportation corridors should be enhanced to communicate the distinct identities of adjacent places within the City, which will entice visitors to experience more of what Ontario has to offer. Views of the San Gabriel Mountains are visible to the north from most north-south corridors. These views provide an important tool to orient residents, employees, and visitors within the City, and they are part of Ontario's identity.

Goal CD-1 A dynamic, progressive city containing distinct and complete places that foster a positive sense of identity and belonging among residents, visitors, and businesses.

Policies

CD-1.1 **City Identity.** We take actions that are consistent with the City being a leading urban center in Southern California while recognizing, enhancing, and preserving the character of our existing viable neighborhoods.

CD-1.2 **Place Types.** We establish Place Types in urban, mixed use, and transit-oriented areas to foster the City's identity as a premier community and require new development within each Place Type to incorporate prescribed urban patterns, forms, and placemaking priorities. (Link to Community Design Element *Urban, Mixed Use, and Transit-oriented Place Types* Section)

- CD-1.3 Existing Neighborhoods.** We require the existing character of viable residential and non-residential neighborhoods be preserved, protected, and enhanced.
- CD-1.4 Transportation Corridors.** We will enhance our major transportation corridors within the City through landscape, hardscape, signage and lighting. The extent of enhancement should be appropriate to the use, type, and context of each corridor.
- CD-1.5 View Corridors.** We require all major north-south streets be designed and redeveloped to feature views of the San Gabriel Mountains, which are part of the City's visual identity and a key to geographic orientation. Such views should be free of visual clutter, including billboards and may be enhanced by framing with trees.

CD-2 Design Quality

Ontario is made up of neighborhoods, commercial areas, public spaces, parks, and roadways that have developed over more than a century. This has resulted in an eclectic built environment with a rich blend of architectural styles including the historic downtown, residential neighborhoods, equestrian properties, commercial centers, and industrial and office complexes.

Intense urban, mixed use, and transit-oriented districts are envisioned to be added in strategic areas of the City, and agricultural areas of the City are expected to redevelop as part of Ontario's complete community. Designs for new urban districts, residential communities, sites, and buildings, will help to realize the City's Vision, improve connectivity, help to realize active transportation and public transit goals, and reflect its intended or existing role within the City by melding appropriate and high quality design, with attention to detail that would be expected in a preeminent Southern California city.

High quality design will be applied citywide to achieve distinct neighborhoods, centers, corridors, and districts. Buildings and places shall respect the site context and further the City's Vision. This approach ensures all physical improvements, including the layout of new communities and urban districts, overall site design, landscaping, building design and orientation, architectural details, site furniture and building materials, coordinate help achieve vibrant places and enhance value and livability throughout the City.

Goal CD-2 A high level of design quality resulting in neighborhoods, commercial areas, public spaces, parks, and streetscapes that are attractive, safe, functional, human- scale, and distinct.

- CD-2.1 Quality Building Design and Architecture.** We encourage all development projects to convey visual interest and character through:
1. Building volume, massing, and height to provide context-appropriate scale and proportion;
 2. A true architectural style which is carried out in plan, section, and elevation through all aspects of the building and site design and appropriate for its setting; and

3. Exterior building materials that are articulated, high quality, durable, and appropriate for the architectural style.

CD-2.2 Neighborhood Design. We create distinct residential neighborhoods that promote a sense of community and identity by emphasizing access, connectivity, livability, and social interaction through such elements as:

1. A pattern of smaller, walkable blocks that promote activity, safety, and access to nearby amenities and services;
2. Varied parcel sizes and lot configurations to accommodate a diversity of housing types;
3. Traffic calming measures to slow traffic and promote walkability while maintaining acceptable traffic flows and emergency evacuation access;
4. Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the “outdoor living room”), as appropriate; and
5. Landscaped parkways, with sidewalks separated from the curb and designed to maximize safety, comfort, and aesthetics for all users.

CD-2.3 Commercial Areas. We desire commercial areas and centers to be distinctive, pedestrian friendly, functional, and vibrant with a range of businesses, places to gather, and connectivity to the neighborhoods they serve.

CD-2.4 Urban, Mixed Use, and Transit-oriented Areas. We establish Place Types to require mixed use, urban, and transit-oriented areas to be designed and developed as pedestrian oriented areas that are integrated with adjacent neighborhoods and promote a vibrant, comfortable, and functional environment, as defined for each Place Type. (Link to Community Design Element *Urban, Mixed Use, and Transit-oriented Place Types* Section)

CD-2.5 Streetscapes. We design new and, when necessary, retrofit existing streets to improve walkability, bicycling and transit integration, strengthen connectivity, and enhance community identity through improvements to the public right-of-way such as sidewalks, street trees, parkways, curbs, street lighting and street furniture.

CD-2.6 Connectivity. We promote development of local street patterns, multimodal networks, and connected public spaces that create and unify neighborhoods, rather than divide them, and create cohesive and continuous corridors, rather than independent “islands” through the following means:

1. Local street networks that provide access both between subdivisions and within neighborhoods and discourage through traffic;
2. A local street system that is logical and understandable for the user. A grid system is preferred to avoid circuitous and confusing travel paths between

internal neighborhood areas and adjacent arterials and to provide adequate emergency and evacuation access; and

3. Pedestrian and bicycle networks that provide convenient access to neighborhoods and nearby destinations, such as schools, parks, other public spaces, commercial areas, and transit stops.

CD-2.7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping, and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials, and construction techniques.

CD-2.8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintaining visibility and accessibility, and using lighting.

CD-2.9 Landscape Design. We encourage durable, sustainable, and drought-tolerant landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

CD-2.10 Parking Areas. We require all development, including single-family residential, to minimize the visual impact of surface, structured, and garage parking areas visible from the public realm in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include:

1. Surface parking: Shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.
2. Structured parking: facade articulation, screening, appropriate lighting, and landscaping.
3. Garage parking: providing access to single-family residential garages through alley access, recessing garages from the frontage to emphasize front doors or active living spaces.

CD-2.11 Entry Statements. We encourage the inclusion of amenities, signage, and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.

CD-2.12 Site and Building Signage. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.

- CD-2.13 Entitlement Process.** We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.
- CD-2.14 Availability of Information.** We provide easy access to information for developers, builders and the public about design quality, construction quality, and sustainable building practices.
- CD-2.15 Leverage Professional and Trade Organizations.** We support excellence in design and construction quality through collaboration with trade and professional organizations that provide expertise, resources and programs for developers, builders, and the public.
- CD-2.16 Transit Stops.** We require transit stops be conveniently located, well lit, safe, and clearly accessible to pedestrians, bicyclists, and people of all abilities.

CD-3 Urban, Mixed Use, and Transit-Oriented Place Types

The development of urban, mixed use, and transit-oriented environments place new demands on the design and quality of buildings, open spaces, and public spaces. These areas, as shown on Exhibit CD-01, Place Types, are distributed throughout the city and are generally centered around areas designed as mixed use. They include a range of scales that are intended to fulfill different roles within city. However, all of the identified urban, mixed use, and transit oriented Place Type areas are envisioned as walkable environments with a safe, comfortable, and inviting public realm that encourages people to spend time.

Goal CD-3 Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

Policies

- CD-3.1 Unique Identity.** We promote development that heightens the unique character and identity of each Place Type by requiring compatible land uses and land planning, site design, and building design that promotes an active public realm.
- CD-3.2 Comfortable, Human-Scale Public Realm.** We require that public spaces, including streets, parks, and plazas on both public and private property be designed to maximize safety, comfort and aesthetics and connect to the citywide pedestrian, vehicular, and bicycle networks.
- CD-3.3 Complete and Connected Network.** We require that pedestrian, vehicular, and bicycle circulation on both public and private property be coordinated to provide connections internally and externally to adjacent neighborhoods and properties (existing and planned) through a system of local roads and trails that promote walking and biking to nearby destinations (including existing and planned parks, commercial areas, and transit stops) and are designed to maximize safety, comfort, and aesthetics.

- CD-3.4 Context-Aware and Appropriate Design.** We require appropriate building and site design that complements existing development, respects the intent and identity of the Place Type, and provides appropriate transitions and connections between adjacent uses to ensure compatibility of scale, maintain an appropriate level of privacy for each use, and minimize potential conflicts.
- CD-3.5 Active Frontages.** We create lively pedestrian streetscapes by requiring primary building, business, and residential entrances, outdoor dining, and storefronts be located on ground floors adjacent to sidewalks or public spaces and designed to maximize safety, comfort, aesthetics, and the intended functionality (as defined by the Place Type).
- CD-3.6 Managed Infrastructure.** We collaborate with developers and property owners to facilitate development that realizes the envisioned character and functionality of the Place Type through the use of green and shared infrastructure within each Place Type.

CD-4 Historic Preservation

Ontario's history remains one of its greatest assets. Its historic districts and resources, physical layout and the legacy of its people, businesses, social and community organizations, and industries contribute to the City's identity. Beyond having been declared the "Model Colony," by an act of Congress, Ontario's history includes citrus farming, viticulture in Guasti, and dairy farming. With some of the most authentic historically distinct residential neighborhoods in Southern California, Ontario is among a handful of communities whose creative and proactive approach to historic preservation serves as a model for others. The Ontario Museum of History and Art and the City's Historic Preservation program have long recognized that the story of its businesses, industries, and people are as important as the history of its neighborhoods, buildings, streets, and landscapes.

Goal CD-4 Historic buildings, streets, landscapes, and neighborhoods, as well as the story of Ontario's people, businesses, and social and community organizations, have been preserved and serve as a focal point for civic pride and identity.

Policies

- CD-4.1 Cultural Resource Management.** We update and maintain an inventory of historic sites and buildings, professional collections, artifacts, manuscripts, photographs, documents, maps, and other archives.
- CD-4.2 Collaboration with Property Owners and Developers.** We educate and collaborate with property owners and developers to implement strategies and best practices that preserve the character of our historic buildings, streetscapes, and unique neighborhoods.
- CD-4.3 Collaboration with Outside Agencies.** We pursue opportunities to team with other agencies, local organizations, and nonprofits in order to preserve and promote Ontario's heritage.

- CD-4.4 Incentives.** We use the Mills Act and other federal, state, regional and local programs to assist property owners with the preservation of select properties and structures.
- CD-4.5 Adaptive Reuse.** We actively promote and support the adaptive reuse of historic sites and buildings to preserve and maintain their viability.
- CD-4.6 Promotion of Public Involvement in Preservation.** We engage in programs to publicize and promote the City's and the public's involvement in preservation efforts.
- CD-4.7 Public Outreach.** We provide opportunities for our residents to research and learn about the history of Ontario through the Planning Department, the Ontario Museum of History and Art, and the Robert E. Ellingwood Model Colony History Room.

CD-5 Protection of Investment

Communities that are well maintained, safe and visually appealing are more desirable places to live and conduct business. Properties that are continually maintained retain their value and encourage others to invest.

Goal CD-5 A sustained level of maintenance and improvement of properties, buildings, and infrastructure that protects the property values and encourages additional public and private investments.

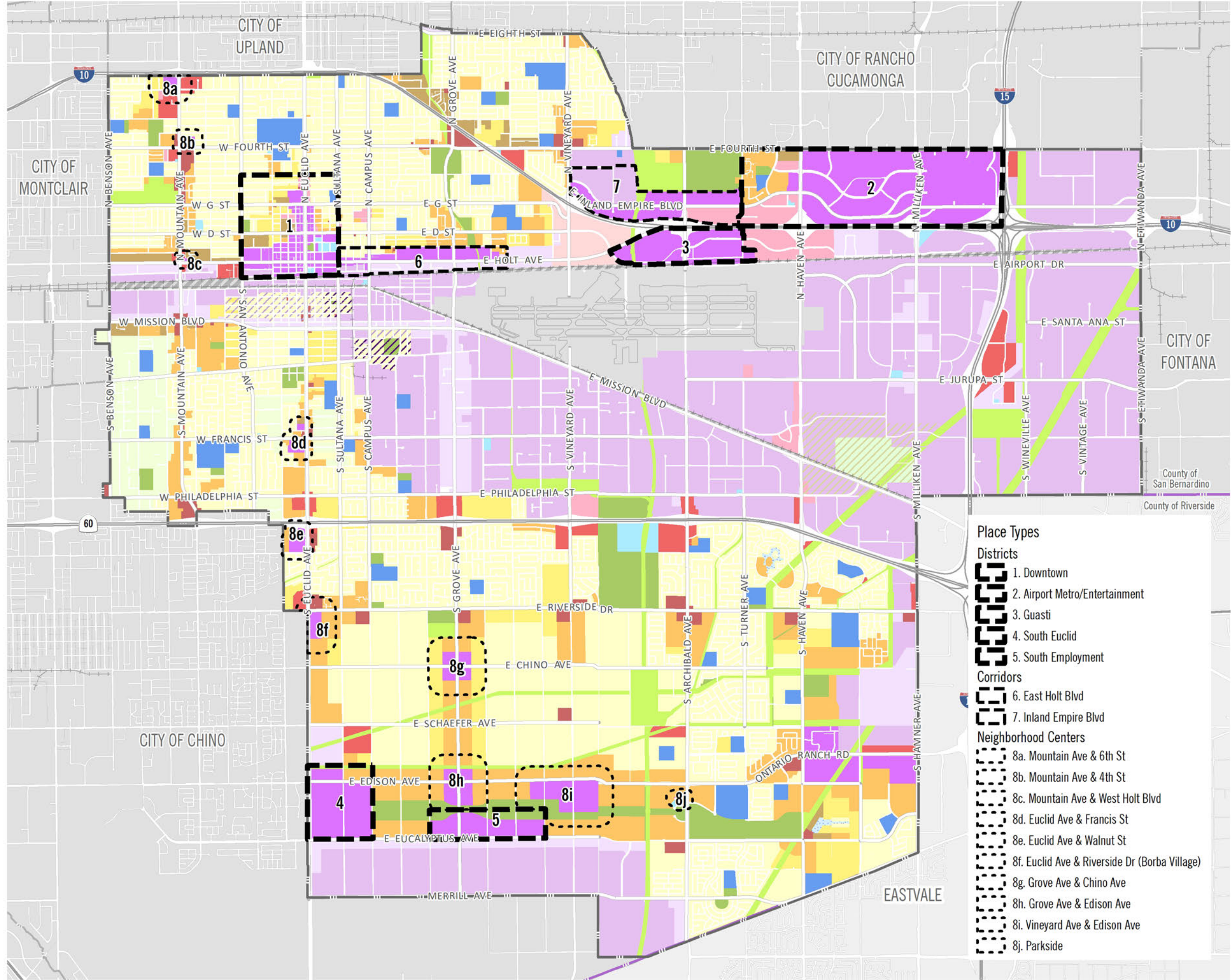
Policies

- CD-5.1 Maintenance of Buildings and Property.** We require all public and privately-owned buildings and property (including trails and easements) to be properly and consistently maintained.
- CD-5.2 Maintenance of Infrastructure.** We require the continual maintenance of infrastructure.
- CD-5.3 Improvements to Property & Infrastructure.** We provide programs to improve property and infrastructure.
- CD-5.4 Neighborhood Involvement.** We encourage active community involvement to implement programs aimed at the beautification and improvement of neighborhoods.

Exhibits: Tables and Figures

COMMUNITY DESIGN

Figure CD-01
Place Types



- Ontario City Boundary
- County Boundary
- Rail Network
- Overlay Zones**
- Business Park
- Industrial
- Landfill Impact Area
- Land Use**
- Residential**
- RR Rural Residential
- LDR Low Density Residential
- LMDR Low Medium Density Residential
- MDR Medium Density Residential
- HDR High Density Residential
- Mixed-Use**
- MU Mixed Use
- Commercial**
- NC Neighborhood Commercial
- GC General Commercial
- OC Office Commercial
- HOS Hospitality
- Employment**
- BP Business Park
- IND Industrial
- Other**
- OS-NR Open Space - Non-Rec
- OS-R Open Space - Recreation
- OS-W Open Space - Water
- PF Public Facility
- PS Public School
- ARPT Airport
- LF Landfill
- Rail

- Place Types**
- Districts**
1. Downtown
 2. Airport Metro/Entertainment
 3. Guasti
 4. South Euclid
 5. South Employment
- Corridors**
6. East Holt Blvd
 7. Inland Empire Blvd
- Neighborhood Centers**
- 8a. Mountain Ave & 6th St
 - 8b. Mountain Ave & 4th St
 - 8c. Mountain Ave & West Holt Blvd
 - 8d. Euclid Ave & Francis St
 - 8e. Euclid Ave & Walnut St
 - 8f. Euclid Ave & Riverside Dr (Borba Village)
 - 8g. Grove Ave & Chino Ave
 - 8h. Grove Ave & Edison Ave
 - 8i. Vineyard Ave & Edison Ave
 - 8j. Parkside

2050
THE ONTARIO PLAN

0 2,500 5,000 10,000 FT

Source: The City of Ontario 2022 Date: 5/9/22

CD-02 Place Type Categories

Place Types

Districts – Districts are geographically large, culturally unique, and regionally significant areas of the city that fulfill different but complementary roles in creating a complete community. As regional destinations, districts serve as the “face of the city.” Each district has a distinct identity that reflects its role within the city context and establishes a defined sense of place through the creation of an amenity-rich, transit-accessible, mixed-use environments where people can live, work, shop, dine, and have many of their daily needs met within a comfortable and convenient walk or bike ride from their residence, transit stop, or parked vehicle.



1

Downtown District

The historic center of Ontario, the Downtown District, is envisioned as the place-based, people-focused, commercial, and cultural “heart” of the City and features several designated historic districts and landmark. Downtown will support a variety of businesses, housing opportunities for residents with a variety of income levels, creative spaces, entertainment options, and institutional and civic uses, while preserving its historic character, buildings, neighborhoods, and places. Complementing the wide mix of uses are designs, layouts and public spaces that give residents, visitors, and businesses a strong sense of connection and creates a place where people want to spend time.



2

Airport / Metro Entertainment District

The Airport / Metro Entertainment District is envisioned as a large-scale regional entertainment hub, leveraging attractions, such as the Toyota Arena and Ontario Mills, supported by a variety of retail and dining options integrated alongside high-intensity office and residential development. This area will evolve into a walkable urban center through the addition of mixed use infill development, a pedestrian-oriented road and trail network, key public realm improvements, and multiple transit options.



3

Guasti District

The historic Guasti (winery) Village sets the theme and backdrop to the Guasti District. The district is envisioned to preserve and enhance the historic buildings as the focal point of a vibrant mixed use area that leverages its proximity to the Ontario International Airport, Ontario Convention Center, and the potential future development of a multimodal transit center. The district will include intensive office, commercial, and lodging uses along the I-10 corridor, and appropriately scaled residential, office, commercial, and lodging near the Guasti Village historic core. The multimodal mixed use area within the Guasti District is intended to leverage proximity to the future multimodal transportation center and link the Convention Center, Guasti compound, and airport through strong physical and visual design.

CD-02 Place Type Categories

Place Types



4

South Euclid District

Lying at the southwest corner of the City, the South Euclid District is envisioned to develop as a vertically- and horizontally- mixed use area to serve the populations of newly developing Ontario Ranch and adjoining communities. The district will include a range of housing types integrated within and alongside retail, commercial, and office uses, as well as public spaces and trails that connect the “Great Park” to Euclid Avenue. The area is intended to be highly walkable with pedestrian-oriented site design and road network and amenitized transit stops that leverage the Bus Rapid Transit (BRT) investments.



5

South Employment District

The South Employment District is envisioned to foster employee-intensive office, entertainment facilities, live/work, and supporting retail uses in a campus environment designed to leverage proximity to the “Great Park” and maintain compatibility with surrounding residential areas. Outside of the Chino Airport safety zone, higher density stand-alone and mixed use residential neighborhoods are intended to orient toward the “Great Park” and incorporate transition areas from the surrounding business park uses.

Corridors – Corridors are important components of the vehicular circulation system, moving traffic through the city and connecting to the greater region. While this remains an important role of the city's corridors, they are envisioned as vibrant and sustainable places, with a mix of uses, a variety of transportation options, and comfortable pedestrian and bicycle access to surrounding neighborhoods.



6

East Holt Boulevard Corridor

Holt Boulevard is an important east-west corridor connecting Ontario to the greater region, with improved connectivity options through the addition of BRT service. This corridor is envisioned to gradually build out over time through infill development that ultimately yields vibrant neighborhood-serving commercial areas, a variety of housing options, and employment opportunities that are clustered around BRT stops that can be easily accessed by pedestrians and bicyclists.

CD-02 Place Type Categories

Place Types



7

Inland Empire Boulevard Corridor

The Inland Empire Boulevard Corridor connects Vineyard Avenue and the Convention Center directly to the heart of the Airport / Metro Entertainment District and future BRT. Near- to mid-term improvements include the addition of significant rows of large street trees to provide shade, spatial definition, and a better pedestrian environment; the addition of new bicycle facilities, infill buildings oriented to the street, and reorientation of existing building entrances toward the street where possible. Over the longer term, this corridor has the potential to become a westward extension of the Airport / Metro Entertainment District environment.

Neighborhood Centers – Neighborhood Centers serve as local gathering places, allow for convenient access to amenities, and help to establish identity for the surrounding neighborhoods. They typically consist of locally serving commercial uses and/or other community-serving uses, and intended to be accessible by foot, bike, transit, or car for those visiting the site or residing in the surrounding area. This place type is generally applied to the area within a short walk of a single or cluster of mixed use parcels along a larger street and/or around an intersection. Neighborhood Centers can be found throughout the existing city fabric (infill) and in areas planned for future growth and development (south of Riverside Drive).



8

(a-e)

Neighborhood Center - Infill

Neighborhood Centers located in areas where the commercial and residential parcels were largely developed in an incremental manner prior to 2020, are north of Riverside Drive, and are categorized as “infill” centers. These centers will range in size, complexity, and relationship to the surrounding streets and neighborhoods. Some are very small centers that may only include a horizontal mix of uses fronting onto a wide sidewalk while others may involve a vertically integrated mix of higher density residential and nonresidential uses oriented around a public plaza or other form of open space. All of these centers are envisioned to incorporate site design that prioritizes safe, and attractive, and well-shaded pedestrian access onsite, from the public rights-of-way, and from the adjacent neighborhoods.

CD-02 Place Type Categories

Place Types



8

(f-j)

Neighborhood Center – South of Riverside Drive

Neighborhood Centers identified south of Riverside Drive are generally underutilized or vacant as of 2022. These Neighborhood Centers are envisioned to bring new vitality and involve some level of master planning, with the roadway, pathways, open spaces, public realm designs, and site designs coordinated prior to the development of all of the parcels. These centers will range in size, complexity, and relationship to the surrounding streets and neighborhoods.

Some of these centers will involve a small node of mixed use development that is connected to the “Great Park” while others will involve larger clusters of mixed use areas that serve as a focus point for the surrounding residential neighborhoods. The largest of these centers are intended to serve not only as a local focal point but also as a major citywide landmark that also connects directly to the “Great Park”. All but the smallest of these centers are envisioned to consist of some amount of vertically integrated and higher intensity residential, office, and retail buildings oriented toward the “Great Park”, large public plazas, and/or large trail systems.

All of these centers are envisioned to incorporate site design that prioritizes safe, attractive, and well-shaded pedestrian access onsite, from the public rights-of-way, and from the adjacent neighborhoods. Parking areas are to be located next to and behind buildings to provide a variety of frontages along streets.

CD-03 Downtown District Place Type

1. Downtown District

District Key



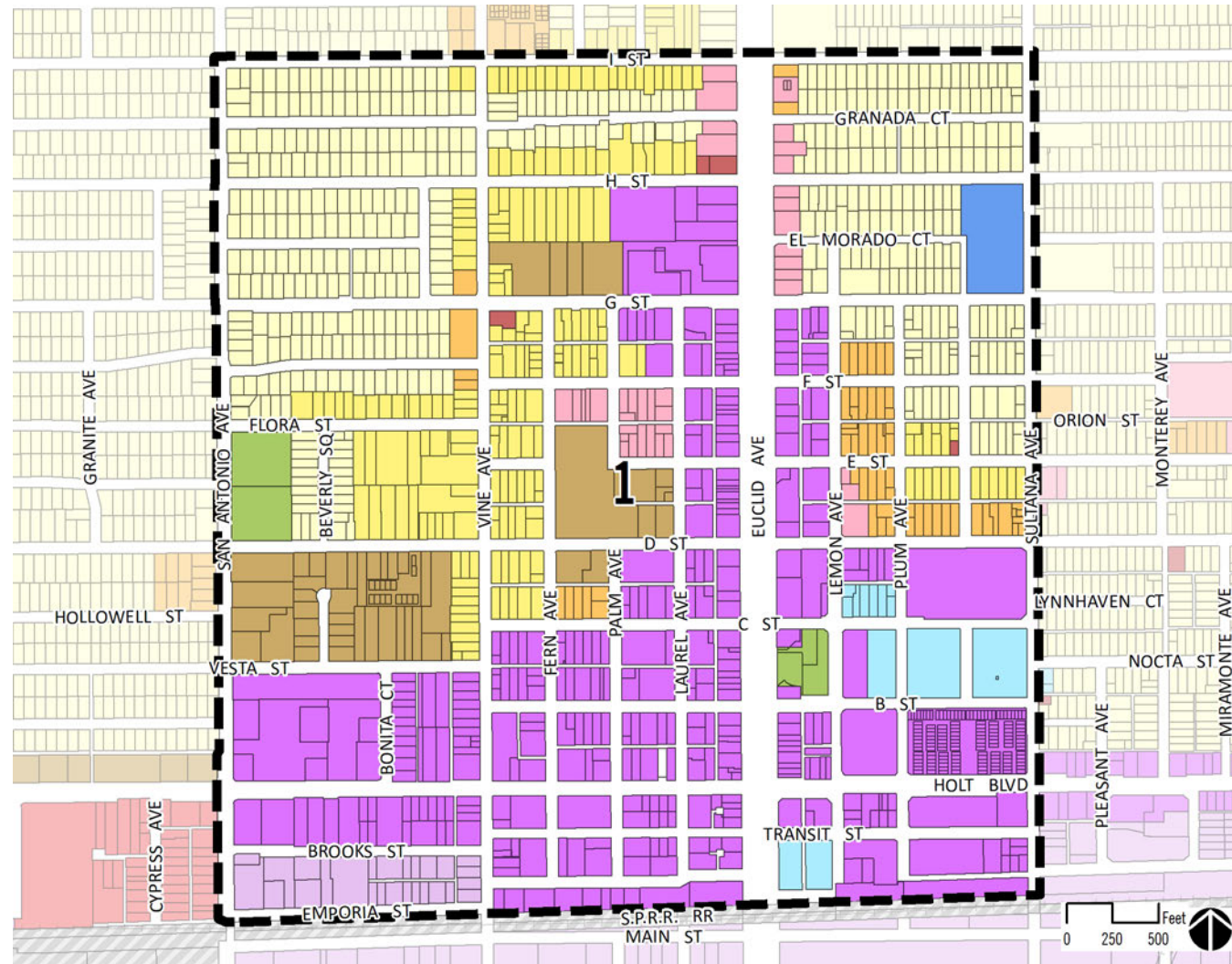
VISION

As the historic center of Ontario, the Downtown District, is envisioned as the placed-based, people-focused, commercial, and cultural "heart" of the City and features several designated historic districts and landmarks. Downtown will support a variety of businesses, housing opportunities for residents with a variety of income levels, creative spaces, entertainment options, and institutional and civic uses while preserving its historic character, buildings, neighborhoods, and places. Complementing the wide mix of uses are designs, layouts and public spaces that give residents, visitors, and businesses a strong sense of connection and creates a place where people want to spend time.



Conceptual reconfiguration of Euclid Ave (within existing curb-to-curb dimension) adding a Class IV bikeway, parking lane planters, and "off-peak" parking along the median to maintain three travel lanes during peak traffic volume times.

LAND USE



Land Use Designation	Residential Density & Non-Residential Intensity	Intention
Low Density	2.1 to 5.0 dwelling units per acre	Single-family detached residences.
Low-Medium Density	5.1 to 11.0 dwelling units per acre	Single/multi-family attached and detached residences, including small lot subdivisions, townhouses, and courtyard homes.
Medium Density	11.1 to 25.0 dwelling units per acre	Single/multi-family attached and detached residences including townhouses, courtyard homes, stacked flats, and small lot single-family subdivisions.
High Density	25.1 to 45.0 dwelling units per acre	Multi-family dwellings including stacked flats and mid-rise and high-rise residential complexes.
Mixed Use - Downtown	25.0 to 75.0 dwelling units per acre 2.0 FAR for retail and office uses	Envisioned as an intensive vertical and horizontal mixture of retail, office, and residential uses in a pedestrian friendly atmosphere. The historic character is enhanced. The most intensive uses are envisioned along Euclid and Holt Avenues.
Neighborhood Commercial	0.40 FAR	Local serving retail, personal service, office, and dining uses, typically located within a predominantly residential neighborhood.
Office Commercial	0.75 FAR	An intense mixture of professional office, supported by regional serving retail, service, tourist-serving, entertainment, dining, and supporting service uses that capitalize on strategic locations in Ontario. This designation also allows for professional offices such as financial, legal, insurance, medical, and other similar uses in a neighborhood setting and/or as adaptive reuse.
Industrial	0.55 FAR	Variety of light industrial uses, including warehousing / distribution, assembly, light manufacturing, research and development, storage, repair facilities, and supporting retail and professional office uses. This designation also accommodates activities that could potentially generate impacts, such as noise, dust, and other nuisances.
Open Space - Recreation	Not applicable	Open space that includes utility easements, and drainage channels. We desire to realize multiple uses from these open spaces, such as trails, greenways, joint-use recreational amenities, landscaped parkways/medians, parking lots, and nurseries.
Public Facility	Not applicable	Public facilities including civic centers, governmental institutions, police and fire stations, transportation facilities, museums, and public libraries.
Public School	Not applicable	Public schools (K-12) and universities.

DOWNTOWN DISTRICT INTENDED OUTCOMES

This section describes the intended result, or outcome, of applying Urban, Mixed Use, and Transit-oriented Place Type Policies (CD-3.1 to CD-3.6) to this Place Type.

Unique Identity



Downtown Ontario is the historic economic, cultural, and civic hub of Ontario, organized by a simple, walkable network of treelined streets, laid out as a new “Model Colony” in the 1880s.

The District’s history, historic buildings, and distinct urban form define downtown’s unique identity. These aspects should be preserved, celebrated, and enhanced to reinforce the area’s existing identity.

Comfortable, Safe, Human Scale Public Realm



The Downtown’s public realm is a traditional network of streets and public open spaces that are lined with a rich variety of buildings built since the 1880s.

Downtown’s streets, parks, and plazas should be enhanced as “outdoor rooms of community,” providing an attractive, comfortable, safe, and lively environment within which to walk, explore, shop, dine, meet friends and neighbors, and participate in community events. Each new public and private improvement in this area should reinforce a “pedestrians first” atmosphere.

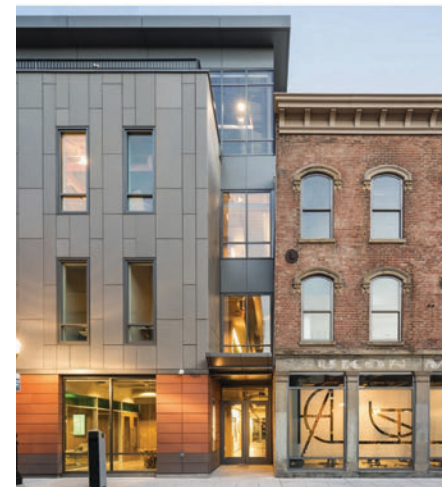
Complete and Connected Network



Downtown Ontario’s robust grid pattern network connects the retail core seamlessly to surrounding neighborhoods, Holt Boulevard, and the Ontario Amtrak station. The layout of street system provides each person with a wide range of choices including travel route and travel mode for each trip.

Downtown’s simple, open network of pedestrian-friendly streets and blocks - with perimeters typically less than 1,600 feet - is the ideal public realm framework for any urban, mixed use, or transit-oriented area, the existing block structure should be maintained and completed.

Context Aware and Appropriate Building Design



Within the Euclid Avenue retail core, buildings from various time periods comfortably mix with newer buildings, unified by a number of important shared characteristics including, 1) tall, clear glass ground floor shopfronts, 2) welcoming entries along the sidewalk that reflect original 25-foot wide lots, 3) upper floor windows that relate to ground floor articulation consistent with a historic main street pattern.

Future infill development and renovations should respect and reflect established facade patterns to preserve Downtown’s historic character. The architecture of new buildings should reflect the time in which they are built, while harmonizing with the scale, rhythm, and character of their historic predecessors.

Active Frontages



Within the retail core, the large clear glass shopfronts built right up to the sidewalk “blur the line” between the outdoor public realm and the indoor private businesses, prominently displaying wares and activities to passerby and in many cases allowing those activities - dining in particular - to spill out onto the sidewalks and into plazas and parks.

On side streets as one moves toward surrounding neighborhoods, ground floor offices and residences require some degree of privacy from passersby, buildings are set back from the sidewalk, reducing visibility into the residence or office, while offering views outward to the street and other public spaces, improving public safety and pedestrian comfort.

Managed Infrastructure



Successful downtowns require active and coordinated management of parking, public realm maintenance, special events, business recruitment, and on-going promotional activities to remain competitive with other downtown areas, lifestyle shopping centers, and “experience retail” destinations. Downtown Ontario and other regional downtowns must compete for customers, visitors, tenants, and events with successful, managed districts, so active, coordinated, management of the Downtown District is necessary to compete.

CD-04 Airport Metro / Entertainment District Place Type

2. Airport Metro / Entertainment District



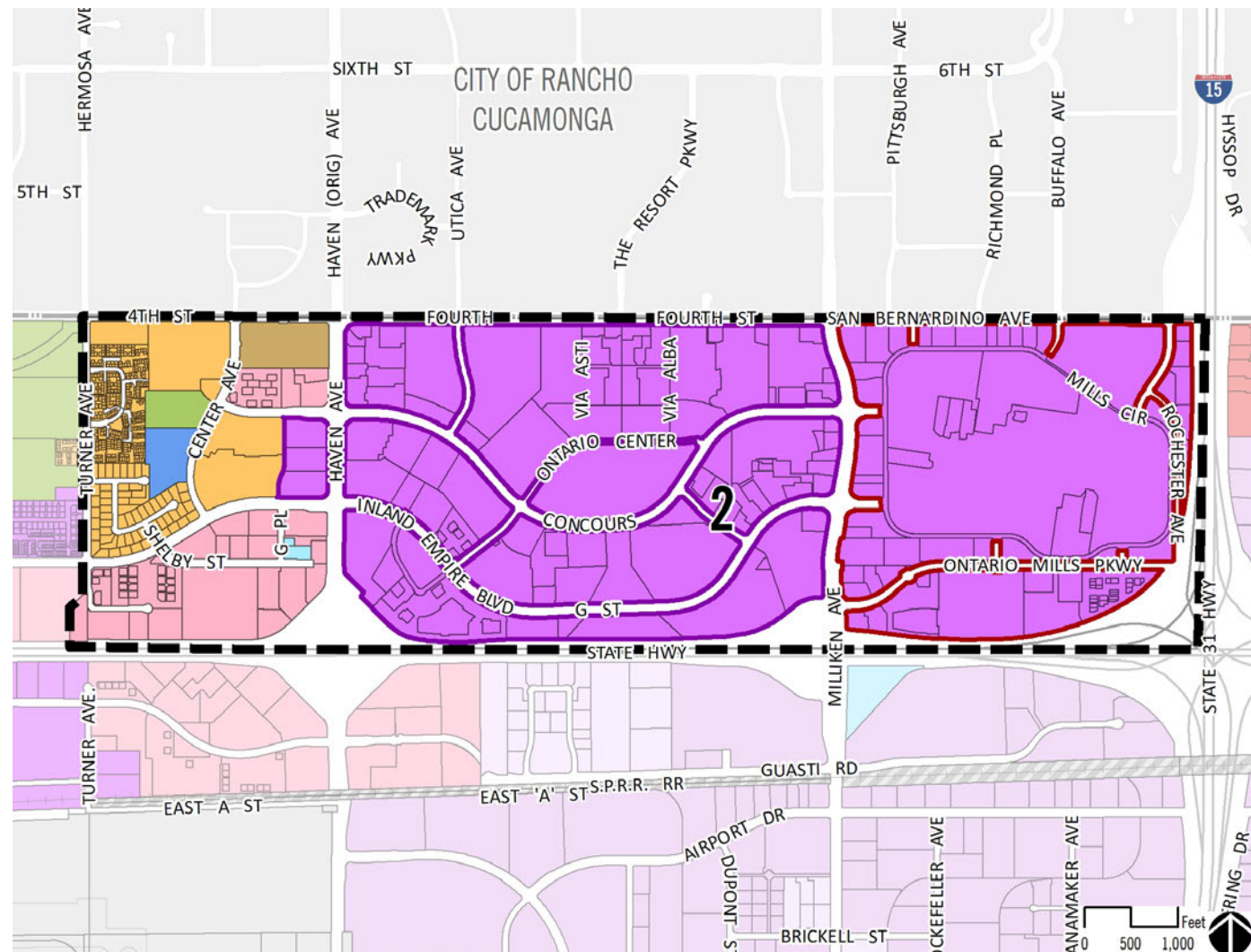
VISION

The Airport / Metro Entertainment District is envisioned as a large-scale regional entertainment hub, leveraging attractions, such as the Toyota Arena and Ontario Mills, supported by a variety of retail and dining options integrated alongside high-intensity office and residential development. This area will evolve into a walkable urban center through the addition of mixed use infill development, a pedestrian-oriented road and trail network, key public realm improvements, and multiple transit options.



Conceptual illustration showing mixed use infill development, streetscaping, and public realm improvements on Concourse Street, near the Toyota Arena.

LAND USE



Land Use Designation	Residential Density & Non-Residential Intensity	Intention
Medium Density	11.1 to 25.0 dwelling units per acre	Single/multi-family attached and detached residences including townhouses, courtyard homes, stacked flats, and small lot single-family subdivisions.
High Density	25.1 to 45.0 dwelling units per acre	Multi-family dwellings including stacked flats and mid-rise and high-rise residential complexes.
Mixed Use - Ontario Center	20.0 to 125.0 dwelling units per acre 2.0 FAR for office uses 1.0 FAR for retail uses	This area is one of the most intensive developments in Ontario and is characterized by low-rise (3-5 stories) and mid-rise (5-10 stories), mixed use buildings, iconic architecture, and regionally significant uses, such as the City owned arena and other cultural and entertainment uses. This area accommodates a vertical and horizontal mixture of entertainment, retail, office, and residential uses in an active, pedestrian oriented atmosphere. In this area, the Haven Corridor is envisioned as an elegant, landscaped boulevard lined multi-story office uses near the I-10 and mixed and residential uses closer to the City's northern boundary along 4th Street. There is an approved Specific Plan on this site that may require amendment to align with TOP.
Mixed Use - Ontario Mills	25.0 to 85.0 dwelling units per acre 1.5 FAR for office uses 1.0 FAR for retail uses	This area will continue to be our regional retail center. We envision intensification of the area to include additional retail and entertainment, office, and multi-family (3-5 story) residential uses. New development is envisioned to occur along the interior loop road and the perimeter of the area. There is an approved Specific Plan on this site that may require amendment to align with TOP.
Office Commercial	0.75 FAR	An intense mixture of professional office, supported by regional serving retail, service, tourist-serving, entertainment, dining, and supporting service uses that capitalize on strategic locations in Ontario. This designation also allows for professional offices such as financial, legal, insurance, medical, and other similar uses in a neighborhood setting and/or as adaptive reuse.
Open Space - Recreation	Not applicable	Open space that includes utility easements, and drainage channels. We desire to realize multiple uses from these open spaces, such as trails, greenways, joint-use recreational amenities, landscaped parkways/medians, parking lots, and nurseries.
Public Facility	Not applicable	Public facilities including civic centers, governmental institutions, police and fire stations, transportation facilities, museums, and public libraries.
Public School	Not applicable	Public schools (K-12) and universities.

AIRPORT METRO / ENTERTAINMENT DISTRICT INTENDED OUTCOMES

This section describes the intended result, or outcome, of applying Urban, Mixed Use, and Transit-oriented Place Type Policies (CD-3.1 to CD-3.6) to this Place Type.

Unique Identity



The Airport Metro/ Entertainment District is envisioned as a major urban hub with a significant concentration of jobs, retail, entertainment, and housing, organized around a pedestrian-oriented street pattern that provides a uniquely urban experience and a rich public realm.

Comfortable, Safe, Human Scale Public Realm



A walkable, urban-scale public realm that includes the street network and a series of connected public spaces is envisioned to create comfortable, human-scale, pedestrian-oriented public spaces.

To achieve this, as new infill development occurs, surface parking lots should be reorganized into smaller blocks by introducing new public and private streets that extend through parking areas to establish a walkable network.

New streets should include generous sidewalks, large shade trees, and new buildings should be sited and designed to form a strong “streetwall” of urban buildings that define the public spaces.

Complete and Connected Network



The existing street network should be reorganized by adding additional local roads (public and private) that incorporate linkages through existing parking lots to create a walkable block pattern with high-quality pedestrian, bicycle, and transit facilities.

In addition to adding more local streets, modifications to existing streets, such as new bicycle facilities, new curbside parking, new bus stops, and new shade trees should be considered to ensure multimodal connectivity, and create walkable, enjoyable urban spaces.

The District’s multimodal network of streets, trails, and transit options should link to nearby destinations, including the Ontario International Airport (ONT), Downtown and Guasti Districts, the multimodal transit center, and nearby Metrolink stations.

Context Aware and Appropriate Building Design



Just as existing major streets can be updated to radically improve walkability and urban character, many existing buildings can be updated to provide better street-facing frontages. New infill development, on the other hand, should be sited and designed to front sidewalk and other public spaces to establish “outdoor rooms” where people want to spend time.

The scale of buildings in this District is envisioned to range from 2 and 3 stories up to 10. Taller buildings should be sited and designed to shape the adjacent public spaces, and providing views of the San Bernardino Mountains and surrounding city from the windows, balconies and roof terraces.

Active Frontages



Commercial frontages in this District should be characterized by tall, clear glass ground floors filled with restaurants and bars, nightclubs and music venues, retail shops, restaurants, and offices. Many of the District’s streets – as well as other open spaces such as plazas, squares, courtyards, balconies, and roof terraces – will be activated by outdoor dining and other interior uses that spill into the street, creating a lively street life and blurring the line between outdoor and indoor uses.

Residential and office uses should overlook the public realm from upper floors, and on some local streets ground floor.

Managed Infrastructure



Active and coordinated management of the public realm will be critical to the success of the Entertainment District as it becomes an increasingly vibrant, active, and intense urban environment.

Shared parking, should be prioritized to efficiently utilize parking throughout the day, limit the amount of land needed to accommodate parking, encourage people to park once and walk to a variety of destinations within the District, and promote transit ridership.

In addition to shared infrastructure, shared maintenance and management of public open spaces throughout the District would ensure a cohesive, efficient, and coordinated series of public spaces.

CD-05 Guasti District Place Type

3. Guasti District



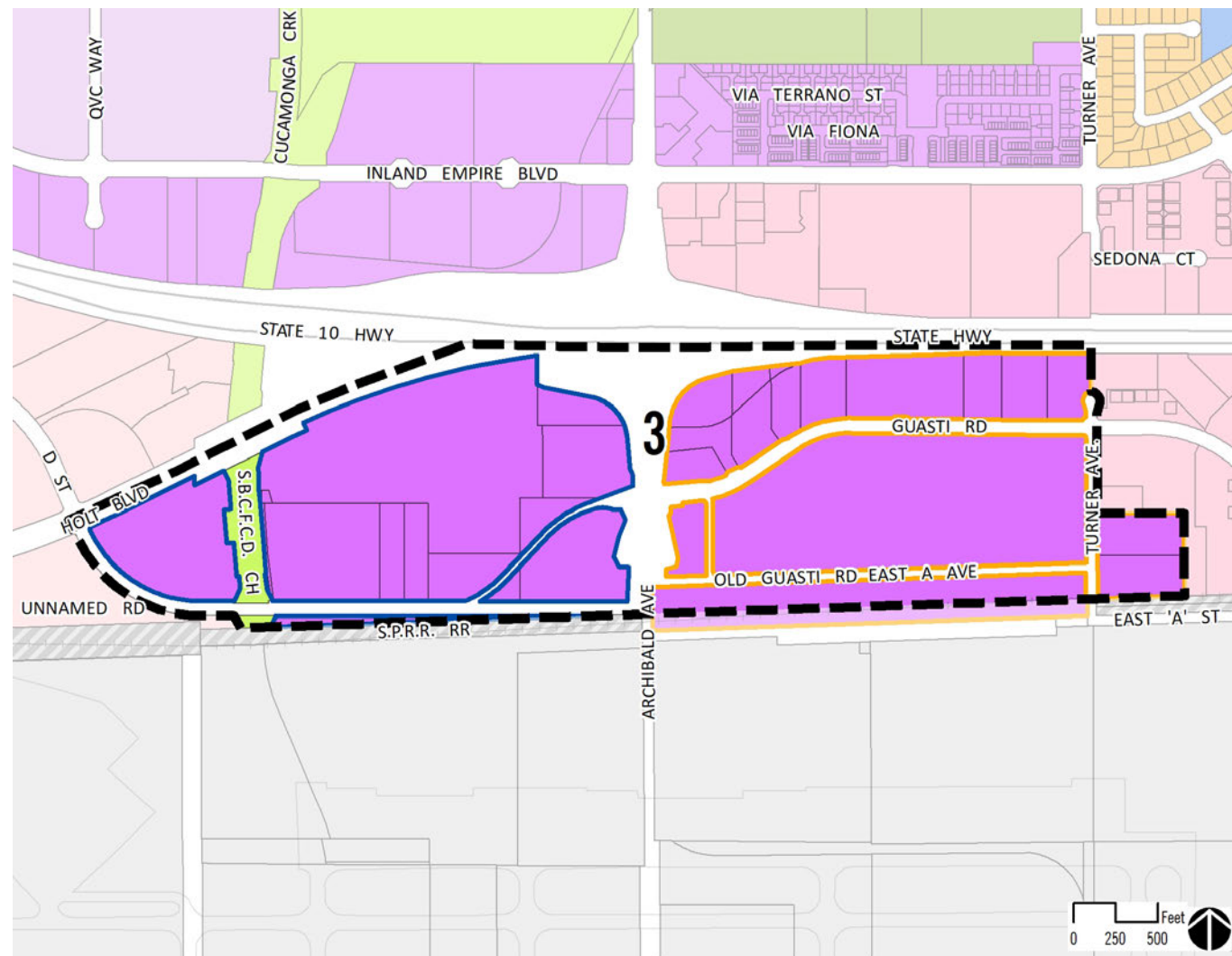
VISION

The historic Guasti (winery) Village sets the theme and backdrop for the Guasti District. The district is envisioned to preserve and enhance the historic buildings as the focal point of a vibrant mixed use area that leverages its proximity to the Ontario International Airport, Ontario Convention Center, and the potential future development of a multimodal transit center. The district will include intense office, commercial, and lodging uses along the I-10 corridor, and – appropriately-scaled residential, office, commercial, and lodging near the Guasti Village historic district core. The multimodal mixed use area within the Guasti District is intended to leverage proximity to the future multimodal transportation center and link the Convention Center, Guasti compound, and airport through strong physical and visual design.



◀ Guasti Village will be the heart of a unique retail/entertainment district and destination that is unlike any-thing else in the city or broader region.

LAND USE



Land Use Designation	Residential Density & Non-Residential Intensity	Intention
Mixed Use - Guasti	25.0 to 65.0 dwelling units per acre 1.0 FAR for office and retail uses	This site includes the Guasti Winery, which is on the National Register of Historic Places. This area is envisioned as a mixture of high-quality office, lodging, retail and residential uses that incorporate the Guasti Winery. More intensive office and commercial uses are envisioned along I-10 while office, commercial, and lodging uses are envisioned in and around the historic structures; the southern portion of the area is being considered as potential site for the multimodal transit center. There is an approved Specific Plan on this site that may require amendment to align with TOP.
Mixed Use - Multimodal	20.0 to 80.0 dwelling units per acre 1.0 FAR for office and retail uses	The Multimodal Mixed Use Area is under consideration for our future multimodal transit station that links rail, regional, local, and Airport transit. Intensive office, retail, and residential uses are envisioned to be integrated with the transit station, which is expected to be within the area or in close proximity. The transit center is envisioned as an iconic, convenient, and intuitively designed multimodal transportation center that serves the City of Ontario and the region at-large.
Open Space - Non-Rec	Not applicable	Open space that includes utility easements, and drainage channels. We desire to realize multiple uses from these open spaces, such as trails, greenways, joint-use recreational amenities, landscaped parkways/medians, parking lots, and nurseries.

GUASTI DISTRICT INTENDED OUTCOMES

This section describes the intended result, or outcome, of applying Urban, Mixed Use, and Transit-oriented Place Type Policies (CD-3.1 to CD-3.6) to this Place Type.

Unique Identity



The remaining historic buildings, structures, landscape, and spatial relationships that represent the viticulture company town of Guasti Village establish a clear and unique identity that will be preserved and celebrated as the area evolves into a unique urban hub that blends modern mixed use development, hospitality, jobs, housing, and transit access with the historic agrarian scale, character, and landscapes of the original Guasti (winery) Village.

Comfortable, Safe, Human Scale Public Realm



A new network of comfortable, human-scale, pedestrian-oriented public spaces that preserve and enhance the historic site is envisioned within this District.

To achieve this, a new network of pedestrian-oriented, shaded, treelined streets, and other public open spaces should be established. The design of the overall District should celebrate and connect the historic site to other points of interest such as the Ontario Convention Center, Ontario International Airport (ONT), and a planned multimodal transit center. Existing heritage trees should be utilized to offer shade, define the boundaries of public spaces, and provide wind-protection.

Complete and Connected Network



A walkable block structure, similar in scale to the Downtown District, is envisioned to create connections between the Ontario International Airport (ONT), the Ontario Convention Center, the Guasti site, higher intensity uses along the I-10 corridor, and the District's multimodal transit options including BRT service, Ontario Airport Loop, and creation of the city's multimodal transit center.

The street pattern should be augmented by a network of public spaces that blend the District's historical roots with more urban forms of land use.

Within this walkable block structure, some variation may occur. For example, some blocks may be dedicated entirely to surface or structured parking to accommodate the needs of regional-serving retail uses and businesses.

Context Aware and Appropriate Building Design



This District is located between the I-10 freeway, and ONT airport, with limited transitions between this area and other uses. As a result, context aware design should focus on providing appropriate transitions between the historic site within the District, the more intense uses along the I-10 corridor, and the hospitality uses near the Ontario Convention Center.

Buildings of all scales should be sited and designed to spatially define the network of local streets and open spaces.

Active Frontages



The array of uses envisioned for this District requires a flexible palette of frontage activation strategies.

Streetscapes and open spaces adjacent to office and employment uses should be activated by large main entries, lobbies, bike parking areas and employee break rooms and yards. Retail, restaurant, and entertainment uses should incorporate large windows that encourages uses to spill out into the street. Hotels or other hospitality uses should face the public realm with lobbies and restaurants, and housing should serve to activate some of the smaller streets, green spaces, and paseos by orienting lobbies, community rooms, terraces, courtyards, and balconies towards public spaces in a way that provides residents with both privacy and views of the streets and other public open spaces.

Managed Infrastructure



To reduce the land area devoted to parking cars to the practical minimum, it will be important that good transit service is provided to the District by the several lines currently in the planning stages, and that shared parking facilities are provided and actively managed.

Coordinated maintenance and management of public and common area landscaping and lighting within cohesive areas of the District should also be required.

CD-06 South Euclid District Place Type

4. South Euclid District



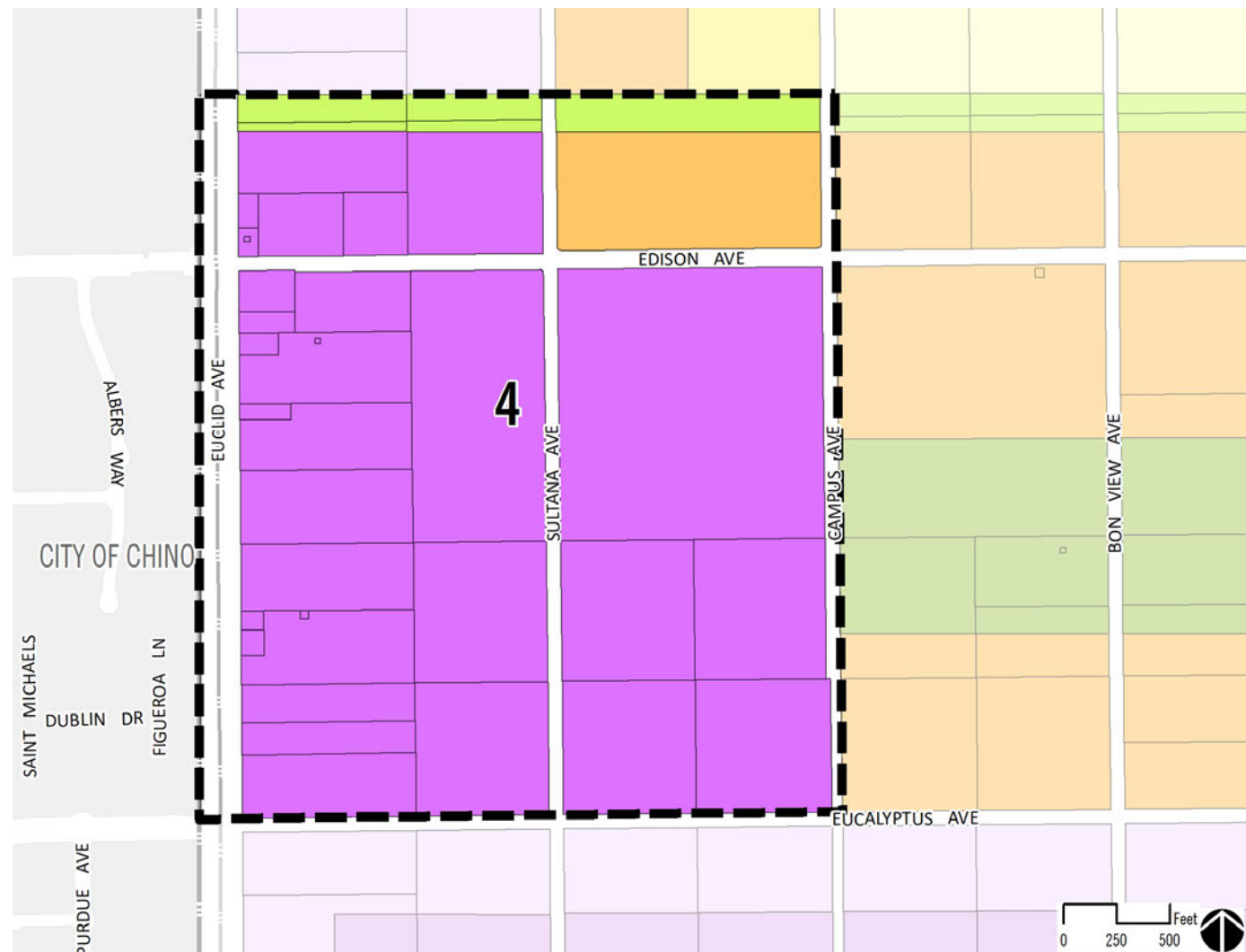
VISION

Lying at the southwest corner of the city, the South Euclid District is envisioned to develop as a vertically- and horizontally mixed use area to serve the populations of newly developing Ontario Ranch and adjoining communities. The district will include a range of housing types integrated within and alongside retail, commercial, and office uses, as well as public spaces and trails that connect the "Great Park" to Euclid Avenue. The area is intended to be highly walkable with pedestrian-oriented site design and road network and amenitized transit stops that leverage the Bus Rapid Transit (BRT) investments.



A new mixed use and lifestyle center within the South Euclid District would create a destination and amenity for neighborhoods in south Ontario - as well as surrounding neighborhoods in Chino, Eastvale, and beyond.

LAND USE



Land Use Designation	Residential Density & Non-Residential Intensity	Intention
Medium Density	20.1 to 25.0 dwelling units per acre ¹	Single/multi-family attached and detached residences including townhouses, courtyard homes, stacked flats, and small lot single-family subdivisions.
Mixed Use - Great Park	14.0 ¹ to 65.0 dwelling units per acre 1.5 FAR for office uses 1.0 FAR for retail uses <i>Subject to Specific Plan¹</i>	The Great Park Mixed Use Areas are envisioned as the southwestern-activity centers for citizens of Ontario. These areas accommodate a vertical and horizontal mixture of commercial, office, entertainment, and residential uses all connecting to the Great Park with a pedestrian oriented atmosphere. It is envisioned that the major roads through these Mixed Use areas are couplets, which are a series of one-way streets that disperse traffic and allow reduced street widths, maximize the sense of community, and emphasize pedestrian accessibility. These Mixed Use areas are envisioned as low-rise (3-5 stories) with some mid-rise (5-10 stories) near the intersection of Euclid and Edison/Ontario Ranch Road.
Open Space - Non-Rec	Not applicable	Open space that includes utility easements, and drainage channels. We desire to realize multiple uses from these open spaces, such as trails, greenways, joint-use recreational amenities, landscaped parkways/medians, parking lots, and nurseries.

Note:

- All medium density and mixed use parcels within the South Euclid District are within the affordable housing overlay zoning district. The overlay, 1) sets a minimum density of 20 dwelling units per acre; 2) waives the specific plan requirement if there is no existing specific plan and the proposed project includes 20 percent of units affordable to lower incomes, consistent with Tier 1 requirements outlined in the development code; and 3) allows a maximum density of 30 dwelling units per acre in the medium density land use category if the project includes 25 percent of units affordable to lower incomes, consistent with Tier 2 requirements outlined in the development code.

SOUTH EUCLID DISTRICT INTENDED OUTCOMES

This section describes the intended result, or outcome, of applying Urban, Mixed Use, and Transit-oriented Place Type Policies (CD-3.1 to CD-3.6) to this Place Type.

Unique Identity



The design character of the South Euclid District is envisioned to be varied, with a retail and civic core near Euclid Avenue that acts as a high-quality lifestyle center, with a highly walkable system of local roads, building and site design that enhance the pedestrian experience, and a connected network of outdoor spaces that link the larger open spaces of the “Great Park” and provide access to transit stops. The District will include a variety of attached and multifamily housing types in a very walkable, sustainable, and urban neighborhood setting.

Comfortable, Safe, Human Scale Public Realm



The public realm in the South Euclid District is envisioned as a walkable local street pattern that links a network of other public spaces, like plazas and parks, together, ultimately connecting to the “Great Park.” The public realm is defined by thoughtfully designed adjacent buildings and appropriate landscaping.

To achieve this, buildings should be sited to create inviting outdoor spaces, and facades should be articulated at regular intervals. Indoor uses should engage adjacent public spaces. Retail shops and restaurants should face wide, shaded sidewalks that provide opportunities for indoor uses, such as dining, to spill out into the street.

Whenever possible, commercial parking should be located in shared parking lots or structures within blocks behind shops and buildings, while residential parking should be alley-loaded.

Complete and Connected Network



The South Euclid District is envisioned to be organized around a simple connective block structure that is similar in scale to the Downtown District, with typical blocks that are approximately 1,600 feet in perimeter, a size that facilitates pedestrian activity.

The street pattern should logically and conveniently link to neighborhoods within and next to the District, and the public space network within the District should establish a strong connection to the “Great Park”, linking it to the District, Euclid Avenue, and transit stops along Ontario Ranch Road.

Some variation in the pattern may occur. Streets may be closed to vehicular access, with buildings fronting public spaces. Alternatively, blocks may be dedicated to surface or structured parking to serve retail and business uses. Maintaining safe and convenient pedestrian access should be prioritized.

Context Aware and Appropriate Building Design



Buildings should be designed and scaled relative to the spaces they front.

In commercial areas, buildings may include vertically mixed use projects with commercial on the ground floor, and housing or offices on the upper floors, stand-alone multifamily housing, or stand-alone office buildings. Less intense development is envisioned to the east along the “Great Park” frontage with, one- to three-story commercial, residential, and office uses that provide a strong presence and enhance the pedestrian environment.

Housing should reflect the scale, massing, and character of nearby buildings, with higher density apartments near the commercial core and around transit stops, transitioning to smaller scale multifamily and attached homes near adjacent neighborhoods.

Active Frontages



Within the commercial areas, ground floor building design should incorporate tall, clear shopfronts that blur the line between outdoor and indoor environments and allow for activities like outdoor dining to occupy adjacent sidewalks or other public spaces.

Residential buildings should be sited and designed to engage the street while maintaining privacy. Front doors and stoops should front the street and outdoor living spaces should overlook residential street and spaces.

Managed Infrastructure



Coordinated management of District-wide facilities and infrastructure will play an integral role in fostering a robust pedestrian atmosphere.

Shared parking strategies in the commercial core should be established to encourage visitors to park once and enjoy the area on foot, fully utilize available parking at all times of day, and provide flexibility to better adapt to changes in land use as well as changing trends in how people travel.

Shared maintenance should also be established to provide efficient, consistent, and sustainable maintenance and management of public spaces.

CD-07 South Employment District Place Type

5. South Employment District



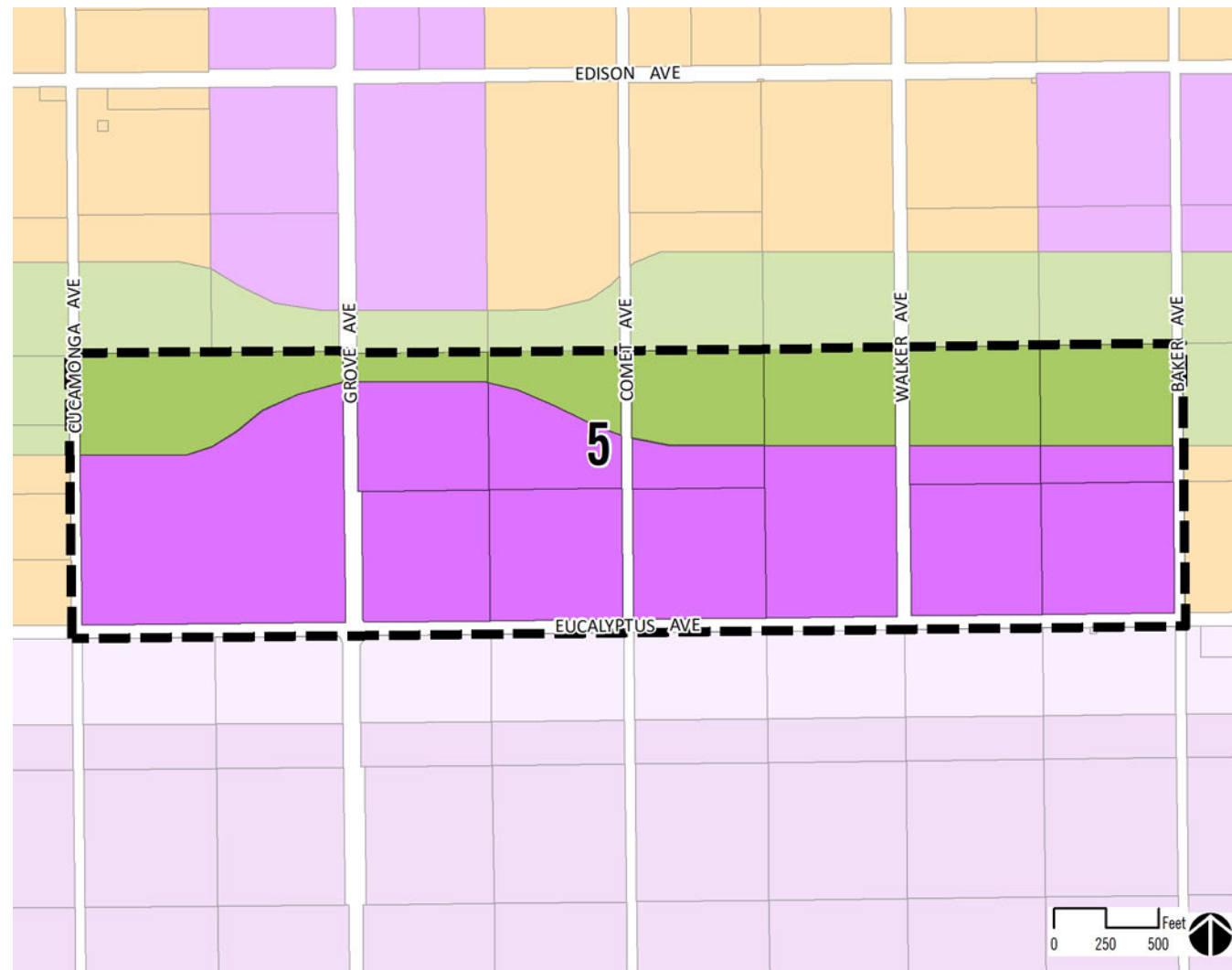
VISION

The South Employment District is envisioned to foster employee-intensive office, entertainment facilities, live/work, and supporting retail uses in a campus environment designed to leverage proximity to the Great Park and maintain compatibility with surrounding residential areas. Outside of the Chino Airport safety zone, higher density stand-alone and mixed use residential neighborhoods are intended to orient toward the Great Park and incorporate transition areas from the surrounding business park uses.



◀ The Great Park-inspired, campus-like setting of the South Employment District will attract a variety of employment generating uses.

LAND USE



Land Use Designation	Residential Density & Non-Residential Intensity	Intention
Mixed Use - Eucalyptus / Chino Airport Overlay	25.0 to 45.0 dwelling units per acre (outside of airport safety zone) 2.0 FAR for office and vertically-mixed uses 0.60 FAR for business park and retail uses <i>Subject to Specific Plan¹</i>	Envisioned to primarily accommodate employee-intensive office, entertainment facilities, live/work, and supporting retail uses in a campus environment designed to leverage proximity to the park and maintain compatibility with surrounding residential areas. Stand-alone and mixed use residential is permitted outside of the Chino Airport safety zone, primarily centered on Grove. Business park uses, such as research and development and “clean” industrial are also permitted provided they do not involve the frequent use of trucks (Class 4 or higher) as part of its primary activities.
Open Space - Recreation	Not applicable	Open space that includes utility easements, and drainage channels. We desire to realize multiple uses from these open spaces, such as trails, greenways, joint-use recreational amenities, landscaped parkways/medians, parking lots, and nurseries.

Note:

- All mixed use parcels within the South Employment District are within the affordable housing overlay zoning district. The overlay, 1) sets a minimum density of 20 dwelling units per acre; 2) waives the specific plan requirement if there is no existing specific plan and the proposed project includes 20 percent of units affordable to lower incomes, consistent with Tier 1 requirements outlined in the development code; and 3) allows a maximum density of 30 dwelling units per acre in the medium density land use category if the project includes 25 percent of units affordable to lower incomes, consistent with Tier 2 requirements outlined in the development code.

SOUTH EMPLOYMENT DISTRICT INTENDED OUTCOMES

This section describes the intended result, or outcome, of applying Urban, Mixed Use, and Transit-oriented Place Type Policies (CD-3.1 to CD-3.6) to this Place Type.

Unique Identity



The South Employment District is envisioned as an employment hub with integrated residential uses concentrated around Grove Avenue, and supporting commercial uses, set in a campus-like environment. Proximity to the “Great Park,” higher-density housing, and transit opportunities along Ontario Ranch Road will drive the success of this District as a pedestrian-first, transit-oriented campus.

A roadway between the mixed use area and the southern park boundary will provide public access to the “Great Park” from within the District, and attract visitors to the area outside of office hours.

Comfortable, Safe, Human Scale Public Realm



A complete network of complete streets will combine with Great Park access to form the foundation for establishing a comfortable, human-scale public realm in this District.

The “Great Park” will bring broad open spaces and citywide trail connections into this area, which should connect to the new network of streets.

The planned roadways in this area provide the foundation for a walkable network of streets and sidewalks, but an interconnected system of secondary local streets that link public spaces to the Great Park should be established.

Complete and Connected Network



The South Employment District is envisioned to be organized around a connective multimodal network in a campus-like environment.

Portions of the network will provide vehicular accesses, but circulation within the District will focus on providing pedestrian and bicycle connections between residences, employment centers, the “Great Park,” and transit stops along Ontario Ranch Road through a network of interconnected pedestrian malls, local streets, and other public spaces.

Context Aware and Appropriate Building Design



Buildings in this District are expected to range from 2 to 4-stories in height, with simple massing, and modern design.

Uses may include office, apartments, attached residential, vertically mixed use projects, buildings that mix offices with and supporting retail and commercial uses. “Clean industrial” and business park uses like R&D and maker spaces should be clustered near Eucalyptus Avenue, and buildings should have minimal dock doors.

To ensure compatibility within and adjacent to the District, housing should be located near the “Great Park.” oriented toward quieter local streets and open spaces, and screened from parking, loading areas, and noises produced by nearby uses.

Commercial uses should be concentrated at strategic locations where they are convenient to workers, residents, and visitors, accessible by all travel modes, and located between major destinations and transit stops.

Active Frontages



Buildings in this District are envisioned to have large footprints and simple massing. They should be designed to activate the public spaces by locating primary entrances, main lobbies, bike parking areas, employee lunchrooms, outdoor break areas, and other active uses along those frontages. Long walls should be articulated using appropriately placed windows, architectural details, and human-scale landscaping. Uses that benefit from limited fenestration should be located toward the rear of the building, while commercial, office, and residential uses should overlook adjacent public spaces.

Service areas like, trash enclosures, loading docks, and utility service entrances should be located to the rear of the buildings, along alleys, loading areas, and parking area.

Managed Infrastructure



Coordinated management of facilities and infrastructure will play an integral role in maintaining a robust district where people want to spend time.

Shared parking strategies should be established to encourage visitors to park once and enjoy the area on foot, fully utilize available parking at all times of day, and provide flexibility to better adapt to changes in land use as well as changing trends in how people travel.

Shared maintenance should also be established to provide efficient, consistent, and sustainable maintenance and management of public spaces.

CD-08 Corridor Place Types

6. East Holt Boulevard Corridor



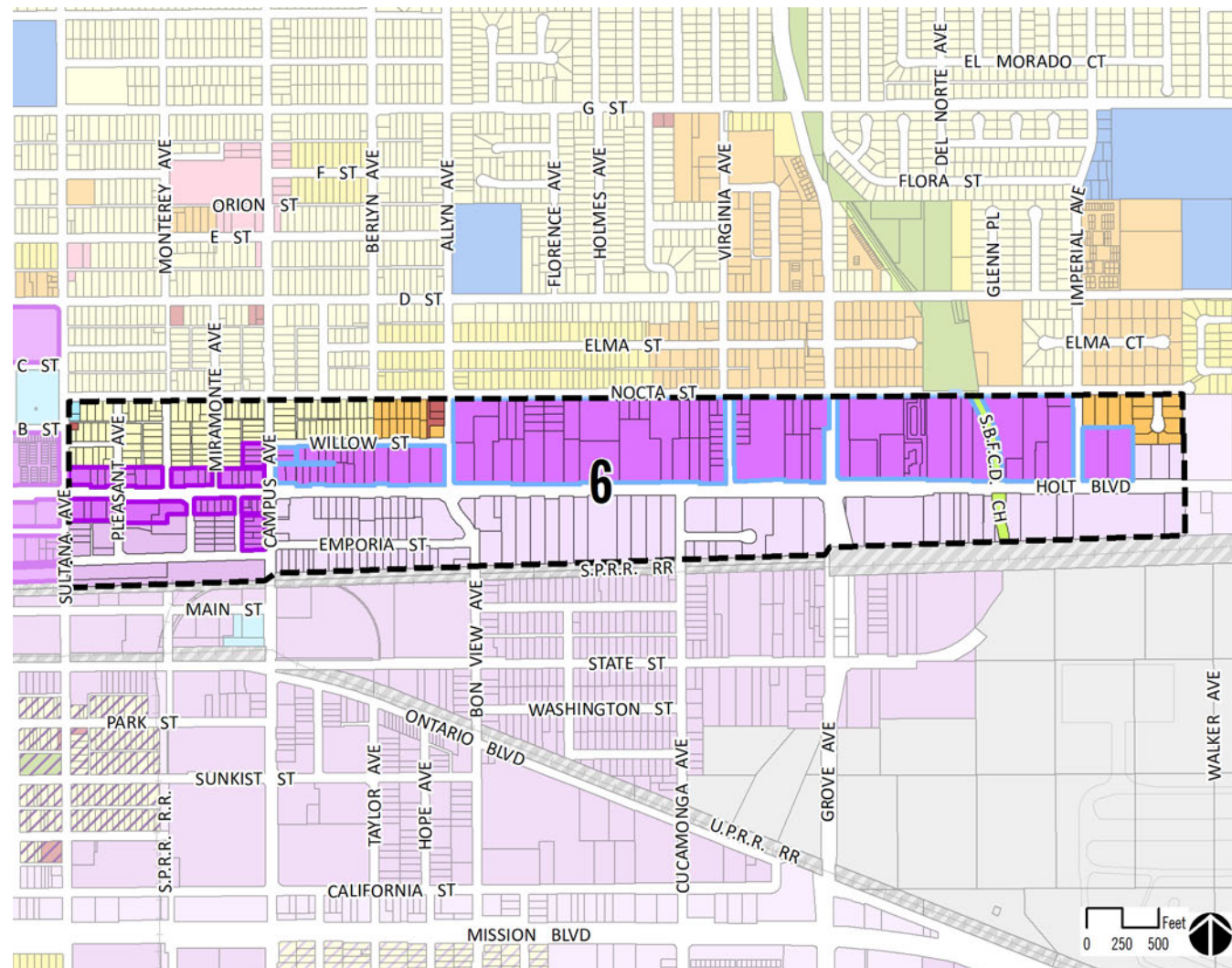
VISION

Holt Boulevard is an important east-west corridor connecting Ontario to the greater region, with improved connectivity options through the addition of BRT service. This corridor is envisioned to gradually build out over time through infill development that ultimately yields vibrant neighborhood-serving commercial areas, a variety of housing options, and employment opportunities that are clustered around BRT stops that can be easily accessed by pedestrians and bicyclists.



◀ Conceptual rendering showing how mixed use and business park infill development could create active frontages, a lively pedestrian experience, and new public spaces along Holt Boulevard.

LAND USE



Land Use Designation	Residential Density & Non-Residential Intensity	Intention
Low Density	2.1 to 5.0 dwelling units per acre	Single-family detached residences.
Medium Density	11.1 to 25.0 dwelling units per acre	Single/multi-family attached and detached residences including townhouses, courtyard homes, stacked flats, and small lot single-family subdivisions.
Mixed Use - Downtown ¹	25.0 to 75.0 dwelling units per acre 2.0 FAR for retail and office uses	Envisioned as an intensive vertical and horizontal mixture of retail, office, and residential uses in a pedestrian friendly atmosphere. The historic character is enhanced. The most intensive uses are envisioned along Euclid and Holt Avenues.
Mixed Use - Holt Blvd ¹	14.0 ² to 40.0 dwelling units per acre 2.0 FAR for office uses 1.0 FAR for retail uses	This area is envisioned as a low-rise (3-5 stories) intensification of the Holt Corridor. The intent is to create identity and place along the Holt Corridor, connect the corridor to Downtown, and connect the Downtown to the Ontario Airport Metro Center.
Neighborhood Commercial	0.40 FAR	Local serving retail, personal service, office, and dining uses, typically located within a predominantly residential neighborhood.
Business Park	0.60 FAR	Employee-intensive office uses including corporate offices, technology centers, research and development, "clean" industry, light manufacturing, and supporting retail within a business park setting.
Industrial	0.55 FAR	Variety of light industrial uses, including warehousing / distribution, assembly, light manufacturing, research and development, storage, repair facilities, and supporting retail and professional office uses. This designation also accommodates activities that could potentially generate impacts, such as noise, dust, and other nuisances.
Open Space - Non-Rec	Not applicable	Open space that includes utility easements, and drainage channels. We desire to realize multiple uses from these open spaces, such as trails, greenways, joint-use recreational amenities, landscaped parkways/medians, parking lots, and nurseries.
Public Facility	Not applicable	Public facilities including civic centers, governmental institutions, police and fire stations, transportation facilities, museums, and public libraries.

Notes:

1. Subject to PUD at City's discretion.
2. Some mixed use parcels within the Holt Boulevard Corridor are within the affordable housing overlay zoning district. The overlay sets a minimum density of 20 dwelling units per acre.

7. Inland Empire Boulevard Corridor

Corridor Key



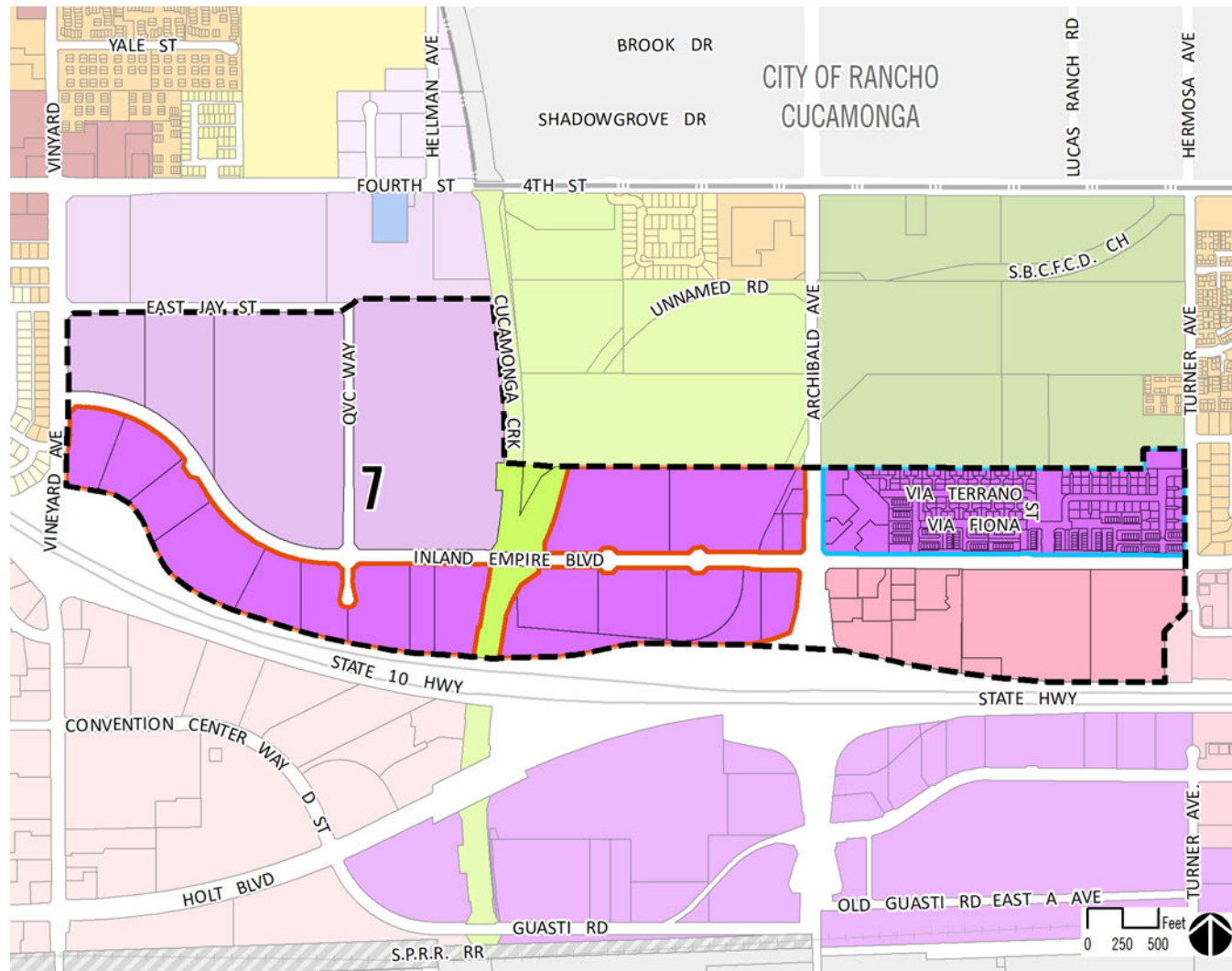
VISION

The Inland Empire Boulevard Corridor connects Vineyard Avenue and the Convention Center directly to the heart of the Airport / Metro Entertainment District and future BRT. Near- to mid-term improvements include the addition of significant rows of large street trees to provide shade, spatial definition, and a better pedestrian environment; the addition of new bicycle facilities, infill buildings oriented to the street, and reorientation of existing building entrances toward the street where possible. Over the longer term, this corridor has the potential to become a westward extension of the Airport / Metro Entertainment District environment.



◀ Aerial image of Inland Empire Boulevard Corridor looking East from Vineyard Avenue.

LAND USE



Land Use Designation	Residential Density & Non-Residential Intensity	Intention
Mixed Use - Meredith	14.0 to 125.0 dwelling units per acre 3.0 FAR for office and retail uses	This area is envisioned as a mixture of mid-rise buildings, regional-serving retail and office centers, and stand-alone high density residential projects.
Mixed Use - Inland Empire Corridor	14.0 to 30.0 dwelling units per acre 2.0 FAR for office uses 1.0 FAR for retail uses	Located along Inland Empire Boulevard, this area is primarily residential with a retail center at the corner of Inland Empire Boulevard and Archibald. Commercial uses that relate to the park are envisioned for the area West of the regional park.
Office Commercial	0.75 FAR	An intense mixture of professional office, supported by regional serving retail, service, tourist-serving, entertainment, dining, and supporting service uses that capitalize on strategic locations in Ontario. This designation also allows for professional offices such as financial, legal, insurance, medical, and other similar uses in a neighborhood setting and/or as adaptive reuse.
Industrial	0.55 FAR	Variety of light industrial uses, including warehousing / distribution, assembly, light manufacturing, research and development, storage, repair facilities, and supporting retail and professional office uses. This designation also accommodates activities that could potentially generate impacts, such as noise, dust, and other nuisances.
Open Space - Non-Rec	Not applicable	Open space that includes utility easements, and drainage channels. We desire to realize multiple uses from these open spaces, such as trails, greenways, joint-use recreational amenities, landscaped parkways/medians, parking lots, and nurseries.

CORRIDOR INTENDED OUTCOMES (ALL CORRIDORS)

This section describes the intended result, or outcome, of applying Urban, Mixed Use, and Transit-oriented Place Type Policies (CD-3.1 to CD-3.6) to this Place Type.

Unique Identity



Corridors present the opportunity to transform an underutilized transportation corridor into a distinctive urban place, with infill development including new housing, transit-accessible job centers, and inviting commercial areas.

Public spaces along the transportation corridors should be designed to allow for widened sidewalks, framed by street trees, and multi-story housing, offices, or mixed use buildings.

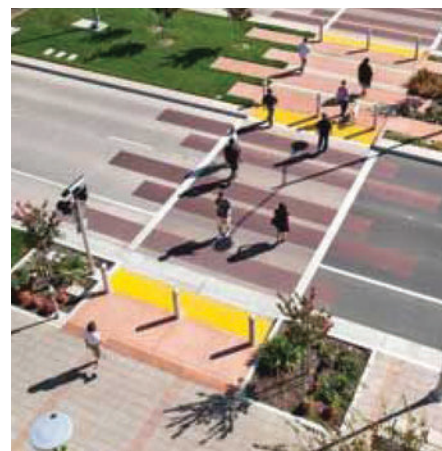
Comfortable, Safe, Human Scale Public Realm



The primary street, and important cross streets within a corridor should be designed to allow for wide, walkable sidewalks that provide access to new housing, offices shops and restaurants, and create a pedestrian experience that is attractive, enjoyable, and meets the daily needs of local residents, transit riders, workers and visitors.

The tree-lined corridor and landscaped cross streets should be augmented by a network of small plazas and public spaces that provide neighborhood gathering points, and ideal locations for transit stops.

Complete and Connected Network



Access to corridor parcels and flanking uses should be organized as regularly spaced multimodal streets, or access points. These intersections should be designed to ensure safe, comfortable, and convenient pedestrian and bicycle travel, with for low-speed vehicular access at limited intervals.

Vacant, underdeveloped, and underutilized parcels should be organized into new walkable blocks that link destinations within the corridor, such as commercial areas and transit stops to adjacent neighborhoods and employment centers.

Large blocks (greater than 500 feet) on corridors adjacent to residential neighborhoods or employment centers should establish mid-block pedestrian paseos to encourage people to walk and bike to destinations along the primary corridor street.

Context Aware and Appropriate Building Design



The linear nature of corridors focuses development and investments in relatively shallow infill sites, leaving little room to transition from an active mixed use environment to a quieter neighborhood. Parcels adjacent to residential uses should provide seamless transitions from more intense corridor development to less intense neighborhoods. Types of transitions will vary based on the type of adjacent uses, scale of buildings, and lot depth.

Appropriate transitions may include, but are not limited to, “step down” building massing that reduces height as it approached residential neighborhoods, landscaped setbacks, scaled down development overall, and interior green or public spaces that provide a buffer between uses.

Active Frontages



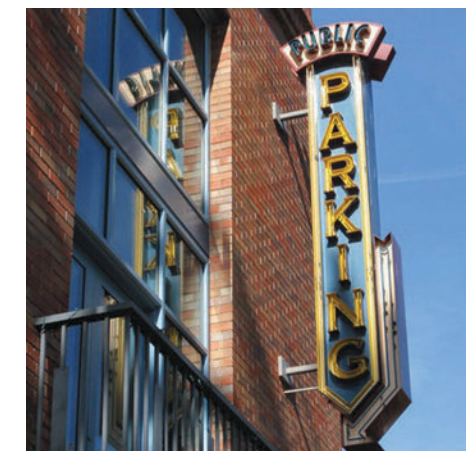
Frontages within corridors should enable and promote human activity in the public realm.

Retail and restaurant frontages should have tall shopfronts that face and wide shaded sidewalks.

Residential frontages should raise the ground floor units above the street to provide adequate privacy, but not so high that pedestrians must walk past a blank ground floor.

All buildings should also provide “eyes on the street” from both ground floor and upper floor windows, terraces, and balconies, to improve safety for transit riders and pedestrians.

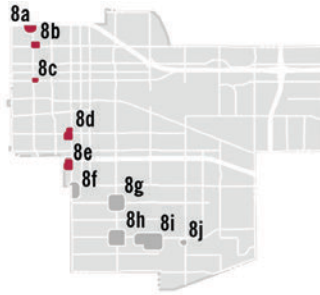
Policy CD-3.6 Managed Infrastructure



Segments of Urban Corridors with significant amounts of non-residential use are prime candidates for shared parking districts and maintenance districts.

CD-09 Neighborhood Center Place Types

8 (a-e) Neighborhood Center - Infill



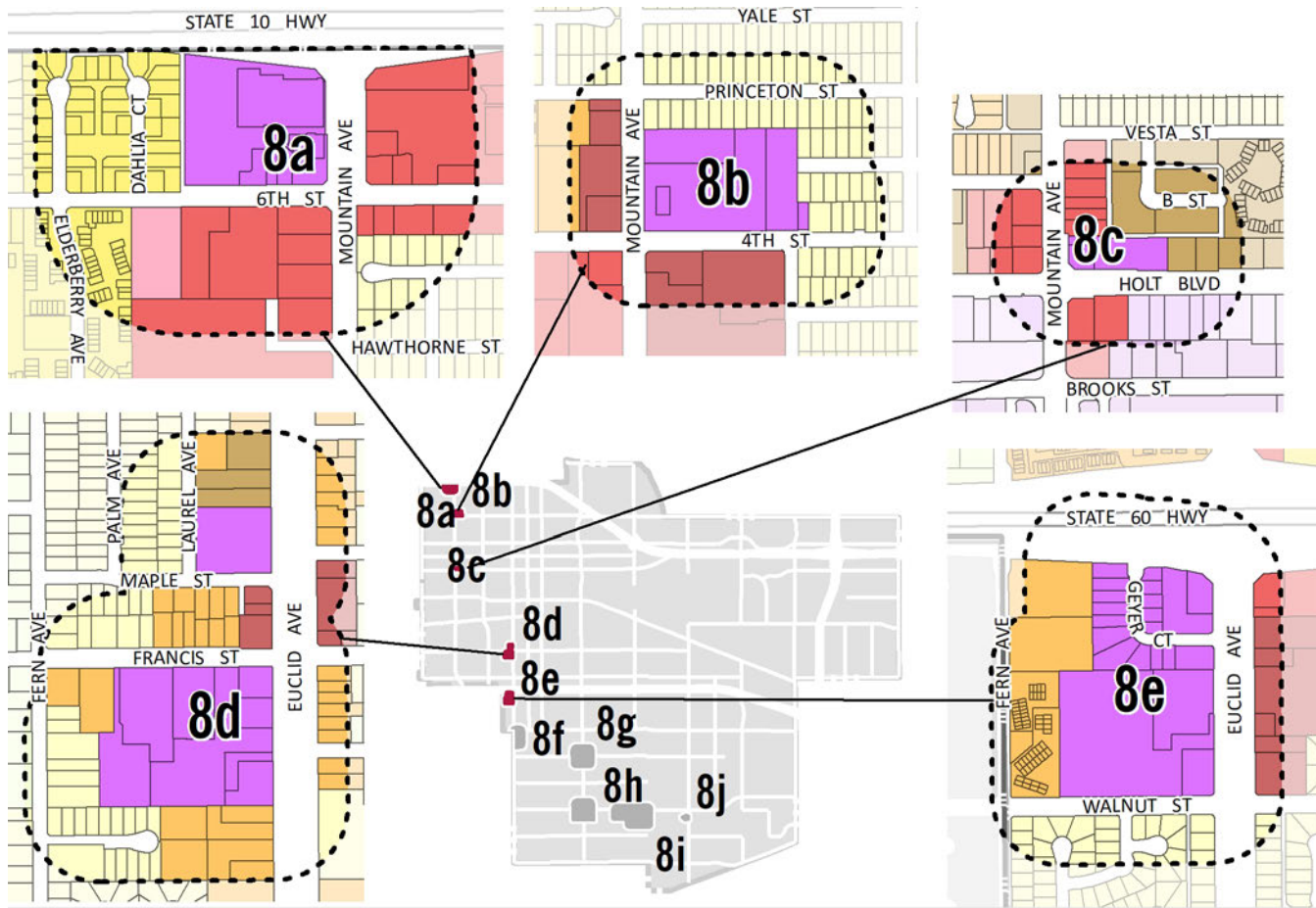
VISION

Neighborhood Centers located in areas where the commercial and residential parcels were largely developed in an incremental manner prior to 2020, are north of Riverside Drive, and are categorized as “infill” centers. These centers will range in size, complexity, and relationship to the surrounding streets and neighborhoods. Some are very small centers that may only include a horizontal mix of uses fronting onto a wide sidewalk while others may involve a vertically integrated mix of higher density residential and nonresidential uses oriented around a public plaza or other form of open space. All of these centers are envisioned to incorporate site design that prioritizes safe, and attractive, and well-shaded pedestrian access onsite, from the public rights-of-way, and from the adjacent neighborhoods.



◀ Mixed use Neighborhood Centers are intended to bring many daily needs and community amenities within easy reach of most residents.

LAND USE



*If any portion of a parcel is within the Place Type boundary, the entire parcel is subject to the Place Type vision and intended outcomes.

Land Use Designation	Residential Density & Non-Residential Intensity	Intention
Low Density	2.1 to 5.0 dwelling units per acre	Single-family detached residences.
Low-Medium Density	5.1 to 11.0 dwelling units per acre	Single/multi-family attached and detached residences, including small lot subdivisions, townhouses, and courtyard homes.
Medium Density	11.1 to 25.0 dwelling units per acre	Single/multi-family attached and detached residences including townhouses, courtyard homes, stacked flats, and small lot single-family subdivisions.
High Density	25.1 to 45.0 dwelling units per acre	Multi-family dwellings including stacked flats and mid-rise and high-rise residential complexes.
Mixed Use - Neighborhood Activity Hubs	20.0 to 75.0 dwelling units per acre 1.0 FAR for retail and office <i>Subject to PUD at City's discretion</i>	Envisioned as a low-rise (3-5 stories), mixture of retail and residential uses that will create identity and place along the corridor and serve the surrounding residents. [Link to Community Design Element <i>Urban, Mixed Use, and Transit-oriented Place Types</i> Section, Community Design Element <i>Exhibits CD-01, CD-02, and CD-09</i>]
Neighborhood Commercial	0.40 FAR	Local serving retail, personal service, office, and dining uses, typically located within a predominantly residential neighborhood.
General Commercial	0.40 FAR	Local and regional serving retail, personal service, entertainment, dining, office, tourist-serving, and related commercial uses.
Office Commercial	0.75 FAR	An intense mixture of professional office, supported by regional serving retail, service, tourist-serving, entertainment, dining, and supporting service uses that capitalize on strategic locations in Ontario. This designation also allows for professional offices such as financial, legal, insurance, medical, and other similar uses in a neighborhood setting and/or as adaptive reuse.
Business Park	0.60 FAR	Employee-intensive office uses including corporate offices, technology centers, research and development, “clean” industry, light manufacturing, and supporting retail within a business park setting.

8 (f-j) Neighborhood Center – South of Riverside Drive

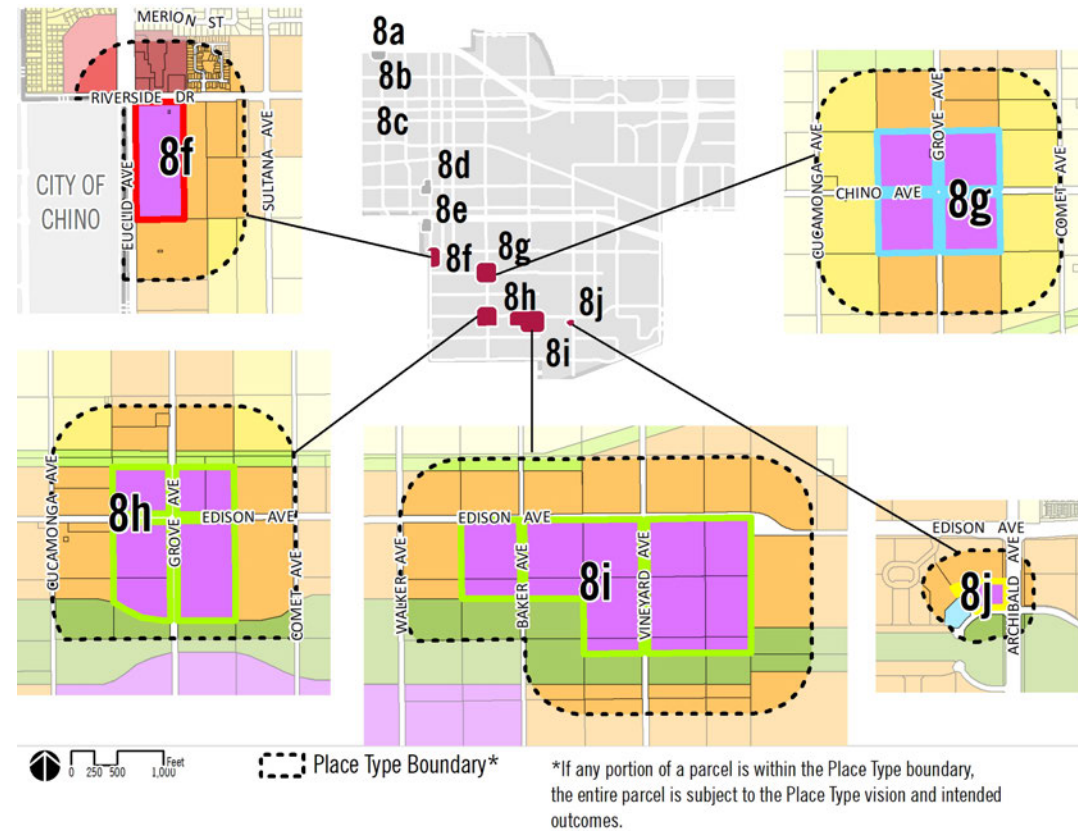
VISION

Neighborhood Centers identified south of Riverside Drive are generally underutilized or vacant as of 2022. These Neighborhood Centers are envisioned to bring new vitality and involve some level of master planning, with the roadway, pathways, open spaces, public realm designs, and site designs coordinated prior to the development of all of the parcels. These centers will range in size, complexity, and relationship to the surrounding streets and neighborhoods.

Some of these centers will involve a small node of mixed use development that is connected to the “Great Park” while others will involve larger clusters of mixed use areas that serve as a focus point for the surrounding residential neighborhoods. The largest of these centers are intended to serve not only as a local focal point but also as a major citywide landmark that also connects directly to the “Great Park”. All but the smallest of these centers are envisioned to consist of some amount of vertically integrated and higher intensity residential, office, and retail buildings oriented toward the “Great Park”, large public plazas, and/or large trail systems.

All of these centers are envisioned to incorporate site design that prioritizes safe, attractive, and well-shaded pedestrian access onsite, from the public rights-of-way, and from the adjacent neighborhoods. Parking areas are to be located next to and behind buildings to provide a variety of frontages along streets.

LAND USE



◀ Conceptual illustration showing a potential Neighborhood Center – South of Riverside Drive with wide sidewalks, buildings oriented toward the streets, and a variety of transportation options.

Land Use Designation	Residential Density & Non-Residential Intensity	Intention
Low-Medium Density	5.1 to 11.0 dwelling units per acre	Single/multi-family attached and detached residences, including small lot subdivisions, townhouses, and courtyard homes.
Medium Density	11.1 ¹ to 25.0 dwelling units per acre	Single/multi-family attached and detached residences including townhouses, courtyard homes, stacked flats, and small lot single-family subdivisions.
Mixed Use - Great Park	14.0 ¹ to 65.0 dwelling units per acre 1.5 FAR for office uses 1.0 FAR for retail uses <i>Subject to Specific Plan¹</i>	The Great Park Mixed Use Areas are envisioned as the southwestern activity centers for citizens of Ontario. These areas accommodate a vertical and horizontal mixture of commercial, office, entertainment, and residential uses all connecting to the Great Park with a pedestrian oriented atmosphere. It is envisioned that the major roads through these Mixed Use areas are couplets, which are a series of one-way streets that disperse traffic and allow reduced street widths, maximize the sense of community, and emphasize pedestrian accessibility. These Mixed Use areas are envisioned as low-rise (3-5 stories) with some mid-rise (5-10 stories) near the intersection of Euclid and Edison/Ontario Ranch Road.
Mixed Use - Grove	14.0 ¹ to 65.0 dwelling units per acre 1.5 FAR for office uses 1.0 FAR for retail uses <i>Subject to Specific Plan¹</i>	Envisioned as a low-rise (3-5 stories), mixture of retail and residential uses that will create identity and place along the corridor and serve the surrounding residents.
Mixed Use - Parkside	25.0 to 45.0 dwelling units per acre 1.0 FAR for retail uses <i>Subject to approved Specific Plan</i>	Envisioned as a low-rise (3-5 stories), mixture of retail and residential uses that will create identity and place and serve the surrounding residents.
Mixed Use Neighborhood Activity Hubs	20.0 to 75.0 dwelling units per acre 1.0 FAR for retail and office <i>Subject to Specific Plan¹</i>	Envisioned as a low-rise (3-5 stories), mixture of retail and residential uses that will create identity and place along the corridor and serve the surrounding residents.
Neighborhood Commercial	0.40 FAR	Local serving retail, personal service, office, and dining uses, typically located within a predominantly residential neighborhood.
General Commercial	0.40 FAR	Local and regional serving retail, personal service, entertainment, dining, office, tourist-serving, and related commercial uses.
Open Space - Recreation	Not applicable	Open space that includes utility easements, and drainage channels. We desire to realize multiple uses from these open spaces, such as trails, greenways, joint-use recreational amenities, landscaped parkways/medians, parking lots, and nurseries.
Open Space - Non-Rec	Not applicable	Open space that includes utility easements, and drainage channels. We desire to realize multiple uses from these open spaces, such as trails, greenways, joint-use recreational amenities, landscaped parkways/medians, parking lots, and nurseries.
Public Facility	Not applicable	Public facilities including civic centers, governmental institutions, police and fire stations, transportation facilities, museums, and public libraries.

Note:

- Some medium density and mixed use parcels in the Neighborhood Centers South of Riverside Drive are within the affordable housing overlay zoning district. The overlay, 1) sets a minimum density of 20 dwelling units per acre; 2) waives the specific plan requirement if there is no existing specific plan and the proposed project includes 20 percent of units affordable to lower incomes, consistent with Tier 1 requirements outlined in the development code; and 3) allows a maximum density of 30 dwelling units per acre in the medium density land use category if the project includes 25 percent of units affordable to lower incomes, consistent with Tier 2 requirements outlined in the development code. If not covered by a specific plan, projects are still subject to a PUD at the City's discretion.

NEIGHBORHOOD CENTERS INTENDED OUTCOMES (ALL CENTERS)

This section describes the intended result, or outcome, of applying Urban, Mixed Use, and Transit-oriented Place Type Policies (CD-3.1 to CD-3.6) to this Place Type.

Unique Identity



Neighborhood Centers are organized by a pattern of walkable blocks, tree-lined streets and varied architecture within a compact area. Each building, business, and use contributes to the Center’s unique character adding value to the Center and surrounding neighborhoods by its proximity and accessibility to them. These centers are connected to adjoining neighborhoods by a network of neighborhood streets and pedestrian connections, not separated from them by loading zones, trash enclosures and tall walls. Appropriate transitions from the busy environment of the commercial core of the center to the quiet neighborhood environments is achieved by gradual transitions in building scale and level of activity.

Comfortable, Safe, Human Scale Public Realm



The public realm of Neighborhood Centers includes streets connecting to neighborhoods, and parking lots and other publicly accessible open spaces. These interwoven open spaces meet the functional requirements for vehicular access and parking that are met in conventional shopping centers by large parking lots connected to large streets with auto-only driveways, and provide a comfortable retail experience for residents and visitors to shop, dine, and spend time with friends and family.

Complete and Connected Network



Centers are organized around a network of local streets, both public and private, that form walkable blocks. Neighborhood centers should be connected to larger cross-town streets and corridors by local cross-streets.

To encourage walking and to provide equitable access to goods and services for residents who are unable to drive, additional paseos and other “pedestrian shortcuts” should be provided when lots or blocks exceed approximately 500 feet in width.

Context Aware and Appropriate Building Design



Most Neighborhood Centers face on crosstown avenues and Neighborhood Corridors. They should front these streets with larger buildings and high levels of commercial activity. The height, size, and scale of buildings and levels of activity should systematically step downward toward adjoining neighborhoods. Building masses near single-family neighborhoods should be reduced in height, width and bulk, and activities should transition for very busy and active retail and restaurant uses to quieter office and residential uses.

Active Frontages



The ground floors of buildings - and the public spaces they front - should be designed to balance the needs of interior uses with the active public realm.

Retail and restaurant businesses near the main avenue and within the retail core of the center, should be adjacent to wide sidewalks, and building facades should include with tall, transparent shopfronts providing clear views of merchandise and activity within each business.

As uses transitions from the retail core to adjacent neighborhoods streetscape improvements and sidewalks should change with landscape pallet giving way to sidewalks behind green parkway strips. Similarly, deeper setbacks should mark the transition from Center to neighborhood.

Managed Infrastructure



Coordinated management of facilities and infrastructure will play an integral role in maintaining a successful Neighborhood Center where people want to spend time.

Shared parking strategies should be established to encourage visitors to park once and enjoy the area on foot, fully utilize available parking at all times of day, and provide flexibility to better adapt to changes in land use as well as changing trends in how people travel.

Shared maintenance should also be established to provide efficient, consistent, and sustainable maintenance and management of public spaces.

SOCIAL RESOURCES ELEMENT

Access to healthcare, nutrition, and healthy environments, quality education, diverse community services and facilities, and cultural opportunities are essential to a prosperous and complete community. All are critical to individual and community well-being and Ontario's ability to attract residents and investors. However, direct control over delivery of healthcare, education, social services, and cultural opportunities is in the hands of outside organizations. Therefore, the City must leverage the efforts of service providers within the region.

Purpose

The Social Resources Element:

- Identifies quality and accessible health care, education, community services, and cultural activities as critical components to achieving Ontario's Vision.
- Provides guidance on addressing community issues that by their nature require extensive coordination and collaboration with outside agencies.
- Addresses critical community issues that are not typically included in City policy plans.

Principles

The City believes:

- The health, wellbeing, education, and enrichment of its citizens are key components to making Ontario prosperous and a desirable place to live.
- Access to healthcare, education, community services, and cultural resources encourages community engagement and healthy lifestyle choices.
- Coordination and collaboration with service providers is essential to a comprehensive system of high quality health, education, cultural activities, and community services in Ontario.
- A diverse range of community and cultural resources, programs, and facilities help make Ontario a more complete and prosperous community.

Goals & Policies

SR-1 Health

Ontario recognizes that there is a strong link between health and individual and community prosperity. The City is committed to strengthening this critical link through collaboration with health-related public, private and nonprofit organizations. The City can have a positive impact

on the health of residents by providing information on and access to, resources for healthcare, fitness and good nutrition.

While the policy foundation for improving residents' health is found throughout the Policy Plan, this section focuses on the issues of access to healthcare providers and facilities, prevention and wellness, safe neighborhoods, and monitoring health in Ontario.

GOAL SR-1 Residents have access to information, services and goods that improve their health and wellbeing.

Policies

SR-1.1 Partnering for Healthcare. We work with healthcare providers, and local, regional, state, and federal agencies to attract and retain a diversity of affordable, quality healthcare and facilities for the entire community.

SR-1.2 Nutrition Choices. We support the promotion of equitable access to affordable healthy food choices in the community, including community gardens, farmers markets, and cooking classes.

SR-1.3 Health Education. We promote equitable access to health education, including disease prevention, mental health, nutrition, and physical fitness.

SR-1.4 Physical Activity. We encourage activities and community design that improve the physical fitness of our community members, with an emphasis on the provision of activities and facilities in environmental justice areas.

SR-2 Education

Ontario's prosperity is determined, to a great extent, by its ability to provide a skilled and educated workforce capable of meeting the requirements of business. A key factor for businesses and residents deciding whether or not to locate in Ontario is the availability of quality preschool, elementary, middle, and high schools, colleges, and vocational training. Ontario fosters lifelong learning so that its citizens can lead more fulfilling and productive lives by providing access to libraries, culture, community events, and activities.

GOAL SR-2 A range of educational and training opportunities for residents and workers of all ages and abilities that improves their life choices and provides a skilled workforce for our businesses.

Policies

SR-2.1 Educational Partners. We partner with educational institutions throughout the region in order to expand the range and quality of educational offerings available to the community.

SR-2.2 Workforce Training. We will work with industrial organizations, businesses, and educational institutions to create opportunities for workforce training.

- SR-2.3 Joint Use of Facilities.** We partner with public and private educational institutions to jointly use facilities for both City and educational purposes.
- SR-2.4 Access to Schools.** We work with local and regional partners to improve the safety in and around schools and to improve access for citizens of all ages and abilities to schools and community services, such as after school and other programs.
- SR-2.5 School Facilities.** We plan and coordinate with school districts for designing and locating school facilities to meet the City's goals, such as for health, walkability, and safety and to minimize impacts to existing neighborhoods.
- SR-2.6 Language.** We promote broad outreach in languages used by the community for proposed projects that could negatively affect environmental justice areas.
- SR-2.7 Community Engagement.** We promote targeted outreach and education to historically underrepresented groups to encourage meaningful participation in decision-making process for projects whose outcomes will affect land use in environmental justice areas.

SR-3 Community & Leisure Activities

Ontario takes pride in having an excellent choice of classes, excursions, events, and special programs for its citizens to learn, recreate, and socialize with friends, family, neighbors, and others in their community. In addition, there are a variety of nonprofit community-based organizations and religious institutions that provide services and activities to members of the Ontario community. Community and leisure activities play an important role in linking Ontarians to their community, neighborhoods, and commercial areas.

Goal SR-3 A range of community and leisure programs and activities provided by public, private, and nonprofit organizations that meet the needs of the community's varied interests, age groups and abilities.

Policies

- SR-3.1 Partnerships.** We partner with local and regional agencies, nonprofit organizations, and the private sector to provide a comprehensive range of community activities and events to citizens.
- SR-3.2 Needs Assessment.** We track the needs and priorities for community services and look for ways to meet demands and avoid duplication of offerings.
- SR-3.3 Program Outreach.** We promote information about leisure activities, classes, special events, and other services and activities to our community.
- SR-3.4 Community Events.** We plan and actively participate in regularly scheduled community events and seasonal or yearly citywide events.

SR-3.5 Community Activities as Crime Deterrents. We promote and participate in community activities as part of our crime prevention efforts. (Link to Safety Element Policy S-7.3)

SR-4 Library

Information and knowledge resources are fundamental for the residents of Ontario to live healthy and prosperous lives. Ontario's libraries offer free access to collections of books and other media, computers and the Internet, and a range of programs and resources, making them key focal points in the community. Ontario has an ambitious, well-established, and functional library program that addresses a broad range of ages, abilities, interests, cultural backgrounds, and income levels.

Goal SR-4 City libraries that connect community members of all ages and abilities to a broad range of programs, communication, and informational resources.

Policies

- SR-4.1 Community Needs.** We identify and monitor community needs for library services, technology, and facilities, and tailor them to effectively meet those needs.
- SR-4.2 Interagency Coordination.** We leverage relationships with outside agencies, educational institutions, and neighboring jurisdictions to share library resources to the benefit of Ontario residents.
- SR-4.3 Library Outreach.** We outreach to the community to increase the patronage of the library.
- SR-4.4 Coordination with Other Community Services.** We coordinate library programs with other recreational and community programs and facilities.
- SR-4.5 Focal Points of the Community.** We design and program Ontario's libraries as focal points of community engagement, including public outreach and community events.
- SR-4.6 Robert E. Ellingwood Model Colony History Room.** We work with the Ontario Museum of History and Art in order to collect, preserve, and display artifacts and images from Ontario's heritage and connect the City's past to the present through the History Room.

SR-5 Entertainment & Culture

The availability of entertainment and culture helps define and promote the City's image locally, regionally, and internationally. These social resources are necessary to become a complete community and are essential to attracting residents and investment. Ontario is committed to making entertainment, arts, and culture readily available to citizens of all ages. These resources promote opportunities for community engagement, personal inspiration, learning, and enjoyment.

Goal SR-5 Local heritage, entertainment, and cultural experiences that enrich the lives of Ontario’s residents, workers, and visitors and serve to attract residents and businesses to the City.

Policies

- SR-5.1** **Provision of Entertainment and Culture.** We support equitable access to a range of entertainment and cultural experiences such as public art, exhibitions, and performances.
- SR-5.2** **Local Heritage Education.** We partner with educational providers to promote culture and heritage. (Link to Community Design Element, *Historic Preservation* Section).
- SR-5.3** **Public Art.** We encourage public art in buildings, parks, open spaces, and other public and private spaces.
- SR-5.4** **Private-Public Sector Events.** We partner with private and nonprofit sectors to provide and promote participation in cultural activities including fairs, festivals, and other events geared to neighborhoods, the City as a whole, and the region.
- SR-5.5** **Promotion of Ontario Artists and Musicians.** We promote awareness of entertainment and culture produced in Ontario.

APPENDIX A. IMPLEMENTATION ACTIONS RELATED TO ENVIRONMENTAL JUSTICE AND CLIMATE ADAPTATION AND RESILIENCY

Purpose

The Policy Plan primarily contains long-term principles, goals, and policies for achieving Ontario's Vision through nine elements: land use, housing, parks and recreation, environmental resources, community economics, safety, mobility, community design, and social resources. Through these nine elements, the City addresses all the topics required by state law and some additional topics that are important to our community.

The City added the topics of environmental justice and climate adaptation and resiliency to the Policy Plan in accordance with state law (introduced through Senate Bills 1000 and 379, respectively). The City has opted to address these topics throughout multiple elements of the Policy Plan to ensure the topics are presented alongside the multitude of issues and topics that affect our residents and resources.

The City will begin to implement the updated Policy Plan immediately upon adoption, working within the municipal organization, with residents and other community stakeholders, and other agencies and service entities. A number of actions will be started immediately following the Policy Plan's adoption, with some completed and/or replaced by new actions in as little as a few months. As state law currently limits the frequency of updating the Policy Plan to four update cycles per year, the City opts to maintain a separate Implementation Plan so that it can update its list of short-term activities on an as-needed basis without affecting the City's Policy Plan update cycle.

However, state law requires that the implementation actions related to environmental justice and climate adaptation and resiliency be incorporated into the City's Policy Plan. Accordingly, this appendix contains those actions from the Implementation Plan that must also be part of the Policy Plan.

The Implementation Plan reflects new activities that the City should conduct in order to implement the new aspects of the updated Policy Plan. The Implementation Plan does not include actions that the City already undertakes based on regulations, programs, or procedures that are already in place. As the majority of updates to the Policy Plan relate to the topics of environmental justice and climate adaptation and resiliency, the majority of the Implementation Plan also reflects actions related to these two topics.

Updates

Future updates to the Implementation Plan will gradually incorporate and identify new activities related to other topics of the Policy Plan, based on changes in socioeconomic conditions, the physical environment, and other factors. Changes to actions listed in the Implementation Plan related to environmental justice or climate adaptation and resiliency will also trigger an update to this appendix, which should be processed during the City's next appropriate Policy Plan update cycle.

Structure

The following actions are presented based on the most directly relevant element and goal. For example, Action LU-2.1 is the first action related to Goal 2 of the Land Use Element. It should be noted that actions may also relate to more than one element and may help implement more than one goal and/or policy.

After each action, in parentheses, are indicators that identify whether the action relates to environmental justice (**EJ**) or climate adaptation and resiliency (**CAR**). Actions related to environmental justice are distributed throughout all elements of the Policy Plan, with additional indicators in parentheses to identify the subtopic enumerated in state law.

Environmental justice indicators:

- Reduce exposure to pollution (EP)
- Promotion of food access (FA)
- Promotion of physical activity (PA)
- Promotion of public facilities (PF)
- Promotion of safe and sanitary housing (H)
- Promotion of civil engagement (CE)

All of the climate adaptation and resiliency actions are associated with are associated with the Safety Element. Some actions related to climate adaptation and resiliency may address environmental justice topics and help those who are disadvantaged, but are not identified as environmental justice actions. For example, S-9.3 Weatherization Program, seeks to expand funding resources to conduct weatherization and structural retrofits that will increase community resilience. This action can help create safe and sanitary housing for those who are disadvantaged, but the action is not targeted toward improving an existing environmental justice problem or prioritized for those who live in environmental justice areas.

Actions

Land Use Element

LU-2.1 **Development standards.** Review existing development and design standards and update as necessary to provide appropriate mitigation or buffers between existing uses, with a focus on additional buffering when new uses could negatively impact environmental justice areas. (**EJ**: EP)

Housing Element

(Note that actions listed as programs in the Housing Element, full text available in the Housing Element Technical Report):

Program 6 **Neighborhood Stabilization.** Implements the City's Neighborhood Preservation Strategy Plan, which identifies revitalization strategies in key neighborhoods in northwest Ontario. (**EJ**: H)

- Program 11 Ontario Ranch.** Encourages development of affordable housing in future developments in Ontario Ranch, the high resource area in the city. (EJ: H, PF)
- Program 23 Public Housing.** Commits to expanding the use of Housing Choice Vouchers (aka Section 8 Vouchers) in high and moderate resource areas. (EJ: H, PF)
- Program 24 Homeownership Program.** Provide resources in multiple languages to reduce language barriers. (EJ: H, CE)
- Program 27 Fair Housing Implementation.** Provide resources in multiple languages to reduce language barriers. (EJ: H, CE)

Parks and Recreation Element

- PR-1.1 Recreation and Parks Master Plan.** Implement recommendations as outlined in the 2021 Master Plan, with a priority on addressing the needs in environmental justice areas. This includes recommendations such as new and/or expanded facilities, new and expanded programs and services, community and agency partnerships, and fee structures. (EJ: PF, PA)

Environmental Resources Element

- ER-4.1 Citywide and regional air quality planning.** Review existing and monitor the development of new air monitoring and emissions reduction plans prepared by the South Coast Air Quality Management District. Gather and evaluate measures and strategies in such plans for their applicability to and feasibility for Ontario. (EJ: EP)
- ER-4.2 Citywide emissions monitoring.** Coordinate with the South Coast Air Quality Management District (SCAQMD) to monitor existing air measurements and recommend new air measurements and locations, with a specific focus on evaluating new measurements and locations in environmental justice areas. Coordinate with SCAQMD to provide monitoring information online to the public, including historical data and an explanation of trends, thresholds, and ongoing monitoring results. (EJ: EP)
- ER-4.3 Community air quality planning.** Engage with the South Coast Air Quality Management District and local stakeholders to evaluate the next steps in pursuing a priority community designation and/or community air protection program incentives for eligible environmental justice areas of the city, with focus on areas with unique needs and highest pollution burden as identified in the latest available CalEnviroScreen tool. If such a designation or incentives are not awarded or available, seek grant funds for activities such as local air quality monitoring.
- Also, explore ways to initiate data collection efforts for a community emissions reduction and/or community air monitoring plan, including the identification of information needed (new or updated), potential data sources and needed resources, and strategies to engage residents and collect information. (EJ: EP)

- ER-4.4 Health risk assessments.** Draft and adopt ordinance to implement new requirements (Policy ER-4.9) for health risk assessments when considering specific uses near existing sensitive receptors. (EJ: EP)
- ER-4.5 Trucks and cargo handling equipment.** Evaluate and implement strategies to reduce emissions associated with truck idling and cargo handling equipment near areas with existing and planned sensitive receptors, with a priority placed on facilities that have not yet finalized building permits and for those facilities in or adjacent to environmental justice areas. (EJ: EP)
- ER-4.6 Healthy indoor options.** Explore options to incentivize or require the provision of indoor recreation space, particularly in environmental justice areas that experience high levels of exposure to air pollution. (EJ: PA, PF, EP)
- ER-4.7 Proactive engagement.** Collaborate with the South Coast Air Quality Management District and local stakeholders in environmental justice areas experiencing local air pollution issues to outline objectives and strategies for monitoring air pollution. Augment existing outreach programs to improve public awareness of state, regional and local agencies' roles, and resources to identify, monitor, and address air quality and other environmental hazards in the community. (EJ: CE, EP)

Community Economics Element

- CE-1.1 Small Business Assistance.** Collaborate with the County Economic Development Agency and California District of the Small Business Administration to expand and promote resources and assistance for small businesses, with a specific focus on those in environmental justice areas. (EJ: PF)

Safety Element

- S-1.1 Seismic Retrofits.** Identify potential funding sources to facilitate seismic retrofits in environmental justice areas and for low-income households. (CAR, EJ: H)
- S-2.1 Entitlement and Permitting.** Update the City's entitlement and permitting process to require hydrological studies that assess the 100-year and 500-year flood zones to assess the impact that the new development will have on the flooding potential of existing development down-gradient. Additionally update the entitlement and permitting process to require mapping for 200-year floodplains. (CAR)
- S-2.2 Floodplain Mapping.** Seek grant funds to map the 200-year floodplain in Ontario outside of the entitlement process. (CAR)
- S-2.3 Public Outreach.** Augment the City's public outreach efforts to disseminate information on flooding, flood control on private property, floodplains, and flood preparedness to the community through the City website, social media, and at City offices. (CAR)

- S-2.4 Floodproofing Existing Buildings.** Seek grant funds to encourage property owners in 200-year (when mapped) or 500-year flood zones to floodproof existing structures. (CAR)
- S-2.5 Open space in 200-year floodplains.** Coordinate with developers to encourage passive recreation or parkland in 200-year floodplains or floodways (when mapped). (CAR)
- S-2.6 Green and Natural Infrastructure.** Coordinate with developers to incorporate the use of existing (or restoration of) natural features and ecosystem processes and low impact development techniques to increase permeable surfaces and promote sustainable management of open space and parks. This may include, but is not limited to, aquatic or terrestrial vegetated open space, systems and practices that use or mimic natural processes, and other engineered systems, to provide clean water, conserve ecosystem values and functions, promote drainage, and provide a wide array of benefits to people and wildlife. (CAR)
- S-3.1 Firefighting services and water supply.** Collaborate with regional water providers and surrounding fire protection agencies to ensure adequate water supply, equipment, and personnel for firefighting in the region given future projections regarding prolonged drought and the potential increase in the number and severity of wildfires in the surrounding area (in line with automatic and mutual aid agreements). (CAR)
- S-3.2 Public Outreach.** Coordinate with network providers to ensure that residents and visitors, especially those in environmental justice communities, have access to emergency notifications. Information should be provided in multiple languages and formats appropriate for people with access and functional needs. (CAR, EJ: CE)
- S-4.1 Vibration studies.** Update development regulations to require vibration-sensitive uses in areas within 200 feet of rail to evaluate for indoor vibration levels and mitigate any exceedances of the Federal Transit Administration vibration-annoyance criteria. (EJ: H)
- S-8.1 Evacuation Assessment Plan.** Prior to or concurrently with the update of the City's Hazard Mitigation Plan, conduct an evacuation time assessment to comply with state law (enacted through Assembly Bill 747), accounting for natural and human-generated hazards, existing and proposed traffic evacuation volumes at buildout. (CAR)
- S-8.2 Evacuation Routes.** Review and update, as necessary, standards so that new development has at least two egress/ingress options, key signage that is visible to emergency responders during extreme weather events, signage that identifies evacuation routes, and adequate water supply for structural suppression. (CAR)

- S-8.3 Evacuation assistance program.** Coordinate with Omnitrans, Metrolink, and other transit providers to develop an evacuation assistance program for those with little mobility and those without a vehicle. (CAR)
- S-8.4 Extreme Heat Response Plan.** Develop an extreme heat response plan that includes establishment of community cooling centers and temperature triggers for when they will open, weatherization of City buildings, and cooling strategies for persons engaged in outdoor work and persons experiencing homelessness. (CAR)
- S-8.5 Resilience Hub and Cooling Centers.** Establish a network of equitably located resilience hubs and cooling centers throughout the City. Ensure that such facilities are located outside of areas at risk of hazards (to the extent possible), offer refuge from extreme heat and poor air quality, and are equipped with renewable energy generation, backup power, and backup water supplies. Such facilities should be in easily accessible locations and be available to all community members. (CAR)
- S-8.6 Cooling Transit Areas.** Coordinate with Omnitrans, Metrolink, and Amtrak to increase shading and heat-mitigating materials on pedestrian walkways, outdoor waiting areas, and transit stops/stations. (CAR)
- S-8.7 Critical Facilities.** Review procedures and update as necessary, to ensure that future critical facilities are located outside of hazards zones. If not feasible, ensure new critical facilities are designed to remain functional during hazardous events and identify funding sources to harden and improve existing facilities to remain functional during hazardous events. (CAR)
- S-8.8 Community Emergency Response Team (CERT).** Increase participation in CERT through program by expanding promotion efforts and course offerings, encouraging CERT participation for City employees, and exploring partnerships (e.g., partner with school districts to offer CERT training to high school students). (CAR)
- S-9.1 Energy independence.** Coordinate with developers to encourage the integration of battery storage systems that can provide backup electrical service during temporary power outages. (CAR)
- S-9.2 Renewable Energy.** Renovate existing city-owned assets and design future city facilities to incorporate renewable energy generation systems, battery storage systems, and energy-efficient design and features, as feasible. (CAR)
- S-9.3 Weatherization Program.** Expand funding resources through partnerships and grant programs for low-income households and businesses to conduct weatherization and structural retrofits that will increase community resilience. (CAR)

Mobility Element

- M-2.1 Priority Improvements.** Refine the City's Capital Improvement Program (CIP) and implementation recommendations of the Active Transportation Master Plan (ATMP) to elevate the priority of improvements proposed in (or serving) environmental justice areas. Continue to identify additional improvements that should be added to complete networks, remove barriers, and create buffers for pedestrians and bicyclists along truck routes, with priority given to those in environmental justice areas. (EJ: PF, PA)
- M-3.1 Expanded Bus and Light Rail Service.** Coordinate with Omnitrans and Metrolink to implement and update the agencies' strategic plans and long range transportation plans to prioritize improvements in and expansion of service in Ontario's environmental justice areas. In the next update of these plans, encourage each agency to increase the weighting of environmental justice factors in the prioritization of improvements and service expansion. (EJ: PF)
- M-4.1 Clean Energy Plan for Trucks and Cargo Handling Equipment.** Develop and implement a plan to evaluate the future needs of clean fueling/recharging and electrified truck parking and onsite cargo handling equipment. Promote the creation of centralized truck parking areas that provide clean energy refueling stations and other support facilities while ensuring adjacent residential neighborhoods are not negatively impacted by trucking activities. Target locations that serve both the northern and southern industrial parts of the city, and coordinate to provide truck parking and charging areas in neighboring jurisdictions that serve regional truck travel. Require proposed truck-intensive uses to provide robust onsite clean energy fueling and/or electric charging stations for trucks and onsite cargo handling equipment. For existing and proposed uses, to contribute toward centralized parking solutions (for larger facilities, which will overlap with their compliance with the South Coast Air Quality Management District's Indirect Source Rule). (EJ: EP)

Community Design Element

- CD-2.1 Objective Development and Design Standards.** Prepare objective development and design standards to help streamline the approval and production of new housing, while creating a more walkable, vibrant, and unified community. (EJ: H)
- CD-2.2 Transit Stops.** Coordinate with Omnitrans to implement and update the agency's Bus Stop Safety Improvement Plan, prioritizing improvements in Ontario's environmental justice areas. In the next update, encourage Omnitrans to increase the weighting of environmental justice factors in the evaluation, identification, and prioritization of improvements. (EJ: PF, PA)
- CD-2.3 Bus Rapid Transit.** Coordinate with Omnitrans on street cross section designs for BRT corridors to influence street designs to maximize walkability and bicycle access/safety of the transit riders. (EJ: PF)

Social Resources Element

- SR-1.1 Healthy Ontario.** Continue to pursue programs, incentives, grants, and partnerships to fund and conduct the activities and achieve the objectives of the Healthy Ontario Initiative, including those on prevention and wellness, healthcare access and utilization, education and lifelong learning, and safe and complete neighborhoods. Augment the initiative to prioritize activities and objectives to address neighborhoods in environmental justice areas. **(EJ: FA, PF, PA)**
- SR-1.2 Collaboration.** Develop active, strategic partnerships with public, private, and nonprofit entities, such as the Community Healthy Improvement Association of Ontario (CHIA) and Center for Community Action and Environmental Justice (CCA EJ), to improve health outcomes by leveraging capacity, resources, and programs around mutually beneficial initiatives that promote health, equity, and civil engagement for residents, with a priority on those living in environmental justice area areas. **(EJ: CE, PA)**
- SR. 1.3 Resident Satisfaction Survey.** Augment the City’s resident satisfaction survey (conducted in English and Spanish) to address issues related to community health, pollution, parks, community engagement, and community services, with focused outreach for environment justice concerns and priority areas. Report findings of survey through the City’s various media platforms and utilize input to update the Implementation Plan and Policy Plan, as necessary. **(EJ: PA, PF)**
- SR-1.4 Community Centers.** Continue to implement the City’s 2021 Recreation and Parks Master Plan and emphasize the creation of new community centers in environmental justice areas where residents are more than one-half mile from an existing community center. **(EJ: PF)**
- SR-1.5 Expanded Representation.** Expand representation of residents concerned about environmental justice issues by extending invitations to such residents/groups (focused especially on those who also live in environmental justice areas within the City) to participate in civic government activities. **(EJ: CE)**

APPENDIX B. GLOSSARY OF TERMS AND ABBREVIATIONS

Purpose

The following is a glossary of terms and abbreviations to help users read and understand the Policy Plan and the meaning and intent of its goals and policies. Please note that this glossary is not intended to be comprehensive or define terms, acronyms, or abbreviations that are generally well understood or easily looked up using other resources. Additionally, some terms listed in this glossary are not used in the text of Policy Plan. The focus is on terms that have multiple potential meanings (either in or out of the Policy Plan context), and to define the intended meaning of such terms to ensure the correct interpretation and implementation of the Policy Plan.

A-C

Accommodate: To make room for or provisions for something that is specifically intended to occur.

Action: A specific, measurable step that needs to be taken to meet stated objectives. An action is not mere compliance with the law or a commonly adopted or accepted practice or protocol. An action is short in duration and is meant to be updated and replaced once action has been taken. In the context of the Implementation Plan, the activity is typically to be undertaken by the City or another public entity.

Adaptive Reuse: The conversion of obsolescent or historic buildings from their original or most recent use to a new use.

Advocate: To openly support and promote an action, objective, or goal; also, to speak on behalf of an individual or group.

Airport Land Use Compatibility Plan (ALUCP): The ALUCP provides for the orderly growth of the areas within an airports noise contour, runway protection zones, approach zones, and Part 77 zones and describes appropriate land uses, maximum population density, maximum site coverage, height restrictions, and required notification/disclosure areas.

Airport Master Plan (AMP): An AMP provides for the maintenance, development, and operation of the airport itself and includes operational forecasts, fleet mixes and the ultimate runway configuration and airport plan.

Allow: To be open to something occurring but without any particular effort to make it happen.

Always: The action will apply to future decisions without exception.

Analyze: To methodically examine the desirability or feasibility of something, with a view toward letting the evidence determine the appropriate level of commitment.

Aspire: To direct one's ambitions toward achieving something.

Augment: To add to something that is already in place to make it more desirable, functional, or beneficial.

Avoid: To not enable something that will lead to an undesirable outcome.

Best Management Practices (BMPs): Methods or techniques found to be the most effective and practical means in achieving an objective (such as preventing or minimizing pollution) while making the optimum use of resources.

Buffer / buffer zone: An area of land, structure, feature, or design treatment separating two uses that acts to soften or mitigate the effects of one use on the other.

Building Density and Intensity: Building density and intensity includes both residential density, expressed as units per acre, and non-residential intensity, expressed as floor area ratio, which is the amount of building square feet in relation to the size of the lot.

Buildout: Build-out is an urban planners estimate of the amount and location of potential development for an area. Buildout for The Ontario Plan can be found in Exhibit LU-03.

Built Environment: Human-made buildings and structures, as opposed to natural features.

Carbon Footprint: A carbon footprint is a measure of the impact human activities have on the environment in terms of the amount of greenhouse gases produced.

Carbon Sequestration: The removal and storage of carbon from the atmosphere in carbon sinks (such as oceans, forests, or soils) through physical or biological processes, such as photosynthesis.

Community Development Block Grant (CDBG): A grant program administered by the U.S. Department of Housing and Urban Development (HUD) that provides federal grants directly to larger urban cities and counties for activities that benefit low- and moderate-income individuals, eliminate blight, or address a serious and immediate threat to public health and welfare. States distribute CDBG funds to smaller cities and towns. Grant amounts are determined by a formula based upon need.

California Environmental Quality Act (CEQA): A state law requiring state and local agencies to assess the environmental impacts of public or private projects they undertake or permit. Agencies must mitigate adverse impacts of the project to the extent feasible. If a proposed activity has the potential for significant adverse environmental impacts, an Environmental Impact Report (EIR) must be prepared and certified as legally adequate by the public agency before taking action on the proposed project.

Class I Trail: Known as a Shared-use Path, is a completely separated facility for the exclusive use of bicycles and pedestrians with crossflow by motor vehicles minimized.

Class II Trail: Known as a Bike lane and a Buffered Bike lane, is a portion of the roadway that is designated by striping, signaling, and/or pavement markings for the exclusive use of bicyclists established along streets and corridors where there is significant demand and distinct needs. Bike lanes can be paired with a designated buffer space composed of painted striped and pavement markings adjacent bike lane, known as a Buffered Bike Lane.

Class III Trail: Known as a Bike Boulevard, is a special type of bike route where a street is designed to accommodate bicyclists with a wide variety of skill levels.

Class IV: Known as a Separated Bikeway and Raised Separated Bikeway, also known as a cycle track or protected bike lane, is a one- or two-way bike way for the exclusive use of bicycles that includes a physical, vertical barrier between bicyclists and motor vehicle traffic within the roadway. They can be designed to be either at the same grade as the adjacent sidewalk or set as an intermediate level mountable curb between the roadway and sidewalk, known as a Raised Separated Bikeway.

“Clean” Industry: “Clean” industries include industrial type uses whose operations produce lower quantities of hazardous substances and air emissions than traditional industrial uses. Operations of such facilities should have minor adverse effects on neighboring land uses and are therefore more compatible in proximity to sensitive uses, like residential development, schools, and childcare facilities than traditional industry. Examples of “clean” industry include incubator space for small start-up and creative businesses, clean/green technology, biomedical device manufacturing, digital arts, furniture and apparel design, etc.

Collaborate: To intentionally, willingly work together toward a common objective or goal.

Compatible: In relation to use, the ability for two or more uses to coexist without conflict, with minimal conflict that can be mitigated, or in a beneficial manner. When used in relation to a structure, indicates that the structure is built so that its appearance is similar to that of the principal unit to which the structure is accessory or to the general character of the neighborhood or community with regards to color, materials, construction, lighting, signs, or the emission of sounds, noises and vibrations. See also *incompatible*.

Complete streets network: A system of on- and off-street facilities (e.g., sidewalks, bike lanes, and trails), that enable all users of all ages and abilities to navigate within or through a community area, with an emphasis on mobility focus areas. The system can consist of one or more facility type based on the intended users and access requirements. Individual facilities may overlap or not, serve all or just some users, and be contiguous or disconnected. Complete streets facilities and improvements are subject to physical constraints presented by the local context and financial feasibility limitations.

Comply with: To follow specified existing ordinances, regulations, or procedures.

Complement / complementary: Combining or coexisting (e.g., two buildings or land uses) in such a way as to enhance or emphasize the qualities of each other.

Comprehensive Housing Affordability Strategy (CHAS): A plan prepared by state or local agencies as a prerequisite for receiving assistance under certain U.S. Department of Housing and Urban Development (HUD) programs.

Concurrent: Services, facilities, activities, or other things that are provided or are occurring at the same time as something else.

Consider: To remain open to and evaluate a range of possible actions or outcomes as part of a decision.

Consistent: To be or act in harmony or aligned with something; see also inconsistent.

Consolidate: To bring together aspects, features, or components of a system or locale that can serve better in a consolidated form, compared to existing or provided separately.

Construct: To build something: buildings, roads, channels, etc. Context. Local or regional environmental, social, and economic conditions.

Continue: To maintain and/or resume an action.

Cooperate: To work in a positive effort with another entity toward a mutually beneficial end. Such work may take the form of direct action, passive support, or even inaction.

Coordinate: To work in a positive effort with another entity in the process of conducting individual actions or initiatives that relate to each other and that can benefit from concurrent or cooperative activity.

Council of Governments (COG): A single or multi-county entity created by a joint power agreement. COGs are responsible for determining the share of the regional need for housing for each of the counties and cities within the COG's region. In most cases, the COG also serves as the Regional Transportation Planning Agency (RTPA) responsible for preparing regional transportation plans and expenditure programs.

Community Noise Equivalent Level (CNEL): A 24-hour energy equivalent level derived from a variety of single-noise events, with weighting factors of 5 and 10 dBA applied to the evening (7 p.m. to 10 p.m.) and nighttime (10 p.m. to 7 a.m.) periods to allow for greater sensitivity to noise during these hours.

Connect SoCal: The Connect SoCal plan (also known as the 2020-2045 Regional Transportation Plan/Sustainable Communities Strategy or RTP/SCS) represents the vision for Southern California's future, including policies, strategies, and projects for advancing the region's mobility, economy, and sustainability through 2045. The plan details how the region will address its transportation and land use challenges and opportunities in order to achieve its regional emissions standards and greenhouse gas (GHG) reduction targets.

The components of Connect SoCal are required by federal and state legislation and is an important planning document for the region, allowing project sponsors to qualify for federal funding. SCAG is required to update this long-range planning document every four years.

Complete Community: A diversity of housing choices and educational opportunities; jobs for a variety of skills and education; recreation and culture; a full-range of shopping, entertainment and services; and the opportunity to improve individual quality of life.

Crime Prevention Through Environmental Design (CPTED): Crime Prevention through Environmental Design (CPTED) is the proper design and effective use of the built environment which may lead to a reduction in the fear and incidence of crime, and an improvement of the quality of life." – National Crime Prevention Institute.

Critical Facilities: Facilities housing or serving many people, and are necessary in the event of an earthquake or flood, such as hospitals, fire, police, and emergency service facilities, utility lifeline facilities, such as water, electricity, and gas supply. Critical Facilities within Ontario are mapped in the Safety Element.

D-I

Dedicate: To offer or set aside for a specific program, action, or use; when in the context of land dedication, the land is generally set aside for public ownership, use, and/or operation or maintenance.

Department of Housing and Community Development (HCD): A state agency that administers state housing programs. HCD also administers the federal HOME and CDBG programs on behalf of jurisdictions that are not directly assisted by HUD.

Department of Housing and Urban Development (HUD): A federal, cabinet level department responsible for overseeing, implementing, and administering U.S. government housing and urban development programs.

Design: To conceive in advance the size, shape, qualities, appearance, open space, and other attributes of a proposed development, building and related improvements, generally rendered in graphic forms that communicate the anticipated outcome.

Determine: To calculate in advance the nature or outcome of an anticipated action, situation, or proposal.

Discourage: To openly deter (or attempt to deter) an individual, group, or organization from doing something.

Discretionary: An action taken by a governmental agency that calls for the exercise of judgment in deciding whether to approve and/or how to carry out a project.

Emergency Communication & Dispatch Center: The Dispatch center provides daily dispatch services for the Fire & Police Departments and contract fire agencies. The dispatch Center is also a designated facility as part of California's Fire Rescue Mutual Aid System.

Encourage: To promote, support, or champion a concept or action; such support may be in terms of political support and coordination, staff resources, and/or financial resources.

Endangered Species: A native California bird, mammal, fish, amphibian, reptile, or plant (species, subspecies, or variety) is endangered when it is in serious danger of becoming extinct throughout all, or a significant portion of its range due to one or more causes, including loss of habitat, change of habitat, over-exploitation, predation, competition or disease (Section 2062, Fish and Game Code).

Enhance: To improve existing conditions in quality, value, or characteristic.

Equitable: A condition or treatment that is fair and impartial.

Establish: To bring something into being that does not currently exist.

Explore: To search for or examine the nature of a possibility before a commitment is made. See also *analyze*.

Expand: To increase in detail, extent, number, volume, or scope.

Environmental Justice Areas: Disadvantaged communities that face the highest risk of exposure to pollution and are also burdened by socioeconomic and health issue. Determined using the latest version of CalEnviroScreen, a screening tool developed by the California Office of Environmental Health Hazard Assessment (OEHHA) used to help identify communities disproportionately burdened by multiple sources of pollution and with population characteristics that make them more sensitive to pollution.

Environmental Infrastructure: The basic facilities, services, and installations needed for the functioning of key systems such as the sewerage network, drainage systems as well as improvements made to water courses and habitats.

Feasible: Capable of being done, executed, or managed successfully taking into consideration social, physical, environmental, and/or economic factors.

Facilitate: To make an action or process easier or to help bring about.

Fair share: An allocation of resources, costs, or fees considered equitable and proportional to the needs, impacts, or activity of an existing or proposed project.

Feasible, technically: Capable of being implemented because the industrial, mechanical, or application technology exists.

Feature: A noticeable or important characteristic, attribute, or aspect of something.

Finding(s): The result(s) of an investigation and the basis upon which decisions are made. Findings are used by government agents and bodies to justify action taken or a decision made by the entity.

Fire Conflagration: An uncontrolled burning that threatens human life, health, property, or ecology that may produce a firestorm, in which the central column of rising heated air induces strong inward winds, which supply oxygen to the fire.

Flash Flood: A flash flood is a rapid flooding of geomorphic low-lying areas – washes, rivers, and streams. It is caused by heavy rain associated with a thunderstorm, hurricane, or tropical storm. Flash floods can also occur after the collapse of an ice dam, or a human structure.

Flood Insurance Rate Map (FIRM): For each community, the official map on which the Federal Insurance Administration has delineated areas of special flood hazard and the risk premium zones applicable to that community.

Floodplain: The area, adjacent to a watercourse or other body of water, subject to recurring floods. Floodplains may change over time as a result of natural processes, the characteristics of a

watershed, or the construction of bridges or channels. Floodplain can also serve as a reference to areas subject to flooding and mapped under FEMA's NFIP.

Floor Area Ratio (FAR): The gross floor area permitted on a site divided by the total net area of the site, expressed in decimals to one or two places.

Food insecurity: The state of being without reliable access to sufficient quantity of affordable, nutritious food.

Fund: To make money available for a prescribed purpose; may be a one-time, periodic, or ongoing commitment.

Global Gateway: Ontario is increasingly identified for the competitive advantages it provides to businesses that want to succeed in the global marketplace and for its growing importance to the state, national and international economies. Ontario's distinction as a Global Gateway is due to its strategic Southern California location at the center of a rapidly developing freight movement system that includes LA-Ontario International Airport (ONT), two railroads, four major freeways and an expanding network of freight forwarders. ONT is one of the fastest growing and ambitious cargo airports outside the booming Asia-Pacific market and home to United Parcel Services Western Regional Hub providing daily direct flights to China. The Ontario Foreign Trade Zone is also significant to businesses involved in international trade. This envied transportation network links Ontario-based companies to the Ports of Los Angeles and Long Beach as well as western, national, and international markets.

Global Warming: An increase in the earth's atmospheric and oceanic temperatures widely predicted to occur due to an increase in the greenhouse effect resulting especially from pollution.

Goal: A statement of desired future conditions regarding a particular topic; a goal paints a picture of how something will be in the future. A goal in and of itself is not sufficient to understand its intent, extent, or context. A goal itself is kept simple, with policies, objectives, and implementation actions providing further definition.

Greenhouse Gases (GHG): A Greenhouse gas is one that contributes to the warming of the Earth's atmosphere by reflecting radiation from the Earth's surface. (e.g., carbon dioxide, ozone, or water vapor).

Green Infrastructure: Green infrastructure is strategically planned and managed networks of natural lands, working landscapes and other open spaces that conserve ecosystem values and functions and provide associated benefits to human populations.

Harden: The use of site design and materials to make a building or site able to withstand the onset or severity of damage from an event such as a fire, earthquake, flood, or other similar event

Hazard Mitigation Plan: The Local Hazard Mitigation Plan is a comprehensive resource document that serves many purposes, including enhancing public awareness and understanding, creating a decision tool for management, promoting compliance with State and Federal program requirements, enhancing local policies for hazard mitigation capability, and providing inter-jurisdictional coordination. The Federal Disaster Mitigation Act of 2000 requires all local

governments to create such a disaster plan in order to qualify for funding in the future. The City's Hazard Mitigation Plan is incorporated by reference into the Policy Plan.

Hazardous Material: Any substance that, because of its quantity, concentration, or physical or chemical characteristics, poses a significant present or potential hazard to human health and safety or to the environment if released into the workplace or the environment. The term includes, but is not limited to, hazardous substances and hazardous wastes.

HOME (HOME Investment Partnership Program): A federal housing program that provides formula grants to states and localities. Communities use the grants (often in partnership with local nonprofit groups) to fund a wide range of activities. These include building, buying, or rehabilitating affordable housing for renters and homeowners, and providing direct rental.

Impact: Generally, the result of an action or inaction; when the term is used in the context of an environmental analysis under the California Environmental Quality Act (CEQA), the term "impact" refers to how a proposed project may directly or indirectly effect specific environmental, infrastructure, and public service factors; a negative impact damages those environmental factors or increases burdens on selected facilities and services).

Impervious Surface: A surface through which water cannot penetrate, such as a roof, road, sidewalk, or paved parking lot. The amount of impervious surface increases with development and establishes the need for drainage facilities to carry the increased runoff.

Identify: To determine characteristics or facts without any predetermination regarding future commitments; gather information only.

Include: To make a part of a consideration or issue under public scrutiny.

Incompatible: To be so opposed in character as to be incapable of existing together without causing conflicts or harm upon one or all parts; conflicting; see also *compatible*.

Incorporate: The same as *include*.

Inconsistent: Varying to the point of conflict or contradiction; see also *consistent*.

Innovative: Methods or approaches that are new and original, often (but not always) more advanced.

Install: To put in place at a specified time or place or in a particular sequence.

Integrate: To blend new and existing projects or activities together in a compatible fashion.

Investigate: To examine the accuracy of information provided in support of a proposed project, process, or program.

J-Q

Jobs-Housing Balance: The jobs/housing ratio divides the number of jobs in an area by the number of households. A ratio greater than 1.0 indicates a net in-commute (job-rich community); less than 1.0 indicates a net out-commute (housing-rich community).

ONT: Ontario International Airport.

[no terms that begin with the letter "K"]

Land Use Designation Boundary: Where a boundary line is indicated as following a street or alley, the boundary line shall be construed as following the centerline of the right-of way. Where a street or alley is officially vacated or abandoned, the area within vacated street or alley on each side of the centerline shall be classified in the same land use designation as the adjoining property.

Level of Service (Traffic): A scale that measures the amount of traffic that a roadway or intersection can accommodate, based on such factors as maneuverability, driver dissatisfaction, and delay.

Limit: To confine a project or activity within prescribed specifications or performance criteria.

Link: To connect a project, area, function, or activity to another feature or features that would be mutually beneficial and reinforce desired functions.

Locate: To place an improvement, function, or use in a particular locale, on property with certain characteristics, or in proximity to specified features to bring about benefits and/or efficiency.

Maintain: To keep a system, facility, area, or activity in continuously sound condition, as defined by approved standards.

May: To offer the possibility but not the probability or promise of a certain commitment of resources or support; associated with action or activity that is often desirable and allowed, but implementation of such action or activity is often conditional.

Minimize: To reduce something to the smallest amount or degree feasible.

Monitor: To track the performance of a particular physical condition, activity accomplishment or aspect of change, with a view toward using the results to inform City determination of its appropriate commitments.

Multipurpose Trail: Also called a Shared-use Path (per the ATMP), is an off-street trail that accommodates pedestrian and bicycle travel. These trails typically double as Class I bikeways.

National Flood Insurance Program: A federal program that authorizes the sale of federally subsidized flood insurance in communities where such flood insurance is not available privately. The areas of high risk are mapped by the Federal Emergency Management Agency (FEMA) and are known as Flood Insurance Rate Maps (FIRMs).

Natural Ecosystem: Natural ecosystem is a natural unit consisting of all plants, animals, and microorganisms in an area functioning together with all the non-living physical of the environment. That or, a place where plants and animals are dependent upon one another – and their particular surroundings – for survival.

Never: Something (e.g., action or land use) will not take place or occur under any circumstance.

Noise-Sensitive Uses: Land uses where quiet environments are necessary for enjoyment and public health and safety. Residences, schools, motels and hotels, libraries, religious institutions, hospitals, and nursing homes are examples. Such uses are also considered vibration-sensitive uses.

Notify: To advise a specified party or parties regarding an event or change for which there is an obligation to inform or need to know.

Ontario Ranch: An area that spans over 8,000 acres and 13 square miles, offering master-planned neighborhoods, commercial centers, and recreation spaces. A visionary model for California Growth, Ontario Ranch is the first gigabit community in Southern California and will feature ultra-high bandwidth home-data services, plus an expansive park and trail system, new schools, and independent sources of water. (Generally, the area south of Riverside Drive)

Parkland: Land used for public recreational and open space purposes. Such spaces and facilities can be public or private.

Particulate Matter: “Particulate matter,” also known as particle pollution or PM, is a complex mixture of extremely small particles and liquid droplets. Particle pollution is made up of a number of components, including acids (such as nitrates and sulfates), organic chemicals, metals, and soil or dust particles.

Partner: To join with another entity in conducting an activity or building a facility that would be mutually beneficial and add value more efficiently than if pursued independently; does not necessarily require financial commitments. See also *cooperate*.

Periodically: The action will take place or apply to future decisions at specified intervals or times.

Places: Functional and appealing places where people want to live, work, and visit.

Place Types: Distributed throughout Ontario and are generally centered around areas designed as mixed use. They include a range of scales that are intended to fulfill different roles within city. Such areas are envisioned as walkable and transit-friendly environments with a safe, comfortable, and inviting public realm that encourages people to spend time, whether living, working, or visiting.

Policy: A statement that guides decision-making and specifies public commitment. A policy defines and directs how the City intends to achieve goals.

Potable Water: Water of sufficient quality to serve as drinking water, whether it is used for drinking or not.

Premier Community: A community with a distinct identity or character such as historic development, viable neighborhoods, design, and image.

Preserve: To maintain something in its original or existing state (verb). An area dedicated to the protection and conservation of biological resources and/or landscapes (noun).

Prevent: To keep a particular use, condition, activity, or circumstance from occurring; it does not imply complete (100 percent) avoidance or elimination.

Prioritize: To intentionally direct discretionary investments or actions to one area ahead of other areas. The act of prioritizing is an ad-hoc process that considers a variety of relevant factors. State and federal law and agency requirements, court orders, and City/county-declared emergencies take precedent over the prioritization direction in the Policy Plan.

Principle: An assumption, fundamental rule, or doctrine that guides Policy Plan policies, proposals, standards, and implementation measures.

Prohibit: To absolutely disallow something from happening or being built.

Promote: To actively stimulate the likelihood and desirability of something happening; does not require investing public resources in its direct development.

Protect: To shield from damage to people or property.

Provide: To make something available, typically to a community, organization, or business; it does not imply that the thing is being made available at no cost.

Public (noun): Residents, businesses, visitors, and travelers.

Public realm: Framework for an urban, mixed use, or transit-oriented area that is vibrant, active, and intense urban environment.

Pursue: To strive to obtain or to seek to accomplish something, often through direct actions, partnerships, or indirect assistance. See also *support*.

[no terms that begin with the letter "Q"]

R-Z

Rare Species: A native California plant (species, subspecies, or variety) is rare when, although not presently threatened with extinction, it is in such small numbers throughout its range that it may become endangered if its present environment worsens (Section 1901, Fish and Game Code). Since 1985, this designation applies to plants only.

Recycled Water: Former wastewater (sewage) that has been treated to remove solids and certain impurities, and then allowed to recharge the aquifer or used for non-potable purposes rather than being discharged to surface water.

Reflect: To embody or represent (something) in a faithful or appropriate way.

Renewable Energy: Energy that is virtually inexhaustible in duration but limited in the amount of energy that is available per unit of time. Renewable energy includes biomass, hydropower, geothermal, solar, wind, ocean thermal, wave action, and tidal action.

Report: To provide public information on a subject or condition.

Require: To absolutely impose an obligation or standard.

Resilience / Resiliency: The capacity of any entity to prepare for disruptions, to recover from shocks and stresses, and to adapt and grow from a disruptive experience. Adaptation actions contribute to increasing resilience.

Respect: To give due regard for something or to hold something in high regard.

SBCTA / SBCOG: San Bernardino County Transportation Authority / San Bernardino Council of Governments.

SCAG: Southern California Association of Governments.

Sensitive Land Uses: Uses that by their nature and characteristics can be significantly impacted by the noise, odor, vibration, air quality and inherent hazards associated with other uses near it. Sensitive uses include, but are not limited to: single and multiple family residences, schools, churches, hospitals, day care facilities and nursing homes.

Sensitive Species: “Sensitive” refers to naturally-reproducing fish and wildlife species, subspecies, or populations that are facing one or more threats to their populations and/or habitats.

Shall: Will always be carried out or required; no exceptions.

Should: Will be carried out or required most of the time, unless a very good reason is identified why an exception is acceptable.

Solar Access: The provision of direct sunlight to an area specified for solar energy collection when the sun's azimuth is within 45 degrees of true south.

Solid Waste: Any unwanted or discarded material that is not a liquid or gas. Includes organic wastes, paper products, metals, glass, plastics, cloth, brick, rock, soil, leather, rubber, yard wastes, and wood, but does not include sewage and hazardous materials. Organic wastes and paper products comprise about 75 percent of typical urban solid waste.

Sometimes: Will apply to future decisions under specified conditions or circumstances.

Specify: To establish distinct requirements.

Strive: To make great efforts or devote serious effort to achieve or obtain something.

Support: To provide assistance or promotion; see *encourage*.

Sustainability: A process in which the exploitation of resources, the direction of investments, the orientation of technological development, and institutional change are all in harmony and enhance both current and future potential to meet human needs and aspiration; sustainability integrates the political, social, economic, and environmental. A more common and concise definition of sustainability is a system that “meets the needs of the present without compromising the ability of future generations to meet their own needs”.

Sustainable development: Development that maintains or enhances equity, economic opportunity, and community well-being while protecting and restoring the natural environment upon which people and economies depend. Sustainable development meets the needs of the present without compromising the ability of future generations to meet their own needs.

Trucking-intensive Business or Use: In the context of defining a trucking-intensive business, a truck is a vehicle identified by the Federal Highway Administration vehicle as Class 4 or higher, with the exception of dually trucks and recreational vehicles. A trucking-intensive business is a permitted use that includes the frequent use of trucks as part of its primary activities including, logistics facilities, warehousing, fulfillment centers, distribution centers, truck yards, hazardous materials or waste facilities, container storage, and container parking.

Urban Run-off: Surface runoff of rainwater over impervious surfaces. During rainstorms and other precipitation events (including run-off from overwatering landscape areas), these surfaces (built from materials such as asphalt, cement, and concrete), along with rooftops, carry polluted stormwater to storm drains, instead of allowing the water to percolate through soil. This causes lowering of the water table (because groundwater recharge is lessened) and flooding since the amount of water that remains on the surface is greater.

Vibration-Sensitive Uses: Land uses where quiet environments are necessary for enjoyment and public health and safety. Residences, schools, motels and hotels, libraries, religious institutions, hospitals, and nursing homes are examples. Such uses are also considered noise-sensitive uses.

Vehicle Miles Traveled (VMT): The total amount of driving over a given area. In 2013, the State of California passed Senate Bill 743, which mandates that jurisdictions can no longer use automobile delay – commonly measured by Level of Service – in transportation analysis under the California Environmental Quality Act. The State issued guidelines that direct jurisdictions to use of a broader measure called VMT, with the intent to reduce overall VMT and per capita VMT.

Vulnerability Assessment: An analysis of the vulnerability of various City asset types and people to climate change hazards. A vulnerability assessment is required by state law as part of a jurisdiction’s Safety Element and is used to generate policies and actions that will lead to greater safety, climate adaptation, and resiliency. The City’s Vulnerability Assessment is incorporated by reference into its Safety Element.

Vulnerable Population or Community: Populations or communities that experience heightened risk and increased sensitivity to natural disasters, emergencies, or severe weather events or conditions; and have less capacity and fewer resources to cope with, adapt to, or recover from such events or conditions.

Waste Diversion: The act of preventing garbage from being disposed of in landfills or incinerators by reducing the amount of materials that you use or buy, reusing products, recycling or composting.

Wastewater: Spent or used water with dissolved or suspended solids, discharged from homes, commercial establishments, farms, and industries.

Williamson Act: Known formally as the California Land Conservation Act of 1965, it was designed as an incentive to retain prime agricultural land and open-space in agricultural use, thereby slowing its conversion to urban and suburban development. The program entails a ten-year contract between a jurisdiction and an owner of land whereby the land is taxed on the basis of its agricultural use rather than its market value. The land becomes subject to certain enforceable restrictions, and certain conditions need to be met prior to approval of an agreement. Contracts may be exited at the option of the landowner or local government by initiating the process of term nonrenewal. Other circumstances and processes may also apply.

[no terms that begin with the letter "X"]

[no terms that begin with the letter "Y"]

Zoning: The division of a jurisdiction by legislative regulations into areas (aka land use zoning districts), which specify allowable uses for real property and size restrictions for buildings within these areas; a program that implements policies and the land use categories of the Policy Plan.

ATTACHMENT B:

***VULNERABILITY ASSESSMENT REPORT FOR THE
ONTARIO PLAN 2050 UPDATE***

A Component of The Ontario Plan 2050

April 2022

VULNERABILITY ASSESSMENT REPORT FOR THE ONTARIO PLAN 2050 UPDATE

City of Ontario

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TABLE OF CONTENTS

Introduction	1
Community Profile	1
Regulatory Framework.....	3
Climate Science Overview.....	4
Vulnerability Assessment Method	5
Step 1: Identify Exposure	5
Step 2. Identify Sensitivities and Potential Impacts.....	7
Step 3. Assess Adaptive Capacity.....	8
Step 4. Prioritize Vulnerability Scoring	9
Hazards of Concern	10
Climate Change Hazards	10
Agriculture Pests and Diseases	10
Air Quality	10
Drought	11
Extreme Heat and Warm Nights	11
Flooding.....	13
Human Health Hazards	15
Severe Weather	15
Wildfire and Smoke.....	15
Natural Hazards Not Evaluated in the Vulnerability Assessment.....	16
Seismic Hazards.....	16
Geologic Hazards.....	17
Populations and Assets	18
Vulnerability Assessment Results	20
Population and Asset Considerations	20
Populations	21
Built Systems and Services.....	22
Economic Drivers	25
Implications for TOP 2050.....	27
Emergency Preparedness and Response	27
Mutual-Aid Agreements.....	28
Evacuation Needs.....	28

Equity and Uncertainty	28
Adaptation and Resilience Opportunities.....	29
Resilience in Other Planning Mechanisms.....	31
Bibliography	32
Printed References.....	32
Websites	33
List of Preparers	35
Lead Agency	35
PlaceWorks, Consultant to the City of Ontario.....	35
 Appendices	
Appendix A: list of Populations and Assets.....	36
Appendix B: Vulnerability Assessment Results Matrix	42
 List of Tables	
Table 1. Rubric for Impact Scoring.....	8
Table 2. Rubric for Adaptive Capacity Scoring.....	9
Table 3. Populations and Assets Included in the Vulnerability Assessment.....	18
 List of Figures	
Figure 1. Community Overview Map	2
Figure 2. California Adaptation Planning Guide Recommended Method	5
Figure 3. Vulnerability Scoring Matrix.....	9
Figure 4. Projected Extreme Heat Days in Ontario	12
Figure 5. Project Warm Nights in Ontario.....	12
Figure 6. FEMA Flood Hazard Zones	14

INTRODUCTION

The City of Ontario (City) prepared a Climate Change Vulnerability Assessment in conformance with State of California requirements to assess climate change vulnerability and address climate change adaptation and resilience as part of The Ontario Plan 2050 Update (TOP 2050). California Government Code Section 65302(g) requires communities to address climate adaptation and resilience in their general plans. The goal of this requirement is to enable the community to prepare for, respond to, withstand, and recover from disruptions created or caused by climate change. The vulnerability assessment acts as a foundation for integrating adaptation and resilience policies into the Safety Element and The Ontario Plan by identifying a set of priority vulnerabilities in the City of Ontario. This report presents the results of the City's Climate Change Vulnerability Assessment and includes an overview of the regulatory framework and method for preparing a vulnerability assessment and incorporating adaptation and resilience into TOP 2050, a summary of the climate change hazards affecting the resilience of Ontario, and characterizes specific populations and assets included in the assessment

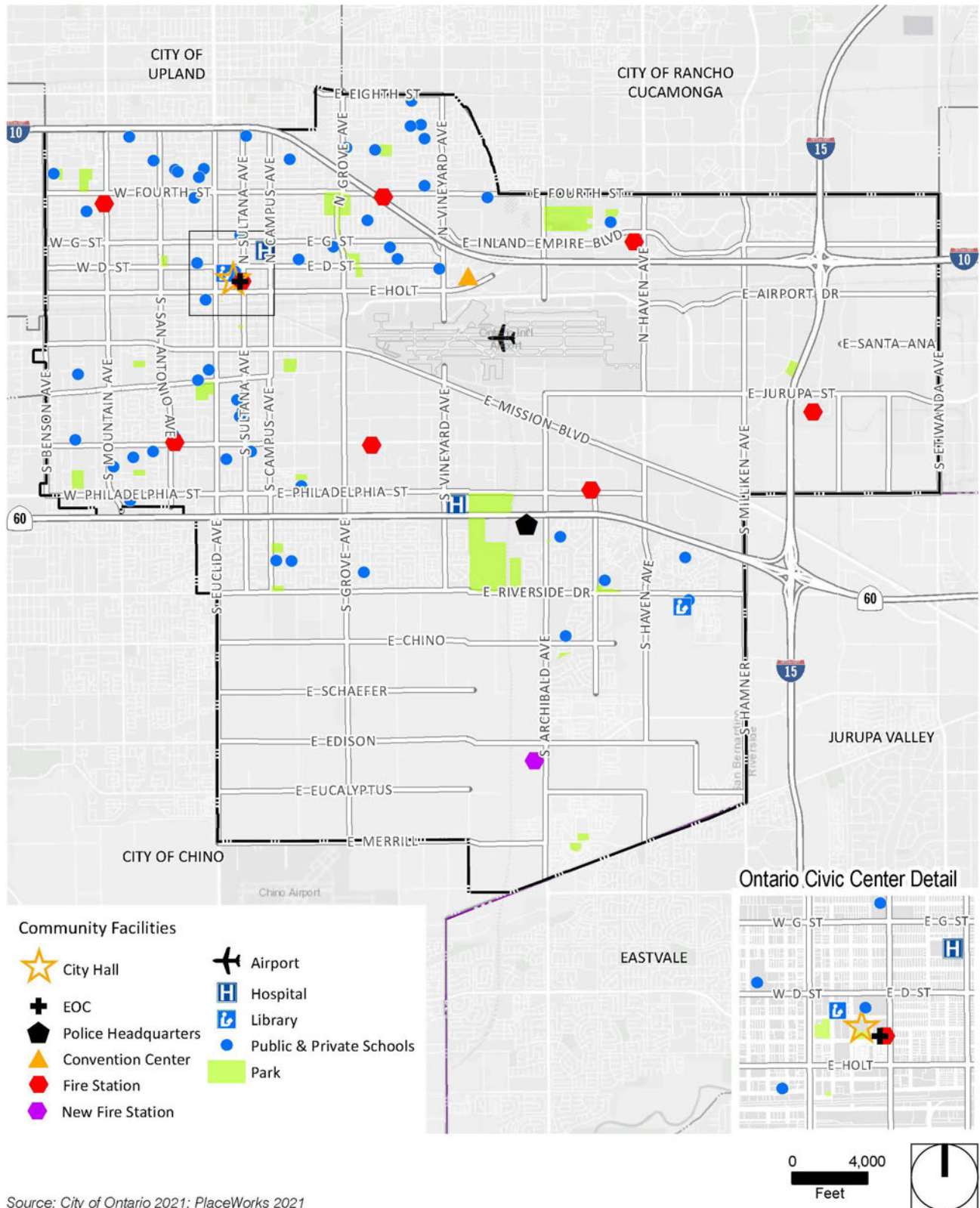
Community Profile

The City of Ontario is in the valley portion of San Bernardino County, surrounded by the cities of Rancho Cucamonga and Upland to the north, the cities of Fontana and Jurupa Valley to the east, the City of Eastvale to the south, and City of Chino to the west. The Los Angeles National Forest in the San Gabriel Mountains rise nearly 10,064 feet above sea level, and the city sits just south of this mountain range at 1,004 feet above sea level. According to American Community Survey 2019, 5-year estimates, Ontario is home to approximately 176,760 residents, over 37 percent of which are 24 years old or younger and 31 percent of which are between 25 and 44 years old.

The City of Ontario covers approximately 50 square miles, as shown in **Figure 1**, which was originally home to the Tongva (Gabrielino) and Kizh (Gabrielino) tribal nations. The city is developed with residential, commercial, office, public facility, industrial, and agricultural land uses. Residential, commercial, public facility, and office land uses are distributed throughout the city. Industrial land uses are concentrated in the central portion of the city, surrounding the Ontario International Airport, as well as in the eastern and southern portions of the city. Agricultural land uses comprise most of the land use in the southern portion of Ontario; however, this use will likely transition to residential, mixed-use, and commercial land uses by 2050. Major economic sectors include education services, Ontario International Airport, retail centers, and warehouses and logistics centers.

The city has a Mediterranean climate, with hot summers and mild winters with rain. On average, annual high temperatures in Ontario range from 65 degrees Fahrenheit (°F) in January to 94 in August. Low temperatures range from 43 °F in December to 65°F in August. The city receives an average of approximately 15.04 inches of precipitation per year, with all precipitation falling as rain. Most precipitation falls during the winter months with rare occurrences of summer storms. The Southern California inland valley climate was historically famous for growing citrus because of the hot summers and winters without frost. The Santa Ana winds flow through this area of San Bernardino County from October to April, with the strongest Santa Anas occurring in December and January.

Figure 1. Community Overview Map



Source: City of Ontario 2021; PlaceWorks 2021

Ontario's primary transportation access is from Interstate (I-) 10, I-15, State Route (SR-) 60, and SR-83. I-10 runs west through San Bernardino County to downtown Los Angeles and east through the southern United States to the east coast. I-15 runs south through San Bernardino County to downtown San Diego and north through the western United States to Canada. SR-60 runs east to west, and SR-83 runs north to south through Ontario, connecting the valley region of San Bernardino County to the mountains to the north and San Gabriel Valley to the east. Other major roadways include Mission Boulevard, 4th Street, Airport Drive, Haven Avenue, and Miliken Avenue. Ontario International Airport is in the north-central portion of the city. Omnitrans operates six public bus routes, including one express route, through Ontario, which residents can use to travel to key destinations in Ontario and other areas of San Bernardino County. Metrolink, via the Union Pacific Railroad, provides long-distance train service via Ontario to Los Angeles, which connects with other train services to major cities such as Seattle, Chicago, and New Orleans. The City also maintains bike routes along Riverside Drive, Deer Creek Loop, Lytle Creek Loop, and Inland Empire Boulevard.

Regulatory Framework

In 2015, the State adopted Senate Bill (SB) 379, amending Section 65302(g) of the California Government Code to require cities and counties to update the Safety Element of their general plan to include more information about wildfire hazards, flooding risks, and other short-term and long-term threats posed by climate change. SB 379 is the foundation for adaptation and resiliency in general plan safety elements, as it requires local governments to conduct vulnerability assessments as part of their long-range public safety planning efforts and to prepare adaptation and resilience goals, policies, and implementation measures that respond to the findings of the vulnerability assessment and protect against harm caused by climate change.

Other important updates to Section 65302(g) of the California Government Code related to Safety Elements include SB 1035, SB 99, and Assembly Bill (AB) 747/1409. SB 1035, which established Section 65302(g)(6) of the California Government Code, builds on previous legislation and requires local governments to review and update as needed their Safety Element during an update to their Housing Element or Local Hazard Mitigation Plan (LHMP) (or no less than every eight years). Any revisions should include updated information related to flood hazards, fire hazards, and climate adaptation and resilience. SB 99 established Section 65302(g)(5) of the California Government Code and requires jurisdictions to review and update the Safety Element to include information identifying residential developments in hazard areas that do not have at least two emergency evacuation routes. AB 747 added Section 65302.15 to the California Government Code (amended by AB 1409), which became effective in January 2022, requires local governments to identify the capacity, safety, and viability of evacuation routes and locations in the Safety Element or LHMP on or before the next update of their LHMP. This Vulnerability Assessment, along with the update to the Safety Element of TOP 2050, supports the City of Ontario to meet these requirements.

The State of California prepared a guidance document, the [California Adaptation Planning Guide](#), to assist communities in addressing climate adaptation and resilience, and complying with Section 65302(g) of the California Government Code. This guide presents a step-by-step process for gathering the best-available climate change science, completing a climate change vulnerability assessment, creating adaptation strategies, and integrating those strategies into general plans and other policy documents. The City's

vulnerability assessment is consistent with the guidance and recommended methods provided in the *California Adaptation Planning Guide* and the *Resilient IE*.

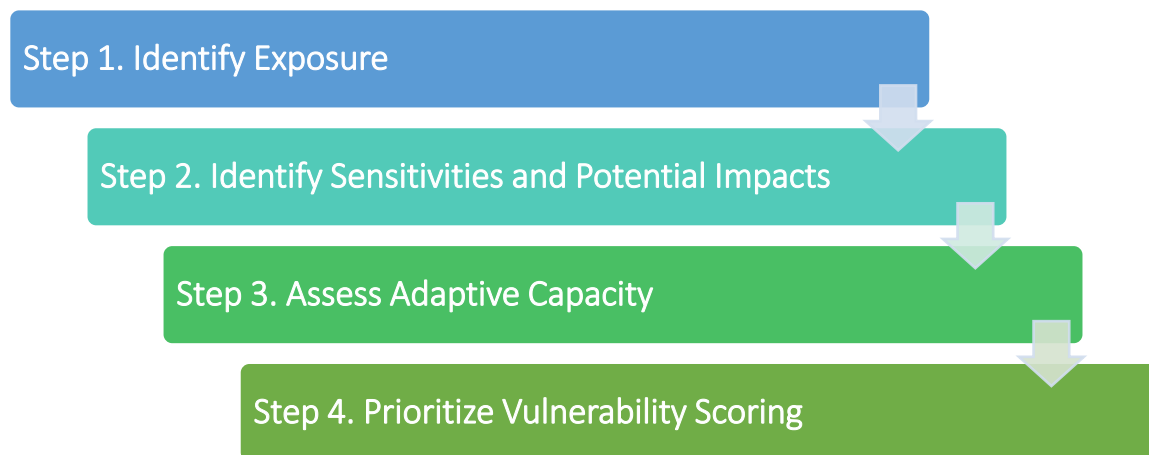
Climate Science Overview

Climate change is a long-term change in the average meteorological conditions in an area. Currently, the global climate is changing due to an increase in greenhouse gas (GHG) emissions that trap heat near the Earth's surface. While some levels of these gases are necessary to maintain a comfortable temperature on Earth, an increased concentration of these gases due to human activity traps additional heat, changing Earth's climate system in several ways. According to the *California Fourth Climate Change Assessment*, these effects cause primary climate stressors, which include warmer temperatures and changes in precipitation patterns, which can cause and exacerbate secondary climate stressors. These secondary climate stressors, also known as climate change hazards, can include flooding, severe weather, poor air quality, and drought conditions, among others. According to the *California Adaptation Planning Guide*, these hazards have the potential to cause fatalities, injuries, property and infrastructure damage, interruption of business, and other types of harm or loss. This Vulnerability Assessment Report presents the local and regional impacts created by climate change hazards and the ability of Ontario's populations and community to resist these hazards, to assess which aspects of the community are most vulnerable to climate change.

VULNERABILITY ASSESSMENT METHOD

The Vulnerability Assessment analyzes how a changing climate may harm the City of Ontario, and which aspects of the community – including people, buildings and infrastructure, services, and economic drivers – are most vulnerable to its effects. The vulnerability assessment primarily follows the recommended process published in the *California Adaptation Planning Guide* in 2020 by the California Governor’s Office of Emergency Services. This includes a four-step process: (1) characterizing the community’s exposure to current and projected climate hazards; (2) identifying potential sensitivities and potential impacts to community populations and assets; (3) evaluating the current ability of the populations and assets to cope with climate impacts, also referred to as its adaptive capacity; and (4) identifying priority vulnerabilities based on systematic scoring. **Figure 2** shows these steps.

Figure 2. California Adaptation Planning Guide Recommended Method



Step 1: Identify Exposure

The goal of this step is to characterize the community’s exposure to current and projected climate change hazards. Many projections of climate change hazards rely on multiple scenarios that reflect different levels of how global GHG emissions and atmospheric GHG concentrations may change over time. The Intergovernmental Panel on Climate Change (IPCC), an organization that represents the global scientific consensus about climate change, has identified four climate scenarios, also called Representative Concentration Pathways (RCPs), that can be used to project future conditions. RCPs are labeled with different numbers (e.g., RCP 2.6, RCP 6) that refer to the increase in the amount of energy that reaches each square meter of Earth’s surface under that scenario. The four RCPs are:

- **RCP 2.6:** Under this scenario, global GHG emissions peak around 2020 and then decline quickly.
- **RCP 4.5:** Under this scenario, global GHG emissions peak around 2040 and then decline.
- **RCP 6:** Global emissions continue to rise until the middle of the century.
- **RCP 8.5:** Global emissions continue to increase at least until the end of the century.

The [Cal-Adapt](#) database, which provides California-specific climate change hazard projections, uses RCP 4.5 for a low emissions scenario and RCP 8.5 for a high emissions scenario. The Governor’s Office of Planning and Research *Planning and Investing for a Resilient California* document and the *Adaptation Planning Guide* recommend using RCP 8.5 for analyses considering impacts through 2050, as there are minimal differences between emission scenarios for the first half of the century. This guide also recommends using RCP 8.5 for late-century projections, for a more conservative and risk-adverse approach. The City used the RCP 8.5 GHG emission scenario to input into global climate models on the Cal-Adapt database and other resources.ⁱ

Exposure: The presence of people, infrastructure, natural systems, and economic, cultural, and social resources in areas that are subject to harm.

Source: California Adaptation Planning Guide

The first step of this vulnerability assessment was to confirm which climate change hazards are expected to affect Ontario. The City identified eight climate change hazards for this assessment, listed here and discussed in more detail in the Climate Change Hazards section.

1. Agriculture Pests and Diseases
2. Air Quality
3. Drought
4. Extreme Heat and Warm Nights
5. Flooding
6. Human Health Hazards
7. Severe Weather
8. Wildfire and Smoke

The City derived the climate change hazard data from up-to-date information, including the state Cal-Adapt database, the *California Adaptation Planning Guide*, the *California 4th Climate Change Assessment*, the National Weather Service, the Federal Emergency Management Agency (FEMA), the *San Bernardino County Multi-Jurisdictional Hazard Mitigation Plan*, and the *San Bernardino County Vulnerability Assessment* conducted as part of Resilient Inland Empire (IE).

ⁱ The IPCC recently released “The Physical Science Basis” of the Sixth Assessment Report that updates global climate change projections for the near-term, mid-term, and long-term based on greenhouse gas emission trends from the past decade. It moves away from using RCPs, instead using five different scenarios called “shared socioeconomic pathways”, which consider socioeconomic trends underlying each scenario. This Vulnerability Assessment does not use these updated projections because at time of writing they are not available at a local scale. However, the IPCC report does reaffirm the use of projections comparable to RCP 8.5 as the suggested emission scenario to use for Cal-Adapt data.

Step 2. Identify Sensitivities and Potential Impacts

This step included evaluating past and potential future climate change impacts to community populations and assets. The City first identified a list of populations and assets to include in the assessment with the following five categories:

1. **Populations:** People that experience a heightened risk or increased sensitivity to climate change or have a lower capacity or fewer resources to adapt to or recover from climate impacts.
2. **Infrastructure:** Structures that provide essential services to Ontario community members and visitors.
3. **Buildings:** Homes, nonresidential buildings, and other building types.
4. **Important Economic Assets:** Properties and activities that make significant contributions to the Ontario economy.
5. **Key Community Services:** Important and essential functions to community members provided by government agencies and private companies.

Sensitivity: The level to which a species, natural system, or community, government, etc., would be affected by changing climate conditions.

Impact: The effects (especially the negative effects) of a hazard or other conditions associated with climate change.

Source: California Adaptation Planning Guide

This list included 17 populations, 17 infrastructure types, 9 building types, 7 economic drivers, and 9 key community services. After confirmation of this list, the City looked at which hazards are applicable in Ontario, or likely to affect which populations and assets, because not all hazards affect all populations or assets. For example, human health hazards are likely to impact most populations, but would not physically affect community parks or school buildings.

The outcome of this step was a matrix that identified whether a population or asset is likely to be exposed to a hazard. If a population or asset has the potential to be affected directly or indirectly by a hazard, a “yes” was indicated in the appropriate box. Direct impacts affect buildings and infrastructure, health or populations, or immediate operations of economic drivers or community services, and they can lead to indirect impacts on the broader system or community, including populations or asset types in a different category. For example, severe weather can *directly* damage electrical transmission lines causing power outages, which can *indirectly* impact persons with chronic illnesses who depend on the electricity for life support systems. Therefore, the City marked both electrical transmission lines and persons with chronic illnesses as “yes” for being affected by severe weather and would be evaluated in the assessment.

After the applicability review, the City evaluated potential impacts to the applicable populations and community assets. To identify how great the impacts of each relevant hazard are on the populations and community assets, the City considered a number of different questions that helped ensure the assessment broadly covered a range of potential harm. Examples of these questions include:

- Could the hazards cause injury or damage? Is there a risk of behavioral or mental harm, loss of economic activity, or other nonphysical effects?
- How many people or community assets could be harmed both directly and indirectly?
- How long would the impacts persist?
- Is there a substantial chance of death or widespread destruction?

Based on the results of the impact assessment, the City ranked each sensitivity as low, medium, or high impact for each relevant exposure. Impact is considered a negative quality, and therefore a higher impact score means that there is a higher potential for harm to a population or asset. A lower impact score means that there is a lower potential for harm to a population or asset. **Table 1** provides more detail about what each score means.

Table 1. Rubric for Impact Scoring

Impact Score	Meaning (People and Ecosystems)	Meaning (Buildings, Infrastructure, Services, and Economic Drivers)
Low Impact	Community members may not notice any change. If noticed, effect would be minor with only occasional disruptions.	Damage, interruption in service, or impacts on the local economy is small or intermittent enough to mostly go unnoticed. If noticed, effects are only minor.
Medium Impact	There is a marked impact to the community. Quality of life may decline. Impacts may be chronic, and at times substantial.	Damage, service interruptions, and other impacts are clearly evident. Impacts may be chronic and occasionally substantial.
High Impact	The well-being of the community declines significantly. The community's current lifestyle and behavior may no longer be possible. There is a severe risk of widespread injury or death to people, or of significant or total ecosystem loss.	Buildings, infrastructure, and services may be often or always cannot function as intended or needed to meet community demand. Large sections of the economy experience major hardships or are not feasible.

Step 3. Assess Adaptive Capacity

Adaptive capacity is the ability of populations and community assets to prepare for, respond to, and recover from the impacts of climate change. The City evaluated each population and assets for adaptive capacity by considering the following questions:

1. Are there existing programs, policies, or funding to provide assistance?
2. Are there barriers that limit response or recovery? Are these barriers, financial limitations, political challenges, lack of access to technology or other resources, or others?
3. Do alternatives exist in or near Ontario that community members can use?

Adaptive Capacity: The “combination of the strengths, attributes, and resources available to an individual, community, society, or organization that can be used to prepare for and undertake actions to reduce adverse impacts, moderate harm, or exploit beneficial opportunities”.

Source: California Adaptation Planning Guide

Based on the results of the adaptive capacity assessment, the City ranked each population or asset as low, medium, or high adaptive capacity. Adaptive capacity is considered a positive attribute, so a higher adaptive capacity score will mean that a population or asset may be more adaptable to the hazard. A lower adaptive capacity score means that a population or asset may have a harder time adjusting to the changing conditions. **Table 2** provides more detail about what each score means.

Table 2. Rubric for Adaptive Capacity Scoring

Adaptive Capacity Score	Meaning
Low Adaptive Capacity	Adaptive solutions are available, but they are expensive, technologically difficult, and/or politically unpopular. Alternatives may not exist that can provide similar services. Some assets may not have feasible means to adapt.
Medium Adaptive Capacity	Some adaptation methods are available, but not always feasible. Adapting may create significant challenges for some sensitivities. Some alternatives exist within the jurisdiction area that can provide similar services.
High Adaptive Capacity	Adaptation solutions are feasible for most or all sensitivities. There may be occasional or small-scale challenges to implementing adaptation methods, but populations and assets can adapt with little or no effort. Many alternatives exist in the area that can provide similar services.

Step 4. Prioritize Vulnerability Scoring

The City used the impact and adaptive capacity scores for each population and asset for each relevant hazard to determine the vulnerability score. The vulnerability (V) score reflects how susceptible the population or asset is to harm from a particular hazard. Vulnerability is assessed on a scale as low, medium, or high. The Vulnerability Scoring Matrix (**Figure 3**) shows how impact and adaptive capacity scores combine and translate into a vulnerability score. For example, extreme heat would create a high impact on energy delivery because mechanical failures, heat damage, and high demand for electricity from cooling equipment can disrupt this service. Adaptive capacity is low because many community members need to use more electricity on extreme heat days to keep cool, especially since over half of homes were built prior to 1980 and may not be well insulated and retrofitting electrical equipment can be expensive. Therefore, energy delivery services are highly vulnerable to extreme heat.

Vulnerability: The degree to which natural, built, and human systems are susceptible “...to harm from exposure to stresses associated with environmental and social change and from the absence of capacity to adapt”.

Source: California Adaptation Planning Guide

Figure 3. Vulnerability Scoring Matrix

		Impact Score		
		Low	Medium	High
Adaptive Capacity Score	Low	Medium	High	High
	Medium	Low	Medium	High
	High	Low	Low	Medium

HAZARDS OF CONCERN

As described in the *California Adaptation Planning Guide*, hazards are events or physical conditions that have the potential to cause fatalities, injuries, property and infrastructure damage, interruption of business, and other types of harm or loss. Some natural hazards, such as earthquakes, do not have a known connection with climate change, but have been included in this report to provide an overview of those hazards for TOP 2050.

Climate Change Hazards

Climate change hazards focus on natural hazards that can change in frequency and intensity due to climate change. The Vulnerability Assessment assesses the climate change hazards that are most relevant to the City of Ontario, as stated previously. This section discusses the climate change hazards based on projections provided by Cal-Adapt, the *California Fourth Climate Change Assessment*, the Resilient IE Toolkit, the *San Bernardino County Multi-Jurisdictional Hazard Mitigation Plan*, FEMA, and scholarly research.

AGRICULTURE PESTS AND DISEASES

According to the *2020 San Bernardino County Crop Report*, agriculture and livestock had total gross production of over \$420 billion in 2020, with milk, milk products, and cattle being the largest-grossing crop. Agricultural pests and diseases can affect livestock and crops supporting the livestock in the Ontario Ranch area of the city. This hazard is measured by the number of pests and disease incidents, which are likely to increase as higher temperatures allow insects to reproduce more rapidly. In 2020, the San Bernardino County Agriculture/Weights & Measures inspected 406 samples of incoming plant materials from other states and countries, of which, 44 samples contained pests or diseases and 69 samples were rejected.

These pests and diseases, such as the scales, mealybugs, whitefly, Asian Citrus Psyllid, and European Pine Shoot Moth, can slow the growth of plants and animals, damage them so that their products are less appealing and harder to sell, or even kill them. Though there are treatment options for many agricultural pests and diseases, some have no cure. Many pests and organisms that carry diseases are most active during warmer months, so the threat of infection or infestation is higher during that time of year. Projection trends show temperatures getting warmer earlier in the year and remaining warmer until later in the year due to increases in air temperature, which creates a wider activity window for pests and diseases. However, agriculture production within the City of Ontario is projected to transition to residential, mixed-use, and commercial land uses by 2050.

AIR QUALITY

The dominant sources of air pollution in the City of Ontario are ozone pollution from vehicle exhaust and agricultural soils, fine particulate matter and diesel particulate matter from vehicles and stationary sources, and smoke from wildfires in the region. According to the California Office of Health Hazard Assessment, in 2016, ozone levels in Ontario were higher than 83 percent of census tracts in California and fine particulate matter concentrations were higher than 97 percent of census tracts in California. Higher future temperatures will likely increase the production of ground-level ozone, especially in

Southern California valley cities like Ontario, which are already experiencing high levels of this pollutant. Ground-level ozone is associated with a variety of negative health outcomes, including reduced lung function, pneumonia, asthma, cardiovascular diseases, and premature death. Smoke from wildfires in the region can also increase air pollution levels and create a significant health risk in the region.

DROUGHT

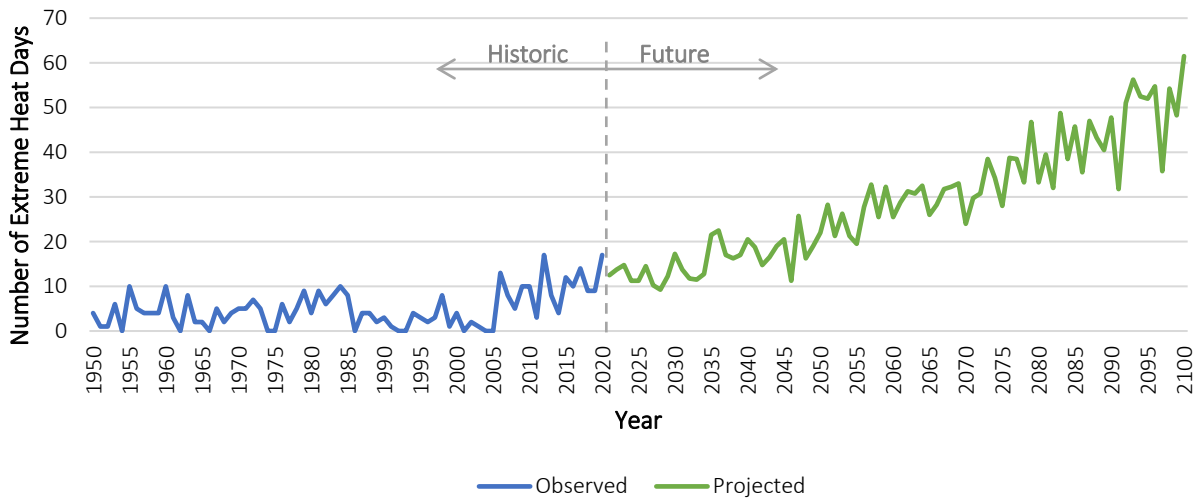
A drought occurs when conditions are drier than normal for an extended period, making less water available for people and ecosystems. Droughts are a regular occurrence in California; however, according to the *California Fourth Climate Change Assessment*, it is expected that climate change will lead to more frequent and more intense droughts statewide. According to the U.S. Drought Monitor, San Bernardino County and Ontario are in severe drought conditions and much of California is in Extreme or Exceptional Drought conditions. Based on the *Infrastructure Report for Hydrology, Sewer, Water, and Water Quality* prepared for TOP 2050, the City of Ontario receives water from three primary sources: (1) local groundwater sources; (2) imported water, primarily from the State Water Project, treated and distributed by the Water Facilities Authority, Chino Basin Desalter Authority, and the San Antonio Water Company; and (3) recycled water from the Inland Empire Utility Agency. Of these sources, most of the community's water supply is from the Chino Groundwater Basin (52 percent) and the State Water Project via the Rialto feeder and Agua de Lejos Treatment Plant in Upland (28 percent).

Snowpack levels in the Sierra Nevada, which feed the State Water Project, dropped by 25 percent during the 2011 to 2016 drought, and average springtime snowpack is expected to drop 64 percent by 2100. In 2021, the snowpack in the Northern Sierra was 70 percent of the average, but the rain was less than 50 percent of the annual average, making it the third-driest year on record. During drought conditions, water stored in the State Water Project's primary reservoirs could decrease due to lack of rainfall and reduction in snowpack due to higher temperatures. This can cause water shortages for water agencies relying on the State Water Project and heavier reliance on groundwater supplies to meet the needs of Ontario residents and businesses. The City of Ontario and Cucamonga Valley Water District directly provide water to residents and businesses, and both agencies have Water Shortage Contingency Plans as part of their 2020 Urban Water Management Plans. The Water Shortage Contingency Plans provide specific demand reduction actions to conserve water and ensure reliable supplies, including water restrictions for non-essential uses, increased use of recycled water, and penalties for violating water shortage provisions.

EXTREME HEAT AND WARM NIGHTS

Extreme heat occurs when temperatures rise significantly above normal levels. In Ontario, an extreme heat day occurs when temperatures reach above 103.2 °F. As shown in **Figure 4**, the number of extreme heat days in Ontario is projected to increase from a historic annual average (1961 to 1990) of 4 days per year to an average of 25 extreme heat days per year by mid-century and an average of 41 extreme heat days per year by end of century.

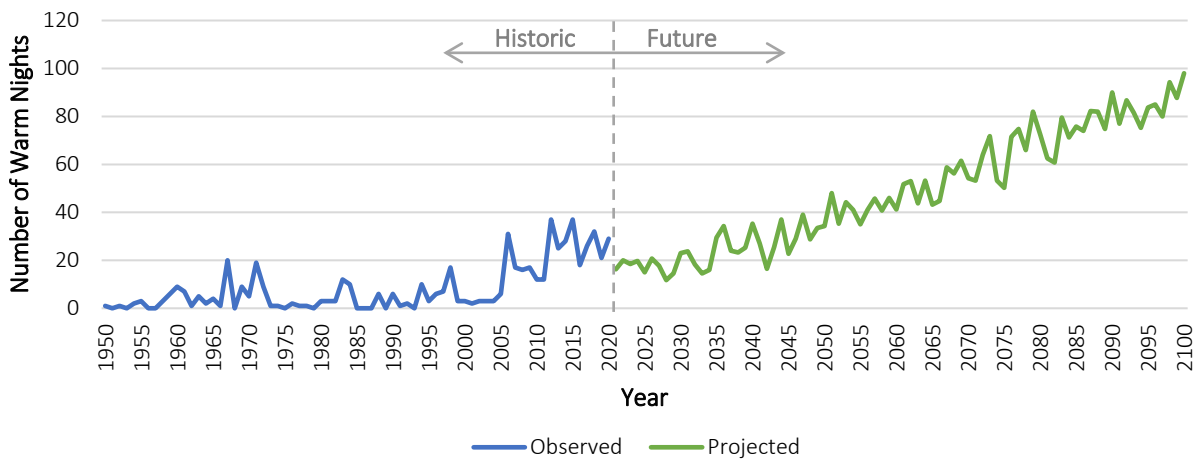
Figure 4. Projected Extreme Heat Days in Ontario



Sources: Cal-Adapt, 2021; National Weather Service, 2021

Extreme heat can also occur in the form of warmer nights, as temperatures do not cool down overnight and provide relief from the heat. In Ontario, a warm night occurs when the temperature remains above 68.6 °F. As shown in **Figure 5**ⁱⁱ, the number of warm nights in Ontario is projected to increase from a historic annual average (1961 to 1990) of 5 days per year to an average of 41 warm nights per year by mid-century and an average of 74 warm nights per year by the end of the century.

Figure 5. Project Warm Nights in Ontario



Sources: Cal-Adapt, 2021; National Weather Service, 2021

ⁱⁱ The visual change from 2020 and 2021 is due to using a combination of sources to fill in gaps from 2005 to 2020. The data from 1950 to 2005 and 2020 to 2100 is derived from Cal-Adapt. The data from 2006 to 2020 is from the National Weather Service. Therefore, the observed extreme heat days/warm nights differ slightly from the projections. This is showing that the observations exceed what the average projections are showing.

Extreme heat can cause heat-related illnesses, such as heat cramps, heat exhaustion, and heat stroke, in addition to exacerbating respiratory and cardiovascular conditions. Some homes in Ontario may lack air conditioning, and as a result, people living in these homes may be more susceptible to harm from extreme heat events. If homes have air conditioning, residents may find increased use cost prohibitive, especially for older or less-efficient systems, typically found in buildings built prior to 1990. Some types of infrastructure, including power lines and roadways, face greater stresses during high temperatures that make failure more likely.

FLOODING

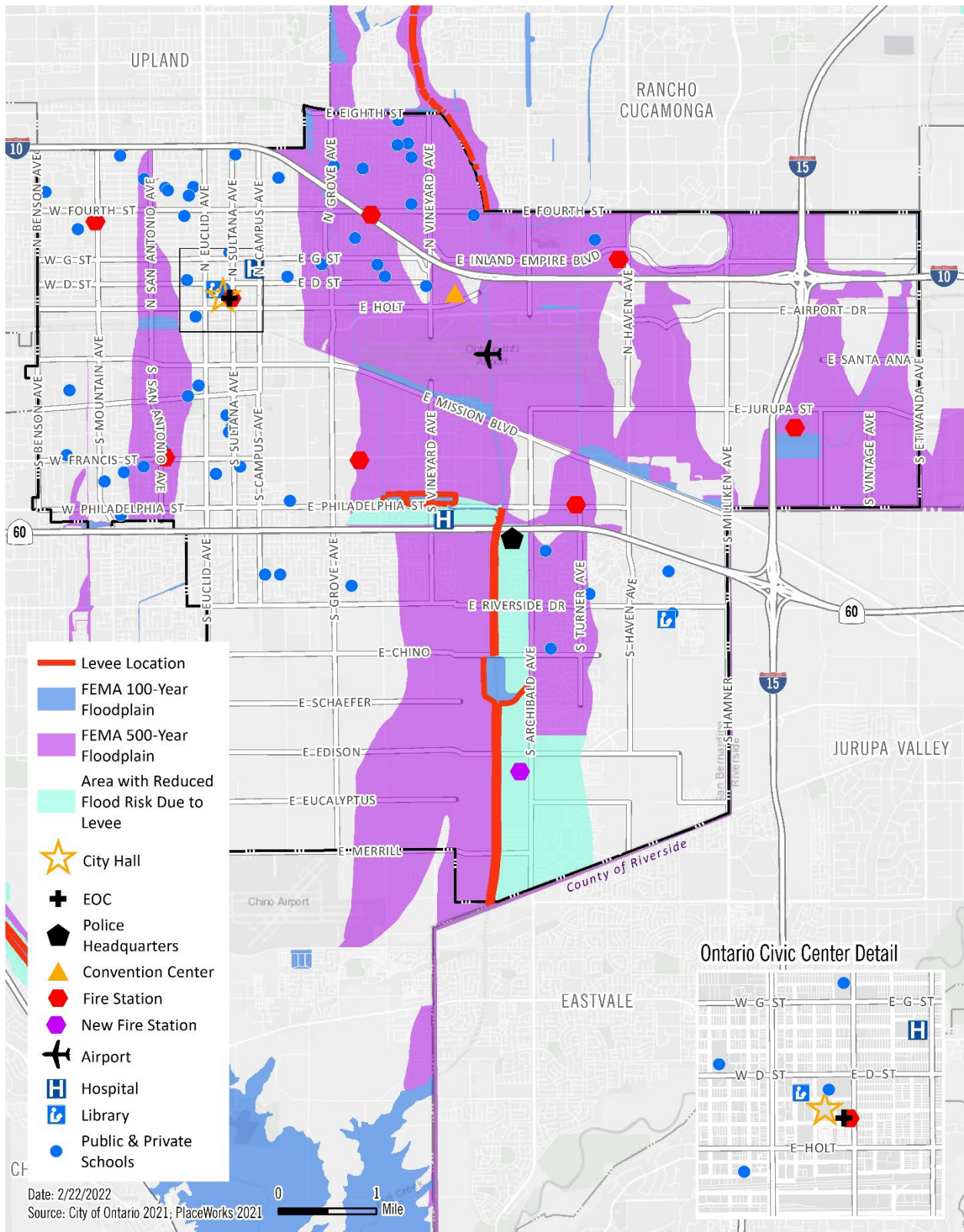
Flooding can cause significant harm to buildings, people, and infrastructure. Floodwater can be deep enough to drown people and may move fast enough to carry people or heavy objects (such as cars) away. Flooding can be caused by heavy rainfall, extended periods of moderate rainfall, or clogged drains during periods of rainfall. In rare instances, a break in a dam, water pipe, or water tank can also cause flooding. Flash floods are floods that develop very quickly, and they can be especially dangerous because they give little or no warning. Persons experiencing homelessness and others who may be outdoors in the path of a flood can face particularly high risks from these events.

Figure 6 shows the current 100-year and 500-year flood hazard areas within Ontario. FEMA defines a 100-year floodplain as areas with a one in 100-year chance, or 1 percent, annual chance of flooding in any given year and a 500-year floodplain as an area with a one in 500-year, or 0.2 percent, annual chance of flooding in any given year. According to the *City of Ontario 2018 Hazard Mitigation Plan*, approximately 580 acres, or 1.8 percent, of land within the city is within the FEMA-designated 100-year flood zone, and 26,526 acres, or 83 percent, of land is within the FEMA-designated 500-year flood zone, with 2,260 acres, or 7 percent, of land within the FEMA 500-year floodplain protected by a levee.

According to the *City of Ontario 2018 Hazard Mitigation Plan*, historic flooding has occurred periodically within Ontario, primarily consisting of urban street flooding due to the storm drain system exceeding capacity. According to the *California Fourth Climate Change Assessment, Los Angeles Regional Report*, storm drainage systems throughout the city collect stormwater runoff and convey water to prevent flooding, although these systems are typically designed based on winter storms recorded in the past and may not be designed to accommodate more extreme storms.

According to the *California Fourth Climate Change Assessment*, climate change will likely increase the frequency and intensity of floods within Ontario, although total annual precipitation levels are only expected to increase slightly. Up to half of California's precipitation comes from a relatively small number of intense winter storms, which are expected to become more intense with climate change. For example, what is currently a 100-year flood, or a flood that has a 1-percent chance of occurring annually, may occur every 20 or 50 years.

Figure 6. FEMA Flood Hazard Zones



HUMAN HEALTH HAZARDS

There are several diseases, such as hantavirus pulmonary syndrome, Lyme disease, West Nile virus, and influenza, which are linked to climate change and can be debilitating or fatal for some of the population. Pests, such as mice, rats, ticks, and mosquitos carry these diseases. Climate change can increase the rates of infections because many of the animals that carry diseases are more active during warmer weather and may expand in population size due to higher levels of rainfall during storm events and stagnant water after flooding, increasing the time for disease transmittal.

Some diseases and illnesses have the potential to become epidemics or pandemics if they spread within communities, regions, or over multiple countries. Epidemics and pandemics, such as the COVID-19 pandemic, can worsen existing health conditions as well as cause economic disruptions within the city and region. Additionally, following natural disasters, such as flooding or severe weather events, mental health and stress-related disorders can make people even more vulnerable. The City evaluated health hazards from air pollutants as part of the air quality hazard discussion.

SEVERE WEATHER

Severe weather can include high winds, hail, and lightning, which are usually caused by intense storm systems, although types of high winds and sandstorms can occur without a storm. Severe winds, such as the Santa Ana winds, tend to be most frequent during October to April and can have average speeds of 40 miles per hour. These winds can destroy buildings, knock over trees, damage power lines and electrical equipment, and fan small sparks into large wildfires in the region. Severe weather can also include heavy rainfall, which can cause flash floods and ponding in areas not protected by a levee in the city. While less common in the city, hail and lightning can damage the buildings and infrastructure supporting economic sectors and key services within the city. As described in the *Los Angeles Summary Report* from the *California Fourth Climate Change Assessment*, the connection between climate change and severe weather is not as well established as other hazards, but new evidence suggests that these forms of severe weather may occur more often than in the past.

WILDFIRE AND SMOKE

Wildfires are a regular feature of the landscape in much of California. They can be sparked by lightning, malfunctioning equipment, vehicle crashes, or many other causes. Warmer temperatures, an increase in drought conditions, and extreme wind events, are likely to create more fuel for fires in natural and rural areas, leading to a greater chance that a spark will grow into a potentially dangerous blaze. Climate change is also expected to extend the fire season throughout much (or even all) of the year.

According to the California Department of Forestry and Fire Protection, there are no fire hazard severity zones within or surrounding the City of Ontario and the city is not located within the Wildland-Urban Interface. The nearest Very High Fire Hazard Severity Zones are located in Upland and Rancho Cucamonga to the north, Fontana and Jurupa Valley to the east, and Norco and Chino Hills the south. According to the *City of Ontario 2018 Hazard Mitigation Plan*, there have been two major wildfires in the City of Ontario, including the 1958 Pole Line Fire in northeast Ontario, and the 2007 Walker Fire in southern Ontario.

While there are no fire hazard severity zones within the city limits, the smoke from wildfires increases air pollution levels, creating a significant health risk in the region. Most of the populations within Ontario have a high or medium vulnerability to wildfire and smoke conditions. In addition, planned Public Safety Power Shutoffs, which occur during red flag days when wind speeds are high and humidity is low, to prevent wildfires in the region have already impacted persons who depend on electricity for air conditioning or their medically necessary equipment. Public Safety Power Shutoffs can last for hours or days depending on the severity of the wind event and the wildfire conditions.

Natural Hazards Not Evaluated in the Vulnerability Assessment

Seismic and geologic hazards are caused by the movement of different parts of the Earth's crust, or surface. Seismic hazards include earthquakes and hazardous events caused by them. Geologic hazards are other hazards involving land movements that are not linked to seismic activity and are capable of inflicting harm to people or property. While climate change is unlikely to increase earthquake frequency or strength, the threats from seismic and geologic hazards are expected to continue in the future. The Safety Element of TOP 2050 contains mapping related to seismic and geologic hazards. However, these hazards were not included in the Vulnerability Assessment evaluation.

SEISMIC HAZARDS

Seismic activity occurs along boundaries in the Earth's crust, called faults. Pressure along the faults builds over time and is ultimately released, resulting in ground shaking that we refer to as an earthquake. Earthquakes can also trigger other hazards in Ontario, including surface rupture (cracks in the ground surface) and liquefaction (causing loose soil to lose its integrity). Earthquakes and other seismic hazards often damage or destroy property and public infrastructure, including roadways and utility lines. Additionally, falling objects or structures due to earthquakes pose a risk of injury or death. In Southern California, earthquakes have the greatest potential for loss of life and/or property and economic damage compared to other hazards, especially when they trigger secondary effects that overwhelm the ability of local jurisdictions to respond. Earthquakes can also cause human-caused hazards, such as urban fires, dam failures, and toxic chemical releases.

Earthquake risk is very high in Ontario due to the presence of several active faults in the region. Major fault zones in the region include the San Andreas Fault approximately 14 miles northeast of the city, the San Jacinto Fault approximately 7 miles northeast of the city, the Elsinore Fault approximately 3 miles southeast of the city, and the Sierra Madre Fault approximately 5 miles north of the city. These faults are all capable of producing earthquakes of magnitude 6.7 or greater. A major earthquake along any of these four faults could result in substantial casualties and damage resulting from collapsed buildings, damaged roads and bridges, fires, flooding, and other threats to life and property.

In the event of an earthquake, the location of the epicenter, as well as the time of day and season of the year, would have a profound effect on the number of deaths, injuries, and property damage. There are a number of small-scale earthquakes that happen weekly, but larger scale or catastrophic shaking is less likely. Property and human life in Ontario are at risk from a significant earthquake causing catastrophic damage and strains on response and recovery resources. Most of the loss of life and injuries from earthquakes are from damage and collapse of buildings and structures. The California Building Standards Code, adopted by reference into Ontario Municipal Code Title 8, *Building Regulations*, provides more

stringent requirements for new construction intended to protect life safety and prevent collapse of structures. However, in Ontario, structures built prior to the enactment of these improved building codes may not have been upgraded to current standards and could be vulnerable to earthquakes. Comprehensive hazard mitigation programs that include the identification and mapping of hazards, prudent planning and enforcement of building codes, and expedient retrofitting and rehabilitation of weak structures can significantly reduce the scope of an earthquake disaster.

Liquefaction is a potentially destructive secondary effect of strong seismic shaking. Liquefaction occurs when the force of an earthquake causes loosely packed sediment and saturated layers to lose strength and integrity and behave like a fluid. In addition to the composition of the soil—sand and gravel are more porous and hold more water—the liquefaction risk also depends on the height of the groundwater table. Areas with a shallow groundwater table, such as areas near water bodies, are at more risk of liquefaction. During an earthquake, highly saturated soil can lose much or all its stability, which can damage any structure built on it. In some cases of liquefaction, structures built on the soil may collapse completely. Liquefied soils may also damage or destroy underground utility lines. This can cause flooding if water lines are broken or create a risk of fire if there is damage to natural gas lines.

Liquefaction-related effects include loss of bearing strength, ground oscillations, lateral spreading, and flow failures or slumping. Site-specific geotechnical studies are the only practical and reliable way of determining the specific liquefaction potential of a site; however, a determination of general risk potential can be provided based on soil type and depth of groundwater. Ontario has delineated areas of known and potential liquefaction hazards in the New Model Colony in the Policy Plan of TOP 2050. Areas that contain loosely packed sandy or silty materials saturated with water, includes areas with a high groundwater table, are also vulnerable to liquefaction.

In most cases, proper design and construction of subgrade soils and building foundations provide a mechanism to mitigate the risk of seismic hazards to an acceptable level in conformance with the California Building Code. The representation of areas having a liquefaction potential is only intended as notification to seek further site-specific information and analysis of this potential hazard as part of future site development. It should not be solely relied upon, without site-specific information and analysis, for design or decision-making purposes.

GEOLOGIC HAZARDS

Subsidence is the major geologic hazard in Ontario. Subsidence refers to the sudden sinking or gradual downward settling and compaction of soil and other surface material with little or no horizontal motion. It may be caused by a variety of human and natural activities, including underground mining, groundwater extraction, sinkholes, or drainage and decomposition of organic soils. Most of the early documented cases of subsidence affected only agricultural land or open space. As urban areas have expanded, so too have the impacts of subsidence on structures for human occupancy. Although there is no data currently available documenting the precise areas where subsidence could occur, it is most likely to occur near active groundwater wells as a result of seismic shaking or changes in subsurface conditions.

POPULATIONS AND ASSETS

Populations and assets are the people, infrastructure, services, and economic drivers in the City of Ontario that can be affected by climate change. The Vulnerability Assessment looks at how each population and community asset may be affected by each of the climate change hazards discussed previously. The *California Adaptation Planning Guide* provides a general list of populations and assets, which the City of Ontario refined and used to develop five distinct asset categories: (1) populations, (2) buildings, (3) infrastructure, (4) economic drivers, and (5) key services. In total, Ontario identified 59 distinct populations and assets, as shown in **Table 3**. Appendix A provides additional details on the populations and assets selected for the Vulnerability Assessment. **Figure 1** shows the location of key community and critical facilities for the Vulnerability Assessment.

Table 3. Populations and Assets Included in the Vulnerability Assessment

CATEGORY	POPULATION OR ASSET
Populations	Children (under 10)
	Cost-burdened households
	Households in poverty
	Immigrants and refugees
	Linguistically isolated populations
	Low-income households
	Outdoor workers
	Overcrowded households
	Persons experiencing homelessness
	Person living in mobile homes
	Persons with chronic illnesses
	Persons with disabilities
	Persons without access to lifelines
	Renters
	Seniors (65+)
	Seniors living alone
Undocumented persons	
Infrastructure	Airports
	Bike routes
	Bridges
	Communication facilities
	Electrical substations and transmission lines
	Electric vehicle charging stations
	Evacuation routes
	Flood control infrastructure
	Hazardous materials sites
	Major roads and highways
	Natural gas pipelines
	Parks and open space
	Power plants

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VULNERABILITY ASSESSMENT REPORT-FINAL
APRIL 2022

CATEGORY	POPULATION OR ASSET
	Railways
	Solid waste facilities and landfills
	Transit stops
	Water and wastewater infrastructure
Buildings	Community centers
	Commercial business, shopping, and entertainment centers
	Homeless shelters
	Government buildings
	Homes and residential structures
	Libraries
	Medical and care facilities
	Public safety buildings
Schools	
Economic Drivers	Agriculture
	Education services
	Major employers
	Ontario International Airport
	Outdoor recreation
	Retail centers
	Warehouses and logistics centers
Key Services	Communication services
	Emergency medical response
	Energy delivery
	Freight and shipping
	Government administration & community services
	Public safety response
	Public transit access
	Solid waste removal
Water and wastewater treatment, delivery, and collection	

VULNERABILITY ASSESSMENT RESULTS

The Vulnerability Assessment evaluates the impact and adaptive capacity of each population and asset for each relevant hazard to assess vulnerability. As discussed in Section 2, Vulnerability Assessment Method, the City assigned vulnerability scores of low, medium, or high to reflect how susceptible the population or asset is to harm posed by the hazard. Out of the 472 possible hazard and asset pairing, the City assessed 302 different pairings for vulnerability. This section provides a summary of the key vulnerabilities within the city. For a complete list of vulnerability scores for all populations and assets, refer to Appendix B.

Population and Asset Considerations

While selecting and assessing various populations and assets to include in the Vulnerability Assessment, the City considered the differences in the population sample pool between datasets and how some populations and asset categories may appear to refer to the same thing.

Statistics, especially statistics related to population, use the concept of a “sample pool”. In the context of this Vulnerability Assessment, the sample pool draws from the overall group of people that are being measured or studied. For example, in a political poll among registered voters, the sample pool only includes registered voters, since unregistered voters do not fall into this category.

This concept is important because some of the demographics used in the Vulnerability Assessment have different sample pools. Most of the demographic data come from the U.S. Census Bureau’s American Community Survey (ACS), and most of these data have a sample pool of either all residents or all households in the city. However, a few are different, such as:

- Data on linguistically isolated populations only count people who are at least 14 years old instead of the total population, since young children generally are not proficient in any language.
- Statistics that only count the noninstitutionalized population (e.g., people not in prisons or long-term care homes).

This does not affect the outcome of the Vulnerability Assessment, but it creates slight differences in the number of people counted as part of each population.

Of the 59 populations and assets in the Vulnerability Assessment, a few may appear redundant. For example, the Vulnerability Assessment looked at both public safety buildings (as a Buildings asset) and at public safety response (as a Key Service asset). In the same way, the Vulnerability Assessment looks at frontline or underserved populations separately from the homes they live in or the industries where they work.

To be as comprehensive as possible, the Vulnerability Assessment looks at physical structures separately from the services or benefits they provide. This is because the effects of climate change on one type of population or asset can be different from the effects on related populations and assets. For example, if a flood damaged or blocked a major highway, such as I-10, it would have a significant impact on public safety services in and around the city. However, the loss of I-10 would not cause physical damage to public safety facilities, such as police or fire stations. Similarly, a drought can have a major effect on water and wastewater services by reducing the amount of water available for water supply and wastewater conveyance, but droughts have little or no physical effect on water and wastewater infrastructure, such as pipelines or pumps.

Populations

Of the 17 populations evaluated in the vulnerability assessment, 13 are highly vulnerable to at least one hazard type. Populations in Ontario are most vulnerable to extreme heat and flooding. The most vulnerable populations include households in poverty and undocumented persons. The list below summarizes the high vulnerabilities, grouping together populations where similar impacts or adaptive capacity occur.

Households with financial instability, which are households that may have limited incomes or be cost-burdened (paying at least a third of their gross income on housing costs), are highly vulnerable to all hazards, except agriculture pests and diseases, that will impact Ontario. These persons, especially households in poverty, are more likely to live in older homes with less insulation, less structural stability, or lack of air conditioning, increasing exposure to unsafe living conditions due to mold and mildew damage from flooding, high indoor air temperatures from extreme heat and warm nights, and poor indoor air quality from air pollution and severe weather. Households with financial instability may lack the financial means and health insurance to recover from property damage or illnesses caused by climate change hazards.

Persons that spend an extended amount of time outdoors, including children and outdoor workers, are highly vulnerable to air quality, extreme heat, flooding, human health hazards, severe weather, and smoke from wildfires. Children tend to spend more time outdoors than many adults and may not be aware of the onset of heat-related illnesses, such as heat exhaustion or heat stroke. Reduced air quality from smoke, ozone, or particulate matter can also decrease the ability of young children to adapt to warmer temperatures over time. Outdoor workers are directly exposed to extreme heat, human health hazards, and the poor air quality due to the outdoor nature of their occupations. Outdoor work often involves physically intense work, which can increase the risk of medical complications. Outdoor work can be halted during hazardous conditions, such as severe weather, poor air quality, or flooding, creating economic hardships for outdoor workers. This may also limit the ability of outdoor workers to seek medical attention for heat or air quality-related illnesses.

Persons with existing health conditions or limited mobility, such as seniors, seniors living alone, persons with chronic illnesses, persons with disabilities, and persons without access to lifelines, are highly vulnerable to air quality, extreme heat, human health hazards, severe weather, and wildfire smoke. Extreme heat, pathogens from human health hazards, and smoke from wildfires can cause cardiovascular and respiratory illnesses, which may be difficult for many people, especially those with compromised immune systems or chronic illnesses, to recover from. According to the Southern California Association of Governments, Ontario's obesity rate was nearly 40 percent and asthma rate was nearly 13 percent in 2018. Seniors, especially, are usually more susceptible to heat-related illnesses, pathogens, and smoke conditions because they are more likely to have medical conditions that can worsen with extreme heat and poor air quality, and often take medicine that makes it harder for them to stay cool. These individuals may also have difficulty evacuating during emergencies caused by flooding or severe weather. Power outages can also isolate these persons and potentially cause life-support systems to fail if battery backups are not available.

Persons with language barriers and citizenship uncertainty are highly vulnerable to multiple hazards. Most of these populations may be hesitant to seek help or may not qualify for financial assistance programs that can help them prepare and recover from a disaster. People with citizenship or immigration concerns, such as undocumented immigrants, are highly vulnerable to nearly all climate change hazards, because this population may not live in structures that adequately shelter from these hazards and they may be required to work in extreme heat or poor air quality conditions. According to the Asian Pacific Environmental Network's *Mapping Resilience Report*, these populations may not feel welcome or safe to participate in government-provided support services or assistance programs. If employers halt work because of hazardous conditions, undocumented persons may face financial hardship. These individuals may also live in low-lying areas or in structures that are less resilient to damage, and therefore are highly vulnerable to severe weather and flooding.

Persons experiencing homelessness lack permanent and often temporary shelters, which can leave them directly exposed to high temperatures, poor air quality, flooding, severe weather, and smoke from wildfires. These persons may not have access to cool locations, water, sunscreen, or protective equipment to increase resiliency to poor air quality, extreme heat, and human health hazards. Persons experiencing homelessness are also severely vulnerable to severe weather and may have a challenging time recovering if high winds and intense rain events damage or destroy temporary shelters and personal property.

Persons living in mobile homes are located throughout the city and are highly vulnerable to extreme heat, flooding, and severe weather because mobile homes are generally not as resilient as permanent structured homes, making them more susceptible to damage from intense rain events and high winds. Mobile homes may have less insulation than permanent structures, causing indoor air temperatures to rise during extreme heat events. Mobile homes can typically be retrofitted to protect against damage from flooding and severe storms; however, persons living in mobile homes may have lower or fixed incomes that can make it more difficult to make appropriate retrofits or repairs and mobile homes may be ineligible for recovery funding due to the home type.

Overcrowded households are highly vulnerable to human health hazards because persons in these households may be living in conditions that increase their chances of catching vector-borne illnesses. Persons living in overcrowded households may be unable to quarantine if a contagious illness is contracted. These persons may lack financial means to acquire different housing and may not be able to install air filtration devices.

Built Systems and Services

The built system includes vulnerabilities to infrastructure, buildings, and services that the City of Ontario and other organizations provide. The built systems are most vulnerable to flooding and severe weather. Of the 36 infrastructure, building, and service assets included in the assessment, 18 are highly or severely vulnerable (scored V4 or V5) for at least one hazard.

Public Transit Services and Infrastructure is highly vulnerable to poor air quality, extreme heat, flooding, severe weather, and wildfire, because these hazards create unhealthy conditions for those riding public transit and may disrupt public transit routes. Many of the Omnitrans bus routes and Metrolink railway are also within a flood hazard zone. Flooding and severe weather can prevent buses or other vehicles from

traveling on certain roadways in the city, causing delays or preventing people from using public transit. According to a recent survey conducted through Resilient IE, poor air quality, extreme heat, and smoke conditions can cause fewer people to use public transit, since it may be more difficult to wait outside for buses or trains. While there are some alternative roadways buses can use and shade structures can be installed to ensure people can access public transit, delays or negative health outcomes may be unavoidable.

Energy infrastructure and delivery services are highly or severely vulnerable to extreme heat, flooding, severe weather, and wildfire. Energy delivery services are dependent on regional and local overhead power lines and underground natural gas pipelines owned and operated by Southern California Edison (SCE) and Southern California Gas Company (SoCal Gas), which are susceptible to extreme heat, flooding, high winds, and wildfire.

Extreme heat can cause power outages due to mechanical failure of electrical equipment, heat damage to the above-ground infrastructure, and a high demand for electricity due to air conditioning units. Increased electricity usage for air conditioning during heat waves can overtax electrical transmission lines and transformers, which may disfunction or fail, causing power outages. Solar and wind energy production could also decrease due to extreme heat. According to the U.S. Department of Energy, solar photovoltaic panels experience a decrease in efficiency due to excessive heat of converting solar energy to electric energy due to an increase in current but a larger decrease in voltage. SCE and its partners incentivize reducing energy demand during peak-energy usage times of the day, which may help reduce the amount of energy demand on extreme heat days. Severe weather can also have wind speeds that cause transmission lines to sway in close proximity of each other, potentially leading to arcing. This can generate sparks, excessive heat, and damage the lines. High winds from severe weather can also cause SCE to conduct a Public Safety Power Shutoff and turn off electricity to prevent sparks, which disrupts energy delivery to the city. SCE and SoCal Gas can retrofit power lines and other equipment to insulate them against extreme heat events and severe weather, as well as underground utilities to protect them from severe winds and wildfires.

Evacuation route and transportation infrastructure are highly or moderately vulnerable to extreme heat, flooding, and severe weather. Extreme heat and prolonged periods of high temperatures can crack, heave, or deform roadway and railway materials, damaging major roads, transit corridors, and evacuation routes or making them difficult to drive on. Evacuation routes, such as I-10 and SR-60, have sections that are within the 100-year or 500-year floodplain. Floodwaters can cause these major routes, including bridges and underpasses, to become impassable and prevent people from evacuating during an emergency. The Ontario International Airport and Union Pacific railways are also in the 100-year or 500-year floodplain. Flooding can inundate runways, damage terminal buildings, and damage railway tracks, disrupting the regional transportation network. Roadways and railways can be retrofitted with materials with higher temperature thresholds, evacuation-designated roadways can be raised to prevent flooding, and the airport can be hardened to prevent damage to facilities. While Caltrans may have the funds for increased maintenance of roadways, it may be expensive for Ontario to increase maintenance and repairs on repetitive loss roadways.

Public safety services and buildings are highly vulnerable to flooding, which can cause the facilities supporting the services to be inundated and damaged. Three police stations and six fire stations are within the 100-year or 500-year floodplain. Floodwaters can damage these buildings or cause mold and mildew to grow, causing the space to become unusable and unhealthy indoor air quality. Chronic flooding may make these buildings difficult to maintain. Public safety response services rely heavily on major roads and highways, which can be blocked by floodwaters and prevent personnel from traveling through the city to respond to public safety needs. Alternative routes may be available, but not always the most efficient way to travel to important destinations.

Flood-control infrastructure is highly vulnerable to flooding and severe weather, which have the potential to overwhelm these systems and cause them to fail. During periods of heavy rainfall, flood-control channels and basins have a higher risk of overflowing their banks, causing flooding in residential, commercial, and industrial areas of Ontario. The flood-control systems and storm drains are maintained by the U.S. Army Corps of Engineers, California Department of Water Resources, the San Bernardino County Flood Control District, and Ontario Public Works Department; therefore, coordination would have to occur across agencies to maintain adequate flood-control structures.

Homes and residential structures are highly vulnerable to flooding and severe weather. Homes throughout the city are within the 100-year or 500-year floodplains and can be destroyed by large floods or flash floods. If flooding does not destroy a home, it can leave mold and mildew, making the structure uninhabitable without remediation, repair, and replacement. Homes, especially older homes, can also be damaged by high winds and other severe weather. The Community Action Partnership of San Bernardino County and La Cooperativa Campesina de California can help low-income residents and homeowners retrofit their homes and protect them from flooding and severe weather. However, some retrofits may not be economically feasible for all residents that do not qualify for this program.

Freight and shipping services are dependent on roadways and railways, and therefore are highly vulnerable to flooding and severe weather. Freight and shipping services can be disrupted if highways and major roadways become impassable during severe weather or flooding events. Delays in services may last days or weeks depending on the severity. Smaller freight and shipping services can use alternative roadways outside of flood zones to meet deliveries. However, large freight trucks and train cars may not be able to use alternative routes if they require designated truck routes.

Water and wastewater treatment, delivery, and collection are highly vulnerable to drought and flooding. Flooding can cause a wastewater treatment plant to be inundated with wet weather flows due to higher levels of stormwater, preventing the system from functioning properly. Failure of the wastewater treatment plant could cause sewer collection systems to back up and potentially overflowing raw sewage into streams and water systems. Water and wastewater systems can be retrofitted to prevent damage from flooding and ensure continuity of service. However, wastewater cannot be easily rerouted if the system is damaged or disrupted.

Drought conditions can stress water agencies that provide water to Ontario, as supplies from the State Water Project and groundwater may be significantly reduced due to lack of snowmelt into the system or recharge into the groundwater basins. Water agencies may apply demand-reduction actions, such as water restrictions for non-essential uses, increased use of recycled water, and penalties for violating

water shortage provisions. Water delivery agencies can also provide water rebate or retrofit programs to help businesses and residents in the area reduce overall water consumption.

Parks and open space are highly vulnerable to drought. Severe drought conditions would require agencies to curtail watering of parks, causing drought stress and plant damage that can limit aesthetic appeal and recreational potential. The City of Ontario has recycled water that can be used to irrigate parks and sports fields with highly treated wastewater. The installation of drought-tolerant plants and low-water use landscaping can also reduce water use and improve drought resiliency. However, these retrofits may be expensive for the city to complete.

Homeless shelters and supportive facilities are highly vulnerable to severe weather, which can be damaged by high winds, hail, and lightning. The services supported by these buildings may be prevented from providing adequate services if the facilities are damaged. Homeless shelters are typically managed by nonprofit or faith-based organizations that may not get financial support from local agencies and may not have the funding for repairs if their facilities are damaged. However, these facilities have the potential to obtain grant funding to be converted into multi-use resilience hubs with resilient energy options and retrofitted structures to minimize disruptions from high winds and flooding events.

Communication services, including phone, cable, and internet, are highly vulnerable to severe weather that includes high winds. This can cause power outages that turn off communication infrastructure, preventing businesses and residents from receiving emergency notifications and evacuation notices. Communication facilities can be retrofitted to prevent damage and keep communication capabilities on. However, there may not be many redundancies in the communication system in the city that can be brought online if others fail.

Emergency medical response is highly vulnerable to human health hazards, as an increase in vector-borne and other illnesses may cause the demand to outweigh the capacity of emergency medical response services. There may be shortages of health care facilities, equipment, pharmaceuticals, and personnel if health care workers become sick or if supply chains are disrupted. Local and regional medical centers and providers can strengthen medical supply chains and prepare emergency contingency plans for if or when human health hazards increase in frequency and intensity. However, this may take time and require extensive coordination with multiple agencies and companies.

Economic Drivers

Economic drivers are the main contributors to the economy and employment within the City of Ontario. Important economic sectors are most vulnerable to extreme heat and flooding. Of the seven economic assets included in the assessment, five were highly vulnerable to at least one hazard.

Outdoor recreation is highly vulnerable to poor air quality, extreme heat, and wildfire smoke. Outdoor recreation can be curtailed due to unhealthy air quality conditions resulting from increased ozone or particulate matter or high temperatures in the region. Intense physical exertion from running or bicycling can be dangerous during poor air quality conditions and extreme heat, and people must stay hydrated to avoid heat and respiratory illnesses. Although parks and open space are not in wildfire-prone areas, visitors and residents could be deterred from traveling to the city if smoke and extreme heat create unfavorable conditions, especially if water stations are not available at outdoor recreation sites.

Alternative locations for outdoor activities may be available in other parts of San Bernardino County, where temperatures are lower or there is better air quality, but these locations would likely not be in Ontario or nearby cities.

Agriculture, which consists of livestock and crops for livestock in Ontario, is highly vulnerable to drought and extreme heat. Many livestock species, especially cattle, require large amounts of water. The problem is particularly significant for animals raised on feed, as feed crops themselves require large amounts of water. Insufficient access to water can cause livestock growth to slow or stop and may increase the risk of sickness or animal mortality. Temperatures above 100 degrees can create heat stress, increasing the risk of infection, reducing milk production and fertility, and may lead to death, particularly among animals that are already stressed by illness. Livestock facility operators can purchase feed, reduce herd size, wean animals (as applicable) early, and seek assistance through government relief programs. Extreme heat is also highly harmful to livestock animals. Providing shade structures, misters, and making water available can help reduce heat stress. However, these may not be feasible options for all operations.

Ontario International Airport is severely vulnerable to extreme heat and flooding because airplanes may not be able to take off or land during dangerous conditions. During extreme heat events that reach 115°F to 120°F, aircraft may be unable to generate enough lift during take-off because of less dense air. This can ground airplanes and cause delays in air services that can last hours. Flooding can create unsafe conditions for planes to take off or land on the airport runways, preventing the airport from functioning properly. These events can delay air services that could last hours or days, depending on the severity. Visitors and residents that use Ontario International Airport may be able to use alternative airports, but these may not have the same commercial flights or be farther away.

Major employers, warehouses, and logistics centers are highly vulnerable to flooding. Several warehouses and logistics centers on the eastern portion of the city, which include many of the major employers in the city, are within the 100-year or 500-year floodplain. Warehouses can be damaged by floodwaters and impassable roadways, or freight rail, can prevent logistics centers from shipping the goods they need to. Other major employers may experience a disruption in business from flooding that blocks roadways and damages facilities, preventing the movement of goods, services, and people. Existing levee and flood channel systems prevent flooding of some facilities and roadways in the city. However, employees may not be able to get through major intersections to travel to warehouses, logistics centers, and major employers in the city until the water recedes.

IMPLICATIONS FOR TOP 2050

Emergency Preparedness and Response

The City is prepared to meet emergency situations, such as fire, medical, or hazardous events. The Ontario Office of Emergency Management, Fire Department, and Police Department conduct emergency preparedness and response activities in Ontario. The Office of Emergency Management leads the update and implementation of the City's Hazard Mitigation Plan and Emergency Operations Plan, which provides employee and citizen education in preparedness, or CERT programs, and trains City employees in disaster response, management, and recovery. The Ontario Fire Department provides a variety of public safety services, including fire protection, medical aid, rescue, vector control, sandbag distribution, hazardous materials response, and educational safety programs, such as CPR, first aid classes, and Fire Explorers. Other services include fire code enforcement and regulation, plan reviews, home, and business inspections, and fire code permits. The Ontario Police Department provides evacuation and traffic control during emergencies and disasters.

The Emergency Operations Center (EOC) at Ontario Fire Station No.1, provides a centralized location for emergency management in the event of a major emergency or disaster within the city. The EOC operations are directed by the Office of Emergency Management, emergency management staff (City Department heads), and representatives from organizations who are assigned emergency responsibilities (Red Cross, schools, hospitals, etc.). The EOC is equipped with essential administrative supplies to sustain operations for an extended period. In addition, all necessary forms, i.e., communications message forms, separate journals for each emergency service, shelter registration cards, volunteer registration forms, emergency requisition forms, and damage assessment survey sheets for all possible contingencies, are stocked in the EOC.

The City of Ontario uses AlertOntario, an Everbridge mass notification system, to notify the community and distribute emergency information and instructions before, during, and after a disaster. This system alerts all residents in the area about disasters, major emergencies, and other urgent information via text messages, email, phone, landline, and other means of communication. The Telephone Emergency Notification System is used by the San Bernardino County Sheriff and Fire Departments to quickly communicate information on impending dangers using telephone and text messages.

Other emergency alert systems include the Emergency Alert Systems (EAS) and the Emergency Digital Information System (EDIS). The EAS is a national public warning system commonly used by state and local authorities to deliver important emergency information, such as weather and AMBER alerts, to affected communities. EAS participants include radio and television broadcasters, cable systems, satellite radio, and television providers, and wireline video providers. FEMA, the Federal Communications System, and the National Oceanic and Atmospheric Administration's (NOAA's) National Weather Service work collaboratively to maintain the EAS and [Wireless Emergency Alerts](#), which are the two main components of the national public warning system and enable authorities at all levels of government to send urgent emergency information to the public. EDIS is a wireless emergency and disaster information service operated by the State of California Governor's Office of Emergency Services and is an enhancement to the EAS. These systems are available in multiple languages.

MUTUAL-AID AGREEMENTS

The City of Ontario participates in the California Master Mutual-Aid Agreement. The Standardized Emergency Management System have adopted the California Master Mutual-Aid Agreement, and is designed to ensure that adequate resources, facilities, and other support are provided to jurisdictions whenever their own resources are insufficient to cope with the needs of a given emergency. The State Office of Emergency Services Southern Region (Mutual Aid Region I) serves the mutual-aid region that encompasses San Bernardino County. Automatic aid pacts with the San Bernardino County Office of Emergency Services and local fire departments or offices of emergency services in nearby cities provide additional emergency management and response services in Ontario.

EVACUATION NEEDS

With advanced warning, evacuation can be effective in reducing injury and loss of life during a catastrophic event. Primary emergency access and evacuation routes in Ontario, as shown on Exhibit S-11 of the TOP 2050 Safety Element, include I-15, I-10, SR-83, and SR-60; Fourth Street, Holt Boulevard, Mission Boulevard, Riverside Drive, Chino Avenue, and Edison Avenue (east-west roadways); and South Mountain Avenue, Euclid Avenue, Grove Avenue, Archibald Avenue, and Haven Avenue, (north-south roadways). All evacuation routes in Ontario face a potential disruption from a flood or earthquake event, which may block roadways, damage the roadway surface, or collapse overpasses. In the event of widespread disruption to local evacuation routes, remaining evacuation routes may become congested, slowing down evacuation of the community or specific neighborhoods. This issue may be compounded since evacuation routes for Ontario will also likely serve as evacuation routes for surrounding communities, and so potential disruptions may have regional effects. An analysis of the City's roadway network and parcels conducted as part of The Ontario Plan preparation has determined that there are currently several residential parcels within the city with evacuation constraints, as shown on Exhibit S-10 of the TOP 2050 Safety Element. All parcels within an evacuation constraint are in at least one hazard-prone area and most are clustered in cul-de-sac designed neighborhoods that feed into the same roadway. The lack of multiple emergency access points in these neighborhoods limits roadway access for these properties, which may create congestion and difficulties if there is a need to evacuate.

Equity and Uncertainty

When addressing vulnerability and adaptation through TOP 2050 and the associated implementation plan, the *Adaptation Planning Guide* and *General Plan Guidelines* recommend consideration of equity and uncertainty.

Equity means that all people are justly and fairly included in society, and that everyone can participate, prosper, and achieve their full potential. Equitable climate adaptation planning involves identifying persons who are most vulnerable to climate change hazards, and ensuring that the planning process, distribution of resources, and efforts to address systematic wrongs are all conducted in an equitable manner. This Vulnerability Assessment identifies 17 vulnerable populations and assesses climate change impacts and the ability of these populations to prepare for, respond to, and recover from climate change hazards (see list in Populations and Assets Section).

Uncertainty is the second component to consider when determining how hazardous conditions may affect Ontario. Climate change is driven by the concentration of GHGs in the atmosphere, which is

affected by how our communities use resources and how we regulate those uses through local, state, federal, and international GHG-reduction goals, regulations, plans, and programs. As more action is taken to reduce GHG emissions, the less severe the effects of climate change are expected to be. Climate change models consider the concentrations of atmospheric GHG emissions and the changes in these levels over time to project future extent or intensity of hazardous events.

Even with the extensive modeling, potential impacts are projections of more likely future conditions and are not certain. Similarly, there is also substantial uncertainty about the future state of technology, socioeconomic conditions, and other factors. According to recent studies, the best approach to uncertainty is to prepare and adapt by monitoring how the future evolves and allow for adjustments over time as new climate data and studies are completed. The State and the City have ample evidence to support science-based policy and decision-making.

Adaptation and Resilience Opportunities

Ontario currently experiences a wide range of climate change hazards that are projected to increase in frequency and intensity in the future. While GHG reduction measures will help reduce the amount of carbon dioxide in the atmosphere, adaptation strategies will be needed to increase the resilience of residents and businesses in Ontario. The general plan is required to integrate adaptation measures into the update process that will help the community prepare for, respond to, and recover from climate change hazards.

- **Safety.** Due to the recent update of the California Government Code Section 653029(g) with the approval of SB 379, Safety Elements are required to address climate adaptation and resilience strategies. The Safety Element's goals, policies, and implementation actions can provide resilience strategies that support both reduced impacts and improved adaptive capacity of the community to climate change-related hazards, along with policies on required hazards, such as flooding, fire, and geologic hazards. Policies within this element can ensure that health, safety, and economic concerns of the community are met, even with an increase in frequency and intensity of climate change hazards. Examples of specific policies or implementation actions could include:
 - Create an extreme heat response plan that includes establishment of community cooling centers, weatherization of City buildings, and cooling strategies for persons engaged in outdoor work and persons experiencing homelessness.
 - Expand participation of programs and services that provide funding resources for economically disadvantaged households and businesses to conduct water conservation and weatherization retrofits.
 - Collaborate with Omnitrans and San Bernardino County Transportation Authority to help in evacuation efforts of seniors, persons with reduced mobility, and people without vehicles during emergency events.
 - Provide alerts about potential, developing, and ongoing emergency situations through extensive early-warning and notification systems that convey information to all residents, in multiple languages and formats to ensure it is widely accessible.

- Focus hazard mitigation planning in in evacuation constrained residential areas identified on Exhibit S-10, *Evacuation Constrained Areas*, of the TOP 2050 Safety Element.
- **Mobility.** The Mobility Element includes policies for roadway systems, public transit, airport environs, and regional transportation. Transportation and public transit infrastructure are included in the Vulnerability Assessment Results discussion above, and therefore resilience strategies for these systems would be appropriate to add to the Mobility Element goals and policies. Potential policies may include coordinating with regional transit providers to identify alternative routes and stops if normal infrastructure is damaged or closed as a result of flooding; raising or hardening roadways to protect them from flooding; ensuring heat-resistant materials are used on roadways; and implementation of other transportation-related resilience features. This element can also have evacuation related policies to improve evacuation access throughout the city, including requiring new development to have at least two ingress and egress routes.
- **Environmental Resources.** The Environmental Resources Element discusses water, wastewater, energy, and other utilities assessed as vulnerable in this Vulnerability Assessment. To address these vulnerabilities, policies can include funding efforts to underground electricity lines citywide, create redundancies in the communication infrastructure, and provide sustainable back-up power supplies. The City can also encourage or incentivize residential and on-site solar energy systems, especially when paired with battery storage to provide a resilient energy supply for homes. To protect infrastructure and services from flooding, policies can include the promotion of water conservation measures, low-impact development, and green stormwater infrastructure that can help convey stormwater. The Environmental Resources Element can also include policies to increase tree canopy cover, which can have the added benefit of reducing the urban heat island effect and filter out air pollutants.
- **Community Design.** The Community Design Element discusses the design of buildings, streets, and neighborhoods in Ontario. The design of streets, specifically for evacuation purposes, is included in the Emergency Response and Preparedness section above. To address neighborhood scale evacuation vulnerabilities, the Community Design Element can include policies for new neighborhoods to be designed to ensure at least two ingress and egress route in all new neighborhoods.
- **Social Resources.** The Social Resources Element includes goals and policies for health, education, community and leisure activities, libraries, and other community facilities. Many of these facilities, or the services they provide, are identified as vulnerable, as described earlier in this report. To address health impacts, the City can add policies to coordinate with San Bernardino County Public Health to ensure emergency and public health services can meet the needs of the population during poor air quality and extreme heat days, as well as human health hazard events.

The Social Resources Element also provides goals and policies for some public facilities, including community centers and libraries, throughout the city. [Resilient IE](#) provides a key resilience strategy for public facilities, the integration of physical and virtual resilience hubs. These can serve as centralized locations for resources about climate change, opportunities to reduce emissions, and techniques to increase resilience, showcases for sustainability, energy efficiency, and low carbon building, and to help residents obtain essential resources and information during and after a disaster. Examples of specific implementation actions could include:

- Identify equitably located existing facilities, outside of flood hazard zones, to serve as resilience hubs and cooling centers that open during emergencies or specific temperature triggers for residents to go to seek refuge from extreme heat days or emergency shelter.
- Coordinate with emergency management services to establish backup power, preferably from renewable energy sources, and water resources at emergency shelters, resilience hubs, and cooling centers in case of power outages.

Resilience in Other Planning Mechanisms

Resilience policies and programs should not be limited to TOP 2050. Adaptation and resilience rely on a cross-department, multi-disciplinary approach to successful implementation. The City should consider how adaptation and resilience can also be incorporated into other City plans, codes, projects, and implementation programs. Addressing climate change hazard events in TOP 2050 can support other essential safety documents, such as the Ontario Annex of the *San Bernardino County Multi-Jurisdictional Hazard Mitigation Plan*. Development standards such as residential building codes for buildings in flood zones and development of adequate evacuation routes can be integrated into the Ontario Municipal Code. Policies that focus on emergency response to hazards can be included in an evacuation plan or an emergency operations plan. Adaptation measures that also reduce GHG emissions can be integrated into the *Ontario Community Climate Action Plan* and may support the *Ontario Active Transportation Master Plan*. Coordination between the City and its energy providers can incentivize energy-related GHG reduction measures. Policies related to drought and flooding may be integrated into the City of Ontario's and Cucamonga Valley Water District's Urban Water Management Plans and the Ontario Storm Drain Master Plan. Furthermore, programs such as the Ontario Municipal Code and Capital Improvement Program can help implement the resilience policies developed in the General Plan through specific projects, development codes, and budgeting.

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APPENDIX A: LIST OF POPULATIONS AND ASSETS

List of Items to Include in the Ontario Plan Vulnerability Assessment

PlaceWorks proposed the following populations and other assets for inclusion in the Vulnerability Assessment. Each list includes a description and source of data needed to support the Vulnerability Assessment.

POPULATIONS

PlaceWorks collected population data from the U.S. Census, the California Healthy Places Index, and Homeless Point-in-Time Count. These populations include:

- Children (under 10)¹
- Cost-burdened households: households paying 30 percent or more of their income towards housing expenses²
- Households in poverty: households with an income below the poverty line, which is \$26,500 for a household of four in the United States.³
- Immigrants and refugees⁴
- Linguistically isolated populations: especially Spanish, Tagalog, and Chinese⁵
- Low-income households: households with an income of 80 percent or less of the median household income. The low-income threshold is approximately \$52,037 in Ontario.⁶ The State of California identifies \$77,500 as the low-income threshold for a household of four people in San Bernardino County.⁷
- Outdoor workers (e.g., construction, farmworkers, outdoor recreation workers, landscapers)⁸
- Overcrowded households: housing units that have 1.0 persons or more per room (excluding bathrooms and kitchens).⁹
- Persons experiencing homelessness: 2020 point-in-time count reported 28 sheltered and 74 unsheltered (102 total) persons experiencing homelessness in the city.¹⁰
- Persons living in mobile homes
- Persons with chronic illnesses
- Persons with disabilities¹¹
- Persons without access to lifelines: Persons without access to a car, transit, or communication systems.¹²
- Renters¹³
- Seniors (65+)¹⁴
- Seniors living alone¹⁵
- Undocumented persons

INFRASTRUCTURE

PlaceWorks collected infrastructure data from a variety of sources. These infrastructure assets include:

- Airports: Ontario International Airport (Data source: Caltrans, Division of Aeronautics)
- Bike routes: City routes, West Cucamonga Creek Trail System (Data source: City of Ontario website: Parks and Street Maintenance)
- Bridges (Data source: Caltrans GIS)
- Communication facilities (e.g., cell phone towers, ham radio antennae, AT&T switching buildings) (Data source: TBC)
- Electrical substations and transmission lines: 11 substations and transmission lines operated by Southern California Edison. (Data source: California Energy Commission)
- Electric vehicle charging stations: 27 public stations (Data source: Alternative Fuels Data Center)
- Evacuation routes (Data source: Resilient IE, 2020)
- Flood-control infrastructure: levees along Cucamonga Creek and the City's stormwater management system (Data source: DWR Levee Protection Zones)
- Hazardous materials sites (Data source: EnviroStor)
- Major roads and highways: I-10, I-15, SR-60 (Data source: Caltrans)
- Natural gas pipelines: Southern California Gas Company (Data source: Cal OES)
- Parks and open space: 30 City parks, including Creekside Park and Golf Course, De Anza Park, Guasti Regional Park, Homer Briggs Park, John Galvin Park, Munoz Park, Ontario Soccer Complex, Westwind Park, and Whispering Lakes Golf Course (Data source: Parks Department website)
- Power plants: 8 natural gas facilities, 11 solar energy facilities, 1 landfill gas facility, 3 battery energy storage facilities, and small-scale energy generation facilities (Data source: California Energy Commission)
- Railways: Alhambra and Los Angeles railway subdivisions, Metrolink, Amtrak, Union Pacific (Data source: California Active Rail Lines, Amtrak)
- Solid waste facilities and landfills: 8 facilities including Caltrans Upland Maintenance Station, Recycled Wood Products, Ontario Agricultural Commodities, Residual Recovery Group, Beneficial Agriculture Services, Recycled Wood Products Ontario 2, Partida Fertilizer, and Chino Valley Rock, all of which are active. (Data source: CalRecycle)
- Transit stops: Ontario Metrolink Station, East Ontario Metrolink Station, Omnitrans (Data source: SBCTA)
- Water and wastewater infrastructure: Inland Empire Utilities Agency Regional Water Recycling Plant, City of Ontario

BUILDINGS

PlaceWorks collected buildings data from Google maps, the Ontario Parks & Recreation Department, the City of Ontario land use GIS layer, and the California School Database. These assets include:

- Community centers: Armstrong Community Center, De Anza Community & Teen Center (designated cooling center), Anthony Munoz Community Center, Dorothy A. Quesada Community Center, Ontario Senior Center, Veterans Memorial Community Center, Westwind Community Center.

- Commercial business, shopping, and entertainment centers: Toyota Arena, Ontario Mills Shopping Center, Ontario Convention Center, Ontario Auto Center, Stater Bros Plaza, Archibald Ranch Town Center, Archibalds Plaza, Pacific Plaza, Ontario Airport Commerce Center, Ontario Business Center, Ontario Center, Prologis Park.
- Homeless shelters: Mercy House Ontario Access Center, House of Ruth, Foothill Family Shelter.
- Government buildings: Ontario City Hall and City facilities, San Bernardino Department of Human Services.
- Homes and residential structures
- Libraries: Ovitt Family Community Library (designated cooling center), Lewis Family Branch, South Ontario Library.
- Medical and care facilities: Healthcare Center of Bella Vista, Inland Christian Home, Kaiser Foundation Hospital, Kindred Hospital Ontario, Las Colinas Post Acute, Ontario Dialysis Center, Ontario Healthcare Center, Ontario Holt Dialysis Center, Ontario Mills Dialysis (48 total – only medical facilities with patient capacity of 20 or more listed)
- Public safety buildings: City of Ontario Police Department, Ontario Policy Department Mills Station, City of Ontario Fire Department Stations #1, #2, #3, #4, #5, #6, #7, #8, #9, #10, and San Bernardino County – West Valley Detention Center.
- Schools: Cambridge College, American Career College, Pacific College Inland Empire, Platt Collage, SJVC Ontario, Carrington College, University of La Verne College of Law, 8 private schools, 1 unaffiliated pre-school, 5 high schools with CIUHSD, 2 elementary schools with CSD, 3 middle or elementary schools with CVUSD, 4 middle or elementary schools with MVSD, 22 middle or elementary schools with OMSD.

ECONOMIC DRIVERS

PlaceWorks determined important economic assets based on the 2020 Comprehensive Annual Financial Report and land uses surrounding the City of Ontario. These assets include:

- Agriculture
- Education services
- Major employers: Ontario International Airport, United Parcel Service, Ontario Montclair School District, Niagara Bottling LLC, Chaffey Joint Union High School District, QVC Ontario LLC, Home Depot, ULINE, FedEx, Cardinal Health.¹⁶
- Ontario International Airport
- Outdoor recreation (i.e., walking, biking, and court or field sports)
- Retail centers
- Warehouses and logistics centers

KEY COMMUNITY SERVICES

These assets are based on typical services provided in cities throughout California, which are supported by the infrastructure and buildings listed above. Key community services include the operation and functions needed to provide and maintain services instead of just the structures that support them. These assets include:

- Communication services
- Emergency medical response: City of Ontario Fire Department
- Energy delivery: Southern California Edison, Southern California Gas Company
- Freight and shipping
- Government administration and community services
- Public safety response: City of Ontario Police Department
- Public transit access: Metrolink, Omnitrans, SBCTA
- Solid waste removal: Ontario Integrated Waste Management
- Water and wastewater treatment, delivery, and collection: City of Ontario Municipal Utilities Company, Inland Empire Utilities Agency Regional Water Recycling Plan No. 1.

Endnotes for Appendix A

- ¹ American Community Survey. 2019. 2019: ACS 5-Year Estimates, Age and Sex, S0101.
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- ⁸ American Community Survey. 2018. 2018: ACS 5-Year Estimates, B24011 and B24021.
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- ¹⁰ County of San Bernardino. 2020. San Bernardino County Homeless Count and Subpopulation Survey Final Report.
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¹³ American Community Survey. 2019. 2019: ACS 5-Year Estimates, Financial Characteristics, S2503.
<https://data.census.gov/cedsci/table?t=Financial%20Characteristics&g=1600000US0653896&tid=ACSST5Y2019.S2503&hidePreview=false>.

¹⁴ American Community Survey. 2019. 2019: ACS 5-Year Estimates, Age and Sex, S0101.
<https://data.census.gov/cedsci/table?q=Ontario&t=Age%20and%20Sex&tid=ACSST5Y2019.S0101&hidePreview=false>.

¹⁵ American Community Survey. 2019. 2019: ACS 5-Year Estimates, Selected Social Characteristics in the United States, DP02.
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¹⁶ City of Ontario. June 2020. Comprehensive Annual Financial Report.
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APPENDIX B: VULNERABILITY ASSESSMENT RESULTS MATRIX

POPULATIONS AND ASSETS	AGRICULTURE PESTS & DISEASES	AIR QUALITY	DROUGHT	EXTREME HEAT & WARM NIGHTS	FLOODING	HUMAN HEALTH HAZARDS	SEVERE WEATHER	WILDFIRE & SMOKE
Populations								
Children (Under 10)	-	High	Low	High	High	Medium	Medium	High
Cost-burdened households	-	Medium	Low	Medium	Medium	Low	Low	Medium
Households in poverty	-	High	High	High	High	High	High	High
Immigrants and refugees	Low	High	Medium	High	High	High	High	High
Linguistically isolated populations	-	Medium	Low	Medium	Medium	Medium	Medium	Medium
Low-income households	-	Medium	Medium	Medium	Medium	Medium	Medium	Medium
Outdoor workers	Medium	High	Medium	High	High	High	High	High
Overcrowded households	-	Medium	Low	Medium	Medium	High	Low	Medium
Persons experiencing homelessness	-	High	Medium	High	High	High	High	High
Persons living in mobile homes	-	Medium	Low	High	High	Medium	High	Medium
Persons with chronic illnesses	-	High	High	High	Medium	High	High	High

POPULATIONS AND ASSETS	AGRICULTURE PESTS & DISEASES	AIR QUALITY	DROUGHT	EXTREME HEAT & WARM NIGHTS	FLOODING	HUMAN HEALTH HAZARDS	SEVERE WEATHER	WILDFIRE & SMOKE
Persons with disabilities	-	Medium	Low	Medium	High	Medium	Medium	Medium
Persons without access to lifelines	-	Medium	Medium	High	High	Medium	Medium	Medium
Renters	-	Medium	Low	Medium	Medium	Low	Low	Medium
Seniors (65+)	-	High	Low	High	High	High	Medium	High
Seniors living alone	-	High	Low	High	High	High	High	High
Undocumented persons	Medium	High	High	High	High	High	High	High
Infrastructure								
Airports	-	-	-	Medium	High	-	Medium	-
Bike routes	-	-	-	Low	Medium	-	Low	-
Bridges	-	-	-	Medium	High	-	Medium	-
Communication facilities	-	-	-	Medium	Low	-	Low	Low
Electrical substations and transmission lines	-	-	-	High	High	-	High	Medium
Electric vehicle charging stations	-	-	-	Medium	Medium	-	Low	-

POPULATIONS AND ASSETS	AGRICULTURE PESTS & DISEASES	AIR QUALITY	DROUGHT	EXTREME HEAT & WARM NIGHTS	FLOODING	HUMAN HEALTH HAZARDS	SEVERE WEATHER	WILDFIRE & SMOKE
Evacuation routes	-	-	-	High	High	-	High	Low
Flood control infrastructure	-	-	-	-	High	-	High	-
Hazardous materials sites	-	-	-	Low	Low	-	Low	-
Major roads and highways	-	-	-	High	High	-	High	Low
Natural gas pipelines	-	-	-	-	Low	-	-	-
Parks and open space	Low	-	High	Low	Medium	-	Low	-
Power plants	-	-	-	Medium	Medium	-	Low	-
Railways	-	-	-	High	High	-	Medium	Low
Solid waste facilities and landfills	-	-	-	Low	-	-	Low	-
Transit stops	-	-	-	-	Medium	-	Low	-
Water and wastewater infrastructure	-	-	Low	-	Medium	-	-	-

POPULATIONS AND ASSETS	AGRICULTURE PESTS & DISEASES	AIR QUALITY	DROUGHT	EXTREME HEAT & WARM NIGHTS	FLOODING	HUMAN HEALTH HAZARDS	SEVERE WEATHER	WILDFIRE & SMOKE
Buildings								
Community centers	-	-	-	Low	Medium	-	Medium	Low
Commercial business, shopping, and entertainment centers	-	-	-	Low	Medium	-	Low	Low
Homeless shelters	-	-	-	Medium	-	-	High	Medium
Government buildings	-	-	-	Low	-	-	Low	Low
Homes and residential structures	-	-	-	Medium	High	-	High	Medium
Libraries	-	-	-	Low	-	-	Medium	Low
Medical and care facilities	-	-	-	Medium	Medium	-	Low	Low
Public safety buildings	-	-	-	Low	High	-	Low	Low
Schools	-	-	-	Medium	Medium	-	Medium	Medium

POPULATIONS AND ASSETS	AGRICULTURE PESTS & DISEASES	AIR QUALITY	DROUGHT	EXTREME HEAT & WARM NIGHTS	FLOODING	HUMAN HEALTH HAZARDS	SEVERE WEATHER	WILDFIRE & SMOKE
Economic Drivers								
Agriculture	Medium	Medium	High	High	Medium	Medium	Medium	Medium
Education services	-	Medium	-	Medium	Low	Medium	Low	Medium
Major employers	-	Low	Medium	Low	High	Low	Low	Low
Ontario International Airport	-	Medium	-	High	High	Low	Medium	Medium
Outdoor recreation	Low	High	-	High	Low	Medium	Medium	High
Retail centers	-	Low	-	Medium	Medium	Medium	Medium	Medium
Warehouses and logistics centers	-	Low	-	Low	High	Low	Low	Low
Key Services								
Communication services	-	-	-	Low	Low	-	High	Low
Emergency medical response	-	Medium	-	Medium	Medium	High	Medium	Medium
Energy delivery	-	-	Low	High	Medium	-	High	High
Freight and shipping	-	-	-	Medium	High	Low	High	Low
Government administration & community services	-	-	-	Low	Low	Low	Low	Low

POPULATIONS AND ASSETS	AGRICULTURE PESTS & DISEASES	AIR QUALITY	DROUGHT	EXTREME HEAT & WARM NIGHTS	FLOODING	HUMAN HEALTH HAZARDS	SEVERE WEATHER	WILDFIRE & SMOKE
Public safety response	-	-	-	Medium	High	Medium	Medium	Medium
Public transit access	-	High	-	High	High	Low	High	High
Solid waste removal	-	Medium	-	Medium	Medium	Low	Medium	Medium
Water and wastewater treatment, delivery, and collection	-	-	High	Medium	High	-	Medium	Medium

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING THE CITY COUNCIL APPROVE FILE NO. PADV22-002 TO AMEND THE COMMUNITY CLIMATE ACTION PLAN, AN IMPLEMENTATION COMPONENT OF THE ONTARIO PLAN SUPPORTING THE SUPPLEMENTAL ENVIRONMENTAL IMPACT REPORT (SCH No. 2021070364).

WHEREAS, the City of Ontario Certified the Environmental Impact Report prepared for The Ontario Plan and associated Statement of Overriding Considerations, and issued Resolution No. 2010-003; and

WHEREAS, the City of Ontario adopted the Policy Plan (General Plan) as part of the component framework for The Ontario Plan on January 27, 2010, and issued Resolution No. 2010-004; and

WHEREAS, on July 17, 2021, the City of Ontario adopted a Municipal Climate Action Plan and issued Resolution No. 2012-055, and on December 16, 2014, The City of Ontario adopted the Community Climate Action Plan and issued Resolution No. 2014-122, to comply with The Ontario Plan in January 2010. Since the adoption of the Municipal and Community Climate Action, the City has made progress to the 2020 Greenhouse Gas Reduction Target of 30% Below 2020 Business-as-Usual through implementation of 42 environmental sector measures; and

WHEREAS, the 2022 Update to the Ontario Community Climate Action Plan (CCAP) serves as the strategic plan for how the City of Ontario (City) will reduce greenhouse gas (GHG) emissions and foster a sustainable community through 2050 and beyond; and

WHEREAS, the 2022 CCAP Identifies and updates sources of both municipal and community GHG emissions within the City's municipal boundaries for the calendar year of 2019 and estimates how these emissions may change over time; and

WHEREAS, the 2022 CCAP serves as the programmatic tiering document for review of the climate change impacts of projects under CEQA. The 2022 CCAP is included in the Supplemental Environmental Impact Report (SEIR) for The Ontario Plan 2050; and

WHEREAS, the 2022 CCAP includes a greenhouse gas inventory baseline for the Year 2019, a future "business-as-usual" projection of greenhouse gas emissions for the Year 2030 and Year 2050, an emissions reduction target consistent with the State's emissions reduction goals and an implementation plan identifying specific measures and a timeline for implementation of said measures to reach the reduction targets for Year 2030 and Year 2050; and

WHEREAS, the SEIR for File Nos. PGPA22-002 and PADV22-002 were circulated for a 45-day public review period and a notice of its availability was published in a local newspaper and posted in the Office of the Clerk of the Board of Supervisors of San Bernardino County; and

WHEREAS, by way of the 2022 CCAP's Implementation Plan, the City Council directs Staff to implement the 2022 CCAP's measures in phases; and

WHEREAS, the Staff has authority to revise and/or adjust said measures as required and provide yearly updates, led by the City's Sustainability Program Manager; and

WHEREAS, the CCAP meets the functional and legal mandate of a general plan; and

WHEREAS, on July 26, 2022, the Planning Commission of the City of Ontario conducted a hearing and recommended the City Council approve the 2022 Community Climate Action Plan; and

WHEREAS, all other legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. Based on the entire record before the Planning Commission and all written and oral evidence presented to the Planning Commission, the Planning Commission recommends that the City Council finds that the TOP 2050 SEIR, which includes the 2022 CCAP, complies with CEQA (Public Res. Code, § 21000 et seq.), the State CEQA Guidelines (14 CCR § 1500 et seq.), and the City's CEQA Guidelines. The Planning Commission further recommends that the City Council find that the SEIR was prepared in compliance with CEQA, that the City complied with CEQA's procedural and substantive requirements, and that the SEIR is an accurate and objective statement that fully reflects the independent judgement of the City.

SECTION 2: Planning Commission Action. As the recommending body for the Project, the Planning Commission does hereby make the following recommendations:

1. The 2022 CCAP meets the requirements of TOP 2050 SEIR and its mitigation measures and identifies goals and strategies to reduce GHG emissions 40 percent below 1990 levels by 2030 and 80 percent below 1990 levels by 2050; and

2. The 2022 CCAP serves as the programmatic tiering document for review of the climate change impacts of projects under CEQA.

SECTION 3: **Recommendation.** Based upon the findings and conclusions set forth in Sections 1 and 2 above, the Planning Commission hereby recommends that the City Council approve an update to the CCAP.

SECTION 4: **Custodian of Records.** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 5: **Certification to Adoption.** The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26th day of July 2022, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Jim Willoughby
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____, was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on July 26, 2022, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

COMMUNITY CLIMATE ACTION PLAN

A Component of The Ontario Plan 2050



Ontario Community Climate Action Plan

Public Review Draft
May 2022





Ontario Community Climate Action Plan

Public Review Draft | May 2022



Prepared By: PlaceWorks

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ACKNOWLEDGEMENTS

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Sergeant Joe Estrada, Police Department

TABLE OF CONTENTS

List of Abbreviations

Executive Summary	ES-1
Greenhouse Gas Inventory and Forecast	ES-3
Greenhouse Gas Reduction Strategies	ES-5
1. Introduction	1
Purpose of the Community Climate Action Plan	1
2014 Community Climate Action Plan	1
2022 Community Climate Action Plan	2
How to Use This Plan	3
2. Background	5
Community Profile and History	5
Sustainability in Ontario	7
Climate Change and Equity	9
Regulatory Framework	13
3. Ontario’s Greenhouse Gas Emissions	23
Climate Science	23
Local GHG Emissions	25
Community-Wide GHG Inventory	26
Community-Wide Forecast	41
4. GHG Reduction Targets	45
Background	45
2014 CCAP GHG Reduction Targets	45
State Targets and Recommendations	45
2022 CCAP GHG Reduction Targets	46

5. Greenhouse Gas Emission Reduction Strategy	47
Introduction	47
Calculating Credit	47
Progress Toward GHG Reduction Goals	47
New GHG Reduction Strategies	50
Summary of Total GHG Emissions Reductions.....	73
6. Implementation	75
Implementation Strategies	76
Work Plan	77
7. Glossary	85
Appendix A. GHG Technical Appendix.....	A-1
Community-Wide Forecast	A-1
State GHG Emission Reductions.....	A-5
Technical Data for Existing and Planned Activities.....	A-6

TABLES

Table ES-1	Ontario Community-Wide Per Capita GHG Emissions by Sector, 2008 to 2019 (MTCO _{2e})	ES-4
Table ES-2	Ontario Community-wide GHG Emissions Forecast, 2008 to 2050 (MTCO _{2e}).....	ES-5
Table ES-3	Ontario Community-Wide Emission Reductions from Existing State, Regional, and Local Actions, 2030–2050	ES-6
Table ES-4	List of 2022 CCAP GHG Reduction Strategies	ES-6
Table ES-5	Progress to Absolute and Per Capita GHG Emissions Targets.....	ES-11
Table ES-6	List of Implementation Strategies	ES-11
Table 1	Emissions Factors, 2008 to 2019	27
Table 2	City of Ontario Community-Wide Demographic Change, 2019.....	29
Table 3	Annual Per Capita GHG Emissions by Sector, 2008 to 2019	30
Table 4	Proportions of Annual Per Capita GHG Emissions by Sector in 2008 to 2019	31
Table 5	Transportation Activity Data and Per Capita GHG Emissions by Subsector, 2008 to 2019	33
Table 6	Nonresidential Electricity Activity Data and Per Capita GHG Emissions, 2008 to 2019	33

Table 7	Residential Electricity Activity Data and Per Capita GHG Emissions, 2008 to 2019	34
Table 8	Nonresidential Natural Gas Activity Data and Per Capita GHG Emissions, 2008 to 2019	35
Table 9	Residential Natural Gas Activity Data and Per Capita GHG Emissions, 2008 to 2019	35
Table 10	Solid Waste Activity Data and Per Capita GHG Emissions by Subsector, 2008 to 2019	36
Table 11	Off-Road Equipment Per Capita GHG Emissions by Subsector, 2008 to 2019	37
Table 12	Agriculture Activity and Per Capita GHG Emissions by Subsector, 2008 to 2019	38
Table 13	Water and Wastewater Activity Data and Per Capita GHG Emissions by Subsector, 2008 to 2019.....	39
Table 14	Land Use and Sequestration Activity Data and GHG Emissions by Subsector, 2008 to 2019.....	40
Table 15	Stationary Source GHG Emissions, 2008 to 2019	41
Table 16	City of Ontario Community-Wide Demographic Projection, 2019 to 2050.....	41
Table 17	Forecast Per Capita GHG Emissions, 2019–2050	43
Table 18	Per Capita Targets for Ontario	46
Table 19	Ontario Per Capita GHG Emission Reductions from State Actions, 2019 to 2050	49
Table 20	Absolute and Per Capita GHG Emission Reduction Potential by Sector	73
Table 21	Progress to Absolute and Per Capita GHG Emissions Targets.....	74
Table 22	GHG Reduction Strategy Implementation Work Plan	79
Table A-1	City of Ontario Community-Wide Demographic Projection, 2019 to 2050....	A-1
Table A-2	Forecast Per Capita GHG Emissions, 2008–2050	A-2
Table A-3	Ontario Community-Wide Forecast Activity Data.....	A-4
Table A-4	Ontario Per Capita GHG Emission Reductions from State Actions, 2019 to 2050	A-6
Table A-5	Emissions Factors with State Reductions	A-7

FIGURES

Figure 1	Map of Ontario	11
Figure 2	The Greenhouse Effect	23
Figure 3	Annual Per Capita GHG Emissions by Sector, 2008 to 2019	31
Figure 4	City of Ontario Community-Wide Per Capita GHG Emissions, 2019–2050	44
Figure 5	Ontario Per Capita GHG Emissions with Reduction Strategies, 2019 to 2050 ..	74

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LIST OF ABBREVIATIONS

AB	Assembly Bill
ACS	American Community Survey
ADC	alternative daily cover
AQMP	air quality management plan
BRT	bus rapid transit
CARB	California Air Resources Board
CCAP	community climate action plan
CEQA	California Environmental Quality Act
CH ₄	methane
CO ₂	carbon dioxide
CO _{2e}	carbon dioxide equivalent
CALGreen	California Green Building Standards Code
DOE	Department of Energy (U.S.)
EIR	Environmental Impact Report
EO	Executive Order
EPA	Environmental Protection Agency (U.S.)
EV	electric vehicle
FMMP	Farmland Mapping and Monitoring Program
GHG	greenhouse gas
Global Protocol	Global Protocol for Community-Scale Greenhouse Gas Inventories
GWP	global warming potential
HCD	Housing and Community Development Department (California)
HFC	hydrofluorocarbon
IEUA	Inland Empire Utilities Agency
IPCC	Intergovernmental Panel on Climate Change
IT	information technology
kWh	kilowatt-hour
LED	light-emitting diode
LEED	Leadership in Energy and Environmental Design
LCFS	low-carbon fuel standard
MMT	million metric tons
MT	metric ton
MTCO _{2e}	metric tons of carbon dioxide equivalent
MSW	Municipal solid waste
N ₂ O	nitrous oxide
NF ₃	nitrogen trifluoride

LIST OF ABBREVIATIONS (CONTINUED)

OMUC	Ontario Municipal Utilities Company
PFC	perfluorocarbon
PV	photovoltaic
RPS	Renewable Portfolio Standard
RTTAC	Regional Transit Technical Advisory Committee
SB	Senate Bill
SBCOG	San Bernardino Council of Governments
SBCTA	San Bernardino County Transportation Authority
SCAQMD	South Coast Air Quality Management District
SCAG	Southern California Association of Governments
SF ₆	sulfur hexafluoride
SoCalREN	Southern California Regional Energy Network
TOP	The Ontario Plan
TOD	transit-oriented development
U.S. Community Protocol	United States Community Protocol for Accounting and Reporting of Greenhouse Gas Emissions
VMT	vehicle miles traveled
ZEV	zero-emission vehicle

EXECUTIVE SUMMARY

The 2022 Community Climate Action Plan (CCAP) is Ontario's strategic plan to reduce greenhouse gas (GHG) emissions and foster a sustainable community through 2050 and beyond. The Ontario Plan (TOP), the City's comprehensive plan or general plan, directs the preparation, ongoing implementation, and update of the CCAP, providing the framework for Ontario to reduce its community-wide GHG emissions in a manner consistent with the adopted state reduction target for 2030 and the longer-term goals for 2045 and 2050. The CCAP allows the City of Ontario decision-makers, staff, and community to understand the sources and magnitude of local GHG emissions, reduce GHG emissions, and prioritize steps to achieve GHG emission-reduction targets.

The CCAP is consistent with the California Environmental Quality Act (CEQA) Guidelines for Plans for the Reduction of Greenhouse Gas Emissions (California Code of Regulations Section 15183.5). This allows the 2022 CCAP to support and streamline environmental review of GHG emissions for future development projects in the city. The 2022 CCAP serves as the programmatic tiering document for review of the climate change impacts of projects under CEQA and is included in the Supplemental Environmental Impact Report for TOP 2050.

The 2022 CCAP is an update of the 2014 Community Climate Action Plan (2014 CCAP), providing updated analyses and background information, an expanded set of GHG reduction strategies, and a planning horizon out to 2050. The CCAP contains an inventory of the community's GHG emissions from the transportation, energy, waste, off-road equipment, agriculture, water, and wastewater sectors; land use sequestration; stationary sources; and goals, strategies, and actions to address climate change adaptation and GHG emissions. The CCAP also presents a work plan for the City to implement strategies and track progress over time.

The City of Ontario prepared the 2022 CCAP to update the community's strategic path to reducing GHG emissions beyond 2020, consistent with State requirements and TOP 2050. Specifically, the CCAP does the following:

- Provides an introduction and context for the CCAP in Chapters 1 and 2, including community history and demographics, an overview of the City's existing actions to address climate change, a discussion of climate change and equity, and the relationship between the CCAP and state and regional planning efforts in.
- Identifies and updates sources of GHG emissions within the City of Ontario's municipal boundaries for the calendar year of 2019 and estimates how these emissions may change over time (Chapter 3).

- Presents the GHG emission reduction targets established by state regulations and guidance and the City’s GHG reduction targets (Chapter 4). Chapter 4 shows where the City currently stands in its efforts to reduce GHG emissions and the GHG reductions required by the 2022 CCAP to meet its targets.
- Provides strategies (see Chapter 5) in various sectors to meet or exceed the state targets of reducing emissions 40 percent below 1990 levels by 2030 and 80 percent below 1990 levels by 2050, consistent with the direction of the State of California via Assembly Bill (AB) 32, Governor’s Executive Order S-03-05, and California Public Resources Code Section 21083.3. Chapter 5, GHG Emission Reduction Strategy, has two sections:
 - The Existing GHG Reduction Strategies section outlines the existing and planned local and state initiatives that are expected to reduce Ontario’s future GHG emissions and move the community closer to achieving its targets.
 - The New GHG Reduction Strategies section outlines the new 2022 strategies that show a viable path for Ontario to reach its established GHG emission reduction targets. This includes goals, strategies, and actions; their impacts on GHG emissions; and the co-benefits they provide to enhance ecological, economic, and social well-being. These strategies include actions that will be led by City staff and actions or project features for inclusion in certain development projects as identified by the strategy.
- Provides substantial evidence that the emission reductions estimated in the CCAP are feasible in Chapter 5, with supporting technical detail in Appendix A.
- Provides an implementation program (see Chapter 6) and discusses the various outcomes of reduction efforts and how these reduction efforts can be implemented.

The CCAP is summarized in Section 5.8, Greenhouse Gas Emissions, of the Supplemental Environmental Impact Report for The Ontario Plan 2050. The measures in this 2022 CCAP are not substantially different than those of the 2014 CCAP, and therefore there is no change in the environmental impacts associated with the CCAP.

The CCAP allows community members, City staff and officials, and other stakeholders to understand Ontario’s existing efforts and strategies to achieve its GHG reduction goals. It builds on the 2014 CCAP and is consistent with the regional San Bernadino County GHG Reduction Plan.

Greenhouse Gas Inventory and Forecast

A GHG inventory is a summary of the GHG emissions generated by activities that take place within a community. The GHG emissions inventories and the GHG forecast lay the groundwork for the CCAP, which seeks to align the City's GHG reduction efforts with state-recommended targets. The community-wide GHG inventories, which include both community-wide sectors and municipal operations, assess GHG emissions for the years 2008, 2016, and 2019. The 2022 CCAP update is informed by the latest science in GHG accounting, best practices, and emissions factors.

The community-wide GHG inventory assessed GHG emissions from nine categories of activities, known as sectors.

- **Transportation.** GHG emissions created by driving on-road vehicles in Ontario, including passenger and freight vehicles.
- **Residential energy.** GHG emissions attributed to the use of electricity and natural gas in residential buildings.
- **Nonresidential energy.** GHG emissions attributed to the use of electricity and natural gas in nonresidential buildings.
- **Solid waste.** GHG emissions released from trash collected from residents and businesses.
- **Off-road equipment.** GHG emissions from equipment that does not provide on-road transportation (excluding agricultural equipment), such as tractors for construction or equipment used for landscape maintenance.
- **Agriculture.** GHG emissions from various agricultural activities, including agricultural equipment, crop cultivation and harvesting, and livestock operations.
- **Water and wastewater.** This sector accounts for the electricity used to transport every gallon of water to or wastewater from city residents and businesses, as well as direct emissions from processing wastewater.
- **Land use and sequestration.** Emissions released into the atmosphere from development of previously undeveloped land and GHG emissions absorbed and stored in trees and soils on locally controlled lands as part of healthy ecosystems.
- **Stationary sources.** Emissions from fuel use at major industrial facilities permitted by state and regional air quality authorities. Stationary source emissions are informational and are not included in the community total.

Throughout the CCAP, emissions are provided in terms of per capita GHG emissions. Per capita emissions show the average number of emissions per service population, which is the sum of community residents and employees in Ontario. Per capita GHG emissions show changes in emissions over time and are helpful for showing consistency with state-recommended GHG reduction targets. The City’s per-capita emissions targets are 6.0 metric tons of carbon dioxide equivalent (MTCO_{2e}) per capita by 2030 and 2.0 MTCO_{2e} per capita below 1990 levels by 2050.

As shown in **Table ES-1**, per capita GHG emissions have declined steadily from 2008 levels. Per capita emissions in Ontario decreased from approximately 8.25 MTCO_{2e} in 2008 to approximately 5.55 MTCO_{2e} per person in 2019, a decrease of 33 percent. Transportation was highest-emitting sector, responsible for approximately 55 percent of the city’s total GHG emissions in 2019, followed by the nonresidential and residential energy sectors as the second- and third-largest sources of GHG emissions. The four sectors that experienced the largest decreases in annual GHG emissions between 2008 and 2019 are land use and sequestration, agriculture, water and wastewater, and nonresidential energy, which reduced emissions by 77 percent, 75 percent, 56 percent, and 54 percent, respectively.

Table ES-1 Ontario Community-Wide Per Capita GHG Emissions by Sector, 2008 to 2019 (MTCO_{2e})

Sector	2008	2016	2019	Percentage Change, 2008–2019
Transportation	3.51	3.00	3.04	-13%
Nonresidential energy	2.77	1.80	1.29	-54%
Residential energy	0.72	0.55	0.50	-30%
Solid waste	0.28	0.22	0.27	-4%
Off-road equipment	0.17	0.08	0.21	23%
Agriculture	0.63	0.21	0.16	-75%
Water and wastewater	0.15	0.08	0.06	-56%
Land use and sequestration	0.01	0.00	0.00	-77%
Total annual per capita MTCO_{2e}	8.25	5.95	5.54	-33%
Informational Items				
Stationary sources	1.50	0.25	1.05	-30%

The community-wide forecast is a projection of future GHG emissions based on the results of the 2019 inventory and estimates of future community population and job growth. The CCAP includes a GHG forecast for the calendar years 2030 and 2050, relying on population, housing, and employment projections based on the estimates of future buildout in TOP 2050. As shown in Table ES-2, the city’s forecast per-capita emissions are expected to decrease by approximately 45 percent between 2008 and 2050.

Table ES-2 Ontario Community-wide GHG Emissions Forecast, 2008 to 2050 (MTCO_{2e})

Sector	2008 ¹	2016 ¹	2019 ¹	2030	2050	Percentage Change, 2008-2050
Transportation	3.51	3.00	3.04	2.84	2.15	-39%
Nonresidential energy	2.77	1.80	1.29	1.30	1.29	-54%
Residential energy	0.72	0.55	0.50	0.53	0.57	-21%
Solid waste	0.28	0.22	0.27	0.27	0.27	-4%
Off-road equipment	0.17	0.08	0.21	0.21	0.21	23%
Agriculture ²	0.63	0.21	0.16	0.08	0.00	-100%
Water and wastewater	0.15	0.08	0.06	0.06	0.06	-56%
Land use and sequestration	0.01	Less than 0.01	Less than 0.01	Less than 0.01	Less than 0.01	-81%
Total	8.25	5.95	5.54	5.30	4.56	-45%

1. Data shown for 2008, 2016, 2019 are the actual per capita GHG emissions. The data shown for 2030, 2040, and 2050 are GHG per capita emission forecasts, calculated based on projections from the 2019 inventory.
2. GHG emission projects for the agriculture are based on the assumption that, by 2050, all agricultural land will be developed and converted to other land uses, so agricultural emissions will decrease to zero by 2050.

Greenhouse Gas Reduction Strategies

To evaluate the City’s current progress toward meeting its emissions reductions targets, this CCAP acknowledges existing climate action policies and programs, planned future actions, and actions soon-to-be implemented at the state and local level.

The CCAP quantifies GHG emissions reductions associated with the implementation of these actions. As shown in Table ES-3, with the implementation of existing and planned state and local actions, the community’s GHG emissions are projected to be 3.54 MTCO_{2e} per capita and 1.31 MTCO_{2e} per capita in 2030 and 2050, respectively.

Table ES-3 Ontario Community-Wide Emission Reductions from Existing State, Regional, and Local Actions, 2030–2050

Changes in Per Capita Emissions	2030 MTCO ₂ e	2050 MTCO ₂ e
Forecast per-capita emissions without state actions	5.30	4.56
Emission reductions from existing and planned state actions	-1.09	-2.15
Emission reductions from planned local actions without CCAP	-0.15	Less than - 0.01
Community-wide emissions with existing and planned state and local actions	4.06	2.40

Though implementation of existing state and local actions will help the City meet its GHG reduction targets, these actions are insufficient on their own. Thus, the CCAP identifies future strategies that, if implemented at the levels specified in the technical appendix, will allow the community to achieve its emissions-reductions targets. Refer to [Table ES-4](#) for a list of the CCAP strategies, explained in more detail in Chapter 5.

Table ES-4 List of 2022 CCAP GHG Reduction Strategies

Strategy Number	Strategy Name	Strategy Language	Per Capita Emission Reductions	
			2030 MTCO ₂ e	2050 MTCO ₂ e
1	Building electrification	Promote and incentivize the phase-out of gas appliances in new and existing homes and businesses throughout the community to advance GHG reductions, increase energy efficiency, and protect public safety and environmental health.	0.0257	0.1453
2	Onsite solar energy for existing residential development.	Continue to support and facilitate installation of rooftop solar photovoltaic and onsite solar energy systems in existing residential development.	0.0051	0.0000

Strategy Number	Strategy Name	Strategy Language	Per Capita Emission Reductions	
			2030 MTCO _{2e}	2050 MTCO _{2e}
3	Onsite solar energy systems for nonresidential development.	<p>Ensure new large nonresidential development, including City facilities, includes onsite renewable energy to support the site's energy needs by requiring solar photovoltaic panels or other appropriate onsite renewable energy generation systems for the following types of projects:</p> <ul style="list-style-type: none"> • New commercial and office buildings, or existing commercial and office building expansions greater or equal to 45,000 square feet in size. • New industrial or existing industrial buildings expansions greater or equal to 100,000 square feet in size. 	0.0659	0.0000
4	Green roofs	Promote and incentivize residents and business owners to install green roofs to conserve energy and reduce surface water runoff.	0.0000	0.0000
5	Urban cooling	Maintain and expand the city's existing tree canopy, with a goal of planting 500 trees annually through 2050 and promote the use of pervious concrete and cool pavement for pavement projects.	0.0140	0.0180
6	Energy efficiency retrofits for low-income households	Promote and incentivize voluntary energy efficiency retrofits of homes to reduce natural gas and electricity usage, with the goal of retrofitting 9,000 low-income homes by 2050. Partner with community services agencies to fund energy efficiency projects, including heating, ventilation, air conditioning, indoor lighting, water heating equipment, insulation, and weatherization for low-income residents.	0.0023	0.0020

Strategy Number	Strategy Name	Strategy Language	Per Capita Emission Reductions	
			2030 MTCO _{2e}	2050 MTCO _{2e}
7	Energy efficiency retrofits	Promote and incentivize voluntary energy efficiency retrofits to reduce in natural gas and electricity usage. Partner with regional agencies to expand access to existing energy efficiency and conservation opportunities, incentives, and technical assistance for residents and businesses.	0.0288	0.0221
8	Smart Growth and Infill	Encourage revitalization of neighborhoods through higher-density, mixed-use, infill development and creative reuse of under-utilized sites within the urban core.	0.0000	0.0000
9	Transit-Oriented Development	Encourage development of compact, mixed-use, and transit-oriented development to improve the regional jobs-housing balance, especially on corridors served by high-ridership transit and bus rapid transit (BRT), such as Holt Avenue.	0.0008	0.0020
10	Increase Transportation Ridership	Ensure a reliable and responsive transit system with dedicated and secure funding and resources to support increased ridership.	0.0697	0.0445
11	Traffic signal synchronization and roadway management	Implement traffic and roadway management strategies to improve mobility and efficiency and reduce associated emissions.	0.0000	0.0000
12	Community vehicle electrification	Promote and incentivize the adoption of electric vehicles (EV) citywide, including light-duty and heavy-duty vehicles, for municipal, commercial, and residential uses.	0.1565	0.6584
13	Active transportation networks	Work with transit agencies, school districts, and employers to facilitate an interconnected transportation system that allows a shift in travel from private passenger vehicles to alternative modes, including public transit, ride sharing, car sharing, bicycling, and walking.	0.0045	0.0054

Strategy Number	Strategy Name	Strategy Language	Per Capita Emission Reductions	
			2030 MTCO _{2e}	2050 MTCO _{2e}
14	Vehicle idling	Limit idling of heavy-duty trucks. Support the South Coast Air Quality Management District (SCAMQD) and ARB anti-idling requirements and provide signage in key areas where idling that is not consistent with SCAMQD or ARB requirements might occur.	0.0049	0.0025
15	Parking policy and event parking	Adopt a comprehensive parking policy that encourages carpooling and the use of alternative transportation, including providing parking spaces for car-share vehicles at convenient locations with access to public transportation.	0.0056	0.0115
16	Electrification of construction and landscaping equipment	Promote and incentivize the transition to electric construction and landscaping equipment.	0.0120	0.0368
17	Idling ordinance for construction equipment	Limit idling of heavy-duty off-road construction equipment to reduce air pollution and GHG emissions from construction activity.	0.0001	0.0001
18	Methane capture at landfills	Support efforts to reduce methane emissions from regional landfills.	0.0558	0.0812
19	Waste diversion	Exceed waste diversion goals recommended by AB 939 and CALGreen by adopting a citywide diversion target of at least 75 percent of waste.	0.0361	0.0547
20	Construction and Demolition Waste Recovery Ordinance	Increase the amount of waste recycled during construction and demolition of buildings.	0.0000	0.0000
21	Indoor water efficiency	Encourage water-efficient retrofits of new and existing buildings by working with water providers and regional agencies.	0.0114	0.0036

Strategy Number	Strategy Name	Strategy Language	Per Capita Emission Reductions	
			2030 MTCO _{2e}	2050 MTCO _{2e}
22	Water efficient landscapes and water recycling	Promote drought-tolerant and fire-wise landscaping. Encourage increased use of reclaimed water for landscape irrigation, agricultural, and industrial use.	0.0085	0.0000
23	Water system and wastewater operations efficiency	Maximize efficiency at drinking water treatment, pumping, and distribution facilities, including development of off-peak demand schedules for heavy commercial and industrial users.	0.0004	0.0000
24	Methane capture for wastewater treatment	Work with Inland Empire Utilities Agency (IEUA), the local wastewater treatment provider, to increase methane capture rate.	0.0024	0.0032
25	Methane capture for dairy operations	Encourage and incentivize local dairy operations to reduce methane emissions through methane capture technology.	0.0079	0.0000
26	Climate change awareness and education	Promote climate change awareness and GHG reduction community-wide through a variety of mechanisms, including through support of climate change education in schools or community colleges.	0.0000	0.0000
27	Carbon sequestration	Establish a citywide carbon sequestration project and sequestration goal of 5,000 MT CO ₂ per year.	0.0000	0.0000
28	Green jobs	Support green job trainings and opportunities to create sustainable, living wage, quality employment opportunities.	0.0000	0.0000
Total GHG emission reductions from all CCAP strategies			0.52	1.09
Community-wide emissions with CCAP			3.54	1.31

In conjunction with existing and planned local and state programs, these updated strategies provide a flexible path to reduce the community's GHG emissions to meet the City's 2030 and 2050 emission targets and to support the State's adopted target for 2030 and its goal to achieve carbon neutrality by 2045. The City's GHG reduction targets are to reduce emissions to a level below 6.0 MTCO_{2e} per capita by 2030 and 2.0 MTCO_{2e} per capita by 2050 as shown in [Table ES-5](#). Refer to

Chapter 5 for additional detail on GHG emissions reductions from state actions, existing City actions, and planned City actions.

Table ES-5 Progress to Absolute and Per Capita GHG Emissions Targets

Target	2030 Description	2050 Description
Baseline Emissions	8.25 MTCO ₂ e per person	8.25 MTCO ₂ e per person
Target	6.0 MTCO ₂ e per person	2.0 MTCO ₂ e per person
Emissions with All Reductions	3.52 MTCO ₂ e per person	1.31 MTCO ₂ e per person

Implementing the CCAP will require City leadership to put the CCAP strategies into effect and report progress. To ensure that the implementation process is efficient and transparent, this CCAP includes a work plan that identifies responsible departments, partners, time frames, and relative costs associated with each strategy. Implementation strategies are shown in [Table ES-6](#).

Table ES-6 List of Implementation Strategies

CCAP Implementation Strategies
Implementation Strategy 1. Annually monitor and report progress toward CCAP target achievement.
Implementation Strategy 2. Continue collaborative partnerships with agencies and community groups that support implementation of the CCAP.
Implementation Strategy 3. Secure necessary funding to implement the CCAP.
Implementation Strategy 4. Continue to update the baseline emissions inventory and CCAP every five years.
Implementation Strategy 5. Maintain and update the CCAP to promote greater resilience.

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1. INTRODUCTION

Purpose of the Community Climate Action Plan

The Ontario Community Climate Action Plan (CCAP) serves as the strategic plan for how the City of Ontario (City) will reduce greenhouse gas (GHG) emissions and foster a sustainable community through 2050 and beyond. The Ontario Plan (TOP), the City's comprehensive plan or general plan, directs the preparation, ongoing implementation, and update of the CCAP, providing the framework for Ontario to reduce its community-wide GHG emissions in a manner consistent with the adopted state reduction target for 2030 and the longer-term goals for 2045 and 2050. This document describes the City's successes to date in reducing GHG emissions and provides a framework for continued progress. The 2022 CCAP is an update to the 2014 CCAP.

This chapter includes an updated description of the regulatory framework for the CCAP, a community profile, guiding principles for the City's climate action planning, and a summary of this update process. Subsequent chapters analyze Ontario's progress to date in meeting its adopted GHG reduction targets and contain new information to achieve more significant and longer-term GHG reductions.

The CCAP is consistent with the California Environmental Quality Act (CEQA) Guidelines for Plans for the Reduction of Greenhouse Gas Emissions (California Code of Regulations Section 15183.5). This allows the 2022 CCAP to support and streamline environmental review of GHG emissions for future development projects in the city.

2014 Community Climate Action Plan

On December 16, 2014, the City Council adopted the City's first Community Climate Action Plan (2014 CCAP), which assembled the City's existing climate action efforts into one centralized plan informed by technical analyses. The 2014 CCAP described specific strategies, actions, and cost-effective opportunities for existing and future residents, businesses, and development projects. The 2014 CCAP also served as a qualified GHG reduction strategy under CEQA, streamlining development review.

A key task of the 2014 CCAP was to establish a baseline of community GHG emissions to measure future changes and to inform actions the community could take to reduce emissions by 2020. In April 2012, Ontario developed the 2008 Community Greenhouse Gas Emissions Inventory and 2020 Forecast as part of a regional partnership with San Bernardino County, led by the San Bernardino County Transportation Authority (SBCTA). (Also referred to as the San Bernardino

Council of Governments or SBCOG.) The 2008 community-wide inventory report, included as Appendix A in the 2014 CCAP, revealed the major sources of emissions from community activities and gave a basis of comparison for the 2014 CCAP and future assessment reports. The 2014 CCAP also included a GHG emissions forecast that projected future emissions from community activities in 2020.

The 2014 CCAP included 42 measures to reduce GHG emissions across various sector such as new development; building energy; renewable energy; wastewater treatment; solid waste management; on-road transportation; off-road equipment; and water transport, distribution, and treatment. The largest potential reductions in GHG emissions came from strategies in the renewable energy, transportation, and off-road equipment sectors. The City has made continued progress in implementing the measures in the 2014 CCAP.

2022 Community Climate Action Plan

The City of Ontario prepared the 2022 CCAP to update the community's strategic path to reducing GHG emissions beyond 2020, consistent with State requirements and TOP 2050. Specifically, this CCAP does the following:

- Identifies and updates sources of GHG emissions within the City of Ontario's municipal boundaries for the calendar year of 2019 and estimates how these emissions may change over time, as presented in Chapter 3.
- Identifies GHG reduction targets in Chapter 4 that are consistent with State targets.
- Provides strategies (see Chapter 5) in various sectors to meet or exceed the state target and goals of reducing emissions 40 percent below 1990 levels by 2030 and 80 percent below 1990 levels by 2050 and in support of the State's pathway to achieve carbon neutrality by 2045, consistent with the direction of the State of California via Assembly Bill (SB) 32, Governor's Executive Order S-03-05, California Public Resources Code Section 21083.3, and Governor's Executive Order B-55-18.
- Provides substantial evidence that the emission reductions estimated in the CCAP are feasible in Chapter 5, with supporting technical detail in Appendix A.
- Provides an implementation program (see Chapter 6) and discusses the various outcomes of reduction efforts and how these reduction efforts can be implemented.

The CCAP is summarized in Section 5.8, Greenhouse Gas Emissions, of the Supplemental Environmental Impact Report for The Ontario Plan 2050. The measures included in this 2022 CCAP are not substantially different than those of the 2014 CCAP, and therefore there is no change in the environmental impacts associated with the CCAP.

How to Use This Plan

The following chapters of the 2022 CCAP provide details of the community's GHG emissions and strategies to reduce emissions.

- Chapters 1 and 2 provide an introduction and context for the CCAP, including community history and demographics, an overview of the City's existing actions to address climate change, a discussion of climate change and equity, and the relationship between the CCAP and state and regional planning efforts.
- Chapter 3, Ontario GHG Emissions, provides an introduction to climate science and describes the results of Ontario's recent-year GHG inventories and forecast GHG emissions for 2030 and 2050. This allows the community to see how its emissions have changed over time and charts progress toward state and local emissions reduction targets, informing emissions reduction strategies.
- Chapter 4, GHG Reduction Targets, outlines the GHG emission reduction targets established by state regulations and guidance and the City's GHG reduction targets. This chapter shows where the City currently stands in its efforts to reduce GHG emissions and the GHG reductions required by the 2022 CCAP to meet its targets.
- Chapter 5, GHG Emission Reduction Strategy has two sections:
 - The Existing GHG Reduction Strategies section outlines the existing and planned local and state initiatives that are expected to reduce Ontario's future GHG emissions and move the community closer to achieving its targets.
 - The New GHG Reduction Strategies section outlines the new 2022 strategies that show a viable path for Ontario to reach its established GHG emission reduction targets. This includes goals, strategies, and actions; their impacts on GHG emissions; and the co-benefits they provide to enhance ecological, economic, and social well-being. These strategies include actions that will be led by City staff and actions or project features for inclusion in certain development projects as identified by the strategy.
- Chapter 6, CCAP Implementation Strategy, describes the implementation details for the strategies in the CCAP and a potential approach to putting these strategies into effect.

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2. BACKGROUND

Community Profile and History

This section provides an overview of Ontario, highlighting key community characteristics, natural resource use, development patterns, and notable history.

Community History

Ontario is in the west San Bernardino Valley, bordered by the cities of Chino and Montclair to the west, Eastvale and Jurupa Valley to the south, Fontana to the east, and Upland and Rancho Cucamonga to the north. Ontario is well connected to the neighboring counties of Los Angeles, Orange, and Riverside by four major highways – Interstate 10 and State Route 60 traveling east-west, and State Route 83 (Euclid Avenue) and Interstate 15 traveling north-south – and by Amtrak and Metrolink stations.

Archaeological evidence dating back to 10,000 BC shows that the San Bernardino Valley has been inhabited for over 12,000 years. The Gabrieleno native peoples flourished in the west San Bernardino Valley for 3,000 years. Shortly after Spanish explorers entered the area in the 1770s, pioneers established ranching operations in the valley. Spanish rancher Don Antonio Lugo and his family managed the Rancho San Bernardino, which encompassed the entire valley, between 1842 and 1851. Subsequently, the Mormon Colony acquired the ranch. The discovery of gold in the San Bernardino Mountains initiated a period of gold and silver mining in the 1860s to 1880s.¹

Ontario was established in the 1880s based on the founders' vision of a planned community and guiding principles that including a mutual water company, prohibition of liquor, a grand thoroughfare through the city (Euclid Avenue), and an agricultural college for general education. Ontario was incorporated in 1891, and in 1903, Ontario was declared the "Model Colony" by an Act of Congress for its establishment of a new standard for urban living.²

The City first developed as an agricultural community, primarily producing citrus and dairy. The town expanded around Euclid Avenue, creating what are now two of Ontario's historic districts, the Historic Downtown and the College Park Historic District. Ontario's population grew in the 1950s as the city shifted from an agricultural-based economy to an industry-based economy and manufacturing jobs became increasingly available. Mass production housing was constructed in a grid development pattern, radiating from the city's historic core, and establishing most of the suburban residential neighborhoods that dominate northwestern Ontario today.

Ontario's rapid job growth continued through the 1980s and 1990s, especially in industrial expansion of automotive plants, air cargo, commerce centers, and housing to match the growing job opportunities. During the 1980s, Ontario was ranked the seventh-fastest-growing city in California. Housing development during this period focused on master-planned communities south of Riverside Drive in the southern and eastern areas of Ontario. Today, these communities are areas of medium and high resources.³

As the Ontario population and economy continues to grow, urban development is transitioning the remaining agricultural land within the city limits. By 2050, all the city's agricultural land is expected to be converted to urban uses. Western Ontario Ranch (west of Archibald Avenue to Euclid Avenue), which is largely dairy and other farms, is expected to transition into a mixed-use area of residential homes, commercial centers, and industrial and business parks.

Population, Income, and Employment

Ontario is the third most populous city in San Bernardino County after San Bernardino and Fontana, with a population of 178,606 residents in 2019, which is the baseline year of the 2022 CCAP. Approximately 68 percent of the city's adult residents were below age 44 as of 2019, and nearly 37 percent of all residents were below age 24.⁴

Ontario, much like California as a whole, is experiencing racial and ethnic diversification. The percentage of non-White residents has increased to 84 percent in 2019 from 79 percent in 2010. The population of Hispanics increased by 14.5 percent between 2010 and 2019, and it is the largest ethnic group in Ontario.⁵ Based on the 2019 American Community Survey, Ontario's population is 70 percent Hispanic, 16 percent non-Hispanic White, 7 percent Asian, 5 percent African American, and 2 percent multiracial. Approximately 58 percent of Ontario residents primarily speak a language other than English at home, compared with 42 percent in San Bernardino County.⁶ In Ontario, the most spoken language at home is Spanish, followed by English, Tagalog, and Chinese.⁷

According to the American Community Survey (ACS), median household income has increased by 30 percent since 2010, from \$57,771 to \$75,266 in 2019.⁸ The discrepancy in median income between Ontario and San Bernardino County has widened in the past decade. In 2010, the median income in Ontario was only 3.3 percent higher than in the county, but in 2019 the gap in median income between the city and county grew to 10 percent – \$75,266 in Ontario compared with \$67,903 in San Bernardino County.

The cost of housing can be particularly vexing for lower-income families, with 56 percent of families paying more than 30 percent of their income on housing. According to data from ACS, citywide median rent increased by approximately 122 percent between 2000 and 2019, outpacing the countywide median rent increase of 110 percent. On a neighborhood level, the eastern and southern areas of Ontario experienced the highest increases in rent in the city (157 and 139 percent median rent increase, respectively).⁹

EV CHARGING STATIONS

The City of Ontario currently owns 21 electric vehicle (EV) charging stations. The City partnered with Southern California Edison through the Charge Ready program to install EV charging stations at the Citizens Business Bank Arena, Ontario Convention Center, City Hall, Library, and the community center. The City continues to invest in expansion of citywide electric vehicle charging infrastructure to promote electric vehicle use. Through the Smart Ontario initiative, 4 EV charging stations are planned at City facilities. In downtown, the City plans to install 17 additional electric vehicle charging stations through a partnership with Tesla funded by the South Coast Air Quality Management District.

Ontario's economy today is still largely dominated by industrial sectors. The largest economic sector in Ontario is the logistics industry, facilitated by the city's proximity to major Southern California transportation networks and the Ontario International Airport. In 2019, the three sectors with the highest shares of employment were transportation and warehousing, waste management and remediation, and retail trade, which made up 17 percent, 13 percent, and 11 percent of jobs in Ontario, respectively.

Sustainability in Ontario

Since adopting the 2014 CCAP, Ontario has made progress in improving energy efficiency and reducing GHG emissions in both municipal operations and community-wide sectors.

To improve energy efficiency of municipal buildings and operations, the City launched the Smart Ontario initiative, which involves an energy audit of City properties, comprehensive upgrade of municipal utility infrastructure, and heating, ventilation, and air conditioning (HVAC) equipment improvements. As of March 2022, the City has retrofitted all city streetlights with light-emitting diode (LED) light fixtures as well as all interior and exterior light fixtures in City buildings (approximately 15,000); has replaced over 100 HVAC components in City facilities; and replaced 350 thermostats in all City buildings. The City has installed 1.8 megawatts of solar photovoltaic systems at the Ontario Convention Center and the Ontario Police Department, generating 2,571,125 kilowatt-hours

of energy in 2019. The Smart Ontario initiative will save an estimated \$75 million over the lifetime of the new equipment. To reduce the GHG emissions of newly constructed City buildings, the City

committed to all new municipal buildings achieving Leadership in Energy and Environmental Design (LEED) certified by the U.S. Green Building Council.

The City has also implemented numerous projects to reduce GHG emissions from the transportation sector, including installation of 21 EV charging stations, adoption of an Active Transportation Master Plan, synchronization of 30 percent of traffic signals in Ontario, and completion of pedestrian and bicycle infrastructure improvements through Safe Routes to School and Active Transportation Program grants. Future projects include a citywide e-scooter share program (projected to launch in March 2023), the Multimodal Transportation Center (needs assessment completed in March 2022), and the West Valley Corridor Bus Rapid Transit, a zero-emission bus line (completion expected in 2024).

Since adopting the 2014 CCAP, the City has implemented its GHG reduction measures in new development projects through the Development Review Process. This process involves documentation of GHG reduction measures incorporated into the design and construction of development projects using “screening tables” that assign point values for each measure that correspond to the minimum emission reduction expected from that measure. The use of screening tables to document implementation of CCAP strategies ensures that they will be implemented as part of the construction of new developments, helps to reduce GHG emissions in construction of new development, supports improved monitoring and evaluation of GHG reduction strategies, and ensures compliance with CEQA provisions for evaluating and mitigating climate change impacts. Implementation of the 2022 CCAP will update and continue use of screening tables for eligible projects.

NONRESIDENTIAL ELECTRICITY SAVINGS

Between 2008 and 2019, the city substantially reduced its GHG emissions from the nonresidential electricity sector, including commercial and industrial electricity use.

Absolute GHG emissions from this sector decreased by 31 percent, from 375,540 MTCO₂e in 2005 to 257,820 MTCO₂e in 2019.

Energy use or activity in this sector only decreased slightly during this period; the substantial decline in emissions is attributed to procurement of more renewable energy by Ontario’s energy providers, in accordance with State law, and to greater energy efficiency in commercial and industrial facilities and equipment.

Climate Change and Equity

Historically and currently, institutions and structural systems drive and perpetuate inequitable distribution of resources, access to opportunities, and poor life outcomes that many frontline communities face. Examples include redlining, exclusionary housing policies, forced removal of Indigenous communities, and Jim Crow segregation. Not only did these policies exacerbate the wealth and income gap between white families and families of color, they also resulted in a greater number of lower-income and communities of color living in areas at greater exposure to environmental pollution and adverse health outcomes.

According to a citywide analysis of Housing and Community Development (HCD) Opportunity Areas, Ontario's mixture of high-resource, moderate-resource, and low-resource areas have varying urban infrastructure and levels of exposure to environmental pollutants. The State's Opportunity Mapping is a data-driven approach to measure and map the distribution of resources and opportunity in California communities. It was developed by the California Fair Housing Task Force, a group of independent organizations and research centers convened by the Department of Housing and Community Development and the California Tax Credit Allocation Committee. The approach measures opportunity through an assessment of positive or negative outcomes related to economic, educational, and health indicators across communities in comparable areas (nine identified regions in California with common economic, political, and geography characteristics), and in specific areas classified as high-poverty, racially segregated areas in which over 30 percent of the population is below the federal poverty line. The aggregate assessment of the indicators and outcomes are converted into composite scores that are used to categorize areas as "high resource", "medium resource", and "low-resource", and they are mapped to show geographic trends. The composite scores and opportunity area maps are used primarily to inform the allocation of low-income housing tax credits for eligible projects. The maps are also used to inform analyses of fair housing issues in long-term policy and planning efforts such as housing elements.

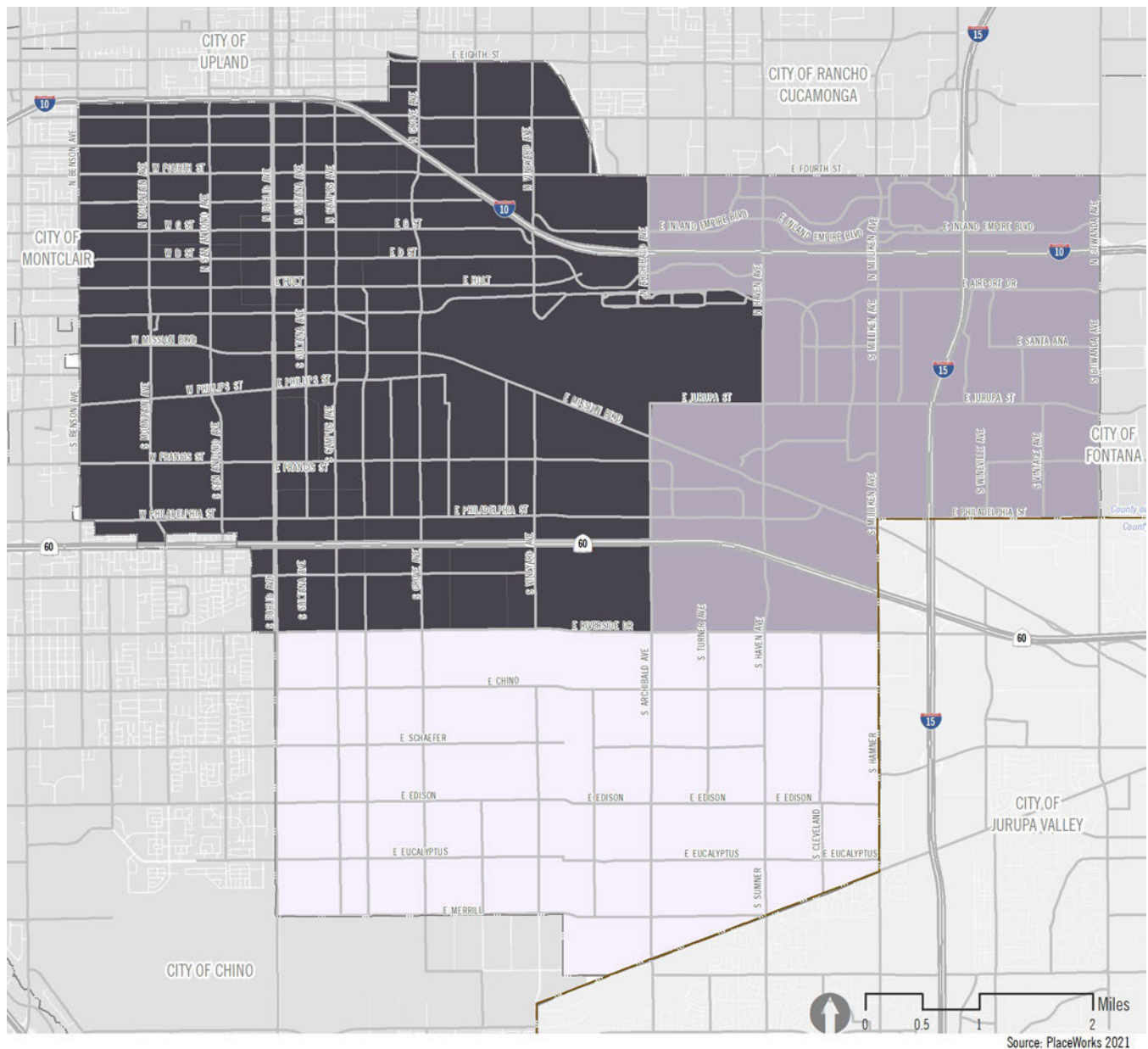
Ontario neighborhoods can be grouped into three major geographic areas based on the HCD analysis of resource distribution across the city. These areas, listed below and shown in **Figure 1**, share common socioeconomic characteristics that can be traced back to historical urban development patterns.

- **Northwestern Ontario.** West of Archibald Avenue, inclusive of Ontario International Airport to the city's western limits and north of Riverside Drive to the city's northern limits.
- **Eastern area/Eastern Ontario.** East of the Ontario International Airport and Archibald Avenue to the city limits and north of Riverside Drive.
- **South Ontario.** South of Riverside Drive, bounded by the city's southern limits.

Currently, southeastern Ontario has higher rates of owner-occupied housing and generally higher median incomes than northwestern Ontario. The southeast areas, considered Ontario's high-resource areas, are characterized by good access to employment, higher median incomes, and higher rates of home-ownership than households in northwestern Ontario. Southeastern Ontario includes much of the city's recent and future planned development, including single-family housing south of Highway 60 in eastern Ontario and business parks and industrial commerce centers in east Ontario north of Highway 60. By contrast, households in the northwestern area are primarily renter occupied, and more than half of households in this area are considered low resource, with relatively low access to education and employment opportunities.

CalEnviroScreen, a mapping tool developed by the California Office of Environmental Health Hazard Assessment, has identified similar issues. This tool uses environmental, health, and socioeconomic indicators to map and compare community environmental scores on a scale of 0 to 100. A community with a high score has higher levels of pollution and other negative environmental or social indicators. All neighborhoods in Ontario score 50 or higher, meaning that they score higher (i.e., worse) than 50 percent of neighborhoods in the state. In particular, most or all of Ontario scores high for several types of air pollution, releases of toxic materials from major goods movement transportation corridors and industrial facilities, levels of cardiovascular disease, levels of poverty, and limited educational attainment. However, the highest scores in Ontario for these and numerous other factors are concentrated in the city's low-resource areas, showing that residents of these areas are disproportionately burdened by multiple sources of pollution and socioeconomic inequities.

Figure 1 Map of Ontario



Source: PlaceWorks 2021

Areas of Ontario

Northwest area/Northwest Ontario

Northwest area/Northwest Ontario

Southeast area/Southeastern Ontario

Eastern area/Eastern Ontario

Southern area/South Ontario

Boundaries

Ontario City Boundary

County Boundary

Source: City of Ontario Draft Housing Element, 2022

ONTARIO TOGETHER

Ontario Together is a community plan for advancing environmental quality and racial equity in disadvantaged communities in central Ontario. Its mission is to achieve sustainable neighborhood transformation through implementation of various GHG reduction strategies in the project area, a five-square-mile area in the heart of downtown. The strategies include improvements in access to solar energy, public transit, affordable housing, green jobs, and bicycle and pedestrian facilities for residents. Project outcomes include planting 365 trees, distributing 100 free bus passes, and implementing numerous bicycle and pedestrian infrastructure improvements, including five miles of bicycle lanes and three miles of sidewalks along Mission Blvd, and crosswalk installations on Euclid and Holt. Planned projects include Omnitrans Bus service improvements, Workforce Development, Small Business Support, and the Ontario Carbon Farm. The Ontario Carbon Farm is a partnership with Huerta del Valle Community Garden to establish a community composting site that will generate high-quality compost from food waste collected in the city.

Although the 2022 CCAP is not a dedicated equity plan, many of the factors that contribute to inequities are related to GHG emissions. For example, many sources of air pollution are also sources of GHGs, so reducing air pollution from these activities often helps to reduce Ontario's GHG emissions. Additionally, the 2022 CAP recommends numerous strategies to reduce GHG emissions that provide other co-benefits that enhance quality of life in Ontario, including improving inequities and supporting efforts to expand resources and opportunity in low-resource areas.

Through direct reductions in air pollution and indirect improvements to community well-being, the CCAP can contribute significantly in many ways to achieving a more equitable and fair Ontario. CCAP efforts that can address these issues may include:

- Improved transit service can help improve job access, provide greater mobility options for people who are unable to drive, and allow people to meet many of their daily needs at a lower cost than owning a car. Transit also releases less air pollution per rider than a private vehicle, helping to improve air quality.
- Expanded pedestrian and bicycle infrastructure can encourage more active lifestyles and recreational activities. Increased pedestrian and bicycle activity also allows people to reduce their dependencies on private vehicles and associated costs and further improves local air quality.
- Energy efficiency retrofits can result in significant cost savings for homes and businesses and help create more comfortable, safer living and working conditions.

- Green space can encourage recreation, provide shade, and improve neighborhood aesthetics.
- Increased water and energy conservation can improve community resilience to droughts and peak demand energy shortages.
- Increased adoption of solar energy systems can improve community resilience to power outages and power safety shut-off events.
- Increased demand for energy efficiency retrofits, solar and battery energy systems, electric vehicle charging installation, and water-efficient landscaping can provide new job opportunities in the community. These can be well-paying jobs that often can be made available to members of underserved communities, particularly when the community or local agencies provide free or low-cost job-training resources.

Regulatory Framework

California law first directly addressed climate change in 1988, when AB 4420 directed state agencies to prepare a GHG inventory and study the impacts of climate change. Since then, California has adopted several laws to assess climate change, analyze GHG emissions and their effects, reduce emissions, identify climate change impacts, and prepare for the impacts of climate change. Many of these laws and associated regulations affect local governments, although only some create specific requirements for individual communities. In addition to state regulations, there are local and regional planning and regulatory processes that support or direct the reduction of GHGs. These laws and associated regulations are briefly summarized in this section.

The Ontario Plan 2050

TOP 2050 provides direction on how the City will fulfill its strategic vision and manage fiscal sustainability, daily operations, and implementation of the goals and policies during the next 30 years. TOP 2050 is a “business plan” that focuses on conducting technical updates to comply with state law, including regulations that address housing, community health, environmental justice, climate adaptation, and mobility. In addition, TOP 2050 focuses on long-term growth and fiscal sustainability and advancing the Implementation Plan’s tracking and feedback component.¹⁰

Several TOP policies are relevant to climate change, including policies addressing environmental justice and policies in the Safety Element. Environmental justice policies address inequities in the built environment in compliance with Senate Bill 1000 (SB 1000). SB 1000, approved in 2016, requires local governments to identify disadvantaged communities and address environmental justice in the general plan update through policies that aim to reduce health risks in disadvantaged

communities, promote inclusive civic engagement in the planning process, and prioritize improvements that address the needs of disadvantaged communities.¹¹ In compliance with SB 1000, TOP 2050 identifies goals, policies, and objectives that reduce health risks caused by environmental pollution and reduce disparities in access to amenities, prioritizing improvements in disadvantaged communities most affected.

The Safety Element addresses public safety concerns related to the natural and built environment, including climate change, in compliance with SB 379. SB 379, approved in 2015, requires local governments to include a vulnerability assessment in the safety element update that identifies the unique risks to the community caused by climate change and informs policies to protect those vulnerabilities.¹² The Safety Element provides information about risks from natural and human-made hazards and includes goals, policies, and actions designed to protect the community and its property from hazards. Other TOP 2050 elements include policies and actions that affect GHG emission reduction and resilience.

- **The Land Use Element** encourages infill development, mixed-use development, and development around transit to reduce vehicle miles traveled (VMT).
- **The Housing Element** encourages development of low-income, moderate-income, and market rate housing at higher densities that meet HCD criteria for each income category.
- **The Mobility Element** addresses improvements in active transportation infrastructure based on the Ontario Active Transportation Master Plan and Transit Priority Areas.

Each of these elements plays a role in enhancing the overall sustainability and resilience of the community.

TOP 2050, which is scheduled for adoption in 2022, will reference the 2022 CCAP, recognizing the City's climate action planning efforts must be updated on a more regular basis than the General Plan to be responsive to changing regulations, guidance, technology, best practices, and science.

The San Bernardino County Regional Greenhouse Gas Reduction Plan

In 2021, SBCTA adopted an update to the San Bernardino County Regional Greenhouse Gas Reduction Plan, a regional effort to address statewide greenhouse gas emissions in response to AB 32. The plan assesses existing and projected GHG emissions for all participating communities in San Bernardino County to identify regional and local strategies to reduce GHGs. The regional effort is the product of an informal partnership among 25 jurisdictions, including the City of Ontario, led by SBCTA. The plan includes a 2016 GHG inventory, GHG emissions forecasts for 2030 and 2045, and strategies for reducing emissions in Ontario, including 7 state measures and 27 local measures. The GHG reduction measures have been vetted and adopted by all 25 partnership

cities. The plan is accompanied by an assessment, monitoring, and implementation tool, called the San Bernardino County GHG Reduction Tool, which allows the City to quantify emissions reductions and measure progress in achieving emissions reductions from state and local measures. The 2022 Ontario CCAP is consistent with the regional plan and draws from the plan's GHG reduction strategies.

Title 24: Building Standards Code

Title 24 is the section of the California Code of Regulations that establishes standards to promote energy efficiency, public health, and greenhouse gas reduction standards for the construction of new commercial, residential, and public school buildings. Title 24 includes Part 6, Building Energy Efficiency Standards (also known as the Energy Code), which is focused on reducing energy consumption of new buildings, and Part 11, CALGreen Code, which is focused on reducing environmental impacts and improving public health through mandatory measures in the building sector.¹³

Title 24 is applied at the local level through the building permit and development review process, implemented through the municipal building code. On November 19, 2019, the City enacted an ordinance to amend the municipal code and adopt by reference the 2019 California Building Standards Code, which includes the CALGreen Code. This ordinance will require compliance with statewide Title 24 standards that improve energy efficiency, public health, and environmental sustainability in new homes and nonresidential buildings, such as high-efficiency electric air and water heating systems, improved ventilation systems, and rooftop solar and battery storage systems.¹⁴ This CCAP was prepared with the 2019 standards in effect. The next update, the 2022 Title 24 standards, will go into effect on January 1, 2023.

South Coast Air Quality Management District

The South Coast Air Quality Management District (SCAQMD) is the regional air district responsible for air quality regulation in most of Southern California, including Ontario and the rest of the valley region of San Bernardino County. The SCAQMD's primary responsibility is to regulate stationary sources and develop plans to achieve and maintain air quality standards.

Released in 2016, the latest update to the SCAQMD's Air Quality Management Plan (AQMP) provides a regional strategy to protect public health and the environment to eliminate health risk disparities from exposure to air pollution among Southern California communities. To make progress toward all state and federal air quality standards, the 2016 Air Quality Management Plan includes a wide range of goals in partnership with other agencies to decrease critical pollutants, greenhouse gases, and toxins as well as improve efficiency of energy use, on-road transportation, and goods movement. The most effective way to reduce air pollution impacts to residents of the

Southern California region, especially disproportionately impacted communities along transportation corridors, is to reduce emissions from mobile sources such as vehicles, buses, and trucks. The SCAQMD has partnered with the California Air Resources Board (CARB) and the U.S. Environmental Protection Agency (EPA) to update the AQMP to address mobile sources, including transportation control measures developed by the Southern California Association of Governments (SCAG). The 2016 plan emphasizes collaboration between agencies to develop new regulations that monitor and control emissions and the pursuit of grant funding and incentives to accelerate the adoption of cleaner technologies in vehicles, buildings, and industrial facilities.

The South Coast Air Basin is classified as an extreme nonattainment area for ground-level ozone under the EPA's current National Ambient Air Quality Standards, which means that it greatly exceeds the threshold for ozone levels deemed acceptable for human health. The 2022 AQMP is in the process of being updated and will include requirements for meeting the 2015 ozone National Ambient Air Quality Standards. According to the EPA, GHG emissions and ozone levels are correlated because ozone formation in the atmosphere accelerates as temperature increases. Ozone exposure can exacerbate human health conditions such as cardiovascular disease, asthma, and other respiratory diseases.¹⁵

Warehouse Indirect Source Rule 2305

On May 8, 2021, South Coast AQMD adopted Warehouse Indirect Source Rule 2305, which includes the Warehouse Actions and Investments to Reduce Emissions (WAIRE) Program and Rule 316. Rule 2305 establishes for the first time in Ontario a regulatory program designed to reduce air pollution (and indirect GHG emissions) caused by warehouse-related activities and is focused on emissions from vehicles that service large warehouses.

State of California Regulations and Guidance

California has adopted several laws to assess climate change, analyze GHG emissions and their effects, reduce emissions, and prepare for the impacts of climate change. These laws and associated regulations are briefly summarized here.

Executive Order S-03-05

In 2005, Governor Arnold Schwarzenegger issued Executive Order (EO) S-03-05, which established the first statewide GHG reduction goals for California: reduce emissions to 2000 levels by 2010, reduce emissions to 1990 levels by 2020, and reduce emissions 80 percent below 1990 levels by 2050.

Assembly Bill 32: California Global Warming Solutions Act of 2006

State of California guidance and targets for reductions in GHG emissions are generally embodied in the Global Warming Solutions Act, adopted with passage of AB 32 by the California state legislature on August 31, 2006. AB 32 follows the 2020 emissions reduction goal established in EO S-03-05. AB 32 includes the major GHGs and groups of GHGs that are being emitted into the atmosphere. These gases include carbon dioxide (CO₂), methane (CH₄), nitrous oxide (N₂O), hydrofluorocarbons (HFCs), perfluorocarbons (PFCs), sulfur hexafluoride (SF₆), and nitrogen trifluoride (NF₃).

Through the adoption of AB 32, the California Legislature declared:

(a) Global warming poses a serious threat to the economic well-being, public health, natural resources, and the environment of California. The potential adverse impacts of global warming include the exacerbation of air quality problems, a reduction in the quality and supply of water to the state from the Sierra snowpack, a rise in sea levels resulting in the displacement of thousands of coastal businesses and residences, damage to marine ecosystems and the natural environment, and an increase in the incidences of infectious diseases, asthma, and other human health-related problems.

(b) Global warming will have detrimental effects on some of California's largest industries, including agriculture, wine, tourism, skiing, recreational and commercial fishing, and forestry. It will also increase the strain on electricity supplies necessary to meet the demand for summer air-conditioning in the hottest parts of the state.

Climate Change Scoping Plan

The Climate Change Scoping Plan (Scoping Plan), adopted in 2008 and updated in 2014 and 2017, employs a variety of GHG reduction strategies that include direct regulations, alternate compliance mechanisms, incentives, voluntary actions, and market-based approaches like a cap-and-trade program. The 2008 Scoping Plan identified local governments as strategic partners to achieving the state goal and translated California's 2020 GHG reduction goal of returning to 1990 emission levels to a 15 percent reduction of "existing" emissions by 2020 (emissions between 2005 and 2008). On December 14, 2017, CARB adopted the 2017 Climate Change Scoping Plan Update (2017 Scoping Plan) to address the 2030 target and 2050 goal for the state. The 2017 Scoping Plan lays out a path to reduce statewide GHG emissions 40 percent below 1990 levels by 2030.

California's climate strategy outlined in the 2017 Scoping Plan requires contributions from all sectors of the economy, including enhanced focus on zero-emission vehicles (ZEVs) and near-ZEV technologies; continued investment in renewables such as solar roofs, wind, and other types of distributed generation; greater use of low carbon fuels; integrated land conservation and development strategies; coordinated efforts to reduce emissions of short-lived climate pollutants

(methane, black carbon, and fluorinated gases); and an increased focus on integrated land use planning to support livable, transit-connected communities and conserve agricultural and other lands. Requirements for GHG reductions at stationary sources complement local air pollution control efforts by the local air districts to tighten criteria air pollutant and toxic air contaminant emissions limits on a broad spectrum of industrial sources.

In addition to these statewide strategies, the 2017 Climate Change Scoping Plan identified local governments as essential partners in achieving the state's long-term GHG reduction goals and recommended local actions to reduce GHG emissions. It proposes goals for local governments to reduce their GHG emissions to 40 percent below 2020 levels by 2030 (1,128,580 metric tons of CO₂ equivalent [MTCO₂e]) and 80 percent below 2020 levels by 2050, consistent with statewide reduction goals. In per capita terms, statewide guidance recommends targets of to 6 MTCO₂e per capita by 2030, and 2 MTCO₂e per capita by 2050 (376,190 MTCO₂e). CARB recommends that local governments evaluate and adopt robust and quantitative, locally appropriate goals that align with the statewide per capita targets and sustainable development objectives and develop plans to achieve the local goals.

As of April 2022, CARB is working on a third update to the Scoping Plan in response to the adoption of Senate Bill 32 in 2016 and the governor's 2018 goal of achieving statewide carbon neutrality by 2045. The updated Scoping Plan was released for public review in May 2022 and is set to be adopted sometime in late 2022.

California Cap-and-Trade Program

The Cap-and-Trade Program, which took effect in 2012, was originally developed under AB 32 as a market mechanism-based measure to reduce statewide emissions and move the state toward meeting the year 2020 GHG reduction target. In 2017, passage of Assembly Bill 398 extended the program to 2030, which coincides with the year 2030 GHG reduction target set under SB 32. The Cap-and-Trade Program would deliver the additional GHG reductions necessary to ensure that the 2030 GHG reduction target is met.¹⁶ Overall, the program covers approximately 85 percent of all emissions in California and covers a variety of emissions sectors, such as electricity generators and large industrial facilities, which include refineries that generate 25,000 MTCO₂e or more per year. In addition, the program also covers distributors of transportation fuels and requires fuel distributors to reduce GHG emissions by either supplying low carbon fuels or purchasing pollution permits to cover GHG emissions generated from combustion of conventional petroleum-based fuel the distributors supply.¹⁷

Senate Bill 375: Sustainable Communities and Climate Protection Act of 2008

In 2008, SB 375, the Sustainable Communities and Climate Protection Act, was adopted to connect the GHG emissions-reductions targets established in the 2008 Scoping Plan to local land use decisions that affect travel behavior. Its intent is to reduce GHG emissions from light-duty trucks and automobiles (excludes emissions associated with goods movement) by aligning regional long-range transportation plans, investments, and housing allocations to local land use planning to reduce VMT and vehicle trips. Specifically, SB 375 required CARB to establish GHG emissions-reduction targets for each of the 18 metropolitan planning organizations. SCAG is the metropolitan planning organization for the Southern California region, which includes the City of Ontario. Pursuant to the recommendations of the Regional Transit Technical Advisory Committee (RTTAC), CARB adopted per capita reduction targets for each of the metropolitan planning organizations. The reduction targets for the Southern California Association of Governments are to reduce GHG emissions from passenger vehicles and light duty trucks by 8 percent per capita by 2020 and 19 percent by 2035 relative to 2005 emissions levels.¹⁸

Executive Order B-30-15

EO B-30-15 was signed April 29, 2015, by Governor Jerry Brown and set a goal to reduce GHG emissions in the state to 40 percent below 1990 levels by year 2030. EO B-30-15 directed CARB to update the Scoping Plan to quantify the 2030 GHG reduction goal for the state and requires state agencies to implement strategies to meet the interim 2030 goal. It also requires the California Natural Resources Agency to conduct triennial updates of the California adaption strategy, Safeguarding California, to ensure climate change is accounted for in state planning and investment decisions.

Senate Bill 32 and Assembly Bill 197

In September 2016, Governor Brown signed SB 32 and AB 197, making the Executive Order goal for year 2030 into a statewide, mandated legislative target. AB 197 established a joint legislative committee on climate change policies and requires CARB to prioritize direct emissions reductions rather than the market-based cap-and-trade program for large stationary, mobile, and other sources.

Executive Order B-55-18

Executive Order B-55-18, signed September 10, 2018, sets a goal “to achieve carbon neutrality as soon as possible, and no later than 2045, and achieve and maintain net negative emissions thereafter”. EO B-55-18 directs CARB to work with relevant state agencies to ensure future Scoping Plans identify and recommend measures to achieve the carbon neutrality goal. The goal of carbon neutrality by 2045 is in addition to other statewide goals, meaning that not only should emissions

be reduced to 80 percent below 1990 levels by 2050, but that, by no later than 2045, the remaining emissions should be offset by equivalent net removals of CO₂e from the atmosphere, including through sequestration in forests, soils, and other natural landscapes.

Executive Order N-79-20

On September 23, 2020, Governor Newsom issued EO N-79-20, which sets a time frame for the transition to ZEVs, including passenger vehicles, trucks, and off-road equipment. It directs CARB to develop and propose passenger vehicle and truck regulations requiring increasing volumes of new ZEVs sold in the California toward the target of 100 percent of in-state sales by 2035, and similar regulations for medium- and heavy-duty vehicles to achieve a target of 100 percent fleet electrification by 2045. In addition, Executive Order N-79-20 provides strategies – in cooperation with other state agencies, the EPA, and local air districts – to achieve 100 percent zero emissions from all off-road vehicles and equipment operations in California by 2035.

State Regulations of GHG Emissions from Medium- and Heavy-Duty Trucks

Existing heavy-duty vehicle regulations in California include engine criteria emission standards, tractor-trailer GHG emission reduction requirements, and in-use fleet retrofit requirements, such as the Truck and Bus Regulation. The following state strategies reduce GHG emissions from medium- and heavy-duty trucks:

- CARB’s Phase 1 and 2 Heavy-Duty Vehicle GHG Standards establish emission limits on truck and engine manufacturers and harmonize with the EPA rule for new trucks and engines nationally.
- CARB’s Mobile Source Strategy focuses on reducing GHGs through the transition to ZEVs and low-emission vehicles and from medium-duty and heavy-duty trucks.¹⁹
- CARB’s Advanced Clean Fleets regulation, starting in 2025, will require public agencies, drayage operations, and high-priority fleets to prepare for the transition to zero-emission trucks and buses where feasible.
- CARB’s Advanced Clean Trucks regulation, starting in 2025, will require that manufacturers of trucks meet a minimum sales requirement for ZEVs.
- CARB’s Sustainable Freight Action Plan establishes a goal to improve freight efficiency by 25 percent by 2030, deploy over 100,000 freight vehicles and equipment capable of zero-emission operation, and maximize both zero-emissions and near-zero emission freight vehicles and equipment powered by renewable energy by 2030.²⁰

- CARB's Emissions Reduction Plan for Ports and Goods Movement (Goods Movement Plan) in California focuses on reducing heavy-duty truck-related emissions by the establishment of emissions standards for trucks, fleet turnover, truck retrofits, and restriction on truck idling.
- CARB's Small Off-Road Engines (SORE) regulation that will require most newly manufactured small off-road equipment, such as leaf blowers and lawn mowers, to meet zero-emission standards by 2024, and portable generators to meet these standards in 2028.

These state regulations would contribute to reducing medium-duty and heavy-duty truck GHG emissions in Ontario.

California Environmental Quality Act

CEQA requires that many proposed development projects conduct an environmental review to identify how the project may impact the environment, including an analysis of GHG emissions from the project and their contribution to climate change. The guidelines encourage the adoption of policies or programs as a means of comprehensively addressing the cumulative GHG emissions and associated impacts of projects (See CEQA Guidelines, Sections 15064(h)(3) and 15130 (c)). Consistent with the CEQA Guidelines, lead agencies may use adopted GHG reduction plans to assess the cumulative impacts of discretionary projects on climate change. In addition, the CEQA Guidelines provide a mechanism to streamline development review of future projects.

Specifically, lead agencies may use adopted plans consistent with State CEQA Guidelines Section 15183.5 to analyze and mitigate the significant effects of GHGs under CEQA at a programmatic level by adopting a plan for the reduction of GHG emissions. Later, as individual projects are proposed, project-specific environmental documents may tier from and/or incorporate by reference that existing programmatic review in their cumulative impact analysis. If a proposed development within the City of Ontario is consistent with the emission reduction strategies included in the CCAP and the programs are developed as a result of the CCAP, the project would have a less-than-significant impact on climate change.

A project-specific environmental document that relies on this 2022 CCAP for its cumulative impacts analysis must identify specific GHG reduction strategies applicable to the project and demonstrate the project's incorporation of the strategies. Project applicants and City staff will identify specific strategies applicable to each project during project review. If applicable strategies are not otherwise binding and enforceable, they must be incorporated as mitigation strategies for the project. If substantial evidence indicates that the GHG emissions of a proposed project may be cumulatively considerable, notwithstanding the project's compliance with specific strategies in this 2022 CCAP, an Environmental Impact Report (EIR) must be prepared for the project.

This 2022 CCAP meets the CEQA Guidelines and commitments, as follows:

- Quantifies emissions, both existing and projected over a specified time period, resulting from activities within a defined geographic area.
- Establishes a level, based on substantial evidence, below which the contribution of emissions from activities covered by the plan would not be cumulatively considerable. The 2022 CCAP builds off the 2014 CCAP's GHG reduction targets by establishing two new targets for 2030 and 2050, consistent with State guidance, and provides substantial evidence for how Ontario can meet these targets and provide substantial progress toward the State's statewide goal of carbon neutrality by 2045.
- Identifies and analyzes the emissions resulting from specific actions or categories of actions anticipated within the geographic area.
- Specifies strategies and associated actions, that, if implemented, substantial evidence demonstrates they would collectively achieve the specified future per capita emissions level.
- Establishes a mechanism to monitor the 2022 CCAP's progress toward achieving specific levels and to require amendment if the plan is not achieving those levels.
- Environmental review of the CCAP has been conducted as a component of the Supplemental Environmental Impact Report (SEIR) for The Ontario Plan 2050. As identified in the Draft SEIR, the CCAP is summarized in Section 5.8, Greenhouse Gas Emissions. The measures included in the 2022 update to the CCAP are not substantially different than those of the 2014 CCAP, and therefore there is no change in the environmental impacts associated with the CCAP.

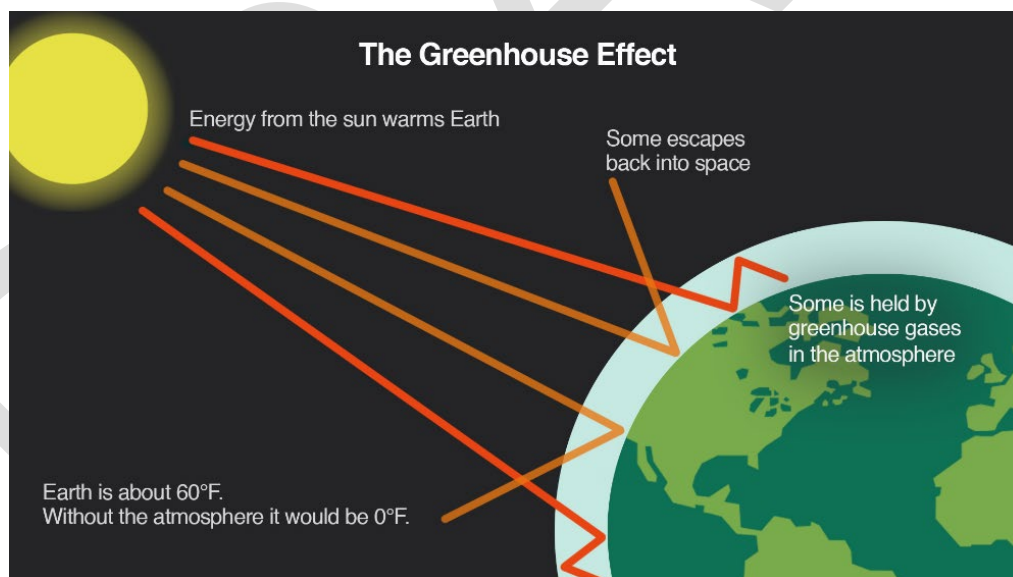
3. ONTARIO'S GREENHOUSE GAS EMISSIONS

Climate Science

Climate is the long-term average of weather conditions, such as temperature and precipitation. While it is normal for Earth's climate system to experience long-term shifts in these average conditions, human activity is causing global climate change at a much faster pace than in the past. These activities, predominantly the burning of fossil fuels, emit heat-trapping gases called GHGs that build up in the atmosphere. As GHG levels increase, Earth's atmosphere traps more heat, triggering changes in the global climate system that may have serious and potentially catastrophic impacts on people, community assets, and natural systems.

The heat-trapping effect of GHGs is known as the "greenhouse effect" because the Earth's atmosphere acts like a greenhouse, warming the planet in much the same way that an ordinary greenhouse warms the air inside its glass walls. This process is shown in **Figure 2**.

Figure 2 The Greenhouse Effect



Source: Climate Central²¹

3. Ontario's Greenhouse Gas Emissions

GHGs are naturally occurring gases, such as water vapor, carbon dioxide (CO₂), methane (CH₄), and nitrous oxide (N₂O), that absorb heat radiated from the Earth's surface. GHGs are transparent to certain wavelengths of the Sun's radiant energy, including visible light, allowing sunlight to penetrate deep into the atmosphere or all the way to Earth's surface. Clouds, ice caps, and particles in the air reflect about 30 percent of this radiation, but oceans and land masses absorb the remaining 70 percent before releasing it back toward space as infrared radiation. GHGs and clouds effectively prevent some of the infrared radiation from escaping; they trap the heat near Earth's surface where it warms the lower atmosphere. The Intergovernmental Panel on Climate Change (IPCC) states that if this natural barrier of atmospheric gases was not present, the heat would escape into space, and Earth's average global temperatures could be as much as 61 degrees Fahrenheit (°F) cooler.²²

The IPCC's Sixth Assessment Report summarizes the latest scientific understanding of global climate change and projects future conditions using the most comprehensive set of recognized global climate models. The report, released in 2021, considers all impacts that human activities have on global climate, temperature, natural and managed ecosystems, natural resources, human mental and physical health, human migration, urban infrastructure, and food systems. The report describes the existing impacts of atmospheric warming that are currently being observed as a result of the atmospheric imbalance and projected conditions under different scenarios, depending on the success of climate mitigation on a global scale. The report states that:

... widespread and severe loss and damage to human and natural systems are being driven by human-induced climate changes increasing the frequency and/or intensity and/or duration of extreme weather events, including droughts, wildfires, terrestrial and marine heatwaves, cyclones (high confidence), and flood (low confidence). Extremes are surpassing the resilience of some ecological and human systems, and challenging the adaptation capacities of others, including impacts with irreversible consequences (high confidence).²³

Human activities are exerting a major and growing influence on the climate by changing the composition of the atmosphere and by modifying the land surface. Particularly, the increased consumption of fossil fuels (e.g., natural gas, coal, gasoline) has substantially increased atmospheric levels of GHGs. The Sixth Assessment Report projects five different temperature scenarios, all of which project 2021–2040 temperatures 2.16°F to 3.2°F warmer than the 1986–2005 average temperature, and potentially over 10.26°F warmer by 2100 under the highest emissions scenario.

The Sixth Assessment Report notes that if trends remain unchanged, continued GHG emissions above current rates will lead to further changes in the global climate system and pose even greater risks than those currently seen.

Local GHG Emissions

A community-wide GHG inventory identifies GHG emissions that result from activities of Ontario residents, employees, visitors, and other community members. Examples include residents driving cars, homes using water, and businesses using electricity. A forecast is a projection of these emissions for future years. Preparing the GHG inventory and forecast is the first step in creating a strategy to reduce Ontario's annual emissions. Determining the annual level of GHG emissions helps the City to establish attainable goals for reducing emissions and develop policies and programs to achieve those goals.

In some cases, the GHG emissions themselves are occurring within the city limits of Ontario, such as emissions from combustion of natural gas used in community homes or businesses. In some cases, the activity occurs within Ontario, but the emissions occur elsewhere, such as GHG emissions from power plants in other communities that generate the electricity used in Ontario. In a few cases, the activity occurs partially in Ontario and partially elsewhere, such as emissions from vehicle trips between Ontario and another community. Overall, the community-wide inventory is an assessment of GHG emissions that are attributed to Ontario, although the emissions themselves or even the activities may not occur within the Ontario city limits.

As part of the preparation of the 2014 CCAP, the City of Ontario and its regional partners and technical consultants prepared community-wide GHG inventories for the calendar year 2008 and a projection of GHG emissions in 2020. The 2014 CCAP identified the year 2008 as the baseline year for emission reductions because this was considered a year with good data availability at the time, consistent with state guidance, and without any unusual factors that might affect GHG emissions. In 2021, SBCTA prepared a regional GHG inventory for the calendar year 2016 as part of the San Bernardino County Regional GHG Reduction Plan.

As part of the 2022 CCAP, the City prepared a set of new and revised community-wide GHG inventories and forecasts. The project team prepared a new 2019 community-wide GHG inventory, revised the existing 2008 and 2016 inventories to ensure they used the same methods and data sources as the 2019 inventory so they could be accurately compared, and prepared 2030 and 2050 forecasts. The team selected 2019 for the new inventory since 2019 was the most recent year with complete data that was not affected by the COVID-19 pandemic, as the changes in GHG emissions created by the pandemic are expected to be mostly temporary and are not an appropriate basis for long-term GHG emission planning.

This chapter presents the results of the updated and new City of Ontario community-wide GHG inventories and forecasts and a discussion of the methods used to prepare and update the GHG inventories.

3. Ontario's Greenhouse Gas Emissions

Community-Wide GHG Inventory

Protocols and Methods

A series of guidance documents, called protocols, recommend how to adequately assess GHG emissions. The project team prepared the new GHG inventory and updated the past GHG inventories consistent with the guidance in widely adopted, standard protocol documents. These protocols provide guidance on what activities should be evaluated in the GHG inventories and how emissions from those activities should be assessed. Using standard methods also allows an easy comparison of GHG emission levels across multiple years and communities.

The community-wide GHG inventory uses the United States Community Protocol for Accounting and Reporting of Greenhouse Gas Emissions (U.S. Community Protocol), which was first developed in 2012 and updated most recently in 2019. The California Governor's Office of Planning and Research encourages cities and counties in California to follow the U.S. Community Protocol for community-wide GHG emissions.

The Global Protocol for Community-Scale Greenhouse Gas Inventories (Global Protocol) was first developed in 2014 for preparing international community-scale GHG inventories. It is mostly consistent with the U.S. Community Protocol, although it contains additional guidance and resources to support a wider range of activities in other countries. The project team used the Global Protocol to assess GHG emissions from sources that are not covered in the U.S. Community Protocol.

GHG inventories are estimates of GHG emissions based on these standard methods and verified datasets. They are not direct measurements of GHG emissions, but when the standard methods in the protocols are used with accurate data from appropriate sources, GHG inventories provide reliable estimates of local emission levels. However, due to potential data limitations, some inconsistencies in methods may remain. Any concerns about inconsistent methods are noted in the appropriate sector discussion.

UNITS OF MEASURE AND EMISSION FACTORS

GHG inventories assess emissions in a unit called carbon dioxide equivalent (CO_2e), which is a combined unit of all GHGs analyzed in the inventory. Because different GHGs have different effects on the processes that drive climate change, CO_2e is a weighted unit that reflects the relative potency of the different GHGs. These inventories report amounts of GHGs in metric tons of CO_2e (MTCO_2e), that is, 1,000 kilograms or approximately 2,205 pounds.

The project team calculated most of the GHG emissions using data on GHG-generating activities in combination with emission factors. An emissions factor describes how many metric tons of CO₂e emissions are released per unit of an activity. For instance, an emissions factor for electricity describes the MTCO₂e emissions produced per kilowatt-hour (kWh) of electricity used, and an emission factor for on-road transportation describes the MTCO₂e emissions produced per mile of driving. **Table 1** shows the emissions factors for 2019. Some sectors do not have specific emission factors, including agriculture and off-road emissions, and were calculated using formulae or models.

Table 1 Emissions Factors, 2008 to 2019

Sector	Unit	2008	2016	2019	Percentage Change, 2008–2019	Source
SCE electricity	MTCO ₂ e/ kWh	0.000295	0.000256	0.000208	-29%	SCE
Natural gas	MTCO ₂ e / therm	0.005677	0.007341	0.005272	-7%	US Community Protocol
On-road transportation (light- and medium-duty vehicles)	MTCO ₂ e / mile	0.000432	0.000363	0.000348	-19%	California Air Resources Board
On-road transportation (heavy-duty vehicles)	MTCO ₂ e / mile	0.001007	0.001248	0.001256	25%	California Air Resources Board
On-road transportation (total)	MTCO ₂ e / mile	0.000478	0.000430	0.000434	-9%	California Air Resources Board
Municipal solid waste (MSW)	MTCO ₂ e / ton	0.296215	0.286062	0.286062	-3%	CalRecycle
Alternative daily cover (ADC)	MTCO ₂ e / ton	-	-	0.247191	-	CalRecycle

3. Ontario's Greenhouse Gas Emissions

Sectors

The community-wide GHG inventory assessed GHG emissions from nine categories of activities, known as sectors.

- **Transportation.** GHG emissions created by driving on-road vehicles in Ontario, including passenger and freight vehicles.
- **Residential energy.** GHG emissions attributed to the use of electricity and natural gas in residential buildings.
- **Nonresidential energy.** GHG emissions attributed to the use of electricity and natural gas in nonresidential buildings.
- **Solid waste.** GHG emissions released from trash collected from residents and businesses.
- **Off-road equipment.** GHG emissions from equipment that does not provide on-road transportation (excluding agricultural equipment), such as tractors for construction or equipment used for landscape maintenance.
- **Agriculture.** GHG emissions from various agricultural activities, including agricultural equipment, crop cultivation and harvesting, and livestock operations.
- **Water and wastewater.** This sector accounts for the electricity used to transport every gallon of water to or wastewater from city residents and businesses, as well as direct emissions from processing wastewater.
- **Land use and sequestration.** Emissions released into the atmosphere from development of previously undeveloped land and GHG emissions absorbed and stored in trees and soils on locally controlled lands as part of healthy ecosystems.
- **Stationary sources.** Emissions from fuel use at major industrial facilities permitted by state and regional air quality authorities. Stationary source emissions are informational and are not included in the community total.

Per Capita GHG Emissions

In addition to analyzing absolute or total community-wide GHG emissions, the project team also assessed GHG emissions per capita (per person) in Ontario. Per capita emissions show the average number of emissions per service population, which is the sum of community residents and employees in Ontario, so the service population per capita emissions account for activities of both residents and workers in Ontario. Per capita GHG emissions show changes in emissions over time

and are helpful for showing consistency with state-recommended GHG reduction targets. **Table 2** provides demographic context for the GHG inventory by showing a summary of change in population, households, jobs, service population, and VMT between 2008 and 2019. Throughout this document, the GHG emissions are presented in terms of per capita emissions using units of MTCO_{2e} per person in the Ontario service population.

Table 2 City of Ontario Community-Wide Demographic Change, 2019

Metric	2008	2016	2019	Percentage Change 2008–2019	Relevant Sectors
Population	163,951	172,249	178,606	9%	Off-road equipment
Households	44,673	46,001	48,275	8%	Residential energy, off-road equipment
Residents per household	104,233	113,859	128,637	23%	None
Jobs	268,184	286,108	307,243	15%	Nonresidential energy, off-road equipment
Service population	163,951	172,249	178,606	130%	Solid, water and wastewater, off-road equipment
Vehicle Miles Traveled (VMT)	1,969,786,450	1,995,399,670	2,151,423,590	9%	Transportation

Sources: 2019 demographic numbers are from US Census, the Department of Finance, and San Bernardino County Transportation Authority (SBCTA).

Notes: Service population is the sum of populations and jobs. All numbers except residents per household are rounded to the nearest 10.

3. Ontario's Greenhouse Gas Emissions

Inventory Results

The 2022 CCAP includes 2008 and 2016 community-wide GHG inventories from the San Bernardino Regional GHG Reduction Plan, with a few additional sources of GHG emissions and some changes to the methods to be consistent with current guidance, and a 2019 community-wide GHG inventory.

Table 3 and **Figure 3** show the per capita GHG emissions by sector for the three inventory years. Per capita emissions in Ontario decreased from approximately 8.25 MTCO_{2e} in 2008 to approximately 5.55 MTCO_{2e} per person in 2019, a decrease of 33 percent. In terms of absolute GHG emissions, Ontario's community-wide GHG emissions decreased from 2,212,900 MTCO_{2e} in 2008 to 1,703,730 MTCO_{2e} in 2019, a decrease of approximately 23 percent. The sectors with the highest per capita emissions corresponded with the highest-emitting sectors, the transportation and nonresidential energy sectors. **Table 4** shows how each of the community's sectors contributes to annual GHG emissions.

Table 3 Annual Per Capita GHG Emissions by Sector, 2008 to 2019

SECTOR	2008 MTCO _{2e}	2016 MTCO _{2e}	2019 MTCO _{2e}	Percentage Change, 2008–2019
Transportation	3.51	3.00	3.04	-13%
Nonresidential energy	2.77	1.80	1.29	-54%
Residential energy	0.72	0.55	0.50	-30%
Solid waste	0.28	0.22	0.27	-4%
Off-road equipment	0.17	0.08	0.21	23%
Agriculture	0.63	0.21	0.16	-75%
Water and wastewater	0.15	0.08	0.06	-56%
Land use and sequestration	0.01	0.00	0.00	-77%
Total	8.25	5.95	5.54	-33%
Informational Items				
Stationary sources	1.50	0.25	1.05	-30%

Figure 3 Annual Per Capita GHG Emissions by Sector, 2008 to 2019

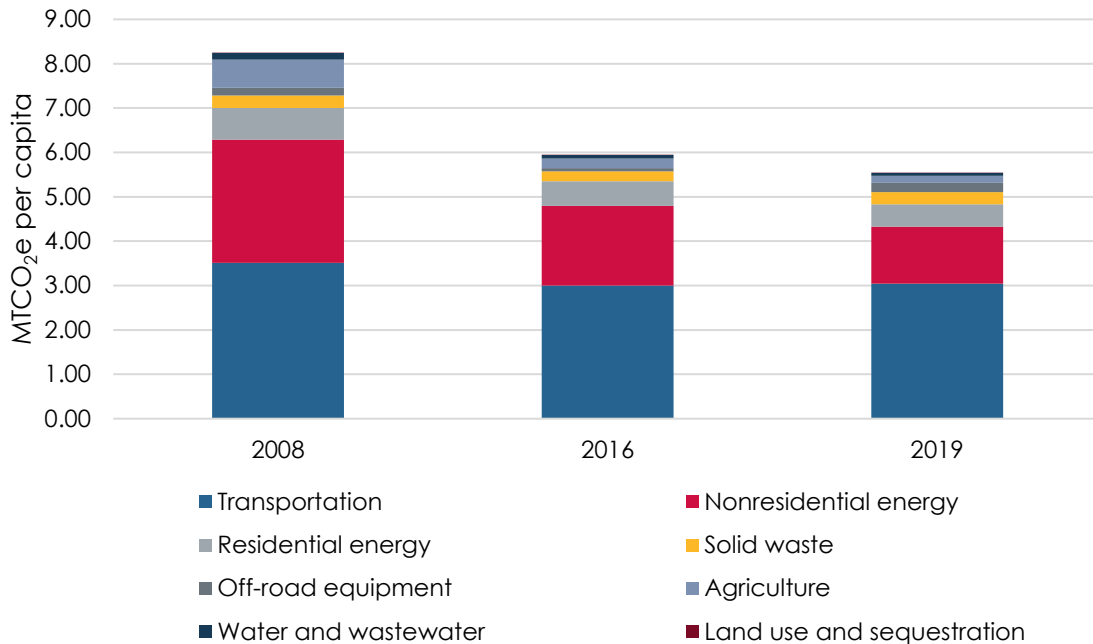


Table 4 Proportions of Annual Per Capita GHG Emissions by Sector in 2008 to 2019

Sector	2008 Proportion of Total	2016 Proportion of Total	2019 Proportion of Total
Transportation	43%	50%	55%
Nonresidential energy	34%	30%	23%
Residential energy	9%	9%	9%
Solid waste	3%	4%	5%
Off-road equipment	2%	1%	4%
Agriculture	8%	4%	3%
Water and wastewater	2%	1%	1%
Land use and sequestration	Less than -1%	Less than -1%	Less than -1%
Total annual MTCO₂e per capita	100%	100%	100%

The four sectors that experienced the largest decreases in annual per-capita GHG emissions between 2008 and 2019 are land use and sequestration, agriculture, water and wastewater, and nonresidential energy, which reduced emissions by 77 percent, 75 percent, 56 percent, and 54 percent, respectively. These changes are discussed more in the sector detail discussions below but are likely due to increases in supplies of renewable and carbon-free electricity, declines in natural gas use, and changes in agricultural activities and land use patterns.

3. Ontario's Greenhouse Gas Emissions

The only sector that experienced an increase in per capita emissions is the off-road equipment sector. Between 2008 and 2019, emissions from off-road equipment increased by 23 percent.

- The transportation sector remains the largest source of GHG emissions in Ontario, and this sector's share of community-wide GHG emissions has increased over time. Transportation emissions made up 43 percent, 50 percent, and 55 percent of total GHG emissions in 2008, 2016, and 2019, respectively.
- The transportation sector is followed by the nonresidential and residential energy sectors as the second- and third-largest sources of GHG emissions.
 - The nonresidential energy sector's share of GHG emissions steadily decreased between 2008 and 2019, from 34 percent in 2008 to 23 percent in 2019.
 - The residential energy sector's share of GHG emissions remained constant (9 percent) between 2008 and 2019.
- Solid waste is the fourth-largest source of GHG emissions. This sector's share of GHG emissions increased steadily from 3 percent in 2008 to 5 percent in 2019. Similarly, the off-road equipment sector's share of GHG emissions increased from 2 percent to 4 percent between 2008 and 2019.
- The agriculture sector's share of GHG emissions decreased dramatically, from 8 percent in 2008 to 3 percent in 2019. The land use and sequestration sector, the smallest source of GHG emissions in Ontario, remained at less than 1 percent during this period.
- The water and wastewater sector's share of GHG emissions also decreased, from 2 percent in 2008 to 1 percent in 2019.

Sector Details

Transportation

City of Ontario community members drove approximately 1.97 billion vehicle miles in 2008, which increased 9 percent to 2.15 billion vehicle miles in 2019.²⁴ The VMT in 2008 resulted in the release of approximately 942,020 MTCO_{2e} which dropped to approximately 934,590 MTCO_{2e} in 2019, a 1 percent decrease in absolute emissions. In per capita terms, on-road vehicle emissions decreased from 3.51 MTCO_{2e} per capita in 2008 to 3.04 MTCO_{2e} per capita in 2019, a decrease of 13 percent. There was a slight decrease in emissions despite an increase in VMT during this period due to the increasing use of fuel-efficient and electric vehicles. **Table 5** provides a breakdown of the activity data and per capita emissions for on-road transportation in the 2008, 2016, and 2019 community-wide inventory.

Table 5 Transportation Activity Data and Per Capita GHG Emissions by Subsector, 2008 to 2019

Sector	2008	2016	2019	Percentage Change, 2008–2019
Activity data (VMT)				
Light duty vehicles	1,767,683,550	1,843,389,270	1,945,876,580	10%
Heavy duty vehicles	202,102,900	152,010,400	205,547,010	2%
Total annual VMT	1,969,786,450	1,995,399,670	2,151,423,590	9%
Emissions (MTCO_{2e} per capita)				
Light duty vehicles	2.78	2.34	2.20	-21%
Heavy duty vehicles	0.74	0.66	0.84	14%
Total annual MTCO_{2e} per capita	3.51	3.00	3.04	-13%

All absolute numbers are rounded to the nearest 10. Totals may not equal the sum of individual columns.

Nonresidential Electricity

Ontario’s GHG emissions from nonresidential electricity, including commercial and industrial electricity use, decreased by 31 percent, from 375,540 MTCO_{2e} in 2005 to 257,820 MTCO_{2e} in 2019. In per capita terms, emissions from nonresidential electricity use decreased from 1.40 MTCO_{2e} per capita in 2008 to 0.84 MTCO_{2e} per capita in 2019, a decrease of 40 percent. Electricity use only decreased slightly during this period; however, the substantial decline in emissions is likely because of increased procurement of renewable energy by Ontario’s energy providers, in accordance with state law, and increased energy efficiency of commercial and industrial facilities and equipment that largely balances out increased energy use from job growth. [Table 6](#) provides a breakdown of the activity data and per capita GHG emissions for nonresidential electricity.

Table 6 Nonresidential Electricity Activity Data and Per Capita GHG Emissions, 2008 to 2019

Sector	2008	2016	2019	Percentage Change, 2008–2019
Activity data				
Nonresidential electricity (kWh)	1,273,004,280	1,275,792,750	1,242,306,720	-2%
Emissions (MTCO_{2e} per capita)				
Nonresidential electricity	1.40	1.14	0.84	-40%

All absolute numbers are rounded to the nearest 10. Totals may not equal the sum of individual columns.

3. Ontario's Greenhouse Gas Emissions

Residential Electricity

Ontario's GHG emissions from residential electricity decreased by 30 percent, from 93,680 MTCO_{2e} in 2008 to 65,690 MTCO_{2e} in 2019, although residential energy use declined less than 1 percent. In per capita terms, emissions from residential electricity use decreased from 0.35 MTCO_{2e} per capita in 2008 to 0.21 MTCO_{2e} per capita in 2019, a decrease of 39 percent. As with the nonresidential electricity sector, this is attributed to cleaner sources of electricity and gains in energy efficiency despite a growing residential population. [Table 7](#) provides a breakdown of the activity data and per capita GHG emissions for residential electricity.

Table 7 Residential Electricity Activity Data and Per Capita GHG Emissions, 2008 to 2019

Sector	2008	2016	2019	Percentage Change, 2008–2019
Activity data				
Residential electricity (kWh)	317,534,340	309,859,420	316,529,750	Less than -1%
Emissions (MTCO_{2e} per capita)				
Residential electricity	0.35	0.28	0.21	-39%

All absolute numbers are rounded to the nearest 10. Totals may not equal the sum of individual columns.

Nonresidential Natural Gas

Emissions from the nonresidential natural gas sector decreased by 63 percent, from 368,460 MTCO_{2e} in 2008 to 137,960 MTCO_{2e} in 2019. In per capita terms, emissions from nonresidential natural gas use decreased from 1.37 MTCO_{2e} per capita in 2008 to 0.45 MTCO_{2e} per capita in 2019, a decrease of 67 percent. This dramatic decline in GHG emissions is attributed to decreased use and combustion of natural gas, reflected in the proportional decline in nonresidential natural gas activity between 2008 and 2019. [Table 8](#) provides a breakdown of the activity data and per capita GHG emissions for nonresidential natural gas.

Table 8 Nonresidential Natural Gas Activity Data and Per Capita GHG Emissions, 2008 to 2019

Subsector	2008	2016	2019	Percentage Change, 2008–2019
Activity data				
Nonresidential natural gas (therms)	65,367,180	21,861,030	26,168,160	-60%
Emissions (MTCO_{2e} per capita)				
Nonresidential natural gas	1.37	0.66	0.45	-67%

All absolute numbers are rounded to the nearest 10. Totals may not equal the sum of individual columns.

Residential Natural Gas

Emissions from the residential natural gas sector decreased by 9 percent, from 98,620 MTCO_{2e} in 2008 to 89,340 MTCO_{2e} in 2019. In per capita terms, emissions from residential natural gas use decreased from 0.37 MTCO_{2e} per capita in 2008 to 0.29 MTCO_{2e} per capita in 2019, a decrease of 21 percent. Natural gas consumption in the residential sector remained fairly constant between 2008 and 2019. **Table 9** provides a breakdown of the activity data and per capita GHG emissions for residential natural gas.

Table 9 Residential Natural Gas Activity Data and Per Capita GHG Emissions, 2008 to 2019

Sector	2008	2016	2019	Percentage Change, 2008–2019
Activity data				
Residential natural gas (therms)	16,911,770	14,595,200	16,945,380	Less than 1%
Emissions (MTCO_{2e} per capita)				
Residential natural gas	0.37	0.28	0.29	-21%

All absolute numbers are rounded to the nearest 10. Totals may not equal the sum of individual columns.

Solid Waste

Ontario's GHG emissions associated with solid waste come from the decomposition of solid waste thrown away by community members and alternative daily cover (ADC, i.e., material applied at the landfill as pest and erosion control), and emissions associated with combustion of waste (transform tons). Emissions from the solid waste sector increased by 10 percent, from 75,930 MTCO_{2e} to 83,400 MTCO_{2e}, between 2008 and 2019. In per capita terms, emissions from solid waste decreased from 0.28 MTCO_{2e} per capita in 2008 to 0.27 MTCO_{2e} per capita in 2019, a

3. Ontario's Greenhouse Gas Emissions

decrease of 4 percent. Although Ontario community members generated more waste in 2019 than in 2008, the growing population helped offset this increase, resulting in a decrease in per capita emissions from the solid waste sector. **Table 10** provides a breakdown of activity and per capita emissions for the solid waste sector.

Table 10 Solid Waste Activity Data and Per Capita GHG Emissions by Subsector, 2008 to 2019

Sector	2008	2016	2019	Percentage Change, 2008–2019
Activity data (Tons)				
Solid waste	256,328	220,370	287,980	12%
ADC	-	-	2,670	-
Transform tons	-	-	1,030	-
Total activity (Tons)	256,328	220,370	291,680	14%
Emissions (MTCO₂e per capita)				
Solid waste	0.283	0.220	0.268	-5%
ADC	-	-	0.002	-
Transform tons	-	-	0.001	-
Total annual per capita MTCO₂e	0.283	0.220	0.271	-4%

All absolute numbers are rounded to the nearest 10. Totals may not equal the sum of individual columns.

Off-Road Equipment

Emissions from off-road equipment in Ontario increased by 41 percent, from 46,500 MTCO₂e to 65,480 MTCO₂e between 2008 and 2019. In per capita terms, emissions from off-road equipment increased from 0.173 MTCO₂e per capita in 2008 to 0.213 MTCO₂e per capita in 2019, an increase of 23 percent. Of the different types of off-road equipment, the three largest emissions sources are construction and mining equipment, portable equipment (compressors, generators, etc.), and industrial equipment such as forklifts and scrubbers. Increases in emissions from these sectors are at least partly due to an increase in construction and industrial/warehouse equipment use. Note that there is no activity data for this sector because emissions from off-road equipment are estimated from a model developed by CARB rather than from measurable metrics such as fuel consumption. **Table 11** provides off-road equipment per capita GHG emissions by subsector.

Table 11 Off-Road Equipment Per Capita GHG Emissions by Subsector, 2008 to 2019

Sector	2008	2016	2019	Percentage Change, 2008–2019
Emissions (MTCO_{2e} per capita)				
Agricultural	0.00004	0.00339	0.00020	424%
Airport Ground Support	0.032	0.000	0.031	-1%
Construction and Mining	0.049	0.036	0.075	53%
Industrial	0.026	0.003	0.026	1%
Lawn and Garden	0.001	0.002	0.001	-8%
Light Commercial	0.008	0.003	0.008	1%
Pleasure Craft	0.001	0.022	0.001	31%
Portable Equipment	0.053	0.000	0.059	11%
Recreational	0.004	0.001	0.004	-1%
Transportation Refrigeration Units	0.001	0.006	0.009	1,113%
Total annual per capita MTCO_{2e}	0.173	0.077	0.213	23%

All absolute numbers are rounded to the nearest 10. Totals may not equal the sum of individual columns.

Agriculture

Ontario has a small agricultural sector that includes approximately 3,000 acres of land for crop production and a small dairy sector with approximately 11,000 head of dairy cows. The sector includes N₂O emissions from nitrogen fertilizer (crop production), CH₄ gas emissions from livestock (enteric fermentation), and CH₄ emissions from livestock waste (manure management). The largest source of per capita GHG emissions in the agriculture sector is enteric fermentation, which was responsible for three-quarters of the total emissions for agriculture in 2019 (see [Table 12](#)).

Emissions from crop production decreased by 26 percent, from 1,150 MTCO_{2e} to 850 MTCO_{2e}, between 2008 and 2019. In per capita terms, emissions from crop production decreased from 0.004 MTCO_{2e} per capita in 2008 to 0.003 MTCO_{2e} per capita in 2019, a decrease of 35 percent. Although crop acreages did not change significantly, less fertilizer was applied in 2019 than in 2008 due to a reduction in the types of crops that require high applications of nitrogen fertilizer. Per capita emissions from enteric fermentation and manure management (methane released from anaerobic decomposition of animal wastes) decreased by 75 percent and 76 percent, respectively, between 2008 and 2019. This dramatic decrease in emissions is the result of a substantial decline in the number of dairy cows in livestock operations within the city limits. According to the San Bernardino County Crop Reports and the USDA Milk Production Report, the number of dairy cows in Ontario decreased by 80 percent between 2008 and 2019.

3. Ontario's Greenhouse Gas Emissions

Table 12 Agriculture Activity and Per Capita GHG Emissions by Subsector, 2008 to 2019

Sector	2008	2016	2019	Percentage Change, 2008–2019
Activity data				
Crops (acreage)	3,070	3,180	3,180	4%
Livestock (effective annual population)	53,240	18,900	10,890	-80%
Emissions (MTCO₂e per capita)				
Crop production	0.004	0.003	0.003	-35%
Enteric fermentation	0.471	0.157	0.118	-75%
Manure management	0.158	0.051	0.037	-76%
Total annual per capita MTCO₂e	0.633	0.211	0.158	-75%

All absolute numbers are rounded to the nearest 10. Totals may not equal the sum of individual columns.

Water and Wastewater

Emissions associated with the water and wastewater sector are counted as indirect or direct emissions. Indirect water emissions refer to emissions created by the electricity required to treat and move water to where it is used. Indirect wastewater emissions refer to electricity needed to move wastewater to water treatment facilities and to process and discharge it. Direct wastewater emissions refer to biological emissions produced directly by decomposition of wastewater solids during the treatment process.

Overall activity and emissions decreased in Ontario's water and wastewater sector. Emissions from this sector decreased by 49 percent between 2008 and 2019.

- Indirect water GHG emissions declined by 53 percent and indirect water consumption decreased by 40 percent between 2008 and 2019. In per capita terms, emissions from indirect water consumption decreased from 0.11 MTCO₂e per capita in 2008 to 0.04 MTCO₂e per capita in 2019, a decrease of 60 percent.
- Indirect wastewater GHG emissions decreased by 70 percent between 2008 and 2019. Emissions reductions in this sector were likely the result of increased water conservation and recycling efforts. In per capita terms, emissions from indirect wastewater decreased from 0.02 MTCO₂e per capita in 2008 to 0.01 MTCO₂e per capita in 2019, a decrease of 74 percent.

- Direct wastewater emissions increased by 7 percent from 2008 to 2019. In per capita terms, emissions from direct water consumption decreased from 0.15 MTCO_{2e} per capita in 2008 to 0.06 MTCO_{2e} per capita in 2019, a decrease of 6 percent.

The emissions data in **Table 13** provides a breakdown of the activity data and per capita GHG emissions for water and wastewater use.

Table 13 Water and Wastewater Activity Data and Per Capita GHG Emissions by Subsector, 2008 to 2019

Sector	2008	2016	2019	Percentage Change, 2008-2019
Activity data				
Water use (million gallons)	17,315	9,757	10,463	-40%
Water electricity use (kWh)	139,120,870	63,020,780	61,387,950	-56%
Wastewater electricity use (kWh)	9,735,890	6,806,891	7,672,160	-21%
Total activity (kWh)	148,856,760	69,827,671	69,060,110	-54%
Emissions (MTCO_{2e} per capita)				
Indirect water	0.10828	0.04851	0.0436	-60%
Indirect wastewater	0.02457	0.01887	0.0064	-74%
Direct wastewater	0.01574	0.01517	0.0147	-6%
Total annual per capita MTCO_{2e}	0.1486	0.0826	0.0648	-56%

All absolute numbers are rounded to the nearest 10. Totals may not equal the sum of individual columns.

Land Use and Sequestration

GHG emissions from land use and sequestration can be positive (a source of emissions) or negative (removing emissions from the atmosphere, creating what is known as a carbon “sink”). Natural and agricultural lands absorb carbon, storing it in biomass such as wood, plants, and soil. As a result, when these lands are preserved or when more street trees are planted, emissions from this sector are negative because GHGs are being removed from the atmosphere. However, developing natural or agricultural lands or converting them to a different form (for example, replacing forests with crop land) or removing street trees causes carbon to be released, creating GHG emissions.

This sector includes emission sources and sinks from two types of activities: sequestration of GHG emissions in street trees in urbanized areas, and emissions caused by permanently removing vegetation from natural lands or farmlands as a part of development. Carbon sequestration by citywide street trees remained fairly constant between 2008 and 2019 (ranging from -710 MTCO_{2e} to -730 MTCO_{2e}), offsetting some of the emissions caused by urban development. Between 2008

3. Ontario's Greenhouse Gas Emissions

and 2019, emissions from the development of agricultural land decreased by 57 percent (from 2,540 MTCO_{2e} to 660 MTCO_{2e}) due to a reduced rate of urban development during this period. In per capita terms, emissions from development of agricultural land decreased from 0.012 MTCO_{2e} per capita in 2008 to 0.005 MTCO_{2e} per capita in 2019, a decrease of 63 percent.

Most of the land developed between 2008 and 2019 was not land used for agricultural production and was categorized "Other Lands" by the Farmland Mapping and Monitoring Program (FMMP). According to FMMP, these lands include low density rural development, heavily forested land, mined land, or government land with restrictions on use. **Table 14** provides a breakdown of the land use change and street tree biomass acreage and per capita emissions.

Table 14 Land Use and Sequestration Activity Data and GHG Emissions by Subsector, 2008 to 2019

Sector	2008	2016	2019	Percentage Change, 2008–2019
Activity data (acres)				
Land use change (conversion of agricultural to urban land)	3,520	1,700	1,700	-52%
Street tree biomass	740	760	760	3%
Emissions (MTCO_{2e} per capita)				
Land use change (conversion of agricultural to development)	0.012	0.005	0.005	-63%
Street tree biomass	-0.003	-0.003	-0.002	-10%
Total annual per capita MTCO_{2e}	0.009	0.002	0.002	-77%

Notes: All absolute numbers are rounded to the nearest 10. Totals may not equal the sum of individual columns. Methodology assumes that all agricultural land was converted into urban land uses and no street trees were removed on those urban lands.

Stationary Sources

Stationary source emissions result from fuel use, such as natural gas or propane, at major industrial facilities in the community permitted by state and regional air quality authorities. In Ontario, stationary sources include an industrial gas supplier, a paper and pulp mill, and Southern California Edison's Mira Loma Peaker power plant. The natural gas use reported as part of these facilities' fuel use may be included in the nonresidential natural gas use reported above in **Table 8**. **Table 15** shows the emissions from stationary sources in Ontario. Per capita emissions from stationary sources decreased by 30 percent between 2008 and 2019. There is no activity data with this subsector, and these emissions are presented for informational purposes only; they are not included in the community total.

Table 15 Stationary Source GHG Emissions, 2008 to 2019

Sector	2008	2016	2019	Percentage Change, 2008–2019
Emissions (MTCO_{2e} per capita)				
Stationary sources	1.50	0.25	1.05	-30%

Community-Wide Forecast

The community-wide forecast is a projection of future GHG emissions from the sources in Ontario’s GHG inventory. The community-wide forecast of GHG emissions is based on the results of the 2019 community-wide GHG emissions inventory combined with Ontario’s 2019 demographics and future demographic projections. These population, housing, and employment projections are based on the estimates of future buildout in TOP 2050. **Table 16** shows the demographic projections used to prepare the community-wide GHG emissions forecast.

Table 16 City of Ontario Community-Wide Demographic Projection, 2019 to 2050

Metric	2019	2030	2050	Percentage Change 2019–2050	Relevant Sectors
Population	178,610	232,120	410,490	130%	Off-road equipment
Households	48,280	66,680	124,380	158%	Residential energy, off-road equipment
Residents per household	3.70	3.48	3.30	-11%	None
Jobs	128,640	169,600	296,000	130%	Nonresidential energy, off-road equipment
Service population	307,240	401,720	706,500	130%	Solid, water and wastewater, off-road equipment
Vehicle Miles Traveled (VMT)	2,151,423,590	2,631,468,770	3,504,276,490	63%	Transportation

Sources: 2019 demographic numbers are from US Census, the Department of Finance, and San Bernardino County Transportation Authority (SBCTA).

Future demographics are from TOP 2050 buildout projections. Vehicle miles traveled are derived from Fehr and Peers (2019).

Service population is the sum of populations and jobs. All numbers except residents per household are rounded to the nearest 10.

3. Ontario's Greenhouse Gas Emissions

In addition to demographic and economic changes forecasted in TOP 2050, land use changes are expected to impact activity and emissions within the city limits. Although the city's total land area will not change, intensification of land uses in 2050 are listed as follows:

- All agricultural land is projected to be developed with urban uses by 2050, reducing emissions from the city's agricultural sector to zero emissions in 2050.
- Infill development in select areas will intensify commercial and mixed uses in Downtown, Toyota Arena, and Ontario Mills Mall.
- Select commercial and business park uses will be converted to mixed uses, which may contribute to reduced VMT.
- Residential development will intensify to promote higher density housing, especially in Ontario Ranch and southwest Ontario.
- Mixed use and higher density residential development is planned specifically to align with future bus rapid transit services along multiple north-south and east-west corridors throughout the city, which may contribute to reduced VMT.
- Land area dedicated to public open space will increase with the development of the Great Park, a 370-acre planned linear park stretching 3.5 miles from east to west in south Ontario.

This forecast is also referred to as the business-as-usual forecast. It assumes that, for most sectors, each person in Ontario will continue to contribute the same amount of GHG emissions to the community total as they did in 2019, so the amount of GHG emissions changes proportionally to the projected change in community demographics. However, some emissions, notably those from on-road transportation, grow at a slower rate due to the nature of the modeling process. The overall community-wide absolute emissions are projected to grow 89 percent from 2019 to 2050, and Ontario's service population is projected to grow 130 percent over this period. Since the rate of population increase is greater than the rate of GHG emission increase, the per-capita emissions decrease over time. This is a worst-case projection that assumes that no one at any level takes action, including state, regional, and local agencies. This projection also accounts for emission changes from land use and carbon sequestration. [Table 17](#) and [Figure 4](#) show Ontario's forecast community-wide GHG emissions through 2050.

Ontario's residential population and worker population are both projected to grow by 130 percent between 2019 and 2050. During the same period, the city's per capita emissions are expected to decrease from 5.54 MTCO₂e to 4.56 MTCO₂e per person in 2050, a decrease of 18 percent (see [Table 17](#)).

The forecast assumes emissions scale proportionally to demographic growth, which means most of the sectors grow at a similar rate to the growth in demographics, as seen by comparing the percentage changes in [Table 17](#).

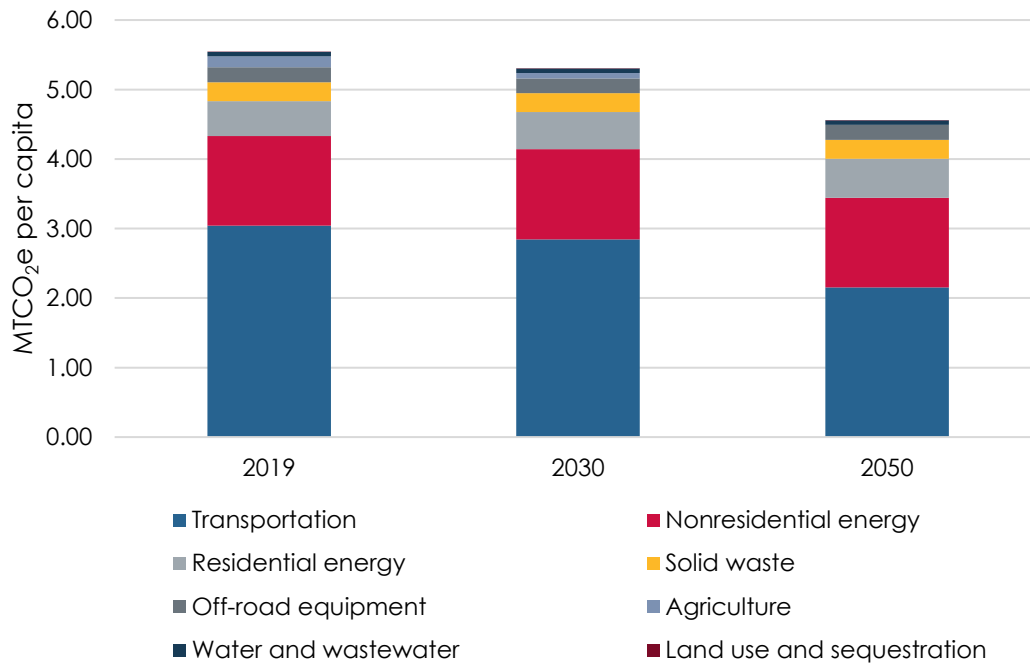
Table 17 Forecast Per Capita GHG Emissions, 2019–2050

Sector	2019 ¹	2030	2050	Percentage Change, 2019–2050
Transportation	3.04	2.84	2.15	-29%
Nonresidential energy	1.29	1.30	1.29	Less than 1%
Residential energy	0.50	0.53	0.57	12%
Solid waste	0.27	0.27	0.27	Less than 1%
Off-road equipment	0.21	0.21	0.21	Less than 1%
Agriculture ²	0.16	0.08	0.00	-100%
Water and wastewater	0.06	0.06	0.06	Less than 1%
Land use and sequestration	Less than 0.01	Less than 0.01	Less than 0.01	-15%
Total	5.54	5.30	4.56	-18%
<i>Stationary Sources³</i>	1.05	-	-	-

1. Data shown for 2019 are the actual per capita GHG emissions. The data shown for 2030, 2040, and 2050 are GHG per capita emission forecasts, calculated based on projections from the 2019 inventory.
2. GHG emission projects for the agriculture are based on the assumption that by 2050 all agricultural land will be developed and converted to other land uses, so agricultural emissions will decrease to zero by 2050.
3. Stationary sources are informational; they vary significantly from year to year independent of demographic changes, so they are not forecast.

3. Ontario's Greenhouse Gas Emissions

Figure 4 City of Ontario Community-Wide Per Capita GHG Emissions, 2019-2050



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4. GHG REDUCTION TARGETS

Background

Establishing GHG emission reduction targets is a key component of the CCAP to ensure that Ontario has a number to strive toward as it implements GHG reduction strategies. The strategies and actions adopted as part of the CCAP are often quantifiable to ensure that each action the City takes can be measured for its performance in advancing Ontario to the adopted GHG emission reduction goal. Jurisdictions can choose to adopt GHG emission reduction targets that match state guidance or can elect to increase reduction targets to accelerate action at a local level.

2014 CCAP GHG Reduction Targets

As part of the 2014 CCAP, the City adopted a GHG reduction target of 30 percent below the forecast emissions levels in 2020. This target is approximately equal to the state-recommended reduction target of 15 percent below 2005 – 2008 levels by 2020.

In 2019, Ontario reached its 2020 GHG reduction target of 15 percent below 2008 levels, which translates to absolute GHG emissions of 1,880,970 MTCO₂e or 7.01 MTCO₂e per capita. According to the 2019 GHG inventory conducted as part of the 2022 CCAP update, Ontario's community-wide emissions in 2019 were 1,707,380 MTCO₂e (not including the informational emissions from stationary sources), 23 percent below 2008 levels (as adjusted to be consistent with current protocols as part of this CCAP update). In per capita terms, Ontario's emissions in 2019 were 5.54 MTCO₂e, a 33 percent decrease relative to 2008 levels.

State Targets and Recommendations

As stated in “Regulatory Framework” in Chapter 2, the state has set GHG reduction targets to:

- Reduce 2030 emissions 40 percent below 1990 levels, codified into law by SB 32 in 2016.
- Achieve carbon-neutral emissions by 2045, established (and not yet codified into law) by EO B-55-18.
- Reduce 2050 emissions 80 percent below 1990 levels, established (and not yet codified into law) by EOS-03-05.

The Scoping Plan, also described in “Regulatory Framework” in Chapter 2, recommends that local governments select targets consistent with state targets, in absolute or per-capita form. Per-capita targets establish a level of GHG emissions by a certain year per person. Unlike absolute targets, the total level of GHG emission reductions specified by per-capita targets varies depending on future demographic changes. The Scoping Plan recommends the following per-capita targets:

- Reduce 2030 emissions to 6.0 MTCO_{2e} per capita
- Reduce 2050 emissions to 2.0 MTCO_{2e} per capita

These targets are based on the state targets and goals in SB 32 and Executive Order S-03-05, discussed previously. California’s statewide target of 40 percent below 1990 levels by 2030 translates to 260 million MTCO_{2e}. The state is projected to be home to approximately 44 million residents in 2030, resulting in an average of approximately 6.0 MTCO_{2e} per person. Similarly, the statewide goal of 80 percent below 1990 levels by 2050 translates to 86 million MTCO_{2e}. California is projected to have approximately 49 million residents in 2050, resulting in an average of approximately 2.0 MTCO_{2e} per person.

Since the statewide per-capita targets are based on the statewide GHG emissions inventory that includes all emissions sectors in the state, the Scoping Plan notes that it is appropriate for local jurisdictions to derive evidence-based local per-capita targets based on local emissions sectors and population projections that are consistent with the framework used to develop the statewide per-capita targets. The resulting GHG emissions trajectory should show a downward trend consistent with the statewide objectives.

2022 CCAP GHG Reduction Targets

The state-recommended per-capita targets (6.0 MTCO_{2e} per person by 2030 and 2.0 MTCO_{2e} per person by 2050) are relevant and appropriate for Ontario, and they are the targets in this 2022 CCAP. Ontario’s targets are based on the service population in the community (the sum of both residents and jobs), as shown in [Table 18](#).

Table 18 Per Capita Targets for Ontario

Target	Absolute Emissions Target		Per Capita Emissions Target
	MTCO _{2e}	Description	MTCO _{2e}
2030 target	1,175,390	40% below 1990 levels	6.0 MTCO _{2e} per person (service population)
2050 target	391,800	80% below 1990 levels	2.0 MTCO _{2e} per person (service population)

5. GREENHOUSE GAS EMISSION REDUCTION STRATEGY

Introduction

There are several ways in which GHG emissions can be reduced over time. States, counties, cities, regional agencies, nonprofit organizations, businesses, and each individual can work together or separately to run projects and programs that either reduce GHG emissions before they are released or sequester carbon once it is in the atmosphere. This chapter describes Ontario's path to reducing GHG emissions to meet or exceed its targets, including existing/planned efforts led by state, regional, and local agencies, and new strategies in this 2022 CCAP.

Calculating Credit

This 2022 CCAP uses a process called quantification to determine how much GHG emissions are reduced by each strategy. The foundation for the quantification calculations is the baseline GHG inventories and forecast. Activity data from the inventory, such as VMT or kWh, are combined with participation rates and data about the reduction in activity data from each action to calculate the GHG reduction benefit of each strategy. This approach ensures that the GHG reductions from the 2022 CCAP strategies are tied to current and future activities that are occurring in the community.

Calculations for reduction in activity data come from tools and reports provided by government agencies; these agencies include the U. S. EPA, the California Energy Commission, CARB, the California Air Pollution Control Officers Association, the U.S. Department of Energy (DOE), and local air districts. If accurate data are not available from these sources, the quantification uses case studies from comparable communities and applicable scholarly research.

The project team was able to quantify GHG reductions for most of the strategies in this 2022 CCAP. However, there are a few that do not have a specific reduction level due to missing data or the lack of a reliable method. These efforts are still expected to reduce GHG emissions, but the level cannot be accurately determined. These strategies are labeled "supportive".

Progress Toward GHG Reduction Goals

To understand the level of action necessary to achieve the City's reduction targets, this 2022 CCAP analyzes existing, planned, and future actions. By first looking at past accomplishments, the City can understand progress achieved and outstanding opportunities while looking at state initiatives, which may result in further GHG reductions on a local level. Existing and current efforts provide a

5. Greenhouse Gas Emission Reduction Strategy

foundation for this 2022 CCAP. The GHG reductions from past and current activities help inform the creation of new GHG reduction strategies to reduce GHG emissions even further. These new strategies can further close the gap between projected GHG emission levels and the reduction targets, and guide development and implementation of future programs. Together, these efforts serve as the City's multipronged strategy to achieve reduction targets.

State Initiatives to Reduce GHG Emissions

Since passing AB 32, the State has enacted regulations and programs to reduce GHG emissions. Although statewide in scope, these actions affect several sources of Ontario's emissions, so the local benefits of these State efforts can be "credited" to Ontario even in cases where the community did not need to take any action. This CCAP includes the local benefits from five State policies:

- **Renewable Portfolio Standard:** The RPS was first established in 2002 and has been amended multiple times, most recently in 2018 by SB 100. It requires all electricity providers in the state to obtain at least 33 percent of their electricity from eligible renewable resources by the end of 2030, and all their electricity from carbon-free (although not necessarily eligible renewable) resources by the end of 2045. This policy reduces GHG emission from electricity use, including electricity used to transport and process water and wastewater, and electricity used for electric vehicles.
- **Clean Car Standards:** In 2002, California adopted AB 1493, the New Passenger Motor Vehicle Greenhouse Gas Emission Standards, or Pavley standard. It required a reduction in tailpipe GHG emissions from new vehicles produced from 2009 to 2015. In 2012 CARB adopted an extension of this policy, the Advanced Clean Car Standards, which requires more stringent reductions in tailpipe GHG emissions from vehicles produced from 2016 to 2025. The Clean Car Standards (including the Advanced Clean Car Standards) reduce GHG emissions from on-road transportation vehicles, including commercial and public transit vehicles.
- **Title 24 Energy Efficiency Standards:** California Code of Regulations, Title 24, Part 6 is California's energy efficiency standards for new and renovated buildings, applied at the local level through the project review/building permit process. The standards are strengthened every three years. The most recent set of Title 24 standards went into effect on January 1, 2020, although the reductions shown from this policy include past, current, and anticipated future Title 24 standards. This policy will reduce GHG emissions from electricity and natural gas use in new and substantially renovated homes and nonresidential buildings.

- **Local Carbon Fuel Standard:** The Low Carbon Fuel Standard (LCFS) was adopted in 2009 and required a 10 percent reduction in the carbon intensity of all transportation and equipment fuels by 2020. This policy reduces GHG emissions from on-road transportation and from off-road equipment.
- **The short-lived climate pollutants law (SB 1383):** SB 1383, approved in 2016, proposes a comprehensive strategy to reduce methane and other emissions of short-lived greenhouse gases through regulations on dairy operations and urban landfills, including higher diversion rates of food waste from landfills.

Renewable Energy Emissions Reductions in 2050

As required by the state’s Renewables Portfolio Standard, all electricity sold in California must be carbon free by 2045. Consequently, future GHG reduction strategies that only reduce electricity use or increase renewable electricity supplies will show zero GHG reductions in 2050. Since there will already be no emissions from electricity use in 2050, the City cannot count additional reductions associated with electricity in that year. This 2022 CCAP already credits reductions from the RPS as an existing state program.

Under the business-as-usual forecast in [Table 17](#), per capita GHG emissions in Ontario decrease by 18 percent between 2019 and 2050. However, when state actions are accounted for, per capita GHG emissions decrease by 57 percent between 2019 to 2050. [Table 19](#) shows the per capita GHG emissions reductions resulting from state actions.

Table 19 Ontario Per Capita GHG Emission Reductions from State Actions, 2019 to 2050

Sector	2019 MTCO ₂ e	2030 MTCO ₂ e	2050 MTCO ₂ e	Percentage Change 2019–2050
Per capita emissions without state actions	5.54	5.30	4.56	-18%
Per capita emissions reductions from RPS		0.21	1.13	-
Per capita emissions reductions from Clean Car standards		0.53	0.69	-
Per capita emissions reductions from Title 24		0.19	0.28	-
Per capita emissions reductions from LCFS (off-road equipment emissions)		0.01	0.01	-
Per capita emissions from SB 1383		0.06	0.06	-
Per capita emission reductions from all state actions		1.00	2.17	-
Per capita emissions with state actions	5.54	4.31	2.39	-57%

5. Greenhouse Gas Emission Reduction Strategy

Existing Local Actions to Reduce GHG Emissions

Regional and local initiatives help to further reduce Ontario's community-wide GHG emissions.

There are various local efforts that are expected to reduce GHG emissions, including:

- Smart Ontario, an initiative to promote energy efficiency through comprehensive municipal utility infrastructure improvements.
- Municipal and residential rooftop solar installations.
- Implementation of the Active Transportation Master Plan and synchronization of traffic lights through the City's Traffic Management Center.
- Use of recycled water from Inland Empire Utilities Agency (IEUA) for irrigation, industrial uses, and agricultural uses.

As with the state-level policies, the 2022 CCAP assesses the strategies that have the most direct and apparent benefit to Ontario's GHG emissions. The City and local partners have also completed other efforts, which are not listed here, that may not directly reduce GHG emissions in a measurable way but still contribute to progress on overall climate action.

New GHG Reduction Strategies

A central goal of the 2022 CCAP is to achieve additional GHG reductions to work toward Ontario's 2030 and 2050 reduction targets, recognizing that the reduction strategies in the 2014 CCAP are insufficient to meet these reductions. To identify these additional reductions, the 2021 CCAP project team began with the GHG reduction strategies in the City's 2014 CCAP. Some of these strategies have been fully implemented and do not need to be carried forward into the 2022 CCAP. Others are still applicable and can be revised or expanded to achieve additional GHG reductions. There are also opportunities to add entirely new strategies to address new and emerging issues not covered in the 2014 CCAP.

The project team based the revised and new GHG reduction strategies on several sources, including:

- Past and recent GHG inventories and forecast.
- The existing and planned state, regional, and local accomplishments.
- GHG reduction strategies in the 2014 CCAP.
- GHG reduction strategies in the San Bernardino Regional GHG Reduction Plan.

The project team developed 28 GHG reduction strategies and 106 recommended implementation actions. These strategies include a mix of education and outreach programs to encourage GHG reduction activities, financial subsidies, and other enticements to incentivize GHG reductions, and mandates to require GHG efforts. These 28 strategies are organized into 10 categories:

1. Energy
2. Transportation
3. Off-road
4. Waste
5. Water
6. Agriculture
7. Wastewater
8. Leadership
9. Carbon Sequestration
10. Economic Development

The results of quantifying GHG reduction potential of Ontario's existing and planned local strategies show that, with successful implementation of the strategies, Ontario can meet its 2030 and 2050 emissions per capita reduction targets. Collectively, local strategies are forecast to reduce emissions by 0.54 MTCO₂e per capita by 2030 and 1.08 MTCO₂e per capita by 2050 in addition to the reductions achieved by existing State, regional, and local accomplishments.

Each strategy entry includes a description, the anticipated 2030 and 2050 GHG reductions achieved by the strategy at the projected performance level, and the recommended actions necessary to implement it. For each strategy, more details on performance metrics, assumptions, and quantification methods are provided in Technical Appendix A. Recommended actions represent the City's current understanding of best practices in achieving GHG emissions reductions and community equity, availability of technology, and local regulations as well as the current State and federal regulatory environment. City staff are encouraged to revisit these recommended actions as conditions change and new opportunities become available.

5. Greenhouse Gas Emission Reduction Strategy

Energy

Most buildings, both residential and nonresidential, use electricity and natural gas to operate appliances and other pieces of equipment. While sources of electricity have become much cleaner over time and will continue to become cleaner due to State law and utility policies, the GHG emissions associated with using a unit of natural gas have remained mostly constant because natural gas is a fossil fuel. Buildings that receive most or all their energy from electricity instead of natural gas can significantly reduce their GHG emissions. Buildings can be constructed to be mostly electric or all electric, or existing buildings can be electrified as part of retrofit activities.

Retrofits can improve energy efficiency of the building envelope, especially in older buildings. And advances in electric appliances, such as those used for space heating, water heating, and cooking, have helped improve energy efficiency and make electrification more cost effective.

When a building has optimized energy efficiency and maximized energy conservation, it is a good candidate for onsite renewable energy. Most residential buildings are candidates for rooftop solar or a rooftop photovoltaic system, which includes electricity-generating solar panels mounted on the rooftop of a residential or commercial building or structure. Larger properties or more energy intensive uses can use ground-mounted solar energy systems or a combination of renewable energy technologies to meet all or part of their energy demand.

ONTARIO SHINES PROGRAM

Ontario Shines is a project of Ontario Together whose goal is to install solar panels on up to 100 single-family homes and include solar installations at various multifamily housing sites, including the all-electric, LEED gold standard development, Vista Verde Apartments. As of March 2022, the project team, in collaboration with nonprofit partner GRID Alternatives, has successfully installed 130 kW of solar arrays at single family homes and 250 kW of solar arrays at multifamily homes as part of the Ontario Shines program.

Building electrification

Promote and incentivize the phase-out of gas appliances in new and existing homes and businesses throughout the community to advance GHG reductions, increase energy efficiency, and protect public safety and environmental health.

Strategy 1 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO ₂ e)	10,310	102,640
Per capita GHG Reduction (MTCO ₂ e per capita)	0.0257	0.1453

STRATEGY 1 ACTIONS:

- Support education and outreach to raise awareness and increase participation in electric appliance rebate programs offered by SCE, Southern California Regional Energy Network (SoCalREN), and other providers, with a focus on contractors and residents of older properties (constructed in or before 1975).
- Identify and remove any existing code, permitting, or other City requirements that provide barriers to all-electric conversions of existing homes and businesses and assess opportunities to provide incentives, such as permit streamlining or fee reductions, as feasible.
- Update the City’s permit tracking system as appropriate to track energy efficiency improvements.
- Review City-supported weatherization and energy efficiency programs and requirements, if applicable, to ensure they support all-electric, high efficiency appliances.
- Support education opportunities for contractors about the opportunities and benefits of converting homes to high efficiency, all-electric appliances.
- Audit existing City facilities on a regular basis to assess energy use and efficiency, identify energy efficiency improvements that optimize energy performance of buildings and operations, and prioritize energy efficiency improvements.

5. Greenhouse Gas Emission Reduction Strategy

STRATEGY 2 ONSITE SOLAR ENERGY FOR EXISTING RESIDENTIAL DEVELOPMENT

Continue to support and facilitate installation of rooftop solar photovoltaic and onsite solar energy systems in existing residential development.

Strategy 2 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO ₂ e)	2,040	0
Per capita GHG Reduction (MTCO ₂ e per capita)	0.0051	0

STRATEGY 2 ACTIONS:

- Conduct outreach to raise awareness about the benefits of solar energy for homes and increase installations of residential solar photovoltaic (PV) systems in Ontario.
- Establish a solar permitting web page on the City's website that summarizes requirements for installing solar PV systems to ensure the information is easily accessible to the public.
- Update City permit tracking as appropriate to track size and number of renewable energy installations.

STRATEGY 3 ONSITE SOLAR ENERGY SYSTEMS FOR NONRESIDENTIAL DEVELOPMENT

Ensure new large nonresidential development, including City facilities, includes onsite renewable energy to support the site's energy needs by requiring solar photovoltaic panels or other appropriate onsite renewable energy generation systems for the following types of projects:

- New commercial and office buildings, or existing commercial and office building expansions greater or equal to 45,000 square feet in size.
- New industrial or existing industrial buildings expansions greater or equal to 100,000 square feet in size.

Strategy 3 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO ₂ e)	26,480	0
Per capita GHG Reduction (MTCO ₂ e per capita)	0.0659	0

STRATEGY 3 ACTIONS:

- Provide and support incentives and rebates for solar PV systems for industrial uses to encourage increased local use of renewable energy.

- Conduct outreach to raise awareness about the benefits of solar energy for businesses, including industrial facilities, and increase installations of nonresidential solar PV systems in Ontario.
- Assess the feasibility of placing solar on City-owned properties.

STRATEGY 4 GREEN ROOFS

Promote and incentivize residents and business owners to install green roofs to conserve energy and reduce surface water runoff.

Strategy 4 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO _{2e})	Less than 10	Less than 10
Per capita GHG Reduction (MTCO _{2e} per capita)	Less than 10	Less than 10

STRATEGY 4 ACTIONS:

- Provide and support incentives for installation of green roofs in new and existing buildings.
- Conduct outreach to raise awareness about and increase participation in green roof rebate program.

STRATEGY 5 URBAN COOLING

Maintain and expand the City's existing tree canopy, with a goal of planting 500 trees annually through 2050 and promote the use of pervious concrete and cool pavement for pavement projects.

Strategy 5 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO _{2e})	5,610	12,730
Per capita GHG Reduction (MTCO _{2e} per capita)	0.0140	0.0180

STRATEGY 5 ACTIONS:

- Maintain and expand an urban forestry program to manage citywide tree planting, track citywide tree inventory and canopy cover, and monitor tree health to maintain and expand the City's existing tree canopy.
- Identify priority areas for tree planting, focusing on environmental justice communities, to mitigate the heat island effect in underserved neighborhoods.
- Ensure that new and retrofitted large hardscaped areas, such as parking lots, incorporate trees and other green infrastructure appropriate for current and future climate conditions.
- Explore grant funding opportunities for urban forestry, pervious concrete, and cool pavement.

5. Greenhouse Gas Emission Reduction Strategy

STRATEGY 6 ENERGY EFFICIENCY RETROFITS FOR LOW-INCOME HOUSEHOLDS

Promote and incentivize voluntary energy efficiency retrofits of homes to reduce natural gas and electricity usage, with the goal of retrofitting 9,000 low-income homes by 2050. Partner with community services agencies to fund energy efficiency projects, including heating, ventilation, air conditioning, indoor lighting, water heating equipment, insulation, and weatherization for low-income residents.

Strategy 6 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO ₂ e)	940	1,440
Per capita GHG Reduction (MTCO ₂ e per capita)	0.0023	0.0020

STRATEGY 6 ACTIONS:

- Assist San Bernardino County staff and other partners with their targeted outreach campaigns, including hosting workshops and promoting programs through City communication channels to increase awareness of the benefits of weatherization among low-income households.
- Expand and better integrate funding and support for existing programs that support weatherization and increase energy efficiency in low-income households, including through partnerships with SoCalREN and San Bernardino County.
- Engage with tenants and owners of multifamily homes and promote the SoCalREN multifamily program.

STRATEGY 7 ENERGY EFFICIENCY RETROFITS

Promote and incentivize voluntary energy efficiency retrofits to reduce in natural gas and electricity usage. Partner with regional agencies to expand access to existing energy efficiency and conservation opportunities, incentives, and technical assistance for residents and businesses.

Strategy 7 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO ₂ e)	11,570	15,600
Per capita GHG Reduction (MTCO ₂ e per capita)	0.0288	0.0221

STRATEGY 7 ACTIONS:

- Assist partners with their outreach campaigns by hosting workshops, promoting programs through City communication channels, promoting programs through interactions with homeowners and contractors, and sharing permitting data to inform targeted outreach.

- Establish a web page on the City's website that summarizes permitting requirements, available rebates, and other information about energy efficiency retrofits to ensure that information is easily available to the public.
- Support SoCalREN and San Bernardino County efforts to conduct outreach and education with local contractors to ensure they are updated on local code requirements and energy-efficient appliances and devices.
- Encourage owners of existing commercial, industrial, and residential buildings 10,000 square feet or larger to prepare an annual energy and water benchmarking report and submit it to the City, to help community awareness of building performance and identify opportunities for energy- and water-efficiency retrofits.
- If Ontario is not on track to meet GHG reduction targets by 2025, develop and implement additional energy efficiency standards for existing buildings.
- Research, consider, and implement an incentive program to encourage community members to reduce energy consumption.

**NONRESIDENTIAL
NATURAL GAS
SAVINGS**

Between 2008 and 2019, the City substantially reduced its GHG emissions from the nonresidential natural gas sector. Emissions from the nonresidential natural gas sector decreased by 63 percent, from 368,460 MTCO₂e in 2008 to 137,960 MTCO₂e in 2019.

Transportation

The private automobile has long dominated Ontario's transportation landscape. While convenient, private vehicle travel releases significant volumes of GHGs, emits air pollutants, degrades roads, and poses safety concerns for the community. Allowing for affordable, accessible, and reliable transportation options available to the Ontario community makes it easier for all residents to participate in public life and gives all community members the freedom to choose transportation modes that promote health, reduce fuel costs and time lost in traffic, and help the City meet its climate action goals. This 2022 CCAP aims to provide viable alternatives to single-occupancy vehicle trips by promoting alternative modes of transportation through high-frequency bus service and expansion of bicycle and pedestrian facilities, especially near schools, business districts, and employment centers. In addition, this CCAP aims to reduce the emissions released by passenger vehicles by promoting electric vehicle adoption through municipal fleet electrification, community-wide EV charging stations, and rebates for EV purchases.

5. Greenhouse Gas Emission Reduction Strategy

STRATEGY 8 SMART GROWTH AND INFILL

Encourage revitalization of neighborhoods through higher-density, mixed-use, infill development and creative reuse of under-utilized sites within the urban core. This measure is supportive of community-wide GHG reduction goals and does not have directly measurable GHG reductions.

Strategy 8 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO ₂ e)	None (supportive)	None (supportive)
Per capita GHG Reduction (MTCO ₂ e per capita)	None (supportive)	None (supportive)

STRATEGY 8 ACTIONS:

- Identify opportunities to increase residential density in and around the urban core.
- Examine the development review process and other zoning approval processes to reduce or eliminate regulations that increase costs for higher density development.
- Regularly identify underutilized parcels in existing infill areas to support increased housing and business opportunities consistent with the adopted Housing Element.

STRATEGY 9 TRANSIT-ORIENTED DEVELOPMENT

Encourage development of compact, mixed-use, and transit-oriented development to improve the regional jobs-housing balance, especially on corridors served by high-ridership transit and bus rapid transit (BRT), such as Holt Avenue.

Strategy 9 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO ₂ e)	330	1,440
Per capita GHG Reduction (MTCO ₂ e per capita)	0.0008	0.0020

STRATEGY 9 ACTIONS:

- Require that development within the transit-oriented development (TOD) corridor maintains and improves the mobility of people and vehicles along and across the corridor through safety considerations such as separated sidewalks and bike lanes or traffic-calming measures.
- Require development within the TOD corridor to protect and improve as needed transportation infrastructure, equitably considering the safety, comfort, and effectiveness of all transportation modes.

- Establish and maintain land use patterns in the TOD corridor that will attract and serve riders of public transit, such as higher-density mixed-use buildings near existing high-frequency transit stops.
- Revise land use and development standards in the TOD corridor as needed to remove any barriers to appropriate higher-density development.

STRATEGY 10 INCREASE TRANSPORTATION RIDERSHIP

Ensure a reliable and responsive transit system with dedicated and secure funding and resources to support increased ridership.

Strategy 10 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO _{2e})	27,980	31,450
Per capita GHG Reduction (MTCO _{2e} per capita)	0.0697	0.0445

STRATEGY 10 ACTIONS:

- Collaborate with LA Metro, Metrolink, and Omnitrans to implement “Smart Bus” technology, global positioning systems (GPS), and electronic displays at all transit stops to provide customers with “real-time” arrival and departure time information.
- Support efforts to establish dedicated bus lanes.
- Collaborate with regional transit operators on programs to increase use of the City’s public transportation system. To the extent feasible, support shorter transit passenger travel time through reduced headways and increased speed. Support efforts by regional transit operators to reduce the average fleet travel time by 5 minutes.
- In collaboration with transit agencies, support new transit routes, increased transit frequency and operating hours, and other improvements to transit quality.
- Support citywide outreach campaigns by transit agencies to promote the use of public transit, including fare-free days to encourage mode shifts.
- Coordinate with large regional employers to establish a shuttle service that connects transit centers to employment centers during commuting hours.
- Encourage employers to offer ridership programs, public transit passes, and telecommuting to employees.

5. Greenhouse Gas Emission Reduction Strategy

STRATEGY 11 TRAFFIC SIGNAL SYNCHRONIZATION AND ROADWAY MANAGEMENT

Implement traffic and roadway management strategies to improve mobility and efficiency and reduce associated emissions. This measure is supportive of community-wide GHG reduction goals and does not have directly measurable GHG reductions.

Strategy 11 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO _{2e})	None (supportive)	None (supportive)
Per capita GHG Reduction (MTCO _{2e} per capita)	None (supportive)	None (supportive)

STRATEGY 11 ACTIONS:

- Work with the Traffic Management Center to develop an annual signal synchronization program for identified priority corridors to understand changing traffic conditions, including peak hour conditions and traffic congestion patterns; update signal timing to match the specific needs of study corridors; and report on progress in travel time, average speed, and delay to measure effectiveness of the signal synchronization project.
- Work with the Traffic Management Center to ensure that signal timing is updated on a regular basis to comply with latest State requirements and accommodate pedestrian and bicycle crossing time for roadways with crosswalks and/or bicycle facilities.

STRATEGY 12 COMMUNITY VEHICLE ELECTRIFICATION

Promote and incentivize the adoption of electric vehicles (EV) citywide, including light-duty and heavy-duty vehicles, for municipal, commercial, and residential uses.

Strategy 12 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO _{2e})	62,880	465,140
Per capita GHG Reduction (MTCO _{2e} per capita)	0.1565	0.6584

STRATEGY 12 ACTIONS:

- Develop and implement a plan to transition the City’s municipal fleet to low-and zero-emission vehicles, to the greatest extent feasible.
- Collaborate with Omnitrans to encourage investment in electric buses.
- Encourage the use of electric vehicles by increasing access to availability of public EV charging stations.

- Work with SCAQMD to participate in CARB’s Proposition 1B–Goods Movement Emission Reduction Program to incentivize investment in cleaner trucks and electrification infrastructure to reduce air pollution emissions from freight activity beyond enforceable requirements.
- Incentivize the purchase of electric vehicles by providing EV rebates for Ontario residents and/or facilitating awareness rebates available from public agencies.
- Provide information about EV rebate programs and other incentives through multiple communication channels to raise awareness about and increase purchases of EVs among residents and business owners, including creating a dedicated web page on the City website with information about purchasing EVs and installing EV chargers.

STRATEGY 13 ACTIVE TRANSPORTATION NETWORKS

Work with transit agencies, school districts, and employers to facilitate an interconnected transportation system that allows a shift in travel from private passenger vehicles to alternative modes, including public transit, ride sharing, car sharing, bicycling, and walking.

Strategy 13 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO _{2e})	1,800	3,840
Per capita GHG Reduction (MTCO _{2e} per capita)	0.0045	0.0054

STRATEGY 13 ACTIONS:

- Encourage local school districts to develop school transit plans to substantially reduce automobile trips to and congestion surrounding schools.
- Increase pedestrian and bicycle facilities through implementation of the Active Transportation Master Plan, expanding the active transportation network to include 186.7 miles of bicycle facilities and pedestrian safety improvements to promote higher bicycle ridership and walking rates.
- Seek funding for active transportation infrastructure improvements.
- Implement scooter-share pilot program in Ontario Ranch in 2023.

5. Greenhouse Gas Emission Reduction Strategy

STRATEGY 14 VEHICLE IDLING

Limit idling of heavy-duty trucks. Support the SCAMQD and CARB anti-idling requirements and provide signage in key areas where idling that is not consistent with SCAMQD or CARB requirements might occur.

Strategy 14 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO _{2e})	1,950	1,780
Per capita GHG Reduction (MTCO _{2e} per capita)	0.0049	0.0025

STRATEGY 14 ACTIONS:

- Evaluate the feasibility of an idling ordinance that reduces the legally allowed idling time for heavy duty trucks (greater than 26,000 gross vehicle weight) to 3 minutes. California law currently limits idling time to 5 minutes. Ordinance must include a violation fee and enforcement mechanisms.
- Create a public awareness campaign to raise awareness about and increase compliance with idling ordinance.
- Install signage regarding new idling limit of 3 minutes in areas designated for industrial uses.

STRATEGY 15 PARKING POLICY AND EVENT PARKING

Adopt a comprehensive parking policy that encourages carpooling and the use of alternative transportation, including providing parking spaces for car-share vehicles at convenient locations accessible by public transportation.

Strategy 15 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO _{2e})	2,230	8,110
Per capita GHG Reduction (MTCO _{2e} per capita)	0.0056	0.0115

STRATEGY 15 ACTIONS:

- Establish a market pricing scheme for on-street parking in Downtown Ontario.
- Designate at least 5 percent of downtown parking spaces as loading and unloading areas for taxis, ridesharing or Transportation Network Companies, buses, and shuttles.
- Designate a portion of parking revenues that can support City-led GHG reduction projects.
- Consider establishing policies and programs to reduce onsite parking demand and promote ride-sharing during events at the Ontario Convention Center and other event venues.

Off-Road

Off-road equipment is used in many important sectors of the Ontario economy, such as construction, landscaping, warehousing and logistics, and others. The use of off-road equipment contributes to community-wide GHG emissions, air pollution, and noise-levels in Ontario. Construction equipment is also a significant source of air pollution in the heavy-duty off-road equipment sector.²⁵ By encouraging the transition to electric off-road equipment, Ontario can improve quality of life and reduce air pollution while reducing GHG emissions.

STRATEGY 16 ELECTRIFICATION OF CONSTRUCTION AND LANDSCAPING EQUIPMENT

Promote and incentivize the transition to electric construction and landscaping equipment.

Strategy 16 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO _{2e})	4,810	26,020
Per capita GHG Reduction (MTCO _{2e} per capita)	0.0120	0.0368

STRATEGY 16 ACTIONS:

- Support state and regional efforts to replace diesel-powered construction and landscaping equipment with electric equipment.
- Conduct targeted outreach to local construction and landscaping companies to raise awareness about and increase participation in available electric equipment rebate programs.

STRATEGY 17 IDLING ORDINANCE FOR CONSTRUCTION EQUIPMENT

Limit idling of heavy-duty off-road construction equipment to reduce air pollution and GHG emissions from construction activity.

Strategy 17 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO _{2e})	40	100
Per capita GHG Reduction (MTCO _{2e} per capita)	0.0001	0.0001

STRATEGY 17 ACTIONS:

- Evaluate the feasibility of an equipment idling ordinance that reduces the legally allowed idling time for heavy duty construction equipment to 3 minutes. Ordinance must include a violation fee and enforcement mechanisms.
- Conduct targeted outreach to local construction companies to raise awareness about and increase compliance with the equipment idling ordinance.

5. Greenhouse Gas Emission Reduction Strategy

Waste

Reducing GHG emissions released from community waste can be done by reducing the amount of waste that the community creates and capturing emissions released from landfills. This reduction can take a variety of forms, from reducing the amount of packing used in commercial products to reimagining alternative uses for items that might otherwise be considered garbage. The types of waste produced in Ontario include food waste, construction and demolition waste, household appliances, and single-use plastics. Strategies to reduce waste must be tailored to reflect the unique uses and composition of each of these materials. Waste reduction and reuse measures reduce emissions by reducing the amount of material that would otherwise decompose in a landfill. The emissions from decomposing waste can be captured and used for electricity, offsetting emissions from electricity use for facility operations.

STRATEGY 18 METHANE CAPTURE AT LANDFILLS

Support efforts to reduce methane emissions from regional landfills.

Strategy 18 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO _{2e})	22,410	57,370
Per capita GHG Reduction (MTCO _{2e} per capita)	0.0558	0.0812

STRATEGY 18 ACTIONS:

- Encourage efforts to coordinate with regional landfills to install methane capture technology and associated monitoring systems with a goal of increasing the methane capture rate to the highest extent feasible.
- Encourage the use of captured methane for generating electricity to offset fossil fuel energy use and reduce GHG emissions.

STRATEGY 19 WASTE DIVERSION

Exceed waste diversion goals recommended by AB 939 and CALGreen by adopting a citywide diversion target of at least 75 percent of waste.

Strategy 19 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO _{2e})	14,490	38,670
Per capita GHG Reduction (MTCO _{2e} per capita)	0.0361	0.0547

STRATEGY 19 ACTIONS:

- Establish a source-reduction program that promotes options to rethink, refuse, reduce, reuse, regenerate, recycle, and recover materials by working with regional partners.
- Establish sharing, exchange, and reuse program(s), including fix-it clinics, swap events, second-hand markets, and shop-local campaigns, by working with community and regional partners.
- Promote awareness of life-cycle emissions of products through public outreach campaigns.
- Explore options to develop and adopt an ordinance to prohibit specific types of single-use or disposable plastics, particularly for use by restaurants, caterers, and other commercial kitchens.
- Engage with businesses using food packaging to determine barriers to switching to biodegradable or compostable food packaging and work to remove those barriers.
- Provide educational outreach programs to multifamily residents, multifamily property owners, single-family homeowners, and business owners to support composting programs.
- Increase the share of Ontario residents and business owners that have green compost carts or bins by working with waste haulers.
- Provide counter-top compost bins to residents of Ontario by working with regional partners and community-based organizations.
- Work with food service facilities to understand barriers to composting and work to mitigate these concerns.
- Encourage local restaurants to partner with food rescue organizations to divert food that would otherwise be thrown away to nonprofit organizations for distribution to those in need.
- Explore options to develop and adopt an ordinance requiring residences, businesses, and City facilities to practice responsible appliance disposal for all decommissioned units, including refrigerators/freezers, window air-conditioning units, and dehumidifiers.

5. Greenhouse Gas Emission Reduction Strategy

STRATEGY 20 CONSTRUCTION AND DEMOLITION WASTE RECOVERY ORDINANCE

Increase the amount of waste recycled during construction and demolition of buildings. This measure is supportive of community-wide GHG reduction goals and does not have directly measurable GHG reductions.

Strategy 20 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO _{2e})	None (supportive)	None (supportive)
Per capita GHG Reduction (MTCO _{2e} per capita)	None (supportive)	None (supportive)

STRATEGY 20 ACTIONS:

- In compliance with AB 939 and CALGreen, implement an ordinance requiring building projects to recycle or reuse at least 50 percent of unused or leftover building materials.
- Incentivize the recycling of construction debris by working with regional partners.
- Research and consider providing financial incentives to encourage the recycling of construction debris.
- Determine how certain construction materials may be donated and reused to help those in need by working with local community-based organizations and construction companies.

Water and Wastewater

Increasing water recycling and efficiency of water use reduces emissions by reducing the amount of energy needed to process, heat, and deliver water. In addition to saving energy, water conservation and efficiency helps protect one of California’s most precious resources, and in turn helps the City of Ontario to be more resilient to drought and water shortage. Meanwhile, individual homes and businesses benefit from reduced utility costs. This CCAP also recommends strategies to increase efficiency of water and wastewater treatment processes, which can reduce the amount of electricity required to operate water treatment facilities, further reducing GHG emissions in the water and wastewater sector.

STRATEGY 21 INDOOR WATER EFFICIENCY

Encourage water-efficient retrofits of new and existing buildings by working with water providers and regional agencies.

Strategy 21 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO ₂ e)	4,580	2,540
Per capita GHG Reduction (MTCO ₂ e per capita)	0.0114	0.0036

STRATEGY 21 ACTIONS::

- Explore establishing a reach code for water efficiency standards applicable to new residential and commercial development.
- Explore a target for reduced indoor and outdoor water use in existing buildings.
- Partner with Ontario Municipal Utilities Company to encourage water conservation at municipal facilities and in the community.
- Publicize available incentives and low-cost solutions to water-efficiency retrofits in existing buildings.
- Provide incentives for water-efficient appliances, plumbing fixtures, and irrigation systems to encourage improved water efficiency.

STRATEGY 22 WATER EFFICIENT LANDSCAPES AND WATER RECYCLING

Promote drought-tolerant and fire-wise landscaping. Encourage increased use of reclaimed water for landscape irrigation, agricultural, and industrial use.

Strategy 22 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO ₂ e)	3,400	0
Per capita GHG Reduction (MTCO ₂ e per capita)	0.0085	0.0000

5. Greenhouse Gas Emission Reduction Strategy

STRATEGY 22 ACTIONS:

- Develop a native, drought-tolerant, and fire-resistant landscaping list and require new development or redevelopment to use this list in landscaping plans.
- Enforce, and update as appropriate, the Water-Efficient Landscaping Ordinance to reduce outdoor water use.
- Encourage water-efficient landscaping practices through incentives and rebates for residential landscapes.
- Reduce municipal outdoor water use through a water monitoring and management system for all City irrigation operations.

STRATEGY 23 WATER SYSTEM AND WASTEWATER OPERATIONS EFFICIENCY

Maximize efficiency at drinking water treatment, pumping, and distribution facilities, including development of off-peak-demand schedules for heavy commercial and industrial users.

Strategy 23 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO _{2e})	150	0
Per capita GHG Reduction (MTCO _{2e} per capita)	0.0004	0.0000

STRATEGY 23 ACTIONS:

- Work with the Inland Empire Utilities Agency, the local wastewater treatment provider, to upgrade and replace wastewater treatment and pumping equipment with more energy efficient equipment as feasible.
- Assess and upgrade City-owned water pumping and treatment equipment, as needed, to increase energy efficiency and save energy costs.

STRATEGY 24 METHANE CAPTURE FOR WASTEWATER TREATMENT

Work with Inland Empire Utilities Agency, the local wastewater treatment provider, to increase methane capture rate.

Strategy 24 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO _{2e})	960	2,280
Per capita GHG Reduction (MTCO _{2e} per capita)	0.0024	0.0032

STRATEGY 24 ACTIONS:

- Work with Inland Empire Utilities Agency to explore the possibility of generating electricity from captured methane to power various facilities and reduce operating costs.

Agriculture

Historically, Ontario was an agricultural community with abundant dairy farms. Dairy farms that remain operational in the city limits contribute to the release of methane, a powerful greenhouse gas with approximately 28 times the global warming potential (GWP) of carbon dioxide over a 100-year period.²⁶ This CCAP recommends strategies that involve collaboration with regional partners, agencies, and members of the agricultural community to reduce GHG emissions from the agriculture sector in Ontario.

STRATEGY 25 METHANE CAPTURE FOR DAIRY OPERATIONS

Encourage and incentivize local dairy operations to reduce methane emissions through methane capture technology.

Strategy 25 GHG Reduction Potential	2030	2050
Absolute GHG Reduction (MTCO _{2e})	3,160	0
Per capita GHG Reduction (MTCO _{2e} per capita)	0.0079	0

STRATEGY 25 ACTIONS:

- To comply with SB 1383, work with local dairies to encourage installation of methane digesters to capture methane emissions from decomposing manure.

In addition to the GHG reduction strategies that address specific sources of Ontario’s GHG emissions, this CCAP recommends strategies that help to successfully implement the CCAP but are not currently quantifiable actions. These actions are included to complement the strategies for specific sectors, including climate change education, carbon sequestration, and green jobs.

5. Greenhouse Gas Emission Reduction Strategy

Leadership

STRATEGY 26 CLIMATE CHANGE AWARENESS AND EDUCATION

Promote climate change awareness and GHG reduction community-wide through a variety of mechanisms, including support of climate change education in schools or community colleges.

STRATEGY 26 ACTIONS:

- Promote educational resources to students and parents each year and encourage community educators to incorporate clean energy and climate change discussions into their curriculum by partnering with CSU San Bernardino and school districts.
- Use City newsletters to spotlight community members, including K-12 teachers and students, who are working on climate change or sustainability and who are making a difference in our community.
- Increase energy educational resources in the Ontario School District and Ontario-Montclair School District by working with the San Bernardino County Office of Education.
- Work with nonprofits and community-based organizations to develop a list of green volunteer opportunities and skills trainings for high school students, such as community gardening, tree planting, bicycle advocacy, food recovery, and composting.
- Promote and support opportunities for Ontario community members to test appliances and equipment that support increased energy and water conservation and transitions to all-electric appliances.

TREE PLANTING IN ONTARIO

Since 2019, the City has planted 1,000 trees of five different species: tulip tree (*Liriodendron tulipifera*), pink trumpet tree (*Handroanthus impetiginosus*), London plane tree (*Platanus x hispanica*), Chinkapin oak (*Quercus muehlenbergii*), and Chinese flame tree (*Koelreuteria bipinnata*). As part of Ontario Together, 365 native oak trees were planted in disadvantaged communities in Central Ontario. Trees provide numerous social and ecological benefits to the community – they provide shade, offer wildlife habitat, help purify the air, sequester carbon, absorb water, and improve aesthetics and property values in neighborhoods and commercial areas.

Carbon Sequestration

STRATEGY 27 CARBON SEQUESTRATION

Establish a City-wide carbon sequestration project and sequestration goal of 5,000 MTCO₂e per year.

STRATEGY 27 ACTIONS:

- Organize citywide tree planting volunteer days, prioritizing public schools and neighborhoods in environmental justice areas, as an extension of the City's urban forestry program.
- Conduct public workshops to educate community members about the ecological, social, and economic benefits of trees and provide basic training in tree planting and maintenance.
- Seek grants in partnership with community-based organizations to fund the carbon sequestration project.
- Publicize the carbon sequestration project to raise awareness about the project and recruit volunteers.
- Explore opportunities to establish a carbon offset program for development projects, ensuring that offset activities occur in California, and that they occur locally or regionally to the greatest extent possible. GHG reductions from carbon offsets must be real, permanent, quantifiable, verifiable, and enforceable.

5. Greenhouse Gas Emission Reduction Strategy

Economy

STRATEGY 28 GREEN JOBS

Support green-jobs training and opportunities to create sustainable, living wage, quality employment opportunities

STRATEGY 28 ACTIONS:

- Partner with community colleges, local nonprofits, and community groups to provide green-jobs training for residents.
- Provide information about green jobs, especially to people currently or recently working in polluting or extractive industries.
- Collaborate with community-based organizations and regional partners to amend the City's economic development strategy and attract businesses to Ontario that contribute to a sustainable economy.

Summary of Total GHG Emissions Reductions

Collectively, the strategies in this chapter achieve substantial GHG reductions for the years 2030 and 2050 that amount to 0.541 MTCO_{2e} per capita and 1.092 MTCO_{2e} per capita, respectively.

Table 20 shows the reductions achieved by topic, and **Figure 5** shows these reductions relative to Ontario’s community-wide emissions. The three sectors with the greatest potential to reduce community-wide GHG emissions are the transportation sector, energy sector, and waste sector, in that order.

Table 20 Absolute and Per Capita GHG Emission Reduction Potential by Sector

Sector	2030 Emissions (MTCO _{2e})	2050 Emissions (MTCO _{2e})	2030 Emissions (MTCO _{2e} per capita)	2050 Emissions (MTCO _{2e} per capita)
Transportation	56,950	132,410	0.142	0.187
Energy	97,170	511,760	0.242	0.724
Off-road	4,850	26,120	0.012	0.037
Waste	36,900	96,040	0.092	0.136
Water	11,290	2,540	0.028	0.004
Agriculture	960	2,280	0.002	0.003
Total	208,120	771,150	0.518	1.092

In total, implementation of this CCAP is projected to reduce Ontario’s GHG emissions to 1,414,620 MTCO_{2e} by 2030 and to 926,610 MTCO_{2e} by 2050. This has the potential to reduce 2030 emissions to 49 percent below 2005 levels and reduce 2050 emissions to 83 percent below 2008 levels. The CCAP strategies provide a flexible path to reduce the community’s GHG emissions to meet the City’s adopted 2030 and 2050 emission targets and to support the State’s adopted target for 2030 and its goal to achieve carbon neutrality by 2045. Ontario’s 2030 and 2050 GHG reduction targets are 6.0 MTCO_{2e} per capita in 2030 and 2.0 MTCO_{2e} per capita in 2050, as shown in **Table 21**.

5. Greenhouse Gas Emission Reduction Strategy

Figure 5 Ontario Per Capita GHG Emissions with Reduction Strategies, 2019 to 2050

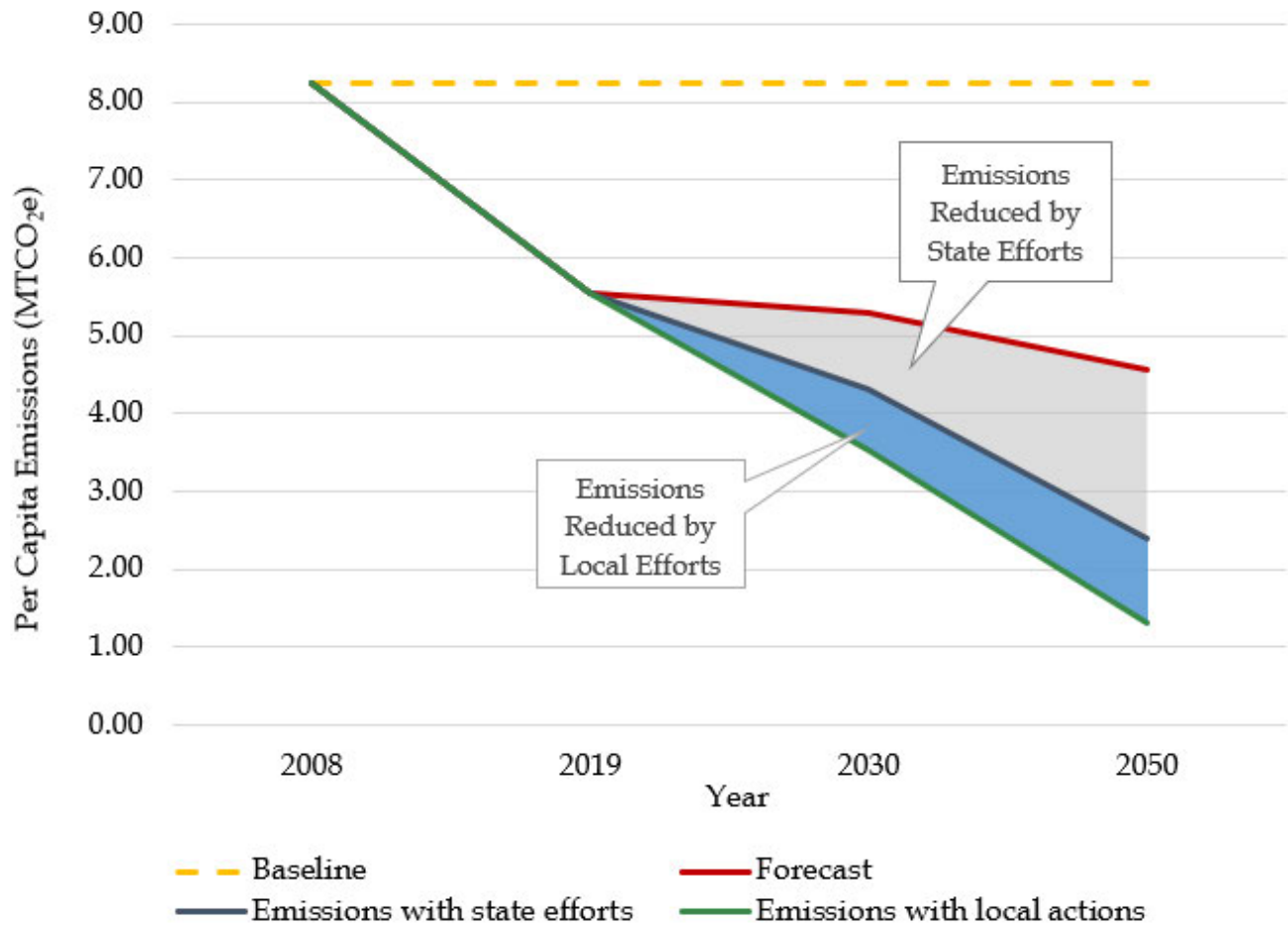


Table 21 Progress to Absolute and Per Capita GHG Emissions Targets

Target	2030 Description	2050 Description
Baseline Emissions	8.25 MTCO _{2e} per person	8.25 MTCO _{2e} per person
Target	6.0 MTCO _{2e} per person	2.0 MTCO _{2e} per person
Emissions with All Measure Reductions	3.52 MTCO _{2e} per person	1.31 MTCO _{2e} per person

6. IMPLEMENTATION

To ensure the success of the 2022 CCAP update, the City of Ontario will integrate the goals and strategies of this plan into TOP 2050 and other City plans as applicable and prioritize and implement the programs and activities in those plans. As the City moves forward with updating other planning documents, such as the municipal and zoning codes or specific plans, staff will ensure that these documents support and are consistent with the CCAP.

Implementing the 2022 CCAP will require City leadership to execute these strategies and report progress. This CCAP contains a work plan that includes responsible departments/divisions, time frames, and relative costs associated with each measure. Staff will monitor progress on an annual basis and will provide an annual update to City decision-makers.

As part of CCAP implementation, staff will apply some strategies and actions to existing or new development projects through the City's permit application and review process. A separate and forthcoming supplement to the Implementation Strategy will be screening tables for use by project applicants as part of applicable permit application processes.

The strategies in this CCAP are accompanied by a list of recommended actions selected by City staff and stakeholders. The list of recommended actions represents suggested means of achieving the measure but are not a prescriptive path to implementation. Furthermore, not all the listed actions may be necessary for the City to achieve its GHG reduction target or support Ontario's GHG reduction goals.

Due to ongoing changes in technology and regulations and the emergence of new best practices and funding opportunities, this approach enables the City to adapt and leverage new opportunities or partnerships without being constrained by a specific implementation pathway. The City's Sustainability Program Manager will serve as an ongoing advisor for CCAP implementation. As part of annual progress reports, the Sustainability Program Manager and City staff will evaluate the effectiveness of each measure to ensure that anticipated reductions are occurring. If reductions do not occur as expected, the City can modify and add additional strategies to the CCAP to ensure the reduction target is achieved.

The following strategies and associated actions are designed to guide Ontario in successfully implementing the CCAP.

Implementation Strategies

Implementation Strategy 1: Monitor and report progress toward Community Climate Action Plan target achievement on an annual basis.

- Assign responsibility for facilitating and supporting CCAP implementation to the City's Community Development Agency.
- Identify key staff from each department responsible for supporting the Sustainability Program Manager with information and updates for annual reporting and monitoring.
- Continue to involve community partners and other key stakeholders in reviewing and recommending CCAP action items.
- Prepare an annual progress report on implementation of the recommended GHG reduction strategies for review and consideration by the City Council. When information is available, provide updates on estimated GHG emissions reductions and current GHG emissions levels.

Implementation Strategy 2: Continue collaborative partnership with agencies and community groups that support Community Climate Action Plan implementation.

- Continue formal membership and participation in local and regional organizations that provide tools and support for energy efficiency, energy conservation, GHG emissions reductions, adaptation, public information, and implementation of this CCAP.
- As appropriate and at the direction of City Council, commit to formal membership through joint powers authorities or other partnerships to implement high priority strategies from the CCAP.
- Provide policy input to partner agencies (e.g., League of Cities) on policy barriers that need to be addressed at the State level.

Implementation Strategy 3: Secure necessary funding to implement the Community Climate Action Plan.

- Identify funding sources and levels for reduction strategies as part of annual reporting.
- Include emissions reduction strategies in department work plans, the capital improvement program, and other plans as appropriate.
- Pursue local, regional, State, and federal grants to support implementation.
- Explore dedicated funding sources for CCAP implementation.
- Explore opportunities to allocate a portion of revenues from revenue-generating strategies to CCAP implementation.

Implementation Strategy 4: Continue to update the baseline emissions inventory and Community Climate Action Plan every five years.

- Prepare a GHG emissions inventory no later than for the calendar year 2024 that shows GHG emissions after emergency conditions created by the COVID-19 pandemic are expected to have ended.
- Update the CCAP within five years of adoption to incorporate new technology, practices, and other options to further reduce emissions.

Implementation Strategy 5: Maintain and update the Community Climate Action Plan to allow for greater resilience.

- Coordinate updates of the CCAP, General Plan Safety Element (TOP 2050), and Local Hazard Mitigation Plan cycle to ensure plan alignment and coordination of climate mitigation and adaptation efforts.
- Assess the implementation status and effectiveness of CCAP strategies.

Work Plan

The Work Plan contains information to support staff and community implementation of the strategies to effectively integrate them into budgets, the capital improvement program, and other programs and projects. The strategies of success are defined as follows:

Strategy Number: The abbreviation that is used to refer to the strategies in the CCAP and all corresponding workbooks.

Strategy: The language used to guide actions needed for reductions.

GHG Reductions (MTCO₂e): Amount of GHG emissions reduced by 2030 and 2050.

City Staff Time: The estimated cost to the City (in staff hours) to complete implementation of the strategy, ranked as:

- Low (less than 80 hours)
- Medium (80 to 500 hours)
- High (more than 500 hours)

Time Frame: The year by which a strategy should be effective by fiscal year's end. The exact status of a strategy will vary based on its actions, and many strategies will be ongoing through and beyond 2030. An effective strategy is one that will be actively on track to achieve its targeted GHG emissions reductions and performance standards. Performance standards are the level that should

be met or exceeded to support overall achievement of the community’s GHG reduction. **Appendix A** includes the performance assumptions and standards for each quantified strategy. For a strategy to be effective, the necessary programs and efforts should be active, and any infrastructure or other capital improvements should be in place. The effective year is not the end year, as many of the strategies are programs that are intended to remain in effect for the foreseeable future, and so they do not have end dates. Time frames for effectively setting up the strategies are described as follows:

Criteria	Immediate	Short-term	Midterm	Long-term
Prioritization	Highest	High	Medium	Low
Initiation Timeline	Within one year	Within 3 years	Within 3 to 5 years (when resources are available)	Within 6+ years
Completion Timeline	As soon as possible	As soon as feasible	High priority after 3 years and completed within 8 years	When appropriate or possible

Lead Department: The lead City department tasked with implementing the strategy.

Partners: Example local organizations that the City will partner with in implementing the given policy. Additional community partners will be welcome.

Although significant GHG reduction policies and initiatives are already in place, the actions proposed in this plan, by necessity, far surpass the scale of existing efforts. Implementing the plan and ensuring that it results in real, additional GHG emissions reductions will require increased coordination across sectors and institutionalizing climate protection efforts across the community. The large number of strategies and programs recommended in this plan will take many years to implement, given limitations in both staff time and funding.

This chapter outlines the main components of the process for turning this plan into action and identifies specific actions from earlier chapters that are recommended for short-term implementation. While short-term priorities are illustrated, please note that priorities can and do shift based on funding availability, advances in technology, new and better ideas, and other reasons. The CCAP and this implementation section should be considered a living document. A summary of the implementation work plan for each strategy can be seen in **Table 22**.

Table 22 GHG Reduction Strategy Implementation Work Plan

Strategy No.	Strategy	2030 GHG Reduction (MTCO _{2e})	2050 GHG Reduction (MTCO _{2e})	2030 Per Capita GHG Reduction (MTCO _{2e} per person)	2050 Per Capita GHG Reduction (MTCO _{2e} per person)	City Staff Time	Time Frame	Lead Department(s)/ Agency	Community Partners
1	Building electrification Promote and incentivize the phase-out of gas appliances in new and existing homes and businesses throughout the community to advance GHG reductions, increase energy efficiency, and protect public safety and environmental health.	10,310	102,640	0.026	0.145	High	Short-term	Building	SCE, SoCalGas, SoCalREN, Building Industry Association of Southern California (BIASC)
2	Onsite solar energy for existing residential development Continue to support and facilitate installation of rooftop solar photovoltaic and onsite solar energy systems in existing residential development.	2,040		0.005	0.000	Medium	Short-term	Building Public Works Agency	SCE, SoCalGas, SoCalREN, Inland Valley Association of Realtors (IVAR)
3	Onsite solar energy systems for nonresidential development Ensure new large non-residential development, including City facilities, includes onsite renewable energy to support the site's energy needs by requiring solar photovoltaic panels or other appropriate onsite renewable energy generation systems for the following types of projects: <ul style="list-style-type: none"> • New commercial and office buildings, or existing commercial and office building expansions greater or equal to 45,000 square feet in size. • New industrial or existing industrial buildings expansions greater or equal to 100,000 square feet in size. 	26,480	0	0.066	0.000	Medium	Short-term	Building	SCE, SoCalGas, SoCalREN, Greater Ontario Chamber of Commerce, Ontario Hispanic Chamber of Commerce
4	Green roofs Promote and incentivize residents and business owners to install green roofs to conserve energy and reduce surface water runoff.	Less than 10	Less than 10	0.000	0.000	Low	Medium-term	Building Public Works Agency	SCE, Summertree Institute, Greater Ontario Chamber of Commerce, Ontario Hispanic Chamber of Commerce

Strategy No.	Strategy	2030 GHG Reduction (MTCO _{2e})	2050 GHG Reduction (MTCO _{2e})	2030 Per Capita GHG Reduction (MTCO _{2e} per person)	2050 Per Capita GHG Reduction (MTCO _{2e} per person)	City Staff Time	Time Frame	Lead Department(s)/ Agency	Community Partners
5	Urban cooling Maintain and expand the City's existing tree canopy, with a goal of planting 500 trees annually through 2050 and promote the use of pervious concrete and cool pavement for pavement projects.	5,610	12,730	0.014	0.018	Medium	Short-term	Landscape Planning Public Works Agency	Inland Empire Resource Conservation District (IERCD), Inland Urban Forest Council
6	Energy efficiency retrofits for low-income households Promote and incentivize voluntary energy efficiency retrofits of homes to reduce natural gas and electricity usage, with the goal of retrofitting 9,000 low-income homes by 2050. Partner with community services agencies to fund energy efficiency projects, including heating, ventilation, air conditioning, indoor lighting, water heating equipment, insulation, and weatherization for low-income residents.	940	1,440	0.002	0.002	High	Short-term	Housing Building	SCE, SoCalGas, SoCalREN
7	Energy efficiency retrofits Promote and incentivize voluntary energy efficiency retrofits to reduce in natural gas and electricity usage. Partner with regional agencies to expand access to existing energy efficiency and conservation opportunities, incentives, and technical assistance for residents and businesses.	11,570	15,600	0.029	0.022	Medium	Short-term	Building	SCE, SoCalGas, SoCalREN
8	Smart growth and infill Encourage revitalization of neighborhoods through higher-density, mixed-use, infill development and creative reuse of under-utilized sites within the urban core.	-	-	-	-	High	Short-term	Planning Housing Economic Development	Downtown Community Benefit District, Healthy Ontario, Ontario Together
9	Transit-oriented development Encourage development of compact, mixed-use, and transit-oriented development to improve the regional jobs-housing balance, especially on corridors served by high-ridership transit and bus rapid transit, such as Holt Avenue.	330	1,440	0.001	0.002	Low	Short-term	Planning Transportation Engineering	Omnitrans, SBCTA
10	Increase transportation ridership Ensure a reliable and responsive transit system with dedicated and secure funding and resources to support increased ridership.	27,980	31,450	0.070	0.045	Medium	Long-term	Planning Transportation Engineering	Omnitrans, SBCTA
11	Traffic signal synchronization and roadway management	-	-	-	-	Medium	Medium-term	Transportation	SCAQMD, SBCTA

Strategy No.	Strategy	2030 GHG Reduction (MTCO _{2e})	2050 GHG Reduction (MTCO _{2e})	2030 Per Capita GHG Reduction (MTCO _{2e} per person)	2050 Per Capita GHG Reduction (MTCO _{2e} per person)	City Staff Time	Time Frame	Lead Department(s)/ Agency	Community Partners
	Implement traffic and roadway management strategies to improve mobility and efficiency and reduce associated emissions.							Engineering	
12	Community vehicle electrification Promote and incentivize the adoption of electric vehicles (EV) citywide, including light-duty and heavy-duty vehicles, for municipal, commercial, and residential uses.	62,880	465,140	0.157	0.658	Low	Medium-term	Public Works Agency Planning	SCAQMD
13	Active Transportation Networks Work with transit agencies, school districts, and employers to facilitate an interconnected transportation system that allows a shift in travel from private passenger vehicles to alternative modes, including public transit, ride sharing, car sharing, bicycling, and walking.	1,800	3,840	0.0045	0.0054	Medium	Medium-term	Transportation Engineering	SCAQMD SBCTA
14	Vehicle idling Limit idling of heavy-duty trucks. Support the SCAMQD and CARB anti-idling requirements and provide signage in key areas where idling that is not consistent with SCAMQD or CARB requirements might occur.	1,950	1,780	0.005	0.003	Low	Short-term	Public Works Agency Planning	SCAQMD
15	Parking policy and event parking Adopt a comprehensive parking policy that encourages carpooling and the use of alternative transportation, including providing parking spaces for car-share vehicles at convenient locations accessible by public transportation.	2,230	8,110	0.006	0.011	Low	Medium-term	Planning Economic Development	Greater Ontario Chamber of Commerce, Ontario Hispanic Chamber of Commerce, Ontario Convention Center, Toyota Arena
16	Electrification of construction and landscaping equipment Promote and incentivize the transition to electric construction and landscaping equipment.	4,810	26,020	0.012	0.037	Low	Long-term	Planning Public Works Agency	SCAQMD, Building Industry Association of Southern California (BIASC)
17	Idling ordinance for construction equipment Limit idling of heavy-duty off-road construction equipment to reduce air pollution and GHG emissions from construction activity.	40	100	0.000	0.000	Low	Medium-term	Planning Building	SCAQMD, Building Industry Association of Southern California (BIASC)
18	Methane capture at landfills Support efforts to reduce methane emissions from regional landfills.	22,410	57,370	0.056	0.081	Short	Medium-term	Public Works	Barstow Landfill and Rialto Landfill
19	Waste diversion Exceed waste diversion goals recommended by	14,490	38,670	0.036	0.055	Medium	Short-term	Integrated Waste	San Bernardino County Waste Management Division

Strategy No.	Strategy	2030 GHG Reduction (MTCO _{2e})	2050 GHG Reduction (MTCO _{2e})	2030 Per Capita GHG Reduction (MTCO _{2e} per person)	2050 Per Capita GHG Reduction (MTCO _{2e} per person)	City Staff Time	Time Frame	Lead Department(s)/ Agency	Community Partners
	AB 939 and CALGreen by adopting a citywide diversion target of at least 75 percent of waste.								
20	Construction and Demolition Waste Recovery Ordinance Increase the amount of waste recycled during construction and demolition of buildings.	-	-	-	-	Low	Medium-term	Integrated Waste	Building Industry Association of Southern California (BIASC), Integrated Waste Management
21	Indoor water efficiency Encourage water-efficient retrofits of new and existing buildings by working with water providers and regional agencies.	4,580	2,540	0.011	0.004	Medium	Medium-term	Ontario Municipal Utilities Company Public Works Agency	San Bernardino County Water Resources Division, Inland Empire Clean Water Partnership, Building Industry Association of Southern California (BIASC), SCE Annual Water Conference
22	Water efficient landscapes and water recycling Promote drought-tolerant and fire-wise landscaping. Encourage increased use of reclaimed water for landscape irrigation, agricultural, and industrial use.	3,400	0	0.008	0.000	Low	Medium-term	Landscape Planning Public Works Agency	Summertree Institute
23	Water system and wastewater operations efficiency Maximize efficiency at drinking water treatment, pumping, and distribution facilities, including development of off-peak-demand schedules for heavy commercial and industrial users.	150	0	0	0	Low	Long-term	Ontario Municipal Utilities Company Inland Empire Utilities Agency (IEUA)	San Bernardino County Water Resources Division, Inland Empire Clean Water Partnership, SCE Water Conference
24	Methane capture for dairy operations Encourage and incentivize local dairy operations to reduce methane emissions through methane capture technology.	3,160	0	0.008	0	Low	Long-term	Planning	Local dairies
25	Methane capture for wastewater treatment Work with Inland Empire Utilities Agency, the local wastewater treatment provider, to increase methane capture rate.	960	2,280	0.002	0.003	Low	Long-term	Ontario Municipal Utilities Company Inland Empire Utilities Agency (IEUA)	
26	Climate change awareness and education Promote climate change awareness and GHG reduction community-wide through a variety of mechanisms, including support of climate change education in schools or community colleges.	-	-	-	-	Low	Medium-term	Management Services Agency Planning	Center for Community Action and Environmental Justice (CCA EJ), Community Climate Connection Inland Empire, Huerta del Valle Community Garden, Platt College, SJVC Ontario,

Strategy No.	Strategy	2030 GHG Reduction (MTCO _{2e})	2050 GHG Reduction (MTCO _{2e})	2030 Per Capita GHG Reduction (MTCO _{2e} per person)	2050 Per Capita GHG Reduction (MTCO _{2e} per person)	City Staff Time	Time Frame	Lead Department(s)/ Agency	Community Partners
27	Carbon sequestration Establish a City-wide carbon sequestration project and sequestration goal of 5,000 MTCO _{2e} per year.	-	-	-	-	Low	Short-term	Landscape Planning Public Works Agency	Cambridge College of Southern California Center for Community Action and Environmental Justice (CCA EJ), Community Climate Connection Inland Empire
28	Green jobs Support green job trainings and opportunities to create sustainable, living wage, quality employment opportunities.	-	-	-	-	Medium	Long-term	Economic Development	San Bernardino County Workforce Development Board, Inland Empire Small Business Development Center (IESBDC)

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7. GLOSSARY

Carbon neutrality: When carbon dioxide and other greenhouse gas (GHG) emissions generated by sources such as transportation, power plants, and industrial processes are less than or equal to the amount of carbon dioxide that is stored, both in natural sinks such as forests and mechanical sequestration such as carbon capture and sequestration. The State of California's goal is to achieve carbon neutrality no later than 2045.

Climate change: A change in the state of the climate that can be identified by changes in the mean and/or the variability of its properties, and that persists for an extended period, typically decades or longer.

Community asset: A valued feature of a community that may be harmed by climate change. Community assets may include buildings, infrastructure, community services, ecosystems, and economic drivers.

Disadvantaged communities: Areas disproportionately affected by environmental pollution and other hazards that can lead to negative public health effects, exposure, or environmental degradation, or with concentrations of people that are of low income, high unemployment, low levels of homeownership, high rent burden, sensitive populations, or low levels of educational attainment. Criteria are determined by California legislation. ^{27,28}

Equity: The state in which each individual or group is allocated the resources needed to reach an equal outcome. ²⁹

Exposure: The presence of people, infrastructure, natural systems, and economic, cultural, and social resources in areas that are subject to harm. ³⁰

Frontline communities: Term used by communities to self-identify as places that experience the impacts of issues such as environmental pollution, climate change, and the economic crisis first and most severely. These communities are most often communities of color and low income. ³¹

Hazard mitigation: Sustained action taken to reduce or eliminate the long-term risk to human life and property through actions that reduce hazard, exposure, and vulnerability. ³²

Impact: The effects (especially the negative effects) of a hazard or other conditions associated with climate change.

Resilience: The capacity of any entity – an individual, a community, an organization, or a natural system – to prepare for disruptions, to recover from shocks and stresses, and to adapt and grow from a disruptive experience. Community resilience is the ability of communities to withstand, recover, and learn from past disasters to strengthen future response and recovery efforts.

Risk: The potential for damage or loss created by the interaction of hazards with assets such as buildings, infrastructure, or natural and cultural resources.

Sensitivity: The level to which a species, natural system, or community, government, etc., would be affected by changing climate conditions.³³

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APPENDIX A. GHG TECHNICAL APPENDIX

This appendix provides details for the GHG emissions forecast and summarizes the data sources, assumptions, and performance metrics used to calculate the potential for GHG savings from the community-wide reduction strategies in the Ontario Community Climate Action Plan (CCAP).

Chapter 3 of the CCAP provides details of the GHG inventory and forecast. **Chapter 5** of the CCAP provides the full list of measures to reduce GHG emissions.

Community-Wide Forecast

The community-wide forecast is a projection of future GHG emissions from the sources in Ontario’s GHG inventory. The community-wide forecast of GHG emissions is based on the results of the 2019 community-wide GHG emissions inventory combined with Ontario’s 2019 demographics and future demographic projections. These population, housing, and employment projections are based on the estimates of future buildout in TOP 2050. **Table A-1** shows the demographic projections used to prepare the community-wide GHG emissions forecast.

Table A-1 City of Ontario Community-Wide Demographic Projection, 2019 to 2050

Metric	2019	2030	2050	Percentage Change 2019–2050	Relevant Sectors
Population	178,610	232,120	410,490	130%	Off-road equipment
Households	48,280	66,680	124,380	158%	Residential energy, off-road equipment
Residents per household	3.70	3.48	3.30	-11%	None
Jobs	128,640	169,600	296,000	130%	Nonresidential energy, off-road equipment
Service population	307,240	401,720	706,500	130%	Solid, water and wastewater, off-road equipment
Vehicle Miles Traveled (VMT)	2,151,423,590	2,631,468,770	3,504,276,490	63%	Transportation

Sources: 2019 demographic numbers are from US Census, the Department of Finance, and San Bernardino County Transportation Authority (SBCTA). Future demographics are from TOP 2050 buildout projections. Vehicle miles traveled are derived from Fehr and Peers (2019).

1. Service population is the sum of populations and jobs.
2. All numbers except residents per household are rounded to the nearest 10.

Appendix A - GHG Technical Appendix

The forecast assumes that in 2030 and 2050 each person in Ontario will continue to contribute the same amount of GHG emissions to the community total as they did in 2019, so the amount of GHG emissions changes proportionally to the projected change in community demographics. **Table A-2** shows Ontario's forecast community-wide per capita GHG emissions through 2050.

Table A-2 Forecast Per Capita GHG Emissions, 2008–2050

Sector	2008 ¹	2016 ¹	2019 ¹	2030	2050	Percentage Change, 2008–2050
Transportation	3.51	3.00	3.04	2.84	2.15	-39%
Nonresidential energy	2.77	1.80	1.29	1.30	1.29	-54%
Residential energy	0.72	0.55	0.50	0.53	0.57	-21%
Solid waste	0.28	0.22	0.27	0.27	0.27	-4%
Off-road equipment	0.17	0.08	0.21	0.21	0.21	23%
Agriculture ²	0.63	0.21	0.16	0.08	0.00	-100%
Water and wastewater	0.15	0.08	0.06	0.06	0.06	-56%
Land use and sequestration	0.01	Less than 0.01	Less than 0.01	Less than 0.01	Less than 0.01	-81%
Total	8.25	5.95	5.54	5.30	4.56	-45%

1. Data shown for 2008, 2016, 2019 are the actual per capita GHG emissions. The data shown for 2030, 2040, and 2050 are GHG per capita emission forecasts, calculated based on projections from the 2019 inventory.
2. GHG emission projects for the agriculture are based on the assumption that, by 2050, all agricultural land will be developed and converted to other land uses, so agricultural emissions will decrease to zero by 2050.

Ontario's residential population and worker population are both projected to grow by 130 percent between 2019 and 2050. During the same period, the city's forecast per capita emissions are expected to decrease from 5.54 MTCO_{2e} to 4.56 MTCO_{2e} per person in 2050, a decrease of 18 percent (see **Table A-3**).

Relative to 2008 levels, the City of Ontario's absolute community-wide GHG emissions are expected to increase by 89 percent in 2050. This projection accounts for emission changes from land use and carbon sequestration. This is a worst-case projection that assumes that no one at any level takes action, including state, regional, and local agencies. In terms of absolute emissions, the four sectors that are forecast to experience the largest increase in GHG emissions between 2019 and 2050 are the residential and nonresidential energy sectors, solid waste, and water and wastewater. The growth in emissions associated with residential and nonresidential energy use would result

the expected large increases in the city's population and number of jobs over the next three decades. Similarly, the growth in emissions forecast for solid waste and water and wastewater are due to the anticipated large increase in the city's service population – i.e., both residential population and employment.

The only sector that experiences a decrease in GHG emissions in the future is the agricultural sector, a sector that is rapidly disappearing in Ontario due to economic change, population growth, and urbanization. By 2050, all agricultural lands existing within the city limits are expected to be replaced by urban development.

The forecast assumes emissions scale proportionally to demographic growth, which means most of the sectors grow at a similar rate to the growth in demographics, as seen by comparing the percentage changes in [Table A-1](#). However, some emissions, notably those from on-road transportation, grow at a slower rate due to the nature of the modeling process. As noted above, the overall community-wide absolute emissions are projected to grow 89 percent from 2019 to 2050, and Ontario's service population is projected to grow 130 percent over this period. Since the rate of population increase is greater than the rate of GHG emission increase, the per-capita emissions decrease over time.

As the city grows, changes in activity across different sectors of the economy are also forecasted for 2030 and 2050. The two sectors that are expected to see the greatest increase in activity are residential electricity and residential natural gas. Residential electricity and natural gas use are both forecasted to increase by 158 percent between 2019 and 2050. [Table A-3](#) shows forecast community-wide activity data by subsector between 2019 and 2050.

Table A-3 Ontario Community-Wide Forecast Activity Data

Subsector	Units	2019	2030	2050	Percent Change, 2019-2050
Residential electricity	kWh	316,529,750	437,202,640	815,532,580	158%
Residential natural gas	Therms	16,945,380	23,405,590	43,659,430	158%
Nonresidential electricity	kWh	1,242,306,720	1,637,865,370	2,858,631,210	130%
Nonresidential natural gas	Therms	26,168,160	34,500,270	60,214,690	130%
Light-duty vehicles	VMT	1,945,876,580	2,380,798,400	3,171,565,080	63%
Heavy-duty vehicles	VMT	205,547,010	250,670,370	332,711,410	62%
Municipal solid waste	Tons	287,980	376,530	662,200	130%
Alternative daily cover	Tons	2,670	3,490	6,140	130%
Transform Waste	Tons	1,030	1,350	2,370	130%
Indirect water energy	kWh	61,387,950	80,264,120	141,159,360	130%
Indirect wastewater energy	kWh	7,672,160	10,031,270	17,641,850	130%
Direct wastewater plant emissions ¹	-	-	-	-	-
Enteric fermentation	Head of livestock	10,890	7,030	0	-100%
Fertilizer application	Tons applied	204	130	0	-100%
Manure management	Head of livestock	10,890	7,026	0	-100%
Off-road equipment sector ²	-	-	-	-	-
Land use change (conversion of agricultural to urban land)	Acres	3,520	1,700	1,700	22%
Street tree biomass	Acres	740	760	760	14%

All numbers except residents per household are rounded to the nearest 10.

1. There is no activity data for this sector because emissions are not correlated with volume of wastewater produced, but rather the treatment process
2. There is no activity data for this sector because emissions from off-road equipment are estimated from a model developed by the California Air Resources Board rather than from measurable metrics such as fuel consumption.

State GHG Emission Reductions

California has adopted and committed to implementing policies to decrease GHG emission levels statewide, including from several of the major GHG emission sectors present in Ontario. Many of these policies are identified in the State's Climate Change Scoping Plan (Scoping Plan), which was originally adopted in 2008 in response to the California Global Warming Solutions Act. The Scoping Plan outlines several regulatory and market-based solutions to achieve California's GHG emission reduction goals. Successive updates to the Scoping Plan in 2014 and 2017 revised these state-level actions and identified additional opportunities for GHG emission reductions, as applicable.

Though the Scoping Plan and related documents lay out several policies to reduce GHG emissions, the 2022 CCAP will focus on the policies that have the most direct and apparent benefits to Ontario. The PlaceWorks team assessed these efforts and their community-wide local GHG emission-reduction benefits to Ontario, allowing the community to receive "credit" for the state's efforts. These efforts are:

- The Renewables Portfolio Standard (RPS) requires increases in renewable electricity supplies.
- The Clean Car Standards require increased fuel efficiency of on-road vehicles and decreased carbon intensity of vehicle fuels.
- The updated Title 24 Building Energy Efficiency Standards require new buildings to achieve increased energy efficiency targets.
- The Low Carbon Fuel Standard (LCFS) mandates reduced carbon intensity of fuels used in off-road equipment.
- The short-lived climate pollutants law (SB 1383) proposes a comprehensive strategy to reduce methane and other emissions of short-lived greenhouse gases through regulations on dairy operations and urban landfills, including higher diversion rates of food waste from landfills.

State level actions have a significant impact on reduction of GHG emissions when analyzed on a per-capita basis. [Table A-4](#) shows the per capita GHG emissions resulting from state actions, both community wide and by sector. Under the forecast presented above, per capita GHG emissions decrease by 18 percent from 2019 to 2050. However, when state actions are accounted for, per capita GHG emissions decrease by 57 percent from 2019 to 2050.

Table A-4 Ontario Per Capita GHG Emission Reductions from State Actions, 2019 to 2050

Description	2019 MTCO _{2e}	2030 MTCO _{2e}	2050 MTCO _{2e}	Percentage Change 2019–2050
Per capita emissions without state actions	5.54	5.30	4.56	-18%
Per capita emissions reductions from RPS		0.21	1.13	-
Per capita emissions reductions from Clean Car standards		0.53	0.69	-
Per capita emissions reductions from Title 24		0.19	0.28	-
Per capita emissions reductions from LCFS (off-road only)		0.01	0.01	-
Per capita emissions from SB 1383		0.06	0.06	-
Per capita emission reductions from all state actions		1.00	2.17	-
Per capita emissions with state actions	5.54	4.31	2.39	-57%

Technical Data for Existing and Planned Activities

Quantification of existing and planned activities are calculations that determine the GHG emission reduction potential of strategies recommended in the CCAP. They are based on four primary types of data and research:

The inventory and forecast of San Carlos' GHG emissions, as discussed above, and summarized in Chapter 3 of the CCAP.

- The San Bernardino County GHG Reduction Tool
- California Air Pollution Control Officers Association's (CAPCOA) Handbook for Analyzing GHG Reductions, Assessing Climate Vulnerabilities, and Advancing Health and Equity
- Peer-reviewed reports and other credible research documents.
- Case studies for similar jurisdictions.

The community-wide inventory and forecast serve as the foundation for the quantification of Ontario's GHG reduction strategies, supplemented with additional data types and research. The inventory and forecast provide activity data, such as kWh of electricity used, or VMT. The project team combined the activity data with data sources that identify the potential for reduction and performance targets that estimate the level of implementation. This provides the total savings in activity data for the years 2030 and 2050. The project team then multiplied the activity data savings by the relevant emissions factor to determine the total GHG savings from the strategy.

This section discusses the data sources, methods, and assumptions for the quantification of the existing and planned local activities. Only some existing and planned local activities are quantified. Other activities, not included here, may have GHG reduction benefits but City staff cannot accurately determine the level of reductions.

Table A-5 shows the emissions factors used in the quantification. These emission factors account for the GHG reductions from existing state actions through 2050, such as RPS. Note that the quantification for many strategies, especially those that related to electricity, will use multiple emissions factors. Some sectors do not have specific emission factors, including direct wastewater and off-road emissions, and were calculated using formulae or models.

Table A-5 Emissions Factors with State Reductions

Sector	Unit	2008	2016	2019	2030	2050	Source
SCE electricity	MTCO ₂ e/ kWh	0.000288	0.000219	0.000208	0.000149	0.000000	SCE
Natural gas	MTCO ₂ e/ therm	0.053816	0.007341	0.005272	0.005272	0.005272	US Community Protocol
On-road transportation (light and medium-duty vehicles)	MTCO ₂ e/ mile	0.000421	0.000363	0.000348	0.000277	0.000253	California Air Resources Board
On-road transportation (heavy duty vehicles)	MTCO ₂ e/ mile	0.000979	0.001248	0.001256	0.001070	0.000734	California Air Resources Board
On-road transportation (total)	MTCO ₂ e/ mile	0.001400	0.001611	0.001604	0.001348	0.001400	California Air Resources Board
Solid waste (municipal solid waste)	MTCO ₂ e/ ton	0.473978	0.532831	0.286062	0.234502	0.473978	CalRecycle
Solid waste (alternative daily cover)	MTCO ₂ e/ ton	-	-	0.247191	0.246418	-	CalRecycle

Existing Activities

Activity 1 Solar PV capacity

GHG Reduction (MTCO _{2e})		Per capita GHG reduction (MTCO _{2e} per person)		Assumptions			Performance Standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
310	0	0.0008	0	Residential installations	35	35	Total residential solar capacity (kW)	3,130	3,130
				Nonresidential installations	2	2	Total nonresidential solar capacity (kW)	2,571,130	2,571,130

Sources:

National Renewable Energy Laboratory (NREL). (n.d.). Monthly PV performance data: Ontario, California. PV watts calculator.

<https://pvwatts.nrel.gov/pvwatts.php>

California Energy Commission (CEC). (2019). Power content label 2019: Southern California Edison (SCE). CEC.

<https://www.energy.ca.gov/filebrowser/download/3265>.

California Energy Commission (CEC). (2022.) California Distributed Generation Statistics. <https://www.californiadgstats.ca.gov/>

National Conference of State Legislatures (NCSL). (2021, August 13). State renewable portfolio standards and goals. NCSL.

<https://www.ncsl.org/research/energy/renewable-portfolio-standards.aspx>

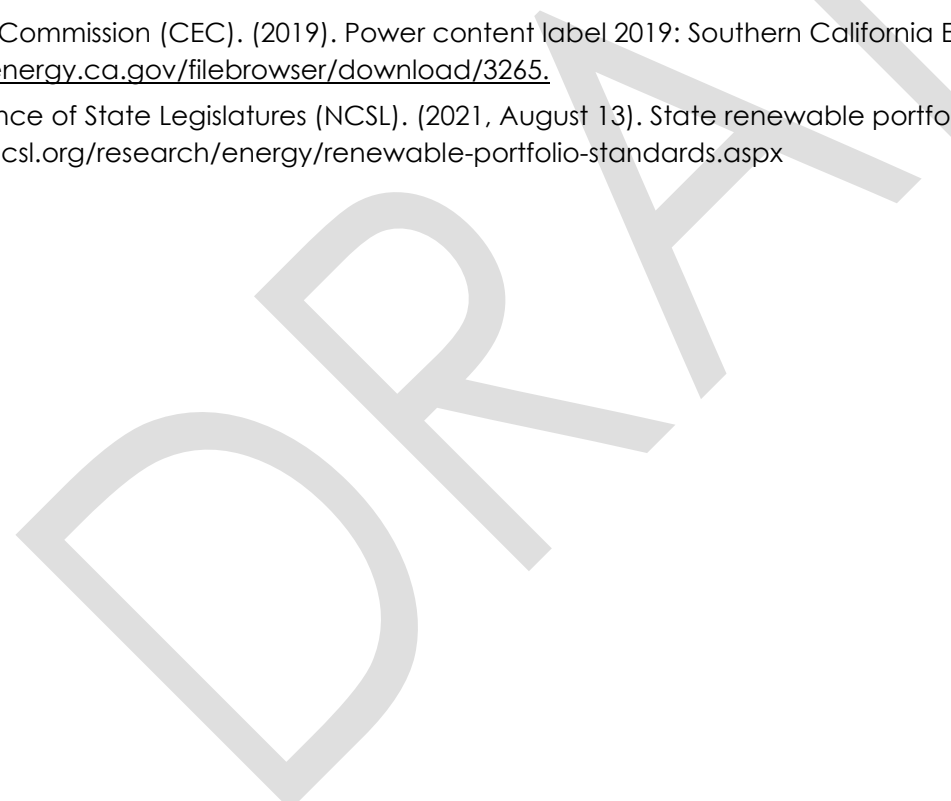
Activity 2 All electric and zero net energy buildings

GHG Reduction (MTCO ₂ e)		Per capita GHG reduction (MTCO ₂ e per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
90	110	0.0002	0.0002	There are no assumptions for this existing action			Number of all-electric residential units	100	100

Sources:

California Energy Commission (CEC). (2019). Power content label 2019: Southern California Edison (SCE). CEC.
<https://www.energy.ca.gov/filebrowser/download/3265>.

National Conference of State Legislatures (NCSL). (2021, August 13). State renewable portfolio standards and goals. NCSL.
<https://www.ncsl.org/research/energy/renewable-portfolio-standards.aspx>



Activity 3 EV charging stations

GHG Reduction (MTCO _{2e})		Per capita GHG reduction (MTCO _{2e} per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
120	140	0.0003	0.0002	Number of EV charging stations	30	30	Number of EV charging stations	30	30

Sources:

California Energy Commission (CEC). (2019). Power content label 2019: Southern California Edison (SCE). CEC.
<https://www.energy.ca.gov/filebrowser/download/3265>.

National Conference of State Legislatures (NCSL). (2021, August 13). State renewable portfolio standards and goals. NCSL.
<https://www.ncsl.org/research/energy/renewable-portfolio-standards.aspx>.

Hermann, P. & Hayes, J. (2022). Draft memorandum: The Ontario Plan Transportation Impact Assessment-Vehicle Miles Travelled (VMT). Fehr and Peers.

Greenblatt, J.B. (2015, March). Modeling California policy impacts on greenhouse gas emissions. Berkeley lab.
<https://eta.lbl.gov/publications/modeling-california-policy-impacts>.

Activity 4 G Street crosstown bicycle route

GHG Reduction (MTCO ₂ e)		Per capita GHG reduction (MTCO ₂ e per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
30	30	0.0001	0	Percent of community-wide light-duty VMT on G St roadway segment	4%	4%	Miles of bike lanes on G Street	18	18

Sources:

City of Ontario. (2015, June 17). G street crosstown bike route and San Antonio avenue bike corridor. City of Ontario.

<https://ceqanet.opr.ca.gov/2015068332>.

California Air Pollution Control Officers Association (CAPCOA). (2021). Handbook for analyzing greenhouse gas emissions reductions, assessing climate vulnerabilities, and advancing health and equity. CAPCOA.

https://www.airquality.org/ClimateChange/Documents/Handbook%20Public%20Draft_2021-Aug.pdf

California Department of Transportation (Caltrans). (2018). California public road data. Caltrans. <https://dot.ca.gov/-/media/dot-media/programs/research-innovation-system-information/documents/california-public-road-data/prd-2018-a11y.pdf>.

Hermann, P. & Hayes, J. (2022). Draft memorandum: The Ontario Plan Transportation Impact Assessment-Vehicle Miles Travelled (VMT). Fehr and Peers.

Appendix A - GHG Technical Appendix

Activity 5 Outdoor lighting

GHG Reduction (MTCO _{2e})		Per capita GHG reduction (MTCO _{2e} per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
300	0	0.0007	0	Percentage of streetlights retrofitted since 2019	65%	65%	Number of retrofitted streetlights	8,130	8,130

Sources:

San Bernardino County Transportation Authority (SBCTA). (2021). GHG reduction tool, San Bernardino County GHG reduction plan.

SBCTA. <https://www.gosbcta.com/plan/regional-greenhouse-gas-reduction-plan/>.

California Energy Commission (CEC). (2019). Power content label 2019: Southern California Edison (SCE). CEC.

<https://www.energy.ca.gov/filebrowser/download/3265>.

National Conference of State Legislatures (NCSL). (2021, August 13). State renewable portfolio standards and goals. NCSL.

<https://www.ncsl.org/research/energy/renewable-portfolio-standards.aspx>.

Activity 6 Energy efficiency retrofits of municipal buildings

GHG Reduction (MTCO _{2e})		Per capita GHG reduction (MTCO _{2e} per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
660	0	0.0016	0	There are no assumptions for this existing action			Estimated annual energy savings from retrofits in 2020 (kWh)	5,571,640	5,571,640

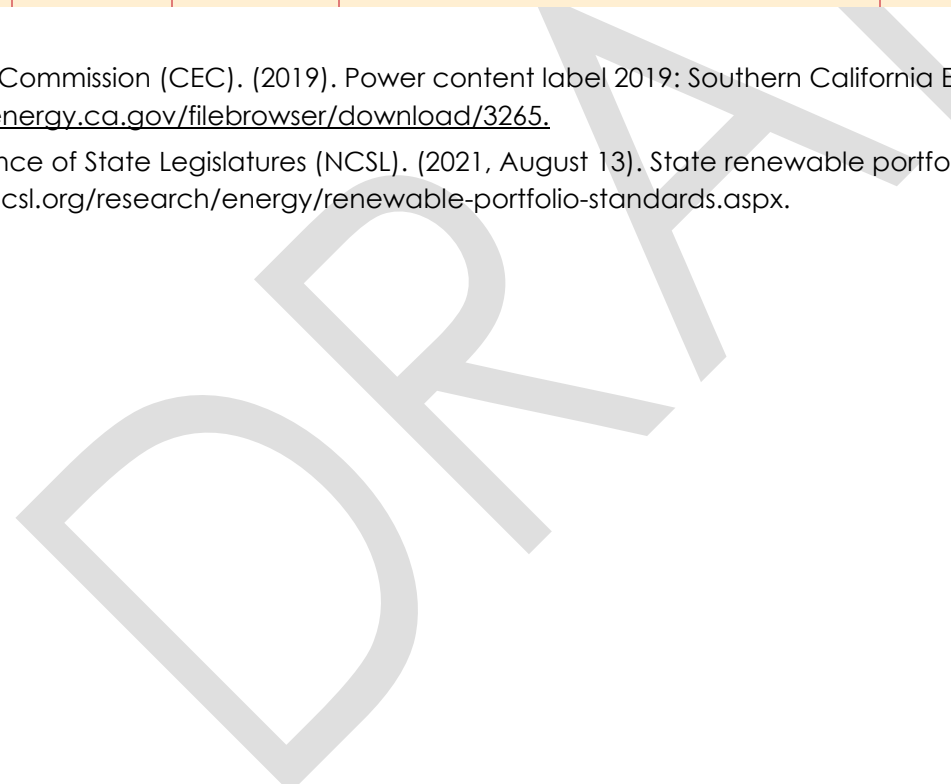
Sources:

California Energy Commission (CEC). (2019). Power content label 2019: Southern California Edison (SCE). CEC.

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National Conference of State Legislatures (NCSL). (2021, August 13). State renewable portfolio standards and goals. NCSL.

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Activity 7 Tree planting

GHG Reduction (MTCO _{2e})		Per capita GHG reduction (MTCO _{2e} per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
1,480	1,480	0.0037	0.0021	There are no assumptions for this existing action			Number of trees planted since 2019	1,000	1,000

Sources:

California Air Pollution Control Officers Association (CAPCOA). (2021). Handbook for analyzing greenhouse gas emissions reductions, assessing climate vulnerabilities, and advancing health and equity. CAPCOA.

https://www.airquality.org/ClimateChange/Documents/Handbook%20Public%20Draft_2021-Aug.pdf.

USDA Forest Service. (2006). I-Tree planting calculator. I-Tree. <https://planting.itreetools.org/>.

California Energy Commission (CEC). (2019). Power content label 2019: Southern California Edison (SCE). CEC.

<https://www.energy.ca.gov/filebrowser/download/3265>.

National Conference of State Legislatures (NCSL). (2021, August 13). State renewable portfolio standards and goals. NCSL.

<https://www.ncsl.org/research/energy/renewable-portfolio-standards.aspx>.

Note:

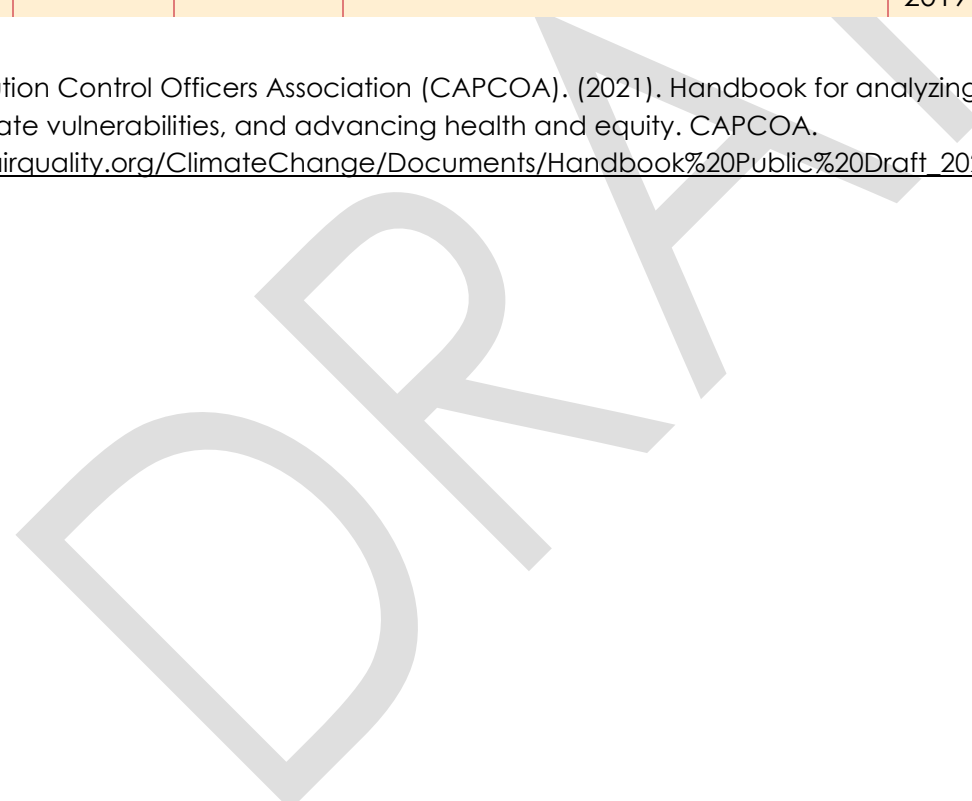
The City currently has plans to plant significantly more than 1,000 trees per year by 2050. The quantification and assumptions for this activity are a minimum threshold and can be exceeded as City and community resources allow.

Activity 8 Installation of efficient industrial water boilers

GHG Reduction (MTCO _{2e})		Per capita GHG reduction (MTCO _{2e} per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
180	180	0.0004	0.0003	There are no assumptions for this existing action			Number of industrial boilers replaced since 2019	6	6

Sources:

California Air Pollution Control Officers Association (CAPCOA). (2021). Handbook for analyzing greenhouse gas emissions reductions, assessing climate vulnerabilities, and advancing health and equity. CAPCOA.
https://www.airquality.org/ClimateChange/Documents/Handbook%20Public%20Draft_2021-Aug.pdf.



Appendix A - GHG Technical Appendix

Activity 9 Increased supply of electricity from renewable sources

GHG Reduction (MTCO _{2e})		Per capita GHG reduction (MTCO _{2e} per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
56,140	0	0.1398	0	There are no assumptions for this existing action			SCE GHG emissions factor (MTCO _{2e} /kWh)	0.000119	0.000000

Sources:

Combs, J. S. and Karlstand, C. A. (2020). SCE 2020 Integrated Resource Plan. Southern California Edison (SCE). <https://docs.cpuc.ca.gov/PublishedDocs/Efile/G000/M346/K291/346291781.PDF>.

California Energy Commission (CEC). (2019). Power content label 2019: Southern California Edison (SCE). CEC. <https://www.energy.ca.gov/filebrowser/download/3265>.

National Conference of State Legislatures (NCSL). (2021, August 13). State renewable portfolio standards and goals. NCSL. <https://www.ncsl.org/research/energy/renewable-portfolio-standards.aspx>

Planned Activities

Strategy 1 Building electrification

GHG Reduction (MTCO ₂ e)		Per capita GHG reduction (MTCO ₂ e per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
10,310	102,640	0.0257	0.1453	Penetration rate for electrification of existing homes	10%	25%	New all-electric homes	5,520	45,660
				Penetration rate for electrification of existing non-residential buildings	5%	12%	Existing homes retrofitted to be all-electric	4,830	12,070
				Penetration rate for electrification of new homes	30%	60%	New all-electric square footage for nonresidential buildings	8,166,530	49,941,430
				Penetration rate for electrification of new non-residential buildings	25%	45%	Existing nonresidential square footage retrofitted to be all-electric	9,158,850	21,981,230

Sources:

San Bernardino County Transportation Authority (SBCTA). (2021). GHG reduction tool, San Bernardino County GHG reduction plan.

SBCTA. <https://www.gosbcta.com/plan/regional-greenhouse-gas-reduction-plan/>.

California Energy Commission (CEC). (2019). Power content label 2019: Southern California Edison (SCE). CEC.

<https://www.energy.ca.gov/filebrowser/download/3265>.

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National Conference of State Legislatures (NCSL). (2021, August 13). State renewable portfolio standards and goals. NCSL.
<https://www.ncsl.org/research/energy/renewable-portfolio-standards.aspx>
PlaceWorks. (2021). TOP 2050, Buildout Projections: Demographic Forecast. PlaceWorks.

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Strategy 2 Onsite solar energy for existing residential development

GHG Reduction (MTCO _{2e})		Per capita GHG reduction (MTCO _{2e} per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
2,040	0	0.0051	0	Percent of existing homes with solar panels installed	5%	25%	Number of existing homes with solar panels installed	2,410	12,070

Sources:

San Bernardino County Transportation Authority (SBCTA). (2021). GHG reduction tool, San Bernardino County GHG reduction plan. SBCTA. <https://www.gosbcta.com/plan/regional-greenhouse-gas-reduction-plan/>.

California Energy Commission (CEC). (2019). Power content label 2019: Southern California Edison (SCE). CEC. <https://www.energy.ca.gov/filebrowser/download/3265>.

National Conference of State Legislatures (NCSL). (2021, August 13). State renewable portfolio standards and goals. NCSL. <https://www.ncsl.org/research/energy/renewable-portfolio-standards.aspx>

National Renewable Energy Laboratory. 2021. "PVWatts." <https://pvwatts.nrel.gov/>.

U.S. Environmental Protection Agency. 2021. "Emissions & generation resource integrated database (eGRID)." <https://www.epa.gov/eGRID>.

Appendix A - GHG Technical Appendix

Strategy 3 Onsite solar energy systems for nonresidential development

GHG Reduction (MTCO ₂ e)		Per capita GHG reduction (MTCO ₂ e per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
26,480	0	0.0659	0	Target percentage of new industrial roof space with electric needs met by on-site solar	90%	90%	kW of solar panels on new industrial space	66,660	45,660
				Target percentage of existing industrial roof space with electric needs met by on-site solar	10%	20%	kW of solar panels on existing industrial space	52,290	12,070
				Target percentage of new nonindustrial nonresidential development with electricity needs met by on-site solar	15%	30%	kW of solar panels on new nonindustrial nonresidential space	4,520	49,941,430
				Target percentage of existing nonindustrial nonresidential development with electricity needs met by on-site solar	5%	15%	kW of solar panels on existing nonindustrial nonresidential space	11,550	21,981,230

Sources:

- San Bernardino County Transportation Authority (SBCTA). (2021). GHG Reduction Tool, GHG Reduction Tool, San Bernardino County GHG Reduction Plan. SBCTA. <https://www.gosbcta.com/plan/regional-greenhouse-gas-reduction-plan/>.
- California Energy Commission (CEC). (2019). Power content label 2019: Southern California Edison (SCE). CEC. <https://www.energy.ca.gov/filebrowser/download/3265>.
- National Conference of State Legislatures (NCSL). (2021, August 13). State renewable portfolio standards and goals. NCSL. <https://www.ncsl.org/research/energy/renewable-portfolio-standards.aspx>
- National Renewable Energy Laboratory (NREL). (2021). "PVWatts." <https://pvwatts.nrel.gov/>.
- U.S. Environmental Protection Agency (EPA). (2021). "Emissions & generation resource integrated database (eGRID)." <https://www.epa.gov/egrid>.
- San Bernardino Council of Governments (SBCOG). (n.d.) "SBCOG analysis: Warehouse data." SBCOG.

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Strategy 4 Green roofs

GHG Reduction (MTCO _{2e})		Per capita GHG reduction (MTCO _{2e} per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
Less than 10	Less than 10	0	0	Target area of residential green roofs installed in Ontario (square feet)	25,000	50,000	Installed square footage of residential green roofs in Ontario	25,000	50,000
				Target area of commercial green roofs installed in Ontario (square feet)	50,000	100,000	Installed square footage of commercial green roofs in Ontario	50,000	100,000

Sources:

San Bernardino County Transportation Authority (SBCTA). (2021). GHG reduction tool, San Bernardino County GHG reduction plan. SBCTA. <https://www.gosbcta.com/plan/regional-greenhouse-gas-reduction-plan/>.

California Energy Commission (CEC). (2019). Power content label 2019: Southern California Edison (SCE). CEC. <https://www.energy.ca.gov/filebrowser/download/3265>.

National Conference of State Legislatures (NCSL). (2021, August 13). State renewable portfolio standards and goals. NCSL. <https://www.ncsl.org/research/energy/renewable-portfolio-standards.aspx>

Strategy 5 Urban cooling

GHG Reduction (MTCO _{2e})		Per capita GHG reduction (MTCO _{2e} per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
5,610	12,730	0.0140	0.0180	Total number of trees planted with urban forestry program	4,000	10,000	Number of trees planted	4,000	10,000

Sources:

California Air Pollution Control Officers Association (CAPCOA). (2021). Handbook for analyzing greenhouse gas emissions reductions, assessing climate vulnerabilities, and advancing health and equity. CAPCOA.

https://www.airquality.org/ClimateChange/Documents/Handbook%20Public%20Draft_2021-Aug.pdf.

California Energy Commission (CEC). (2019). Power content label 2019: Southern California Edison (SCE). CEC.

<https://www.energy.ca.gov/filebrowser/download/3265>.

National Conference of State Legislatures (NCSL). (2021, August 13). State renewable portfolio standards and goals. NCSL.

<https://www.ncsl.org/research/energy/renewable-portfolio-standards.aspx>

USDA Forest Service. (2006). I-Tree planting calculator. I-Tree. <https://planting.itreetools.org/>.

Strategy 6 Energy efficiency retrofits for low-income households

GHG Reduction (MTCO _{2e})		Per capita GHG reduction (MTCO _{2e} per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
940	1,440	0.0023	0.0020	Penetration rate for low-income residential energy efficiency retrofits	30%	60%	Number of retrofitted low-income homes	4,630	9,270
				Anticipated savings in electricity energy efficiency from retrofit activities	20%	20%			
				Anticipated savings in natural gas energy efficiency from retrofit activities	32%	32%			
				Percent of existing households in Ontario that are low income (2019)	27%	27%			

Sources:

California Air Pollution Control Officers Association (CAPCOA). (2021). Handbook for analyzing greenhouse gas emissions reductions, assessing climate vulnerabilities, and advancing health and equity. CAPCOA.

https://www.airquality.org/ClimateChange/Documents/Handbook%20Public%20Draft_2021-Aug.pdf

California Energy Commission (CEC). (2021). "2019 Residential Appliance Saturation Study." CEC. <https://www.energy.ca.gov/data-reports/surveys/2019-residential-appliance-saturation-study>.

California Energy Commission (CEC). (2019). Power content label 2019: Southern California Edison (SCE). CEC. <https://www.energy.ca.gov/filebrowser/download/3265>.

National Conference of State Legislatures (NCSL). (2021, August 13). State renewable portfolio standards and goals. NCSL. <https://www.ncsl.org/research/energy/renewable-portfolio-standards.aspx>

U.S. Department of Housing and Urban Development (HUD). (2017). Comprehensive Housing affordability Strategy (CHAS). Office of Policy Development and Research (PD&R). https://www.huduser.gov/portal/datasets/cp.html#2006-2017_data.

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Appendix A - GHG Technical Appendix

Strategy 7 Energy efficiency retrofits

GHG Reduction (MTCO ₂ e)		Per capita GHG reduction (MTCO ₂ e per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
11,570	15,600	0.0288	0.0221	Penetration rate for residential energy efficiency retrofits	15%	30%	Number of retrofitted homes (excluding low-income households)	4,920	9,270
				Anticipated savings from residential energy efficiency retrofits	25%	25%			
				Penetration rate for nonresidential energy efficiency retrofits	5%	20%			
				Anticipated savings from nonresidential energy efficiency retrofits	40%	40%	Square footage of retrofitted nonresidential space	7,525,540	30,102,170
				Target penetration rate for residential LED	75%	95%			
				Target penetration rate for nonresidential LED	60%	80%			
				Percent electricity savings per LED light	75%	75%			

Sources:

- California Air Pollution Control Officers Association (CAPCOA). (2021). Handbook for analyzing greenhouse gas emissions reductions, assessing climate vulnerabilities, and advancing health and equity. CAPCOA. https://www.airquality.org/ClimateChange/Documents/Handbook%20Public%20Draft_2021-Aug.pdf.
- San Bernardino County Transportation Authority (SBCTA). (2021). GHG reduction tool, San Bernardino County GHG reduction plan. SBCTA. <https://www.gosbcta.com/plan/regional-greenhouse-gas-reduction-plan/>.
- California Energy Commission (CEC). (2021). "2019 Residential Appliance Saturation Study." CEC. <https://www.energy.ca.gov/data-reports/surveys/2019-residential-appliance-saturation-study>.
- California Energy Commission (CEC). (2019). Power content label 2019: Southern California Edison (SCE). CEC. <https://www.energy.ca.gov/filebrowser/download/3265>.
- National Conference of State Legislatures (NCSL). (2021, August 13). State renewable portfolio standards and goals. NCSL. <https://www.ncsl.org/research/energy/renewable-portfolio-standards.aspx>
- U.S. Department of Energy (DOE). (2020). Adoption of light-emitting diodes in common lighting applications. Office of Energy Efficiency & Renewable Energy. <https://www.energy.gov/sites/default/files/2020/09/f78/ssl-led-adoption-aug2020.pdf>.

Strategy 8 Smart growth and infill

Smart growth and infill is not a quantifiable strategy in Ontario. There are no measurable GHG gas reductions for this strategy. There are no assumptions or performance targets.

Sources:

California Air Pollution Control Officers Association (CAPCOA). (2021). Handbook for analyzing greenhouse gas emissions reductions, assessing climate vulnerabilities, and advancing health and equity. CAPCOA.

https://www.airquality.org/ClimateChange/Documents/Handbook%20Public%20Draft_2021-Aug.pdf.

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Strategy 9 Transit-Oriented Development

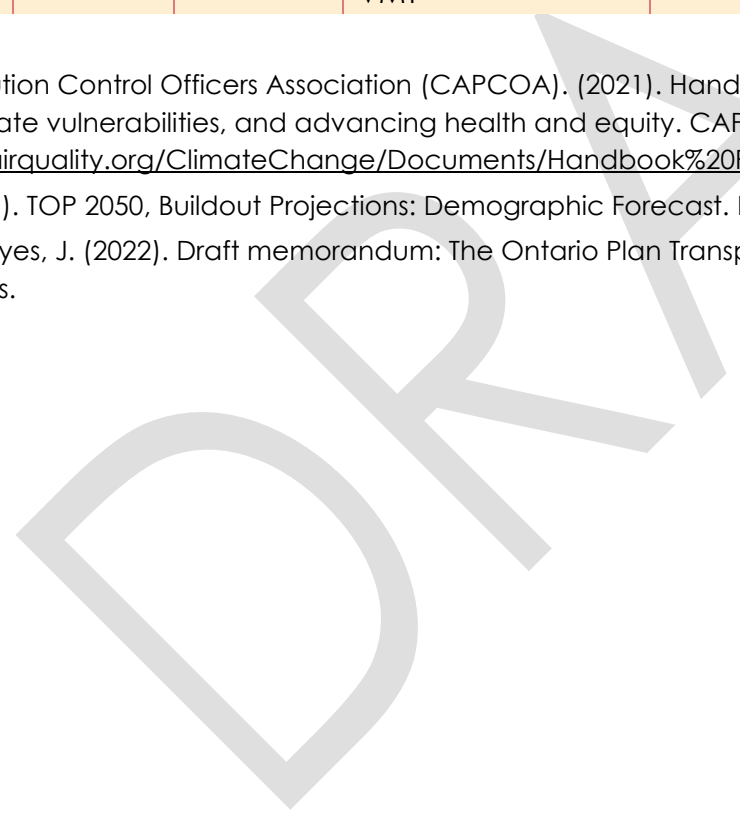
GHG Reduction (MTCO _{2e})		Per capita GHG reduction (MTCO _{2e} per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
330	1,440	0.0008	0.0020	New TOD households' share of new citywide VMT	3%	5%	New housing units in TOD areas	410	3,200

Sources:

California Air Pollution Control Officers Association (CAPCOA). (2021). Handbook for analyzing greenhouse gas emissions reductions, assessing climate vulnerabilities, and advancing health and equity. CAPCOA.
https://www.airquality.org/ClimateChange/Documents/Handbook%20Public%20Draft_2021-Aug.pdf.

PlaceWorks. (2021). TOP 2050, Buildout Projections: Demographic Forecast. PlaceWorks.

Hermann, P. & Hayes, J. (2022). Draft memorandum: The Ontario Plan Transportation Impact Assessment-Vehicle Miles Travelled (VMT). Fehr and Peers.



Strategy 10 Increase transportation ridership

GHG Reduction (MTCO ₂ e)		Per capita GHG reduction (MTCO ₂ e per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
27,980	31,450	0.0697	0.0445	Target participation rate in Commute Trip Reduction Program	4%	4%	Number of employees participating in commute trip reduction	6,780	11,840
				Percent of employees eligible for Commute Trip Reduction Program or ridesharing	80%	80%			
				Target participation rate in ridesharing program in suburban areas	4%	4%	Annual West Valley Connector ridership	25,927,840	25,927,840

Sources:

California Air Pollution Control Officers Association (CAPCOA). (2021). Handbook for analyzing greenhouse gas emissions reductions, assessing climate vulnerabilities, and advancing health and equity. CAPCOA.

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California Air Resources Board (CARB). (2019). Emission Factors model (EMFAC), Emissions inventory tool. CARB.

<https://arb.ca.gov/emfac/emissions-inventory>.

U.S. Census. (2019). Home area profile analysis. Longitudinal Employer-Household Dynamics (LEHD) OnTheMap.

<https://lehd.ces.census.gov/>.

Omnitrans. (2022). Route 83 service frequency. Omnitrans. <https://omnitrans.org/routes/route-83/>

U.S. Census Bureau. (2019). Means of transportation to work, 2019 American Community Survey 5-year estimate data profiles. The Census Bureau. <https://data.census.gov/cedsci/>.

Hermann, P. & Hayes, J. (2022). Draft memorandum: The Ontario Plan Transportation Impact Assessment-Vehicle Miles Travelled (VMT). Fehr and Peers.

Strategy 11 Traffic signal synchronization and roadway management

Traffic signal synchronization is not a quantifiable strategy. There are no measurable GHG gas reductions for this strategy. There are no assumptions or performance targets.

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Appendix A - GHG Technical Appendix

Strategy 12 Community vehicle electrification

GHG Reduction (MTCO ₂ e)		Per capita GHG reduction (MTCO ₂ e per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
62,880	465,140	0.1565	0.6584	Penetration rate for electrification of municipal fleet	20%	40%	EVs in municipal fleet	130	260
				Estimated percentage of municipal vehicle fleet that is light-duty vehicles	80%	80%			
				Estimated percentage of municipal vehicle fleet that is heavy-duty vehicles	20%	20%	Light-duty EVs registered in Ontario	24,510	173,400
				Target for eVMT share of community wide VMT (light-duty)	15%	60%			
				Target for eVMT share of community wide VMT (heavy-duty)	10%	50%	Heavy-duty EVs registered in Ontario	1,490	14,450
				Target number of new charging stations	15	30			

Sources:

- California Air Pollution Control Officers Association (CAPCOA). (2021). Handbook for analyzing greenhouse gas emissions reductions, assessing climate vulnerabilities, and advancing health and equity. CAPCOA.
https://www.airquality.org/ClimateChange/Documents/Handbook%20Public%20Draft_2021-Aug.pdf.
- California Energy Commission (CEC). (2019). Light-duty vehicle population in California. CEC. <https://www.energy.ca.gov/data-reports/energy-almanac/zero-emission-vehicle-and-infrastructure-statistics/light-duty-vehicle>.
- California Air Resources Board (CARB). (2019). Emission Factors model (EMFAC), Emissions inventory tool. CARB.
<https://arb.ca.gov/emfac/emissions-inventory>.
- California Department of Transportation (Caltrans). (2018). California public road data. Caltrans. <https://dot.ca.gov/-/media/dot-media/programs/research-innovation-system-information/documents/california-public-road-data/prd-2018-a11y.pdf>.
- Hermann, P. & Hayes, J. (2022). Draft memorandum: The Ontario Plan Transportation Impact Assessment-Vehicle Miles Travelled (VMT). Fehr and Peers.

Strategy 13 Active transportation networks

GHG Reduction (MTCO _{2e})		Per capita GHG reduction (MTCO _{2e} per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
1,800	3,840	0.0045	0.0054	Percentage of bicycle network completed by 2030 and 2050	25%	75%	Miles of newly-installed bike lanes	42	127
							Miles of newly-installed pedestrian improvements	4	12

Sources:

California Air Pollution Control Officers Association (CAPCOA). (2021). Handbook for analyzing greenhouse gas emissions reductions, assessing climate vulnerabilities, and advancing health and equity. CAPCOA.

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Strategy 14 Vehicle idling

GHG Reduction (MTCO _{2e})		Per capita GHG reduction (MTCO _{2e} per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
1,950	1,780	0.0049	0.0025	Compliance rate with idling ordinance	0.5	0.5	There are no performance metrics associated with this strategy		
				Reduction in idling time with ordinance	40%	40%			
				Percent of total fuel used when idling	6%	6%			
				Reduction in idling time with ordinance	40%	40%			

Sources:

San Bernardino County Transportation Authority (SBCTA). (2021). GHG reduction tool, San Bernardino County GHG reduction plan. SBCTA. <https://www.gosbcta.com/plan/regional-greenhouse-gas-reduction-plan/>.

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Strategy 15 Parking policy and event parking

GHG Reduction (MTCO _{2e})		Per capita GHG reduction (MTCO _{2e} per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
2,230	8,110	0.0056	0.0115	Current parking price	\$0.50	\$0.50	Future parking price	\$0.75	\$1.25
				Future parking price	\$0.75	\$1.25			
				Default percentage of trips parking on the street	75%	75%			

Sources:

California Air Pollution Control Officers Association (CAPCOA). (2021). Handbook for analyzing greenhouse gas emissions reductions, assessing climate vulnerabilities, and advancing health and equity. CAPCOA.

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<https://arb.ca.gov/emfac/emissions-inventory>.

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Strategy 16 Electrification of construction and landscaping equipment

GHG Reduction (MTCO _{2e})		Per capita GHG reduction (MTCO _{2e} per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
4,810	26,020	0.0120	0.0368	Penetration rate of electrification of conventional construction equipment	20%	50%	Pieces of electrified construction equipment	320	1,360
				Penetration rate of electrification of conventional landscaping equipment	20%	50%	Pieces of electrified landscaping equipment	1,070	4,120

Sources:

- San Bernardino County Transportation Authority (SBCTA). (2021). GHG reduction tool, San Bernardino County GHG reduction plan. SBCTA. <https://www.gosbcta.com/plan/regional-greenhouse-gas-reduction-plan/>.
- California Air Resources Board (CARB). (2019). Emission Factors model (EMFAC), Emissions inventory tool. CARB. <https://arb.ca.gov/emfac/emissions-inventory>.

Strategy 17 Idling ordinance for construction equipment

GHG Reduction (MTCO _{2e})		Per capita GHG reduction (MTCO _{2e} per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
40	100	0.0001	0.0001	Reduction in idling time with ordinance	40%	40%	There are no performance metrics associated with this strategy		
				Compliance rate with idling ordinance	50%	50%			

Sources:

San Bernardino County Transportation Authority (SBCTA). (2021). GHG reduction tool, San Bernardino County GHG reduction plan.

SBCTA. <https://www.gosbcta.com/plan/regional-greenhouse-gas-reduction-plan/>.

California Air Resources Board (CARB). (2019). Emission Factors model (EMFAC), Emissions inventory tool. CARB.

<https://arb.ca.gov/emfac/emissions-inventory>.

Strategy 18 Methane capture at landfills

GHG Reduction (MTCO ₂ e)		Per capita GHG reduction (MTCO ₂ e per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
22,410	57,370	0.0558	0.0812	Current methane capture rate	75%	75%	Methane capture rate	80%	85%
				Target for methane capture rate	80%	85%			

Sources:

San Bernardino County Transportation Authority (SBCTA). (2021). GHG reduction tool, San Bernardino County GHG reduction plan. SBCTA. <https://www.gosbcta.com/plan/regional-greenhouse-gas-reduction-plan/>.

California Air Pollution Control Officers Association (CAPCOA). (2021). Handbook for analyzing greenhouse gas emissions reductions, assessing climate vulnerabilities, and advancing health and equity. CAPCOA. https://www.airquality.org/ClimateChange/Documents/Handbook%20Public%20Draft_2021-Aug.pdf.

Strategy 19 Waste diversion

GHG Reduction (MTCO ₂ e)		Per capita GHG reduction (MTCO ₂ e per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
14,490	38,670	0.0361	0.0547	Waste diversion target (AB 939)	75%	85%	Estimated tons of solid waste diverted	267,010	532,190
				Waste diversion rate (BAU)	50%	50%			

Sources:

San Bernardino County Transportation Authority (SBCTA). (2021). GHG reduction tool, San Bernardino County GHG reduction plan.

SBCTA. <https://www.gosbcta.com/plan/regional-greenhouse-gas-reduction-plan/>.

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Strategy 20 Construction and demolition waste recovery ordinance

Construction and demolition waste recovery is not a quantifiable strategy in Ontario. There are no measurable GHG gas reductions for this strategy. There are no assumptions or performance targets.

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Strategy 21 Indoor water efficiency

GHG Reduction (MTCO _{2e})		Per capita GHG reduction (MTCO _{2e} per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
4,580	2,540	0.0114	0.0036	Average reduction in indoor water use in new buildings	20%	40%	Total community water use (million gallons, excluding recycled water)	10,770	18,630
				Average reduction in outdoor water use in new buildings	40%	60%			
				Average reduction in water use in existing buildings	25%	50%	Total community wastewater generation (million gallons)	3,880	6,450
				Average reduction in wastewater generation in existing buildings	25%	50%			

Sources:

San Bernardino County Transportation Authority (SBCTA). (2021). GHG reduction tool, San Bernardino County GHG reduction plan.

SBCTA. <https://www.gosbcta.com/plan/regional-greenhouse-gas-reduction-plan/>.

California Air Pollution Control Officers Association (CAPCOA). (2021). Handbook for analyzing greenhouse gas emissions reductions, assessing climate vulnerabilities, and advancing health and equity. CAPCOA.

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Strategy 22 Water efficient landscapes and water recycling

GHG Reduction (MTCO _{2e})		Per capita GHG reduction (MTCO _{2e} per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
3,400	0	0.0085	0	Percent increase in recycled water use	72%	145%	Recycled water use (million gallons)	4,390	6,240

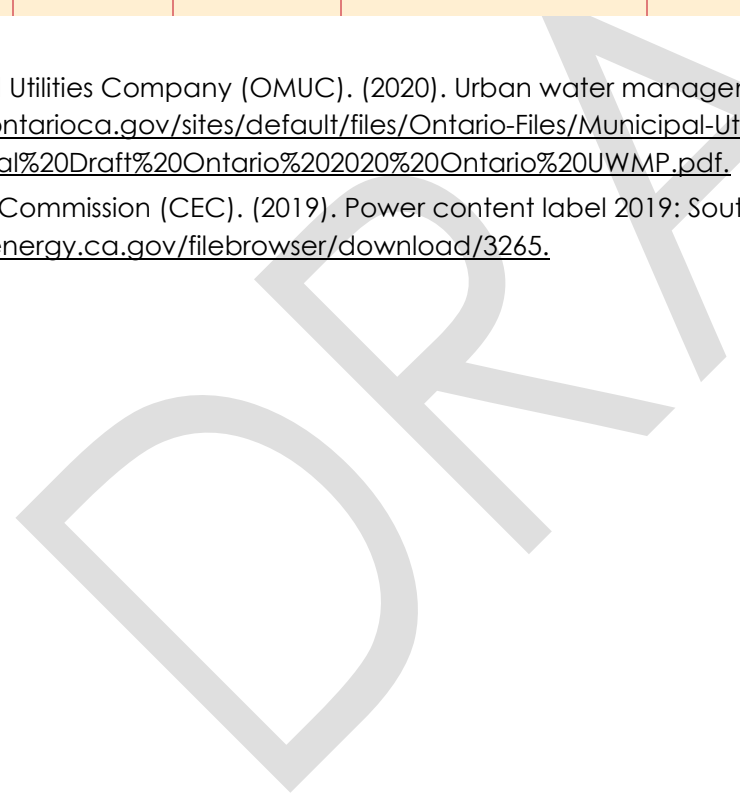
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Strategy 23 Water system and wastewater operations efficiency

GHG Reduction (MTCO ₂ e)		Per capita GHG reduction (MTCO ₂ e per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
150	0	0.0004	0	Efficiency improvement in water treatment and distribution system	10%	25%	Electricity use of water treatment and distribution system (kWh)	56,533,450	66,745,850

Sources:

San Bernardino County Transportation Authority (SBCTA). (2021). GHG reduction tool, San Bernardino County GHG reduction plan. SBCTA. <https://www.gosbcta.com/plan/regional-greenhouse-gas-reduction-plan/>.

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Strategy 24 Methane capture for wastewater treatment

GHG Reduction (MTCO _{2e})		Per capita GHG reduction (MTCO _{2e} per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
960	2,280	0.0024	0.0032	IEUA wastewater methane capture rate	70%	85%	Methane capture rate at IEUA facilities	70%	85%

Sources:

San Bernardino County Transportation Authority (SBCTA). (2021). GHG reduction tool, San Bernardino County GHG reduction plan.

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Strategy 25 Methane capture for dairy operations

GHG Reduction (MTCO ₂ e)		Per capita GHG reduction (MTCO ₂ e per person)		Assumptions			Performance standards		
2030	2050	2030	2050	Description	2030	2050	Description	2030	2050
3,160	0	0.0079	0.0000	Dairies using manure management systems with methane capture	50%	100%	Heads of livestock on dairies with manure capture	3,520	0
				Methane capture rate of installed systems	86%	86%			

Sources:

San Bernardino County Transportation Authority (SBCTA). (2021). GHG reduction tool, San Bernardino County GHG reduction plan. SBCTA. <https://www.gosbcta.com/plan/regional-greenhouse-gas-reduction-plan/>.

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Strategy 26 Climate change awareness and education

Climate change awareness and education is not a quantifiable strategy in Ontario. There are no measurable GHG gas reductions for this strategy. There are no assumptions or performance targets.

Strategy 27 Carbon sequestration

Carbon sequestration is not a quantifiable strategy in Ontario. There are no measurable GHG gas reductions for this strategy. There are no assumptions or performance targets.

Strategy 28 Green jobs

Carbon sequestration is not a quantifiable strategy in Ontario. There are no measurable GHG gas reductions for this strategy. There are no assumptions or performance targets.

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CITY OF ONTARIO

MEMORANDUM

TO: Planning Commission

FROM: Rudy Zeledon, Planning Director *RZ*

DATE: July 26, 2022

SUBJECT: Monthly Activity Reports

The Monthly Activity Reports were not available at the time of agenda packet distribution. These Reports will be made available at the July 26, 2022 Planning Commission meeting.