

CITY OF ONTARIO PLANNING COMMISSION/ HISTORIC PRESERVATION MEETING AGENDA

May 25, 2021

Ontario City Hall
303 East "B" Street, Ontario, California 91764

6:30 PM

WELCOME to a meeting of the Ontario Planning/Historic Preservation Commission.

All documents for public review are on file in the Planning Department located at 303 East B Street, Ontario, CA 91764.

- *Anyone wishing to speak during public comment or on a particular item should fill out a green slip and submit it to the Secretary.*
- *Comments will be limited to 5 minutes. Speakers will be alerted when their time is up. Speakers are then to return to their seats and no further comments will be permitted.*
- *In accordance with State Law, remarks during public comment are to be limited to subjects within the Commission's jurisdiction. Remarks on other agenda items will be limited to those items.*
- *Remarks from those seated or standing in the back of the chambers will not be permitted. All those wishing to speak including Commissioners and Staff need to be recognized by the Chair before speaking.*
- *The City of Ontario will gladly accommodate disabled persons wishing to communicate at a public meeting. Should you need any type of special equipment or assistance in order to communicate at a public meeting, please inform the Planning Department at (909) 395-2036, a minimum of 72 hours prior to the scheduled meeting.*
- *Please turn off all communication devices (phones and beepers) or put them on non-audible mode (vibrate) so as not to cause a disruption in the Commission proceedings.*

ROLL CALL

DeDiemar __ Gage __ Gregorek __ Lampkin __ Ricci __ Willoughby __

PLEDGE OF ALLEGIANCE TO THE FLAG

ANNOUNCEMENTS

- 1) Agenda Items
- 2) Commissioner Items

PUBLIC COMMENTS

Citizens wishing to address the Planning/Historic Preservation Commission on any matter that is not on the agenda may do so at this time. Please state your name and address clearly for the record and limit your remarks to five minutes.

Please note that while the Planning/Historic Preservation Commission values your comments, the Commission cannot respond nor take action until such time as the matter may appear on the forthcoming agenda.

CONSENT CALENDAR ITEMS

All matters listed under CONSENT CALENDAR will be enacted by one summary motion in the order listed below. There will be no separate discussion on these items prior to the time the Commission votes on them, unless a member of the Commission or public requests a specific item be removed from the Consent Calendar for a separate vote. In that case, the balance of the items on the Consent Calendar will be voted on in summary motion and then those items removed for separate vote will be heard.

A-01. MINUTES APPROVAL

Planning/Historic Preservation Commission Minutes of April 27, 2021, approved as written.

- A-02. ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV20-005:** A Development Plan to construct a 256,711 square foot industrial building on 11.3 acres of land located at 875 West State Street, within the IL (Light Industrial) zoning district. Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) for this project. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 1011-161-04 and 1011-161-05) submitted by Inland Harbor LLC. **This item was continued from the April 27, 2021 Planning Commission meeting.**

PUBLIC HEARING ITEMS

For each of the items listed under PUBLIC HEARING ITEMS, the public will be provided an opportunity to speak. After a staff report is provided, the chairperson will open the public hearing. At that time the applicant will be allowed five (5) minutes to make a presentation on the case. Members of the public will then be allowed five (5) minutes each to speak. If a large number of individuals wish to speak on an item, the Planning Commission Chairman may limit the time for individuals wishing to speak to 3 minutes in order to provide an opportunity for more people to be heard. Speakers will be

alerted when their time is up, and no further comments will be permitted. The Planning/Historic Preservation Commission may ask the speakers questions relative to the case and the testimony provided. The question period will not count against your time limit. After all persons have spoken, the applicant will be allowed three minutes to summarize or rebut any public testimony. The chairperson will then close the public hearing portion of the hearing and deliberate the matter.

PLANNING / HISTORIC PRESERVATION COMMISSION ITEMS

- B. TWENTY-FIRST ANNUAL MODEL COLONY AWARDS FILE NO. PHP21-008:**
A request for the Historic Preservation Commission to accept the nominations for the Twenty-first Annual Model Colony Awards; **submitted by City of Ontario. City Council presentation of Awards.**

1. File No. PHP21-008 (Model Colony Awards)

Motion to recommend Approval/Denial

- C. ENVIRONMENTAL ASSESSMENT AND PLANNED UNIT DEVELOPMENT AMENDMENT REVIEW FOR FILE NO. PUD21-001:** An Amendment to the Emporia Family Housing Planned Unit Development to expand the project area from approximately 2.80 acres of land to 4.95 acres of land, establish minimum building setbacks from Palm Avenue and Transit Street rights-of-way, modify minimum parking requirements, allow on-street loading, and update the planting palette. Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) for this project. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP). (APNs: 1049-051-04, 1049-054-02, 1049-054-03, 1049-054-04, 1049-054-06, 1049-059-06, and 1049-059-07) **submitted by The Related Companies of California, LLC. City Council action is required.**

- D. ENVIRONMENTAL ASSESSMENT, CERTIFICATE OF APPROPRIATENESS, TENTATIVE PARCEL MAP, AND DEVELOPMENT PLAN REVIEW FOR FILE NOS. PHP21-003, PMTT21-004 AND PDEV21-008:** A Certificate of Appropriateness (File No. PHP21-003) to demolish 2 historic Tier III buildings located within the project site and a Tentative Parcel Map (File No. PMTT21-004/TPM 20339) to consolidate 4 lots and the vacation of an adjoining section of Fern Avenue, for a total of 2.15 acres of land, in conjunction with a Development Plan (File No. PDEV21-008) to construct 50 multiple-family affordable housing dwelling units, generally located at the northwest and southwest corners of Emporia Street and Palm Avenue, within LUA2-N (Arts District-North) and LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district. Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) for this project. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP). (APNs: 1049-054-02, 1049-054-03, 1049-054-

04, 1049-054-06, 1049-059-06, and 1049-059-07) submitted by The Related Companies of California, LLC.

1. **CEQA Determination**

Motion to recommend Approval/Denial of an Addendum to a previous EIR

2. **File No. PUD21-001** (Planned Unit Development Amendment)

Motion to recommend Approval/Denial

3. **File No. PHP21-003** (Certificate of Appropriateness)

Motion to Approve/Deny

4. **File No. PMTT21-004 (TPM 20339)** (Tentative Parcel Map)

Motion to Approve/Deny

5. **File No. PDEV21-008** (Development Plan)

Motion to Approve/Deny

E. **ENVIRONMENTAL ASSESSMENT AND SPECIFIC PLAN AMENDMENT FOR FILE NO. PSPA20-003**: An Amendment to the California Commerce Center Specific Plan, changing the land use designation on 10.64 acres of land from Commercial/Food/Hotel to Light Industrial, to be consistent with The Ontario Plan Policy Plan (General Plan) Industrial (0.55 FAR) land use designation, located at the northeast corner of Haven Avenue and Airport Drive, within the California Commerce Center Specific Plan. Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) for this project. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0211-222-66) **submitted by Vogel Properties, Inc. City Council action is required.**

F. **ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV20-008**: A Development Plan to construct a 200,291-square foot industrial building on 10.64 acres of land located at the northeast corner of Haven Avenue and Airport Drive, within the proposed Light Industrial land use district of the California Commerce Center Specific Plan. Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) for this project. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the

Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0211-222-66) **submitted by Vogel Properties, Inc.**

1. CEQA Determination

Motion to recommend Approval/Denial of an Addendum to a previous EIR

2. File No. PSPA20-003 (Specific Plan Amendment)

Motion to recommend Approval/Denial

3. File No. PDEV20-008 (Development Plan)

Motion to Approve/Deny

G. ENVIRONMENTAL ASSESSMENT AND SPECIFIC PLAN AMENDMENT FOR FILE NO. PSPA20-006: An Amendment to the Subarea 29 Specific Plan to increase the overall density within Planning Area 27 (Cluster Homes – 7-14 du/ac) from 4.8 to 4.9 dwelling units per gross acre and establish a new residential product type (Motorcourt Cluster D – 8-Plex). Planning Area 27 is bounded by Merrill Avenue to the north, Southern California Edison easement to the west, and the San Bernardino County Flood Control District and the City of Eastvale to the south. Staff has prepared an Addendum to the Subarea 29 Specific Plan Environmental Impact Report (State Clearinghouse No. 2004011009) for this project. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP). The project site is also located within the Airport Influence area of Chino Airport and is consistent with policies and criteria set forth within the 2011 California Airport Land Use Planning Handbook published by the California Department of Transportation, Division of Aeronautics; (APN: 0218-331-42) **submitted by SL Ontario Development Company, LLC. City Council action is required.**

H. ENVIRONMENTAL ASSESSMENT AND TENTATIVE TRACT MAP REVIEW FOR FILE NO. PMTT20-012 (TM 20389): A Tentative Tract Map (TTM 20389) to subdivide 5.99 acres of land into one numbered lot and three lettered lots for condominium purposes. The project is bounded by Merrill Avenue to the north, Southern California Edison easement to the west, and the San Bernardino County Flood Control District and the City of Eastvale to the south. Staff has prepared an Addendum to the Subarea 29 Specific Plan Environmental Impact Report (State Clearinghouse No. 2004011009) for this project. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP). The project site is also located within the Airport Influence area of Chino Airport and is consistent with policies and criteria set forth within the 2011 California Airport Land Use Planning Handbook published by the California Department of Transportation, Division of Aeronautics; (APN: 0218-331-42) **submitted by SL Ontario**

Development Company, LLC.

1. CEQA Determination

Motion to recommend Approval/Denial of an Addendum to a previous EIR

2. File No. PSPA20-006 (Specific Plan Amendment)

Motion to recommend Approval/Denial

3. File No. PMTT20-012 (TTM 20389) (Tentative Tract Map)

Motion to Approve/Deny

I. ENVIRONMENTAL ASSESSMENT, DEVELOPMENT PLAN, AND CONDITIONAL USE PERMIT REVIEW FOR FILE NOS. PDEV18-022 AND PCUP18-021:

A Development Plan (File No. PDEV18-022) to construct a 6,870 square foot industrial building in conjunction with a Conditional Use Permit (File No. PCUP18-021) to establish and operate a towing service and short-term storage of automobiles, vans, light trucks, and tractor trucks on 3.1 acres of land located at 580 East Belmont Street, within the IL (Light Industrial) zoning district. Staff has determined that the project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 1049-491-01, 1049-491-02 and 1049-491-03) **submitted by Four Sisters Enterprises LLC.**

1. CEQA Determination

No action necessary – Exempt: CEQA Guidelines Section § 15332

2. File No. PCUP18-021 (Conditional Use Permit)

Motion to Approve/Deny

3. File No. PDEV18-022 (Development Plan)

Motion to Approve/Deny

MATTERS FROM THE PLANNING/HISTORIC PRESERVATION COMMISSION

1) Old Business

- Reports From Subcommittees

- Historic Preservation (Standing): Met on May 13, 2021.

- 2) New Business
- 3) Nominations for Special Recognition

DIRECTOR'S REPORT

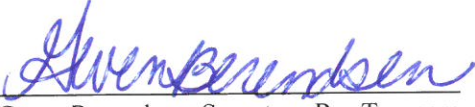
- 1) Monthly Activity Report


If you wish to appeal any decision of the Planning/Historic Preservation Commission, you must do so within ten (10) days of the Commission action. Please contact the Planning Department for information regarding the appeal process.

If you challenge any action of the Planning/Historic Preservation Commission in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the Planning/Historic Preservation Commission at, or prior to, the public hearing.



I, Gwen Berendsen, Administrative Assistant, of the City of Ontario, or my designee, hereby certify that a true, accurate copy of the foregoing agenda was posted on **Friday, May 21, 2021**, at least 72 hours prior to the meeting per Government Code Section 54954.2 at 303 East "B" Street, Ontario.


Gwen Berendsen, Secretary Pro Tempore


Rudy Zeledon, Planning Director
Planning/Historic Preservation
Commission Secretary

**CITY OF ONTARIO PLANNING COMMISSION/
HISTORIC PRESERVATION MEETING**

MINUTES

April 27, 2021

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**CITY OF ONTARIO PLANNING COMMISSION/
HISTORIC PRESERVATION MEETING**

MINUTES

April 27, 2021

REGULAR MEETING: City Hall, 303 East B Street
VIA ZOOM Called to order by Chairman Gage at 6:36 PM

COMMISSIONERS

Present: Chairman Gage, Vice-Chairman Willoughby, DeDiemar, Gregorek, Lampkin, and Ricci

Absent: None

OTHERS PRESENT: Planning Director Zeledon, City Attorney Otto, Principal Planner
VIA ZOOM Mercier, Sustainability Manager Ruddins, Development Agency Administrative Office Womble, Senior Planner Ayala, Senior Planner Mejia, Associate Planner Antuna, Associate Planner Vaughn, Transportation Manager Bautista , and Planning Secretary Berendsen

PLEDGE OF ALLEGIANCE TO THE FLAG

The Pledge of Allegiance was led by Commissioner DeDiemar.

ANNOUNCEMENTS

Mr. Zeledon stated that Item A-02 is being requested to be continued to the next meeting, and Item D is requesting to be continued to the June 22, 2021 meeting to address concerns from a public comment and there are revised COAs for Item B, which were emailed to them, and Items E,F and G will be taken together and session 7 of those item resolutions have been revised to reflect City Council action.

PUBLIC COMMENTS

Mr. Zeledon stated no public comments were received.

Mr. Mercier stated there were no members of the public wishing to speak at this time.

CONSENT CALENDAR ITEMS

Agenda Item A-02 was pulled from the Consent Calendar and requested to be continued to the May 25, 2021 meeting.

Mr. Gregorek stated he needed to abstain from Items A-03 and A-04, as his firm has worked on both projects.

A-01. MINUTES APPROVAL

Planning/Historic Preservation Commission Minutes of March 23, 2021, approved as written.

It was moved by Willoughby, seconded by Gregorek, to approve the Planning Commission Minutes of March 23, 2021, as written. Roll call vote: AYES, DeDietmar, Gage, Gregorek, Lampkin, Ricci, and Willoughby; NOES, none; RECUSE, none; ABSENT, none. The motion was carried 6 to 0.

- A-03. **ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV20-030**: A Development Plan to construct 224 dwellings, including 87 single-family and 137 multiple-family dwellings, on 21.10 acres of land located at the northeast corner of East Edison and South Mill Creek Avenues. The environmental impacts of this project were previously reviewed in conjunction with File No. PSP05-004, the Rich Haven Specific Plan, for which an Environmental Impact Report (State Clearinghouse No. 2006051081) was previously certified by the City Council on December 4, 2007. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0218-211-12) **submitted by Shea Homes.**

- A-04. **ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV20-032**: A Development Plan to construct 106 single-family dwellings on 10.49 acres of land located at the northeast corner of South Manitoba Place and East La Avenida Drive, within the Low-Medium Density land use district of The Avenue Specific Plan. The environmental impacts of this project were previously reviewed in conjunction with File No. PGPA19-008, for which an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was prepared. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0218-652-27) **submitted by Woodside 05S, LP.**

It was moved by Ricci, seconded by Lampkin, to approve File Nos. PDEV20 -030 and PDEV20-032. Roll call vote: AYES, DeDietmar, Gage, Lampkin, Ricci, and Willoughby; NOES, none; RECUSE, Gregorek; ABSENT, none. The motion was carried 5 to 0.

- A-02. **ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV20-005**: A Development Plan to construct a 256,711 square foot industrial building on 11.3 acres of land located at 875 West State Street, within the IL (Light Industrial) zoning district. The environmental impacts of this project were previously reviewed in conjunction with The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), which was certified by City Council on January 27, 2010. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 1011-161-04 and 1011-161-05) **submitted by Inland Harbor LLC.**

It was moved by Lampkin, seconded by Ricci, to continue File No. PDEV20-005 to the May 25, 2021 Planning Commission meeting. Roll call vote: AYES, DeDietmar, Gage, Gregorek, Lampkin, Ricci, and Willoughby; NOES, none; RECUSE, none; ABSENT, none. The motion was carried 6 to 0.

HISTORIC PRESERVATION / PLANNING COMMISSION PUBLIC HEARING ITEMS

- B. **ENVIRONMENTAL ASSESSMENT, CERTIFICATE OF APPROPRIATENESS,**

DEVELOPMENT PLAN, AND TENTATIVE PARCEL MAP REVIEW FOR FILE NOS. PHP20-008, PDEV20-014 AND PMTT20-004: A Certificate of Appropriateness (File No. PHP20-008) and a Development Plan (File No. PDEV20-014) to relocate a Tier III historic single-family residence from its current location approximately 130 feet southeast, to the corner of the site, in conjunction with a Tentative Parcel Map (File No. PMTT20-004/TPM 20255) to subdivide 1.1 acres of land into 4 lots within the LDR-5 (Low Density Residential –2.1 to 5.0 du/ac) zoning district located at 730 West Fourth Street. A Mitigated Negative Declaration of environmental effects has been prepared for this project. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP). (APN: 1047-594-52) **submitted by Fred Herzog. This item was continued from the March 23, 2021 Planning Commission meeting.**

Associate Planner Antuna, presented the staff report. She described the location, surrounding area and history of the property. She described the relocation of the historic residence and the proposed parcel map. She described the Tier III historic residence and the need for a Certificate of Appropriateness. She described the proposed conceptual single level elevations and site plan, which includes ADUs on each lot. She described the Mitigation required by the environmental review. She explained the public comment received by the church to the north and the revised COAs to address the construction noise and the new block wall effecting the existing landscape concerns. She stated that staff is recommending the Planning Commission approve File Nos. PHP20-008, PDEV20-014 and PMTT20-004, pursuant to the facts and reasons contained in the staff report and attached resolutions, and subject to the conditions of approval.

Mr. Lampkin wanted to know if the residence is currently occupied by renters or owners.

Ms. Antuna stated it is occupied by renters.

Mr. Lampkin wanted to know if accommodations been made for the tenants once the house has been moved.

Ms. Antuna referred the question to the applicant.

Mr. Gregorek wanted to know if the ADUs will be constructed at the same time as the homes.

Ms. Antuna referred the question to the applicant.

Mr. Gregorek wanted to know if the ADUs would come back to the Commission for approval.

Ms. Antuna stated they would be an administrative approval through the city's plan check process.

PUBLIC TESTIMONY

Mr. Fred Herzog the applicant and creator of the designs, stated he has talked with the renters several times that we are going through this process, but he hasn't discussed the particulars yet, and he would need to talk to the owners regarding what their plans are. He stated the historical home needs to be moved first and then once it's in place and approved, then the construction would start on the homes, but that would need to be coordinated with the owners.

Mr. Gregorek wanted to know if the ADUs will be constructed at the same time, or is it an option for the buyers.

Mr. Herzog stated that at this time they would be built at the same time.

Mr. Gregorek wanted to clarify that whoever purchases the lot, would be purchasing both dwellings.

Mr. Herzog stated that is correct.

Mr. Gage wanted to know if they would be saving any of the palm trees currently on the lot.

Mr. Herzog stated no.

Mr. Gage wanted to know if the applicant is set on the architecture style of these homes, and if it would match the historic home that is being moved.

Mr. Herzog stated there would be some similarities to the historic craftsman style home, and have similar details like the windows and materials used, and they would be a modern style typical craftsman style home which is more popular and common.

Mr. Gage wanted to know if these would all be single story homes.

Mr. Herzog stated that originally, they wanted to do two story homes, but because of the majority of single story homes in the neighborhood they submitted single story plans.

Mr. Lampkin wanted to know if arrangements will be made for the current occupants.

Mr. Herzog stated he would have to get back with the owners and see how they would handle it, but the renters do know there is work that is going to be done and that we are in the process.

Mr. Mercier stated there were no members of the public wishing to speak on this item.

Mr. Zeledon stated there is a landscaping condition of approval regarding some of the trees being preserved or mitigated.

As there was no one else wishing to speak, Chairman Gage closed the public testimony

Mr. Willoughby stated he was glad to see we are going to keep this historic property and relocate it and appreciated the additional conditions for the landscaping and accommodating the church hours during construction and looks like a good project.

Mr. Gregorek stated he echoed Mr. Willoughby's comments and is glad we are preserving a historic structure which still has value and glad they will be one story homes, so it is consistent with what is in the area.

Ms. DeDiemar stated was glad that the project will improve the street scape, which is currently unattractive.

Mr. Ricci stated he concurs with the other commissioners and drives by there and sees the potential and this project is making a better use of the property and makes it more consistent with the homes in the area.

Mr. Lampkin stated it is a nice improvement to the area and adds a nice mix and that his wish and hope is that there will be a discussion with the current renters, to make sure they are not displaced.

Mr. Gage stated nice to see we are preserving this historic home and would like to see some architectural diversity with the three new dwellings.

HISTORIC PRESERVATION PLANNING COMMISSION ACTION

Acting as the Historic Preservation Commission, it was moved by Ricci, seconded by Gregorek, to adopt a resolution to approve the Mitigated Negative Declaration, the Certificate of Appropriateness, File No. PHP20-008, the Development Plan, File No. PDEV20-014, and the Tentative Parcel Map, File No. PMTT20-004 (TPM 20255), subject to the revised conditions of approval. Roll call vote: AYES, DeDiemar, Gage, Gregorek, Lampkin, Ricci, and Willoughby; NOES, none; RECUSE, none; ABSENT, none. The motion was carried 6 to 0.

- C. **ENVIRONMENTAL ASSESSMENT AND TENTATIVE PARCEL MAP REVIEW FOR FILE NO. PMTT21-001**: A Tentative Parcel Map (TPM 20328) to subdivide 0.49 acres of land into 4 parcels generally located at the northeast corner of Euclid Avenue and Acacia Avenue, at 1325 and 1329 South Euclid Avenue, within the MDR-11 (Medium Density Residential – 5.1 to 11.0 du/ac) zoning district. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15315 (Class 15, Minor Land Divisions) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1049-531-01 & -02) **submitted by Alex Espinoza.**

Associate Planner Antuna, presented the staff report. She described the location and surrounding area, the rock curbing that is current and the architectural designs surrounding it. She described the lots proposed orientation. She described the driveway access to the north and lot line adjustment required for the current garage that is encroaching on the property and the COA that was added for continued access to the driveway for the adjoining lot to the north. She described the conceptual elevations and site plan, and the enhanced architecture to the Euclid facing elevations. She explained the call received in favor of the project. She stated that staff is recommending the Planning Commission approve File No. PMTT21-001, pursuant to the facts and reasons contained in the staff report and attached resolutions, and subject to the conditions of approval.

Mr. Gregorek wanted to know if the driveway easement will be constructed or will there be a fence along it.

Ms. Antuna stated there will be a 20 foot driveway and no fencing to ensure continued access to the north property.

Mr. Zeledon stated currently the drive isle is 13 feet and would be expanded to 20 feet to allow access to the garage for the northern property, with an apron for backup area, as well as provide access for all the proposed new homes.

Mr. Gregorek wanted to clarify this would be access for the new dwelling garages and for the property to the north.

Mr. Zeledon stated that is correct and access off Euclid is very difficult so to use this existing access is a win for both.

Mr. Gregorek wanted to clarify that at the front of the lots is a rock area and that this is for some sort of VMP devise.

Mr. Zeledon stated that is correct, these are for future water quality, and will probably be some sort of dry well system.

Mr. Ricci wanted to know who would be responsible for maintaining the drive isle.

Mr. Zeledon stated that with the recordation of the map a maintenance agreement will be set up between the property owners, for any maintenance or cost related to the drive isle, for the city to review and approve.

Mr. Ricci wanted to know where the property line is or if it would be treated like an easement property.

Ms. Antuna stated that the 4 parcels property line goes all the way back, however the drive isle would be an easement to ensure continued access for the property to the north.

Mr. Zeledon stated the property lines for the properties goes across to the structures to the north, but there is an easement for the drive isle.

Mr. Ricci stated he wanted to make sure there wouldn't be any conflicts regarding who was responsible for it and that there is an agreement that will keep the drive isle maintained.

Mr. Lampkin wanted to make sure there would be a parking plan regarding how cars can park on the drive isle.

Mr. Zeledon stated that currently where the property line is the garage is encroaching on the property and this would be a lot line adjustment so they can keep the garage as is, because the code does not allow for parking on the drive isle and that the drive isle is only for allowing access to the back garage and the proposed parcels.

PUBLIC TESTIMONY

Project applicant Alex Espinoza stated he has worked diligently with staff to prepare this quality project to develop the homes for 4 families within the community, and he thanked the Commission for their service.

Mr. Willoughby wanted to clarify how the applicant envisions that driveway being maintained and what type of requirements will be required of the property owners.

Mr. Espinoza stated that renters currently live there and he has spoken to the property owner regarding their encroachment on this property, and that there will be a lot line adjustment and create a maintenance agreement with the 5 property owners and this will be disclosed to everyone, so everyone understands the responsibilities and very clearly mitigates any future problems that could arise.

Ms. DeDiemar stated she understands a clearly spelled out agreement but will there be monitoring of the compliance.

Mr. Espinoza stated that will be done by the property owners themselves. But the agreement will spell everything out clearly and Abide by the agreement and be good neighbors.

Ms. DeDiemar wanted to know if any residents are non-compliant will the other residents have recourse.

Mr. Espinoza stated yes there are city ordinances that would regulate it.

Mr. Lampkin wanted to know if the maintenance agreement will outline rules for parking.

Mr. Espinoza stated yes it will be very explicit that the driveway will need to be kept clear at all times for emergency purposes it will be disclosed and they will have to acknowledge it.

Mr. Mercier stated there were no members of the public wishing to speak on this item.

As there was no one else wishing to speak, Chairman Gage closed the public testimony

Mr. Gregorek stated this project is well thought out and it is gracious of them to work with the property to the north regarding the encroachment and this will add quality on the corner.

Mr. Ricci stated it looks like a good project and commended staff for their work on the project.

Mr. Lampkin stated it's good change in turning around project lots and maximizing the use of the space and enhancing the look of Euclid Ave.

Mr. Willoughby stated he echoed the other Commissioners and glad of the enhanced architectural designs facing Euclid Ave.

HISTORIC PRESERVATION PLANNING COMMISSION ACTION

Acting as the Historic Preservation Commission it was moved by Gregorek, seconded by Lampkin, to approve File No. PMTT21-001 (TPM 20328). Roll call vote: AYES, DeDiemar, Gage, Gregorek, Lampkin, Ricci, and Willoughby; NOES, none; RECUSE, none; ABSENT, none. The motion was carried 6 to 0.

- D. **ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV20-016:** A Development Plan to construct a 74-foot collocated monopine wireless communications facility (T-Mobile and Verizon) on 0.176-acre of land located at 617 East Park Street within the IL (Light Industrial) zoning district. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP) provided certain conditions are met; (APN: 1049-233-13) **submitted by Joel Taubman, Crown Castle Towers.**

This Item is requested to be continued to the June 22, 2021 Planning Commission meeting.

PUBLIC TESTIMONY

Mr. Mercier stated there were no members of the public wishing to speak on this item.

PLANNING COMMISSION ACTION

It was moved by Lampkin, seconded by DeDiemar, to continue File No. PDEV20-016 to the June 22, 2021 meeting. Roll call vote: AYES, DeDiemar, Gage, Gregorek, Lampkin, Ricci, and Willoughby; NOES, none; RECUSE, none; ABSENT, none. The motion was carried 6 to 0.

- E. **ENVIRONMENTAL ASSESSMENT, TENTATIVE TRACT MAP, AND DEVELOPMENT PLAN REVIEW FOR FILE NOS. PMTT20-003 AND PDEV20-007:** A Tentative Tract Map (File No. PMTT20-003/TT 20345) to subdivide 6.65 acres of land into 1 numbered lot for condominium purposes, 26 numbered lots for single-family dwellings and 20 lettered lots in conjunction with a Development Plan (File No.

PDEV20-007) to construct 26 detached single-family units (6-Pack Cluster) and 77 multiple family units (14-plex Courtyard Townhomes), located at the northeast corner of Ontario Ranch Road and Haven Avenue, within the within the Mixed Use District Planning Area 6A of the Rich Haven Specific Plan. Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by City Council on January 27, 2010. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 218-393-07, 218-393-06, 218-393-10, 218-393-22, 218-393-36, 218-393-38 and 218-393-39) **submitted by BrookCal Ontario LLC. and Brookfield Properties Development.**

- F. **ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT AGREEMENT REVIEW FOR FILE NO. PDA20-002:** A Development Agreement (File No. PDA20-002) between the City of Ontario and Rich Haven Marketplace LLC, to establish the terms and conditions for the development of Planning Areas 7A & 7B pursuant to the proposed Rich-Haven Specific Plan Amendment (File No. PSPA19-006), an 81.1 acre property located at the northwest corner of Hamner Avenue and Ontario Ranch Road, within the proposed Light Industrial and Regional Commercial land use districts of the Rich-Haven Specific Plan. Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by City Council on January 27, 2010. This application introduces no new significant environmental impacts, and all previously-adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport, and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP). (APNs: 0218-211-17; 0218-211-24; and 0218-211-27) **submitted by Rich Haven Marketplace, LLC. City Council action is required.**

- G. **ENVIRONMENTAL ASSESSMENT, GENERAL PLAN AMENDMENT, AND SPECIFIC PLAN AMENDMENT REVIEW FOR FILE NOS. PGPA19-005 AND PSPA19-006:** A request for approval of the following:

[1] A General Plan Amendment (File No. PGPA19-005) to modify Policy Plan (general plan) Exhibit LU-01, Policy Plan Land Use Plan, changing the land use designation on 105.4 acres of Low Density Residential (2.1 – 5 du/ac), 66.01 acres of Mixed Use (9 – NMC East) and 10.36 acres of Open Space – Non Recreation designated property, to 23.41 acres of Low Density Residential (2.1 – 5 du/ac), 24.16 acres of Low-Medium Density Residential (5.1 – 11 du/ac), 57.83 acres of Medium Density Residential (11.1 – 25 du/ac), 20.46 acres of Mixed Use (9 – NMC East), 48.61 acres of Industrial, and 7.3 acres of Open Space – Non Recreation designated property; and modify Policy Plan (general plan) Exhibit LU-03, Future Buildout, to be consistent with the herein described land use changes; and

[2] An amendment (File No. PSPA19-006) to the Rich Haven Specific Plan, which includes the following map and text revisions:

[A] Change the land use designation on 110.1 gross acres of land from Planning Area 1A – 1F (Residential - SFD), to 25.5 gross acres of Planning Area 1A (Residential - SFD), 24.5 gross acres of Planning Area 1B (Residential – SFD/SFA) and 60.6 gross acres of Planning Area 1C (Residential - SFD/SFA);

[B] Change the land use designation on 81.1 gross acres of land within Planning Area 7 (Stand-Alone Residential Overlay, Mixed-Use Overlay, Regional Commercial, and SCE Easement/Gas Easement) to, Planning Area 7A (49.4 gross acres of Light Industrial and 6.6 gross acres of Open Space – Non Recreation) and Planning Area 7B (25.1 gross acres of Regional Commercial);

[C] Change the land use designation on 4.13 acres of land within Planning Area 6A from Regional Commercial to Stand-Alone Residential Overlay.

[D] Change the land use designation on 4.13 acres of land within Planning Area 9A from Stand-Alone Residential Overlay to Regional Commercial; and

[E] Various changes to the Specific Plan development standards, exhibits, and text, to reflect the proposed land uses.

The Rich Haven Specific Plan is generally bounded by Riverside Drive, Colony High School and the SCE substation to the north, Hamner Avenue to the east, Old Edison Road to the south, and Hamner Avenue to the west. Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by City Council on January 27, 2010. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 0218-161-04, 0218-161-05, 0218-161-10, 0218-161-11, 0218-211-17, 0218-211-24, 0218-211-27, 0218-211-01 and 0218-393-10) **submitted by Rich Haven Marketplace LLC and BrookCal Ontario, LLC. City Council action is required. This item was continued from the March 23, 2021 Planning Commission meeting.**

Senior Planner Mejia, presented the staff report. She described the location and the surrounding area and Rich Haven Specific Plan boundaries and the 9 properties involved. She explained the Proposed General Plan Amendment and the land use changes and the changes in the residential density. She highlighted the Specific Plan Amendment changes for the affected areas. She explained the analysis that was completed. She explained the Development Agreement Amendment and the Key points to that amendment. She described the conceptual site plan, proposed elevations, parking plan, park land and landscape plan. She stated that staff is recommending the Planning Commission recommend approval of the Addendum and File Nos. PGPA19-005, PSPA19-006 and PDA20-002 and to approve File Nos. PMTT20-003 and PDEV20-007, pursuant to the facts and reasons contained in the staff report and attached resolutions, and subject to the conditions of approval.

Mr. Lampkin wanted to clarify that the economic feasibility study regarding commercial analysis for junior centers along Ontario Ranch Road was planning area 7A or both 7A and 7B.

Ms. Mejia stated this was referring the entire planning area which includes 7A and 7B.

Mr. Gage wanted some clarity if it is a state law regarding dwelling units being adjusted.

Ms. Mejia stated yes this is a state law.

Mr. Zeledon stated that Senate Bill 330 was put in place to preserve the housing stock capacity within the city and any change to commercial from existing stock would have to be moved to preserve the housing stock.

Mr. Gage wanted to know if the state law requires that it must be in this Specific Plan or can it be anywhere in the city.

Mr. Zeledon stated it can be moved anywhere in the city but Richland owns this property and they can work with us to create those densities. He stated other cities have created overlay areas in typical density areas to add the capacity, but for this area it is consistent with what is in Edenglen.

Mr. Gage wanted to clarify that the houses could go anywhere in the city if we had the landowners in agreement.

Mr. Zeledon stated yes, the applicant would have to secure property somewhere else and then up zone it, but it's difficult to do and this is the second time we have done this, the first being with the Ontario Business Park in the south west area but in the Palmer Meredith project we had excess units and were able to show that adjustment, which is usually difficult to do unless you own the property.

Mr. Lampkin wanted to know with the change to light industrial in planning area 7A, if Rich Haven would be responsible to do an impact study that included a trip truck study.

Mr. Zeledon stated that has already been done in the addendum and they did a traffic study, a water assessment and a noise study and the trips are being reduced, but more truck trips on Hamner but reducing the vehicle trips.

PUBLIC TESTIMONY

Mr. Craig Cristina with Richland one of the applicants, stated he is here to add any additional background or information. He stated they have been working in Ontario Ranch since 2004, and with planning area 7 with the best density and uses in the market, however how do we deal with the high voltage lines and make it feel like home for future residents, and what are the most supported uses with the intensity of the Mira Loma substation which 7A borders, and how the area has developed around it. He stated Planning area 1 looked like a better area for the density. He stated as well that the Ontario Plan states that light industrial can be used as a buffer between residential and heavy industrial.

Mr. Lampkin wanted to know during the commercial demand review why junior anchors weren't considered for the location.

Mr. Cristina stated over time as the commercial uses have been landing in the area and looked at what would be the appropriate design, as residential wouldn't work and we wanted commercial on Ontario Ranch Road, and what is built in the area and what is going to be built and how this property could help with most use of that high traffic area, created a commercial size a dimension that doesn't compete with

Mr. Lampkin wanted to know if they heard any answers from clothing stores as anchors that created an interest for 7A.

Mr. Cristina stated they didn't get that specific as to tenants but the commercial consultant reached out to grocery tenants and gym fitness facility and groups that do a large number of businesses in the area and based on the feedback the demand is going don't want the big deep shallow block, but a shallower area that is more service oriented, not larger tenants.

Mr. Willoughby stated that he looked at the property and the substation and the need for a buffer in there and have you looked at the west side of the facility and how you will buffer that with residents that will come up to Mill Creek across the street.

Mr. Cristina near term vision was area 7 and Ontario Ranch Road and with Brookfield building next to

use in 6A and the negative aspect of being next to the substation and they own planning area 5, intitled it and hesitant working in that area and aren't really focused on it right now.

Mr. Willoughby stated Industrial in that area the best buffer we can get in there and with the amount of homes and people coming in there we need a lot of good commercial and space to service their needs.

Mr. Cristina stated that looking long term we have four other commercial centers, but we want to compliment the whole host of commercial centers and adding to each other.

Mr. Derek Spalding the project manager with Brookfield spoke and thanked staff for their diligent efforts to get them to this point and is available to answer any questions.

Mr. Mercier stated there were no other members of the public wishing to speak on this item.

As there was no one else wishing to speak, Chairman Gage closed the public testimony

Mr. Gage stated he went down and looked at the area and saw the extent of the substation and this makes a whole lot of sense to not to put residential here and this is a good transition and we can use some commercial in this area and will be for it.

Mr. Lampkin stated he also went and looked at the site and this is an impossible situation to deal with and his hope is that the commercial space will bring very good commercial tenants to service the area, and as 7A is developed something nice to look at off Hamner as well as Ontario Ranch Road.

Mr. Willoughby stated he looked at the area and thinks this is the best thing for the area and when we look at the commercial the developer will look at good tenants, sit down restaurants and when it comes in we can make sure it is a really nice project, and he will be supporting this.

PLANNING COMMISSION ACTION

It was moved by Willoughby, seconded by Gregorek, to recommend adoption of a resolution to approve the Addendum, the General Plan Amendment, File No. PGPA19-005, the Specific Plan Amendment, File No. PSPA19-006, and the Development Agreement Amendment, File No., PDA20-002, subject to conditions of approval. Roll call vote: AYES, DeDiemar, Gage, Gregorek, Lampkin, Ricci, and Willoughby; NOES, none; RECUSE, none; ABSENT, none. The motion was carried 6 to 0.

It was moved by Willoughby, seconded by Ricci, to adopt a resolution to approve the Tentative Tract Map, File No., PMTT20-003, and the Development Plan, File No. PDEV20-007, subject to conditions of approval. Roll call vote: AYES, DeDiemar, Gage, Gregorek, Lampkin, Ricci, and Willoughby; NOES, none; RECUSE, none; ABSENT, none. The motion was carried 6 to 0.

MATTERS FROM THE PLANNING COMMISSION

Old Business Reports From Subcommittees

Historic Preservation (Standing): This subcommittee did not meet this month.

Development Code Review (Ad-hoc): This subcommittee did not meet.

Zoning General Plan Consistency (Ad-hoc): This subcommittee did not meet.

New Business

Mr. Gage appointed Subcommittees.

NOMINATIONS FOR SPECIAL RECOGNITION

None at this time.

DIRECTOR'S REPORT

Mr. Zeledon stated the Monthly Activity Reports for February and March are in their packets. He thanked the Commission for the discussion on the last item and for them taking their time to really look at the situation.

ADJOURNMENT

Willoughby motioned to adjourn, seconded by Gregorek. The meeting was adjourned at 8:57 PM, to the next meeting on May 25, 2021.

Secretary Pro Tempore

Chairman, Planning Commission

FILE NO: PDEV20-005

SUBJECT: A Development Plan to construct a 256,711 square foot industrial building on 11.3 acres of land located at 875 West State Street, within the IL (Light Industrial) zoning district. Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140). This application introduces no new significant environmental impacts; (APNs: 1011-161-04 and 1011-161-05) **submitted by Inland Harbor LLC.**

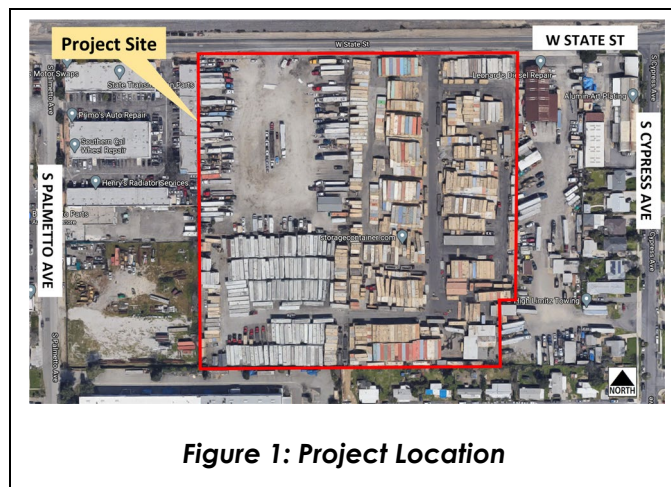
PROPERTY OWNER: Inland Harbor LLC


RECOMMENDED ACTION: That the Planning Commission consider and approve the use of an Addendum to The Ontario Plan Environmental Impact Report and approve File No. PDEV20-005, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval contained in the attached departmental reports.

PROJECT SETTING: The Project site is comprised of 11.3 acres of land located on the south side of State Street, mid-block between Palmetto and Cypress Avenues, at 875 West State Street (formerly 835 and 920 West State Street), and is depicted on Figure 1: Project Location, below. The site is currently used as a metal container and truck trailer storage yard. The site is surrounded by Union Pacific Railroad tracks to the north, a diesel repair shop to the east, an industrial building and the Grove Manor Mobile Home Park to the south, and industrial buildings to the west. The existing surrounding land uses, zoning, and general plan and specific plan land use designations are summarized in the "Surrounding Zoning & Land Uses" table located in the Technical Appendix of this report.

PROJECT ANALYSIS:

(1) Background — On March 4, 2020, the applicant filed the subject Development Plan application, requesting approval to construct a



Case Planner:	Denny D. Chen
Planning Director Approval:	
Submittal Date:	3/4/2020

Hearing Body	Date	Decision	Action
DAB	3/15/21	Approval	Recommend
PC	5/25/21		Final
CC			

256,711 square foot industrial building on the above-described Project site, with a floor area ratio ("FAR") of 0.52.

On March 15, 2021, the Development Advisory Board reviewed the subject application and recommended approval of the Development Plan application to the Planning Commission. The application was subsequently scheduled for Planning Commission review and action but was continued from the March 23, 2021 and April 27, 2021 Planning Commission meetings to allow staff additional time to complete the project's environmental review and prepare an Addendum to The Ontario Plan Environmental Impact Report. On March 22, 2021, prior to the scheduled Planning Commission meeting, the Planning Department received a letter (see Attachment B - Lozeau Drury SAFER Letter) from Richard Drury, of Lozeau Drury LLP, on behalf of the Supporters Alliance for Environmental Responsibility ("SAFER") with concerns and comments on the adequacy of the project's CEQA environmental determination that the environmental impacts of the project were previously reviewed in conjunction with The Ontario Plan EIR. In response to SAFER's concerns, staff prepared Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) to address concerns and comments raised by SAFER.

(2) Site Design/Building Layout — The proposed 256,711 square foot warehouse/distribution building is centrally located on the property, oriented north to south in a cross-dock configuration. The building includes two office areas fronting State Street, located at the northwest and northeast corners of the building, which total 13,910 square feet. The building will be setback 65 feet from the State Street (north) property line, 141 feet from the east property line, 145 feet from the west property line, and 60.5 feet from the south property line (see Exhibit B—Site Plan, attached).

There are two truck yards located on the west and east sides of the proposed building, behind the office areas, which are designed for loading activities, tractor-trailer parking, truck maneuvering, and outdoor staging. A total of 51 loading doors are proposed along the east and west sides of the warehouse building. Twenty-eight dock-high loading doors and one at-grade loading door are proposed on the west side of the building and 23 dock-high loading doors and one at-grade loading door are proposed on the east side of the building. Both truck yard areas will be screened from public street (State Street) views by a combination of building walls (office elements) and screen walls with view-obstructing access gates.

(3) Site Access/Circulation — The Project site will be accessed from three points along State Street (see Exhibit B—Site Plan, attached). Driveways located at the northwest and northeast corners of the Project site provide direct truck access to the secured yard areas. A driveway located mid-point along the street frontage provides automobile access to the site's employee and visitor parking areas.

(4) Parking — The Project has provided 139 off-street parking spaces, consistent with the "Warehouse and Distribution" parking standards specified in the Development Code. Additionally, the City's off-street parking and loading standards require that the Project

provide a minimum of one tractor trailer parking space for each four dock-high loading spaces. Fifteen tractor trailer parking spaces have been provided, exceeding the minimum requirement for the Project, as shown in Table A: Parking Summary, below.

Table A: Parking Summary						
Bldg.	Type of Use	Building Area	Trailer Parking		Vehicle Spaces	
			Required	Provided	Required	Provided
	Warehouse/ Distribution	256,711 SF	12	15	138	139
Total:					138	139

(5) Architecture — The proposed building elevations illustrate the type of high-quality architecture promoted by the Development Code's development standards and design guidelines. The building will be composed of concrete tilt-up walls that incorporate a contemporary architectural design (see Exhibit C—Building Elevations, attached). The proposed building design incorporates articulation in the building footprint, incorporating a combination of recessed and popped-out wall areas, and articulation in the building parapet/roof line, which serves to accentuate the building's entries and breaks up large expanses of building wall. To accent the building design, the following materials, finishes, and fixtures have been incorporated into the design of the building:

- Painted finishes incorporating color blocking;
- Concrete tilt-up panels with wood siding finish;
- Horizontal and vertical reveal lines;
- Recessed windows and wall panels at key locations;
- Aluminum storefronts with clear anodized mullions and blue glazing;
- Prefabricated metal canopies finished to match the storefront mullions; and
- Double glazed windows with clear anodized mullions.

(6) Landscaping — The Project proposes a 10 percent landscape coverage, meeting the minimum requirement for interior lots in the IL (Light Industrial) zoning district. The landscape pallet for the Project incorporates a mix of 24-inch, 36-inch, and 48-inch box accent and shade trees, including Afghan Pine, California Sycamore, and Coast Live Oak, along with a variety of shrubs and groundcovers (see Exhibit D—Landscape Plan). Furthermore, two outdoor employee break areas will be provided. The break areas will be located outside the east and west sides of the office areas. The break areas will feature decorative paving, patio chairs and tables, and a trellis structure with decorative posts (see Exhibit E—Employee Break Area Perspective View, attached).

(7) Utilities (drainage, sewer) — Public utilities (water and sewer) are available to serve the proposed Project. Additionally, the Applicant has submitted a Preliminary Water Quality Management Plan ("PWQMP"), which establishes the project's compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development ("LID") best management practices ("BMPs"), such as retention and infiltration, biotreatment, and evapotranspiration. The PWQMP

proposes a vegetated swale(s) designed to accept runoff from building roofs, parking lots and project roadways, which lead to an underground stormwater infiltration system, which is located along the southern portion of the Project site. Any overflow drainage will be conveyed to the surrounding street.

(8) Community Outreach — The Project site is bordered by residential properties (Grove Manor Mobile Home Park) at the southeast area of the project site and is within close proximity to residential properties to the east of Cypress Avenue. To solicit community input from the surrounding residents, a Community Meeting Notice was mailed to all property owners within a 500-foot radius of the Project site, in both English and Spanish. On March 11, 2021, the Planning Department held a community meeting, via Zoom, to inform the surrounding property owners/residents of the proposed Project, and to address any questions or concerns they may have regarding the Project. In addition to the Applicant's attendees, one member of the public attended the Zoom meeting and no concerns with the project were expressed. As of the preparation of this report, no correspondence, telephone calls, or emails requesting additional information related to the proposed Project have been received.

(9) HRA (Health Risk Assessment) — Senate Bill 1000 (SB-1000), signed into law by the Governor in 2016, requires cities and counties throughout California to include an Environmental Justice element in their General Plans. With this legislation, all cities and counties must identify "disadvantaged communities" and develop strategies to mitigate and reduce environment-related health risks by requiring applicants to provide a Health Risk Assessment ("HRA") report for a proposed development project if the project is located less than 1,000 feet away from sensitive land uses (such as residential areas).

The Project is located less than 1,000 feet away from a sensitive land use and therefore the Applicant was required to submit an HRA report, in conjunction with the subject Application (See Attachment A – Health Risk Assessment). The HRA is a comprehensive analysis to determine the potential health risks from diesel particular matter (DPM) created by the proposed project. The HRA report (Vista Environmental, November 30, 2020) prepared the Project determined that:

(a) The projects emissions would not exceed the SCAQMD (South Coast Air Quality Management District) project level health risk cancer significance threshold of 10 in one million persons; and

(b) The projects emissions would not exceed the SCAQMD project level non-cancer chronic and acute hazard index of 1.0; and

(c) The Project Site will not exceed more than 100 trucks per day and no more than 40 trucks with operating transportation refrigeration units (TRUs) per day.

Therefore, according to the HRA report, the proposed Project will have no significant health impacts on the surrounding environment.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan

(General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

(1) City Council Goals.

- Invest in the Growth and Evolution of the City's Economy
- Maintain the Current High Level of Public Safety
- Operate in a Businesslike Manner
- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)

(2) Governance.

Decision Making:

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
 - G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

(3) Policy Plan (General Plan)

Land Use Element:

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.
 - LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.
 - LU1-6 Complete Community: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).
- Goal LU2: Compatibility between a wide range of uses.
 - LU2-6: Infrastructure Compatibility: We require infrastructure to be aesthetically pleasing and in context with the community character.

Community Economics Element:

- Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.
 - CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.
 - CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.
 - CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.
 - CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Safety Element:

- Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.
 - S1-1 Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

Community Design Element:

- CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:
 - Building volume, massing, and height to provide appropriate scale and proportion;
 - A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
 - Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.
- CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural

daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.

➤ CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

➤ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➤ CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.

➤ CD2-11 Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.

➤ CD2-12 Site and Building Signage. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.

➤ CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

▪ Goal CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

➤ CD3-1 Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.

➤ CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.

- CD3-3 Building Entrances. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces.
- CD3-5 Paving. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.
- CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.
 - Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.
- CD5-1 Maintenance of Buildings and Property. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.
- CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

HOUSING ELEMENT COMPLIANCE: The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

ENVIRONMENTAL REVIEW: Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140). This application introduces no new significant environmental impacts and all mitigation measures previously adopted in conjunction with the certification of The Ontario Plan Environmental Impact Report are a condition of project approval.

CONDITIONS OF APPROVAL: See attached department reports.

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

	Existing Land Use	General Plan Designation	Zoning Designation	Specific Plan Land Use
Site	Industrial/Vacant/Open Space	Industrial	IL (Light Industrial)	N/A
North	Southern Pacific Railroad	Railroad	RC (Rail Corridor)	N/A
South	Warehouse/ Grove Manor Mobile Home Park	Business Park/ Mobile Home Park	IL (Light Industrial)/ MHP (Mobile Home Park)	N/A
East	Industrial Building	Industrial	IL (Light Industrial)	N/A
West	Industrial Buildings	Industrial	IL (Light Industrial)	N/A

General Site & Building Statistics

Item	Proposed	Min./Max. Standard	Meets Y/N
Project Area:	256,711 SF	270,725 SF (Max.)	Y
Lot/Parcel Size:	11.3 acres	10,000 SF (Min.)	Y
Floor Area Ratio:	0.52	0.55 (Max.)	Y
Building Height:	45 FT	55 FT (Max.)	Y

Off-Street Parking:

New Bldg.	Type of Use	Building Area	Trailer Parking		Vehicle Spaces	
			Required	Provided	Required	Provided
	Warehouse / Distribution	124,012 SF	12	15	138	139
Parking Totals:					138	139

Exhibit A—PROJECT LOCATION MAP

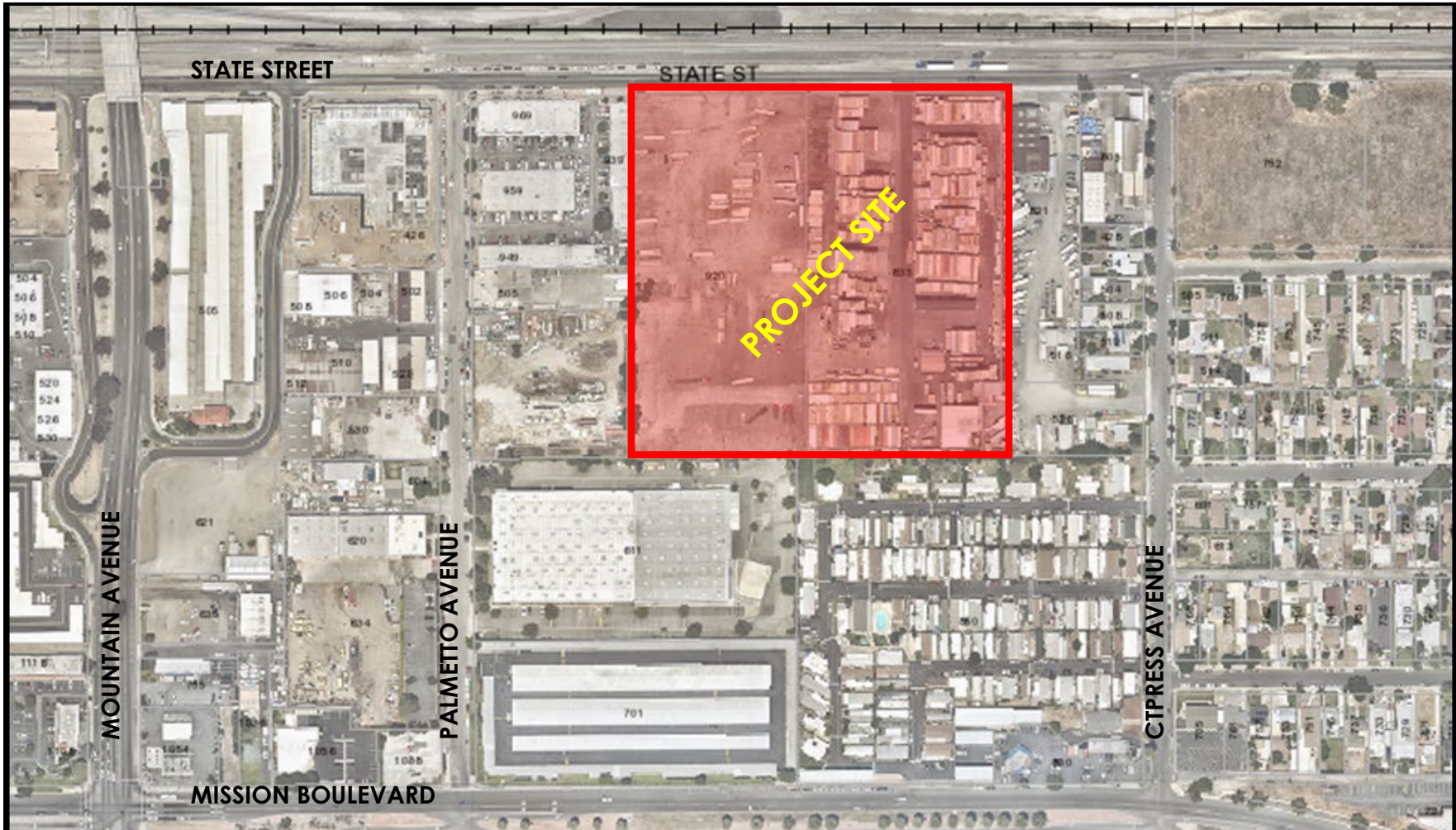


Exhibit B—SITE PLAN

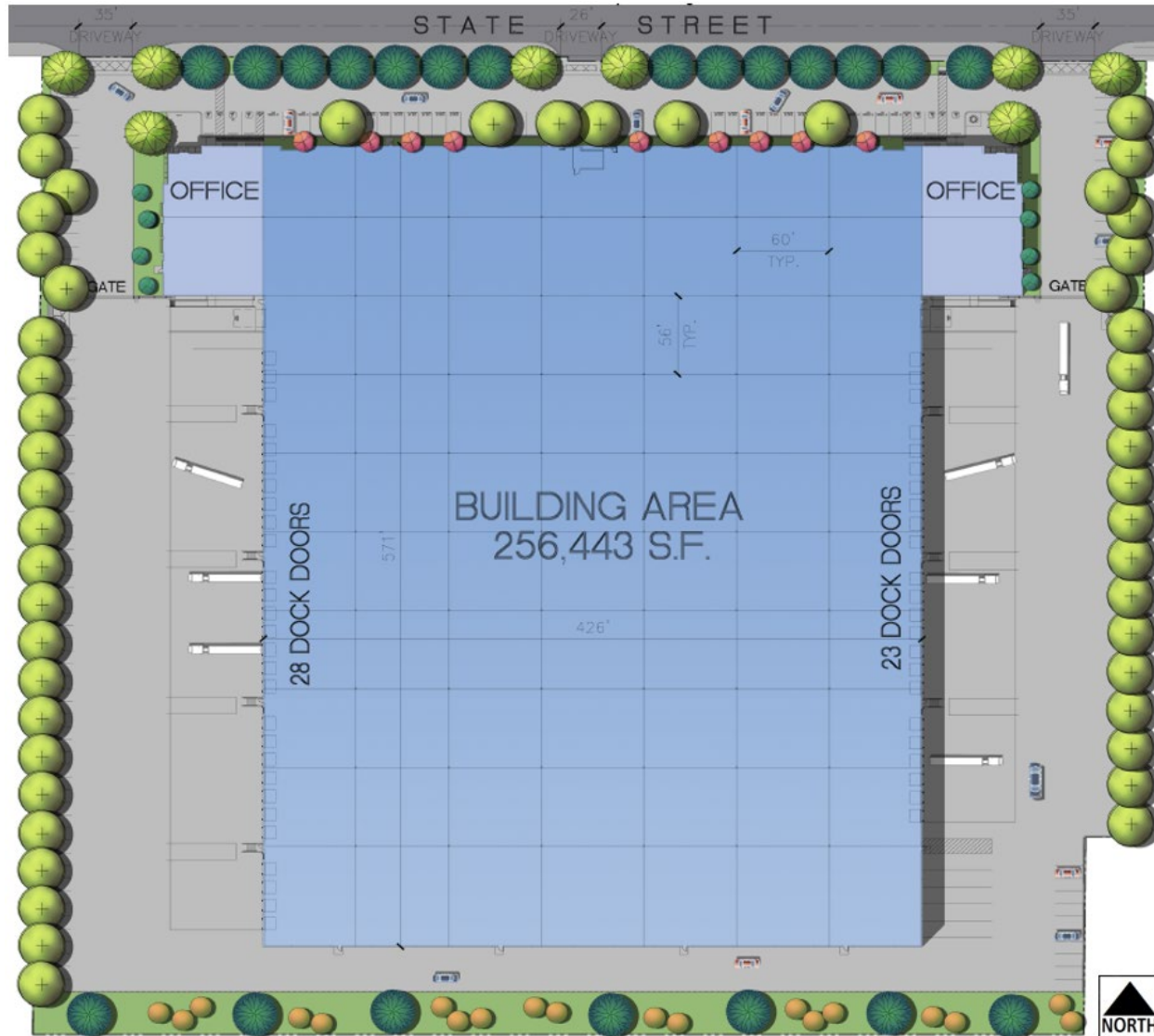


Exhibit C—BUILDING ELEVATIONS



NORTH ELEVATION



SOUTH ELEVATION

Exhibit C—BUILDING ELEVATIONS (CONTINUED)



EAST ELEVATION



WEST ELEVATION

Exhibit D—LANDSCAPE PLAN

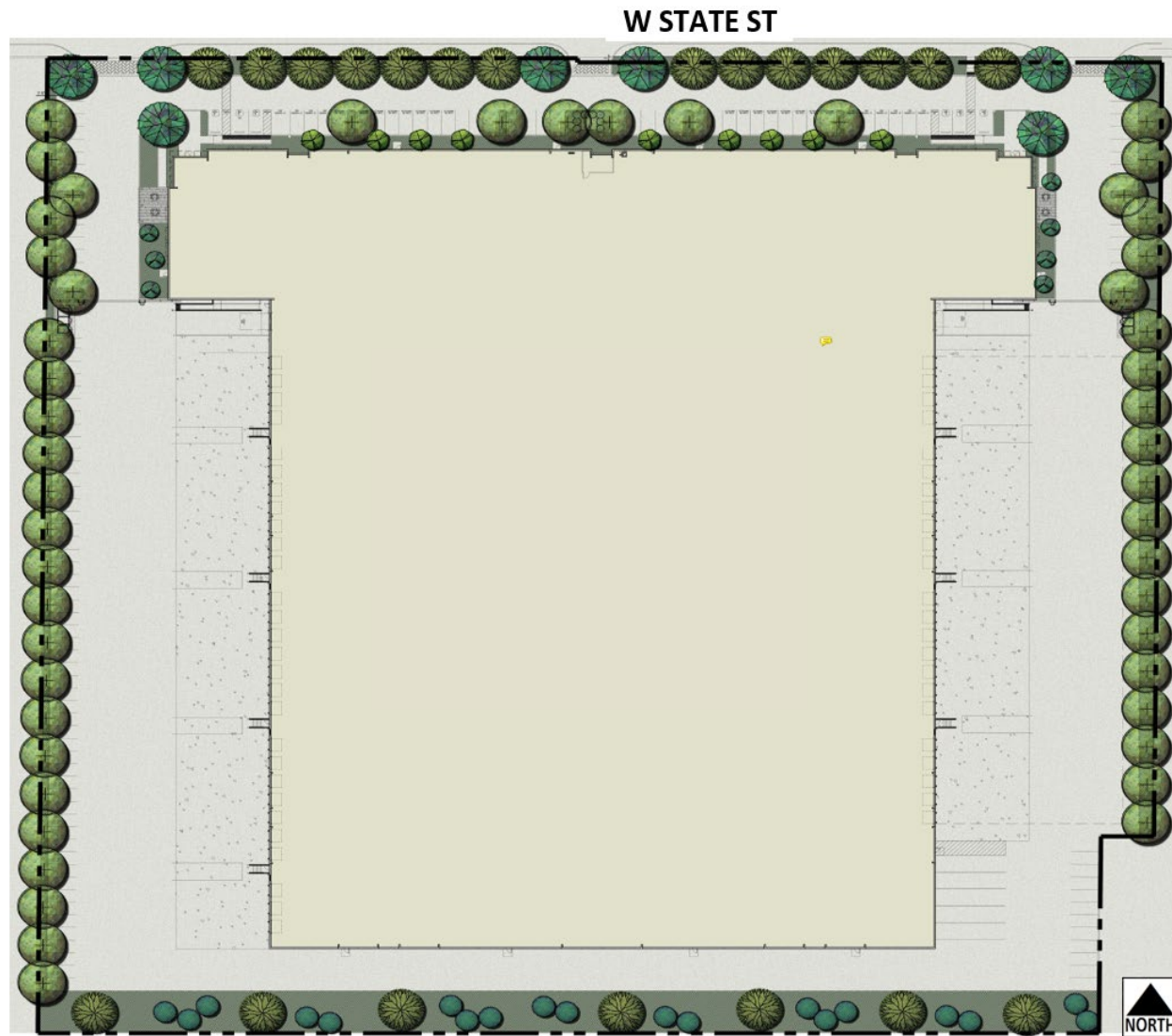


Exhibit E—EMPLOYEE BREAK AREA PERSPECTIVE VIEW



Attachment A:
Diesel Emissions Health Risk Assessment

(Document follows this page)

DIESEL EMISSIONS HEALTH RISK ASSESSMENT
STATE DISTRIBUTION CENTER PROJECT
CITY OF ONTARIO

Lead Agency:

City of Ontario
303 East B Street
Ontario, CA 91764

Prepared by:

Vista Environmental
1021 Didrickson Way
Laguna Beach, California 92651
949 510 5355
Greg Tonkovich, AICP

Project No. 20098

November 30, 2020

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Appendix B – AERMOD Model Years 2022-2024 DPM Concentrations Printouts

Appendix C – AERMOD Model Years 2024-2038 DPM Concentrations Printouts

Appendix D – AERMOD Model Years 2038-2051 DPM Concentrations Printouts

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ACRONYMS AND ABBREVIATIONS

Air Basin	South Coast Air Basin
CalEEMod	California Emissions Estimator Model
CAPCOA	California Air Pollution Control Officers Association
CARB	California Air Resources Board
CEC	California Energy Commission
CEQA	California Environmental Quality Act
City	City of Ontario
DPM	Diesel particulate matter
EPA	Environmental Protection Agency
HAP	Hazardous Air Pollutants
HRA	Health Risk Assessment
MATES	Multiple Air Toxics Exposure Study
MSAT	Mobile Source Air Toxics
NAAQS	National Ambient Air Quality Standards
PM	Particle matter
PM10	Particles that are less than 10 micrometers in diameter
PM2.5	Particles that are less than 2.5 micrometers in diameter
PPM	Parts per million
SB	Senate Bill
SCAQMD	South Coast Air Quality Management District
SCAG	Southern California Association of Governments
TAC	Toxic air contaminants

1.0 INTRODUCTION

1.1 Purpose of Analysis and Study Objectives

This Health Risk Assessment (HRA) has been completed to determine the potential health risks from diesel emissions created by the proposed State Distribution Center project (proposed project) from the impacts created by the diesel particulate matter (DPM) emissions, created from diesel trucks operating on and in the vicinity of the project site. The following is provided in this report:

- A description of the proposed project;
- A description of toxic air contaminants (TAC);
- A description of the modeling parameters utilized in this analysis; and
- An analysis of DPM concentrations created by the proposed project at the nearby residential uses and a comparison of the calculated cancer and acute non-cancer risks with applicable thresholds.

1.2 Site Location and Study Area

The project site is located in the western portion of the City of Ontario (City). The project site is currently utilized as a truck trailer storage facility and is bounded by State Street and the Union Pacific San Gabriel Line to the north, industrial uses to the east, a mobile home park and industrial uses to the south, and industrial uses to the west. The project study area is shown in Figure 1.

Sensitive Receptors in Project Vicinity

The nearest sensitive receptors to the project site is a mobile home park that is adjacent to the south side of the project site. There are also single-family homes scattered between industrial uses to the east, northeast and southwest sides of the project site. The nearest school is Kingsley Elementary School, which is located as near as 0.9-mile northwest of the project site.

1.3 Proposed Project Description

The proposed project consists of development of a 257,921 square foot warehouse distribution facility. The proposed warehouse facility is expected to be used for light industrial, light manufacturing, assembly, and/or distribution of finished goods and materials that may include a last mile delivery facility. The proposed warehouse facility would also include two truck loading areas, with one located on the west side of the warehouse and the other located on the east side of the warehouse. The proposed site plan is shown in Figure 2.

1.4 Summary of Analysis

The principal focus was on assessing the long-term health impacts from DPM created from diesel-powered truck emissions generated from operation of the proposed project. The analyses contained in this report support the following conclusions:

- The proposed project emissions would not exceed the SCAQMD project level health risk cancer significance threshold of 10 in one million persons.
- The proposed project emissions would not exceed the SCAQMD project level non-cancer chronic and acute hazard index of 1.0.



SOURCE: Google Maps. Imagery ©2020 Google, Imagery ©2020 County of San Bernardino, Maxar Technologies, U.S. Geological Survey, USDA Farm Service Agency, Map data ©2020 200 ft



Figure 1
Project Study Area

PROJECT INFORMATION

Owner / Applicant
 CSC
 591 SAN NICOLAS DRIVE, STE 16A,
 16830 BARBERA AVE, STE 100
 TEL.: 949-503-9484
 CONTACT: BRIAN CORNELL

Code Analysts
 2016 CALIFORNIA BUILDING CODE
 2016 CALIFORNIA MECHANICAL CODE
 2016 CALIFORNIA ELECTRICAL CODE
 2016 CALIFORNIA FIRE CODE
 2016 CALIFORNIA ENERGY CODE
 2016 CALIFORNIA GREEN BUILDING STANDARDS

Project Address
 100 W ALDORA,
 CARLSBAD, CA 92008

Construction Type
 CONCRETE TILT-UP BUILDING
 BUILDING OCCUPANCY: S-1 / B
 CONSTRUCTION TYPE: I-4 / B
 EIR SYSTEM
 APP
 N/A
 Zoning
 M-3 MANUFACTURED LIGHT,
 SEEN VEH(4)

SITE PLAN KEYNOTES

1. HEAVY BROOM FINISH CONCRETE FURNISHMENT.
2. REINFORCED CONCRETE (RC) FINISH.
3. CONCRETE WALKWAY, MEDIAN BROOM FINISH.
4. BROWNEY APONS TO BE CONSTRUCTED.
5. 8'-0" TO 8'-6" TYP. THICK CONCRETE EXTERIOR LANDING AND TYP. AT ALL ROOM FRANK.
6. 8'-0" TO 8'-6" TYP. THICK CONCRETE EXTERIOR LANDING AND TYP. AT ALL ROOM FRANK.
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29. 8'-0" TO 8'-6" TYP. THICK CONCRETE EXTERIOR LANDING AND TYP. AT ALL ROOM FRANK.
30. 8'-0" TO 8'-6" TYP. THICK CONCRETE EXTERIOR LANDING AND TYP. AT ALL ROOM FRANK.

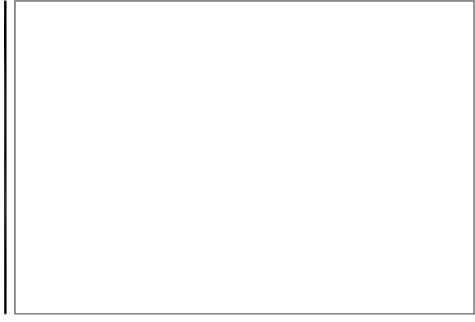
SITE PLAN GENERAL NOTES

1. THE SITE PLAN BASED ON THE SOILS REPORT PREPARED BY...
2. IF SOILS ARE EXPANSIVE IN NATURE, USE STEEL REINFORCING FOR ALL SITE CONCRETE.
3. ALL CONCRETE SHALL BE FINISHED WITH A HEAVY BROOM FINISH.
4. SEE "C" PLANS FOR ALL CONCRETE CORNERS, CUTTERS AND SWALES.
5. THE ENTIRE PROJECT SHALL BE PERMANENTLY MAINTAINED WITH AN AUTOMATIC IRRIGATION SYSTEM.
6. SEE "C" PLANS FOR ALL CONCRETE CORNERS, CUTTERS AND SWALES.
7. PROVIDE POSTING DRAINAGE AWAY FROM BLDG. SEE "C" DRAWINGS.
8. CONTINGERS TO BE 12" MINIMUM FOR ALL HORIZONTAL CONTROL DIMENSIONS. SET PLANS ARE FOR GUIDANCE AND SHARING LAYOUT POINTS.
9. SEE "C" DRAWINGS FOR FISH ORICE ELEVATIONS.
10. CONCRETE JOINTS TO BE A MINIMUM OF 4" THICK W/ TOLDED JOINTS AT EXPANSION JOINTS TO HAVE COMPRESSION EXPANSION FILLER MATERIAL OF 1/4". FINISH TO BE A HEAVY BROOM FINISH UNLESS OTHERWISE NOTED.
11. ALL CONCRETE SHALL BE FINISHED WITH A HEAVY BROOM FINISH UNLESS OTHERWISE NOTED.
12. CONSTRUCTION DOCUMENTS REFERENCING TO THE LANDSCAPE AND IRRIGATION OF THE BUILDING SHALL BE DEVELOPED PRIOR TO ISSUANCE OF BUILDING PERMITS.
13. REFER TO FINAL CITY ORDINANCE. THE LANDSCAPE ARCHITECT SHALL SUBMIT A CERTIFICATE OF COMPLETION TO PUBLIC FACILITIES DEVELOPMENT.
14. ALL LANDSCAPE AND IRRIGATION DESIGN SHALL MEET CURRENT CITY REQUIREMENTS AND BE SUBJECT TO REVIEW BY THE PUBLIC FACILITIES DEVELOPMENT.
15. LANDSCAPED AREAS SHALL BE DELINEATED WITH A MINIMUM SIX INCHES (6") HIGH, GROUND MOUNTED UTILITY STRUCTURES SUCH AS TRANSFORMERS, HANG EQUIPMENT AND BOXES FOR REGULATION VALVES SHALL BE LOCATED OUT OF VIEW FROM THE PUBLIC RIGHT-OF-WAY.
16. ALL CONCRETE SHALL BE FINISHED WITH A HEAVY BROOM FINISH UNLESS OTHERWISE NOTED.
17. ALL CONCRETE SHALL BE FINISHED WITH A HEAVY BROOM FINISH UNLESS OTHERWISE NOTED.
18. ALL CONCRETE SHALL BE FINISHED WITH A HEAVY BROOM FINISH UNLESS OTHERWISE NOTED.
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VICINITY MAP

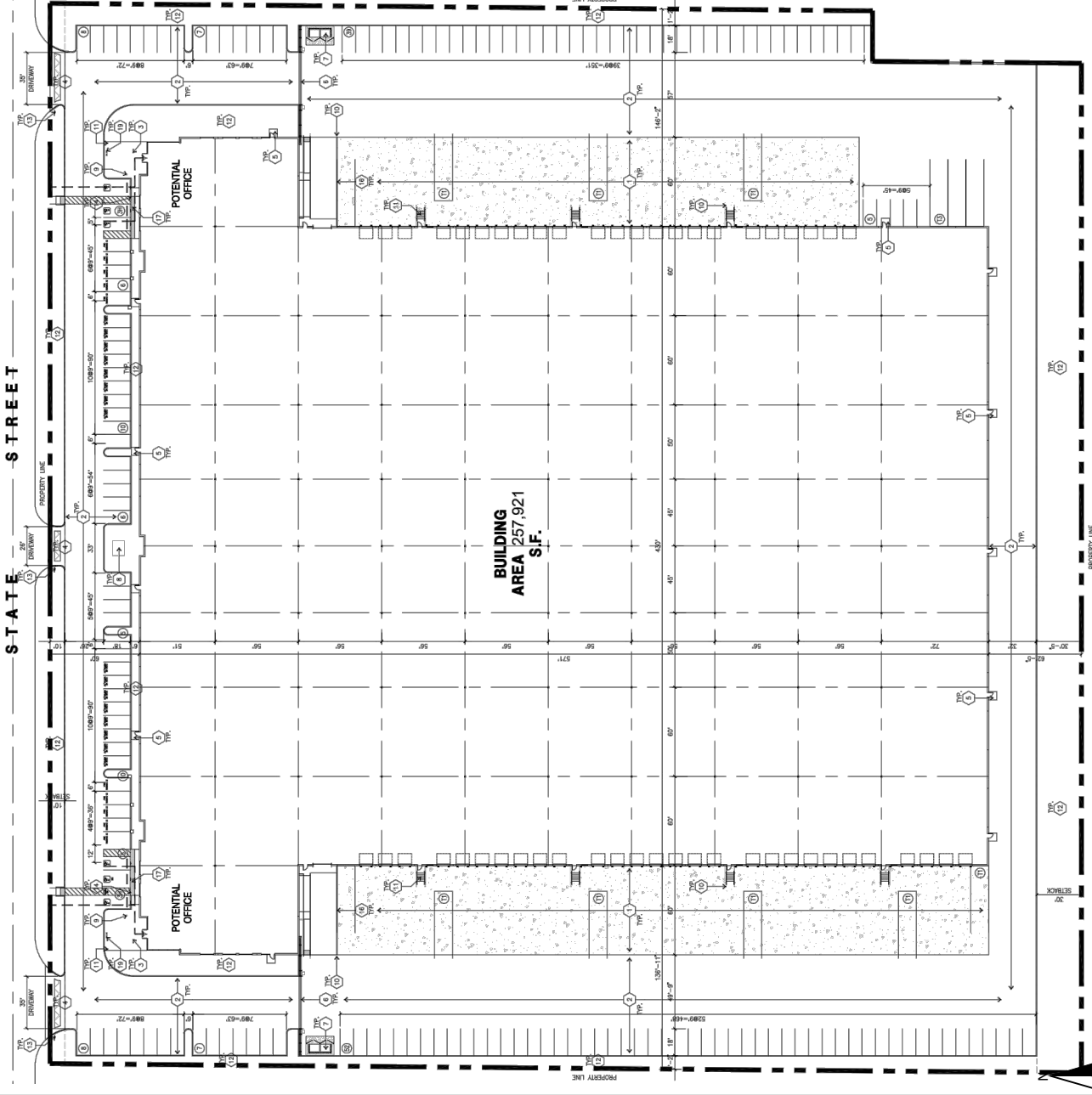


PROJECT DATA



SITE LEGEND

- 1. 2" THICK CONCRETE FURNISHMENT
- 2. 4" THICK CONCRETE FURNISHMENT
- 3. 6" THICK CONCRETE FURNISHMENT
- 4. 8" THICK CONCRETE FURNISHMENT
- 5. 10" THICK CONCRETE FURNISHMENT
- 6. 12" THICK CONCRETE FURNISHMENT
- 7. 14" THICK CONCRETE FURNISHMENT
- 8. 16" THICK CONCRETE FURNISHMENT
- 9. 18" THICK CONCRETE FURNISHMENT
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- 51. 102" THICK CONCRETE FURNISHMENT
- 52. 104" THICK CONCRETE FURNISHMENT
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- 95. 190" THICK CONCRETE FURNISHMENT
- 96. 192" THICK CONCRETE FURNISHMENT
- 97. 194" THICK CONCRETE FURNISHMENT
- 98. 196" THICK CONCRETE FURNISHMENT
- 99. 198" THICK CONCRETE FURNISHMENT
- 100. 200" THICK CONCRETE FURNISHMENT



SOURCE: HPA Architecture.

Figure 2
 Proposed Site Plan

2.0 ATMOSPHERIC SETTING

The project site is located within City of Ontario within the County of San Bernardino, which is part of the South Coast Air Basin (Air Basin) that includes the non-desert portions of Riverside, San Bernardino, and Los Angeles Counties and all of Orange County. The Air Basin is located on a coastal plain with connecting broad valleys and low hills to the east. Regionally, the Air Basin is bounded by the Pacific Ocean to the southwest and high mountains to the east forming the inland perimeter.

The climate of western San Bernardino County, technically called an interior valley subclimate of the Southern California's Mediterranean-type climate, is characterized by hot dry summers, mild moist winters with infrequent rainfall, moderate afternoon breezes, and generally fair weather. Occasional periods of strong Santa Ana winds and winter storms interrupt the otherwise mild weather pattern. The clouds and fog that form along the area's coastline rarely extend as far inland as western San Bernardino County. When morning clouds and fog form, they typically burn off quickly after sunrise. The most important weather pattern from an air quality perspective is associated with the warm season airflow across the densely populated areas located west of the project site. This airflow brings polluted air into western San Bernardino County late in the afternoon. This transport pattern creates unhealthy air quality that may extend to the project site particularly during the summer months.

Winds are an important parameter in characterizing the air quality environment of a project site because they both determine the regional pattern of air pollution transport and control the rate of dispersion near a source. Daytime winds in western San Bernardino County are usually light breezes from off the coast as air moves regionally onshore from the cool Pacific Ocean to the warm Mojave Desert interior of Southern California. These winds allow for good local mixing, but as discussed above, these coastal winds carry significant amounts of industrial and automobile air pollutants from the densely urbanized western portion of the Air Basin into the interior valleys which become trapped by the mountains that border the eastern and northern edges of the Air Basin.

In the summer, strong temperature inversions may occur that limit the vertical depth through which air pollution can be dispersed. Air pollutants concentrate because they cannot rise through the inversion layer and disperse. These inversions are more common and persistent during the summer months. Over time, sunlight produces photochemical reactions within this inversion layer that creates ozone, a particularly harmful air pollutant. Occasionally, strong thermal convections occur which allows the air pollutants to rise high enough to pass over the mountains and ultimately dilute the smog cloud.

In the winter, light nocturnal winds result mainly from the drainage of cool air off of the mountains toward the valley floor while the air aloft over the valley remains warm. This forms a type of inversion known as a radiation inversion. Such winds are characterized by stagnation and poor local mixing and trap pollutants such as automobile exhaust near their source. While these inversions may lead to air pollution "hot spots" in heavily developed coastal areas of the Air Basin, there is not enough traffic in inland valleys to cause any winter air pollution problems. Despite light wind conditions, especially at night and in the early morning, winter is generally a period of good air quality in the project vicinity. The wind rose from Ontario Airport, which is the nearest monitoring station to the project site is shown in Figure 3.

The temperature and precipitation levels for the Claremont Pomona College Monitoring station, which is the nearest weather station to the project site with historical data are shown below in Table A. Table A shows that August is typically the warmest month and December is typically the coolest month. Rainfall in the project area varies considerably in both time and space. Almost all the annual rainfall comes from

the fringes of mid-latitude storms from late November to early April, with summers being almost completely dry.

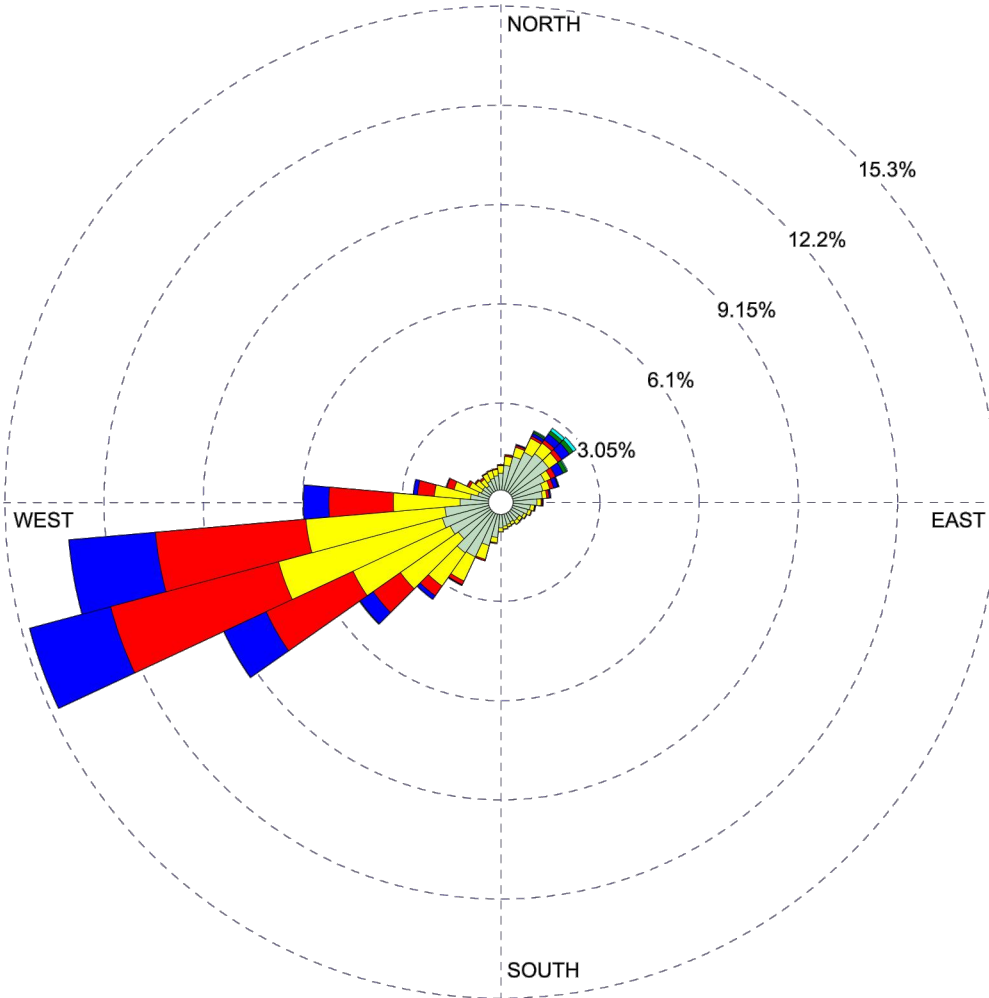
Table A – Monthly Climate Data

Month	Average Maximum Temperature (°F)	Average Minimum Temperature (°F)	Average Total Precipitation (inches)
January	63.7	38.6	3.48
February	65.7	40.5	3.40
March	67.8	42.1	2.89
April	71.6	44.9	1.32
May	75.6	48.6	0.50
June	82.9	52.5	0.06
July	90.4	57.3	0.03
August	90.3	57.7	0.11
September	87.1	55.6	0.28
October	79.8	50.2	0.72
November	72.9	44.0	1.44
December	65.8	39.8	2.72
Annual	76.1	47.7	16.95

Source: <https://wrcc.dri.edu/cgi-bin/cliMAIN.pl?ca1779>

WIND ROSE PLOT:
Station #3102

DISPLAY:
Wind Speed
Direction (blowing from)



WIND SPEED
(Knots)

- >= 21.58
- 17.11 - 21.58
- 11.08 - 17.11
- 7.00 - 11.08
- 4.08 - 7.00
- 0.97 - 4.08

Calms: 2.91%

COMMENTS:

DATA PERIOD:

Start Date: 1/1/2012 - 00:00
End Date: 12/31/2016 - 23:59

COMPANY NAME:

MODELER:

CALM WINDS:

2.91%

TOTAL COUNT:

43599 hrs.

AVG. WIND SPEED:

5.60 Knots

DATE:

11/30/2020

PROJECT NO.:

SOURCE: SCAQMD.

3.0 TOXIC AIR CONTAMINANTS

Toxic air contaminants (TACs) is a term that is defined under the California Clean Air Act and consists of the same substances that are defined as Hazardous Air Pollutants (HAPs) in the Federal Clean Air Act. There are over 700 hundred different types of TACs with varying degrees of toxicity. Sources of TACs include industrial processes such as petroleum refining and chrome plating operations, commercial operations such as gasoline stations and dry cleaners, and motor vehicle exhaust. Cars and trucks release at least 40 different toxic air contaminants. The most important of these TACs, in terms of health risk, are diesel particulates, benzene, formaldehyde, 1,3-butadiene, and acetaldehyde. Public exposure to TACs can result from emissions from normal operations as well as from accidental releases. Health effects of TACs include cancer, birth defects, neurological damage, and death.

3.1 Diesel Particulate Matter

According to *The California Almanac of Emissions and Air Quality 2013 Edition*, the majority of the estimated health risk from TACs can be attributed to relatively few compounds, the most important of which is diesel particulate matter (DPM). DPM is typically considered a subset of PM_{2.5} because the size of diesel particles are typically 2.5 microns and smaller, however this analysis considers DPM to be equal to PM₁₀, in order to provide a conservative analysis. The identification of DPM as a TAC in 1998 led the California Air Resources Board (CARB) to adopt the Risk Reduction Plan to Reduce Particulate Matter Emissions from Diesel-fueled Engines and Vehicles in September 2000. The plan's goals are a 75-percent reduction in DPM by 2010 and an 85-percent reduction by 2020 from the 2000 baseline. Diesel engines emit a complex mixture of air pollutants, composed of gaseous and solid material. The visible emissions in diesel exhaust are known as particulate matter or PM, which includes carbon particles or "soot." Diesel exhaust also contains a variety of harmful gases and over 40 other cancer-causing substances. California's identification of DPM as a toxic air contaminant was based on its potential to cause cancer, premature deaths, and other health problems. Exposure to DPM is a health hazard, particularly to children whose lungs are still developing and the elderly who may have other serious health problems. Overall, diesel engine emissions are responsible for the majority of California's potential airborne cancer risk from combustion sources. The various pollutants within DPM that also cause acute and chronic health impacts are detailed below in Table B. Table B was developed through crosschecking all diesel emissions pollutants provided in San Diego Air Pollution Control District's (SDAPCD) Diesel Fired Engines Emissions Factor Table to the list of acute and chronic reference exposure levels provided at: <http://oehha.ca.gov/air/allrels.html>.

According to the California Office of Environmental Health and Hazards Assessment (OEHHA), no acute risk has been found to be directly created from DPM, so there is no Acute Reference Exposure Level (AREL) assigned to DPM. However, as detailed in Table B, other TAC emissions associated with diesel exhaust do have an acute REL assigned to them. In order to account for the acute risk from all TAC emissions associated with diesel emissions, a hypothetical acute REL was calculated for DPM through multiplying each TAC with an acute REL to its diesel weight fraction and then adding together the results, which resulted in a hypothetical acute AREL of 137 for diesel emissions.

Table B – Diesel Emission Pollutants that Cause Acute and Chronic Health Impacts

TAC	TAC Potency Factors ($\mu\text{g}/\text{m}^3$) ¹		Percent of DPM Emission Rate ³	Target Organ Systems
	Acute REL ²	Chronic REL		
1,3-Butadiene	660	140	0.51%	Development
Acetaldehyde	470	140	1.84%	Eyes, respiratory system (sensory irritation)
Acrolein	2.5	0.35	0.08%	Eyes, respiratory system
Arsenic	0.2	0.015	0.004%	Reproductive/developmental, cardiovascular system, nervous system
Benzene	27	3	0.44%	Hematologic system, immune system, reproductive/developmental
Cadmium	--	0.02	0.004%	kidney, respiratory system
Chlorobenzene	--	1,000	0.0005%	Eyes, respiratory system
Chromium (hexavalent)	--	0.2	0.001%	Respiratory system, hematologic system
Copper	100	--	0.01%	Respiratory system
Ethyl benzene	--	5	0.03%	Liver, kidney, developmental
Formaldehyde	55	9	4.07%	Eyes, immune system, respiratory
Hexane	--	200	0.06%	Nervous system
Hydrogen Chloride	2,100	9	0.44%	Eyes, respiratory system
Manganese	--	0.09	0.01%	Nervous system
Mercury	0.6	0.03	0.005%	Reproductive/developmental
Naphthalene	--	9	0.05%	Respiratory system
Nickel	0.2	002	0.01%	Immune system, respiratory system
Propylene	--	3000	1.10%	Respiratory System
Selenium	--	20	0.01%	Liver, cardiovascular system, nervous system
Toluene	37000	300	0.25%	Nervous system, eyes, respiratory system, reproductive/developmental
Xylene	22000	700	0.10%	Eyes, nervous and respiratory systems
DPM	--	5	--	Respiratory system

Notes:

¹ Potency factors obtained from: <http://www.oehha.ca.gov/risk/ChemicalDB/index.asp>

² REL = Reference Exposure Level

³ Percentage of DPM Emission Rate calculated by dividing the pollutant's pounds per 1,000 gallons rate by the PM2.5 pounds per 1,000 gallons rate provided by the SDAPCD

Sources: SDAPCD, 2011 and OEHHA, 2014.

3.2 Asbestos

Asbestos is listed as a TAC by CARB and as a Hazardous Air Pollutant by the EPA. Asbestos occurs naturally in mineral formations and crushing or breaking these rocks, through construction or other means, can release asbestiform fibers into the air. Asbestos emissions can result from the sale or use of asbestos-containing materials, road surfacing with such materials, grading activities, and surface mining. The risk of disease is dependent upon the intensity and duration of exposure. When inhaled, asbestos fibers may remain in the lungs and with time may be linked to such diseases as asbestosis, lung cancer, and

mesothelioma. The nearest likely locations of naturally occurring asbestos, as identified in the *General Location Guide for Ultramafic Rocks in California*, prepared by the California Division of Mines and Geology, is located in Santa Barbara County. The nearest historic asbestos mine to the project site, as identified in the *Reported Historic Asbestos Mines, Historic Asbestos Prospects, and Other Natural Occurrences of Asbestos in California*, prepared by U.S. Geological Survey, is located at Asbestos Mountain, which is approximately 70 miles southeast of the project site in the San Jacinto Mountains. Due to the distance to the nearest natural occurrences of asbestos, the project site is not likely to contain asbestos.

3.3 TAC Regulatory Setting

The TACs emissions from the nearby existing uses are addressed through the efforts of various federal, state, regional, and local government agencies. These agencies work jointly, as well as individually, to reduce TACs through legislation, regulations, planning, policy-making, education, and a variety of programs. The agencies responsible for improving TACs are discussed below.

Federal and State

The United States Environmental Protection Agency (EPA) is responsible for setting and enforcing the National Ambient Air Quality Standards (NAAQS) for atmospheric pollutants. There are national standards for six common “criteria” air pollutants including ozone, nitrogen dioxide, carbon monoxide, particulate matter (PM₁₀ and PM_{2.5}), lead, and sulfur dioxide, which were identified from provisions of the Clean Air Act of 1970. California, under the California Clean Air Act, has also defined a set of health protective California Ambient Air Quality Standards (CAAQS).

Besides the “criteria” air pollutants, there is another group of substances found in ambient air referred as Hazardous Air Pollutants (HAPs) under the Federal Clean Air Act and Toxic Air Contaminants (TACs) under the California Clean Air Act. These contaminants tend to be localized to their sources and are found in relatively low concentrations in ambient air. They are regulated at the federal, state and regional levels, due to their potential of causing adverse health effects from exposure to low concentrations for long periods of time. HAPs are the air contaminants identified by the EPA as known or suspected to cause cancer, serious illness, birth defects, or death. Many of the contaminants originate from human activities, such as fuel combustion and solvent use. Mobile Source Air Toxics (MSATs) are a subset of the 188 identified HAPs. Of the 21 different HAPs that constitute the MSATs, there are six primary HAPs identified that include diesel exhaust, benzene, formaldehyde, acetaldehyde, acrolein, and 1, 3-butadiene. While vehicle miles traveled in the United States is anticipated to increase by 64 percent between 2000 and 2020, emissions of MSATs are anticipated to decrease between 57 and 67 percent as a result of efforts to control mobile source emissions.

The CARB Statewide comprehensive air toxics program was established in the early 1980s. The TAC Identification and Control Act (Assembly Bill 1807, Tanner 1983 [AB 1807]) created California’s program to reduce exposure to air toxics. The Air Toxics “Hot Spots” Information and Assessment Act (Assembly Bill 2588, Connelly 1987 [AB 2588]) supplements the AB 1807 program by requiring a statewide air toxics inventory, notification of people exposed to a significant health risk, and facility plans to reduce these risks.

AB 1807, requires the CARB to identify and control TACs. In selecting substances, the CARB must consider “the risk of harm to the public health, amount or potential amount of emissions, manner of, and exposure to, usage of the substance in California, persistence in the atmosphere, and ambient concentrations in

the community.” AB 1807 also requires the CARB to use available information gathered from the AB 2588 program to include in the prioritization of compounds. In 1992, the Hot Spots Act was amended by Senate Bill 1731, to require facilities that pose a significant health risk to reduce their risk through a risk management plan.

In 2000, the CARB approved a comprehensive Diesel Risk Reduction Plan to reduce diesel emissions from both new and existing diesel-fueled engines and vehicles. The goal of the plan is to reduce PM emissions and the associated health risks by 75 percent by 2010 and 85 percent by 2020. The plan provides a roadmap that identifies steps CARB will be taking to develop specific regulations to reduce diesel particulate matter (DPM) emissions.

As a result of controls on motor vehicles, fuels, stationary sources, and consumer products, the public’s exposure to air toxics has decreased dramatically. Between the early 1990’s and today, the decrease in statewide average health risk ranged from approximately 20 percent from formaldehyde to approximately 90 for perchlorethylene. 1,3-butadiene and benzene have also seen significant decreases of 80 to 85 percent as a result of CARB’s mobile source control program. In addition dioxins have been reduced by 99 percent in that time period, however that is primarily due to CARB’s restrictions on medical waste incinerators.

Executive Order N-79-20

The California Governor issued Executive Order N-79-20 on September 23, 2020 that requires all new passenger cars and trucks and commercial drayage trucks sold in California to be zero-emissions by the year 2035 and all medium- heavy-duty vehicles (commercial trucks) sold in the state to be zero-emission by 2045 for all operations where feasible. Executive Order N-79-20 also requires all off-road vehicles and equipment to transition to 100 percent zero-emission equipment, where feasible by 2035.

CCR Title 13, Section 2025 - On-Road Diesel Truck Fleets

On December 12, 2008 the CARB adopted Resolution 08-43, which limits NOx, PM10 and PM2.5 emissions from on-road diesel truck fleets that operate in California. On October 12, 2009 Executive Order R-09-010 was adopted that codified Resolution 08-43 into Section 2025, title 13 of the California Code of Regulations. This regulation requires that by the year 2023 all commercial diesel trucks that operate in California shall meet model year 2010 (Tier 4 Final) or latter emission standards. In the interim period, this regulation provides annual interim targets for fleet owners to meet. By January 1, 2020, 90 percent of a truck fleet is required to have installed Best Available Control Technology (BACT) for NOx emissions and 100 percent of a truck fleet installed BACT for PM10 emissions. This regulation also provides a few exemptions including a onetime per year 3-day pass for trucks registered outside of California and a low use exemption for trucks that operate less than 1,000 miles per year or less than 100 hours, if the truck operates in a stationary mode. As of January 1, 2020, the DMV will no longer register any vehicles that do not meet the Section 2025, title 13 requirements. All on-road diesel trucks utilized during construction of the proposed project will be required to comply with Resolution 08-43.

CCR Title 13, Section 2485 - Commercial Vehicle Idling and Auxiliary Power Systems

On October 20, 2005 the CARB approved regulatory measures including the adoption of Title 13, Chapter 9, Article 8, Section 2485 of the California Code of Regulations (CCR) (Section 2485), which regulates idling activities and auxiliary power systems (APS) in commercial vehicle vehicles with a vehicle weight rating of greater than 10,000 pounds. On December 5, 2014, the Office of Administrative Law (OAL) approved new

Amendments Section 2485, which became effective on January 1, 2015, and now all APS systems operated in California are required to meet the model year 2007 or newer emissions standards and all new APS systems are required to meet the Tier 4f emission standards and by 2023 all APS systems operating in California will be required to meet the Tier 4f emissions standards. Section 2485 also restricts vehicle idling to no more than five minutes at any one location and restricts the operation of an APS to no more than five minutes in any location within 100 feet of a sensitive receptor.

Regional

The South Coast Air Quality Management District (SCAQMD) is the agency principally responsible for comprehensive air pollution control for the South Coast Air Basin (Air Basin). The SCAQMD is responsible for regulating emissions primarily from stationary sources and certain areawide and indirect sources, but has no authority over motor vehicle emissions and other non-stationary sources of TAC emissions. To that end, as a regional agency, the SCAQMD works directly with the Southern California Association of Governments (SCAG), county transportation commissions, and local governments and cooperates actively with all federal and state agencies. The SCAQMD with coordination of SCAG is also responsible for developing, updating and implementing the Air Quality Plans for the Air Basin. In addition, the SCAQMD has prepared the *Health Risk Assessment Guidance for Analyzing Cancer Risks from Mobile Source Diesel Idling Emissions for CEQA Air Quality Analysis*, August 2003, which sets forth recommended thresholds of significance, analysis methodologies, and provides guidance on mitigating significant TAC impacts.

In order to determine the Air Basin-wide risks associated with major airborne carcinogens, the SCAQMD conducted the Multiple Air Toxics Exposure Study (MATES) studies. According to the SCAQMD's MATES-IV Interactive Map¹, the project site has an estimated cancer risk of: **1,196 per million persons** chance of cancer from exposure to TAC emissions. In comparison, the average cancer risk for the Air Basin is 991 per million persons, which is based on the use of age-sensitivity factors detailed in the OEHHA Guidelines (OEHHA, 2015). It should be noted that the cancer risks shown in the *Mates-IV Final Report*, prepared May 2015, found that the average cancer risk in the Basin is 367 per million does not align with the values shown in the Interactive Map, which has been updated since the Final Mates-IV Report was released to account for the cancer risk methodology revisions provided in the 2015 OEHHA Guidelines.

In order to provide a perspective of risk, it is often estimated that the incidence in cancer over a lifetime for the U.S. population ranges between 1 in 4 to 1 in 3, or a risk of about 300,000 per million persons. The MATES-III study referenced a Harvard Report on Cancer Prevention, which estimated that of cancers associated with known risk factors, about 30 percent were related to tobacco, about 30 percent were related to diet and obesity, and about 2 percent were associated with environmental pollution related exposures that includes hazardous air pollutants.

1 The Mates-IV Interactive Map is provided at: <https://scaqmd-online.maps.arcgis.com/apps/webappviewer/index.html?id=470c30bc6daf4ef6a43f0082973ff45f>

4.0 MODELING PARAMETERS AND ASSUMPTIONS

The dispersion modeling utilized for analyzing the TAC emissions in this analysis has been based on the recommended methodology described in *Health Risk Assessment Guidance for Analyzing Cancer Risks from Mobile Source Diesel idling Emissions for CEQA Air Quality Analysis* (SCAQMD HRA Guidance), prepared by SCAQMD, 2003, *Air Toxics Hot Spots Program Risk Assessment Guidelines* (OEHHA Guidelines), prepared by Office of Environmental Health Hazard, February 2015, and *Risk Assessment Procedures for Rules 1401, 1401.1 and 212* (SCAQMD Risk Assessment Procedures), prepared by SCAQMD, September 1, 2017. Important issues that affect the dispersion modeling include the following: 1) Model Selection, 2) Source Treatment, 3) Meteorological Data, and 4) Receptor Grid. Each of these issues is addressed below

4.1 Model Selection

The AERMOD View Version 9.9.0 Model was used for all dispersion modeling. Key dispersion modeling options selected included the regulatory default options and urban modeling option for San Bernardino County with a population of 2,035,210. Flagpole receptor height was set to 0 meters, which is based on SCAQMD recommended modeling parameters for AERMAP (the terrain pre-processor for AERMOD) was run with a USGS DEM 1 degree map for West San Bernardino County.

Meteorological Data

Meteorological data from the SCAQMD's Ontario Airport monitoring site was selected for this modeling application, since this is the nearest monitoring site with meteorological data available to the project site. Five full years of sequential meteorological data were collected at the Ontario Airport Station by the SCAQMD for years 2012 to 2016. The SCAQMD processed the data for input to the model. An elevation of 289 meters was utilized for the Ontario Airport Station per SCAQMD.

Receptor Grid

The nearest sensitive receptors to the project site is a mobile home park that is adjacent to the south side of the project site. There are also single-family homes scattered between industrial uses to the east, northeast and southwest sides of the project site. Discrete receptors were placed at nine (9) representative nearby homes. Figure 4 shows the locations of the sources and receptors modeled in the AERMOD model for TAC emissions.

4.2 EMFAC2017 Model

The truck travel and truck idling emission rates were obtained from the EMFAC2017 model Version 1.0.3. The EMFAC2017 model is the latest emissions inventory model released by CARB that calculates motor vehicle emissions from vehicles operating on roads in California. The EMFAC2017 includes the latest data on California's car and truck fleets and travel activity and also reflects the emissions reductions associated with CARB's recent rulemaking, including on-road diesel fleet rules, Advanced Clean Car Standards, and the Smartway/Phase I Heavy-Duty Vehicle GHG Regulations.

The operational tractor-trailer truck trips were modeled in the EMFAC2017 model through use of the Truck 2 Vehicle Category that covers all truck classifications over 14,000 pounds and would represent all of the 4+-axle truck trips. The operational small truck (2-axle and 3-axle) trips were modeled in the EMFAC2017 model through use of the Truck 1 Vehicle Category that covers all truck classifications between 8,500 and 14,000 pounds. Since vehicle emission factors are dependent on vehicle speed,

emission factors were obtained for 10 and 35 miles per hour and idling rates. The EMFAC2017 model run printout is provided in Appendix A.

The cancer risk analysis is based on a 30-year analysis period. Therefore, the analysis period was segmented into three age sensitivity time periods, plus the construction time period, consistent with the cancer risk estimation methodology. The DPM PM10 truck running emission rates utilized in this assessment are shown in Table C; the DPM PM10 truck idling emission rates utilized in this assessment are shown in Table D.

Table C – EMFAC2017 Diesel Truck Running PM10 Emission Rates

Vehicle Class	Speed (mph)	EMFAC2017 PM10 Running Emissions Rates (grams/mile)		
		2022 to 2024	2024 to 2038	2038 to 2051
Truck 1	10	0.0273	0.0184	0.0118
	35	0.0099	0.0073	0.0052
Truck 2	10	0.0200	0.0084	0.0077
	35	0.0116	0.0066	0.0064

Source: EMFAC2017 version 1.0.3.

Table D – EMFAC2017 Diesel Truck Idling PM10 Emission Rates

Vehicle Class	EMFAC2017 PM10 Idling Emissions Rates (grams/hour)		
	2022 to 2024	2024 to 2038	2038 to 2051
Truck 1	0.460	0.492	0.510
Truck 2	0.020	0.013	0.010

Source: EMFAC2017 version 1.0.3.

4.3 TAC Emission Sources

Operational DPM emissions would be generated from truck trips generated by the operation of the proposed project as a distribution center. According to Linscott, Law & Greenspan, the proposed project would generate 209 daily truck trips, of which 35 would be 2-axle, 42 would be 3-axle, and 132 would be 4+-axle truck trips. The project-related truck emissions have been analyzed separately for truck travel and truck idling that utilized emission rates from the EMFAC model.

Operational Truck Travel

Per SCAQMD HRA modeling guidance, the truck travel has been modeled onsite and a quarter mile offsite. It is anticipated half of the truck trips would travel west on State Street to Mountain Avenue and the other half of truck trips would travel east on State Street to San Antonio Avenue.

The emission rates utilized in the AERMOD model were calculated by converting the emissions created for one truck to grams per second and then calculating the time it takes to travel the road length and multiplying this time by the per day and then dividing by 24 hours. The calculated emission rates are shown in Table E. The diesel truck line volume source truck routes were modeled with a 6-foot height and 12-foot width for the onsite roads and a 20-foot width on State Street.

Table E – AERMOD Model Operational DPM Truck Travel Emissions Sources

Source ID	Description	Daily Truck Trips ¹	Length of Truck Route (meters)	DPM Emission Rates (grams/second)		
				2022-2024	2024-2038	2038-2051
Onsite Driveways						
	2-axle and 3-axle Truck Trips	39	194	1.46E-06	9.88E-07	6.34E-07
	4-axle Truck Trips	66	194	1.84E-06	7.72E-07	7.11E-07
RDONW	Onsite West Driveway	105	--	3.31E-06	1.76E-06	1.35E-06
	2-axle and 3-axle Truck Trips	39	168	1.27E-06	8.56E-07	5.49E-07
	4-axle Truck Trips	66	168	1.59E-06	6.68E-07	6.16E-07
RDONE	Onsite East Driveway	105	--	2.86E-06	1.52E-06	1.16E-06
Offsite Roads						
	2-axle and 3-axle Truck Trips	39	476	1.31E-06	9.66E-07	6.88E-07
	4-axle Truck Trips	66	476	2.61E-06	1.50E-06	1.45E-06
RDSTATEW	State Street west of Project Site	105	--	3.92E-06	2.47E-06	2.14E-06
	2-axle and 3-axle Truck Trips	39	350	9.62E-07	7.10E-07	5.06E-07
	4-axle Truck Trips	66	350	1.92E-06	1.10E-06	1.07E-06
RDSTATEE	State Street east of Project Site	105	--	2.88E-06	1.81E-06	1.57E-06

Notes:

¹ Daily truck trips represent one-way trips (i.e., entering the project site or leaving the project site equal one trip).

Source: Linscott, Law & Greenspan, 2020.

Onsite Truck Idling

The onsite diesel truck idling emissions were modeled as a point source located in the center of the loading area on the west side of the proposed warehouse. The analysis was based on each truck delivery idling on the project site for 15 minutes or 5 minutes for arriving to the loading area, 5 minutes for leaving the loading area, and 5 minutes for queueing activities at the loading area. The 5-minute period is based on Section 2485 of the California Code of Regulations that limits commercial truck idling to 5 minutes at any location.

The idling point source was modeled in the AERMOD model with a 12.6-foot height, a 0.1-meter diameter, a velocity of 50 meters per second, and a temperature of 366°K. The idling point source emission rates entered into the AERMOD model are shown in Table F. The idling source emissions were determined by multiplying 15 minutes by the daily truck operations and dividing it by 24 hours in order to determine the percent of daily idling time. The daily idling time was then multiplied by the EMFAC2017 emissions rates that are detailed above and were converted to grams per second.

Table F – AERMOD Model Operational DPM Truck Idling Emissions Sources

Source ID	Description	Daily Truck Deliveries ¹	DPM Emission Rates (grams/second)		
			2022-2024	2024-2038	2038-2051
	2-axle and 3-axle Truck Idling	19	2.56E-05	2.74E-05	2.94E-05
	4-axle Truck Idling	33	1.96E-06	1.20E-06	9.79E-07
IDLINGW	Total Idling West Loading Dock	52	2.76E-05	2.86E-05	2.94E-05
	2-axle and 3-axle Truck Idling	19	2.56E-05	2.74E-05	2.94E-05
	4-axle Truck Idling	33	1.96E-06	1.20E-06	9.79E-07
IDLINGE	Total Idling East Loading Dock	52	2.76E-05	2.86E-05	2.94E-05

Notes:

¹ Each daily truck delivery represent two trips (i.e., one entering the project site and one leaving the project site).

Source: EMFAC2017; Linscott, Law & Greenspan, 2020.

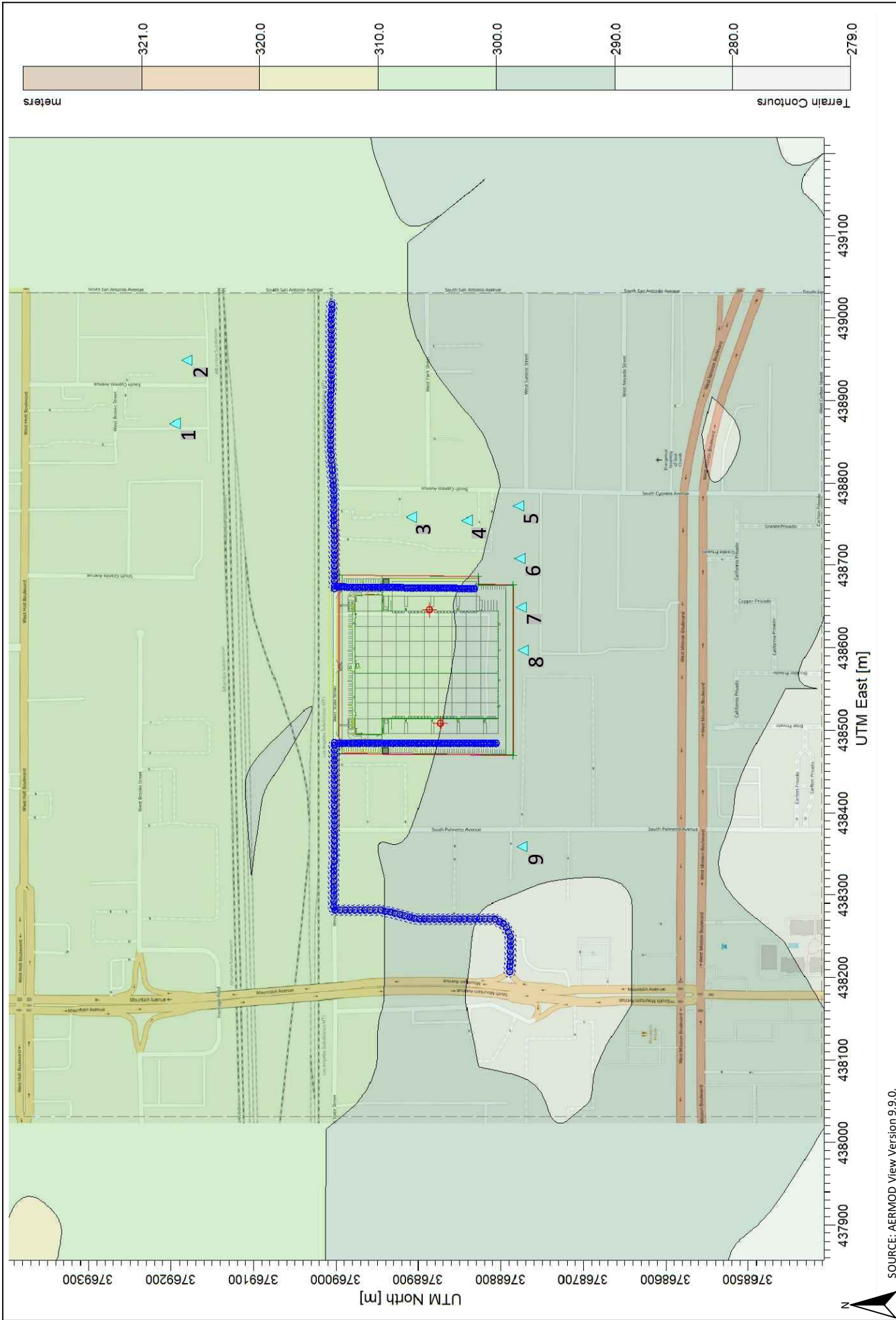


Figure 3
AERMOD Model Sources and Receptors Placement



5.0 THRESHOLDS OF SIGNIFICANCE

Any project with the potential to expose sensitive receptors or the general public to substantial levels of TACs would be deemed to have a potentially significant impact. A health risk is the probability that exposure to a TAC under a given set of conditions will result in an adverse health effect. The health risk is affected by several factors, such as the amount, toxicity, and concentration of the contaminant; meteorological conditions; distance from the emission sources to people; the distance between emission sources; the age, health, and lifestyle of the people living or working at a location; and the length of exposure to the toxic air contaminant.

The term “risk” usually refers to the chance of contracting cancer as a result of an exposure, and it is expressed as a probability: chances-in-a-million. The values expressed for cancer risk do not predict actual cases that will result from exposure to toxic air contaminants. Rather, they state a probability of contracting cancer over and above the background level and over a given exposure to toxic air contaminants.

According to the SCAQMD CEQA Handbook, any project that has the potential to expose the public to toxic air contaminants in excess of the following thresholds would be considered to have a significant air quality impact:

- If the Maximum Incremental Cancer Risk is 10 in one million or greater; or
- Toxic air contaminants from the proposed project would result in a Hazard Index increase of 1 or greater.

6.0 PROJECT IMPACTS

The proposed project would consist of the development of a warehouse distribution facility. Operation of the proposed project would generate diesel truck emissions, which are a known sources of TAC emissions. The TAC impacts to the nearby sensitive receptors have been analyzed through use of the AERMOD model and the model input parameters detailed above in Section 4.0.

Health risks from TACs are twofold. First, TACs are carcinogens according to the State of California. Second, short-term acute and long-term chronic exposure to TACs can cause health effects to the respiratory system. Each of these health risks is discussed below.

6.1 Cancer Risks

According to the OEHHA Guidance (OEHHA, 2015) and *Risk Assessment Procedures for Rules 1401, 1401.1 and 212*, (SCAQMD, 2017), the cancer risk should be calculated using the following formula:

Cancer Risk = [Dose-inh (mg/(Kg-day))] * [Cancer Potency Factor (kg-day)/mg]*[1x10⁶] * Age Sensitivity Factor * Fraction of Time at Home

$$\text{Dose-inh} = (C_{\text{air}} * \text{DBR} * A * \text{EF} * \text{ED} * 10^6) / \text{AT}$$

Where:

C_{air} [Concentration in air (µg/m³)] = (Calculated by AERMOD Model)

DBR [Daily breathing rate (L/kg body weight – day)]

A [Inhalation absorption factor]

EF [Exposure frequency (days/year)]

ED [Exposure duration (years)]

10⁶ [Micrograms to milligrams conversion]

AT [Average time period over which exposure is averaged in days]

The cancer risk parameters used in this evaluation for the nearby residential uses are shown in Table G.

Table G – Cancer Risk Calculation Parameters

Parameter	Cancer Risks at Nearby Residential Uses		
	2022 – 2024 (3 rd Trimester to 2 years)	2024 – 2038 (2 to 16 years)	2038 – 2051 (16 to 30 years)
Cancer Potency Factor (mg/kg-day) for DPM	1.1	1.1	1.1
Daily Breathing Rate (L/kg body weight-day)	1,009 ⁽¹⁾	572	261
Inhalation Absorption Factor	1	1	1
Exposure Frequency (days/year)	350	350	350
Exposure Duration (years)	2.25	14	13.75
Age Sensitivity Factor	10	3	1
Fraction of Time at Home	1.0	1.0	1.0
Averaging Time ² (days)	25,550	25,550	25,550
Potential Cancer Risk =	C _{air} * 342	C _{air} * 362	C _{air} * 39.5

Notes:

¹ Based on 95th percentile breathing rate of 361 for 3rd trimester for 3 months and 1,090 for 0 to 2 years for 24 months (OEHHA, 2015; SCAQMD, 2017).

² Based on a 70-year average lifetime (OEHHA, 2015; SCAQMD, 2017)

Table H provides a summary of the calculated diesel emission concentrations at the nearest sensitive receptors and Appendices B, C, and D provide the AERMOD printouts.

Table H – TAC Cancer Risks at Nearby Sensitive Receptors

Sensitive Receptor ¹	Receptor Location		Annual PM10 Concentration (µg/m ³)			Cancer Risk Per Million People ²
	X	Y	2022-2023	2023- 2038	2038-2051	
1	438,872	3,769,195	0.0004	0.0004	0.0004	0.3
2	438,949	3,769,181	0.0005	0.0005	0.0005	0.3
3	438,759	3,768,908	0.0036	0.0035	0.0035	2.6
4	438,755	3,768,841	0.0016	0.0015	0.0015	1.1
5	438,772	3,768,779	0.0008	0.0007	0.0007	0.5
6	438,708	3,768,777	0.0009	0.0009	0.0009	0.7
7	438,649	3,768,776	0.0011	0.0010	0.0010	0.8
8	438,597	3,768,773	0.0012	0.0012	0.0012	0.9
9	438,359	3,768,775	0.0008	0.0007	0.0007	0.6
Threshold of Significance						10
Exceed Threshold?						No

Notes:

¹ The locations of each Sensitive Receptor are shown above in Figure 4.

² The residential cancer risk based on: $C_{air} (2022-2023) * 342 + C_{air} (2023-2038) * 362 + C_{air} (2038-2051) * 39.5$.

Source: Calculated from ISC-AERMOD View Version 9.9.0.

Table H shows that the cancer risk from the proposed project’s TAC emissions would be as high as **2.6 per million persons** at the single-family homes located east of the project site (Sensitive Receptor 3). The project-related cancer risk from TAC emissions would be within the SCAQMD’s threshold of 10 per million persons. Therefore, operation of the proposed project would result in a less than significant impact due to the cancer risk from TAC emissions.

6.2 Non-Cancer Risks

In addition to the cancer risk from exposure to TAC emissions there is also the potential TAC exposure may result in adverse health impacts from acute and chronic illnesses, which are detailed below.

Chronic Health Impacts

Chronic health effects are characterized by prolonged or repeated exposure to a TAC over many days, months, or years. Symptoms from chronic health impacts may not be immediately apparent and are often irreversible. The chronic hazard index is based on the most impacted sensitive receptor from the proposed project and is calculated from the annual average concentrations of PM10. The relationship for non-cancer chronic health effects is given by the equation:

$$HI_{DPM} = C_{DPM} / REL_{DPM}$$

Where,

HI_{DPM} = Hazard Index; an expression of the potential for non-cancer health effects.

C_{DPM} = Annual average diesel particulate matter concentration in $\mu\text{g}/\text{m}^3$.

REL_{DPM} = Reference Exposure Level (REL) for diesel particulate matter; the diesel particulate matter concentration at which no adverse health effects are anticipated.

The REL_{DPM} is $5 \mu\text{g}/\text{m}^3$. The Office of Environmental Health Hazard Assessment as protective for the respiratory system has established this concentration. As shown above in Table H, the AERMOD model found that the highest annual DPM concentration of $0.0043 \mu\text{g}/\text{m}^3$ for DPM chronic non-cancer risk emissions. The resulting Hazard Index is:

$$HI_{DPM} = 0.0043 / 5 = 0.00086$$

The criterion for significance is a Chronic Hazard Index increase of 1.0 or greater, which is detailed above in Section 5.0. Therefore, the non-cancer chronic health risks to residents at the proposed homes would be within the SCAQMD standards.

Acute Health Impacts

Acute health effects are characterized by sudden and severe exposure and rapid absorption of a TAC. Normally, a single large exposure is involved. Acute health effects are often treatable and reversible. The acute hazard index is calculated from the maximum 24-hour concentrations of PM10 at the point of maximum impact (PMI), which has been calculated with the AERMOD model and the parameters detailed above in Section 4.0 and Appendix B provides the AERMOD output of the 24-hour concentrations. The relationship for non-cancer acute health effects is given by the equation:

$$AHI = C / AREL$$

Where,

AHI = Acute Hazard Index; an expression of the potential for non-cancer health effects.

C = Maximum hourly concentration of either PM10 in $\mu\text{g}/\text{m}^3$.

AREL = Acute Reference Exposure Level.

No acute risk has been found to be directly created from DPM, so there is no AREL assigned to DPM, however in order to provide an DPM equivalent AREL, the ARELs from all of the other TACs that are emitted in diesel exhaust were added together based on their diesel weighting shown above in Table B. This resulted in a diesel emission weighted equivalent AREL of $137 \mu\text{g}/\text{m}^3$. The AERMOD model found that the highest 24-hour concentration at the PMI is $0.00814 \mu\text{g}/\text{m}^3$ for DPM equivalent acute non-cancer risk emissions. The resulting Hazard Index is:

$$AHI = 0.00814 / 137 = 0.00163$$

The criterion for significance is an Acute Hazard Index increase of 1.0 or greater, which is detailed above in Section 5.0. Therefore, the non-cancer acute health risks to the residents at the proposed homes would be within the SCAQMD standards.

7.0 REFERENCES

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APPENDIX A

EMFAC2017 Model Printouts

EMFAC2017 Version 1.0.3

calendar_season	mc	sub_area	vehicle_cla	tempe	relative_process	speed	pollutant	emission_rate
2022	Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.028742
2022	Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.01033
2022	Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.041512
2022	Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.020946
2022	Annual	San Bernar	Truck1		IDLEX		PM10	0.457468
2022	Annual	San Bernar	Truck1		PMTW		PM10	0.010036
2022	Annual	San Bernar	Truck1		PMBW		PM10	0.07915
2022	Annual	San Bernar	Truck2		IDLEX		PM10	0.02432
2022	Annual	San Bernar	Truck2		PMTW		PM10	0.027157
2022	Annual	San Bernar	Truck2		PMBW		PM10	0.087008
2023	Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.027248
2023	Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.009921
2023	Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.009267
2023	Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006825
2023	Annual	San Bernar	Truck1		IDLEX		PM10	0.463256
2023	Annual	San Bernar	Truck1		PMTW		PM10	0.010061
2023	Annual	San Bernar	Truck1		PMBW		PM10	0.079164
2023	Annual	San Bernar	Truck2		IDLEX		PM10	0.016651
2023	Annual	San Bernar	Truck2		PMTW		PM10	0.027152
2023	Annual	San Bernar	Truck2		PMBW		PM10	0.087021
2024	Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.025803
2024	Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.009522
2024	Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.009211
2024	Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006892
2024	Annual	San Bernar	Truck1		IDLEX		PM10	0.468639
2024	Annual	San Bernar	Truck1		PMTW		PM10	0.010082
2024	Annual	San Bernar	Truck1		PMBW		PM10	0.079176
2024	Annual	San Bernar	Truck2		IDLEX		PM10	0.015803
2024	Annual	San Bernar	Truck2		PMTW		PM10	0.02715
2024	Annual	San Bernar	Truck2		PMBW		PM10	0.087029
2025	Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.024442
2025	Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.009141
2025	Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.009096
2025	Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006898
2025	Annual	San Bernar	Truck1		IDLEX		PM10	0.474122
2025	Annual	San Bernar	Truck1		PMTW		PM10	0.010102
2025	Annual	San Bernar	Truck1		PMBW		PM10	0.079186
2025	Annual	San Bernar	Truck2		IDLEX		PM10	0.015069
2025	Annual	San Bernar	Truck2		PMTW		PM10	0.027152
2025	Annual	San Bernar	Truck2		PMBW		PM10	0.087021
2026	Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.023113
2026	Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.008762
2026	Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.008955
2026	Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.00687
2026	Annual	San Bernar	Truck1		IDLEX		PM10	0.47885

calendar_season	mcsub_area	vehicle_class	temp	relative_process	speed	pollutant	emission_rate
2026 Annual	San Bernar	Truck1		PMTW		PM10	0.010118
2026 Annual	San Bernar	Truck1		PMBW		PM10	0.079193
2026 Annual	San Bernar	Truck2		IDLEX		PM10	0.014429
2026 Annual	San Bernar	Truck2		PMTW		PM10	0.02716
2026 Annual	San Bernar	Truck2		PMBW		PM10	0.086998
2027 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.021851
2027 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.008396
2027 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.008793
2027 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006822
2027 Annual	San Bernar	Truck1		IDLEX		PM10	0.482621
2027 Annual	San Bernar	Truck1		PMTW		PM10	0.010132
2027 Annual	San Bernar	Truck1		PMBW		PM10	0.079198
2027 Annual	San Bernar	Truck2		IDLEX		PM10	0.013768
2027 Annual	San Bernar	Truck2		PMTW		PM10	0.027168
2027 Annual	San Bernar	Truck2		PMBW		PM10	0.086974
2028 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.020676
2028 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.00805
2028 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.008671
2028 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006783
2028 Annual	San Bernar	Truck1		IDLEX		PM10	0.486277
2028 Annual	San Bernar	Truck1		PMTW		PM10	0.010144
2028 Annual	San Bernar	Truck1		PMBW		PM10	0.079202
2028 Annual	San Bernar	Truck2		IDLEX		PM10	0.013313
2028 Annual	San Bernar	Truck2		PMTW		PM10	0.027184
2028 Annual	San Bernar	Truck2		PMBW		PM10	0.086931
2029 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.019569
2029 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.007719
2029 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.008536
2029 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006731
2029 Annual	San Bernar	Truck1		IDLEX		PM10	0.489007
2029 Annual	San Bernar	Truck1		PMTW		PM10	0.010154
2029 Annual	San Bernar	Truck1		PMBW		PM10	0.079205
2029 Annual	San Bernar	Truck2		IDLEX		PM10	0.012832
2029 Annual	San Bernar	Truck2		PMTW		PM10	0.027199
2029 Annual	San Bernar	Truck2		PMBW		PM10	0.086885
2030 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.01855
2030 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.00741
2030 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.008406
2030 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006677
2030 Annual	San Bernar	Truck1		IDLEX		PM10	0.491559
2030 Annual	San Bernar	Truck1		PMTW		PM10	0.010162
2030 Annual	San Bernar	Truck1		PMBW		PM10	0.079207
2030 Annual	San Bernar	Truck2		IDLEX		PM10	0.012376
2030 Annual	San Bernar	Truck2		PMTW		PM10	0.027218
2030 Annual	San Bernar	Truck2		PMBW		PM10	0.086832
2031 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.017628

calendar_season	mcsub_area	vehicle_class	temp	relative_process	speed	pollutant	emission_rate
2031 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.007126
2031 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.008289
2031 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006629
2031 Annual	San Bernar	Truck1		IDLEX		PM10	0.493714
2031 Annual	San Bernar	Truck1		PMTW		PM10	0.01017
2031 Annual	San Bernar	Truck1		PMBW		PM10	0.079208
2031 Annual	San Bernar	Truck2		IDLEX		PM10	0.01198
2031 Annual	San Bernar	Truck2		PMTW		PM10	0.027231
2031 Annual	San Bernar	Truck2		PMBW		PM10	0.086795
2032 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.016811
2032 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.006871
2032 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.008198
2032 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.00659
2032 Annual	San Bernar	Truck1		IDLEX		PM10	0.496281
2032 Annual	San Bernar	Truck1		PMTW		PM10	0.010177
2032 Annual	San Bernar	Truck1		PMBW		PM10	0.079207
2032 Annual	San Bernar	Truck2		IDLEX		PM10	0.011729
2032 Annual	San Bernar	Truck2		PMTW		PM10	0.027245
2032 Annual	San Bernar	Truck2		PMBW		PM10	0.086755
2033 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.016081
2033 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.006642
2033 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.008111
2033 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006553
2033 Annual	San Bernar	Truck1		IDLEX		PM10	0.498537
2033 Annual	San Bernar	Truck1		PMTW		PM10	0.010182
2033 Annual	San Bernar	Truck1		PMBW		PM10	0.079207
2033 Annual	San Bernar	Truck2		IDLEX		PM10	0.011508
2033 Annual	San Bernar	Truck2		PMTW		PM10	0.027261
2033 Annual	San Bernar	Truck2		PMBW		PM10	0.08671
2034 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.015399
2034 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.006426
2034 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.008018
2034 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006508
2034 Annual	San Bernar	Truck1		IDLEX		PM10	0.500415
2034 Annual	San Bernar	Truck1		PMTW		PM10	0.010188
2034 Annual	San Bernar	Truck1		PMBW		PM10	0.079206
2034 Annual	San Bernar	Truck2		IDLEX		PM10	0.011286
2034 Annual	San Bernar	Truck2		PMTW		PM10	0.027278
2034 Annual	San Bernar	Truck2		PMBW		PM10	0.086659
2035 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.014735
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2035 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006467
2035 Annual	San Bernar	Truck1		IDLEX		PM10	0.501581
2035 Annual	San Bernar	Truck1		PMTW		PM10	0.010193
2035 Annual	San Bernar	Truck1		PMBW		PM10	0.079205

calendar_season	mcsub_area	vehicle_cla	tempe	relative_process	speed	pollutant	emission_rate
2035 Annual	San Bernar	Truck2		IDLEX		PM10	0.011097
2035 Annual	San Bernar	Truck2		PMTW		PM10	0.027296
2035 Annual	San Bernar	Truck2		PMBW		PM10	0.086608
2036 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.014223
2036 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.006045
2036 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.007872
2036 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006443
2036 Annual	San Bernar	Truck1		IDLEX		PM10	0.50302
2036 Annual	San Bernar	Truck1		PMTW		PM10	0.010196
2036 Annual	San Bernar	Truck1		PMBW		PM10	0.079204
2036 Annual	San Bernar	Truck2		IDLEX		PM10	0.010957
2036 Annual	San Bernar	Truck2		PMTW		PM10	0.027319
2036 Annual	San Bernar	Truck2		PMBW		PM10	0.086543
2037 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.013737
2037 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.005884
2037 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.007828
2037 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006425
2037 Annual	San Bernar	Truck1		IDLEX		PM10	0.504169
2037 Annual	San Bernar	Truck1		PMTW		PM10	0.010198
2037 Annual	San Bernar	Truck1		PMBW		PM10	0.079203
2037 Annual	San Bernar	Truck2		IDLEX		PM10	0.010843
2037 Annual	San Bernar	Truck2		PMTW		PM10	0.027342
2037 Annual	San Bernar	Truck2		PMBW		PM10	0.086476
2038 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.013309
2038 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.005739
2038 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.007794
2038 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006413
2038 Annual	San Bernar	Truck1		IDLEX		PM10	0.504982
2038 Annual	San Bernar	Truck1		PMTW		PM10	0.0102
2038 Annual	San Bernar	Truck1		PMBW		PM10	0.079201
2038 Annual	San Bernar	Truck2		IDLEX		PM10	0.010737
2038 Annual	San Bernar	Truck2		PMTW		PM10	0.027366
2038 Annual	San Bernar	Truck2		PMBW		PM10	0.086407
2039 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.012921
2039 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.005607
2039 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.00777
2039 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006406
2039 Annual	San Bernar	Truck1		IDLEX		PM10	0.50588
2039 Annual	San Bernar	Truck1		PMTW		PM10	0.010201
2039 Annual	San Bernar	Truck1		PMBW		PM10	0.079199
2039 Annual	San Bernar	Truck2		IDLEX		PM10	0.010635
2039 Annual	San Bernar	Truck2		PMTW		PM10	0.02739
2039 Annual	San Bernar	Truck2		PMBW		PM10	0.086339
2040 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.012591
2040 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.005493
2040 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.007751

calendar_season	mcsub_area	vehicle_class	temp	relative_process	speed	pollutant	emission_rate
2040 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006404
2040 Annual	San Bernar	Truck1		IDLEX		PM10	0.506958
2040 Annual	San Bernar	Truck1		PMTW		PM10	0.010203
2040 Annual	San Bernar	Truck1		PMBW		PM10	0.079199
2040 Annual	San Bernar	Truck2		IDLEX		PM10	0.010536
2040 Annual	San Bernar	Truck2		PMTW		PM10	0.027414
2040 Annual	San Bernar	Truck2		PMBW		PM10	0.086269
2041 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.012322
2041 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.005398
2041 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.007733
2041 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006401
2041 Annual	San Bernar	Truck1		IDLEX		PM10	0.507768
2041 Annual	San Bernar	Truck1		PMTW		PM10	0.010205
2041 Annual	San Bernar	Truck1		PMBW		PM10	0.0792
2041 Annual	San Bernar	Truck2		IDLEX		PM10	0.010445
2041 Annual	San Bernar	Truck2		PMTW		PM10	0.027427
2041 Annual	San Bernar	Truck2		PMBW		PM10	0.086232
2042 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.012083
2042 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.005313
2042 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.007725
2042 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006404
2042 Annual	San Bernar	Truck1		IDLEX		PM10	0.508471
2042 Annual	San Bernar	Truck1		PMTW		PM10	0.010206
2042 Annual	San Bernar	Truck1		PMBW		PM10	0.079201
2042 Annual	San Bernar	Truck2		IDLEX		PM10	0.010373
2042 Annual	San Bernar	Truck2		PMTW		PM10	0.027449
2042 Annual	San Bernar	Truck2		PMBW		PM10	0.086171
2043 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.011901
2043 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.005248
2043 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.007718
2043 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006407
2043 Annual	San Bernar	Truck1		IDLEX		PM10	0.50921
2043 Annual	San Bernar	Truck1		PMTW		PM10	0.010207
2043 Annual	San Bernar	Truck1		PMBW		PM10	0.079202
2043 Annual	San Bernar	Truck2		IDLEX		PM10	0.010308
2043 Annual	San Bernar	Truck2		PMTW		PM10	0.02747
2043 Annual	San Bernar	Truck2		PMBW		PM10	0.086111
2044 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.011714
2044 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.005184
2044 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.007711
2044 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006412
2044 Annual	San Bernar	Truck1		IDLEX		PM10	0.510037
2044 Annual	San Bernar	Truck1		PMTW		PM10	0.010209
2044 Annual	San Bernar	Truck1		PMBW		PM10	0.079203
2044 Annual	San Bernar	Truck2		IDLEX		PM10	0.010243
2044 Annual	San Bernar	Truck2		PMTW		PM10	0.027491

calendar_season	mcsub_area	vehicle_class	temp	relative_process	speed	pollutant	emission_rate
2044 Annual	San Bernar	Truck2				PMBW PM10	0.08605
2045 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.011559
2045 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.005131
2045 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.007704
2045 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006416
2045 Annual	San Bernar	Truck1		IDLEX		PM10	0.51099
2045 Annual	San Bernar	Truck1		PMTW		PM10	0.010211
2045 Annual	San Bernar	Truck1		PMBW		PM10	0.079204
2045 Annual	San Bernar	Truck2		IDLEX		PM10	0.010184
2045 Annual	San Bernar	Truck2		PMTW		PM10	0.027512
2045 Annual	San Bernar	Truck2		PMBW		PM10	0.08599
2046 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.011411
2046 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.00508
2046 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.007698
2046 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.00642
2046 Annual	San Bernar	Truck1		IDLEX		PM10	0.511821
2046 Annual	San Bernar	Truck1		PMTW		PM10	0.010212
2046 Annual	San Bernar	Truck1		PMBW		PM10	0.079204
2046 Annual	San Bernar	Truck2		IDLEX		PM10	0.01012
2046 Annual	San Bernar	Truck2		PMTW		PM10	0.02753
2046 Annual	San Bernar	Truck2		PMBW		PM10	0.08594
2047 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.011292
2047 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.00504
2047 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.007688
2047 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006423
2047 Annual	San Bernar	Truck1		IDLEX		PM10	0.51258
2047 Annual	San Bernar	Truck1		PMTW		PM10	0.010213
2047 Annual	San Bernar	Truck1		PMBW		PM10	0.079205
2047 Annual	San Bernar	Truck2		IDLEX		PM10	0.010042
2047 Annual	San Bernar	Truck2		PMTW		PM10	0.027547
2047 Annual	San Bernar	Truck2		PMBW		PM10	0.085892
2048 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.011178
2048 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.005002
2048 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.00769
2048 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006426
2048 Annual	San Bernar	Truck1		IDLEX		PM10	0.513533
2048 Annual	San Bernar	Truck1		PMTW		PM10	0.010215
2048 Annual	San Bernar	Truck1		PMBW		PM10	0.079206
2048 Annual	San Bernar	Truck2		IDLEX		PM10	0.010004
2048 Annual	San Bernar	Truck2		PMTW		PM10	0.027564
2048 Annual	San Bernar	Truck2		PMBW		PM10	0.085843
2049 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.011082
2049 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.004969
2049 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.007694
2049 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006431
2049 Annual	San Bernar	Truck1		IDLEX		PM10	0.514278

calendar_season	mcsub_area	vehicle_class	temp	relative_process	speed	pollutant	emission_rate
2049 Annual	San Bernar	Truck1		PMTW		PM10	0.010216
2049 Annual	San Bernar	Truck1		PMBW		PM10	0.079207
2049 Annual	San Bernar	Truck2		IDLEX		PM10	0.009976
2049 Annual	San Bernar	Truck2		PMTW		PM10	0.02758
2049 Annual	San Bernar	Truck2		PMBW		PM10	0.085796
2050 Annual	San Bernar	Truck1	48	50 RUNEX	10	PM10	0.010965
2050 Annual	San Bernar	Truck1	48	50 RUNEX	35	PM10	0.00493
2050 Annual	San Bernar	Truck2	48	50 RUNEX	10	PM10	0.007699
2050 Annual	San Bernar	Truck2	48	50 RUNEX	35	PM10	0.006435
2050 Annual	San Bernar	Truck1		IDLEX		PM10	0.514463
2050 Annual	San Bernar	Truck1		PMTW		PM10	0.010216
2050 Annual	San Bernar	Truck1		PMBW		PM10	0.079205
2050 Annual	San Bernar	Truck2		IDLEX		PM10	0.009947
2050 Annual	San Bernar	Truck2		PMTW		PM10	0.027596
2050 Annual	San Bernar	Truck2		PMBW		PM10	0.08575

APPENDIX B

AERMOD Model Years 2022-2024 DPM Concentrations Printouts

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**
** AERMOD Input Produced by:
** AERMOD View Ver. 9.9.0
** Lakes Environmental Software Inc.
** Date: 11/25/2020
** File: C:\Vista Env\2020\20098 Ontario\AERMOD\2022\2022.ADI
**
*****
**
**
*****
** AERMOD Control Pathway
*****
**
**
CO STARTING
  TITLEONE State Distribution Project - DPM Emission Years 2022-2024
  TITLETWO PM10
  MODELOPT DFAULT CONC
  AVERTIME 24 ANNUAL
  URBANOPT 2035210 San_Bernardino_Co
  POLLUTID PM_10
  RUNORNOT RUN
  ERRORFIL 2022.err
CO FINISHED
**
*****
** AERMOD Source Pathway
*****
**
**
SO STARTING
** Source Location **
** Source ID - Type - X Coord. - Y Coord. **
** -----
** Line Source Represented by Adjacent Volume Sources
** LINE VOLUME Source ID = RDSTATEW
** DESCRSRC State Street west of Project to Mountain Ave
** PREFIX
** Length of Side = 6.10
** Configuration = Adjacent
** Emission Rate = 3.92E-06
** Vertical Dimension = 1.83
** SZINIT = 0.85
** Nodes = 8
** 438484.689, 3769002.029, 301.34, 0.00, 2.84
** 438281.920, 3769002.622, 302.69, 0.00, 2.84
** 438282.487, 3768946.403, 300.89, 0.00, 2.84
** 438271.538, 3768902.898, 298.12, 0.00, 2.84
** 438271.053, 3768807.851, 289.27, 0.00, 2.84
** 438263.976, 3768793.904, 289.08, 0.00, 2.84
** 438249.196, 3768787.867, 288.87, 0.00, 2.84

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** 438204.233, 3768788.908, 288.46, 0.00, 2.84

**

LOCATION	L0000181	VOLUME	438481.641	3769002.038	301.18
LOCATION	L0000182	VOLUME	438475.545	3769002.056	301.25
LOCATION	L0000183	VOLUME	438469.449	3769002.073	301.30
LOCATION	L0000184	VOLUME	438463.353	3769002.091	301.36
LOCATION	L0000185	VOLUME	438457.257	3769002.109	301.41
LOCATION	L0000186	VOLUME	438451.161	3769002.127	301.47
LOCATION	L0000187	VOLUME	438445.065	3769002.145	301.52
LOCATION	L0000188	VOLUME	438438.969	3769002.163	301.58
LOCATION	L0000189	VOLUME	438432.874	3769002.180	301.63
LOCATION	L0000190	VOLUME	438426.778	3769002.198	301.69
LOCATION	L0000191	VOLUME	438420.682	3769002.216	301.74
LOCATION	L0000192	VOLUME	438414.586	3769002.234	301.80
LOCATION	L0000193	VOLUME	438408.490	3769002.252	301.85
LOCATION	L0000194	VOLUME	438402.394	3769002.269	301.90
LOCATION	L0000195	VOLUME	438396.298	3769002.287	301.96
LOCATION	L0000196	VOLUME	438390.202	3769002.305	302.02
LOCATION	L0000197	VOLUME	438384.106	3769002.323	302.08
LOCATION	L0000198	VOLUME	438378.010	3769002.341	302.14
LOCATION	L0000199	VOLUME	438371.914	3769002.359	302.20
LOCATION	L0000200	VOLUME	438365.818	3769002.376	302.26
LOCATION	L0000201	VOLUME	438359.722	3769002.394	302.32
LOCATION	L0000202	VOLUME	438353.626	3769002.412	302.38
LOCATION	L0000203	VOLUME	438347.530	3769002.430	302.44
LOCATION	L0000204	VOLUME	438341.434	3769002.448	302.50
LOCATION	L0000205	VOLUME	438335.338	3769002.466	302.56
LOCATION	L0000206	VOLUME	438329.242	3769002.483	302.62
LOCATION	L0000207	VOLUME	438323.146	3769002.501	302.67
LOCATION	L0000208	VOLUME	438317.050	3769002.519	302.67
LOCATION	L0000209	VOLUME	438310.954	3769002.537	302.67
LOCATION	L0000210	VOLUME	438304.858	3769002.555	302.67
LOCATION	L0000211	VOLUME	438298.762	3769002.572	302.66
LOCATION	L0000212	VOLUME	438292.666	3769002.590	302.66
LOCATION	L0000213	VOLUME	438286.570	3769002.608	302.66
LOCATION	L0000214	VOLUME	438281.935	3769001.176	302.60
LOCATION	L0000215	VOLUME	438281.996	3768995.080	302.34
LOCATION	L0000216	VOLUME	438282.058	3768988.984	302.07
LOCATION	L0000217	VOLUME	438282.119	3768982.889	301.81
LOCATION	L0000218	VOLUME	438282.180	3768976.793	301.54
LOCATION	L0000219	VOLUME	438282.242	3768970.697	301.28
LOCATION	L0000220	VOLUME	438282.303	3768964.601	301.02
LOCATION	L0000221	VOLUME	438282.365	3768958.506	300.75
LOCATION	L0000222	VOLUME	438282.426	3768952.410	300.49
LOCATION	L0000223	VOLUME	438282.465	3768946.317	300.22
LOCATION	L0000224	VOLUME	438280.977	3768940.405	299.97
LOCATION	L0000225	VOLUME	438279.490	3768934.494	299.71
LOCATION	L0000226	VOLUME	438278.002	3768928.582	299.46
LOCATION	L0000227	VOLUME	438276.514	3768922.670	299.20
LOCATION	L0000228	VOLUME	438275.026	3768916.759	298.87
LOCATION	L0000229	VOLUME	438273.539	3768910.847	298.27
LOCATION	L0000230	VOLUME	438272.051	3768904.935	297.67
LOCATION	L0000231	VOLUME	438271.518	3768898.903	297.06
LOCATION	L0000232	VOLUME	438271.487	3768892.807	296.44

LOCATION	VOLUME				
L0000233	VOLUME	438271.456	3768886.711	295.83	
L0000234	VOLUME	438271.425	3768880.615	295.21	
L0000235	VOLUME	438271.394	3768874.519	294.60	
L0000236	VOLUME	438271.362	3768868.423	293.98	
L0000237	VOLUME	438271.331	3768862.327	293.36	
L0000238	VOLUME	438271.300	3768856.231	292.75	
L0000239	VOLUME	438271.269	3768850.136	292.13	
L0000240	VOLUME	438271.238	3768844.040	291.52	
L0000241	VOLUME	438271.207	3768837.944	290.90	
L0000242	VOLUME	438271.176	3768831.848	290.28	
L0000243	VOLUME	438271.145	3768825.752	289.67	
L0000244	VOLUME	438271.114	3768819.656	289.60	
L0000245	VOLUME	438271.082	3768813.560	289.53	
L0000246	VOLUME	438270.878	3768807.506	289.46	
L0000247	VOLUME	438268.120	3768802.070	289.33	
L0000248	VOLUME	438265.361	3768796.634	289.20	
L0000249	VOLUME	438261.166	3768792.756	289.05	
L0000250	VOLUME	438255.523	3768790.451	288.88	
L0000251	VOLUME	438249.879	3768788.146	288.71	
L0000252	VOLUME	438243.840	3768787.991	288.58	
L0000253	VOLUME	438237.745	3768788.132	288.53	
L0000254	VOLUME	438231.651	3768788.273	288.49	
L0000255	VOLUME	438225.556	3768788.415	288.44	
L0000256	VOLUME	438219.462	3768788.556	288.39	
L0000257	VOLUME	438213.368	3768788.697	288.35	
L0000258	VOLUME	438207.273	3768788.838	288.30	

** End of LINE VOLUME Source ID = RDSTATEW

** -----

** Line Source Represented by Adjacent Volume Sources

** LINE VOLUME Source ID = RDSTATEE

** DESCRSRC State Street East of Project Site to San Antonio Ave

** PREFIX

** Length of Side = 6.10

** Configuration = Adjacent

** Emission Rate = 2.88E-06

** Vertical Dimension = 1.83

** SZINIT = 0.85

** Nodes = 4

** 438672.202, 3769002.020, 302.84, 0.00, 2.84

** 438789.448, 3769002.707, 304.97, 0.00, 2.84

** 438842.357, 3769006.954, 303.08, 0.00, 2.84

** 439021.762, 3769005.515, 302.13, 0.00, 2.84

** -----

L0000337	VOLUME	438675.250	3769002.038	302.76	
L0000338	VOLUME	438681.345	3769002.074	302.98	
L0000339	VOLUME	438687.441	3769002.110	303.19	
L0000340	VOLUME	438693.537	3769002.145	303.41	
L0000341	VOLUME	438699.633	3769002.181	303.63	
L0000342	VOLUME	438705.729	3769002.217	303.85	
L0000343	VOLUME	438711.825	3769002.253	303.98	
L0000344	VOLUME	438717.921	3769002.288	304.05	
L0000345	VOLUME	438724.017	3769002.324	304.13	
L0000346	VOLUME	438730.113	3769002.360	304.20	
L0000347	VOLUME	438736.208	3769002.395	304.27	

LOCATION	VOLUME				
L0000348	VOLUME	438742.304	3769002.431	304.34	
L0000349	VOLUME	438748.400	3769002.467	304.41	
L0000350	VOLUME	438754.496	3769002.503	304.49	
L0000351	VOLUME	438760.592	3769002.538	304.56	
L0000352	VOLUME	438766.688	3769002.574	304.63	
L0000353	VOLUME	438772.784	3769002.610	304.70	
L0000354	VOLUME	438778.880	3769002.645	304.78	
L0000355	VOLUME	438784.976	3769002.681	304.85	
L0000356	VOLUME	438791.066	3769002.837	304.71	
L0000357	VOLUME	438797.143	3769003.325	304.57	
L0000358	VOLUME	438803.219	3769003.813	304.43	
L0000359	VOLUME	438809.296	3769004.301	304.29	
L0000360	VOLUME	438815.372	3769004.788	304.14	
L0000361	VOLUME	438821.449	3769005.276	304.00	
L0000362	VOLUME	438827.525	3769005.764	303.86	
L0000363	VOLUME	438833.602	3769006.252	303.72	
L0000364	VOLUME	438839.678	3769006.739	303.58	
L0000365	VOLUME	438845.765	3769006.927	303.43	
L0000366	VOLUME	438851.861	3769006.878	303.27	
L0000367	VOLUME	438857.957	3769006.829	303.11	
L0000368	VOLUME	438864.053	3769006.780	303.00	
L0000369	VOLUME	438870.148	3769006.731	303.00	
L0000370	VOLUME	438876.244	3769006.682	303.00	
L0000371	VOLUME	438882.340	3769006.633	303.00	
L0000372	VOLUME	438888.436	3769006.585	303.00	
L0000373	VOLUME	438894.532	3769006.536	303.00	
L0000374	VOLUME	438900.628	3769006.487	303.00	
L0000375	VOLUME	438906.723	3769006.438	303.00	
L0000376	VOLUME	438912.819	3769006.389	303.00	
L0000377	VOLUME	438918.915	3769006.340	303.00	
L0000378	VOLUME	438925.011	3769006.291	303.00	
L0000379	VOLUME	438931.107	3769006.242	303.00	
L0000380	VOLUME	438937.202	3769006.193	303.00	
L0000381	VOLUME	438943.298	3769006.144	302.94	
L0000382	VOLUME	438949.394	3769006.095	302.86	
L0000383	VOLUME	438955.490	3769006.047	302.79	
L0000384	VOLUME	438961.586	3769005.998	302.71	
L0000385	VOLUME	438967.681	3769005.949	302.63	
L0000386	VOLUME	438973.777	3769005.900	302.55	
L0000387	VOLUME	438979.873	3769005.851	302.47	
L0000388	VOLUME	438985.969	3769005.802	302.39	
L0000389	VOLUME	438992.065	3769005.753	302.31	
L0000390	VOLUME	438998.160	3769005.704	302.23	
L0000391	VOLUME	439004.256	3769005.655	302.15	
L0000392	VOLUME	439010.352	3769005.606	302.07	
L0000393	VOLUME	439016.448	3769005.557	302.00	

** End of LINE VOLUME Source ID = RDSTATEE

** -----

** Line Source Represented by Adjacent Volume Sources

** LINE VOLUME Source ID = RDONW

** DESCRSRC Onsite West Driveway

** PREFIX

** Length of Side = 3.66

** Configuration = Adjacent

** Emission Rate = 3.31E-06
 ** Vertical Dimension = 1.83
 ** SZINIT = 0.85
 ** Nodes = 2
 ** 438484.240, 3768997.613, 301.37, 0.00, 1.70
 ** 438483.840, 3768803.245, 295.33, 0.00, 1.70

LOCATION		VOLUME			
LOCATION L0000394		VOLUME	438484.236	3768995.784	301.36
LOCATION L0000395		VOLUME	438484.229	3768992.126	301.48
LOCATION L0000396		VOLUME	438484.221	3768988.469	301.60
LOCATION L0000397		VOLUME	438484.214	3768984.811	301.72
LOCATION L0000398		VOLUME	438484.206	3768981.154	301.85
LOCATION L0000399		VOLUME	438484.199	3768977.496	301.97
LOCATION L0000400		VOLUME	438484.191	3768973.838	302.09
LOCATION L0000401		VOLUME	438484.184	3768970.181	302.21
LOCATION L0000402		VOLUME	438484.176	3768966.523	302.33
LOCATION L0000403		VOLUME	438484.169	3768962.866	302.46
LOCATION L0000404		VOLUME	438484.161	3768959.208	302.58
LOCATION L0000405		VOLUME	438484.154	3768955.551	302.70
LOCATION L0000406		VOLUME	438484.146	3768951.893	302.82
LOCATION L0000407		VOLUME	438484.139	3768948.235	302.95
LOCATION L0000408		VOLUME	438484.131	3768944.578	303.07
LOCATION L0000409		VOLUME	438484.124	3768940.920	303.19
LOCATION L0000410		VOLUME	438484.116	3768937.263	303.31
LOCATION L0000411		VOLUME	438484.108	3768933.605	303.43
LOCATION L0000412		VOLUME	438484.101	3768929.947	303.56
LOCATION L0000413		VOLUME	438484.093	3768926.290	303.68
LOCATION L0000414		VOLUME	438484.086	3768922.632	303.80
LOCATION L0000415		VOLUME	438484.078	3768918.975	303.92
LOCATION L0000416		VOLUME	438484.071	3768915.317	303.87
LOCATION L0000417		VOLUME	438484.063	3768911.659	303.52
LOCATION L0000418		VOLUME	438484.056	3768908.002	303.17
LOCATION L0000419		VOLUME	438484.048	3768904.344	302.82
LOCATION L0000420		VOLUME	438484.041	3768900.687	302.47
LOCATION L0000421		VOLUME	438484.033	3768897.029	302.12
LOCATION L0000422		VOLUME	438484.026	3768893.371	301.77
LOCATION L0000423		VOLUME	438484.018	3768889.714	301.43
LOCATION L0000424		VOLUME	438484.011	3768886.056	301.08
LOCATION L0000425		VOLUME	438484.003	3768882.399	300.73
LOCATION L0000426		VOLUME	438483.996	3768878.741	300.38
LOCATION L0000427		VOLUME	438483.988	3768875.084	300.03
LOCATION L0000428		VOLUME	438483.981	3768871.426	299.68
LOCATION L0000429		VOLUME	438483.973	3768867.768	299.33
LOCATION L0000430		VOLUME	438483.966	3768864.111	298.99
LOCATION L0000431		VOLUME	438483.958	3768860.453	298.64
LOCATION L0000432		VOLUME	438483.950	3768856.796	298.29
LOCATION L0000433		VOLUME	438483.943	3768853.138	297.94
LOCATION L0000434		VOLUME	438483.935	3768849.480	297.59
LOCATION L0000435		VOLUME	438483.928	3768845.823	297.24
LOCATION L0000436		VOLUME	438483.920	3768842.165	296.90
LOCATION L0000437		VOLUME	438483.913	3768838.508	296.55
LOCATION L0000438		VOLUME	438483.905	3768834.850	296.20
LOCATION L0000439		VOLUME	438483.898	3768831.192	295.85
LOCATION L0000440		VOLUME	438483.890	3768827.535	295.50

LOCATION	L0000441	VOLUME	438483.883	3768823.877	295.19
LOCATION	L0000442	VOLUME	438483.875	3768820.220	295.19
LOCATION	L0000443	VOLUME	438483.868	3768816.562	295.19
LOCATION	L0000444	VOLUME	438483.860	3768812.904	295.18
LOCATION	L0000445	VOLUME	438483.853	3768809.247	295.18
LOCATION	L0000446	VOLUME	438483.845	3768805.589	295.18

** End of LINE VOLUME Source ID = RDOWN
 ** -----
 ** Line Source Represented by Adjacent Volume Sources
 ** LINE VOLUME Source ID = RDONE
 ** DESCRSRC Onsite East Driveway
 ** PREFIX
 ** Length of Side = 3.66
 ** Configuration = Adjacent
 ** Emission Rate = 2.86E-06
 ** Vertical Dimension = 1.83
 ** SZINIT = 0.85
 ** Nodes = 2
 ** 438674.078, 3768998.696, 302.96, 0.00, 1.70
 ** 438672.120, 3768830.955, 299.03, 0.00, 1.70
 ** -----

LOCATION	L0000447	VOLUME	438674.056	3768996.868	302.76
LOCATION	L0000448	VOLUME	438674.014	3768993.210	302.79
LOCATION	L0000449	VOLUME	438673.971	3768989.553	302.82
LOCATION	L0000450	VOLUME	438673.928	3768985.896	302.85
LOCATION	L0000451	VOLUME	438673.886	3768982.238	302.88
LOCATION	L0000452	VOLUME	438673.843	3768978.581	302.91
LOCATION	L0000453	VOLUME	438673.800	3768974.924	302.94
LOCATION	L0000454	VOLUME	438673.758	3768971.266	302.97
LOCATION	L0000455	VOLUME	438673.715	3768967.609	303.00
LOCATION	L0000456	VOLUME	438673.672	3768963.951	303.03
LOCATION	L0000457	VOLUME	438673.630	3768960.294	303.07
LOCATION	L0000458	VOLUME	438673.587	3768956.637	303.10
LOCATION	L0000459	VOLUME	438673.544	3768952.979	303.13
LOCATION	L0000460	VOLUME	438673.502	3768949.322	303.16
LOCATION	L0000461	VOLUME	438673.459	3768945.665	303.19
LOCATION	L0000462	VOLUME	438673.416	3768942.007	303.22
LOCATION	L0000463	VOLUME	438673.373	3768938.350	303.25
LOCATION	L0000464	VOLUME	438673.331	3768934.693	303.28
LOCATION	L0000465	VOLUME	438673.288	3768931.035	303.31
LOCATION	L0000466	VOLUME	438673.245	3768927.378	303.35
LOCATION	L0000467	VOLUME	438673.203	3768923.721	303.38
LOCATION	L0000468	VOLUME	438673.160	3768920.063	303.41
LOCATION	L0000469	VOLUME	438673.117	3768916.406	303.44
LOCATION	L0000470	VOLUME	438673.075	3768912.749	303.32
LOCATION	L0000471	VOLUME	438673.032	3768909.091	303.15
LOCATION	L0000472	VOLUME	438672.989	3768905.434	302.98
LOCATION	L0000473	VOLUME	438672.947	3768901.777	302.81
LOCATION	L0000474	VOLUME	438672.904	3768898.119	302.63
LOCATION	L0000475	VOLUME	438672.861	3768894.462	302.46
LOCATION	L0000476	VOLUME	438672.819	3768890.804	302.29
LOCATION	L0000477	VOLUME	438672.776	3768887.147	302.12
LOCATION	L0000478	VOLUME	438672.733	3768883.490	301.94
LOCATION	L0000479	VOLUME	438672.691	3768879.832	301.77

LOCATION	L0000480	VOLUME	438672.648	3768876.175	301.60
LOCATION	L0000481	VOLUME	438672.605	3768872.518	301.43
LOCATION	L0000482	VOLUME	438672.563	3768868.860	301.25
LOCATION	L0000483	VOLUME	438672.520	3768865.203	301.08
LOCATION	L0000484	VOLUME	438672.477	3768861.546	300.91
LOCATION	L0000485	VOLUME	438672.435	3768857.888	300.74
LOCATION	L0000486	VOLUME	438672.392	3768854.231	300.56
LOCATION	L0000487	VOLUME	438672.349	3768850.574	300.39
LOCATION	L0000488	VOLUME	438672.306	3768846.916	300.22
LOCATION	L0000489	VOLUME	438672.264	3768843.259	300.04
LOCATION	L0000490	VOLUME	438672.221	3768839.602	299.87
LOCATION	L0000491	VOLUME	438672.178	3768835.944	299.70
LOCATION	L0000492	VOLUME	438672.136	3768832.287	299.52
**	End of LINE VOLUME Source ID = RDONE				
LOCATION	IDLINGW	POINT	438508.800	3768873.340	300.180
**	DESCRSRC Total Idling West Loading Dock				
LOCATION	IDLINGE	POINT	438646.330	3768887.240	302.140
**	DESCRSRC Total Idling East Loading Dock				
**	Source Parameters **				
**	LINE VOLUME Source ID = RDSTATEW				
SRCPARAM	L0000181	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000182	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000183	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000184	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000185	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000186	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000187	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000188	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000189	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000190	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000191	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000192	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000193	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000194	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000195	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000196	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000197	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000198	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000199	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000200	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000201	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000202	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000203	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000204	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000205	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000206	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000207	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000208	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000209	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000210	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000211	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000212	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000213	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000214	0.00000005026	0.00	2.84	0.85

SRCPARAM	L0000215	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000216	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000217	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000218	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000219	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000220	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000221	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000222	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000223	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000224	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000225	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000226	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000227	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000228	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000229	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000230	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000231	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000232	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000233	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000234	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000235	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000236	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000237	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000238	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000239	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000240	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000241	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000242	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000243	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000244	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000245	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000246	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000247	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000248	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000249	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000250	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000251	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000252	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000253	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000254	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000255	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000256	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000257	0.00000005026	0.00	2.84	0.85
SRCPARAM	L0000258	0.00000005026	0.00	2.84	0.85

**

 ** LINE VOLUME Source ID = RDSTATEE

SRCPARAM	L0000337	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000338	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000339	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000340	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000341	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000342	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000343	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000344	0.00000005053	0.00	2.84	0.85

SRCPARAM	L0000345	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000346	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000347	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000348	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000349	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000350	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000351	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000352	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000353	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000354	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000355	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000356	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000357	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000358	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000359	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000360	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000361	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000362	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000363	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000364	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000365	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000366	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000367	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000368	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000369	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000370	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000371	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000372	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000373	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000374	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000375	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000376	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000377	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000378	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000379	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000380	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000381	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000382	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000383	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000384	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000385	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000386	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000387	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000388	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000389	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000390	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000391	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000392	0.00000005053	0.00	2.84	0.85
SRCPARAM	L0000393	0.00000005053	0.00	2.84	0.85

**

 ** LINE VOLUME Source ID = RDONW

SRCPARAM	L0000394	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000395	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000396	0.00000006245	0.00	1.70	0.85

SRCPARAM	L0000397	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000398	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000399	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000400	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000401	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000402	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000403	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000404	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000405	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000406	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000407	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000408	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000409	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000410	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000411	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000412	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000413	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000414	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000415	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000416	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000417	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000418	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000419	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000420	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000421	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000422	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000423	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000424	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000425	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000426	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000427	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000428	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000429	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000430	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000431	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000432	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000433	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000434	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000435	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000436	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000437	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000438	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000439	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000440	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000441	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000442	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000443	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000444	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000445	0.00000006245	0.00	1.70	0.85
SRCPARAM	L0000446	0.00000006245	0.00	1.70	0.85

**

 ** LINE VOLUME Source ID = RDONE

SRCPARAM	L0000447	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000448	0.00000006217	0.00	1.70	0.85

SRCPARAM	L0000449	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000450	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000451	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000452	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000453	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000454	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000455	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000456	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000457	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000458	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000459	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000460	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000461	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000462	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000463	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000464	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000465	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000466	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000467	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000468	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000469	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000470	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000471	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000472	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000473	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000474	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000475	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000476	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000477	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000478	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000479	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000480	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000481	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000482	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000483	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000484	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000485	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000486	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000487	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000488	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000489	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000490	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000491	0.00000006217	0.00	1.70	0.85
SRCPARAM	L0000492	0.00000006217	0.00	1.70	0.85

**

SRCPARAM IDLINGW 0.0000276 3.840 366.000 50.00000
0.100

SRCPARAM IDLINGE 0.0000276 3.840 366.000 50.00000
0.100

URBANSRC ALL
SRCGROUP ALL

SO FINISHED

**

```

** AERMOD Receptor Pathway
*****
**
**
RE STARTING
  INCLUDED 2022.rou
RE FINISHED
**
*****
** AERMOD Meteorology Pathway
*****
**
**
ME STARTING
  SURFFILE ..\OntarioIntlAirportADJU\KONT_V9_ADJU\KONT_v9.SFC
  PROFFILE ..\OntarioIntlAirportADJU\KONT_V9_ADJU\KONT_v9.PFL
  SURFDATA 3102 2012 Ontario_Airport
  UAIRDATA 3190 2012
  PROFBASE 289.0 METERS
ME FINISHED
**
*****
** AERMOD Output Pathway
*****
**
**
OU STARTING
  RECTABLE ALLAVE 1ST
  RECTABLE 24 1ST
** Auto-Generated Plotfiles
  PLOTFILE 24 ALL 1ST 2022.AD\24H1GALL.PLT 31
  PLOTFILE ANNUAL ALL 2022.AD\AN00GALL.PLT 32
  SUMMFILE 2022.sum
OU FINISHED
**
*****
** Project Parameters
*****
** PROJCTN  CoordinateSystemUTM
** DESCPTN  UTM: Universal Transverse Mercator
** DATUM    World Geodetic System 1984
** DTMRGN   Global Definition
** UNITS    m
** ZONE     11
** ZONEINX  0
**

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11/25/20
16:16:52

* AERMOD (19191): State Distribution Project - DPM Emission Years 2022-2024

* AERMET (16216):

* MODELING OPTIONS USED: RegDEFAULT CONC ELEV URBAN ADJ_U*

* PLOT FILE OF ANNUAL VALUES AVERAGED ACROSS 5 YEARS FOR SOURCE GROUP: ALL

* FOR A TOTAL OF 15 RECEPTORS.

* FORMAT: (3(1X,F13.5),3(1X,F8.2),2X,A6,2X,A8,2X,I8,2X,A8)

X	Y	AVERAGE CONC	ZELEV	ZHILL	ZFLAG	AVE	GRP	NUM YRS	NET ID
438872.00000	3769195.00000	0.00043	307.98	3062.00	0.00	ANNUAL	ALL	00000005	
438949.00000	3769181.00000	0.00049	308.58	3062.00	0.00	ANNUAL	ALL	00000005	
438758.87000	3768908.02000	0.00361	302.21	3062.00	0.00	ANNUAL	ALL	00000005	
438755.00000	3768841.00000	0.00157	300.97	3062.00	0.00	ANNUAL	ALL	00000005	
438772.00000	3768779.00000	0.00075	298.50	3062.00	0.00	ANNUAL	ALL	00000005	
438708.00000	3768777.00000	0.00093	297.54	3062.00	0.00	ANNUAL	ALL	00000005	
438649.00000	3768776.00000	0.00111	296.71	3062.00	0.00	ANNUAL	ALL	00000005	
438597.00000	3768773.00000	0.00124	296.40	3062.00	0.00	ANNUAL	ALL	00000005	
438359.00000	3768775.00000	0.00080	291.67	3062.00	0.00	ANNUAL	ALL	00000005	
438469.02000	3768785.16000	0.00156	294.82	3062.00	0.00	ANNUAL	ALL	00000005	
438676.53000	3768785.16000	0.00112	297.48	3062.00	0.00	ANNUAL	ALL	00000005	
438677.48000	3768827.34000	0.00267	299.42	3062.00	0.00	ANNUAL	ALL	00000005	
438686.59000	3768827.34000	0.00211	299.65	3062.00	0.00	ANNUAL	ALL	00000005	
438688.51000	3768996.02000	0.00433	303.23	3062.00	0.00	ANNUAL	ALL	00000005	
438471.42000	3768996.50000	0.00247	301.45	3062.00	0.00	ANNUAL	ALL	00000005	

** CONCUNIT ug/m^3

** DEPUNIT g/m^2

* AERMOD (19191): State Distribution Project - DPM Emission Years 2022-2024

* AERMET (16216): PM10

* MODELING OPTIONS USED: RegDEFAULT CONC ELEV URBAN ADJ_U*

* PLOT FILE OF HIGH 1ST HIGH 24-HR VALUES FOR SOURCE GROUP: ALL

* FOR A TOTAL OF 15 RECEPTORS.

* FORMAT: (3(1X,F13.5),3(1X,F8.2),3X,A5,2X,A8,2X,A5,5X,A8,2X,I8)

X	Y	AVERAGE CONC	ZELEV	ZHILL	ZFLAG	AVE	GRP	RANK	NET ID	DATE (CONC)
438872.00000	3769195.00000	0.00208	307.98	3062.00	0.00	24-HR	ALL	1ST		12120224
438949.00000	3769181.00000	0.00221	308.58	3062.00	0.00	24-HR	ALL	1ST		12120224
438758.87000	3768908.02000	0.00814	302.21	3062.00	0.00	24-HR	ALL	1ST		13020624
438755.00000	3768841.00000	0.00420	300.97	3062.00	0.00	24-HR	ALL	1ST		13112024
438772.00000	3768779.00000	0.00266	298.50	3062.00	0.00	24-HR	ALL	1ST		14010624
438708.00000	3768777.00000	0.00369	297.54	3062.00	0.00	24-HR	ALL	1ST		13010324
438649.00000	3768776.00000	0.00365	296.71	3062.00	0.00	24-HR	ALL	1ST		13010224
438597.00000	3768773.00000	0.00388	296.40	3062.00	0.00	24-HR	ALL	1ST		13110124
438359.00000	3768775.00000	0.00285	291.67	3062.00	0.00	24-HR	ALL	1ST		14120224
438469.02000	3768785.16000	0.00513	294.82	3062.00	0.00	24-HR	ALL	1ST		15120624
438676.53000	3768785.16000	0.00436	297.48	3062.00	0.00	24-HR	ALL	1ST		13010324
438677.48000	3768827.34000	0.00811	299.42	3062.00	0.00	24-HR	ALL	1ST		13010224
438686.59000	3768827.34000	0.00611	299.65	3062.00	0.00	24-HR	ALL	1ST		13010224
438688.51000	3768996.02000	0.00774	303.23	3062.00	0.00	24-HR	ALL	1ST		12120224
438471.42000	3768996.50000	0.00490	301.45	3062.00	0.00	24-HR	ALL	1ST		16092024

** CONCUNIT ug/m^3

** DEFUNIT g/m^2

11/25/20
16:16:52

APPENDIX C

AERMOD Model Years 2024-2038 DPM Concentrations Printouts

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**
*****
**
** AERMOD Input Produced by:
** AERMOD View Ver. 9.9.0
** Lakes Environmental Software Inc.
** Date: 11/25/2020
** File: C:\Vista Env\2020\20098 Ontario\AERMOD\2024\2024.ADI
**
*****
**
**
*****
** AERMOD Control Pathway
*****
**
**
CO STARTING
  TITLEONE State Distribution Project - DPM Emission Years 2024-2038
  TITLETWO PM10
  MODELOPT DFAULT CONC
  AVERTIME 24 ANNUAL
  URBANOPT 2035210 San_Bernardino_Co
  POLLUTID PM_10
  RUNORNOT RUN
  ERRORFIL 2024.err
CO FINISHED
**
*****
** AERMOD Source Pathway
*****
**
**
SO STARTING
** Source Location **
** Source ID - Type - X Coord. - Y Coord. **
** -----
** Line Source Represented by Adjacent Volume Sources
** LINE VOLUME Source ID = RDSTATEW
** DESCRSRC State Street west of Project to Mountain Ave
** PREFIX
** Length of Side = 6.10
** Configuration = Adjacent
** Emission Rate = 2.47E-06
** Vertical Dimension = 1.83
** SZINIT = 0.85
** Nodes = 8
** 438484.689, 3769002.029, 301.34, 0.00, 2.84
** 438281.920, 3769002.622, 302.69, 0.00, 2.84
** 438282.487, 3768946.403, 300.89, 0.00, 2.84
** 438271.538, 3768902.898, 298.12, 0.00, 2.84
** 438271.053, 3768807.851, 289.27, 0.00, 2.84
** 438263.976, 3768793.904, 289.08, 0.00, 2.84
** 438249.196, 3768787.867, 288.87, 0.00, 2.84

```

** 438204.233, 3768788.908, 288.46, 0.00, 2.84

**

LOCATION	L0000493	VOLUME	438481.641	3769002.038	301.18
LOCATION	L0000494	VOLUME	438475.545	3769002.056	301.25
LOCATION	L0000495	VOLUME	438469.449	3769002.073	301.30
LOCATION	L0000496	VOLUME	438463.353	3769002.091	301.36
LOCATION	L0000497	VOLUME	438457.257	3769002.109	301.41
LOCATION	L0000498	VOLUME	438451.161	3769002.127	301.47
LOCATION	L0000499	VOLUME	438445.065	3769002.145	301.52
LOCATION	L0000500	VOLUME	438438.969	3769002.163	301.58
LOCATION	L0000501	VOLUME	438432.874	3769002.180	301.63
LOCATION	L0000502	VOLUME	438426.778	3769002.198	301.69
LOCATION	L0000503	VOLUME	438420.682	3769002.216	301.74
LOCATION	L0000504	VOLUME	438414.586	3769002.234	301.80
LOCATION	L0000505	VOLUME	438408.490	3769002.252	301.85
LOCATION	L0000506	VOLUME	438402.394	3769002.269	301.90
LOCATION	L0000507	VOLUME	438396.298	3769002.287	301.96
LOCATION	L0000508	VOLUME	438390.202	3769002.305	302.02
LOCATION	L0000509	VOLUME	438384.106	3769002.323	302.08
LOCATION	L0000510	VOLUME	438378.010	3769002.341	302.14
LOCATION	L0000511	VOLUME	438371.914	3769002.359	302.20
LOCATION	L0000512	VOLUME	438365.818	3769002.376	302.26
LOCATION	L0000513	VOLUME	438359.722	3769002.394	302.32
LOCATION	L0000514	VOLUME	438353.626	3769002.412	302.38
LOCATION	L0000515	VOLUME	438347.530	3769002.430	302.44
LOCATION	L0000516	VOLUME	438341.434	3769002.448	302.50
LOCATION	L0000517	VOLUME	438335.338	3769002.466	302.56
LOCATION	L0000518	VOLUME	438329.242	3769002.483	302.62
LOCATION	L0000519	VOLUME	438323.146	3769002.501	302.67
LOCATION	L0000520	VOLUME	438317.050	3769002.519	302.67
LOCATION	L0000521	VOLUME	438310.954	3769002.537	302.67
LOCATION	L0000522	VOLUME	438304.858	3769002.555	302.67
LOCATION	L0000523	VOLUME	438298.762	3769002.572	302.66
LOCATION	L0000524	VOLUME	438292.666	3769002.590	302.66
LOCATION	L0000525	VOLUME	438286.570	3769002.608	302.66
LOCATION	L0000526	VOLUME	438281.935	3769001.176	302.60
LOCATION	L0000527	VOLUME	438281.996	3768995.080	302.34
LOCATION	L0000528	VOLUME	438282.058	3768988.984	302.07
LOCATION	L0000529	VOLUME	438282.119	3768982.889	301.81
LOCATION	L0000530	VOLUME	438282.180	3768976.793	301.54
LOCATION	L0000531	VOLUME	438282.242	3768970.697	301.28
LOCATION	L0000532	VOLUME	438282.303	3768964.601	301.02
LOCATION	L0000533	VOLUME	438282.365	3768958.506	300.75
LOCATION	L0000534	VOLUME	438282.426	3768952.410	300.49
LOCATION	L0000535	VOLUME	438282.465	3768946.317	300.22
LOCATION	L0000536	VOLUME	438280.977	3768940.405	299.97
LOCATION	L0000537	VOLUME	438279.490	3768934.494	299.71
LOCATION	L0000538	VOLUME	438278.002	3768928.582	299.46
LOCATION	L0000539	VOLUME	438276.514	3768922.670	299.20
LOCATION	L0000540	VOLUME	438275.026	3768916.759	298.87
LOCATION	L0000541	VOLUME	438273.539	3768910.847	298.27
LOCATION	L0000542	VOLUME	438272.051	3768904.935	297.67
LOCATION	L0000543	VOLUME	438271.518	3768898.903	297.06
LOCATION	L0000544	VOLUME	438271.487	3768892.807	296.44

LOCATION	VOLUME				
L0000545	VOLUME	438271.456	3768886.711	295.83	
L0000546	VOLUME	438271.425	3768880.615	295.21	
L0000547	VOLUME	438271.394	3768874.519	294.60	
L0000548	VOLUME	438271.362	3768868.423	293.98	
L0000549	VOLUME	438271.331	3768862.327	293.36	
L0000550	VOLUME	438271.300	3768856.231	292.75	
L0000551	VOLUME	438271.269	3768850.136	292.13	
L0000552	VOLUME	438271.238	3768844.040	291.52	
L0000553	VOLUME	438271.207	3768837.944	290.90	
L0000554	VOLUME	438271.176	3768831.848	290.28	
L0000555	VOLUME	438271.145	3768825.752	289.67	
L0000556	VOLUME	438271.114	3768819.656	289.60	
L0000557	VOLUME	438271.082	3768813.560	289.53	
L0000558	VOLUME	438270.878	3768807.506	289.46	
L0000559	VOLUME	438268.120	3768802.070	289.33	
L0000560	VOLUME	438265.361	3768796.634	289.20	
L0000561	VOLUME	438261.166	3768792.756	289.05	
L0000562	VOLUME	438255.523	3768790.451	288.88	
L0000563	VOLUME	438249.879	3768788.146	288.71	
L0000564	VOLUME	438243.840	3768787.991	288.58	
L0000565	VOLUME	438237.745	3768788.132	288.53	
L0000566	VOLUME	438231.651	3768788.273	288.49	
L0000567	VOLUME	438225.556	3768788.415	288.44	
L0000568	VOLUME	438219.462	3768788.556	288.39	
L0000569	VOLUME	438213.368	3768788.697	288.35	
L0000570	VOLUME	438207.273	3768788.838	288.30	

** End of LINE VOLUME Source ID = RDSTATEW

** -----

** Line Source Represented by Adjacent Volume Sources

** LINE VOLUME Source ID = RDSTATEE

** DESCRSRC State Street East of Project Site to San Antonio Ave

** PREFIX

** Length of Side = 6.10

** Configuration = Adjacent

** Emission Rate = 1.81E-06

** Vertical Dimension = 1.83

** SZINIT = 0.85

** Nodes = 4

** 438672.202, 3769002.020, 302.84, 0.00, 2.84

** 438789.448, 3769002.707, 304.97, 0.00, 2.84

** 438842.357, 3769006.954, 303.08, 0.00, 2.84

** 439021.762, 3769005.515, 302.13, 0.00, 2.84

** -----

L0000571	VOLUME	438675.250	3769002.038	302.76	
L0000572	VOLUME	438681.345	3769002.074	302.98	
L0000573	VOLUME	438687.441	3769002.110	303.19	
L0000574	VOLUME	438693.537	3769002.145	303.41	
L0000575	VOLUME	438699.633	3769002.181	303.63	
L0000576	VOLUME	438705.729	3769002.217	303.85	
L0000577	VOLUME	438711.825	3769002.253	303.98	
L0000578	VOLUME	438717.921	3769002.288	304.05	
L0000579	VOLUME	438724.017	3769002.324	304.13	
L0000580	VOLUME	438730.113	3769002.360	304.20	
L0000581	VOLUME	438736.208	3769002.395	304.27	

LOCATION	VOLUME				
L0000582	438742.304	3769002.431	304.34		
L0000583	438748.400	3769002.467	304.41		
L0000584	438754.496	3769002.503	304.49		
L0000585	438760.592	3769002.538	304.56		
L0000586	438766.688	3769002.574	304.63		
L0000587	438772.784	3769002.610	304.70		
L0000588	438778.880	3769002.645	304.78		
L0000589	438784.976	3769002.681	304.85		
L0000590	438791.066	3769002.837	304.71		
L0000591	438797.143	3769003.325	304.57		
L0000592	438803.219	3769003.813	304.43		
L0000593	438809.296	3769004.301	304.29		
L0000594	438815.372	3769004.788	304.14		
L0000595	438821.449	3769005.276	304.00		
L0000596	438827.525	3769005.764	303.86		
L0000597	438833.602	3769006.252	303.72		
L0000598	438839.678	3769006.739	303.58		
L0000599	438845.765	3769006.927	303.43		
L0000600	438851.861	3769006.878	303.27		
L0000601	438857.957	3769006.829	303.11		
L0000602	438864.053	3769006.780	303.00		
L0000603	438870.148	3769006.731	303.00		
L0000604	438876.244	3769006.682	303.00		
L0000605	438882.340	3769006.633	303.00		
L0000606	438888.436	3769006.585	303.00		
L0000607	438894.532	3769006.536	303.00		
L0000608	438900.628	3769006.487	303.00		
L0000609	438906.723	3769006.438	303.00		
L0000610	438912.819	3769006.389	303.00		
L0000611	438918.915	3769006.340	303.00		
L0000612	438925.011	3769006.291	303.00		
L0000613	438931.107	3769006.242	303.00		
L0000614	438937.202	3769006.193	303.00		
L0000615	438943.298	3769006.144	302.94		
L0000616	438949.394	3769006.095	302.86		
L0000617	438955.490	3769006.047	302.79		
L0000618	438961.586	3769005.998	302.71		
L0000619	438967.681	3769005.949	302.63		
L0000620	438973.777	3769005.900	302.55		
L0000621	438979.873	3769005.851	302.47		
L0000622	438985.969	3769005.802	302.39		
L0000623	438992.065	3769005.753	302.31		
L0000624	438998.160	3769005.704	302.23		
L0000625	439004.256	3769005.655	302.15		
L0000626	439010.352	3769005.606	302.07		
L0000627	439016.448	3769005.557	302.00		

** End of LINE VOLUME Source ID = RDSTATEE
 ** -----
 ** Line Source Represented by Adjacent Volume Sources
 ** LINE VOLUME Source ID = RDONW
 ** DESCRSRC Onsite West Driveway
 ** PREFIX
 ** Length of Side = 3.66
 ** Configuration = Adjacent

** Emission Rate = 1.76E-06
 ** Vertical Dimension = 1.83
 ** SZINIT = 0.85
 ** Nodes = 2
 ** 438484.240, 3768997.613, 301.37, 0.00, 1.70
 ** 438483.840, 3768803.245, 295.33, 0.00, 1.70

LOCATION	VOLUME				
L0000628	VOLUME	438484.236	3768995.784	301.36	
L0000629	VOLUME	438484.229	3768992.126	301.48	
L0000630	VOLUME	438484.221	3768988.469	301.60	
L0000631	VOLUME	438484.214	3768984.811	301.72	
L0000632	VOLUME	438484.206	3768981.154	301.85	
L0000633	VOLUME	438484.199	3768977.496	301.97	
L0000634	VOLUME	438484.191	3768973.838	302.09	
L0000635	VOLUME	438484.184	3768970.181	302.21	
L0000636	VOLUME	438484.176	3768966.523	302.33	
L0000637	VOLUME	438484.169	3768962.866	302.46	
L0000638	VOLUME	438484.161	3768959.208	302.58	
L0000639	VOLUME	438484.154	3768955.551	302.70	
L0000640	VOLUME	438484.146	3768951.893	302.82	
L0000641	VOLUME	438484.139	3768948.235	302.95	
L0000642	VOLUME	438484.131	3768944.578	303.07	
L0000643	VOLUME	438484.124	3768940.920	303.19	
L0000644	VOLUME	438484.116	3768937.263	303.31	
L0000645	VOLUME	438484.108	3768933.605	303.43	
L0000646	VOLUME	438484.101	3768929.947	303.56	
L0000647	VOLUME	438484.093	3768926.290	303.68	
L0000648	VOLUME	438484.086	3768922.632	303.80	
L0000649	VOLUME	438484.078	3768918.975	303.92	
L0000650	VOLUME	438484.071	3768915.317	303.87	
L0000651	VOLUME	438484.063	3768911.659	303.52	
L0000652	VOLUME	438484.056	3768908.002	303.17	
L0000653	VOLUME	438484.048	3768904.344	302.82	
L0000654	VOLUME	438484.041	3768900.687	302.47	
L0000655	VOLUME	438484.033	3768897.029	302.12	
L0000656	VOLUME	438484.026	3768893.371	301.77	
L0000657	VOLUME	438484.018	3768889.714	301.43	
L0000658	VOLUME	438484.011	3768886.056	301.08	
L0000659	VOLUME	438484.003	3768882.399	300.73	
L0000660	VOLUME	438483.996	3768878.741	300.38	
L0000661	VOLUME	438483.988	3768875.084	300.03	
L0000662	VOLUME	438483.981	3768871.426	299.68	
L0000663	VOLUME	438483.973	3768867.768	299.33	
L0000664	VOLUME	438483.966	3768864.111	298.99	
L0000665	VOLUME	438483.958	3768860.453	298.64	
L0000666	VOLUME	438483.950	3768856.796	298.29	
L0000667	VOLUME	438483.943	3768853.138	297.94	
L0000668	VOLUME	438483.935	3768849.480	297.59	
L0000669	VOLUME	438483.928	3768845.823	297.24	
L0000670	VOLUME	438483.920	3768842.165	296.90	
L0000671	VOLUME	438483.913	3768838.508	296.55	
L0000672	VOLUME	438483.905	3768834.850	296.20	
L0000673	VOLUME	438483.898	3768831.192	295.85	
L0000674	VOLUME	438483.890	3768827.535	295.50	

LOCATION	L0000675	VOLUME	438483.883	3768823.877	295.19
LOCATION	L0000676	VOLUME	438483.875	3768820.220	295.19
LOCATION	L0000677	VOLUME	438483.868	3768816.562	295.19
LOCATION	L0000678	VOLUME	438483.860	3768812.904	295.18
LOCATION	L0000679	VOLUME	438483.853	3768809.247	295.18
LOCATION	L0000680	VOLUME	438483.845	3768805.589	295.18

** End of LINE VOLUME Source ID = RDOWN
 ** -----
 ** Line Source Represented by Adjacent Volume Sources
 ** LINE VOLUME Source ID = RDONE
 ** DESCRSRC Onsite East Driveway
 ** PREFIX
 ** Length of Side = 3.66
 ** Configuration = Adjacent
 ** Emission Rate = 1.52E-06
 ** Vertical Dimension = 1.83
 ** SZINIT = 0.85
 ** Nodes = 2
 ** 438674.078, 3768998.696, 302.96, 0.00, 1.70
 ** 438672.120, 3768830.955, 299.03, 0.00, 1.70
 ** -----

LOCATION	L0000681	VOLUME	438674.056	3768996.868	302.76
LOCATION	L0000682	VOLUME	438674.014	3768993.210	302.79
LOCATION	L0000683	VOLUME	438673.971	3768989.553	302.82
LOCATION	L0000684	VOLUME	438673.928	3768985.896	302.85
LOCATION	L0000685	VOLUME	438673.886	3768982.238	302.88
LOCATION	L0000686	VOLUME	438673.843	3768978.581	302.91
LOCATION	L0000687	VOLUME	438673.800	3768974.924	302.94
LOCATION	L0000688	VOLUME	438673.758	3768971.266	302.97
LOCATION	L0000689	VOLUME	438673.715	3768967.609	303.00
LOCATION	L0000690	VOLUME	438673.672	3768963.951	303.03
LOCATION	L0000691	VOLUME	438673.630	3768960.294	303.07
LOCATION	L0000692	VOLUME	438673.587	3768956.637	303.10
LOCATION	L0000693	VOLUME	438673.544	3768952.979	303.13
LOCATION	L0000694	VOLUME	438673.502	3768949.322	303.16
LOCATION	L0000695	VOLUME	438673.459	3768945.665	303.19
LOCATION	L0000696	VOLUME	438673.416	3768942.007	303.22
LOCATION	L0000697	VOLUME	438673.373	3768938.350	303.25
LOCATION	L0000698	VOLUME	438673.331	3768934.693	303.28
LOCATION	L0000699	VOLUME	438673.288	3768931.035	303.31
LOCATION	L0000700	VOLUME	438673.245	3768927.378	303.35
LOCATION	L0000701	VOLUME	438673.203	3768923.721	303.38
LOCATION	L0000702	VOLUME	438673.160	3768920.063	303.41
LOCATION	L0000703	VOLUME	438673.117	3768916.406	303.44
LOCATION	L0000704	VOLUME	438673.075	3768912.749	303.32
LOCATION	L0000705	VOLUME	438673.032	3768909.091	303.15
LOCATION	L0000706	VOLUME	438672.989	3768905.434	302.98
LOCATION	L0000707	VOLUME	438672.947	3768901.777	302.81
LOCATION	L0000708	VOLUME	438672.904	3768898.119	302.63
LOCATION	L0000709	VOLUME	438672.861	3768894.462	302.46
LOCATION	L0000710	VOLUME	438672.819	3768890.804	302.29
LOCATION	L0000711	VOLUME	438672.776	3768887.147	302.12
LOCATION	L0000712	VOLUME	438672.733	3768883.490	301.94
LOCATION	L0000713	VOLUME	438672.691	3768879.832	301.77

LOCATION	L0000714	VOLUME	438672.648	3768876.175	301.60
LOCATION	L0000715	VOLUME	438672.605	3768872.518	301.43
LOCATION	L0000716	VOLUME	438672.563	3768868.860	301.25
LOCATION	L0000717	VOLUME	438672.520	3768865.203	301.08
LOCATION	L0000718	VOLUME	438672.477	3768861.546	300.91
LOCATION	L0000719	VOLUME	438672.435	3768857.888	300.74
LOCATION	L0000720	VOLUME	438672.392	3768854.231	300.56
LOCATION	L0000721	VOLUME	438672.349	3768850.574	300.39
LOCATION	L0000722	VOLUME	438672.306	3768846.916	300.22
LOCATION	L0000723	VOLUME	438672.264	3768843.259	300.04
LOCATION	L0000724	VOLUME	438672.221	3768839.602	299.87
LOCATION	L0000725	VOLUME	438672.178	3768835.944	299.70
LOCATION	L0000726	VOLUME	438672.136	3768832.287	299.52
**	End of LINE VOLUME Source ID = RDONE				
LOCATION	IDLINGW	POINT	438508.800	3768873.340	300.180
**	DESCRSRC Total Idling West Loading Dock				
LOCATION	IDLINGE	POINT	438646.330	3768887.240	302.140
**	DESCRSRC Total Idling East Loading Dock				
**	Source Parameters **				
**	LINE VOLUME Source ID = RDSTATEW				
SRCPARAM	L0000493	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000494	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000495	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000496	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000497	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000498	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000499	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000500	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000501	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000502	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000503	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000504	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000505	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000506	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000507	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000508	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000509	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000510	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000511	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000512	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000513	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000514	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000515	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000516	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000517	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000518	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000519	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000520	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000521	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000522	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000523	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000524	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000525	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000526	0.00000003167	0.00	2.84	0.85

SRCPARAM	L0000527	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000528	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000529	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000530	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000531	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000532	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000533	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000534	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000535	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000536	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000537	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000538	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000539	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000540	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000541	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000542	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000543	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000544	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000545	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000546	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000547	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000548	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000549	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000550	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000551	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000552	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000553	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000554	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000555	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000556	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000557	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000558	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000559	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000560	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000561	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000562	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000563	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000564	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000565	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000566	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000567	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000568	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000569	0.00000003167	0.00	2.84	0.85
SRCPARAM	L0000570	0.00000003167	0.00	2.84	0.85

**

 ** LINE VOLUME Source ID = RDSTATEE

SRCPARAM	L0000571	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000572	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000573	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000574	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000575	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000576	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000577	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000578	0.00000003175	0.00	2.84	0.85

SRCPARAM	L0000579	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000580	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000581	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000582	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000583	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000584	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000585	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000586	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000587	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000588	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000589	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000590	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000591	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000592	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000593	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000594	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000595	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000596	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000597	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000598	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000599	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000600	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000601	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000602	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000603	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000604	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000605	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000606	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000607	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000608	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000609	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000610	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000611	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000612	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000613	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000614	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000615	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000616	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000617	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000618	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000619	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000620	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000621	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000622	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000623	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000624	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000625	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000626	0.00000003175	0.00	2.84	0.85
SRCPARAM	L0000627	0.00000003175	0.00	2.84	0.85

**

 ** LINE VOLUME Source ID = RDONW

SRCPARAM	L0000628	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000629	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000630	0.00000003321	0.00	1.70	0.85

SRCPARAM	L0000631	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000632	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000633	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000634	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000635	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000636	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000637	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000638	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000639	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000640	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000641	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000642	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000643	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000644	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000645	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000646	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000647	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000648	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000649	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000650	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000651	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000652	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000653	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000654	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000655	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000656	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000657	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000658	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000659	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000660	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000661	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000662	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000663	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000664	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000665	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000666	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000667	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000668	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000669	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000670	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000671	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000672	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000673	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000674	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000675	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000676	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000677	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000678	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000679	0.00000003321	0.00	1.70	0.85
SRCPARAM	L0000680	0.00000003321	0.00	1.70	0.85

**

 ** LINE VOLUME Source ID = RDONE

SRCPARAM	L0000681	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000682	0.00000003304	0.00	1.70	0.85

SRCPARAM	L0000683	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000684	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000685	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000686	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000687	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000688	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000689	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000690	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000691	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000692	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000693	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000694	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000695	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000696	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000697	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000698	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000699	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000700	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000701	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000702	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000703	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000704	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000705	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000706	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000707	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000708	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000709	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000710	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000711	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000712	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000713	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000714	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000715	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000716	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000717	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000718	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000719	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000720	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000721	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000722	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000723	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000724	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000725	0.00000003304	0.00	1.70	0.85
SRCPARAM	L0000726	0.00000003304	0.00	1.70	0.85

**

SRCPARAM	IDLINGW	0.0000286	3.840	366.000	50.00000
0.100					
SRCPARAM	IDLINGE	0.0000286	3.840	366.000	50.00000
0.100					

URBANSRC ALL
SRCGROUP ALL

SO FINISHED

**

```

** AERMOD Receptor Pathway
*****
**
**
RE STARTING
  INCLUDED 2024.rou
RE FINISHED
**
*****
** AERMOD Meteorology Pathway
*****
**
**
ME STARTING
  SURFFILE ..\OntarioIntlAirportADJU\KONT_V9_ADJU\KONT_v9.SFC
  PROFFILE ..\OntarioIntlAirportADJU\KONT_V9_ADJU\KONT_v9.PFL
  SURFDATA 3102 2012 Ontario_Airport
  UAIRDATA 3190 2012
  PROFBASE 289.0 METERS
ME FINISHED
**
*****
** AERMOD Output Pathway
*****
**
**
OU STARTING
  RECTABLE ALLAVE 1ST
  RECTABLE 24 1ST
** Auto-Generated Plotfiles
  PLOTFILE 24 ALL 1ST 2024.AD\24H1GALL.PLT 31
  PLOTFILE ANNUAL ALL 2024.AD\AN00GALL.PLT 32
  SUMMFILE 2024.sum
OU FINISHED
**
*****
** Project Parameters
*****
** PROJCTN  CoordinateSystemUTM
** DESCPTN  UTM: Universal Transverse Mercator
** DATUM    World Geodetic System 1984
** DTMRGN   Global Definition
** UNITS    m
** ZONE     11
** ZONEINX  0
**

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11/25/20
16:25:27

* AERMOD (19191): State Distribution Project - DPM Emission Years 2024-2038

* AERMET (16216):

* MODELING OPTIONS USED: RegDEFAULT CONC ELEV URBAN ADJ_U*

* PLOT FILE OF ANNUAL VALUES AVERAGED ACROSS 5 YEARS FOR SOURCE GROUP: ALL

* FOR A TOTAL OF 15 RECEPTORS.

* FORMAT: (3(1X,F13.5),3(1X,F8.2),2X,A6,2X,A8,2X,I8,2X,A8)

X	Y	AVERAGE CONC	ZELEV	ZHILL	ZFLAG	AVE	GRP	NUM YRS	NET ID
438872.00000	3769195.00000	0.00039	307.98	3062.00	0.00	ANNUAL	ALL	00000005	
438949.00000	3769181.00000	0.00045	308.58	3062.00	0.00	ANNUAL	ALL	00000005	
438758.87000	3768908.02000	0.00346	302.21	3062.00	0.00	ANNUAL	ALL	00000005	
438755.00000	3768841.00000	0.00147	300.97	3062.00	0.00	ANNUAL	ALL	00000005	
438772.00000	3768779.00000	0.00071	298.50	3062.00	0.00	ANNUAL	ALL	00000005	
438708.00000	3768777.00000	0.00086	297.54	3062.00	0.00	ANNUAL	ALL	00000005	
438649.00000	3768776.00000	0.00104	296.71	3062.00	0.00	ANNUAL	ALL	00000005	
438597.00000	3768773.00000	0.00117	296.40	3062.00	0.00	ANNUAL	ALL	00000005	
438359.00000	3768775.00000	0.00073	291.67	3062.00	0.00	ANNUAL	ALL	00000005	
438469.02000	3768785.16000	0.00137	294.82	3062.00	0.00	ANNUAL	ALL	00000005	
438676.53000	3768785.16000	0.00103	297.48	3062.00	0.00	ANNUAL	ALL	00000005	
438677.48000	3768827.34000	0.00205	299.42	3062.00	0.00	ANNUAL	ALL	00000005	
438686.59000	3768827.34000	0.00174	299.65	3062.00	0.00	ANNUAL	ALL	00000005	
438688.51000	3768996.02000	0.00312	303.23	3062.00	0.00	ANNUAL	ALL	00000005	
438471.42000	3768996.50000	0.00172	301.45	3062.00	0.00	ANNUAL	ALL	00000005	

** CONCUNIT ug/m^3

** DEPUNIT g/m^2

APPENDIX D

AERMOD Model Years 2038-2051 DPM Concentrations Printouts

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**
*****
**
** AERMOD Input Produced by:
** AERMOD View Ver. 9.9.0
** Lakes Environmental Software Inc.
** Date: 11/25/2020
** File: C:\Vista Env\2020\20098 Ontario\AERMOD\2038\2038.ADI
**
*****
**
**
*****
** AERMOD Control Pathway
*****
**
**
CO STARTING
  TITLEONE State Distribution Project - DPM Emission Years 2038 - 2051
  TITLETWO PM10
  MODELOPT DFAULT CONC
  AVERTIME 24 ANNUAL
  URBANOPT 2035210 San_Bernardino_Co
  POLLUTID PM_10
  RUNORNOT RUN
  ERRORFIL 2038.err
CO FINISHED
**
*****
** AERMOD Source Pathway
*****
**
**
SO STARTING
** Source Location **
** Source ID - Type - X Coord. - Y Coord. **
** -----
** Line Source Represented by Adjacent Volume Sources
** LINE VOLUME Source ID = RDSTATEW
** DESCRSRC State Street west of Project to Mountain Ave
** PREFIX
** Length of Side = 6.10
** Configuration = Adjacent
** Emission Rate = 2.14E-06
** Vertical Dimension = 1.83
** SZINIT = 0.85
** Nodes = 8
** 438484.689, 3769002.029, 301.34, 0.00, 2.84
** 438281.920, 3769002.622, 302.69, 0.00, 2.84
** 438282.487, 3768946.403, 300.89, 0.00, 2.84
** 438271.538, 3768902.898, 298.12, 0.00, 2.84
** 438271.053, 3768807.851, 289.27, 0.00, 2.84
** 438263.976, 3768793.904, 289.08, 0.00, 2.84
** 438249.196, 3768787.867, 288.87, 0.00, 2.84

```

** 438204.233, 3768788.908, 288.46, 0.00, 2.84

**

LOCATION	L0000727	VOLUME	438481.641	3769002.038	301.18
LOCATION	L0000728	VOLUME	438475.545	3769002.056	301.25
LOCATION	L0000729	VOLUME	438469.449	3769002.073	301.30
LOCATION	L0000730	VOLUME	438463.353	3769002.091	301.36
LOCATION	L0000731	VOLUME	438457.257	3769002.109	301.41
LOCATION	L0000732	VOLUME	438451.161	3769002.127	301.47
LOCATION	L0000733	VOLUME	438445.065	3769002.145	301.52
LOCATION	L0000734	VOLUME	438438.969	3769002.163	301.58
LOCATION	L0000735	VOLUME	438432.874	3769002.180	301.63
LOCATION	L0000736	VOLUME	438426.778	3769002.198	301.69
LOCATION	L0000737	VOLUME	438420.682	3769002.216	301.74
LOCATION	L0000738	VOLUME	438414.586	3769002.234	301.80
LOCATION	L0000739	VOLUME	438408.490	3769002.252	301.85
LOCATION	L0000740	VOLUME	438402.394	3769002.269	301.90
LOCATION	L0000741	VOLUME	438396.298	3769002.287	301.96
LOCATION	L0000742	VOLUME	438390.202	3769002.305	302.02
LOCATION	L0000743	VOLUME	438384.106	3769002.323	302.08
LOCATION	L0000744	VOLUME	438378.010	3769002.341	302.14
LOCATION	L0000745	VOLUME	438371.914	3769002.359	302.20
LOCATION	L0000746	VOLUME	438365.818	3769002.376	302.26
LOCATION	L0000747	VOLUME	438359.722	3769002.394	302.32
LOCATION	L0000748	VOLUME	438353.626	3769002.412	302.38
LOCATION	L0000749	VOLUME	438347.530	3769002.430	302.44
LOCATION	L0000750	VOLUME	438341.434	3769002.448	302.50
LOCATION	L0000751	VOLUME	438335.338	3769002.466	302.56
LOCATION	L0000752	VOLUME	438329.242	3769002.483	302.62
LOCATION	L0000753	VOLUME	438323.146	3769002.501	302.67
LOCATION	L0000754	VOLUME	438317.050	3769002.519	302.67
LOCATION	L0000755	VOLUME	438310.954	3769002.537	302.67
LOCATION	L0000756	VOLUME	438304.858	3769002.555	302.67
LOCATION	L0000757	VOLUME	438298.762	3769002.572	302.66
LOCATION	L0000758	VOLUME	438292.666	3769002.590	302.66
LOCATION	L0000759	VOLUME	438286.570	3769002.608	302.66
LOCATION	L0000760	VOLUME	438281.935	3769001.176	302.60
LOCATION	L0000761	VOLUME	438281.996	3768995.080	302.34
LOCATION	L0000762	VOLUME	438282.058	3768988.984	302.07
LOCATION	L0000763	VOLUME	438282.119	3768982.889	301.81
LOCATION	L0000764	VOLUME	438282.180	3768976.793	301.54
LOCATION	L0000765	VOLUME	438282.242	3768970.697	301.28
LOCATION	L0000766	VOLUME	438282.303	3768964.601	301.02
LOCATION	L0000767	VOLUME	438282.365	3768958.506	300.75
LOCATION	L0000768	VOLUME	438282.426	3768952.410	300.49
LOCATION	L0000769	VOLUME	438282.465	3768946.317	300.22
LOCATION	L0000770	VOLUME	438280.977	3768940.405	299.97
LOCATION	L0000771	VOLUME	438279.490	3768934.494	299.71
LOCATION	L0000772	VOLUME	438278.002	3768928.582	299.46
LOCATION	L0000773	VOLUME	438276.514	3768922.670	299.20
LOCATION	L0000774	VOLUME	438275.026	3768916.759	298.87
LOCATION	L0000775	VOLUME	438273.539	3768910.847	298.27
LOCATION	L0000776	VOLUME	438272.051	3768904.935	297.67
LOCATION	L0000777	VOLUME	438271.518	3768898.903	297.06
LOCATION	L0000778	VOLUME	438271.487	3768892.807	296.44

LOCATION	VOLUME				
L0000779	438271.456	3768886.711	295.83		
L0000780	438271.425	3768880.615	295.21		
L0000781	438271.394	3768874.519	294.60		
L0000782	438271.362	3768868.423	293.98		
L0000783	438271.331	3768862.327	293.36		
L0000784	438271.300	3768856.231	292.75		
L0000785	438271.269	3768850.136	292.13		
L0000786	438271.238	3768844.040	291.52		
L0000787	438271.207	3768837.944	290.90		
L0000788	438271.176	3768831.848	290.28		
L0000789	438271.145	3768825.752	289.67		
L0000790	438271.114	3768819.656	289.60		
L0000791	438271.082	3768813.560	289.53		
L0000792	438270.878	3768807.506	289.46		
L0000793	438268.120	3768802.070	289.33		
L0000794	438265.361	3768796.634	289.20		
L0000795	438261.166	3768792.756	289.05		
L0000796	438255.523	3768790.451	288.88		
L0000797	438249.879	3768788.146	288.71		
L0000798	438243.840	3768787.991	288.58		
L0000799	438237.745	3768788.132	288.53		
L0000800	438231.651	3768788.273	288.49		
L0000801	438225.556	3768788.415	288.44		
L0000802	438219.462	3768788.556	288.39		
L0000803	438213.368	3768788.697	288.35		
L0000804	438207.273	3768788.838	288.30		

** End of LINE VOLUME Source ID = RDSTATEW

** -----

** Line Source Represented by Adjacent Volume Sources

** LINE VOLUME Source ID = RDSTATEE

** DESCRSRC State Street East of Project Site to San Antonio Ave

** PREFIX

** Length of Side = 6.10

** Configuration = Adjacent

** Emission Rate = 1.57E-06

** Vertical Dimension = 1.83

** SZINIT = 0.85

** Nodes = 4

** 438672.202, 3769002.020, 302.84, 0.00, 2.84

** 438789.448, 3769002.707, 304.97, 0.00, 2.84

** 438842.357, 3769006.954, 303.08, 0.00, 2.84

** 439021.762, 3769005.515, 302.13, 0.00, 2.84

** -----

LOCATION	VOLUME				
L0000805	438675.250	3769002.038	302.76		
L0000806	438681.345	3769002.074	302.98		
L0000807	438687.441	3769002.110	303.19		
L0000808	438693.537	3769002.145	303.41		
L0000809	438699.633	3769002.181	303.63		
L0000810	438705.729	3769002.217	303.85		
L0000811	438711.825	3769002.253	303.98		
L0000812	438717.921	3769002.288	304.05		
L0000813	438724.017	3769002.324	304.13		
L0000814	438730.113	3769002.360	304.20		
L0000815	438736.208	3769002.395	304.27		

LOCATION	VOLUME				
L0000816	VOLUME	438742.304	3769002.431	304.34	
L0000817	VOLUME	438748.400	3769002.467	304.41	
L0000818	VOLUME	438754.496	3769002.503	304.49	
L0000819	VOLUME	438760.592	3769002.538	304.56	
L0000820	VOLUME	438766.688	3769002.574	304.63	
L0000821	VOLUME	438772.784	3769002.610	304.70	
L0000822	VOLUME	438778.880	3769002.645	304.78	
L0000823	VOLUME	438784.976	3769002.681	304.85	
L0000824	VOLUME	438791.066	3769002.837	304.71	
L0000825	VOLUME	438797.143	3769003.325	304.57	
L0000826	VOLUME	438803.219	3769003.813	304.43	
L0000827	VOLUME	438809.296	3769004.301	304.29	
L0000828	VOLUME	438815.372	3769004.788	304.14	
L0000829	VOLUME	438821.449	3769005.276	304.00	
L0000830	VOLUME	438827.525	3769005.764	303.86	
L0000831	VOLUME	438833.602	3769006.252	303.72	
L0000832	VOLUME	438839.678	3769006.739	303.58	
L0000833	VOLUME	438845.765	3769006.927	303.43	
L0000834	VOLUME	438851.861	3769006.878	303.27	
L0000835	VOLUME	438857.957	3769006.829	303.11	
L0000836	VOLUME	438864.053	3769006.780	303.00	
L0000837	VOLUME	438870.148	3769006.731	303.00	
L0000838	VOLUME	438876.244	3769006.682	303.00	
L0000839	VOLUME	438882.340	3769006.633	303.00	
L0000840	VOLUME	438888.436	3769006.585	303.00	
L0000841	VOLUME	438894.532	3769006.536	303.00	
L0000842	VOLUME	438900.628	3769006.487	303.00	
L0000843	VOLUME	438906.723	3769006.438	303.00	
L0000844	VOLUME	438912.819	3769006.389	303.00	
L0000845	VOLUME	438918.915	3769006.340	303.00	
L0000846	VOLUME	438925.011	3769006.291	303.00	
L0000847	VOLUME	438931.107	3769006.242	303.00	
L0000848	VOLUME	438937.202	3769006.193	303.00	
L0000849	VOLUME	438943.298	3769006.144	302.94	
L0000850	VOLUME	438949.394	3769006.095	302.86	
L0000851	VOLUME	438955.490	3769006.047	302.79	
L0000852	VOLUME	438961.586	3769005.998	302.71	
L0000853	VOLUME	438967.681	3769005.949	302.63	
L0000854	VOLUME	438973.777	3769005.900	302.55	
L0000855	VOLUME	438979.873	3769005.851	302.47	
L0000856	VOLUME	438985.969	3769005.802	302.39	
L0000857	VOLUME	438992.065	3769005.753	302.31	
L0000858	VOLUME	438998.160	3769005.704	302.23	
L0000859	VOLUME	439004.256	3769005.655	302.15	
L0000860	VOLUME	439010.352	3769005.606	302.07	
L0000861	VOLUME	439016.448	3769005.557	302.00	

** End of LINE VOLUME Source ID = RDSTATEE
 ** -----
 ** Line Source Represented by Adjacent Volume Sources
 ** LINE VOLUME Source ID = RDONW
 ** DESCRSRC Onsite West Driveway
 ** PREFIX
 ** Length of Side = 3.66
 ** Configuration = Adjacent

** Emission Rate = 1.35E-06
 ** Vertical Dimension = 1.83
 ** SZINIT = 0.85
 ** Nodes = 2
 ** 438484.240, 3768997.613, 301.37, 0.00, 1.70
 ** 438483.840, 3768803.245, 295.33, 0.00, 1.70

LOCATION		VOLUME			
LOCATION L0000862		VOLUME	438484.236	3768995.784	301.36
LOCATION L0000863		VOLUME	438484.229	3768992.126	301.48
LOCATION L0000864		VOLUME	438484.221	3768988.469	301.60
LOCATION L0000865		VOLUME	438484.214	3768984.811	301.72
LOCATION L0000866		VOLUME	438484.206	3768981.154	301.85
LOCATION L0000867		VOLUME	438484.199	3768977.496	301.97
LOCATION L0000868		VOLUME	438484.191	3768973.838	302.09
LOCATION L0000869		VOLUME	438484.184	3768970.181	302.21
LOCATION L0000870		VOLUME	438484.176	3768966.523	302.33
LOCATION L0000871		VOLUME	438484.169	3768962.866	302.46
LOCATION L0000872		VOLUME	438484.161	3768959.208	302.58
LOCATION L0000873		VOLUME	438484.154	3768955.551	302.70
LOCATION L0000874		VOLUME	438484.146	3768951.893	302.82
LOCATION L0000875		VOLUME	438484.139	3768948.235	302.95
LOCATION L0000876		VOLUME	438484.131	3768944.578	303.07
LOCATION L0000877		VOLUME	438484.124	3768940.920	303.19
LOCATION L0000878		VOLUME	438484.116	3768937.263	303.31
LOCATION L0000879		VOLUME	438484.108	3768933.605	303.43
LOCATION L0000880		VOLUME	438484.101	3768929.947	303.56
LOCATION L0000881		VOLUME	438484.093	3768926.290	303.68
LOCATION L0000882		VOLUME	438484.086	3768922.632	303.80
LOCATION L0000883		VOLUME	438484.078	3768918.975	303.92
LOCATION L0000884		VOLUME	438484.071	3768915.317	303.87
LOCATION L0000885		VOLUME	438484.063	3768911.659	303.52
LOCATION L0000886		VOLUME	438484.056	3768908.002	303.17
LOCATION L0000887		VOLUME	438484.048	3768904.344	302.82
LOCATION L0000888		VOLUME	438484.041	3768900.687	302.47
LOCATION L0000889		VOLUME	438484.033	3768897.029	302.12
LOCATION L0000890		VOLUME	438484.026	3768893.371	301.77
LOCATION L0000891		VOLUME	438484.018	3768889.714	301.43
LOCATION L0000892		VOLUME	438484.011	3768886.056	301.08
LOCATION L0000893		VOLUME	438484.003	3768882.399	300.73
LOCATION L0000894		VOLUME	438483.996	3768878.741	300.38
LOCATION L0000895		VOLUME	438483.988	3768875.084	300.03
LOCATION L0000896		VOLUME	438483.981	3768871.426	299.68
LOCATION L0000897		VOLUME	438483.973	3768867.768	299.33
LOCATION L0000898		VOLUME	438483.966	3768864.111	298.99
LOCATION L0000899		VOLUME	438483.958	3768860.453	298.64
LOCATION L0000900		VOLUME	438483.950	3768856.796	298.29
LOCATION L0000901		VOLUME	438483.943	3768853.138	297.94
LOCATION L0000902		VOLUME	438483.935	3768849.480	297.59
LOCATION L0000903		VOLUME	438483.928	3768845.823	297.24
LOCATION L0000904		VOLUME	438483.920	3768842.165	296.90
LOCATION L0000905		VOLUME	438483.913	3768838.508	296.55
LOCATION L0000906		VOLUME	438483.905	3768834.850	296.20
LOCATION L0000907		VOLUME	438483.898	3768831.192	295.85
LOCATION L0000908		VOLUME	438483.890	3768827.535	295.50

LOCATION L0000909	VOLUME	438483.883	3768823.877	295.19
LOCATION L0000910	VOLUME	438483.875	3768820.220	295.19
LOCATION L0000911	VOLUME	438483.868	3768816.562	295.19
LOCATION L0000912	VOLUME	438483.860	3768812.904	295.18
LOCATION L0000913	VOLUME	438483.853	3768809.247	295.18
LOCATION L0000914	VOLUME	438483.845	3768805.589	295.18

** End of LINE VOLUME Source ID = RDOWN
 ** -----
 ** Line Source Represented by Adjacent Volume Sources
 ** LINE VOLUME Source ID = RDONE
 ** DESCRSRC Onsite East Driveway
 ** PREFIX
 ** Length of Side = 3.66
 ** Configuration = Adjacent
 ** Emission Rate = 1.16E-06
 ** Vertical Dimension = 1.83
 ** SZINIT = 0.85
 ** Nodes = 2
 ** 438674.078, 3768998.696, 302.96, 0.00, 1.70
 ** 438672.120, 3768830.955, 299.03, 0.00, 1.70
 ** -----

LOCATION L0000915	VOLUME	438674.056	3768996.868	302.76
LOCATION L0000916	VOLUME	438674.014	3768993.210	302.79
LOCATION L0000917	VOLUME	438673.971	3768989.553	302.82
LOCATION L0000918	VOLUME	438673.928	3768985.896	302.85
LOCATION L0000919	VOLUME	438673.886	3768982.238	302.88
LOCATION L0000920	VOLUME	438673.843	3768978.581	302.91
LOCATION L0000921	VOLUME	438673.800	3768974.924	302.94
LOCATION L0000922	VOLUME	438673.758	3768971.266	302.97
LOCATION L0000923	VOLUME	438673.715	3768967.609	303.00
LOCATION L0000924	VOLUME	438673.672	3768963.951	303.03
LOCATION L0000925	VOLUME	438673.630	3768960.294	303.07
LOCATION L0000926	VOLUME	438673.587	3768956.637	303.10
LOCATION L0000927	VOLUME	438673.544	3768952.979	303.13
LOCATION L0000928	VOLUME	438673.502	3768949.322	303.16
LOCATION L0000929	VOLUME	438673.459	3768945.665	303.19
LOCATION L0000930	VOLUME	438673.416	3768942.007	303.22
LOCATION L0000931	VOLUME	438673.373	3768938.350	303.25
LOCATION L0000932	VOLUME	438673.331	3768934.693	303.28
LOCATION L0000933	VOLUME	438673.288	3768931.035	303.31
LOCATION L0000934	VOLUME	438673.245	3768927.378	303.35
LOCATION L0000935	VOLUME	438673.203	3768923.721	303.38
LOCATION L0000936	VOLUME	438673.160	3768920.063	303.41
LOCATION L0000937	VOLUME	438673.117	3768916.406	303.44
LOCATION L0000938	VOLUME	438673.075	3768912.749	303.32
LOCATION L0000939	VOLUME	438673.032	3768909.091	303.15
LOCATION L0000940	VOLUME	438672.989	3768905.434	302.98
LOCATION L0000941	VOLUME	438672.947	3768901.777	302.81
LOCATION L0000942	VOLUME	438672.904	3768898.119	302.63
LOCATION L0000943	VOLUME	438672.861	3768894.462	302.46
LOCATION L0000944	VOLUME	438672.819	3768890.804	302.29
LOCATION L0000945	VOLUME	438672.776	3768887.147	302.12
LOCATION L0000946	VOLUME	438672.733	3768883.490	301.94
LOCATION L0000947	VOLUME	438672.691	3768879.832	301.77

LOCATION	L0000948	VOLUME	438672.648	3768876.175	301.60
LOCATION	L0000949	VOLUME	438672.605	3768872.518	301.43
LOCATION	L0000950	VOLUME	438672.563	3768868.860	301.25
LOCATION	L0000951	VOLUME	438672.520	3768865.203	301.08
LOCATION	L0000952	VOLUME	438672.477	3768861.546	300.91
LOCATION	L0000953	VOLUME	438672.435	3768857.888	300.74
LOCATION	L0000954	VOLUME	438672.392	3768854.231	300.56
LOCATION	L0000955	VOLUME	438672.349	3768850.574	300.39
LOCATION	L0000956	VOLUME	438672.306	3768846.916	300.22
LOCATION	L0000957	VOLUME	438672.264	3768843.259	300.04
LOCATION	L0000958	VOLUME	438672.221	3768839.602	299.87
LOCATION	L0000959	VOLUME	438672.178	3768835.944	299.70
LOCATION	L0000960	VOLUME	438672.136	3768832.287	299.52
** End of LINE VOLUME Source ID = RDONE					
LOCATION	IDLINGW	POINT	438508.800	3768873.340	300.180
** DESCRSRC Total Idling West Loading Dock					
LOCATION	IDLINGE	POINT	438646.330	3768887.240	302.140
** DESCRSRC Total Idling East Loading Dock					
** Source Parameters **					
** LINE VOLUME Source ID = RDSTATEW					
SRCPARAM	L0000727	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000728	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000729	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000730	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000731	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000732	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000733	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000734	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000735	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000736	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000737	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000738	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000739	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000740	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000741	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000742	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000743	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000744	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000745	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000746	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000747	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000748	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000749	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000750	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000751	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000752	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000753	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000754	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000755	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000756	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000757	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000758	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000759	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000760	0.00000002744	0.00	2.84	0.85

SRCPARAM	L0000761	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000762	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000763	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000764	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000765	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000766	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000767	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000768	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000769	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000770	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000771	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000772	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000773	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000774	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000775	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000776	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000777	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000778	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000779	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000780	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000781	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000782	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000783	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000784	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000785	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000786	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000787	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000788	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000789	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000790	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000791	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000792	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000793	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000794	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000795	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000796	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000797	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000798	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000799	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000800	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000801	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000802	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000803	0.00000002744	0.00	2.84	0.85
SRCPARAM	L0000804	0.00000002744	0.00	2.84	0.85

**

** LINE VOLUME Source ID = RDSTATEE

SRCPARAM	L0000805	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000806	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000807	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000808	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000809	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000810	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000811	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000812	0.00000002754	0.00	2.84	0.85

SRCPARAM	L0000813	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000814	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000815	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000816	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000817	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000818	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000819	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000820	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000821	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000822	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000823	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000824	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000825	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000826	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000827	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000828	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000829	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000830	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000831	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000832	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000833	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000834	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000835	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000836	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000837	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000838	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000839	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000840	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000841	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000842	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000843	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000844	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000845	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000846	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000847	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000848	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000849	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000850	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000851	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000852	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000853	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000854	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000855	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000856	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000857	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000858	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000859	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000860	0.00000002754	0.00	2.84	0.85
SRCPARAM	L0000861	0.00000002754	0.00	2.84	0.85

**

 ** LINE VOLUME Source ID = RDONW

SRCPARAM	L0000862	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000863	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000864	0.00000002547	0.00	1.70	0.85

SRCPARAM	L0000865	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000866	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000867	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000868	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000869	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000870	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000871	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000872	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000873	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000874	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000875	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000876	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000877	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000878	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000879	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000880	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000881	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000882	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000883	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000884	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000885	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000886	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000887	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000888	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000889	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000890	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000891	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000892	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000893	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000894	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000895	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000896	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000897	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000898	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000899	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000900	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000901	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000902	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000903	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000904	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000905	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000906	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000907	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000908	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000909	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000910	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000911	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000912	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000913	0.00000002547	0.00	1.70	0.85
SRCPARAM	L0000914	0.00000002547	0.00	1.70	0.85

**

 ** LINE VOLUME Source ID = RDONE

SRCPARAM	L0000915	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000916	0.00000002522	0.00	1.70	0.85

SRCPARAM	L0000917	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000918	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000919	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000920	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000921	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000922	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000923	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000924	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000925	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000926	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000927	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000928	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000929	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000930	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000931	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000932	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000933	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000934	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000935	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000936	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000937	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000938	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000939	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000940	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000941	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000942	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000943	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000944	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000945	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000946	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000947	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000948	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000949	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000950	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000951	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000952	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000953	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000954	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000955	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000956	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000957	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000958	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000959	0.00000002522	0.00	1.70	0.85
SRCPARAM	L0000960	0.00000002522	0.00	1.70	0.85

**

SRCPARAM IDLINGW 0.0000294 3.840 366.000 50.00000
0.100

SRCPARAM IDLINGE 0.0000294 3.840 366.000 50.00000
0.100

URBANSRC ALL
SRCGROUP ALL

SO FINISHED

**

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** AERMOD Receptor Pathway
*****
**
**
RE STARTING
  INCLUDED 2038.rou
RE FINISHED
**
*****
** AERMOD Meteorology Pathway
*****
**
**
ME STARTING
  SURFFILE ..\OntarioIntlAirportADJU\KONT_V9_ADJU\KONT_v9.SFC
  PROFFILE ..\OntarioIntlAirportADJU\KONT_V9_ADJU\KONT_v9.PFL
  SURFDATA 3102 2012 Ontario_Airport
  UAIRDATA 3190 2012
  PROFBASE 289.0 METERS
ME FINISHED
**
*****
** AERMOD Output Pathway
*****
**
**
OU STARTING
  RECTABLE ALLAVE 1ST
  RECTABLE 24 1ST
** Auto-Generated Plotfiles
  PLOTFILE 24 ALL 1ST 2038.AD\24H1GALL.PLT 31
  PLOTFILE ANNUAL ALL 2038.AD\AN00GALL.PLT 32
  SUMMFILE 2038.sum
OU FINISHED
**
*****
** Project Parameters
*****
** PROJCTN  CoordinateSystemUTM
** DESCPTN  UTM: Universal Transverse Mercator
** DATUM    World Geodetic System 1984
** DTMRGN   Global Definition
** UNITS    m
** ZONE     11
** ZONEINX  0
**

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11/25/20
16:33:54

* AERMOD (19191): State Distribution Project - DPM Emission Years 2038 - 2051

* AERMET (16216):

* MODELING OPTIONS USED: RegDEFAULT CONC ELEV URBAN ADJ_U*

* PLOT FILE OF ANNUAL VALUES AVERAGED ACROSS 5 YEARS FOR SOURCE GROUP: ALL

* FOR A TOTAL OF 15 RECEPTORS.

* FORMAT: (3(1X,F13.5),3(1X,F8.2),2X,A6,2X,A8,2X,I8,2X,A8)

X	Y	AVERAGE CONC	ZELEV	ZHILL	ZFLAG	AVE	GRP	NUM YRS	NET ID
438872.00000	3769195.00000	0.00038	307.98	3062.00	0.00	ANNUAL	ALL	00000005	
438949.00000	3769181.00000	0.00045	308.58	3062.00	0.00	ANNUAL	ALL	00000005	
438758.87000	3768908.02000	0.00348	302.21	3062.00	0.00	ANNUAL	ALL	00000005	
438755.00000	3768841.00000	0.00147	300.97	3062.00	0.00	ANNUAL	ALL	00000005	
438772.00000	3768779.00000	0.00071	298.50	3062.00	0.00	ANNUAL	ALL	00000005	
438708.00000	3768777.00000	0.00086	297.54	3062.00	0.00	ANNUAL	ALL	00000005	
438649.00000	3768776.00000	0.00103	296.71	3062.00	0.00	ANNUAL	ALL	00000005	
438597.00000	3768773.00000	0.00117	296.40	3062.00	0.00	ANNUAL	ALL	00000005	
438359.00000	3768775.00000	0.00072	291.67	3062.00	0.00	ANNUAL	ALL	00000005	
438469.02000	3768785.16000	0.00134	294.82	3062.00	0.00	ANNUAL	ALL	00000005	
438676.53000	3768785.16000	0.00103	297.48	3062.00	0.00	ANNUAL	ALL	00000005	
438677.48000	3768827.34000	0.00190	299.42	3062.00	0.00	ANNUAL	ALL	00000005	
438686.59000	3768827.34000	0.00166	299.65	3062.00	0.00	ANNUAL	ALL	00000005	
438688.51000	3768996.02000	0.00284	303.23	3062.00	0.00	ANNUAL	ALL	00000005	
438471.42000	3768996.50000	0.00154	301.45	3062.00	0.00	ANNUAL	ALL	00000005	

** CONCUNIT ug/m^3

** DEPUNIT g/m^2

Attachment B:
LOZEAU DRURY LLP – SAFER
LETTER – MARCH 22,2021

(Document follows this page)



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VIA EMAIL and OVERNIGHT MAIL

March 22, 2021

Honorable Members of the Planning Commission
City of Ontario
Ontario City Hall
303 East B Street
Ontario, CA 91764
planningdirector@ontarioca.gov

Rudy Zeledon, Director
City of Ontario Planning Department
303 East B Street
Ontario, CA 91764
planningdirector@ontarioca.gov

Denny D.Chen, Associate Planner
City of Ontario Planning Department
303 East B Street
Ontario, CA 91764
dchen@ontarioca.gov

Re: Comment on NOD for PDEV20-005: Inland Harbor LLC

Honorable Planning Commissioners, Director Zeledon and Mr. Chen,

I am writing on behalf of Supporters Alliance for Environmental Responsibility ("SAFER") regarding the Notice of Determination ("NOD") and supporting environmental analysis prepared for PDEV20-005, a proposed 256,711 square foot industrial building located on 11.3 acres of land at 875 West State Street, on APNs 1011-161-04 and 1011-161-05, in Ontario, submitted by Inland Harbor LLC ("Project"). This matter is scheduled to be considered by the Planning Commission ("Commission") on March 23, 2021 as agenda item A-02.

The staff report for the Commission hearing proposes to dispense with all review of the large warehouse under the California Environmental Quality Act ("CEQA"). The City is proposing to approve the Project without review under the California Environmental Quality Act ("CEQA"), Pub. Res. Code section 21000, et seq., based on the assertion that the Project was analyzed in The Ontario Plan (File No. PGPA06-001) Environmental Impact Report (SCH# 2008101140), certified by City Council on January 27, 2010. ("2010 TOP EIR"). The City contends that under CEQA Guidelines section 15162 and 15164, no further environmental review is required. The City has not even prepared an Addendum for the Project, but seeks to rely solely on the decade-old TOP EIR.

The City's proposed CEQA review is inadequate because the Project was not analyzed in the 2010 TOP EIR at all. The Ontario Plan is the General Plan for the entire City of Ontario, and the 2010 TOP EIR analyzed environmental impacts at an extremely general level - not at a project-specific level. The proposed Project will generate large amounts of diesel heavy truck traffic, construction emissions, diesel yard equipment such as fork lifts, noise from truck traffic and back-up beepers, and many other impacts. None of these project-specific impacts were analyzed in the 2010 TOP EIR. These impacts must be analyzed and mitigated in a project-level environmental impact report ("EIR").

I. LEGAL STANDARD

CEQA contains a strong presumption in favor of requiring a lead agency to prepare an EIR. This presumption is reflected in the fair argument standard. Under that standard, a lead agency must prepare an EIR whenever substantial evidence in the whole record before the agency supports a fair argument that a project may have a significant effect on the environment. (Pub. Res. Code § 21082.2; *Laurel Heights Improvement Ass'n v. Regents of the University of California* (1993) ("*Laurel Heights II*") 6 Cal.4th 1112, 1123; *No Oil, Inc. v. City of Los Angeles* (1974) 13 Cal.3d 68, 75, 82; *Quail Botanical Gardens v. City of Encinitas* (1994) 29 Cal.App.4th 1597, 1602.)

A. Preparation of an Addendum Under CEQA

The City contends that the Project was already analyzed in the 2010 TOP EIR. (Staff Rpt. At 8). However, the 2010 TOP EIR does not even mention this Project. Furthermore, even if the 2010 TOP EIR analyzed this Project (which it did not), a Supplemental EIR ("SEIR") would be required pursuant to CEQA section 21166 and CEQA Guidelines section 15162. At the very least a CEQA addendum should have been prepared pursuant to CEQA Guidelines section 15164.

Section 15164(a) of the State CEQA Guidelines states that "[t]he lead agency or a responsible agency shall prepare an addendum to a previously certified EIR if some changes or additions are necessary, but none of the conditions described in Section 15162 calling for preparation of a subsequent EIR have occurred." (14 CCR § 15164(a).) Pursuant to Section 15162(a) of the State CEQA Guidelines, "[w]hen an EIR has been certified or a negative declaration adopted for a project, no subsequent EIR shall be prepared for that project" unless the agency determines one or more of the following exists:

- 1) Substantial changes are proposed in the project which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;

(2) Substantial changes occur with respect to the circumstances under which the project is undertaken which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or

(3) New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the previous EIR was certified as complete or the negative declaration was adopted, shows any of the following:

(A) The project will have one or more significant effects not discussed in the previous EIR or negative declaration;

(B) Significant effects previously examined will be substantially more severe than shown in the previous EIR;

(C) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or

(D) Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative.

(14 CCR § 15162(a).)

B. Tiering Under CEQA

CEQA permits agencies to 'tier' EIRs, in which general matters and environmental effects are considered in an EIR "prepared for a policy, plan, program or ordinance followed by narrower or site-specific [EIRs] which incorporate by reference the discussion in any prior [EIR] and which concentrate on the environmental effects which (a) are capable of being mitigated, or (b) were not analyzed as significant effects on the environment in the prior [EIR]." (Cal. Pub. Res. Code ("PRC") § 21068.5.) "[T]iering is appropriate when it helps a public agency to focus upon the issues ripe for decision at each level of environmental review and in order to exclude duplicative analysis of environmental effects examined in previous [EIRs]." (Id. § 21093.) The initial general policy-oriented EIR is called a programmatic EIR ("PEIR") and offers the advantage of allowing "the lead agency to consider broad policy alternatives and program wide mitigation measures at an early time when the agency has greater flexibility to deal with basic problems or cumulative impacts." (14 CCR § 15168(a).) CEQA regulations strongly promote tiering of EIRs, stating that "[EIRs] shall be tiered whenever feasible, as determined by the lead agency." (PRC § 21093.)

"Later activities in the program must be examined in light of the program EIR to determine whether an additional environmental document must be prepared." (14 CCR

§ 15168(c).) The first consideration is whether the activity proposed is covered by the PEIR. (Id. § 15168(c)(2).) If a later project is outside the scope of the program, then it is treated as a separate project and the PEIR may not be relied upon in further review. (See *Sierra Club v. County of Sonoma* (1992) 6 Cal.App.4th 1307, 1320-21.) The second consideration is whether the "later activity would have effects that were not examined in the program EIR." (14 CCR § 15168(c)(1).) A PEIR may only serve "to the extent that it contemplates and adequately analyzes the potential environmental impacts of the project . . ." (*Sierra Nevada Conservation v. County of El Dorado* (2012) 202 Cal.App.4th 1156, 1171 [quoting *Citizens for Responsible Equitable Env'tl. Dev. v. City of San Diego Redevelopment Agency* (2005) 134 Cal.App.4th 598, 615].) If the PEIR does not evaluate the environmental impacts of the project, a tiered EIR must be completed before the project is approved. (Id. at 1184.)

For these inquiries, the "fair argument test" applies. (*Sierra Club*, 6 Cal.App.4th at 1318; see also *Sierra Club v. County of San Diego* (2014) 231 Cal.App.4th 1152, 1164 ("when a prior EIR has been prepared and certified for a program or plan, the question for a court reviewing an agency's decision not to use a tiered EIR for a later project 'is one of law, i.e., 'the sufficiency of the evidence to support a fair argument.'" [quoting *Sierra Club*, 6 Cal.App.4th at 1318]).) Under the fair argument test, a new EIR must be prepared "whenever it can be fairly argued on the basis of substantial evidence that the project may have significant environmental impact. (*Sierra Club*, 6 Cal.App.4th at 1316 [quotations and citations omitted].) When applying the fair argument test, "deference to the agency's determination is not appropriate and its decision not to require an EIR can be upheld only when there is no credible evidence to the contrary." (Id. at 1318.) "[I]f there is substantial evidence in the record that the later project may arguably have a significant adverse effect on the environment which was not examined in the prior program EIR, doubts must be resolved in favor of environmental review and the agency must prepare a new tiered EIR, notwithstanding the existence of contrary evidence." (Id. at 1319.)

II. DISCUSSION

A. THE CITY CANNOT APPROVE THE PROJECT BECAUSE NO EIR OR NEGATIVE DECLARATION HAS BEEN ADOPTED FOR THIS PROJECT.

Most obviously, the City may not approve the Project because the proposed Project has not been subject to CEQA review and no EIR or negative declaration has ever been adopted for the project.

As the California Supreme Court explained in *San Mateo Gardens*, subsequent CEQA review provisions "can apply only if the project has been subject to initial review; they can have no application if the agency has proposed a new project that has not previously been subject to review." (*Friends of Coll. of San Mateo Gardens v. San Mateo County Cmty. Coll. Dist.* ("*San Mateo Gardens*") (2016) 1 Cal.5th 937, 950.)

Agencies can prepare addenda for project modifications or revisions and avoid further environmental review, but only if the project has a previously certified EIR. (See *Save our Heritage v. City of San Diego* (2018) 28 Cal.App.5th 656, 667.) Further, the Resource Agency designed the CEQA Guideline's addendum provision as a device to "mak[e] minor corrections in EIRs without recirculating the EIR." (Id. at 664-65 [referencing Resources Agency, Amendments to the State CEQA Guidelines, Text of Adopted Amendments with Statement of Reasons (Dec. 30, 1982), 100-01].)

The City contends that the Project was analyzed in the 2010 TOP EIR. However, this Project is nowhere mentioned in the 2010 TOP EIR. CEQA Guideline section 15164 requires agencies to prepare an addendum to an EIR or negative declaration if none of the conditions in Guideline section 15162 have occurred. (14 CCR § 15164(a).) However, Guideline section 15162 only applies if an EIR or negative declaration has been adopted for a project, allowing an agency to avoid preparing a "subsequent EIR . . . for that **project**" unless one or more of the listed conditions apply. (Id. [emphasis added].) Therefore, an agency can only prepare an addendum and avoid preparing an EIR for a project that has already undergone CEQA review, and for which an EIR has been certified or a negative declaration has been adopted for, so long as one of the conditions does not apply.

Here, the proposed Project has never undergone CEQA review. The proposed project was not mentioned or discussed in the 2010 TOP EIR and was not considered in the initial CEQA review the City points to for its use of the addendum provision. The City can therefore not rely on the 2010 TOP EIR to avoid CEQA review for the Project. Further, the proposed Project does not modify or revise the 2010 TOP EIR. In fact, the Project has no impact at all on the 2010 TOP EIR.

Since the City cannot rely on CEQA section 21166, or CEQA Guidelines sections 15162 or 15164 to avoid CEQA review for this Project, the Project must therefore undergo CEQA review and follow the tiering process.

B. CEQA REQUIRES THE CITY TO PREPARE A TIERED EIR FOR THE PROJECT.

The 2010 TOP EIR was not a project-specific EIR, which the CEQA Guidelines define as an "EIR [which] examines the environmental impacts of a specific development project." (14 CCR § 15161.) Rather, the 2010 TOP EIR was a General Plan EIR governing zoning across the entire City. As such, the 2010 TOP EIR is a Program EIR, which the CEQA Guidelines define as:

- . . . an EIR which may be prepared on a series of actions that can be characterized as one large project and are related either:
- (1) Geographically,
 - (2) As logical parts in the chain of contemplated actions,

- (3) In connection with issuance of rules, regulations, plans, or other general criteria to govern the conduct of a continuing program, or
- (4) As individual activities carried out under the same authorizing statutory or regulatory authority and having generally similar environmental effects which can be mitigated in similar ways.

(14 CCR § 15168.) Thus, instead of proceeding under the provisions of CEQA Guidelines sections 15162 or 15164, the City should have proceeded under section 15168 provisions for subsequent analysis for a Program EIR.

It has long been established that a General Plan EIR is not a project-specific EIR and does not eliminate the need to prepare project-specific EIRs for particular projects. (*Environmental Planning & Information Council v. County of El Dorado* (1982) 131 Cal.App.3d 350; *Woodward Park Homeowners Assn., Inc. v. City of Fresno* (2007) 149 Cal. App. 4th 683, 698). The General Plan EIR simply does not analyze the impacts of specific projects.

The instant Project is nowhere described in the TOP EIR or any other CEQA document. Without a clear and accurate description of the proposed Project, there is no CEQA review at all. As the Court of Appeal recently affirmed, “[a]n accurate, stable, and [consistent] project description is the *sin[er]e qua non* of an informative and legally sufficient EIR’ because a shifting project description may confuse the public and public decision-makers, thus vitiating the EIR’s usefulness as a vehicle for intelligent public participation. Accordingly, a project description ‘should be sufficiently detailed to provide a foundation for a complete analysis of the environmental impacts,’ and it should include all project components and ‘apprise the parties of the true scope of the project.’” (*Stopthemillenniumhollywood.com v. City of Los Angeles*, 39 Cal. App. 5th 1, (2019) (“Millenium”); quoting, *County of Inyo v. City of Los Angeles* (1977) 71 Cal.App.3d 185.) Put simply, since the instant Project is not described in any prior CEQA document, there has been no adequate CEQA review for the Project. There would have been no way for the public to intelligently comment on the Project in 2010 because the Project was not described, discussed or proposed at that time. See, Millenium; *Washoe Meadows Community v. Dept. of Parks & Recreation* (2017) 17 Cal.App.5th 277.

SAFER hereby requests that the City prepare an environmental impact report (“EIR”) to analyze the significant environmental impacts of the Project and to propose all feasible mitigation measures and alternatives to reduce those impacts. The City may not rely on the 2010 TOP EIR for several reasons, including but not limited to the following:

- The 2010 TOP EIR did not analyze this Project. It conducted only very broad program level analysis and did not analyze Project-level impacts. A prior CEQA document may only be used for a later project that is “essentially the same project” as was analyzed in the prior document. *Sierra Club v. County of*

Sonoma (1992) 6 Cal.App.4th 1307, 1320; *American Canyon Community v. American Canyon*, 145 Cal.App.4th 1062. The 2010 EIR did not analyze the Project at all.

- The 2010 TOP EIR included mitigation measures that were never implemented, including traffic mitigation measures. Since the City has failed to implement the mitigation measures required by the 2010 EIR, it may not now rely on that document. See, *Katzeff v. Dept. of Forestry* (2010) 181 Cal.App.4th 601, 611, 614; *Lincoln Place Tenants v. City of Los Angeles* (2005) 130 Cal.App.4th 1491, 1507 n22.
- The Project will have significant environmental impacts that were not analyzed in the 2010 TOP EIR. For example, the Project will have significant air quality, traffic and noise impacts from diesel trucks and other sources.
- CEQA was amended to require traffic analysis using vehicle miles travelled (VMT) rather than level of service (LOS). CEQA Guidelines Section 15064.3. By July 1, 2020, all CEQA lead agencies must analyze a project's transportation impacts using vehicle miles traveled (VMT). VMT measures the per capita number of car trips generated by a project and distances cars will travel to and from a project, rather than congestion levels at intersections (level of service or "LOS," graded on a scale of A – F). The 2010 TOP EIR used LOS analysis, not VMT. The Project's traffic impacts must be analyzed under the new VMT methodology consistent with Section 15064.3.
- There are many mitigation measures that are now feasible that were not feasible or did not exist in 2009, when the 2010 TOP EIR was prepared. For example, the Project could offset its air pollution and greenhouse gas emissions in part by installing solar photovoltaic panels, operating only 2010 or better diesel trucks, and many other measures that were not feasible in 2009. A new EIR is required to analyze these measures.
- There are numerous changed circumstances that have occurred since 2010 that require renewed environmental review. For example, traffic in the area is much heavier not than in 2009, when the area was at the height of a recession, population has grown in the area, etc.

C. EVEN IF THE 2010 TOP EIR IS STILL RELEVANT TO THE PROJECT, A TIERED EIR IS REQUIRED TO MITIGATE THE SIGNIFICANT UNAVOIDABLE IMPACTS OF THE PROGRAM IDENTIFIED IN THE 2010 TOP EIR.

The 2010 TOP EIR concluded that the program would have significant unavoidable impacts in the areas of:

- Agricultural Resources;
- Air Quality (including VOC, CO, NOx, PM-10, PM-2.5);
- Cultural Resources;
- Climate Change;
- Noise;
- Traffic.

(2010 TOP Draft EIR, pp. 1-19 through 1-36).

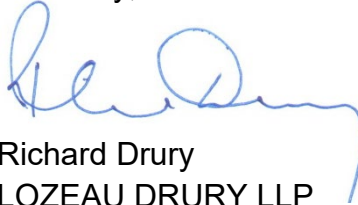
Since the overall program will have significant unavoidable impacts, the City must prepare a project-level supplemental EIR for the proposed Project to determine whether mitigation measures exist to reduce the significant unavoidable impacts identified in the 2010 TOP EIR.

In the case of *Communities for a Better Environment v. Cal. Resources Agency* (2002) 103 Cal.App.4th 98, 122-125, the court of appeal held that when a "first tier" EIR admits a significant, unavoidable environmental impact, then the agency must prepare second tier EIRs for later phases of the project to ensure that those unmitigated impacts are "mitigated or avoided." (*Id.* citing CEQA Guidelines §15152(f)) The court reasoned that the unmitigated impacts were not "adequately addressed" in the first tier EIR since they were not "mitigated or avoided." (*Id.*) Thus, significant effects disclosed in first tier EIRs will trigger second tier EIRs unless such effects have been "adequately addressed," in a way that ensures the effects will be "mitigated or avoided." (*Id.*) Such a second tier EIR is required, even if the impact still cannot be fully mitigated and a statement of overriding considerations will be required. The court explained, "The requirement of a statement of overriding considerations is central to CEQA's role as a public accountability statute; it requires public officials, in approving environmental detrimental projects, to justify their decisions based on counterbalancing social, economic or other benefits, and to point to substantial evidence in support." (*Id.* at 124-125) The court specifically rejected a prior version of the CEQA guidelines regarding tiering that would have allowed a statement of overriding considerations for a program-level project to be used for a later specific project within that program. (*Communities for a Better Env't v. California Res. Agency* (2001) 103 Cal.App.4th 98, 124, disapproved on other grounds by *Berkeley Hillside Pres. v. City of Berkeley* (2015) 60 Cal.4th 1086.) Even though "a prior EIR's analysis of environmental effects may be subject to being incorporated in a later EIR for a later, more specific project, the responsible public officials must still go on the record and explain specifically why they are approving the later project despite its significant unavoidable impacts." (*Id.*, pp. 124-25.)

III. CONCLUSION

For the above and other reasons, the City must prepare an EIR to analyze and mitigate the impacts of the Project. The City may not rely on the decade-old 2010 TOP EIR, which did not even analyze the proposed Project.

Sincerely,



Richard Drury
LOZEAU DRURY LLP

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO APPROVING AN ADDENDUM TO THE ONTARIO PLAN CERTIFIED ENVIRONMENTAL IMPACT REPORT (STATE CLEARINGHOUSE NO. 2008101140), FOR WHICH AN INITIAL STUDY WAS PREPARED, ALL IN ACCORDANCE WITH THE CALIFORNIA ENVIRONMENTAL QUALITY ACT, AS AMENDED, FOR FILE NO. PGPA06-001

WHEREAS, INLAND HARBOR, LLC. (hereinafter referred to as "Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV20-005, which consists of the proposed construction of a 256,711 square foot industrial building on 11.3 acres of land located at 875 West State Street, within the IL (Light Industrial) zoning district, in the City of Ontario, California (hereinafter referred to as "Application" or "Project"); and

WHEREAS, The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was certified by the City Council on January 27, 2010 (hereinafter referred to as "Certified EIR"), in which development and use of the Project site was discussed; and

WHEREAS, prior to the adoption of this Resolution, the Planning Director of the City of Ontario prepared and approved for attachment to the certified Environmental Impact Report, an addendum to The Ontario Plan ("TOP") Certified Environmental Impact Report (State Clearinghouse No. 2008101140) — for File No. PGPA06-001 and PDEV20-005 (hereinafter referred to as "EIR Addendum"), all in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as "CEQA"); and

WHEREAS, the EIR Addendum concluded that implementation of the Project could result in a number of significant effects on the environment and identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) adopted by City Council on January 27, 2010, in conjunction with File No. PGPA06-001 (hereinafter referred to as "Certified EIR"), in which development and use of the Project site was discussed; and

WHEREAS, pursuant to California Environmental Quality Act ("CEQA") Guidelines Section 15164(a), a lead agency shall prepare an addendum to a previously certified EIR if some changes or additions are necessary to a project, but the preparation of a subsequent or supplemental EIR is not required; and

WHEREAS, the City determined that none of the conditions requiring preparation of a subsequent or supplemental Environmental Impact Report would occur from the Project, and that preparation of an addendum to the Environmental Impact Report was appropriate; and

WHEREAS, the City of Ontario is the lead agency on the Project, and the Planning Commission is the decision-making authority for the requested approval to construct and otherwise undertake the Project; and

WHEREAS, the Planning Commission has reviewed and considered the EIR Addendum for the Project, has concluded that none of the conditions requiring preparation of a subsequent or supplemental Environmental Impact Report have occurred, and intends to take actions on the Project in compliance with CEQA and state and local guidelines implementing CEQA; and

WHEREAS, the EIR Addendum for the Project are on file in the Planning Department, located at 303 East B Street, Ontario, CA 91764, are available for inspection by any interested person at that location and are, by this reference, incorporated into this Resolution as if fully set forth herein; and

WHEREAS, on March 23, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and continued the Project to the April 27, 2021 hearing date; and

WHEREAS, on April 27, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and continued the Project to the May 25, 2021 hearing date; and

WHEREAS, on May 25, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the decision-making authority for the Project, The Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The environmental impacts of this project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) adopted by City Council on January 27, 2010, in conjunction with File No. PGPA06-001.

(2) The EIR Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines.

(3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts.

(4) All previously adopted mitigation measures shall be a condition of project approval, as they are applicable to the Project, and are incorporated herein by this reference.

(5) The EIR Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and

(6) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts.

SECTION 2: Additional Environmental Review Not Required. Based on the Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Environmental Impact Report is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

(d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: *Planning Commission Action.* Based upon the findings and conclusions set forth in Sections 1 and 2, above, the Planning Commission hereby finds that based upon the entire record of proceedings before it, and all information received, that there is no substantial evidence that the Project will constitute substantial changes to the Certified EIR, and does hereby approve the EIR Addendum, attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 4: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 5: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 6: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of May 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No.____, was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 25, 2021 by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:
Addendum to The Ontario Plan
Environmental Impact Report

(Addendum to follow this page)



City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

California Environmental Quality Act Addendum to The Ontario Plan Environmental Impact Report

Project Title/File No.: PDEV20-005

Lead Agency: City of Ontario, 303 East "B" Street, Ontario, California 91764, (909) 395-2036

Contact Person: Denny D. Chen, Associate Planner, 909-395-2424

Project Sponsor: Inland Harbor LLC, 3835 Valleybrink Road, Los Angeles, California 90039

Project Location: The project site is located in southwestern San Bernardino County, within the City of Ontario. The City of Ontario is located approximately 40 miles from downtown Los Angeles, 20 miles from downtown San Bernardino, and 30 miles from Orange County. As illustrated on Figures 1 through 2, below, the project site is comprised of 11.3 acres of land located at 875 West State Street. (Assessor Parcel Numbers: 1011-161-04 and 1011-161-05)

FIGURE 1: REGIONAL LOCATION MAP

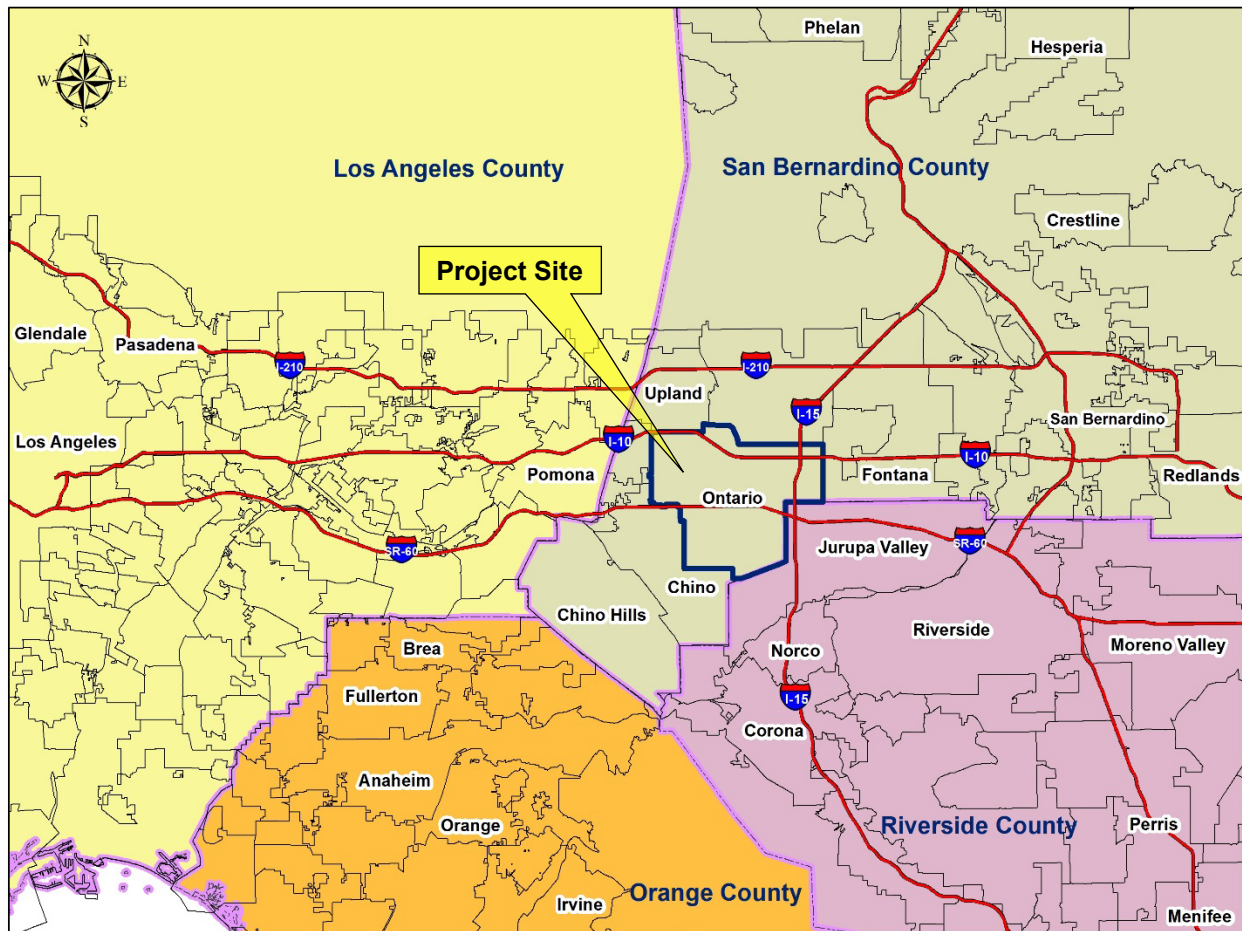


FIGURE 2: PROJECT SITE



General Plan Designation: Industrial (0.55 FAR)

Zoning: IL - Light Industrial

Description of Project: A Development Plan to construct a 256,711 square foot industrial building on 11.3 acres of land located at 875 West State Street (formerly 835 and 920 West State Street), within the IL (Light Industrial) zoning district.

Project Setting: The Project site is comprised of 11.3 acres of land located on the south side of State Street, mid-block between Palmetto and Cypress Avenues, at 875 West State Street. The site is currently used as a metal container and truck trailer storage yard. The site is surrounded by Union Pacific Railroad tracks to the north, a diesel repair shop to the east, an industrial building, and the Grove Manor Mobile Home Park to the south, and industrial buildings to the west.

Background: On January 27, 2010, the Ontario City Council adopted The Ontario Plan ("TOP"). TOP serves as the framework for the City's business plan and provides a foundation for the City to operate as a municipal corporation that consists of six (6) distinct components: 1) Vision; 2) Governance Manual; 3) Policy Plan; 4) Council Priorities; 5) Implementation; and 6) Tracking and Feedback. The Policy Plan component of TOP meets the functional and legal mandate of a General Plan and contains nine elements: Land Use, Housing, Parks and Recreation, Environmental Resources, Community Economics, Safety, Mobility, Community Design and Social Resources.

An Environmental Impact Report ("EIR") prepared for TOP (State Clearinghouse No. 2008101140) was certified by the City Council on January 27, 2010, (hereinafter referred to as "Certified EIR") which included Mitigation, Findings and a Statement of Overriding Considerations pursuant to the requirements of the California Environmental Quality Act, commencing with Public Resources Code Section 21000 ("CEQA"). The Certified EIR analyzed the direct and physical changes in the environment that would be caused by TOP; focusing on changes to land use associated with the buildout of the proposed land use plan, and in the Policy Plan and impacts resultant of population and employment growth in the City. The subject site was analyzed in the Certified EIR as Industrial. The significant unavoidable adverse impacts that were identified in Certified EIR included agriculture resources, air quality, cultural resources, greenhouse gas

emissions, noise, and transportation/traffic.

Analysis: According to CEQA Guidelines Section 15164, an Addendum to a previously certified EIR may be used if some changes or additions are necessary, but none of the conditions described in Section 15162 requiring the preparation of a subsequent Negative Declaration or EIR have occurred. The CEQA Guidelines require that a brief explanation be provided to support the findings that no subsequent EIR or Negative Declaration are needed for further discretionary approval. These findings are described below:

- 1) *Required Finding: Substantial changes are not proposed for the project that will require major revisions of the previous EIR due to the involvement of new, significant environmental effects or a substantial increase in the severity of previously identified effects.*

Substantial changes are not proposed by the Project and project implementation will not require revisions to TOP EIR. TOP EIR analyzed the direct and physical changes in the environment that would be caused by TOP; focusing on changes to land use associated with the buildout of the proposed land use plan. The subject site was analyzed in the Certified EIR as an Industrial land use designation with an assumed 0.55 Floor Area Ratio ("FAR"). The Project proposes to construct a 256,711 square foot industrial building on 11.3 acres of land with a 0.52 FAR, that is below the 0.55 FAR threshold established by TOP Certified EIR.

Since the Project FAR will be less than that originally analyzed in TOP EIR, no revisions to TOP EIR are required. In addition, all previously adopted mitigation measures are a condition of project approval and are incorporated herein by reference. The attached Initial Study provides an analysis of the Project and verification that the Project will not cause environmental impacts such that any of the circumstances identified in CEQA Guidelines Section 15162 are present.

- 2) *Required Finding: Substantial changes have not occurred with respect to the circumstances under which the project is undertaken, that would require major revisions of the previous Environmental Impact Report due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects.*

Substantial changes have not occurred with respect to the circumstances under which the project was undertaken, that would require major revisions to TOP EIR in that the proposed Project is consistent and in keeping with the surrounding area. The subject site was analyzed in the Certified EIR as an Industrial land use designation with an assumed 0.55 FAR. The Project proposes to construct a 256,711 square foot industrial building on 11.3 acres of land with a 0.52 FAR, that is below the 0.55 FAR threshold established by TOP Certified EIR. Therefore, no proposed changes or revisions to the EIR are required. In addition, all previously adopted mitigation measures are a condition of project approval and are incorporated herein by reference. The attached Initial Study provides an analysis of the Project and verification that the Project will not cause environmental impacts such that any of the circumstances identified in CEQA Guidelines Section 15162 are present.

- 3) *Required Finding. No new information has been provided that would indicate that the proposed project would result in one or more significant effects not discussed in the previous EIR.*

No new information has been provided that would indicate the proposed project would result in any new significant effects not previously discussed in TOP EIR. Therefore, no proposed changes or revisions to the EIR are required. In addition, all previously adopted mitigation measures are a condition of project approval and are incorporated herein by reference. The attached Initial Study provides an analysis of the Project and verification that the Project will not cause environmental impacts such that any of the circumstances identified in CEQA Guidelines Section 15162 are present

CEQA Requirements for an Addendum: If changes to a project or its circumstances occur or new information becomes available after adoption of a negative declaration, the lead agency may: (1) prepare a subsequent EIR if the criteria of CEQA Guidelines Section 15162(a) are met, (2) prepare a subsequent negative declaration, (3) prepare an addendum, or (4) prepare no further documentation. (CEQA Guidelines Section 15162(b)). When only minor technical changes or additions to the negative declaration are

necessary and none of the conditions described in section 15162 calling for the preparation of a subsequent EIR or negative declaration have occurred, CEQA allows the lead agency to prepare and adopt an addendum. (CEQA Guidelines Section 15164(b).)

Under Section 15162, a subsequent EIR or negative declaration is required only when:

1) Substantial changes are proposed in the project which will require major revisions of the previous negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;

2) Substantial changes occur with respect to the circumstances under which the project is undertaken which will require major revisions of the negative declaration due to the involvement of any new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or

3) New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the negative declaration was adopted, shows any of the following:

a) The project will have one or more significant effects not discussed in the previous negative declaration;

b) Significant effects previously examined will be substantially more severe than shown in the previous EIR;

c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or

d) Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative.

Thus, if the Project does not result in any of the circumstances listed in Section 15162 (i.e., no new or substantially greater significant impacts), the City may properly adopt an addendum to the Certified EIR.

Conclusion: TOP EIR, certified by City Council on January 27, 2010, was prepared as a Program EIR in accordance with CEQA, the CEQA Guidelines, and the City's Rules for the Implementation of CEQA and in accordance with Section 15121(a) of the CEQA Guidelines (California Code of Regulations, Title 14, Division 6, Chapter 3). TOP EIR considered the direct physical changes and reasonably foreseeable indirect physical changes in the environment that would be caused by The Ontario Plan. Consequently, TOP EIR focused on impacts from changes to land use associated with buildout of the City's Land Use Plan, within the Policy Plan, and impacts from the resulting population and employment growth in the City. The subject site was analyzed in the Certified EIR as an Industrial land use designation with an assumed 0.55 Floor Area Ratio (FAR). The Project proposes to construct a 256,711 square foot industrial building on 11.3 acres of land with a 0.52 FAR, that is below the 0.55 FAR threshold established by TOP Certified EIR. Subsequent activities within TOP Program EIR have been evaluated to determine whether an additional CEQA document needs to be prepared.

Accordingly, and based on the findings and information contained in the Certified EIR, the analysis above, the attached Initial Study, and CEQA statute and CEQA Guidelines, including Sections 15164 and 15162, the Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary, nor is there a need for any additional mitigation measures; therefore, pursuant to CEQA Guidelines Section 15164, the Ontario Planning Commission hereby adopts this Addendum to the Certified EIR.

Surrounding Land Uses:

	Existing Land Use	General Plan Designation	Zoning Designation	Specific Plan Land Use
Site:	Industrial/Vacant/Open Space	Industrial	IL (Light Industrial)	N/A
North:	Southern Pacific Railroad	Railroad	RC (Rail Corridor)	N/A
South:	Warehouse/ Grove Manor Mobile Home Park	Business Park/		
East:	Mobile Home Park	IL (Light Industrial)/		
West:	MHP (Mobile Home Park)	N/A		

Other public agencies whose approval is required (e.g., permits, financing approval or participation agreement): None

Tribal Consultation: Have California Native American tribes traditionally and culturally affiliated with the project area requested consultation pursuant to Public Resources Code section 21080.3.1? Yes No

If "yes," has consultation begun? Yes No Completed

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

- | | | |
|--|---|---|
| <input type="checkbox"/> Aesthetics | <input type="checkbox"/> Agriculture/Forestry Resources | <input type="checkbox"/> Air Quality |
| <input type="checkbox"/> Biological Resources | <input type="checkbox"/> Cultural Resources | <input type="checkbox"/> Geology / Soils |
| <input type="checkbox"/> Greenhouse Gas Emissions | <input type="checkbox"/> Hazards & Hazardous Materials | <input type="checkbox"/> Hydrology / Water Quality |
| <input type="checkbox"/> Land Use / Planning | <input type="checkbox"/> Mineral Resources | <input type="checkbox"/> Noise |
| <input type="checkbox"/> Population / Housing | <input type="checkbox"/> Public Services | <input type="checkbox"/> Recreation |
| <input type="checkbox"/> Transportation | <input type="checkbox"/> Utilities / Service Systems | <input type="checkbox"/> Mandatory Findings of Significance |
| <input type="checkbox"/> Tribal Cultural Resources | <input type="checkbox"/> Wildfire | <input type="checkbox"/> Energy |

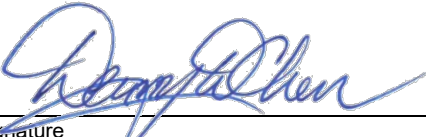
DETERMINATION (To be completed by the Lead Agency)

On the basis of this initial evaluation:

- I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
- I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
- I find that the proposed project MAY have a "potentially significant" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an

earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.

- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.



Denny D. Chen, Associate Planner
Printed Name and Title

May 12, 2021
Date

City of Ontario – Planning Department
For

EVALUATION OF ENVIRONMENTAL IMPACTS

1) A brief explanation is required for all answers except "No Impact" answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g. the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general standards (e.g. the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).

2) All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.

3) Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. "Potentially Significant Impact" is appropriate if there is substantial evidence that an effect is significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.

4) "Negative Declaration: Less Than Significant with Mitigation Incorporated" applies where the incorporation of mitigation measures has reduced an effect from "Potentially Significant Impact" to a "Less than Significant Impact." The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from the "Earlier Analyses" Section may be cross-referenced).

5) Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or Negative Declaration. Section 15063(c)(3)(D). In this case, a brief discussion should identify the following:

- a) Earlier Analyses Used. Identify and state where they are available for review.
- b) Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
- c) Mitigation Measures. For effects that are "Less than Significant with Mitigation Measures Incorporated," describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.

6) Lead agencies are encouraged to incorporate into the checklist references to information sources

for potential impacts (e.g. general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.

7) Supporting Information Sources. A source list should be attached, and other sources used, or individuals contacted should be cited in the discussion.

8) This is only a suggested form, and lead agencies are free to use different formats; however, lead agencies should normally address the questions from this checklist that are relevant to a project's environmental effects in whatever format is selected.

9) The explanation of each issue should identify:

- a) The significance criteria or threshold, if any, used to evaluate each question; and
- b) The mitigation measure identified, if any, to reduce the impact to less than significance.

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
1. AESTHETICS. Would the project:				
a. Have a substantial adverse effect on a scenic vista?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. AGRICULTURE AND FOREST RESOURCES. In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest protocols adopted by the California Air Resources Board. Would the project:				
a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
b. Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in the loss of forest land or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3. AIR QUALITY. Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:				
a. Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in other emissions (such as those leading to odors adversely affecting a substantial number of people?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4. BIOLOGICAL RESOURCES. Would the project:				
a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
5. CULTURAL RESOURCES. Would the project:				
a. Cause a substantial adverse change in the significance of a historical resource pursuant to Section 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Disturb any human remains, including those interred outside of dedicated cemeteries?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
6. ENERGY. Would the project:				
a. Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
7. GEOLOGY AND SOILS. Would the project:				
a. Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury or death involving:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii. Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii. Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv. Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Be located on expansive soil, as defined in Table 18.1 B of the Uniform Building Code, creating substantial direct or indirect risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of waste water?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
8. GREENHOUSE GAS EMISSIONS. Would the project:				
a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emission of greenhouse gases?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
9. HAZARDS AND HAZARDOUS MATERIALS. Would the project:				
a. Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard or excessive noise for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
g. Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
10. HYDROLOGY AND WATER QUALITY. Would the project:				
a. Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i. result in substantial erosion or siltation on- or off-site;	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii. substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or offsite;	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii. create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff; or	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv. impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
11. LAND USE AND PLANNING. Would the project:				
a. Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
12. MINERAL RESOURCES. Would the project:				
a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
13. NOISE. Would the project result in:				
a. Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
14. POPULATION AND HOUSING. Would the project:				
a. Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of road or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
15. PUBLIC SERVICES. Would the project:				
a. Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:				
i. Fire protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii. Police protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii. Schools?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv. Parks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
v. Other public facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
16. RECREATION. Would the project:				
a. Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Does the project include recreational facilities or require the construction or expansion of recreational facilities which have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
17. TRANSPORTATION. Would the project:				
a. Conflict with a program plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Would the project conflict or be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
18. TRIBAL CULTURAL RESOURCES. Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is				
a. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
19. UTILITIES AND SERVICE SYSTEMS. Would the project:				
a. Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Generate solid waste in excess of State or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
e. Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
20. WILDFIRES. If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:				
a. Substantially impair an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
21. MANDATORY FINDINGS OF SIGNIFICANCE.				
a. Does the project have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current project, and the effects of probable future projects.)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>Note: Authority cited: Public Resources Code sections 21083, 21083.05, 21083.09.</p> <p>Reference: Gov. Code section 65088.4; Public Resources Code sections 21073, 21074, 21080(c), 21080.1, 21080.3, 21080.3.1, 21080.3.2, 21082.3, 21083, 21083.3, 21083.5, 21084.2, 21084.3, 21093, 21094, 21095 and 21151; <i>Sundstrom v. County of Mendocino</i> (1988) 202 Cal.App.3d 296; <i>Leonoff v. Monterey Board of Supervisors</i> (1990) 222 Cal.App.3d 1337; <i>Eureka Citizens for Responsible Govt. v. City of Eureka</i> (2007) 147 Cal.App.4th 357; <i>Protect the Historic Amador Waterways v. Amador Water Agency</i> (2004) 116 Cal.App.4th 1099, 1109; <i>San Franciscans Upholding the Downtown Plan v. City and County of San Francisco</i> (2002) 102 Cal.App.4th 656.</p>				

EXPLANATION OF ISSUES

1. AESTHETICS. Would the project:

a. Have a substantial adverse effect on a scenic vista?

Discussion of Effects: The Policy Plan (General Plan) does not identify scenic vistas within the City. However, the Policy Plan (Policy CD1-5) requires all major north-south streets be designed and redeveloped to feature views of the San Gabriel Mountains. The subject site was analyzed in the Certified EIR as an Industrial land use designation with an assumed 0.55 FAR. The Project proposes to construct a 256,711 square foot industrial building on 11.3 acres of land with a 0.52 FAR. Also, the project site is located at 875 West State Street, a major east-west arterial street, as identified in the Functional Roadway Classification Plan (Figure M-2) of the Mobility Element within the Policy Plan. Therefore, no adverse impacts are anticipated in relation to the project.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings and historic buildings within a state scenic highway?

Discussion of Effects: The City of Ontario is served by three freeways: I-10, I-15, and SR-60. I-10 and SR-60 traverse the northern and central portion of the City, respectively, in an east to west direction. I-15 traverses the northeastern portion of the City in a north-south direction. These segments of I-10, I-15, and SR-60 have not been officially designated as scenic highways by the California Department of Transportation. In addition, there are no historic buildings, or any scenic resources identified on or in the vicinity of the project site. Therefore, it will not result in adverse environmental impacts.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified EIR analyses are necessary.

c. In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?

Discussion of Effects: The Project would not degrade the existing visual character or quality of the site or its surroundings. The project site is located in an area that is characterized by industrial development and is surrounded by urban land uses.

Any development proposals that would subsequently occur from the proposed Project will be required to be in accordance with the policies of the Community Design Element of the Policy Plan (General Plan) and zoning designations on the property. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Create a new source of substantial light or glare that would adversely affect day or nighttime views in the area?

Discussion of Effects: The Project proposes to construct a 256,711 square foot industrial building on 11.3 acres of land with a 0.52 FAR. Pursuant to the requirements of the City's Development Code, on-site lighting will be shielded, diffused or indirect, to avoid glare to pedestrians or motorists. In addition,

lighting fixtures will be selected and located to confine the area of illumination to within the project site and minimize light spillage.

Site lighting plans will be subject to review by the Planning Department and Police Department prior to issuance of building permits (pursuant to the City's Building Security Ordinance). Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

2. AGRICULTURE AND FOREST RESOURCES. In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board. Would the project:

a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?

Discussion of Effects: The site is currently used as a metal container and truck trailer storage yard. Further, the site is identified as Urban and Built-up Land on the map prepared by the California Resources Agency, pursuant to the Farmland Mapping and Monitoring Program. As a result, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Conflict with existing zoning for agricultural use, or a Williamson Act contract?

Discussion of Effects: The project site is not zoned for agricultural use. The Project proposes to construct a 256,711 square foot industrial building on 11.3 acres of land with a 0.52 FAR. Furthermore, there are no Williamson Act contracts in effect on the subject site. Therefore, no impacts to agricultural uses are anticipated, nor will there be any conflict with Williamson Act contracts.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?

Discussion of Effects: The subject site was analyzed in the Certified EIR as an Industrial land use designation with an assumed 0.55 Floor Area Ratio (FAR). The Project proposes to construct a 256,711 square foot industrial building on 11.3 acres of land with a 0.52 FAR, that is below the 0.55 FAR threshold established by TOP Certified EIR. This would not result in the rezoning of forest land, timberland, or timberland zoned Timberland Production because such land use designations do not exist within the City of Ontario. Therefore, no impacts to forest or timberland are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Result in the loss of forest land or conversion of forest land to non-forest use?

Discussion of Effects: There is currently no land in the City of Ontario that qualifies as forest land as defined in Public Resources Code section 12220(g). Neither TOP nor the City's Zoning Code provide designations for forest land. Consequently, the proposed project would not result in the loss or conversion of forest land.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified EIR analyses are necessary.

e. Involve other changes in the existing environment, which, due to their location or nature, could individually or cumulatively result in loss of Farmland to non-agricultural use or conversion of forest land to non-forest use?

Discussion of Effects: Implementation of the Project would not result in changes to the existing environment other than those previously addressed in the Certified EIR. While conversion of farmland increases the potential for adjacent areas to also be converted from farmland to urban uses, there are no agricultural uses occurring onsite or in the vicinity and the Project does not directly or indirectly result in conversion of farmland. No new cumulative impacts beyond those identified in the Certified EIR would result from Project implementation. As a result, the project will not result in loss of Farmland to non-agricultural use.

Additionally, there is currently no land in the City of Ontario that qualifies as forest land as defined in Public Resources Code Section 12220(g). Neither TOP nor the City's Zoning Code provide designations for forest land. Consequently, to the extent that the proposed project would result in changes to the existing environment, those changes would not impact forest land.

Mitigation Required: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified EIR analyses are necessary.

3. AIR QUALITY. Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:

a. Conflict with or obstruct implementation of the applicable air quality plan?

Discussion of Effects: The subject site was analyzed in the Certified EIR as an Industrial land use designation with an assumed 0.55 Floor Area Ratio (FAR). The Project proposes to construct a 256,711 square foot industrial building on 11.3 acres of land with a 0.52 FAR, that is below the 0.55 FAR threshold established by TOP Certified EIR. The project will not conflict with or obstruct implementation of any air quality plan. As noted in the Certified EIR (Section 5.3), pollutant levels in the Ontario area already exceed Federal and State standards. To reduce pollutant levels, the City of Ontario is actively participating in efforts to enhance air quality by implementing Control Measures in the Air Quality Management Plan for local jurisdictions within the South Coast Air Basin.

The proposed project is consistent with The Ontario Plan, for which the EIR was prepared and impacts evaluated. Furthermore, the project is consistent with the City's participation in the Air Quality Management Plan and, because of the project's limited size and scope, will not conflict with or obstruct implementation of the plan. Mitigation (Mitigation Measure 5.3-2) has been adopted by the City that requires fugitive dust control measures pursuant to SCAQMD's Rule 403, use of Tier 3 construction equipment, proper service and maintenance of construction equipment, limiting nonessential idling of construction

equipment, and use of Super-Compliant VOC paints for coating and architectural surfaces. Any future development proposals on the project site will be required to comply with Mitigation Measure 5.3-2. No new impacts beyond those identified in the Certified EIR would result from Project implementation.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?

Discussion of Effects: The project will not result in a cumulatively considerable net increase of any criteria pollutant for which the region is in non-attainment under an applicable federal or state ambient air quality because the Project is a Specific Plan Amendment that adds permitted industrial uses to property specified for industrial land uses by the Official Land Use Map (Exhibit LU-01) of the Policy Plan component of TOP. Mitigation (Mitigation Measure 5.3-1) has already been adopted by the City that would facilitate continued City cooperation with the SCAQMD and SCAG to achieve regional air quality improvement goals, promote energy conservation design and development techniques, encourage alternative modes of transportation, and implement transportation demand strategies. The project will comply with the air quality standards of the Certified EIR and the SCAQMD resulting in impacts that are less than significant [please refer to Sections 3(a) and 3(b)].

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Expose sensitive receptors to substantial pollutant concentrations?

Discussion of Effects: The subject site was analyzed in the Certified EIR as an Industrial land use designation with an assumed 0.55 FAR. The Project proposes to construct a 256,711 square foot industrial building on 11.3 acres of land with a 0.52 FAR, that is below the 0.55 FAR threshold established by TOP Certified EIR. As discussed in Section 5.3 of the Certified EIR, the proposed Project is within a non-attainment region of the South Coast Air Basin (“SCAB”). The Project is located less than 1,000 feet away from a sensitive land use and therefore the Applicant was required to submit a Health Risk Assessment (“HRA”) report (Attachment A – Health Risk Assessment). The HRA is a comprehensive analysis to determine the potential health risks from diesel particular matter (“DPM”) created by the proposed project. The HRA report (Vista Environmental, November 30, 2020) prepared the Project determined that:

1. The projects emissions would not exceed the SCAQMD (South Coast Air Quality Management District) project level health risk cancer significance threshold of 10 in one million persons; and
2. The projects emissions would not exceed the SCAQMD project level non-cancer chronic and acute hazard index of 1.0; and
3. The Project Site will not exceed more than 100 trucks per day and no more than 40 trucks with operating transportation refrigeration units (“TRUs”) per day.

Therefore, according to the HRA report, the proposed Project will not generate significant new or greater air quality impacts than identified in the Certified EIR.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?

Discussion of Effects: The subject site was analyzed in the Certified EIR as an Industrial land use designation with an assumed 0.55 FAR. The Project proposes to construct a 256,711 square foot industrial

building on 11.3 acres of land with a 0.52 FAR, that is below the 0.55 FAR threshold established by TOP Certified EIR. The proposed Project for an Industrial warehouse on the subject site does not create objectionable odors. Further, the project shall comply with the policies of the Ontario Municipal Code and the Policy Plan (General Plan). Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified EIR analyses are necessary.

4. BIOLOGICAL RESOURCES. Would the project:

a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?

Discussion of Effects: The project site is located within an area that has not been identified as containing species identified as a candidate, sensitive, or special status species in local or regional plans, policies or regulations or by the California Department of Fish and Game or the U.S. Fish and Wildlife Service. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified analyses are necessary.

b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?

Discussion of Effects: The site does not contain any riparian habitat or other sensitive natural community identified by the Department of Fish and Game or Fish and Wildlife Service. Therefore, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?

Discussion of Effects: No wetland habitat is present on site. Therefore, project implementation would have no impact on these resources.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?

Discussion of Effects: The site is currently used as a metal container and truck trailer storage yard. The site is surrounded by Union Pacific Railroad tracks to the north, a diesel repair shop to the east, an industrial building and the Grove Manor Mobile Home Park to the south, and industrial buildings to the west. As a result, there are no wildlife corridors connecting this site to other areas. Therefore, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?

Discussion of Effects: The City of Ontario does not have any specific policies or ordinances protecting biological resources. Further, project site does not contain any mature trees necessitating the need for preservation. As a result, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

f. Conflict with the provisions of an adopted Habitat Conservation Plan (“HCP”), Natural Community Conservation Plan (“NCCP”), or other approved local, regional, or state habitat conservation plan?

Discussion of Effects: The site is not part of an adopted HCP, NCCP or other approved habitat conservation plan. As a result, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

5. CULTURAL RESOURCES. Would the project:

a. Cause a substantial adverse change in the significance of a historical resource pursuant to Section 15064.5?

Discussion of Effects: The site is currently used as a metal container and truck trailer storage yard and does not contain any buildings or structures and therefore cannot be considered for eligibility for listing in the California Register of Historic Resources. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5?

Discussion of Effects: The Certified EIR (Section 5.5) indicates no archeological sites or resources have been recorded in the City with the Archeological Information Center at San Bernardino County Museum. Figure 5.5-2 of the Certified EIR shows that the Project site has not been surveyed for archeological resources. The Project site has been highly disturbed by modern human activities that would have displaced potential surface and subsurface archeological resources. Therefore, the proposed Project will not impact cultural (prehistoric, historic, or historic built environments) resources and no mitigation measures are recommended. While no adverse impacts to archeological resources are anticipated at this site due to its urbanized nature, standard conditions have been imposed on the project that in the event of unanticipated archeological discoveries, construction activities will not continue or will moved to other parts of the project site and a qualified archaeologist shall be contacted to determine significance of these resources. If the find is discovered to be historical or unique archeological resources, as defined in Section 15064.5 of the CEQA Guidelines, avoidance or other appropriate measures shall be implemented.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Disturb any human remains, including those interred outside of formal cemeteries?

Discussion of Effects: The proposed project is in an area that has been previously disturbed by human activity. No known religious or sacred sites exist within the Project area. Thus, human remains are not expected to be encountered during any construction activities. However, in the unlikely event that human remains are discovered, existing regulations, including the California Public Resources Code Section 5097.98, would afford protection for human remains discovered during development activities. Furthermore, standard conditions have been imposed on the project that in the event of unanticipated discoveries of human remains are identified during excavation, construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and/or Native American consultation has been completed, if deemed applicable.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

6. ENERGY Would the project:

a. Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?

Discussion of Effects: Energy was not analyzed in the Certified EIR but has been included as part of the 2019 revisions to the CEQA Guidelines. Implementation of the Project would increase the demand for electricity and natural gas at the Project site and gasoline consumption in the region during construction and operation. A detailed discussion is provided below.

Electricity

Construction. Temporary electric power would be required for lighting and electronic equipment (e.g., computers) located in trailers used by the construction crew. However, the electricity used for such activities would be temporary and would have a negligible contribution to the project's overall energy consumption.

Operational. The subject site was analyzed in the Certified EIR as an Industrial land use designation with an assumed 0.55. The Project proposes to construct a 256,711 square foot industrial building on 11.3 acres of land with a 0.52 FAR, that is below the 0.55 FAR threshold established by TOP Certified EIR. During the hypothetical operation of the Project, a warehouse, distribution, use would require electricity for multiple purposes, such as: building heating and cooling, lighting, appliances, and electronics. The proposed development would be required to comply with the CALGreen Building Code requirements in effect at the time of development. Moreover, the Project includes a sample Greenhouse Gas Reduction Measure Screening Table for Industrial Development. The Screening Table includes measures energy efficient development, indoor space efficiency measures, building efficiency measures, renewable energy measures, and water conservation measures. Measures that would reduce electricity consumption include, but are not limited to: greatly enhanced window insulation, an enhanced cool-roof, an improved efficiency heating, ventilation, and air conditioning ("HVAC") system, blower doors HERS verified Envelope leakage or equivalent, enhanced duct insulation, Energy Star appliances, water efficient landscaping and irrigation systems, and water-efficient toilets and faucets. Although electricity consumption would increase at the site under implementation of the Project, the building envelope, HVAC, lighting, and other systems, would be designed to maximize energy performance. The project would also be subject to statewide mandatory energy requirements as outlined in the CALGreen Code. In addition, the project would implement additional measures, as detailed in the GHG reduction measures screening table, that would further reduce electricity consumption. Electricity that would be consumed by the Project would also be subject to the cap-and-trade regulation. For these reasons, the electricity that would be consumed by the Project is not considered to be

inefficient or wasteful, and impacts would be less than significant.

Natural Gas

Construction. Natural gas consumption is not anticipated during construction of the Project. Fuels used for construction would generally consists of diesel and gasoline, which are discussed in the next subsection. Any amounts of natural gas that may be consumed during project construction would be nominal and would have a negligible contribution to the project's overall energy consumption.

Operational. The subject site was analyzed in the Certified EIR as an Industrial land use designation with an assumed 0.55 FAR. The Project proposes to construct a 256,711 square foot industrial building on 11.3 acres of land with a 0.52 FAR, that is below the 0.55 FAR threshold established by TOP Certified EIR. During operation of the Project, a warehouse and distribution use would require natural gas consumption for various purposes, such as building heating and cooling. Any future structure developed on the site would be built to the 2021 Title 24 CALGreen efficiency requirements or the code in effect at the time of development. In addition, measures will be applied based on the information contained in the County's GHG DRP checklist. These measures include, but are not limited to: enhanced wall, attic, and window insulation; high efficiency water heater, and optimized building orientation. Although natural gas consumption would increase at the site under implementation of the Project, the building envelope, HVAC, lighting, and other systems, would be designed to maximize energy performance. The project would be subject to statewide mandatory energy requirements as outlined in the CALGreen Code. In addition, the project would implement additional measures, as detailed in the GHG reduction measures screening table, that would further reduce natural gas consumption. Natural gas that would be consumed by the Project would also be subject to the cap-and-trade regulation. For these reasons, the natural gas that would be consumed by the project is not considered to be inefficient or wasteful, and impacts would be less than significant.

Diesel and Gasoline Fuel

Construction. Diesel and gasoline fuels, also referred to as petroleum in this subsection, would be consumed throughout construction of the Project. Fuel consumed by construction equipment would be the primary energy resource consumed over the course of construction, and vehicle miles traveled ("VMT") associated with the transportation of construction materials (e.g., deliveries to the site) and worker trips to and from the site would also result in petroleum consumption. Whereas on-site, heavy-duty construction equipment and delivery trucks would predominantly use diesel fuel, construction workers would generally rely on gasoline-powered vehicles. Any future development would be required to comply with CARB's Airborne Toxic Control Measures, which restricts heavy-duty diesel vehicle idling to five minutes. Since petroleum use during construction would be temporary and required to conduct development activities, it would not be wasteful or inefficient, and impacts would be less than significant.

Operational. Fuel consumption associated with development pursuant to the Project's operational phase would primarily be attributable to workers commuting to and from the Project and the operation of large, diesel-powered trucks (e.g., semi-trucks) needed to transport goods. Over the lifetime of the Project, the fuel efficiency of the vehicles being used by the employees is expected to increase. As such, the amount of petroleum consumed as a result of vehicular trips to and from the Project site during operation is anticipated to decrease over time. There are numerous regulations in place that require and encourage fuel efficiency. For example, CARB has adopted an approach to passenger vehicles by combining the control of smog-causing pollutants and GHG emissions into a single, coordinated package of standards. The approach also includes efforts to support an accelerate the number of plug-in hybrids and ZEVs in California. In addition, per the requirements identified in SB 375, CARB adopted a regional goal for the SCAG region of reducing per-capita GHG emissions from 2005 levels by 8 percent by 2020 and 19 percent by 2035 for light-duty passenger vehicles. Accordingly, operation of the Project is expected to decrease the amount of petroleum it consumes in the future due to advances in fuel economy. Although the Project would increase petroleum use in the region during construction and operation, the use would be a small fraction of the statewide use and, due to efficiency increases, would diminish over time. As such, petroleum consumption associated with the Project would not be considered inefficient or wasteful and would result in a less-than-significant impact.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts. No changes or additions to the Certified EIR analyses are necessary.

b. Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?

Discussion of Effects: The Project would not conflict with or obstruct a state or local plan adopted for the purposes of increasing the amount of renewable energy or energy efficiency. The California Title 24 Building Code contains energy efficiency standards for non-residential buildings. These standards address electricity and natural gas efficiency in lighting, water, heating, and air conditioning, as well as the effects of the building envelope (e.g., windows, doors, walls and roofs, etc.) on energy consumption. As described above, the Project would be required to comply with the 2019 Title 24 CALGreen standards and would implement additional measures as identified in the County's GHG DRP checklist. Since the Project would comply with applicable State standards and adhere to the County's energy reductions measures identified in the GHG Emissions Reduction Plan, the Project would not conflict with nor obstruct a state or local plan for renewable energy or energy efficiency. This impact would be less than significant.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts. No changes or additions to the Certified EIR analyses are necessary.

7. GEOLOGY & SOILS. Would the project:

a. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury or death involving:

i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.

Discussion of Effects: There are no active faults known on the site and the project site is located outside the Fault Rapture Hazard Zone (formerly Alquist-Priolo Zone). The Certified EIR (Section 5.7/Figure 5.7-2) identifies eight active or potentially active fault zones near the City. Given that the closest fault zone is located more than ten miles from the project site, fault rupture within the project area is not likely. All development will comply with the Uniform Building Code seismic design standards to reduce geologic hazard susceptibility. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

ii. Strong seismic ground shaking?

Discussion of Effects: There are no active faults known on the site and the project site is located outside the Fault Rapture Hazard Zone (formerly Alquist-Priolo Zone). The Certified EIR (Section 5.7/Figure 5.7-2) identifies eight active or potentially active fault zones near the City. The closest fault zone is located more than ten miles from the project site. The proximity of the site to the active faults will result in ground shaking during moderate to severe seismic events. All construction will comply with the California Building Code, the Ontario Municipal Code, The Ontario Plan and all other ordinances adopted by the City related to construction and safety. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

iii. Seismic-related ground failure, including liquefaction?

Discussion of Effects: As identified in the Certified EIR (Section 5.7), groundwater saturation of sediments is required for earthquake induced liquefaction. In general, groundwater depths shallower than 10 feet to the surface can cause the highest liquefaction susceptibility. Depth to ground water at the project site during the winter months is estimated to be between 250 to 450 feet below ground surface. Therefore, the liquefaction potential within the project area is minimal. Implementation of The Ontario Plan strategies, Uniform Building Code and Ontario Municipal code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

iv. Landslides?

Discussion of Effects: The project would not expose people or structures to potential adverse effects, including the risk of loss, injury, or death involving landslides because the relatively flat topography of the project site (less than 2 percent slope across the City) makes the chance of landslides remote. The proposed industrial building will not create greater landslide potential impacts than were identified in the Certified EIR. Implementation of The Ontario Plan strategies, Uniform Building Code and Ontario Municipal Code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Result in substantial soil erosion or the loss of topsoil?

Discussion of Effects: The project will not result in significant soil erosion or loss of topsoil because of the previously disturbed nature of the Project site and the limited size and scope of the Project. Grading increases the potential for erosion by removing protective vegetation, changing natural drainage patterns, and constructing slopes. However, compliance with the California Building Code and review of grading plans by the City Engineer will ensure no significant impacts will occur. In addition, the City requires an erosion/dust control plan for projects located within this area. Implementation of a NPDES program, the Environmental Resource Element of the Policy Plan (General Plan) strategies, Uniform Building Code and Ontario Municipal code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse?

Discussion of Effects: The proposed project would not result in a geologic unit or soil that is unstable, or that would become unstable because as previously discussed, the potential for liquefaction and landslides associated with the project is less than significant. The Certified EIR (Section 5.7) indicates that subsidence is generally associated with large decreases or withdrawals of water from the aquifer. The project would not withdraw water from the existing aquifer. Further, implementation of The Ontario Plan strategies, Uniform Building Code and Ontario Municipal code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?

Discussion of Effects: The majority of Ontario, including the project site, is located on alluvial and eolian soil deposits. These types of soils are not considered to be expansive. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

e. Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?

Discussion of Effects: The area is served by the local sewer system and the use of alternative systems is not necessary. There will be no impact to the sewage system.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

f. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?

Discussion of Effects: The City of Ontario is underlain by deposits of Quaternary and Upper-Pleistocene sediments deposited during the Pliocene and early Pleistocene time, Quaternary Older Alluvial sediments may contain significant, nonrenewable, paleontological resources and are, therefore, considered to have high sensitivity at depths of 10 feet or more below ground surface. In addition, the Certified EIR (Section 5.5) indicates that one paleontological resource has been discovered in the City. Therefore, it is concluded that the proposed Project will not impact paleontological resources or unique geological features and as such no mitigation measures are recommended. While no adverse impacts are anticipated, standard conditions have been imposed on the Project that in the event of unanticipated paleontological resources are identified during excavation, construction activities will not continue or will be moved to other parts of the Project site and a qualified paleontologist shall be contacted to determine significance of these resources. If the find is determined to be significant, avoidance or other appropriate measures shall be implemented.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

8. GREENHOUSE GAS EMISSIONS. Would the project:

a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an Industrial land use. Additionally, the impact of buildout of The Ontario Plan on the environment due to the emission of greenhouse gases ("GHGs") was analyzed in the Certified EIR. According to the EIR, this impact would be significant and unavoidable (Re-circulated Portions of the Ontario Plan Draft Environmental Impact Report, p. 2-118.) This EIR was certified by the City on January 27, 2010, at which time a statement of overriding considerations was also adopted for The Ontario Plan's significant and unavoidable impacts, including that concerning the emission of greenhouse gases. The proposed industrial building will not create significantly greater impacts than were identified in the Certified EIR. The Project includes a sample GHG Reduction Measures Screening Threshold Table, which provides guidance in measuring the reduction of greenhouse gas ("GHG") emissions attributable to certain design and construction measures incorporated into development projects. The analysis, methodology, and significance determination (thresholds) are

based upon the City's Climate Action Plan ("CAP"), which includes GHG emission inventories (2008 and 2020 forecasts), a year 2020 emission reduction target, the goals and policies to reach the target, together with the Addendum prepared for the CAP. The Screening Table assigns points for each option incorporated into a project as mitigation or a project design feature (collectively referred to as "feature"). The point values correspond to the minimum emissions reduction expected from each feature. The menu of features allows maximum flexibility and options for how development projects can implement the GHG reduction measures. The point levels are based upon improvements compared to 2008 emission levels of efficiency. The Project will garner at least 100 points and be consistent with the reduction quantities anticipated in the City's CAP. Therefore, quantification of Project-specific GHG emissions is not required.

Additionally, pursuant to Public Resources Code Section 21083.3, this impact need not be analyzed further, because (1) the proposed project would result in an impact that was previously analyzed in the Certified EIR, which was certified by the City; (2) the proposed project would not result in any greenhouse gas impacts that were not addressed in the Certified EIR; (3) the proposed project is consistent with The Ontario Plan. The proposed impacts of the project were already analyzed in the Certified EIR and the project will be built to current energy efficient standards. Potential impacts of project implementation will be less than significant with mitigation already required under the Certified EIR and current energy efficiency standards. No changes or additions to the Certified EIR analyses are necessary.

Mitigation Required: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to TOP EIR analyses are necessary. The mitigation measures adopted as part of TOP EIR adequately address any potential significant impacts and there is no need for any additional mitigation measures. The City has reviewed the emission reduction measures and concepts in The Ontario Plan EIR's MM 6-2 and 6-3, and has determined that the following actions apply and shall be undertaken by the applicant in connection with the project: energy efficient design, efficient irrigation systems, electric vehicle charging stations, and compliance with Title 24 of the California Code of Regulations.

b. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an Industrial land use. The proposed Project is consistent with The Ontario Plan Goal ER 4 of improving air quality by, among other things, implementation of Policy ER4-3, regarding the reduction of greenhouse gas emissions in accordance with regional, State, and federal regulations. In addition, the proposed Project is consistent with the policies outlined in Section 5.6.4 of the Environmental Impact Report for The Ontario Plan, which aims to reduce the City's contribution of greenhouse gas emissions at build-out by fifteen (15 percent), because the project is upholding the applicable City's adopted mitigation measures as represented in 6-1 through 6-6 and energy efficient design, efficient irrigation systems, electric vehicle charging stations, and compliance with Title 24 of the California Code of Regulations. The Project is consistent with the City's Climate Action Plan. Therefore, the proposed Project does not conflict with an applicable plan, policy or regulation adopted for the purpose of reducing emissions of greenhouse gases.

Mitigation Required: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

9. HAZARDS & HAZARDOUS MATERIALS. Would the project:

a. Create a significant hazard to the public or the environment through the routine transport, use or disposal of hazardous materials?

Discussion of Effects: The subject site was previously analyzed by TOP EIR as an Industrial land use. The project is not anticipated to involve the transport, use or disposal of hazardous materials during either construction or project implementation. Therefore, no adverse impacts are anticipated. However, in

the unlikely event of an accident, implementation of the strategies included in The Ontario Plan will decrease the potential for health and safety risks from hazardous materials to a less than significant impact.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial land use. The proposed project does not include the use of hazardous materials or volatile fuels. In addition, there are no known stationary commercial or industrial land uses within close proximity to the subject site, which use/store hazardous materials to the extent that they would pose a significant hazard to visitors/occupants to the subject site, in the event of an upset condition resulting in the release of a hazardous material.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances or waste within one-quarter mile of an existing or proposed school?

Discussion of Effects: The proposed project does not include the use, emissions or handling of hazardous or acutely hazardous materials, substances, or waste. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?

Discussion of Effects: The proposed project site is not listed on the hazardous materials site compiled pursuant to Government Code Section 65962.5. Therefore, the project would not create a hazard to the public or the environment and no impact is anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

e. For a project located within the safety zone of the airport land use compatibility plan for ONT or Chino Airports, would the project result in a safety hazard for people residing or working in the project area?

Discussion of Effects: The proposed project was reviewed and found to be located within the Airport Influence Area of Ontario International Airport ("ONT") and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan ("ALUCP") for ONT. The site is located within the airport influence area but outside the airport safety zones. The proposed Industrial warehouse is compatible with the ALUCP. In addition, the project site lies outside the boundaries of the Chino Airport Influence Area. Therefore, any impacts would be reduced to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

f. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?

Discussion of Effects: The City's Safety Element, as contained within The Ontario Plan, includes policies and procedures to be administered in the event of a disaster. The Ontario Plan seeks interdepartmental and inter-jurisdictional coordination and collaboration to be prepared for, respond to and recover from every day and disaster emergencies. In addition, the project will comply with the requirements of the Ontario Fire Department and all City requirements for fire and other emergency access. Because the project is required to comply with all applicable City codes, any impacts would be reduced to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

g. Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?

Discussion of Effects: The project site is not located in or near wildlands. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

10. HYDROLOGY & WATER QUALITY. Would the project:

a. Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality?

Discussion of Effects: The project site is served by City water and sewer service and will not affect water quality standards or waste discharge requirements. Discharge of storm water pollutants from areas of materials storage, vehicle or equipment fueling, vehicle or equipment maintenance (including washing, waste handling, hazardous materials handling or storage, delivery areas or loading docks, or other outdoor work) areas could result in a temporary increase in the amount of suspended solids, trash and debris, oil and grease, organic compounds, pesticides, nutrients, heavy metals and bacteria pathogens in surface flows during a concurrent storm event, thus resulting in surface water quality impacts. The site is required to comply with the statewide National Pollutant Discharge Elimination System ("NPDES") General Industrial Activities Stormwater Permit, the San Bernardino County Area-Wide Urban Runoff Permit (MS4 permit) and the City of Ontario's Municipal Code (Title 6, Chapter 6 (Stormwater Drainage System)). This would reduce any impacts to below a level of significance. Furthermore, any future applicant to develop the site would be required to submit a Preliminary Water Quality Management Plan ("PWQMP"), which would establish the site's compliance with storm water discharge and water quality management requirements. The PWQMP will include site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development ("LID") best management practices ("BMPs"), such as retention and infiltration, biotreatment and evapotranspiration. The PWQMP would include the use of an underground stormwater infiltration system for the site. Any overflow drainage from future development of the site will be conveyed to the public street by way of parkway culverts.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial land use. No increases in the current amount of water flow to the project site are anticipated, and the proposed project will not deplete groundwater supplies, nor will it interfere with recharge. The water use associated with the proposed use of the property was included in the Certified EIR analysis. The development of the site will require the grading of the site and excavation is expected to be less than three feet and would not affect the existing aquifer, estimated to be about 230 to 250 feet below the ground surface. No adverse impacts are anticipated.

Mitigation: No additional mitigation required.

c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:

i. Result in substantial erosion or siltation on- or off-site?

Discussion of Effects: It is not anticipated that the Project would alter the drainage pattern of the site or area, in a manner that would result in erosion, siltation or flooding on-or-off site, nor will the proposed Project increase the erosion of the subject site or surrounding areas. The existing drainage pattern of the site will not be altered, and it will have no significant impact on downstream hydrology. Stormwater generated by the project will be discharged in compliance with the statewide NPDES General Construction Activities Stormwater Permit and San Bernardino County MS4 permit requirements. With the full implementation of a Storm Water Pollution Prevention Plan developed in compliance with the General Construction Activities Permit requirements, the Best Management Practices included in the SWPPP, and a stormwater monitoring program would reduce any impacts to below a level of significance. No streams or streambeds are present on the site. No changes in erosion off-site are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

ii. Substantially increase the rate or amount of surface runoff water in a manner which would result in flooding on- or off-site?

Discussion of Effects: The proposed Project is not anticipated to increase the flow velocity or volume of storm water runoff to cause environmental harm from the site and will not create a burden on existing infrastructure. Furthermore, with the implementation of an approved Water Quality Management Plan developed for the site, in compliance with the San Bernardino County MS4 Permit requirements, stormwater runoff volume shall be reduced to below a level of significance.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

iii. Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?

Discussion of Effects: It is not anticipated that the project would create or contribute runoff water that would exceed the capacity of existing or planned stormwater drainage systems or create or contribute stormwater runoff pollutants during construction and/or post-construction activity. The stormwater flows will enter an existing storm pipe in Jurupa Street. Pursuant to the requirements of The Ontario Plan, the City's Development Code, and the San Bernardino County MS4 Permit's "Water Quality Management Plan" ("WQMP"), individual developments must provide site drainage and WQMP plans according to guidelines established by the City's Engineering Department. If master drainage facilities are not in place at the time of project development, then standard engineering practices for controlling post-development runoff may be required, which could include the construction of on-site storm water detention and/or retention/infiltration facilities. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

iv. Impede or redirect flood flows?

Discussion of Effects: Urbanization in the areas surrounding the project site have resulted in increased responsiveness of the basin to rainfall. The increase in impervious surfaces such as roofs, roads, and parking lots has resulted in a decrease in groundwater infiltration and larger storm surges. The Project site currently slopes southwest, and the existing drainage pattern is characterized by sheet flows that follow the slope to the northeast. The project site is not impacted by offsite flows. The project site is not located in a FEMA Firm Panel designated Flood Zone Risk, and according to the United States Fish and Wildlife Service National Wetlands Inventory (“NWI”) no wetlands exist on the property. The Project could lead to the conversion of permeable surfaces to impermeable surfaces such as parking areas and building foundation areas. The Project site would discharge onsite flows into an existing storm drain facility. As such, the proposed project would not impede or redirect flood flows. With adherence to existing federal, state, and local regulation no changes to the existing flood flows would occur.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?

Discussion of Effects: Impacts associated with flooding are primarily related to the construction or placement of structures in areas prone to flooding including within an unprotected 100-year flood zone, and in areas susceptible to high tides, tsunamis, seiches, mudflows or sea level rise. Specifically, structures placed in flood prone areas, if flooded, would be damaged, and could subject people to injury or death. The National Flood Insurance Act of 1968 requires the identification of floodplain areas and establishment of flood-risk zones within those areas. FEMA administers the programs and coordinates with communities to establish effective floodplain management standards. According to FEMA, the Project is not located in a known floodplain. Furthermore, this area is not known to flood and is not typically subjected to flooding. The Project site is not located in a floodplain as shown in Figure S-2 of TOP. No wetlands have been mapped on the project site according to the NWI. According to the FEMA, the Project is not located in an area that is subject to flood hazard, tsunami, or seiche zones. The project site is located over 60 miles east of the Pacific Ocean and is not located in a mapped tsunami zone. Therefore, the project would not have a significant risk of flood hazard, tsunami, seiche zones, release of pollutants due to project inundation.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

e. Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?

Discussion of Effects: The Regional Water Quality Control Board's Basin Plan is designed to preserve and enhance water quality and protect the beneficial uses of all regional waters. Specifically, the Basin Plan (i) designates beneficial uses for surface and ground waters, (ii) sets narrative and numerical objectives that must be attained or maintained to protect the designated beneficial uses and conform to the state's anti-degradation policy, and (iii) describes implementation programs to protect all waters in the region. Development allowed by the Project would be required to adhere to requirements of the water quality control plan, including all existing regulation and permitting requirements. This would include the incorporation of best management practices (“BMPs”) to protect water quality during construction and operational periods. The Project is subject to all existing water quality regulations and programs, as described in the regulatory section above, including all applicable construction permits. Existing General

Plan policies related to water quality would also be applicable to the Project. Implementation of these policies, in conjunction with compliance with existing regulatory programs, would ensure that water quality impacts related to the Project would be less than significant.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

11. LAND USE & PLANNING. Would the project:

a. Physically divide an established community?

Discussion of Effects: The project site is located in an area that is currently developed with urban land uses. This project will be of similar design and size to surrounding developments. No adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an Industrial land use. The proposed industrial warehouse building will not create greater impacts than were identified in the Certified EIR. The proposed project does not interfere with any policies for environmental protection. As such, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

12. MINERAL RESOURCES. Would the project:

a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?

Discussion of Effects: The Project site is located within a mostly developed area surrounded by urban land uses. There are no known mineral resources in the area. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?

Discussion of Effects: There are no known mineral resources in the area. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

13. NOISE. Would the project result in:

a. Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial land use and the Project will not expose people to or generate noise levels in excess of standards as established in the Certified EIR (Section 5.12). No additional analysis will be required.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Generation of excessive groundborne vibration or groundborne noise levels?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial land use and the uses associated with this project normally do not induce groundborne vibrations. As such, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. For a project located within the vicinity of a private airstrip or the noise impact zones of the airport land use compatibility plan for ONT and Chino Airports, would the project expose people residing or working in the project area to excessive noise levels?

Discussion of Effects: The proposed project was reviewed and found to be located within the Airport Influence Area of Ontario International Airport ("ONT") and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan ("ALUCP") for ONT. The project site is located within the 60-65 CNEL noise contour and industrial warehouses are a compatible land use. In addition, the project site lies outside the boundaries of the Chino Airport Influence Area. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

14. POPULATION & HOUSING. Would the project:

a. Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of road or other infrastructure)?

Discussion of Effects: The subject site was analyzed in the Certified EIR as an Industrial land use designation with an assumed 0.55 Floor Area Ratio (FAR). The Project proposes to construct a 256,711 square foot industrial building on 11.3 acres of land with a 0.52 FAR, that is below the 0.55 FAR threshold established by TOP Certified EIR and would not induce significant population growth. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?

Discussion of Effects: The project site does not contain existing housing and will not create existing housing displacement impacts.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

15. PUBLIC SERVICES. Would the project:

a. Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:

i. Fire protection?

Discussion of Effects: The site is in a developed area currently served by the Ontario Fire Department. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

ii. Police protection?

Discussion of Effects: The site is in a developed area, currently served by the Ontario Police Department. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

iii. Schools?

Discussion of Effects: Upon development, the Project will be required to pay school fees as prescribed by state law prior to the issuance of building permits. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

iv. Parks?

Discussion of Effects: The site is in a developed area, currently served by the City of Ontario. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

v. Other public facilities?

Discussion of Effects: The site is in a developed area, currently served by the City of Ontario. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

16. RECREATION. Would the project:

a. Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?

Discussion of Effects: This project is not proposing any new housing or large employment generator that would cause an increase in the use of neighborhood parks or other recreational facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Does the project include recreational facilities or require the construction or expansion of recreational facilities that have an adverse physical effect on the environment?

Discussion of Effects: This project is not proposing any new housing or large employment generator that would require the construction of neighborhood parks or other recreational facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

17. TRANSPORTATION. Would the project:

a. Conflict with a program plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?

Discussion of Effects: The subject site was analyzed in the Certified EIR as an Industrial land use designation with an assumed 0.55 Floor Area Ratio (FAR). The Project proposes to construct a 256,711 square foot industrial building on 11.3 acres of land with a 0.52 FAR, that is below the 0.55 FAR threshold established by TOP Certified EIR.

Therefore, the traffic impacts will be consistent with the traffic impacts projected and analyzed under the Certified EIR. The project will not create a substantial increase in the number of vehicle trips, traffic volume or congestion at intersections. Less than significant impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Conflict or be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)?

Discussion of Effects: CEQA Guidelines Section 15064.3 subdivision (b) has been included in the 2018 CEQA Guidelines as part of the implementation of SB 743 which requires local jurisdictions to use

Vehicle Miles Travelled (VMT) instead of Level of Service (LOS) methodologies for the purpose of determining the significance of traffic impacts under CEQA. Also, as part of the implementation of SB 743 local jurisdiction were given until July 1, 2020 to develop and implement thresholds of significance criteria and methodologies for evaluating VMT under the new SB 743 requirements. The City of Ontario has adopted and established a VMT analysis threshold or analysis methodology based on our Policy Plan (General Plan) baseline. However, the project was submitted prior to the adoption of the threshold and therefore not subject to the adopted thresholds. Subsequently, the subject site was analyzed in the Certified EIR as an Industrial land use designation with an assumed 0.55 Floor Area Ratio (FAR). The Project proposes to construct a 256,711 square foot industrial building on 11.3 acres of land with a 0.52 FAR, that is below the 0.55 FAR threshold established by TOP Certified EIR. Therefore, impacts with respect to CEQA Guidelines Section 15064.3(b) are less than significant.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?

Discussion of Effects: The Project is in an area that is mostly developed. All street improvements are complete, and the Project will be required to construct right-of-way improvements along the project street frontage. The Project will, therefore, not create a substantial increase in hazards due to a design feature. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Result in inadequate emergency access?

Discussion of Effects: Development of the Project will be designed to provide access for all emergency vehicles and will therefore not create an inadequate emergency access. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

e. Result in inadequate parking capacity?

Discussion of Effects: The Project will be required to meet parking standards established by the Ontario Development Code and will therefore not create an inadequate parking capacity. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

18. TRIBAL CULTURAL RESOURCES. Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:

a. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k)?

Discussion of Effects: The subject site was previously analyzed by the TOP FEIR as an industrial use and is not listed in the California Register of Historic Resources or local register of historical resources. Development of the site will not create greater impacts than were identified in the Certified TOP FEIR.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.

Discussion of Effects: The subject site is not listed in the California Register of Historic Resources. No consultation had been initiated and no impacts are anticipated through Project implementation.

Mitigation: No new mitigation measures are required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

19. UTILITIES AND SERVICE SYSTEMS. Would the project:

a. Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?

Discussion of Effects: The proposed project is served by the City of Ontario sewer system and which has waste treated by the Inland Empire Utilities Agency at the RP-1 treatment plant. RP-1 is not at capacity and this project will not cause RP-1 to exceed capacity. The project will therefore not require the construction of new wastewater treatment facilities, or the expansion of existing facilities. No impacts are anticipated.

As discussed in the energy section above, the Project will have less than significant impacts with regard to electric power and natural gas. In addition, the Project will not have an impact on telecommunications facilities.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years? In making this determination, the City shall consider whether the project is subject to the water supply assessment requirements of Water Code Section 10910, et seq. (SB 610), and the requirements of Government Code Section 664737 (SB 221).

Discussion of Effects: The Project site is served by the City of Ontario water system. There is currently sufficient water supply available to the City of Ontario to serve this Project as per the findings of TOP EIR. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Result in a determination by the wastewater treatment provider that serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?

Discussion of Effects: The Project site is served by the City of Ontario sewer system, which has waste treated by the Inland Empire Utilities Agency at the RP-1 treatment plant. RP-1 is not at capacity and future development of this project site will not cause RP-1 to exceed capacity. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Generate solid waste in excess of state or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?

Discussion of Effects: City of Ontario serves the Project site. Currently, the City of Ontario contracts with a waste disposal company that transports trash to a landfill with sufficient capacity to handle the City's solid waste disposal needs. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to analyses are necessary.

e. Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?

Discussion of Effects: This Project complies with federal, state, and local statutes and regulations regarding solid waste. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

20. WILDFIRE. If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:

a. Substantially impair an adopted emergency response plan or emergency evacuation plan?

Discussion of Effects: Wildfire impacts were not analyzed in the Certified EIR. A discussion of potential wildfire impacts is provided herein.

The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?

Discussion of Effects: The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?

Discussion of Effects: The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?

Discussion of Effects: The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

21. MANDATORY FINDINGS OF SIGNIFICANCE.

a. Does the project have the potential to substantially degrade the quality of the environment, substantially reduce the habitat or a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?

Discussion of Effects: The proposed Project does not have the potential to reduce wildlife habitat and threaten a wildlife species; therefore, no environmental impacts resulting from the Project are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Does the project have the potential to achieve short-term environmental goals to the disadvantage of long-term environmental goals?

Discussion of Effects: The Project does not have the potential to achieve short-term environmental goals to the disadvantage of long-term environmental goals.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current project, and the effects of probable future projects.)

Discussion of Effects: The Project does not have impacts that are cumulatively considerable.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Does the project have environmental effects that will cause substantial adverse effects on human beings, either directly or indirectly?

Discussion of Effects: The Project does not have environmental effects that will cause substantial adverse effects on human beings, either directly or indirectly.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

EARLIER ANALYSES

(Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, one or more effects have been adequately analyzed in an earlier EIR or Negative Declaration. Section 15063(c)(3)(D)):

- 1) Earlier Analyses Used. Identify earlier analyses used and state where they are available for review.
 - a) The Ontario Plan Final EIR
 - b) The Ontario Plan
 - c) City of Ontario Official Zoning Map
 - d) City of Ontario Development Code
 - e) Ontario International Airport Land Use Compatibility Plan
 - f) Ontario International Airport Land Use Compatibility Plan Negative Declaration (State Clearinghouse No. 2011011081)
 - g) Vista Environmental – Diesel Emissions Health Risk Assessment – November 30, 2020

All documents listed above are on file with the City of Ontario Planning Department, 303 East “B” Street, Ontario, California 91764, (909) 395-2036.

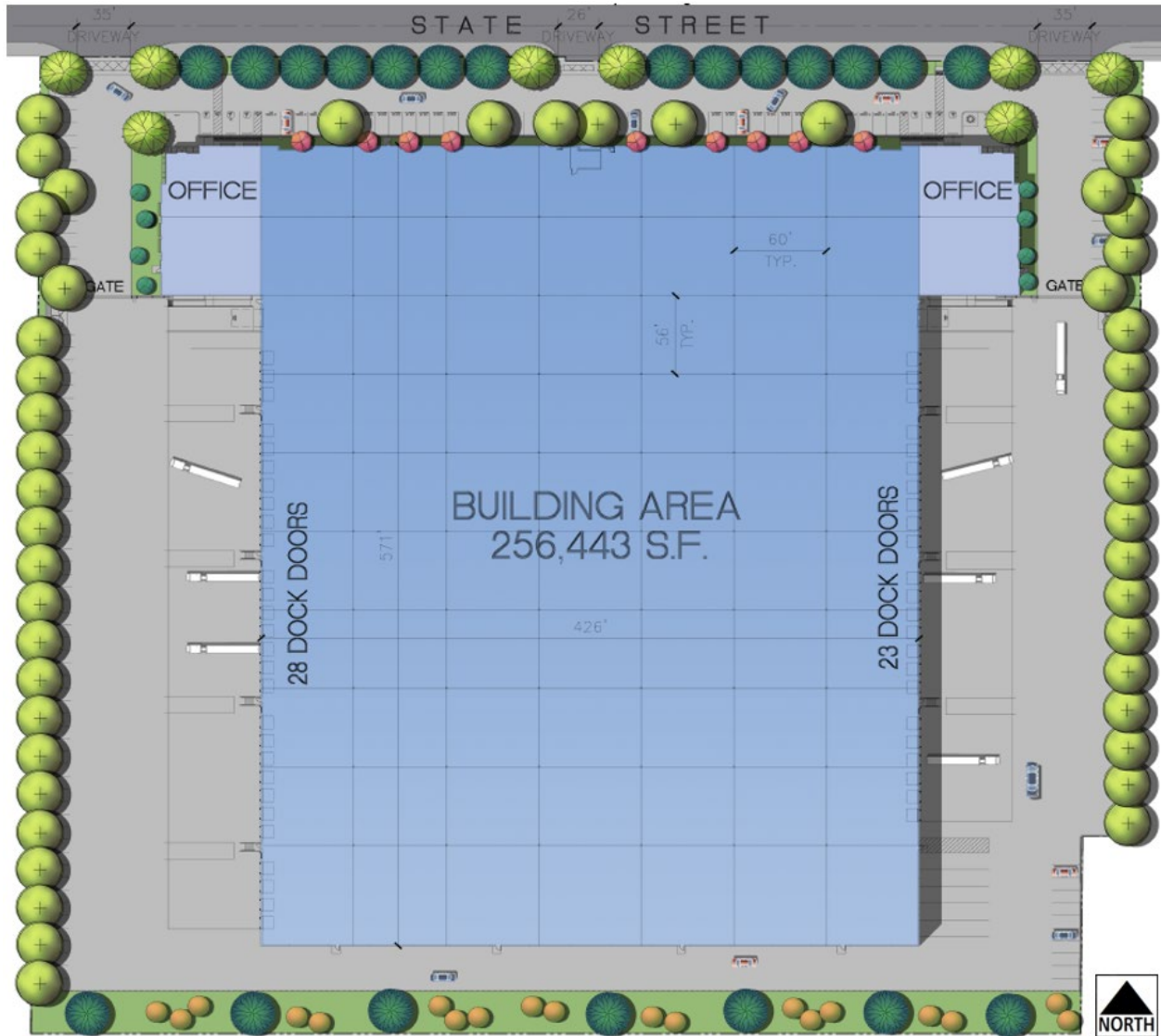
- 2) Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of, and adequately analyzed in, an earlier document pursuant to applicable legal standards.

MITIGATION MEASURES

(For effects that are “Less than Significant with Mitigation Incorporated,” describe the mitigation measures, which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.)

The Mitigation Measures contained in the Certified EIR adequately mitigate the impacts of the proposed Project. These mitigation measures are contained in the attached Mitigation Monitoring Program. No additional mitigation beyond that previously imposed is required.

Exhibit A: PDEV20-005 – SITE PLAN



Mitigation Monitoring Program for The Ontario Plan Environmental Impact Report

1. Introduction

1.1 PURPOSE OF MITIGATION MONITORING PROGRAM

This Mitigation Monitoring Program has been developed to provide a vehicle by which to monitor mitigation measures and conditions of approval outlined in The Ontario Plan Environmental Impact Report (EIR), State Clearinghouse No. 2008101140. The Mitigation Monitoring Program has been prepared in conformance with Section 21081.6 of the Public Resources Code and City of Ontario Monitoring Requirements. Section 21081.6 states:

(a) When making the findings required by paragraph (1) of subdivision subsection (a) of Section 21081 or when adopting a mitigated negative declaration pursuant to paragraph (2) of subdivision (c) of Section 21080, the following requirements shall apply:

(1) The public agency shall adopt a reporting or monitoring program for the changes made to the project or conditions of project approval, adopted in order to mitigate or avoid significant effects on the environment. The reporting or monitoring program shall be designed to ensure compliance during project implementation. For those changes which have been required or incorporated into the project at the request of a responsible agency or a public agency having jurisdiction by law over natural resources affected by the project, that agency shall, if so requested by the lead agency or a responsible agency, prepare and submit a proposed reporting or monitoring program.

(2) The lead agency shall specify the location and custodian of the documents or other material which constitute the record of proceedings upon which its decision is based.

(b) A public agency shall provide that measures to mitigate or avoid significant effects on the environment are fully enforceable through permit conditions, agreements, or other measures. Conditions of project approval may be set forth in referenced documents which address required mitigation measures or, in the case of the adoption of a plan, policy, regulation, or other public project, by incorporating the mitigation measures into the plan, policy, regulation, or project design.

(c) Prior to the close of the public review period for a draft environmental impact report or mitigated negative declaration, a responsible agency, or a public agency having jurisdiction over natural resources affected by the project, shall either submit to the lead agency complete and detailed performance objectives for mitigation measures which would address the significant effects on the environment identified by the responsible agency or agency having jurisdiction over natural resources affected by the project, or refer the lead agency to appropriate, readily available guidelines or reference documents. Any mitigation measures submitted to a lead agency by a responsible agency or an agency having jurisdiction over natural resources affected by the project shall be limited to measures which mitigate impacts to resources which are subject to the statutory authority of, and definitions applicable to, that agency. Compliance or noncompliance by a responsible agency or agency having jurisdiction over natural resources affected by a project with that requirement shall not limit the authority of the responsible agency or agency having jurisdiction over natural resources affected by a project, or the authority of the lead agency, to approve, condition, or deny projects as provided by this division or any other provision of law.

1.2 PROJECT SUMMARY

The proposed project is the preparation of The Ontario Plan, which consists of a Vision, Governance Manual, Policy Plan, City Council Priorities, Implementation Plans, and Tracking and

Feedback. The Ontario Plan integrates components of city governance documents into a single guidance system that shapes the community 20 years or more into the future.

(a) The Ontario **Vision** describes the future community of Ontario. Its basic purpose is to improve the quality of life for the people of Ontario. It is the rationale and motivation for everything the City does.

(b) The **Governance Manual** describes the foundation for conducting the public's business on behalf of the present and future people of Ontario. It explains how The Ontario Plan is a tool for decision-making and communication.

(c) **City Council Priorities** define the short-term direction in City actions and initiatives. They are the primary means for exercising leadership in carrying out The Plan and realizing the Vision.

(d) The **Policy Plan** connects intent with action through the broad range of Goals and Policies that would guide the long-term growth and development required for the City to achieve its Vision. It also satisfies the California Government Code requirement for a general plan. Figure 3-6, *Proposed Land Use Plan*, shows the proposed General Plan land use designations that guide and regulate land use patterns, distributions, densities and intensities in the City of Ontario, including residential employment, retail, recreation, and public uses.

(e) **Implementation** consists of actions taken to carry out Plan policies. This includes initiatives by the City and decisions on public and private development projects.

(f) **Tracking and Feedback** allows the City to learn from experience and redirect efforts.

Pursuant to CEQA Guidelines Section 15064(d), the EIR considers the direct physical changes and reasonably foreseeable indirect physical changes in the environment that would be caused by The Ontario Plan. Consequently, the EIR focuses on impacts from changes to land use associated with buildout of the Proposed Land Use Plan, within the Policy Plan, and impacts from the resultant population and employment growth in the City. The Ontario Plan Proposed Land Use Plan for the ultimate development of the City is not linked to a timeline. However, for the purpose of this environmental analysis, buildout of the Proposed Land Use Plan is forecast for the year 2035.

1.3 PROJECT LOCATION

The City of Ontario is in the southwestern corner of San Bernardino County and is surrounded by the Cities of Chino and Montclair, and unincorporated areas of San Bernardino County to the west; the Cities of Upland and Rancho Cucamonga to the north; the City of Fontana and unincorporated land in San Bernardino County to the east; and unincorporated Riverside County land to the south. The City is in the central part of the Upper Santa Ana River Valley. This portion of the valley is bounded by the San Gabriel Mountains to the north; the Chino Hills, Puente Hills, and San Jose Hills to the west; the Santa Ana River to the south; and Lytle Creek Wash on the east.

The City comprises approximately 50 square miles (31,958 acres), which includes the 8,200-acre New Model Colony (NMC) in the southern portion of the City (formerly the City's Sphere of Influence). The northern urbanized portion of the City is known as the Original Model Colony (OMC). The City is generally bounded by Benson Avenue and Euclid Avenue on the west; Interstate 10 (I-10), 8th Street, and 4th Street on the north; Etiwanda Avenue and Hamner Avenue on the east; and Merrill Avenue and the San Bernardino County/Riverside County boundary on the south. Regional circulation to and through the City is provided by I-10 and State Route 60 (SR-60) east-west, and by I-15 and SR-83 (Euclid Avenue) north-south.

1.4 ENVIRONMENTAL IMPACTS

The environmental document for this project is a “program EIR” as defined by CEQA Guidelines (Section 15161, California Code of Regulations, Title 14, Division 6, Chapter 3). As provided in Section 15168 of the CEQA Guidelines, a Program EIR may be prepared on a series of actions that may be characterized as one large project that are related either 1) geographically; 2) as logical parts of a chain of contemplated events; 3) in connection with issuance of rules, regulations, plans, or other general criteria to govern the conduct of a continuing program; or 4) as individual activities carried out under the same authorizing statutory or regulatory authority and have generally similar environmental effects that can be mitigated in similar ways.

Although the legally required contents of a Program EIR are the same as those of a Project EIR, Program EIRs are typically more conceptual and may contain a more general discussion of impacts, alternatives, and mitigation measures than a Project EIR. Once a Program EIR has been prepared, subsequent activities within the program must be evaluated to determine whether an additional CEQA document needs to be prepared. However, if the Program EIR addresses the program’s effects as specifically and comprehensively as possible, many subsequent activities could be found to be within the Program EIR scope and additional environmental documents may not be required (Guidelines Section 15168[c]). When a Program EIR is relied on for a subsequent activity, the lead agency must incorporate feasible mitigation measures and alternatives developed in the Program EIR into the subsequent activities (Guidelines Section 15168[c][1]). If a later activity would have effects that were not examined in the Program EIR, a new Initial Study would need to be prepared leading to either an EIR or a Negative Declaration. Pursuant to Sections 15126.2 and 15126.4 of the CEQA Guidelines, the EIR should identify any potentially significant adverse impacts and recommend mitigation that would reduce or eliminate these impacts to levels of insignificance.

1.4.1 Impacts Considered Less Than Significant

Ten environmental categories are identified as having less than significant impacts that do not require mitigation. These categories are:

- Aesthetics
- Biological Resources
- Geology/Soils
- Hazards & Hazardous Materials
- Hydrology/Water Quality
- Land Use & Planning
- Mineral Resources
- Population and Housing
- Public Services
- Recreation

1.4.2 Potentially Significant Adverse Impacts That Can Be Mitigated, Avoided, or Substantially Lessened

The following have been identified as potentially resulting in significant adverse impacts that can be mitigated, avoided, or substantially lessened:

- Cultural Resources: Mitigation Measures 5-2 through 5-4 would reduce archeological and prehistoric cultural resource impacts to less than significant.
- Noise: Mitigation Measure 12-3 would ensure that any new vibration-sensitive structures near the Union Pacific Railroad or Southern California Regional Rail Authority right-of-way would be constructed so that train-related vibration would not be perceptible and operational vibration impacts would be less than significant.
- Utilities and Service Systems: Mitigation Measures 17-1 through 17-4 would reduce impacts on water supply and demand from buildout of The Ontario Plan to less than significant.

1.4.3 Unavoidable Significant Adverse Impacts

There are six environmental categories considered to have impacts that would be significant and unavoidable and would not be lessened through mitigation.

Agricultural Resources

Buildout of The Ontario Plan would convert 3,269.3 acres of California Resource Agency designated Prime Farmland, Unique Farmland, and Farmland of Statewide Importance to residential, commercial, mixed-use, and industrial land uses. Consequently, impacts to Farmland would remain significant and unavoidable.

There are a number of Williamson Act contracts within the City that have yet to expire. Buildout of The Ontario Plan would most likely require the cancellation or nonrenewal of these contracts. The current use of these contracts would slow the rate of conversion from agricultural to nonagricultural land, but it would not impede the conversion. Since there are some Williamson Act contracts still active in the New Model Colony, implementation of the proposed land use plan for The Ontario Plan would conflict with these contracts and cause a significant impact. Consequently, impacts to Williamson Act contracts would remain significant and unavoidable.

Development of the City in accordance with The Ontario Plan would increase the amount of nonagricultural land uses. When nonagricultural land uses are placed near agricultural uses, the odors, noises, and other hazards related to agriculture conflict with the activities and the quality of life of the people living and working in the surrounding areas. Consequently, conversion of agricultural uses in the city may cause farms and agricultural land uses outside the City to be converted to nonagricultural uses because of the nuisances related to agriculture and impacts would remain significant and unavoidable.

Air Quality

The project would not be consistent with the Air Quality Management Plan (AQMP) because air pollutant emissions associated with buildout of the City of Ontario would cumulatively contribute to the nonattainment designations in the South Coast Air Basin (SoCAB). Furthermore, buildout of the Proposed Land Use Plan would exceed current estimates of population, employment, and vehicle miles traveled for Ontario and therefore these emissions are not included in the current regional emissions inventory for the SoCAB. As both criteria must be met in order for a project to be considered consistent with the AQMP, the project would be considered inconsistent with the AQMP. Consequently, this impact would remain significant and unavoidable.

Construction activities associated with buildout of The Ontario Plan would generate short-term emissions that exceed the South Coast Air Quality Management District's (SCAQMD) regional significance thresholds; cumulatively contribute to the SoCAB's nonattainment designations for ozone (O₃), coarse inhalable particulate matter (PM₁₀), and fine inhalable particulate matter (PM_{2.5}); and potentially elevate concentrations of air pollutants at sensitive receptors. Mitigation Measure 3-1 would reduce The Ontario Plan's short-term construction-related volatile organic compounds (VOC), carbon monoxide (CO), oxides of nitrogen (NO_x), PM₁₀, and PM_{2.5} emissions but they would not be reduced to levels below the SCAQMD's regional thresholds and they would not reduce these impacts to less than significant. Consequently, construction air pollutant emissions generated by buildout of The Ontario Plan would remain significant and unavoidable.

Buildout of The Ontario Plan would generate long-term emissions that would exceed SCAQMD'S regional significance thresholds and cumulatively contribute to the SoCAB nonattainment designations for O₃, PM₁₀, and PM_{2.5}. Mitigation Measure 3-2 would reduce long-term operational emissions of VOC, CO, NO_x, PM₁₀, and PM_{2.5} related to the buildout of The Ontario Plan but they would not reduce these emissions to levels below the SCAQMD's regional significance thresholds and impacts would not be less than significant. Consequently, operational impacts from buildout of The Ontario Plan would remain significant and unavoidable.

Approval of residential and other sensitive land uses within 500 feet of Interstate-10, Interstate-15, or State Route-60 would result in exposure of persons to substantial concentrations of diesel particulate matter. Mitigation Measure 3-3 would reduce impacts related to the exposure of sensitive receptors (residential and other sensitive land uses) to diesel particulate matter because of their placement near freeways within the City. However, it would not reduce this impact to be less than significant.

Conversion of agricultural land to nonagricultural uses would temporarily expose residents to objectionable odors and impacts would remain significant and unavoidable.

Cultural Resources

Although protective regulations are in place and preservation policies are included in The Ontario Plan, implementation of the Proposed Land Use Plan, especially within growth focus areas, has the potential to impact Tier III historic resources. Mitigation Measure 5-1 would require a historical evaluation for properties within historic resources in the Focus Areas under the City's ordinance. However, the ordinance does not provide a high level of protection for Tier III resources. As a result, demolition of historical resources categorized under the Ordinance as Tier III could potentially be impacted with implementation of the Proposed Land Use Plan. Consequently, Tier III historic resource impacts would remain significant and unavoidable.

Global Climate Change

Buildout of the City of Ontario would generate greenhouse gas emissions that would significantly contribute to global climate change impacts in California. Greenhouse gas (GHG) emissions generated in the City would significantly contribute to climate change impacts in California as a result of the growth in population and employment in the City and scale of development activity associated with buildout of the Proposed Land Use Plan. Mitigation Measures 6-1 through 6-6 would act to reduce the contributions of The Ontario Plan to global climate change but they would not reduce the impacts to less than significant.

Noise

Buildout of the Proposed Land Use Plan would result in an increase in traffic on local roadways in the City of Ontario, which would substantially increase noise levels. Consequently, impacts would remain significant and unavoidable.

Noise-sensitive uses could be exposed to elevated noise levels from transportation sources. Any siting of new sensitive land uses within a noise environment that exceeds the normally acceptable land use compatibility criterion would result in a potentially significant impact and would require a separate noise study through the development review process to determine the level of impacts and required mitigation. Mitigation Measure 12-1 would decrease the exposure of sensitive receptors to excessive noise levels within 65 dBA CNEL contours, whether near Los Angeles/Ontario International Airport (LAONT) or other noise-producing areas such as freeways and railroads, but it would not reduce these impacts to less than significant.

Construction activities associated with buildout of the individual land uses associated with the Proposed Land Use Plan would expose sensitive uses to strong levels of groundborne vibration. Mitigation Measure 12-2 would reduce the impacts caused by construction-related vibrations on sensitive receptors but it would not reduce the impact to less than significant.

Impact 5.12-5. Significant. Construction activities associated with buildout of the individual land uses associated with the Proposed Land Use Plan would substantially elevate noise levels in the vicinity of sensitive land uses. Mitigation Measure 12-4 calls for the use of noise-reducing techniques during construction projects that would impact nearby sensitive receptors, such as the use of temporary sound walls and reduced unnecessary truck idling. However, these impacts would not be reduced to levels considered less than significant.

Noise-sensitive land uses within the 65 dBA CNEL contour of the Los Angeles/Ontario International Airport would be exposed to substantial levels of airport-related noise. Consequently, impacts would remain significant and unavoidable.

Transportation and Traffic

The increased development and population growth associated with the buildout of the Proposed Land Use Plan would cause deficient levels of service at area intersections without implementation of the recommended lane geometry improvements. In addition, buildout of the Proposed Land Use Plan would also cumulatively contribute to the cumulatively significant freeway level of service impact that is already projected to occur in the future. Mitigation Measure 16-1 would require the buildout of The Ontario Plan to be consistent with the traffic study prepared by Kimley-Horn and Associates. This traffic study indicates the appropriate lane geometry for area intersections. This would allow for intersections to have LOS values of E or above, but it would not improve the cumulative freeway LOS standards to appropriate levels. The City has no jurisdiction over Caltrans projects, such as freeway improvements. Therefore, the impacts related to cumulative LOS deficiencies on freeways would not be reduced to levels considered less than significant.

2. Mitigation Monitoring Process

2.1 MITIGATION MONITORING PROGRAM ORGANIZATION

CEQA requires that a reporting or monitoring program be adopted for the conditions of project approval that are necessary to mitigate or avoid significant effects on the environment (Public Resources Code 21081.6). The mitigation monitoring and reporting program is designed to ensure compliance with adopted mitigation measures during project implementation. For each mitigation measure recommended in the Environmental Impact Report, specifications are made herein that identify the action required and the monitoring that must occur. In addition, a responsible agency is identified for verifying compliance with individual conditions of approval contained in the Mitigation Monitoring Program (MMP). In order to effectively track and document the status of mitigation measures, a mitigation matrix has been prepared and includes:

- Responsibility for implementation
- Timing
- Responsibility for monitoring
- Monitor

Mitigation measure timing of verification has been apportioned into several specific timing increments. Of these, the most common are:

- Prior to project approval
- Prior to issuance of grading permit(s)
- During construction

Information pertaining to compliance with mitigation measures or any necessary modifications or refinements will be documented in the comments portion of the matrix.

2.2 MITIGATION MONITORING PROCEDURES

The City of Ontario Planning Department is the designated lead agency for the Mitigation Monitoring and Reporting Program. The City of Ontario includes the Mitigation Measures within the Special Conditions of Approval. The City is responsible for review of all monitoring reports, enforcement actions, and document disposition. The Planning Department shall designate a Project Mitigation Monitor for the proposed project.

2.2.1 In-Field Monitoring

The Responsible Monitoring Party shall exercise caution and professional practices at all times when monitoring construction. Protective wear (hard hats, glasses, etc.) shall be worn at all times in construction areas. Injuries shall be reported immediately to the Project Mitigation Monitor.

2.2.2 Coordination with Contractors

The construction manager/superintendent is responsible for coordination of contractors and for contractor completion of required measures in accordance with the provisions of this program.

2.2.3 Recognized Experts

The use of recognized experts as a component of the monitoring team is required to ensure compliance with scientific and engineering mitigation measures. While the recognized experts assess

compliance with required mitigation measures, consultation with the City of Ontario planning staff shall take place in the event of a dispute.

2.2.4 Enforcement

Agencies may enforce conditions of approval through their existing police power, using stop-work orders, fines, infraction citations, loss of entitlements, refusal to issue building permits or certificates of use and occupancy or, in some cases, notice of violation for tax purposes. Criminal misdemeanor sanctions could be available where the agency has adopted an ordinance requiring compliance with the monitoring program, similar to the provision in many zoning ordinances that affirm the enforcement power to bring suit against violators of the ordinances.

3. Mitigation Monitoring Requirements

3.1 CATEGORIZED MITIGATION MEASURES/MATRIX

Project-specific mitigation measures have been categorized in matrix format, as shown in Table 3-1. The matrix identifies the environmental factor, specific mitigation measures, schedule, and responsible monitor. The mitigation matrix will serve as the basis for scheduling the implementation of, and compliance with, all mitigation measures.

3.2 IN-FIELD MONITORING

Project monitors and technical subconsultants shall exercise caution and professional practices at all times when monitoring implementation of mitigation measures. Protective wear (e.g., hard hat, glasses) shall be worn at all times in construction areas. Injuries shall be immediately reported to the mitigation monitoring committee.

3.3 DATABASE MANAGEMENT

All mitigation monitoring reports, letters, and memos shall be prepared using Microsoft Word software on IBM-compatible PCs and processed according to the City's Environmental Compliance Program.

3.4 COORDINATION WITH CONTRACTORS

The construction manager is responsible for coordination of contractors and for contractor completion of required mitigation measures.

3.5 LONG-TERM MONITORING

Long-term monitoring related to several mitigation measures will be required, including fire safety inspections. Post-construction fire inspections are conducted on a routine basis by the Ontario Fire Department.

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
5.3 AIR QUALITY				
<p>3-1 The City of Ontario Building Department shall require that all new construction projects incorporate feasible mitigation measures to reduce air quality emissions. Potential measures shall be incorporated as conditions of approval for a project and may include:</p> <ul style="list-style-type: none"> • Requiring fugitive dust control measures that exceed South Coast Air Quality Management District's Rule 403, such as: <ul style="list-style-type: none"> ○ Requiring use of nontoxic soil stabilizers to reduce wind erosion. ○ Applying water every four hours to active soil-disturbing activities. ○ Tarping and/or maintaining a minimum of 24 inches of freeboard on trucks hauling dirt, sand, soil, or other loose materials. • Using construction equipment rated by the United States Environmental Protection Agency as having Tier 3 or higher exhaust emission limits. • Ensuring construction equipment is properly serviced and maintained to the manufacturer's standards. • Limiting nonessential idling of construction equipment to no more than five consecutive minutes. • Using Super-Compliant VOC paints for coating of architectural surfaces whenever possible. A list of Super-Compliant architectural coating manufactures can be found on the South Coast Air Quality Management District's website at: http://www.aqmd.gov/prdas/brochures/Super-Compliant_AIM.pdf . 	<p>City of Ontario Building Department in coordination with the landowner/project applicant's construction contractor</p>	<p>During construction</p>	<p>City of Ontario Building and Department and Developer/Contractor</p>	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
3-2 The City of Ontario shall evaluate new development proposals within the City and require all developments to include access or linkages to alternative modes of transportation, such as transit stops, bike paths, and/or pedestrian paths (e.g., sidewalks).	City of Ontario Planning/Engineering Department in coordination with the landowner/project applicant	Prior to individual project approvals	City of Ontario Planning Department	
3-3 The City of Ontario shall evaluate new development proposals within the City for potential incompatibilities with regard to the California Air Resources Board's <i>Air Quality and Land Use Handbook: A Community Health Perspective</i> (April 2005). New development that is inconsistent with the recommended buffer distances shall only be approved if feasible mitigation measures, such as high efficiency Minimum Efficiency Reporting Value filters have been incorporated into the project design to protect future sensitive receptors from harmful concentrations of air pollutants as a result of proximity to existing air pollution sources.	City of Ontario Planning Department in coordination with the landowner/project applicant	Prior to individual project approvals	City of Ontario Planning Department	
5.5 CULTURAL RESOURCES				
5-1 Historic or potentially historic resources in the City shall be evaluated for historic significance through the City's tier system prior to the issuance of plan or development approvals.	City of Ontario Planning Department	Prior to individual project approvals	City of Ontario Planning Department	
5-2 In areas of documented or inferred archaeological and/or paleontological resource presence, City staff shall require applicants for development permits to provide studies to document the presence/absence of such resources. On properties where resources are identified, such studies shall provide a detailed mitigation plan, including a monitoring program and recovery and/or in situ preservation plan, based on the recommendations of a qualified cultural	City of Ontario Planning Department in coordination with the Landowner/Project Applicant	Prior to individual project approvals	City of Ontario Planning Department	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>preservation expert. The mitigation plan shall include the following requirements:</p> <ul style="list-style-type: none"> • Archaeologists and/or paleontologist shall be retained for the project and will be on call during grading and other significant ground-disturbing activities. • Should any cultural resources be discovered, no further grading shall occur in the area of the discovery until the Planning Director or designee is satisfied that adequate provisions are in place to protect these resources. • Unanticipated discoveries shall be evaluated for significance by a San Bernardino County Certified Professional Archaeologist/Paleontologist. If significance criteria are met, then the project shall be required to perform data recovery, professional identification, radiocarbon dates, and other special studies; submit materials to a museum for permanent curation; and provide a comprehensive final report including catalog with museum numbers. 				
<p>5-3 Upon receipt of an application for a Specific Plan or a project that requires a General Plan amendment subject to CEQA and is within the City's jurisdiction, the City's representative shall consult with the relevant tribe(s)' representative(s) to determine if the proposed project is within a culturally sensitive area to the tribe. If sufficient evidence is provided to reasonably ascertain that the site is within a [tribal] culturally sensitive area, then a cultural resources assessment prepared by an archaeologist shall be required. The findings of the cultural resources assessment shall be incorporated into the CEQA documentation. A copy of the report shall be forwarded to the tribe(s). If mitigation is recommended in the CEQA document, the procedure described in Mitigation Measure 5-4 shall be followed.</p>	<p>City of Ontario Planning Department</p>	<p>Prior to individual project approvals</p>	<p>City of Ontario Planning Department</p>	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>5-4 Prior to the issuance of grading permits for a Specific Plan or project that requires a General Plan amendment for which the CEQA document defines cultural resource mitigation for potential tribal resources, the project applicant shall contact the designated tribe(s) to notify them of the grading, excavation, and monitoring program. The applicant shall coordinate with the City of Ontario and the tribal representative(s) to develop mitigation measures that address the designation, responsibilities, and participation of tribal monitors during grading, excavation, and ground-disturbing activities; scheduling; terms of compensation; and treatment and final disposition of any cultural resources, sacred sites, and human remains discovered on the site. The City of Ontario shall be the final arbiter of the conditions for projects within the City's jurisdiction.</p>	<p>City of Ontario Planning Department in coordination with the Landowner/Project Applicant</p>	<p>Prior to issuance of grading permit(s)</p>	<p>City of Ontario Planning Department</p>	
5.6 Global Climate Change				
<p>6-1 The City of Ontario shall prepare a Climate Action Plan within 18 months after adopting The Ontario Plan. The goal of the Climate Action Plan shall be to reduce GHG emissions from all activities within the City boundaries to support the State's efforts under AB 32 and to mitigate the impact of climate change on the City, State, and world. Once completed, the City shall update The Ontario Plan and associated policies, as necessary, to be consistent with the Climate Action Plan and prepare a subsequent or supplemental Environmental Impact Report, if new significant impacts are identified. The Climate Action Plan shall include the following:</p> <ul style="list-style-type: none"> • Emission Inventories: The City shall establish GHG emissions inventories including emissions from all sectors within the City, using methods approved by, or consistent with guidance from, the CARB; the City shall update inventories every 3 years or as determined by state standards to incorporate improved methods, better 	<p>City of Ontario</p>	<p>Within 18 months of adopting The Ontario Plan</p>	<p>City of Ontario Planning Department/ Municipal Utilities Agency (MUA)</p>	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>data, and more accurate tools and methods, and to assess progress. If the City is not on-schedule to achieve the GHG reduction targets, additional measures shall be implemented, as identified in the CAP.</p> <ul style="list-style-type: none"> ○ The City shall establish a baseline inventory of GHG emissions including municipal emissions, and emissions from all business sectors and the community. ○ The City shall define a “business as usual” scenario of municipal, economic, and community activities, and prepare a projected inventory for 2020 based on that scenario. ● Emission Targets: The City will develop Plans to reduce or encourage reductions in GHG emissions from all sectors within the City: <ul style="list-style-type: none"> ○ A Municipal Climate Action Plan which shall include measures to reduce GHG emissions from municipal activities by at least 30 percent by 2020 compared to the "business as usual" municipal emissions (including any reductions required by the California Air Resource Board under AB 32. ○ A Business Climate Action Plan in collaboration with the business community, which shall include measures to reduce GHG emissions from business activities, and which shall seek to reduce emissions by at least 30 percent by 2020 compared to "business as usual" business emissions. ○ A Community Climate Action Plan in collaboration with the stakeholders from the community at large, which shall include measures reduce GHG emissions from community activities, and which shall seek to reduce emissions by at least 30 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
percent by 2020 compared to "business as usual" community emissions.				
<p>6-2 The Climate Action Plan shall include specific measures to achieve the GHG emissions reduction targets identified in Mitigation Measure 6-1. The Climate Action Plan shall quantify the approximate greenhouse gas emissions reductions of each measure and measures shall be enforceable. Measures listed below, along with others, shall be considered during the development of the Climate Action Plan (CAP):</p> <ul style="list-style-type: none"> • Require all new or renovated municipal buildings to seek Silver or higher Leadership in Energy and Environmental Design (LEED) standard, or compliance with similar green building rating criteria. • Require all municipal fleet purchases to be fuel efficient vehicles for their intended use based on the fuel type, design, size, and cost efficiency. • Require that new development projects in Ontario that require demolition prepare a demolition plan to reduce waste by recycling and/or salvaging a nonhazardous construction and demolition debris. • Require that new developments design buildings to be energy efficient by siting buildings to take advantage of shade, prevailing winds, landscaping, and sun screening to reduce energy required for cooling. • Require that cool roofs for non-residential development and cool pavement to be incorporated into the site/building design for new development where appropriate. • Evaluate the feasibility of implementing a Public Transit Fee to support Omnitrans in developing additional transit service in the City. 	City of Ontario Planning Department	Within 18 months of adopting The Ontario Plan	City of Ontario Planning Department	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Require diesel emission reduction strategies to eliminate and/or reduce idling at truck stops, warehouses, and distribution facilities throughout the City. • Install energy efficient lighting and lighting control systems in all municipal buildings. • Require all new traffic lights installed be energy efficient traffic signals. • Require the use of reclaimed water for landscape irrigation in all new development and on public property where such connections are within the service boundaries of the City's reclaimed water system. • Require all new landscaping irrigation systems installed within the City to be automated, high-efficient irrigation systems to reduce water use and require use of bubbler irrigation; low-angle, low-flow spray heads; or moisture sensors. • Conduct energy efficiency audits of existing municipal buildings by checking, repairing, and readjusting heating, ventilation, and air conditioning systems, lighting, water heating equipment, insulation, and weatherization. • Ensure that its local Climate Action, Land Use, Housing, and Transportation Plans are aligned with, support, and enhance any regional plans that have been developed consistent with state guidance to achieve reductions in GHG emissions. • Mitigate climate change by decreasing heat gain from pavement and other hard surfaces associated with infrastructure. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Reduce heat gain from pavement and other similar hardscaping. • Work with appropriate agencies to create an interconnected transportation system that allows a shift in travel from private passenger vehicles to alternative modes, including public transit, ride sharing, car-sharing, bicycling and walking. • Provide safe and convenient access for pedestrians and bicyclists to, across, and along major transit priority streets. • Facilitate employment opportunities that minimize the need for private vehicle trips, by: <ul style="list-style-type: none"> ○ Amending zoning ordinances and the Development Code to include live/work sites and satellite work centers in appropriate locations. ○ Encouraging telecommuting options with new and existing employers, through project review and incentives, as appropriate. • Establish policies and programs to reduce onsite parking demand and promote ride-sharing and public transit at large events. • Support and promote the use of low-and zero-emission vehicles, by: <ul style="list-style-type: none"> ○ Encouraging the necessary infrastructure to facilitate the use of zero- emission vehicles and clean alternative fuels, such as electric vehicle charging facilities and conveniently located alternative fueling stations. ○ Encouraging new construction to include vehicle access to properly wired outdoor receptacles to 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>accommodate ZEV and/or plug in electric hybrids (PHEV).</p> <ul style="list-style-type: none"> ○ Encouraging transportation fleet standards to achieve the lowest emissions possible, using a mix of alternate fuels, PZEV or better fleet mixes. ○ Establishing incentives, as appropriate, to taxicab owners to use alternative fuel or gas-electric hybrid vehicles. <ul style="list-style-type: none"> ● Establish green building requirements and standards for new development and redevelopment projects, and work to provide incentives for green building practices and remove barriers that impede their use. ● Allow increased height limits and/or flexibility in other standards for projects that incorporate energy efficient green building practices where not prohibited by Airport Land Use Compatibility Plan (ALUCP)/Federal Aviation Administration (FAA). ● Identify and remove regulatory or procedural barriers to implementing green building practices within its jurisdiction, such as updating codes, guidelines, and zoning, and ensure that all plan review and building inspection staff are trained in green building materials, practices, and techniques. ● Support the use of green building practices by: <ul style="list-style-type: none"> ○ Providing information, marketing, training, and technical assistance about green building practices. ○ Adopting a Green Building ordinance with guidelines for green building practices in residential and commercial development. ○ Adopt energy efficiency performance standards for buildings designed to achieve a greater 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>reduction in energy and water use than currently required by state law, including:</p> <ul style="list-style-type: none"> ○ Standards for the installation of "cool roofs". ○ Standards for improved overall efficiency of lighting systems. ○ Requirements for the use of Energy Star appliances and fixtures in discretionary new development. <ul style="list-style-type: none"> • Encourage the performance of energy audits for residential and commercial buildings prior to completion of sale, and that audit results and information about opportunities for energy efficiency improvements be presented to the buyer. • Establish policies and programs that facilitate the siting of new renewable energy generation. • Require that any building constructed in whole or in part with City funds incorporate passive solar design features, such as daylighting and passive solar heating, where feasible. • Prepare and implement a comprehensive plan to improve energy efficiency of municipal facilities, including: <ul style="list-style-type: none"> ○ Conducting energy audits. ○ Retrofitting municipal facilities for energy efficiency where feasible and when remodeling or replacing components, including increased insulation, installing green or reflective roofs and low-emissive window glass. ○ Implementing an energy tracking and management system for its municipal facilities. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Installing energy-efficient exit signs, street signs, and traffic lighting, subject to life/safety considerations. ○ Installing energy-efficient lighting retrofits and occupancy sensors, and institute a "lights out at night" policy, subject to life/safety considerations. ○ Retrofitting heating and cooling systems to optimize efficiency (e.g., replace chillers, boilers, fans, pumps, belts, etc.). ○ Installing Energy Star® appliances and energy-efficient vending machines. ○ Improving water use efficiency, including a schedule to replace or retrofit system components with high- efficiency units (i.e., ultra-low-flow toilets, fixtures, etc.). ○ Installing irrigation control systems which maximize water use efficiency and minimize off- peak use. ○ Adopting an accelerated replacement schedule for energy inefficient systems and components. ● Ensure that staff receives appropriate training and support to implement objectives and policies to reduce GHG emissions, including: <ul style="list-style-type: none"> ○ Providing energy efficiency training to design, engineering, building operations, and maintenance staff. ○ Providing information on energy use and management, including data from the tracking and management system, to managers and 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>others making decisions that influence energy use.</p> <ul style="list-style-type: none"> ○ Providing energy design review services to departments undertaking new construction or renovation projects, to facilitate compliance with LEED standards. • Maximize efficiency at drinking water treatment, pumping, and distribution facilities, including development of off-peak demand schedules for heavy commercial and industrial users. • Establish a replacement policy and schedule to replace fleet vehicles and equipment with the most fuel-efficient vehicles practical, including gasoline hybrid and alternative fuel or electric models. • Require the installation of outdoor electrical outlets on buildings to support the use, where practical, of electric lawn and garden equipment, and other tools that would otherwise be run with small gas engines or portable generators. • Implement measures to reduce employee vehicle trips and to mitigate emissions impacts from municipal travel. • Conduct a comprehensive inventory and analysis of the urban forest, and coordinate tree maintenance responsibilities with all responsible departments, consistent with best management practices. • Evaluate existing landscaping and options to convert reflective and impervious surfaces to landscaping and will install or replace vegetation with drought-tolerant, low- maintenance native species or edible landscaping that can also provide shade and reduce heat-island effects. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Implement enhanced programs to divert solid waste from landfill operations, by: <ul style="list-style-type: none"> ○ Establishing a diversion target which meets or exceeds AB 939 requirements. ○ Promoting and expanding recycling programs, purchasing policies, and employee education to reduce the amount of waste produced. • Reduce per capita water consumption consistent with state law by 2020. • Establish a water conservation plan that may include such policies and actions as: <ul style="list-style-type: none"> ○ Maintaining and refining the City’s tiered rate structure for water use. ○ Establishing restrictions on time of use for landscape watering, or other demand management strategies. ○ Establishing performance standards for irrigation equipment and water fixtures, consistent with state law. • Establish programs and policies to increase the use of recycled water, including: <ul style="list-style-type: none"> ○ Promoting the use of recycled water for agricultural, industrial, and irrigation purposes, including grey water systems for residential irrigation. • Ensure that building standards and permit approval processes promote and support water conservation, by: <ul style="list-style-type: none"> ○ Establishing building design guidelines and criteria to promote water-efficient building design, 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>including minimizing the amount of non-roof impervious surfaces around the building(s).</p> <ul style="list-style-type: none"> ○ Establishing menus and checklists for developers and contractors to ensure water-efficient infrastructure and technology are used in new construction, including low-flow toilets and shower heads, moisture-sensing irrigation, and other such advances. • Organize workshops on waste reduction activities for the home or business, such as backyard composting, or office paper recycling, and shall schedule recycling drop-off events and neighborhood chipping/mulching days. • Organize workshops on steps to increase energy efficiency in the home or business, such as weatherizing the home or building envelope, installing smart lighting systems, and how to conduct a self-audit for energy use and efficiency. 				
<p>6-3 The City of Ontario will amend the Municipal Code within 18 months after adopting The Ontario Plan, with provisions implementing the following GHG emission reduction concepts:</p> <ul style="list-style-type: none"> • Increase densities in urban core areas to support public transit, by, among other means: <ul style="list-style-type: none"> ○ Removing barriers to the development of accessory dwelling units in existing residential neighborhoods. • Reduce required road width standards wherever feasible to calm traffic and encourage alternative modes of transportation. 	City of Ontario Planning Department	Within 18 months of adopting The Ontario Plan	City of Ontario Planning Department	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Add bicycle facilities to city streets and public spaces, where feasible. • Promote infill, mixed-use, and higher density development, and provide incentives to support the creation of affordable housing in mixed use zones. • Plan for and create incentives for mixed-use development. • Identify sites suitable for mixed-use development and establish appropriate site- specific standards to accommodate mixed uses which could include: <ul style="list-style-type: none"> ○ Increasing allowable building height or allow height limit bonuses, in appropriate areas and where safe to do so. ○ Allowing flexibility in applying development standards (such as FAR2 and lot coverage) based on the location, type, and size of the units, and the design of the development. ○ Allowing reduced and shared parking based on the use mix, and availability of and proximity to public transit stops. ○ Allowing for tandem parking, shared parking and off-site parking leases. • Enable prototype mixed-use structures for use in neighborhood center zones that can be adapted to new uses over time with minimal internal remodeling. • Identify and facilitate the inclusion of complementary land uses not already present in local zoning districts, such as supermarkets, parks and recreational fields, schools in neighborhoods, and residential uses in business districts, to reduce the vehicle miles traveled and promote bicycling and walking to these uses. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Revise zoning ordinance(s) to allow local-serving businesses, such as childcare centers, restaurants, banks, family medical offices, drug stores, and other similar services near employment centers to minimize midday vehicle use. • Develop form-based community design standards to be applied to development projects and land use plans, for areas designated mixed-use. • Implement a Housing Overlay Zone for residential properties at transit centers and along transit corridors. This may include average minimum residential densities of 25 units per acre within one quarter miles of transit centers; average minimum densities of 15 units per acre within one quarter mile of transit corridors; and minimum FAR of 0.5:1 for non- residential uses within a quarter mile of transit centers or corridors. • Identify transit centers appropriate for mixed-use development, and promote transit-oriented, mixed-use development within these targeted areas, by: <ul style="list-style-type: none"> ○ Providing maximum parking standards and flexible building height limitations. ○ Providing density bonus programs. ○ Establishing guidelines for private and public spaces for transit-oriented and mixed-use development. ○ Discouraging auto-oriented development. • Ensure new development is designed to make public transit a viable choice for residents, including: <ul style="list-style-type: none"> ○ Locating medium to high density development near activity centers that can be served efficiently 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>by public transit and alternative transportation modes.</p> <ul style="list-style-type: none"> ○ Locating medium to high density development near streets served by public transit whenever feasible. ○ Linking neighborhoods to bus stops by continuous sidewalks or pedestrian paths. ● Develop form-based community design standards to be applied to development projects and land use plans, for areas designated mixed-use. ● Create and preserve distinct, identifiable neighborhoods whose characteristics support pedestrian travel, especially within, but not limited to, mixed-use and transit-oriented development areas, by: <ul style="list-style-type: none"> ○ Designing or maintaining neighborhoods where the neighborhood amenities can be reached in approximately five minutes of walking. ○ Encouraging pedestrian-only streets and/or plazas within developments, and destinations that may be reached conveniently by public transportation, walking, or bicycling. ○ Allowing flexible parking strategies in neighborhood activity centers to foster a pedestrian-oriented streetscape. ○ Providing continuous sidewalks with shade trees and landscape strips to separate pedestrians from traffic. ○ Encouraging neighborhood parks and recreational centers near concentrations of residential areas (preferably within one quarter 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>mile) and include pedestrian walkways and bicycle paths that encourage non- motorized travel.</p> <ul style="list-style-type: none"> • Ensure pedestrian access to activities and services, especially within, but not limited to, mixed-use and transit- oriented development areas, by: <ul style="list-style-type: none"> ○ Ensuring new development that provides pedestrian connections in as many locations as possible to adjacent development, arterial streets, and thoroughfares. ○ Ensuring a balanced mix of housing, workplaces, shopping, recreational opportunities, and institutional uses, including mixed-use structures. ○ Locating schools in neighborhoods, within safe and easy walking distances of residences served. ○ Encouraging new development in which primary entrances are pedestrian entrances, with automobile entrances and parking located to the rear. ○ Supporting development where automobile access to buildings does not impede pedestrian access, by consolidating driveways between buildings or developing alley access. ○ Utilizing street parking as a buffer between sidewalk pedestrian traffic and the automobile portion of the roadway. ○ Prioritizing the physical development of pedestrian connectors for existing areas that do not meet established connectivity standards. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Mitigate climate change by decreasing heat gain from pavement and other hard surfaces associated with infrastructure. • Reduce heat gain from pavement and other similar hardscaping, by: <ul style="list-style-type: none"> ○ Including low-water landscaping in place of hardscaping around transportation infrastructure and in parking areas. ○ Establishing standards that provide for pervious pavement options. ○ Removing obstacles to natural, drought tolerant landscaping and low-water landscaping. • Coordinate with appropriate agencies to create an interconnected transportation system that allows a shift in travel from private passenger vehicles to alternative modes, including public transit, ride sharing, car-sharing, bicycling and walking, including, but not limited to: <ul style="list-style-type: none"> ○ Providing safe and convenient access for pedestrians and bicyclists to, across, and along major transit priority streets. • Upgrade and maintain the following transit system infrastructure to enhance public use, including: <ul style="list-style-type: none"> ○ Ensuring transit stops and bus lanes are safe, convenient, clean and efficient. ○ Ensuring transit stops have clearly marked street-level designation and are accessible. ○ Ensuring transit stops are safe, sheltered, benches are clean, and lighting is adequate. ○ Working with transit providers to place transit stations along transit corridors within mixed-use or 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>transit- oriented development areas at intervals appropriate for the mode of transit.</p> <ul style="list-style-type: none"> • Facilitate employment opportunities that minimize the need for private vehicle trips, by: <ul style="list-style-type: none"> ○ Amending zoning ordinances and the Development Code to include live/work sites and satellite work centers in appropriate locations. ○ Encouraging telecommuting options with new and existing employers, through project review and incentives, as appropriate. • Establish standards for new development and redevelopment projects to support bicycle use, including: <ul style="list-style-type: none"> ○ Amending the Development Code to include standards for pedestrian and bicyclist accommodations, including: <ul style="list-style-type: none"> ➤ Providing access for pedestrians and bicyclist to public transportation through construction of dedicated paths, where feasible. ○ Requiring new development and redevelopment projects to include bicycle facilities, as appropriate with the new land use, including: <ul style="list-style-type: none"> ➤ Where feasible, promote the construction of weatherproof bicycle facilities and at a minimum, provide bicycle racks or covered, secure parking near the building entrances. • Establish a network of multi-use trails to facilitate direct off- street bicycle and pedestrian travel and will provide bike racks along these trails at secure, lighted locations. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Establish policies and programs to reduce onsite parking demand and promote ride-sharing and public transit at large events. • Require new commercial and retail developments to provide prioritized parking for electric vehicles and vehicles using alternative fuels. • Support and promote the use of low-and zero-emission vehicles (NEV), by: <ul style="list-style-type: none"> ○ Encouraging the necessary infrastructure to facilitate the use of zero- emission vehicles and clean alternative fuels, such as electric vehicle charging facilities and conveniently located alternative fueling stations. ○ Encouraging new construction to include vehicle access to properly wired outdoor receptacles to accommodate ZEV and/or plug in electric hybrids (PHEV). ○ Encouraging transportation fleet standards to achieve the lowest emissions possible, using a mix of alternate fuels, PZEV or better fleet mixes. ○ Establishing incentives, as appropriate, to taxicab owners to use alternative fuel or gas-electric hybrid vehicles. • Establish green building requirements and standards for new development and redevelopment projects, and work to provide incentives for green building practices and remove barriers that impede their use. • Allow increased height limits and/or flexibility in other standards for projects that incorporate energy efficient green building practices where not prohibited by ALUCP/FAA. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Identify and remove regulatory or procedural barriers to implementing green building practices within its jurisdiction, such as updating codes, guidelines, and zoning, and ensure that all plan review and building inspection staff are trained in green building materials, practices, and techniques. • Support the use of green building practices by: <ul style="list-style-type: none"> ○ Establishing guidelines for green building practices in residential and commercial development. ○ Providing incentives, which may include reduction in development fees, administrative fees, and/or expedited permit processing for projects that use green building practices. • Adopt energy efficiency performance standards for buildings that achieve a greater reduction in energy and water use than otherwise required by current state law, including: <ul style="list-style-type: none"> ○ Standards for the installation of "cool roofs". ○ Standards for improved overall efficiency of lighting systems. ○ Requirements for the use of Energy Star appliances and fixtures in discretionary new development. ○ Requirements for new residential lots and/or structures to be arranged and oriented to maximize effective use of passive solar energy. • Require that affordable housing development incorporate energy efficient design and features to the maximum extent feasible. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Identify possible sites for production of renewable energy (such as solar, wind, small hydro, and biogas). • Identify and remove or otherwise address barriers to renewable energy production, including: <ul style="list-style-type: none"> ○ Reviewing and revising building and development codes, design guidelines, and zoning ordinances to remove renewable energy production barriers. ○ Working with related agencies, such as fire, water, health and others that may have policies or requirements that adversely impact the development or use of renewable energy technologies. ○ Developing protocols for safe storage of renewable and alternative energy products with the potential to leak, ignite or explode, such as biodiesel, hydrogen, and/or compressed air. • Allow renewable energy projects in areas zoned for open space, where consistent with the Land Use element, and other uses and values. • Promote and encourage renewable energy generation, and co-generation projects where feasible and appropriate. • Require that, where feasible, all new buildings be constructed to allow for easy, cost-effective installation of solar energy systems in the future, using such “solar-ready” features as: <ul style="list-style-type: none"> ○ Optimal roof orientation (between 20 to 55 degrees from the horizontal), with sufficient south-sloped roof surface, where such buildings architecture and construction are designed for sloped roofs. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Clear access without obstructions (chimneys, heating and plumbing vents, etc.) on the south sloped roof. ○ Roof framing that will support the addition of solar panels. ○ Installation of electrical conduit to accept solar electric system wiring. ○ Installation of plumbing to support a solar hot water system and provision of space for a solar hot water storage tank. • Require that any building constructed in whole or in part with City funds incorporate passive solar design features, such as daylighting and passive solar heating, where feasible. • Prepare and implement a comprehensive plan to improve energy efficiency of municipal facilities, including: <ul style="list-style-type: none"> ○ Conducting energy audits. ○ Retrofitting municipal facilities for energy efficiency where feasible and when remodeling or replacing components, including increased insulation, installing green or reflective roofs and low-emissive window glass. ○ Implementing an energy tracking and management system for its municipal facilities. ○ Installing energy-efficient exit signs, street signs, and traffic lighting, subject to life/safety considerations. ○ Installing energy-efficient lighting retrofits and occupancy sensors, and institute a "lights out at night" policy, subject to life/safety considerations. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Retrofitting heating and cooling systems to optimize efficiency (e.g., replace chillers, boilers, fans, pumps, belts, etc.). ○ Installing Energy Star® appliances and energy-efficient vending machines. ○ Improving water use efficiency, including a schedule to replace or retrofit system components with high- efficiency units (i.e., ultra-low-flow toilets, fixtures, etc.). ○ Installing irrigation control systems maximizing water use efficiency and minimizing off- peak use. ○ Adopting an accelerated replacement schedule for energy inefficient systems and components. ● Require that any newly constructed, purchased, or leased municipal space meet minimum standards, such as: <ul style="list-style-type: none"> ○ The Energy Star® New Homes Program established by U.S. EPA. ○ The incorporation of passive solar design features in new buildings, including daylighting and passive solar heating. ● Reduce per capita water consumption consistent with state law by 2020. ● Establish a water conservation plan that may include such policies and actions as: <ul style="list-style-type: none"> ○ Maintaining and refining the City's tiered rate structure for water use. ○ Establishing restrictions on time of use for landscape watering, or other demand management strategies. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Establishing performance standards for irrigation equipment and water fixtures, consistent with State Law. ● The City will establish programs and policies to increase the use of recycled water, including: <ul style="list-style-type: none"> ○ Promoting the use of recycled water for agricultural, industrial, and irrigation purposes, including grey water systems for residential irrigation. ● Ensure that building standards and permit approval processes promote and support water conservation, by: <ul style="list-style-type: none"> ○ Establishing building design guidelines and criteria to promote water-efficient building design, including minimizing the amount of non-roof impervious surfaces around the building(s). ○ Establishing menus and check-lists for developers and contractors to ensure water-efficient infrastructure and technology are used in new construction, including low- flow toilets and shower heads, moisture-sensing irrigation, and other such advances. ● Install water-efficient landscapes and irrigation, including: <ul style="list-style-type: none"> ○ Requiring planting drought-tolerant and native species and covering exposed dirt with moisture-retaining mulch or other materials such as decomposed granite. ○ Requiring the installation of water-efficient irrigation systems and devices, including advanced technology such as moisture-sensing irrigation controls. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Promote the planting of shade trees and establish shade tree guidelines and specifications, including: <ul style="list-style-type: none"> ○ Establishing guidelines for tree planting based on the land use (residential, commercial, parking lots, etc.). ○ Establishing guidelines for tree types based on species size, branching patterns, whether deciduous or evergreen, whether roots are invasive, etc. ○ Establishing tree guidelines for placement, including distance from structures, density of planting, and orientation relative to structures and the sun. • Develop an Urban Forestry Program to consolidate policies and ordinances regarding tree planting, maintenance, and removal, including: <ul style="list-style-type: none"> ○ Establishing guidelines for tree planting, including criteria for selecting deciduous or evergreen trees low-VOC- producing trees, and emphasizing the use of drought- tolerant native trees and vegetation. 				
6-4 Measures listed in Mitigation Measure 6-2 and 6-3 shall be considered by the City while reviewing all new development, as appropriate, between the time of adoption of The Ontario Plan and adoption of the Climate Action Plan (CAP).	City of Ontario Planning Department	Prior to adoption of the Climate Action Plan	City of Ontario Planning Department	
6-5 Pursuant to a goal of overall consistency with the Sustainable Communities Strategies, the City of Ontario shall evaluate new development for consistency with the development pattern set forth in the Sustainable Communities Strategies plan, upon adoption of the plan by the Southern California Association of Governments.	City of Ontario Planning Department	Prior to individual project approvals	City of Ontario Planning Department	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
6-6 The City of Ontario shall participate in the County of San Bernardino's Green Valley Initiative.	City of Ontario Planning Department	Prior to individual project approvals	City of Ontario Planning Department	
5.12 NOISE				
12-1 Prior to the issuance of building permits for any project that involves a noise-sensitive use within the 65 dBA CNEL contour along major roadways, freeways, railroads, or the Los Angeles/Ontario International Airport, the project property owner/developers shall retain an acoustical engineer to conduct an acoustic analysis and identify, where appropriate, site design features (e.g., setbacks, berms, or sound walls) and/or required building acoustical improvements (e.g., sound transmission class rated windows, doors, and attic baffling), to ensure compliance with the City's Noise Compatibility Criteria and the California State Building Code and California Noise Insulation Standards (Title 24 and 21 of the California Code of Regulations).	City of Ontario Planning/Building Department in coordination with the Landowner/Project Applicant	Prior to individual project approvals	City of Ontario Planning/Building Department	
12-2 Individual projects that involve vibration-intensive construction activities, such as pile drivers, jack hammers, and vibratory rollers, occurring near sensitive receptors shall be evaluated for potential vibration impacts. If construction-related vibration is determined to be perceptible at vibration-sensitive uses (i.e., exceed the Federal Transit Administration vibration-annoyance criteria of 78 VdB during the daytime), additional requirements, such as use of less vibration intensive equipment or construction techniques, shall be implemented during construction (e.g., drilled piles to eliminate use of vibration-intensive pile driver).	City of Ontario Planning/Building/MUA Department in coordination with the Landowner/Project Applicant's construction contractor	During construction	City of Ontario Building/MUA Department	
12-3 Prior to the issuance of building permits for any project that involves a vibration-sensitive use directly adjacent to the Union Pacific Railroad or Southern California Regional Rail Authority main lines shall retain an acoustical engineer to evaluate potential for trains to create perceptible levels of	City of Ontario Planning/Building Department with collaboration with the Landowner/Project	Prior to individual project approvals	City of Ontario Building Department	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
vibration indoors. If vibration- related impacts are found, mitigation measures, such as use of concrete, iron, or steel, or masonry materials to ensure that levels of vibration amplification are within acceptable limits to building occupants, shall be implemented. Pursuant to the Federal Transit Administration vibration-annoyance criteria, these acceptable limits are 78 VdB during the daytime and 72 VdB during the nighttime for residential uses, 84 VdB for office uses, and 90 VdB for workshops.	Applicant			
12-4 Construction activities associated with new development that occurs near sensitive receptors shall be evaluated for potential noise impacts. Mitigation measures such as installation of temporary sound barriers for adjacent construction activities that occur adjacent to occupied noise-sensitive structures, equipping construction equipment with mufflers, and reducing non-essential idling of construction equipment to no more than five minutes shall be incorporated into the construction operations to reduce construction-related noise to the extent feasible.	City of Ontario Building/Planning/MUA Department in coordination with the Landowner/Project Applicant's construction contractor	During construction	City of Ontario Building/Planning/MUA Department	
5.16 TRANSPORTATION AND CIRCULATION				
16-1 The Mobility Element of the Ontario Plan shall be consistent with the traffic study prepared by Kimley-Horn and Associates in 2009. Table 5.16-6 in Section 5.16, <i>Transportation and Traffic</i> , shows the recommended lane geometry for the Proposed Land Use Plan.	City of Ontario Engineering/Planning Department	Ongoing	City of Ontario Engineering/Planning Department	
5.17 UTILITIES AND SERVICE SYSTEMS				
17-1 The City shall include a policy in the Policy Plan that requires water conservation measures for development projects to improve water use efficiency and reduce overall water demand. Reduce potable water demand, through conservation measures, including but not limited to:	City of Ontario Planning/MUA Department	Ongoing	City of Ontario Planning/MUA/Engineering Department	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Work cooperatively with all developers to incorporate conservation measures into project designs (such as those recommended by the California Urban Water Conservation Council). • Continue to develop and implement drought contingency plans to assist citizens and businesses reduce water use during water shortages and emergencies. • Revise the City Code to include a Water-Efficient Landscape Ordinance to encourage or, as appropriate, require the use of water-efficient landscaping consistent with AB 1881. 				
<p>17-2 The City shall include a policy in the Policy Plan that maximizes the use of recycled water as an irrigation (nonpotable) source for landscaping, parks, and other irrigation opportunities in all areas of the City and requires use of recycled water in dual-system office and industrial uses in selected urban areas of the City, where available and feasible.</p>	<p>City of Ontario Planning Department in coordination with City of Ontario MUA/Engineering Department</p>	Ongoing	<p>City of Ontario Planning/MUA/Engineering Department</p>	
<p>17-3 The City shall include a policy in the Policy Plan that the City participate through the Chino Basin Water Master and the Inland Empire Utilities Agency in regional efforts to develop finding additional sources of water for groundwater recharge, such as capture of stormwater runoff, recycled water, or other sources to ensure that the Chino Basin stays in long-term hydraulic balance and sustainability and that adequate additional local water sources would be available to increase the flexibility of the City's water supply.</p>	<p>City of Ontario Planning Department in coordination with City of Ontario MUA/Engineering Department</p>	Ongoing	<p>City of Ontario Planning/MUA/Engineering Department</p>	

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PDEV20-005, A DEVELOPMENT PLAN TO CONSTRUCT A 256,711 SQUARE FOOT INDUSTRIAL BUILDING ON 11.3 ACRES LOCATED AT 875 WEST STATE STREET, WITHIN THE IL (LIGHT INDUSTRIAL) ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF—APNS: 1011-161-04 & 1011-161-05.

WHEREAS, Inland Harbor LLC ("Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV20-005, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 11.3 acres of land generally located south of State Street, mid-block between Palmetto and Cypress Avenues, at 875 West State Street, within the IL (Light Industrial) zoning district; and

WHEREAS, the property to the north of the Project site is within the RC (Rail Corridor) zoning district and is developed with the Union Pacific Railroad. The property to the east is within the IL (Light Industrial) zoning district and is developed with industrial building. The properties to the south are within the IL (Light Industrial) and the MHP (Mobile Home Park) zoning districts and is developed with one industrial building and the Grove Manor Mobile Home Park. The property to the west is within the IL (Light Industrial) zoning district, and is developed with industrial buildings; and

WHEREAS, on March 4, 2020, the applicant submitted a Development Plan (File No. PDEV20-005) to construct a 256,711 square foot industrial warehouse building on 11.3 acres, located south of State Street, between Palmetto and Cypress Avenues; and

WHEREAS, the proposed 256,711 square foot warehouse/distribution building is centrally located on the property oriented north to south in a cross-dock configuration. The building includes two office areas fronting State Street, totaling 13,910 square feet located at the northwest and northeast corners of the building; and

WHEREAS, the Project site will have three access points along State Street for vehicular and tractor-trailer access. The 35-foot wide driveways located at the northwest and northeast corners of the Project site provide direct truck access to the secured yard areas. The 26-foot wide driveway located mid-point along the street frontage provides automobile access to the site's employee and visitor parking areas; and

WHEREAS, the Project has provided 139 off-street parking spaces, consistent with the "Warehouse and Distribution" parking standards specified in the Development Code. Additionally, the City's off-street parking and loading standards require that the Project provide a minimum of one tractor trailer parking space for each four dock-high loading

spaces. Fifteen tractor trailer parking spaces have been provided, exceeding the minimum requirement for the Project; and

WHEREAS, the proposed building will be composed of concrete tilt-up walls that incorporate a contemporary architectural design. The proposed building design incorporates articulation in the building footprint, incorporating a combination of recessed and popped-out wall areas, and articulation in the building parapet/roof line, which serves to accentuate the building's entries and breaks up large expanses of building wall; and

WHEREAS, the Project proposes a 10 percent landscape coverage, meeting the minimum requirement for interior lots in the IL (Light Industrial) zoning district; and

WHEREAS, Public utilities (water and sewer) are available to serve the project. Additionally, the Applicant has submitted a Preliminary Water Quality Management Plan ("PWQMP"), which establishes the project's compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development ("LID") best management practices ("BMPs"), such as retention and infiltration, biotreatment, and evapotranspiration. The PWQMP proposes a vegetated swale(s) designed to accept runoff from building roofs, parking lots and project roadways, which lead to an underground stormwater infiltration system, which is located along the southern portion of the Project site. Any overflow drainage will be conveyed to the surrounding street; and

WHEREAS, the Project is located less than 1,000 feet away from a sensitive land use and therefore the Applicant was required to submit, a Health Risk Analysis (HRA) report in conjunction with the subject Application. The HRA report (Vista Environmental, November 30, 2020) was prepared for the Project determined that the Project will not have significant health impacts on the surrounding environment; and

WHEREAS, on March 11, 2021, the Planning Department held a community meeting, via Zoom, to inform the surrounding property owners/residents of the proposed Project, and to address any questions or concerns they may have regarding the Project. In addition to the Applicant's attendees, one member of the public attended the Zoom meeting and no concerns with the project were expressed; and

WHEREAS, The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was certified on January 27, 2010 (hereinafter referred to as "Certified EIR"), in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as "EIR Addendum") in accordance with the requirements of

the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as "CEQA"); and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an EIR Addendum has been prepared to determine possible environmental impacts; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan ("ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on March 15, 2021, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision DAB21-006, recommending the Planning Commission approve the Application; and

WHEREAS, on March 23, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and continued the Project to the April 27, 2021 hearing date; and

WHEREAS, on April 27, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and continued the Project to the May 25, 2021 hearing date; and

WHEREAS, on May 25, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the decision-making body for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The administrative record has been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(2) The environmental impacts of this project were previously reviewed in conjunction with The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), which was certified by the City Council on January 27, 2010 ("Certified EIR"); and

(3) The previous Certified EIR contains a complete and accurate reporting of the environmental impacts associated with the Project; and

(4) The previous Certified EIR was completed in compliance with CEQA and the Guidelines promulgated thereunder, and the City of Ontario Local CEQA Guidelines; and

(5) The previous Certified EIR reflects the independent judgment of the Planning Commission; and

(6) The proposed project will introduce no new significant environmental impacts beyond those previously analyzed in the previous Certified EIR, and all mitigation measures previously adopted with the Certified EIR, are incorporated herein by this reference.

SECTION 2: Subsequent or Supplemental Environmental Review Not Required. Based on the information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

(d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: Housing Element Compliance. Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at

the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 4: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan (“ALUCP”), establishing the Airport Influence Area for Ontario International Airport (“ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making body for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the Planning Commission, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 5: *Concluding Facts and Reasons.* Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing and upon the specific findings set forth in Sections 1 through 4, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Project is located within the Industrial land use district of the Policy Plan Land Use Map, and the IL (Light Industrial) zoning district. The development standards and conditions under which the proposed Project will be constructed and maintained is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (general plan), and City Council Priorities components of The Ontario Plan; and

(2) ***The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in***

which the site is located. The Project has been designed consistent with the requirements of the City of Ontario Development Code and the IL (Light Industrial) zoning district, as-well-as building intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions; and

(3) ***The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed project.*** The Development Advisory Board has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: [i] the purposes of the IL (Light Industrial) zoning district, which is the zoning district in which the Project is located, are maintained; [ii] the Project will not endanger the public health, safety or general welfare; [iii] the Project will not result in any significant environmental impacts; [iv] the Project will be in harmony with the area in which it is located; and [v] the Project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan. Furthermore, the proposed Project will replace the current metal container storage yard business with a single story industrial/warehouse building, which will compliment and improve the surrounding neighborhood; and

(4) ***The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development.*** The proposed Project has been reviewed for consistency with the general development standards and guidelines of the Development Code that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking and loading spaces, parking lot dimensions, design and landscaping, bicycle parking, on-site landscaping, and fences and walls, as-well-as those development standards and guidelines specifically related to the IL (Light Industrial) zoning district. As a result of this review, the Development Advisory Board has determined that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the development standards and guidelines described in the Development Code.

SECTION 6: Planning Commission Action. Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as “Attachment A,” and incorporated herein by this reference.

SECTION 7: Indemnification. The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to

attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 8: Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East “B” Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 9: Certification to Adoption. The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of May 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary of Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 25, 2021 by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**File No. PDEV20-005
Departmental Conditions of Approval**

(Departmental conditions of approval to follow this page)



City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

Planning Department Land Development Division Conditions of Approval

Meeting Date: May 25, 2021

File No: PDEV20-005

Related Files: None

Project Description: A Development Plan to construct a 256,711 square foot industrial building on 11.3 acres of land located at 875 West State Street, within the IL (Light Industrial) zoning district. (APNs: 1011-161-04 and 1011-161-05); **submitted by Inland Harbor LLC.**

Prepared By: Denny D. Chen, Associate Planner
Phone: 909.395.2424 (direct)
Email: dchen@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits.

(a) Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

2.2 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.3 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b) Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(c) Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

(d) Any dead or missing landscaping shall be replaced and a sprinkler system shall be installed that is set to a timer.

(e) Outside plaza areas, in front of the office pods, shall feature decorative color paving.

2.4 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

2.5 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b) All drive approaches shall be provided with an enhanced color pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.

(c) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(d) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

(e) Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).

(f) Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11).

(g) A minimum of 138 on-site parking spaces and 12 truck trailer parking spaces shall be provided.

Outdoor Loading and Storage Areas.

(h) Loading facilities shall be designed and constructed pursuant to Development Code Division 6.03 (Off-Street Parking and Loading).

(i) Areas designated for off-street parking, loading, and vehicular circulation and maneuvering, shall not be used for the outdoor storage of materials or equipment.

(j) Outdoor loading and storage areas, and loading doors, shall be screened from public view pursuant to the requirements of Development Code Paragraph 6.02.025.A.2 (Screening of Outdoor Loading and Storage Areas, and Loading Doors) Et Seq.

(k) Outdoor loading and storage areas shall be provided with gates that are view-obstructing by one of the following methods:

(i) Construct gates with a perforated metal sheet affixed to the inside of the gate surface (50 percent screen); or

(ii) Construct gates with minimum one-inch square tube steel pickets spaced at maximum 2-inches apart.

(l) The minimum gate height for screen wall openings shall be established based upon the corresponding wall height, as follows:

Screen Wall Height	Minimum Gate Height
14 feet:	10 feet
12 feet:	9 feet
10 feet:	8 feet
8 feet:	8 feet
6 feet:	6 feet

2.6 Site Lighting.

(a) All off-street parking facilities shall be provided with nighttime security lighting pursuant to Ontario Municipal Code Section 4-11.08 (Special Residential Building Provisions) and Section 4-11.09 (Special Commercial/Industrial Building Provisions), designed to confine emitted light to the parking areas. Parking facilities shall be lighted from sunset until sunrise, daily, and shall be operated by a photocell switch.

(b) Unless intended as part of a master lighting program, no operation, activity, or lighting fixture shall create illumination on any adjacent property.

(c) Wall packs will not be allowed within the public view areas. All fixtures shall be decorative. Cut sheets shall be submitted to Planning during plan check review.

(d) Decorative light fixtures shall be used on the building to match the proposed architectural style.

2.7 Mechanical and Rooftop Equipment.

(a) All exterior roof-mounted mechanical, heating and air conditioning equipment, and all appurtenances thereto, shall be completely screened from public view by parapet walls or roof screens that are architecturally treated so as to be consistent with the building architecture.

(b) All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.

2.8 Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

2.9 Signs. All Project signage shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations).

2.10 Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

2.11 Environmental Review.

(a) The environmental impacts of this project were previously reviewed in conjunction with an EIR for which The Ontario Plan (TOP) Environmental Impact Report (State Clearinghouse No. 2008101140), which was certified by City Council on January 27, 2010 ("Certified EIR"); and

(b) The previous Certified EIR contains a complete and accurate reporting of the environmental impacts associated with the Project; and

(c) The previous Certified EIR was completed in compliance with CEQA and the Guidelines promulgated thereunder, and the City of Ontario Local CEQA Guidelines; and

(d) The previous Certified EIR reflects the independent judgment of the Planning Commission; and

(e) The proposed project will introduce no new significant environmental impacts beyond those previously analyzed in the previous Certified EIR, and all mitigation measures previously adopted with the Certified EIR, are incorporated herein by this reference.

(f) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(g) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

2.12 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.13 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County "Clerk of the Board of Supervisors," along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

2.14 Additional Requirements.

(a) The proposed metal canopies over both office entrances (North Elevation) shall project a minimum of 5-feet from the building face. Applicant shall work with staff during the plan check process, to finalize the design, color, and projection of the metal canopies.

(b) A minimum 8-foot tall decorative wall (concrete tilt-up or decorative masonry block) with view-obstructing gates shall be constructed along the north side of the truck loading area to fully screen all roll-up doors, trucks and trailers from public view. Applicant shall work with staff during the plan check process to finalize the screen wall and gate designs.

(c) The proposed 8-foot tall, concrete tilt-up screen wall shall be constructed along the project's east, south, and west sides. The minimum height for the proposed screen wall shall be 8 feet. The maximum height of fences and walls, within the project's front setback, shall be 6 feet and shall be non-view obstructing type of fencing, such as wrought iron or tubular steel type fencing. Installation of chain link fences around the perimeter of the project site shall be prohibited. All steel fencing shall be powder coated to prevent rust.

(d) Building roof projections must also have returns of a minimum length of 6-feet, to avoid a false front/unfinished appearance. Applicant shall work with staff during plan check process to finalize the minimum length of the roof returns.

(e) Truck trailers shall utilize only the designated area the dock-high loading area for parking. Truck trailers shall not be parked within areas designated for automobile parking or within truck maneuvering areas.

(f) The building's maximum office area, including any future mezzanine area and/or office addition, shall not exceed 25,671 square feet or 10 percent of the building's GFA (Gross Floor Area). If office area exceeds 10 percent of the building's GFA, additional parking shall be provided for the area (office square footage) exceeding 25,671 square feet.

(g) Decorative paving shall be provided at both employee break areas.

(h) Both employee break areas, located on the East and West sides of the building, shall feature decorative trellis posts. Posts shall be wrapped with a decorative stone base and shall be painted to match the building. Applicant shall work with staff, during plan check process, to finalize trellis post color, materials, and design.

(i) The Ontario Climate Action Plan (CAP) requires new development to be 25% more efficient. The applicant has elected to utilize the Screening Tables provided in the CAP instead of preparing separate emissions calculations. By electing to utilize the Screening Tables the applicant shall

be required to garner a minimum of 100 points to be consistent with the reduction quantities outlined in the CAP. The applicant shall identify on the construction drawings the items identified in the Screening Tables.



CITY OF ONTARIO MEMORANDUM



TO: Denny Chen
FROM: Kimberly Ruddins
DATE: 01/15/2021
SUBJECT: File No. PDEV20-005

PROJECT COMMENTS AND RECOMMENDATIONS:

Healthy Ontario has reviewed the proposed Development Plan to construct a new 257,921 sf warehouse building on 11.24 acres within the light industrial (IL) zoning district, located at 835 West State Street, (APNs: 1011-161-05 and 1011-161-04). We have the following comments and recommendations to meet the requirements of SB-1000 (Safety and Environmental Justice):

Projects located less than 1000 ft. away from sensitive land uses shall require a Health Risk Assessment. This project has sensitive uses within 550 ft. of the project area.

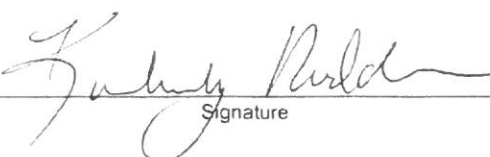
Applicant HRA dated November 30, 2020 - Complete

In order to comply with the 'Civic Engagement' requirement and promote equitable, inclusive and meaningful community engagement, we recommend:

- Notifying existing and potential tenants, occupants, and residents within 500ft from project site.
- Consider hosting a neighborhood meeting.
- Notifications should be in a bilingual format.

The Light Industrial Zoning District should accommodate lighter manufacturing and assembly activities, and storage and warehousing activities. Using CARB's air quality and Land Use handbook as a tool, it was mentioned that cumulative air pollution can occur with activities such as truck idling, traffic congestion and warehouse facilities.

- Per CARB's recommendation, distribution centers located less than 1,000 feet from sensitive land uses should not exceed more than 100 trucks per day and more than 40 trucks with operating transportation refrigeration units (TRU's) per day or where TRU unit operations exceed 300 hours per week.


Department _____ Signature _____ Title *Sustainability PM* Date *1/15/2021*



**ENGINEERING DEPARTMENT
CONDITIONS OF APPROVAL**

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Broadband Operations & Investment and Revenue Resources Department Conditions incorporated)

<input checked="" type="checkbox"/> DEVELOPMENT PLAN <input type="checkbox"/> OTHER	<input type="checkbox"/> PARCEL MAP <input type="checkbox"/> TRACT MAP <input type="checkbox"/> FOR CONDOMINIUM PURPOSES
PROJECT FILE NO. <u>PDEV20-005</u> RELATED FILE NO(S). _____	
<input checked="" type="checkbox"/> ORIGINAL <input type="checkbox"/> REVISED: <u> </u> / <u> </u> / <u> </u>	

CITY PROJECT ENGINEER & PHONE NO: Eric Woosley, P.E. (909) 395-2134 *EW*

CITY PROJECT PLANNER & PHONE NO: Denny Chen (909) 395-2424

DAB MEETING DATE: March 15, 2021

PROJECT NAME / DESCRIPTION: A Development Plan to construct a 256,711-square foot industrial building on 11.3 acres of land.

LOCATION: 835 West State Street

APPLICANT: Inland Harbor, LLC

REVIEWED BY: *Raymond Lee* 2/23/21
 Raymond Lee, P.E. Date
 Assistant City Engineer

APPROVED BY: *Khol Do* 2-26-21
 Khol Do, P.E. Date
 City Engineer



THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.

1. PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, APPLICANT SHALL: Check When Complete

- 1.01 Dedicate to the City of Ontario, the right-of-way, described below:
 _____ feet on _____
 Property line corner 'cut-back' required at the intersection of _____
 and _____
- 1.02 Dedicate to the City of Ontario, the following easement(s): _____

- 1.03 Restrict vehicular access to the site as follows: _____
- 1.04 Vacate the following street(s) and/or easement(s): _____
- 1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.
- 1.06 Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.
- 1.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.
- 1.08 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Financial Services Department at (909) 395-2124 regarding this requirement.
 (1) _____
 (2) _____
- 1.09 Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.



- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ci.ontario.ca.us) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.
- 1.13 New Model Colony (NMC) Developments:
 - 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
 - 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
 - 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions: _____

2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:

**A. GENERAL
 (Permits Includes Grading, Building, Demolition and Encroachment)**

- 2.01 Record Parcel Map/Tract Map No. _____ pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 Submit a PDF of the recorded map to the City Engineer's office.
- 2.03 **Note that the subject parcel is a recognized parcel in the City of Ontario per Monte Vista Tract No. 2, Map Book 16, Page 33.**
- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of _____.
- 2.05 **Apply for a: Certificate of Compliance with a Record of Survey; Lot Line Adjustment – to merge lots, APN:1011-161-04 and APN: 1011-161-05 into one lot.**
- 2.06 Make a Dedication of Easement.
 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.



- 2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.
- 2.08 **Submit a soils/geology report.**
- 2.09 Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:
 - State of California Department of Transportation (Caltrans)
 - San Bernardino County Road Department (SBCRD)
 - San Bernardino County Flood Control District (SBCFCD)
 - Federal Emergency Management Agency (FEMA)
 - Cucamonga Valley Water District (CVWD) for sewer/water service
 - United States Army Corps of Engineers (USACE)
 - California Department of Fish & Game
 - Inland Empire Utilities Agency (IEUA)
 - Other: _____
- 2.10 **Dedicate to the City of Ontario the right-of-way described below:**

3-feet on State Street to make an ultimate half street width of 33-ft from centerline to right-of-way.

Property line corner 'cut-back' required at the intersection of _____
 and _____.
- 2.11 Dedicate to the City of Ontario the following easement(s): _____
- 2.12 New Model Colony (NMC) Developments:
 - 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.
 - 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.
 - 3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall).
- 2.13 **Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at 100% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.**



- 2.14 The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.
- 2.15 **Pay all Development Impact Fees (DIF) to the Building Department. Storm Drain Development Impact Fee, approximately \$258,436, shall be paid to the Building Department. Final fee shall be determined based on the approved site plan.**
- 2.16 **Other conditions: The applicant/developer shall obtain a private easement with the adjacent property owner located at APN:1011-161-01 for storm water overflow drainage purposes.**



B. PUBLIC IMPROVEMENTS

(See attached Exhibit 'A' for plan check submittal requirements.)

- 2.17 Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):

Improvement	State Street	Street 2	Street 3	Street 4
Curb and Gutter	<input checked="" type="checkbox"/> New; 20-ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
AC Pavement	<input checked="" type="checkbox"/> Replacement <input checked="" type="checkbox"/> Widen 6 additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Drive Approaches	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Sidewalk	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
ADA Access Ramp	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Parkway	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
Raised Landscaped Median	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Fire Hydrants	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation



Sewer (see Sec. 2.C)	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Water (see Sec. 2.D)	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input checked="" type="checkbox"/> New <input checked="" type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Street Light (see Sec. 2.F)	<input checked="" type="checkbox"/> New / Upgrade <input checked="" type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2.G)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Fiber Optics (see Sec. 2.K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
Removal of Improvements	_____	_____	_____	_____
Other Improvements	_____	_____	_____	_____

Specific notes for improvements listed in item no. 2.17, above:

The applicant/developer shall be responsible to widen State Street and construct new full asphalt concrete pavement section along the entire project frontage from street centerline to the gutter line, plus a minimum of 5 feet grind and overlay beyond the street centerline per City Standards and of the satisfaction of the City Engineer.

- 2.18 Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s): State Street, 5 feet beyond centerline.



- 2.19 **Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing Number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along the property frontage, from street centerline to curb/gutter.**
- 2.20 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide water service sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.
- 2.21 Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately _____, for undergrounding of utilities in accordance with Section 7-7.303.e of the City's Municipal Code.
- 2.22 Other conditions: _____

C. SEWER

- 2.23 A _____ inch sewer main is available for connection by this project in _____ (Ref: Sewer plan bar code: _____)
- 2.24 **Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately 344 feet west at the intersection of Palmetto Avenue and State Street. The main is to be installed from the point of connection to the easterly property limits.**
- 2.25 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.
- 2.26 **Other conditions:**
 - 1. **Construct a monitoring manhole upstream of the proposed sewer lateral. The manhole to be located at the back of the property line and on private property.**
 - 2. **The Occupant/Applicant shall apply for a Wastewater Discharge Permit for their Establishment, and shall comply will all the requirements of the Wastewater Discharge Permit (https://www.ontarioca.gov/sites/default/files/Ontario-Files/Municipal-Utilities-Company/industrial_wastewater_permit_application.pdf). Requirements of the Wastewater Discharge Permit may include, but not limited to: Installation of wastewater pretreatment equipment, such as clarifiers.**
 - 3. **As part of the precise grading plans submittal, provide a Final Utilities Systems Map that shows all existing and proposed Utilities (Potable Water, Recycled Water, Sewer, Storm Drain, and other utilities) including each of the City's public utilities' points of connection to the existing systems.**

D. WATER

- 2.27 **A 12-inch water main is available for connection by this project in State Street (Ref: Water plan bar code: W11936)**
- 2.28 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.29 **Other conditions:**
 - 1. **Provide a separate domestic water service with back flow device per city's standard.**
 - 2. **Provide a separate irrigation service with back flow device per city's standard.**
 - 3. **This building is to have two fire services and looping them onsite.**
 - 4. **Provide at minimum 10' wall to wall separation for all water services and sewer laterals.**



5. Construct two fire hydrants fronting State Street.

E. RECYCLED WATER

- 2.30 **No recycled water main is available for connection by this project.** (Ref: Recycled Water plan bar code: N/A)
 - 2.31 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.
 - 2.32 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.
 - 2.33 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.
- Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.
- 2.34 Other conditions: _____

F. TRAFFIC / TRANSPORTATION

- 2.35 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer:
 - 1. On-site and off-site circulation
 - 2. Traffic level of service (LOS) at 'build-out' and future years
 - 3. Impact at specific intersections as selected by the City Engineer
- 2.36 New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.
- 2.37 **Other conditions:**
 - 1. **The applicant/developer shall design and construct in-fill public streetlights along the frontage of State Street. Street lighting shall be LED-type in accordance with City's Traffic and Transportation Design Guidelines.**
 - a. **Applicant/developer's Engineer shall meet the City Engineering staff prior to the starting street lighting design plans.**
 - 2. **The applicant/developer shall construct proposed driveways in accordance with City of Ontario Standard Drawing No. 1204 for Commercial Driveways.**
 - 3. **Property frontage along State Street shall be signed "No Parking Anytime".**

G. DRAINAGE / HYDROLOGY

- 2.38 **No storm drain main is available to accept flows from this project.** (Ref: Storm Drain plan bar code: N/A)
- 2.39 **Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.**



- 2.40 An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100 year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.
- 2.41 Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.
- 2.42 Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.
- 2.43 Other conditions: _____

H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)

- 2.44 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels.
 If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.
 Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.
- 2.45 Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: <http://www.sbcounty.gov/dpw/land/npdes.asp>.
- 2.46 Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.
- 2.47 Other conditions: _____

J. SPECIAL DISTRICTS

- 2.48 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.



2.49 Other conditions: _____

K. FIBER OPTIC

- 2.50 Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole. Generally located along the property frontage of State Street.
- 2.51 Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Broadband Operations Department at (909) 395-2000, regarding this requirement.

L. Solid Waste

- 2.52 Onsite solid waste shall be designed in accordance with the City's Solid Waste Manual located at:
<https://www.ontarioca.gov/OMUC/IntegratedWaste>
- 2.53 Other conditions:
 - 1. Proposed Trash Enclosure (TE): TE to have a solid roof.
 - 2. Final Solid Waste Handling Plan (SWHP): Prior to approval of any building permits, submit a final SWHP with the Precise Grading Plan for review and approval of Ontario Municipal Utilities Company.

3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:

- 3.01 Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.
- 3.02 Complete all requirements for recycled water usage.
 - 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.
 - 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.
 - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.
- 3.03 The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.
- 3.04 NMC Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.
- 3.05 Confirm payment of all Development Impact Fees (DIF) to the Building Department.



- 3.06 Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).

4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:

- 4.01 Complete all Conditions of Approval listed under Sections 1-3 above.
- 4.02 Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.
- 4.03 The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.
- 4.04 Submit record drawings (PDF) for all public improvements identified within Section 2 of these Conditions of Approval.



EXHIBIT 'A'

**ENGINEERING DEPARTMENT
First Plan Check Submittal Checklist**

Project Number: PDEV20-005, and/or Parcel Map/Tract Map No. _____

The following items are required to be included with the first plan check submittal:

1. **A copy of this check list**
2. **Payment of fee for Plan Checking**
3. **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4. **One (1) copy of project Conditions of Approval**
5. **Include a PDF (electronic submittal) of each required improvement plan at every submittal.**
6. **Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).**
7. **Three (3) sets of Public Street Improvement plan with street cross-sections**
8. **Three (3) sets of Private Street improvement plan with street cross-sections**
9. **Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)**
10. **Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)**
11. **Four (4) sets of Public Sewer improvement plan**
12. **Five (5) sets of Public Storm Drain improvement plan**
13. **Three (3) sets of Public Street Light improvement plan**
14. **Three (3) sets of Signing and Striping improvement plan**
15. **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
16. **Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)**
17. **Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.**
18. **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**
19. **One (1) copy of Hydrology/Drainage study**
20. **One (1) copy of Soils/Geology report**



- 21. Payment for Final Map/Parcel Map processing fee
- 22. Three (3) copies of Final Map/Parcel Map
- 23. One (1) copy of approved Tentative Map
- 24. **One (1) copy of Preliminary Title Report (current within 30 days)**
- 25. One (1) copy of Traverse Closure Calculations
- 26. **One (1) set of supporting documents and maps (legible copies): referenced Improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.**
- 27. Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
- 28. Other: _____



CITY OF ONTARIO MEMORANDUM



DATE: February 22, 2021
TO: Eric Woosley, Engineering
CC: Denny Chen, Planning
FROM: Peter Tran, Utilities Engineering
SUBJECT: DPR #3 – Conditions of Approval (COA) - Utilities Comments (#7328)
PROJECT NO.: PDEV20-005 (A Development Plan to construct one industrial building)

BRIEF DESCRIPTION

A Development Plan to construct one (1) warehouse building totaling 257,920 square feet on 11.24 acres of land located at 835 W. State Street within the IL zoning district (APNs: 1011-161-05 and 1011-161-04).

THIS SUBMITTAL IS COMPLETE AND RECOMMENDED FOR APPROVAL.

CONDITIONS OF APPROVAL: *The Ontario Municipal Utilities Company (OMUC) recommends this application for approval subject to the conditions outlined below and compliance with the City's Design Development Guidelines, Specifications Design Criteria, and City Standards.*

General Conditions:

1. **Standard Conditions of Approval:** Project shall comply with the requirements as set forth in the Amendment to the Standard Conditions of Approval for New Development Projects adopted by the City Council (Resolution No. 2017-027) on April 18, 2017; as well as project-specific conditions/requirements as outlined below:
2. **Final Utilities Systems Map (USM):** As part of the precise grading plans submittal, provide a Final Utilities Systems Map that shows all existing and proposed Utilities (Potable Water, Recycled Water, Sewer, Storm Drain, and other utilities) including each of the City's public utilities' points of connection to the existing systems.

Potable Water Conditions:

3. **Domestic Water Service:** Provide a separate domestic water service with back flow device per city's standard, cannot be shared with irrigation service.
4. **Irrigation Service:** Provide a separate irrigation service with back flow device per city's standard, cannot be shared with the domestic water service.
5. **Fire Service:** This building is to have two fire services and looping them onsite.
6. **Horizontal Separation:** Provide at minimum 10' separation for all water services away from sewer lateral, wall to wall to meet with Division Drinking Water Department.
7. **Fire Hydrants:** Construct two fire hydrants fronting State Street.

Sanitary Sewer Comments:

8. **State Street Sewer Main:** Construct an 8" public sewer main by extending the existing sewer main from the west at the intersection of Palmetto and State across the project site, to the easterly property line. Also, submit plans for design along with the precise grading plans at the time of submittal.
9. **Monitoring Manhole:** Construct a monitoring manhole upstream of the proposed sewer lateral. The manhole to be located at the back of the property line and on private property.
10. Note the following:

The Occupant/Applicant shall apply for a Wastewater Discharge Permit for their Establishment, and shall comply will all the requirements of the Wastewater Discharge Permit (https://www.ontarioca.gov/sites/default/files/Ontario-Files/Municipal-Utilities-Company/industrial_wastewater_permit_application.pdf).

Requirements of the Wastewater Discharge Permit may include, but not limited to: Installation of wastewater pretreatment equipment, such as clarifiers. For wastewater permit application questions, please contact:

Michael Birmelin, Environmental Programs Manager

omucenvironmental@ontarioca.gov

Phone: (909) 395-2661

Solid Waste Comments:

11. Proposed Trash Enclosure (TE): TE to have a solid roof, it is appeared to be lattice per the proposed shown on plans, specifically on sheet DAB-A4.1
12. Final Solid Waste Handling Plan (SWHP): Prior to approval of any building permits, submit a final SWHP with the Precise Grading Plan for review and approval of Ontario Municipal Utilities Company.



CITY OF ONTARIO
MEMORANDUM

TO: Scott Murphy, Community Development Director
Rudy Zeledon, Planning Director (Copy of memo only)
Diane Ayala, Advanced Planning Division (Copy of memo only)
Charity Hernandez, Economic Development
James Caro, Building Official
Khoi Do, City Engineer
Jamie Richardson, Landscape Planning Division *Philip M.*
Ahmed Aly, Municipal Utility Company
Gabriel Gutierrez, Police Department
Mike Gerken, Deputy Fire Chief/Fire Marshal
Jay Bautista, T. E., Traffic/Transportation Manager
Lorena Mejia, Airport Planning
Eric Woosley, Engineering/NPDES
Robin Lucero, Code Enforcement (Copy of memo only)
Jimmy Chang, IT Department

REVISION NO. 2

FROM: Denny Chen, Associate Planner

DATE: December 15, 2020

SUBJECT: FILE #: PDEV20-005 Finance Acct#:

The following project has been resubmitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by .

PROJECT DESCRIPTION: A Development Plan to construct a 256,711-square foot industrial building on 11.3 acres of land located at 875 West State Street, within the IL (Light Industrial) zoning district (APNs: 1011-161-05 and 1011-161-04).


- The plan does adequately address the departmental concerns at this time.
 - No comments
 - See previous report for Conditions
 - Report attached (1 copy and email 1 copy)
 - Standard Conditions of Approval apply
- The plan does not adequately address the departmental concerns.
 - The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

Landscape Planning Division
Department Signature *Landscape Planner* Title *1/11/2021* Date

CITY OF ONTARIO
LANDSCAPE PLANNING DIVISION
 303 East "B" Street, Ontario, CA 91764

DAB CONDITIONS OF APPROVAL

Sign Off


 Jamie Richardson, Sr. Landscape Planner

1/11/2021
 Date

Reviewer's Name: Jamie Richardson, Sr. Landscape Planner	Phone: (909) 395-2615
D.A.B. File No.: PDEV20-005	Case Planner: Denny Chen
Project Name and Location: Commercial/Industrial Bldg. 835 W State Street	
Applicant/Representative: Inland Harbor LLC – Nancy Park 18831 Bardeen Ave., Suite 100 Irvine, CA 92612	
<input checked="" type="checkbox"/>	A Preliminary Landscape Plan (12/15/2020) meets the Standard Conditions for New Development and has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.
<input type="checkbox"/>	A Preliminary Landscape Plan (dated) has not been approved. Corrections noted below are required prior to Preliminary Landscape Plan approval.

A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED AS INCOMPLETE.
 Landscape construction plans with plan check number may be emailed to: landscapeplancheck@ontarioca.gov
DIGITAL SUBMITTALS MUST BE 10MB OR LESS.

Civil/ Site Plans

1. Provide an arborist report and tree inventory for offsite trees within 15' of adjacent property that would be affected by new walls, footings or on-site tree planting. Include genus, species, trunk diameter, canopy width and condition. Add tree protection notes on construction and demo plans to protect trees to remain. Replacement and mitigation for removed trees shall be equal to trunk diameter of heritage trees removed per the Development Code Tree Preservation Policy and Protection Measures, section 6.05.020.
2. Show backflow devices set back 4' from paving all sides. Locate on level grade.
3. Dimension, show and call out for step-outs at parking spaces adjacent to planters; a 12" wide monolithic concrete curb, DG paving or pavers with edging.

Landscape Plans

4. Provide an arborist report and tree inventory as noted in #1.
5. Show all utilities on the landscape plans. Coordinate so utilities are clear of tree locations.
6. Dimension all planters to have 6" curbs and 12" wide curbs where parking spaces are adjacent to planters.
7. Provide landscaping in 8' parkway along State St. Approved street tree for State Street is *Tristania laurina*. Parkway trees are to be 30' apart.
8. Provide planter spaces adjacent to the parking at entries to building. Provide a tree at parking ends.
9. Landscape construction plans shall meet the requirements of the Landscape Development Guidelines. See <http://www.ontarioca.gov/landscape-planning/standards>
10. After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections at a rate established by resolution of the City Council. Fees are:

Plan Check—5 or more acres	\$2,791.00
Inspection—Construction (up to 3 inspections per phase).....	<u>\$600.00</u>
Total.....	\$3,391.00

Landscape construction plans with building permit number for plan check may be emailed to: landscapeplancheck@ontarioca.gov

CITY OF ONTARIO

MEMORANDUM

TO: PLANNING DEPARTMENT, Denny Chen
FROM: BUILDING DEPARTMENT, Kevin Shear
DATE: March 09, 2020
SUBJECT: PDEV20-005

-
- The plan **does** adequately address the departmental concerns at this time.
- No comments
- Report below.

Conditions of Approval

1. Address for the project is 875 W State St
2. Standard Conditions of Approval apply.

KS:lr



CITY OF ONTARIO

MEMORANDUM

TO: Denny Chen, Associate Planner
Planning Department

FROM: Paul Ehrman, Deputy Fire Chief/Fire Marshal
Fire Department

DATE: July 20, 2020

SUBJECT: PDEV20-005 – A Development Plan to construct a 256,443-square foot industrial building on 11.24 acres of land located at 835 West State Street, within the IL (Light Industrial) zoning district (APNs: 1011-161-05 and 1011-161-04).
(Revision 1)

-
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.
-

SITE AND BUILDING FEATURES:

- A. 2016 CBC Type of Construction: III B
- B. Type of Roof Materials: Panelized
- C. Ground Floor Area(s): 256,443 Sq. Ft.
- D. Number of Stories: 1
- E. Total Square Footage: 256,443 Sq. Ft.
- F. 2016 CBC Occupancy Classification(s): S

CONDITIONS OF APPROVAL:

1.0 GENERAL

- 1.1 The following are the Ontario Fire Department ("Fire Department") requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards ("Standards.") It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at www.ontarioca.gov, click on "Fire Department" and then on "Standards and Forms."
- 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

2.0 FIRE DEPARTMENT ACCESS

- 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25') inside and forty-five feet (45') outside turning radius per Standard #B-005.
- 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150') in length shall have an approved turn-around per Standard #B-002.
- 2.4 Access drive aisles which cross property lines shall be provided with CC&Rs, access easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of building plan check.
- 2.5 "No Parking-Fire Lane" signs and /or red painted curbs with lettering are required to be installed in interior access roadways, in locations where vehicle parking would obstruct the minimum clear width requirement. Installation shall be per Standard #B-001.
- 2.6 Security gates or other barriers on fire access roadways shall be provided with a Knox brand key switch or padlock to allow Fire Department access. See Standards #B-003, B-004 and H-001.
- 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-four (24) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by Fire Department and other emergency services.

3.0 WATER SUPPLY

- 3.1 The required fire flow per Fire Department standards, based on the 2016 California Fire Code, Appendix B, is 4000 gallons per minute (g.p.m.) for 4 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.
- 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.
- 3.3 Buildings that exceed 100,000 square feet in floor area shall provide an onsite looped fire protection water line around the building(s.) The loops shall be required to have two or more points of connection from a public circulating water main.
- 3.4 The water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

4.0 FIRE PROTECTION SYSTEMS

- 4.1 On-site private fire hydrants are required per Standard #D-005, and identified in accordance with Standard #D-002. Installation and locations(s) are subject to the approval of the Fire Department. An application with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- 4.2 Underground fire mains which cross property lines shall be provided with CC & R, easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of Fire Department plan check. The shared use of private fire mains or fire pumps is allowable only between immediately adjacent properties and shall not cross any public street.
- 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- 4.5 Fire Department Connections (FDC) shall be located on the address side of the building within one hundred fifty feet (150') of a public fire hydrant on the same side of the street. Provide identification for all fire sprinkler control valves and Fire Department connections per Standard #D-007. Raised curbs adjacent to Fire Department connection(s) shall be painted red, five feet either side, per City standards.
- 4.6 A fire alarm system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 72. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.

- 4.7 Portable fire extinguishers are required to be installed prior to occupancy per Standard #C-001. Please contact the Fire Prevention Bureau to determine the exact number, type and placement required.

5.0 BUILDING CONSTRUCTION FEATURES

- 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- 5.6 Knox ® brand key-box(es) shall be installed in location(s) acceptable to the Fire Department. All Knox boxes shall be monitored for tamper by the building fire alarm system. See Standard #H-001 for specific requirements.
- 5.7 Placards shall be installed in acceptable locations on buildings that store, use or handle hazardous materials in excess of the quantities specified in the CFC. Placards shall meet the requirements of National Fire Protection Association (NFPA) Standard 704.

6.0 OTHER SPECIAL USES

- 6.1 The storage, use, dispensing, or handling of any hazardous materials shall be approved by the Fire Department, and adequate fire protection features shall be required. If hazardous materials are proposed, a Fire Department Hazardous Materials Information Packet, including Disclosure Form and Information Worksheet, shall be completed and submitted with Material Safety Data Sheets to the Fire Department along with building construction plans.
- 6.2 Any High Piled Storage, or storage of combustible materials greater than twelve (12') feet in height for ordinary (Class I-IV) commodities or storage greater than six feet (6') in height of high hazard (Group A plastics, rubber tires, flammable liquids, etc.) shall be approved by the Fire Department, and adequate fire protection features shall be required. If High Piled Storage is proposed, a Fire Department High Piled Storage Worksheet shall be completed and detailed racking plans or floor plans submitted prior to occupancy of the building.
- 6.3 Underground fuel tanks, their associated piping and dispensers shall be reviewed, approved, and permitted by Ontario Building Department, Ontario Fire Department, and San Bernardino County Fire Department Hazardous Materials Division. In fueling facilities, an exterior emergency pump shut-off switch shall be provided.

7.0 PROJECT SPECIFIC CONDITIONS

- 7.1 NOTE: Due to the depth of the property off of State Street, private hydrants will be required throughout the project



CITY OF ONTARIO

MEMORANDUM

TO: Denny Chen, Associate Planner

FROM: Officer Emily Hernandez, Police Department

DATE: March 30, 2020

SUBJECT: PDEV20-005- A DEVELOPMENT PLAN TO CONSRUCT ONE COMMERCIAL/INDUSTRIAL BUILDING TOTALING 257,920 SQUARE FEET LOCATED AT 835 WEST STATE STREET.

The “Standard Conditions of Approval” contained in Resolution No. 2017-027 apply. The applicant shall read and be thoroughly familiar with these conditions, including, but not limited to, the requirements below.

- Required lighting for all walkways, driveways, doorways, parking lots, hallways and other areas used by the public shall be provided. Lights shall operate via photosensor. Photometrics shall be provided to the Police Department and include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- Rooftop addresses shall be installed on the buildings as stated in the Standard Conditions. The numbers shall be at a minimum 6 feet tall and 2 foot wide, in reflective white paint on a flat black background, and oriented with the bottom of the numbers towards the addressed street. Associated letters shall also be included.
- First floor common stairwells shall be constructed to either allow for visibility through the stairwell risers or to prohibit public access to the areas behind stairwells.
- The Applicant shall comply with construction site security requirements as stated in the Standard Conditions.

The Applicant is invited to contact Officer Emily Hernandez at (909) 408-1755 with any questions or concerns regarding these conditions.

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT



Project File No.: PDE20-005
 Address: 835 West State Street
 APN: 1011-161-05 & 1011-161-04
 Existing Land Use: Trucking Yard
 Proposed Land Use: Development Plan to construct a 257,920 SF Industrial building
 Site Acreage: 11.24 Proposed Structure Height: 43 FT
 ONT-IAC Project Review: N/A
 Airport Influence Area: ONT

Reviewed By: Lorena Mejia
 Contact Info: 909-395-2276
 Project Planner: Denny Chen
 Date: 9/16/2020
 CD No.: 2020-003
 PALU No.: N/A

The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="checkbox"/> High Terrain Zone	<input type="checkbox"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input checked="" type="checkbox"/> FAA Notification Surfaces	<input checked="" type="checkbox"/> Recorded Overflight Notification
<input type="checkbox"/> Zone 2	<input type="checkbox"/> 65 - 70 dB CNEL	<input checked="" type="checkbox"/> Airspace Obstruction Surfaces	<input type="checkbox"/> Real Estate Transaction Disclosure
<input type="checkbox"/> Zone 3	<input checked="" type="checkbox"/> 60 - 65 dB CNEL	<input type="checkbox"/> Airspace Avigation Easement Area	
<input type="checkbox"/> Zone 4		Allowable Height: 160 FT	
<input type="radio"/> Zone 5			

The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1
 Zone 2
 Zone 3
 Zone 4
 Zone 5
 Zone 6
 Allowable Height: _____

CONSISTENCY DETERMINATION

This proposed Project is: Exempt from the ALUCP
 Consistent
 Consistent with Conditions
 Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

Airport Planner Signature: 

PLANNING / HISTORIC PRESERVATION COMMISSION STAFF REPORT



DATE: May 25, 2021

FILE NO: PHP21-008

SUBJECT: 2021 "Model Colony" Awards

LOCATION: Citywide

APPLICANT: City Initiated

PROPERTY OWNER: N/A

RECOMMENDATION:

That the Planning/Historic Preservation Commission consider and approve the 2021 "Model Colony" Award nominations.


BACKGROUND:

In 2000, the City Council adopted the Model Colony Awards to recognize outstanding efforts to restore, rehabilitate, and preserve Ontario's historic places. This is the twenty-first consecutive year that the City has conducted the awards program. The award categories include: Restoration, Rehabilitation, John S. Armstrong Landscape, Founder's Heritage Award, George Chaffey Memorial, and Merit. Past Model Colony Award recipients included Ontario's schools, churches, single-family residences, multiple-family properties, and joint public/private preservation projects.

There are 3 nominations this year, one for a single-family residence, one for a downtown infill project, and one for a local community leader. The nominees represent excellence in preservation of the community's heritage. The 2021 Model Colony Awards will be presented to award recipients by the City Council during a special ceremony which has been tentatively scheduled for June 15, 2021.

2021 AWARD NOMINEES:

For their outstanding efforts in the field of historic preservation, the nominees are:

Case Planner:	Elly Antuna, Associate Planner	Hearing Body	Date	Decision	Action
Planning Director Approval:		HPSC:			
Submittal Date:	N/A	PC / HPC:	05/25/2021		Final
Hearing Deadline:	N/A	CC:	06/15/2021		Presentation

Award of Merit: Dr. Jerome Titus House
Award Recipient: Charles and Sara Kumlander

The Dr. Jerome Titus House is a two-story residence constructed in 1912 for Corry B. White, treasurer of the Hot Point Electric Company. Dr. Jerome Titus, a prominent local doctor, purchased the house in 1917. The home remained in the Titus family until 1967. The residence is situated on a corner lot within the El Morado Court Historic District, a well-established, single-family neighborhood that consists of many styles of homes, including Craftsman Bungalow, Pueblo, and Prairie Revival.



The single-family residence is an unrivaled example of the Craftsman architectural style with Shingle style influences and features a steeply pitched, side-facing double gable roof with a prominent hipped roof dormer and a second, small, front-facing gable dormer over a recessed balcony with French doors. The Dr. Jerome Titus House has wide, overhanging eaves, ornate exposed rafter tails and triangular braces at the gable ends. The residence is clad in wood shingles, features a wraparound porch supported by heavy squared stone piers and features a stone chimney on the east facade. A circular bay window with multi-pane casement wood windows is also located on the east façade. The interior of the home retains the original wood flooring and trim, doors and hardware. The interior also features an impressive fireplace flanked by built-in wood shelves, and ceilings with wood box beams. A detached garage with matching exterior features is located south of the residence.

A Mills Act Contract was recorded on the property in 2017, and the current property owners purchased the home in 2019. Since purchasing the home, the current owners have made progress on the scheduled items listed on the Mills Act Contract. The exteriors of the residence and the detached garage have been painted, wood shingles that were broken or missing were replaced with custom cut shingles, overgrown landscaping was removed, and moss was cleared from the stone front porch and concrete stairs. The original wood windows were sanded and weather-stripped, ropes, weights and damaged glass panes were repaired or replaced, and two custom multiple-pane wood windows were constructed to replace deteriorated windows in the laundry room. The present owners intentionally sought out a historic house and recognize that ongoing maintenance is key to preservation. The owners have continued to maintain and preserve this historic home and are excellent stewards, ensuring that the Dr. Jerome Titus House remains a one-of-a kind architectural historic resource and a significant Contributor to the El Morado Court Historic District.

Award of Merit: Downtown Starbucks Infill
Award Recipient: Hannibal Petrossi

Historic Euclid Avenue is the centerpiece of Downtown Ontario. In 2005, Euclid Avenue and the median from Philadelphia Street in Ontario to 24th Street in Upland, was listed on the National Register of Historic Places as a historic district. The northwest corner of Euclid Avenue and E Street is located along this historic district, and within the proposed Downtown Historic District and the 1950s Subdistrict of the Downtown Design Guidelines. The commercial properties within this subdistrict have been constructed in a variety of styles from the 1950s, including Modern Commercial, Googie, and New Formalism. The subdistrict is predominately single-story commercial development. The 1950s were associated with the technological advances related to the atomic age but also built on the Art Deco and Art Moderne architecture of the previous decades. The materials used during this time ranged from various metals such as aluminum, structural glass, colored glazed bricks, or mosaic tiles.



In July 2019, Certificate of Appropriateness and Development Plan applications were submitted to facilitate the infill construction of a 2,430 square foot commercial building with a 480 square foot patio at the northwest corner of Euclid Avenue and E Street. The property owner considered the historic context of the Project site and successfully collaborated with staff and design team to ensure appropriate scale, massing, architecture, and design for the infill construction. The Secretary of the Interior Standards for the Treatment of Historic Properties and the Downtown Design Guidelines were used to guide the work which meets or exceeds those guidelines.

The commercial building was constructed in a Modern style with Art Deco elements and features two-story massing adjacent to the Euclid Avenue right-of-way. The primary façade of the building is divided by a smooth plaster finished tower feature and four-square plaster columns. The large storefront windows are under cantilevered metal awnings. Above the awnings are decorative metal treatments that give a transom window appearance. The centrally located double-wide entry door is glass and has sidelight windows. The walls are predominately covered in horizontal redwood siding and smooth plaster finish and have large subway tiles along the base of the building. The covered patio is enclosed with a 48-inch-tall decorative metal fence and is surrounded with abundant landscaping. The successful infill project is an excellent example of appropriate

commercial infill in an Historic Downtown and is consistent with that of the surrounding area. The design, site configuration and landscaping of the commercial building is appropriate in scale and massing for the infill construction and is a seamless addition on this block of Euclid Avenue and to the proposed Downtown Historic District.

Award of Merit: George Chaffey Memorial Award
Award Recipient: Robert Gregerok

For 25 years, Robert Gregerok has actively served on the Planning Commission. For last 18 years he has also served on the Historic Preservation Commission and the Historic Preservation Subcommittee where he has reviewed and approved local historic landmark and district designations, Mills Act Contracts, Certificates of Appropriateness, and other projects affecting historic properties. During Mr. Gregerok's time on the Commission, the City adopted a Historic Preservation Ordinance, became a Certified Local Government, and established the Ontario Register of Historic Resources that currently contains over 1,400 historic resources. Also during this time, 8 local historic districts and 99 local landmarks have been designated, 78 Mills Act Contracts have been executed and numerous Certificates of Appropriateness have been reviewed and approved.



Mr. Gregerok has attended conferences, workshops and trainings related to Historic Preservation, strengthening his interest and knowledge in the field. Mr. Gregerok's professional background in Geology has also contributed to his work as a Historic Preservation Commissioner, giving him a unique understanding of building codes and seismic retrofit requirements and how it relates to unreinforced masonry buildings and overall construction. In addition to serving on the Historic Preservation Commission and Historic Preservation Subcommittee, Mr. Gregerok served on the City of Ontario's Agricultural Preserve Advisory Committee and Convention Center Citizens Advisory Committee. Mr. Gregerok is also a member of Ontario Heritage, the American Planning Association, the National Trust for Historic Preservation, and the California Preservation Foundation.

The George Chaffey Memorial Award is reserved for distinguished leaders that have made significant contributions to the preservation of Ontario's historic resources and the stories behind those resources. Since the historic program's inception, Robert Gregerok has been a leader in historic preservation and his contributions have ensured that countless historic resources are protected, preserved, and documented for future

generations to enjoy.

COMPLIANCE WITH THE ONTARIO PLAN:

The Model Colony Awards Program is consistent with the principles, goals and policies contained in the following components of The Ontario Plan (TOP), including: (1) Vision, (2) Governance, and (3) Policy Plan (General Plan):

[1] City Council Priorities

Goals:

- Invest in the Growth and Evolution of the City's Economy
- Focus Resources in Ontario's Commercial and Residential Neighborhoods
- Encourage, Provide or Support Enhanced Recreational, Educational, Cultural and Healthy City Programs, Policies and Activities.

[2] Vision

Distinctive Development

- Development Quality: A community that is so well maintained and litter-free that its properties uniformly convey a sense of prosperity that is readily apparent and a symbol of community pride.

Dynamic Balance

- An appreciation for the “personality and charm” of this community, preserving important characteristics and values even as growth and change occur, all the while retaining a distinctive local feel where people love to be.

[3] Governance

Governance – Decision Making

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
 - G1-1 Consistency with Policies. We require that staff recommendations to the City Council be consistent with adopted City Council Priorities (Goals and Objectives) and the Policy Plan.

- G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision.

[4] Policy Plan (General Plan)

Community Design – Image & Identity

- Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.
 - CD1-3: Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.

Community Design – Historic Preservation

- Goal CD4: Historic buildings, streets, landscapes and neighborhoods, as well as the story of Ontario's people, businesses, and social and community organizations, that have been preserved and serve as a focal point for civic pride and identity.
 - CD4-6: Promotion of Public Involvement in Preservation. We engage in programs to publicize and promote the City's and the public's involvement in preservation efforts.
 - CD4-7: Public Outreach. We provide opportunities for our residents to research and learn about the history of Ontario through the Planning Department, Museum of History and Art, Ontario and the Robert E. Ellingwood Model Colony History Room.

Community Design – Protection of Investment

- Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.
 - CD5-4: Neighborhood Involvement. We encourage active community involvement to implement programs aimed at the beautification and improvement of neighborhoods.

FILE NO: PUD21-001

SUBJECT: An Amendment to the Emporia Family Housing Planned Unit Development to expand the Project area from approximately 2.80-acres of land to 4.95-acres of land, establish minimum building setbacks from Palm Avenue and Transit Street rights-of-way, modify minimum parking requirements, allow on-street loading, and update the planting palette; (APNs: 1049-051-04, 1049-054-02, 1049-054-03, 1049-054-04, 1049-054-06, 1049-059-06, and 1049-059-07) **submitted by The Related Companies of California, LLC. City Council action is required.**

PROPERTY OWNER: City of Ontario Housing Authority and American Legion Post No. 112

RECOMMENDED ACTION: That the Planning Commission consider and adopt resolutions recommending that the City Council: [1] approve the use of an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140); and [2] approve File No. PUD21-001, pursuant to the facts and reasons contained in the staff report and attached resolution.

PROJECT SETTING: The Project site is comprised of 4.95-acres of land and encompasses a 3-block area bordered by Holt Boulevard and Transit Street on the north, Vine Avenue on the west, Fern Avenue and Palm Avenue on the east, and Emporia Street on the south, and 2 lots located at 303 and 309 West Emporia Street, within LUA2-N (Arts District- North) and LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed Use) zoning district, and is depicted in Figure 1: Project Location.

Existing land uses on the Project site includes a 75-unit affordable housing development known as Emporia Family Housing — Phase 1,

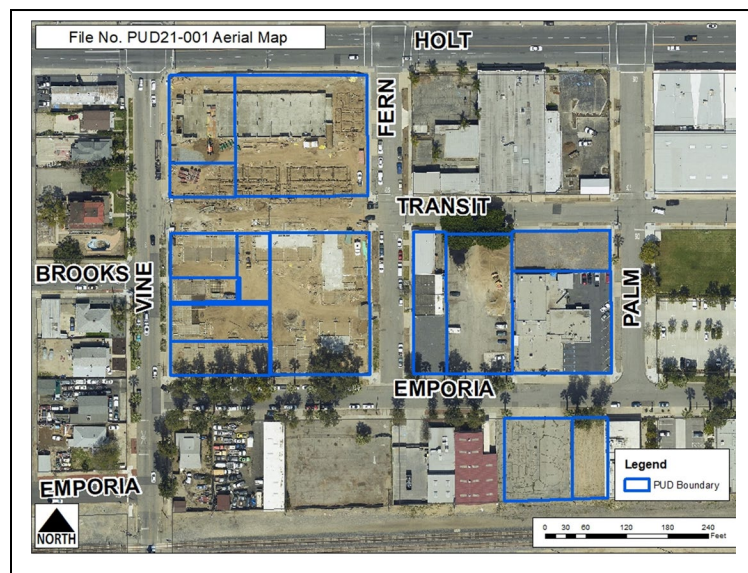



Figure 1: Project Location

Case Planner:	Diane Ayala
Planning Director Approval:	
Submittal Date:	02/23/2021

Hearing Body	Date	Decision	Action
PC	05/25/2021		Recommend
CC	06/15/2021		1 st Reading
CC	07/06/2021		Final

unimproved land and two single-story commercial buildings that have been determined to be Tier III historic resources and are listed on the local register of historic properties. The existing land uses south of Emporia Street, within the project site, are unimproved. Land uses surrounding the project site are characterized by a mixture of legal nonconforming residential uses, light industrial, and commercial uses. The existing surrounding land uses, zoning, and general plan land use designations are summarized in the "Surrounding Zoning & Land Uses" table located in the Technical Appendix of this report.

PROJECT ANALYSIS:

(1) Background — The project site is located within the Center City Redevelopment Project Area, established in 1983. And while the redevelopment practice was eliminated by the State, the Center City Redevelopment Project Area Plan is still in effect and encourages the development of a high intensity, multi-use central business district. In addition, The Ontario Plan contains goals and policies for the City's original downtown and the Downtown Mixed Use District, which further support the goals of the Center City Redevelopment Project Area Plan. The Center City Redevelopment Project Area Plan envisions revitalization of the City's downtown area, in part, by infusing high-density residential and mixed-use developments into the downtown core. The Ontario Plan ("TOP") was established to further this vision and is intended to create an intensive mixture of retail, office, and residential uses in a pedestrian friendly atmosphere, ensure the historic character of the district is enhanced, and concentrate the most intense/dense development along Euclid Avenue and Holt Boulevard. Furthermore, The Ontario Plan specifies a residential density range of 25 to 75 units per acre and a maximum floor area ratio of 2.0 for commercial office and retail developments within the Downtown Mixed Use District.

On May 23, 2017, the Planning Commission approved a Tentative Parcel Map (File No. PMTT17-005) to consolidate lots and vacate a portion of Transit Street, in conjunction with a Development Plan (File No. PDEV17-017) to construct 75 multiple-family residential affordable apartment units on 2.95-acres of land generally located south of Holt Boulevard, east of Fern Avenue, north of Emporia Street, and west of Vine Avenue. The Tentative Parcel Map and Development Plan approvals were contingent upon City Council approval of the Emporia Family Housing Planned Unit Development ("PUD").

On June 6, 2017, the City Council approved the Emporia Family Housing PUD, File No. PUD17-001, establishing development standards and guidelines for the development of a high density residential apartment project on 2.95 acres of land bordered by Fern Avenue on the east, Vine Avenue on the west, Holt Boulevard on the north, and Emporia Street on the south. Construction of Emporia Family Housing - Phase 1, consisting of 75 multiple-family dwelling units, has been completed.

On February 23, 2021, the Applicant submitted an Amendment to the Emporia Family Housing PUD (File No. PUD21-001), in conjunction with Tentative Parcel Map (File No. PMTT21-004), and Development Plan (File No. PDEV21-008) applications to expand the

PUD project area by 2.15-acres of land, consolidate parcels of land, vacate a portion of Fern Avenue south of Transit Street and north of Emporia Street, and construct 50 multiple-family affordable apartment units (known as “Emporia Family Housing- Phase 2”), and a Certificate of Appropriateness application (File No. PHP21-003) to allow the demolition of two Tier III historic resources to accommodate the proposed multiple-family residential development.

(2) Site Design/Building Layout — The PUD Amendment proposes to allow 50 walk-up units in a stacked flat configuration, at an overall residential density of 25.25 dwelling units per acre, meeting TOP’s Mixed Use land use designation requirement of minimum 25.1 dwelling units per acre. Minimum building setbacks proposed along Palm Avenue and Transit Street are 5-feet and are consistent with current PUD setback requirements.

(3) Site Access/Circulation/Utilities — In order to create a single development site connecting Phase 1 and Phase 2, Fern Avenue, between Transit and Emporia Streets, will be vacated. Existing public utilities in Fern Avenue will be relocated to adjacent streets. However, a 30-foot wide public utilities easement will be reserved within the vacated street right-of-way.

Vehicular access to the fully developed Phase 1 area will be maintained. A new vehicular access for Phase 2 will be provided from Palm Avenue. Additionally, vehicular access to a new off-street parking facilities will be provided from Emporia Street. The PUD will continue to provide separate east-west and north-south pedestrian paseos and connecting walkway links to the residential units, parking, and common areas throughout the site, for an increase in walkability (see Figure 2: Circulation Plan).

(4) Parking — Special consideration of reduced parking requirements for affordable housing projects is given by the State of California. On November 29, 2018, the California Department of Housing and Community Development (HCD) released the guidelines for the Streamlined Ministerial Approval Process created by Senate Bill No. 35 (SB 35 Guidelines), for eligible affordable housing projects. SB 35 allows local jurisdictions to reduce parking requirements for eligible projects to a maximum of one space per dwelling unit or waive all parking requirements for Project sites located within one-half mile of public transit. While the proposed Project is not

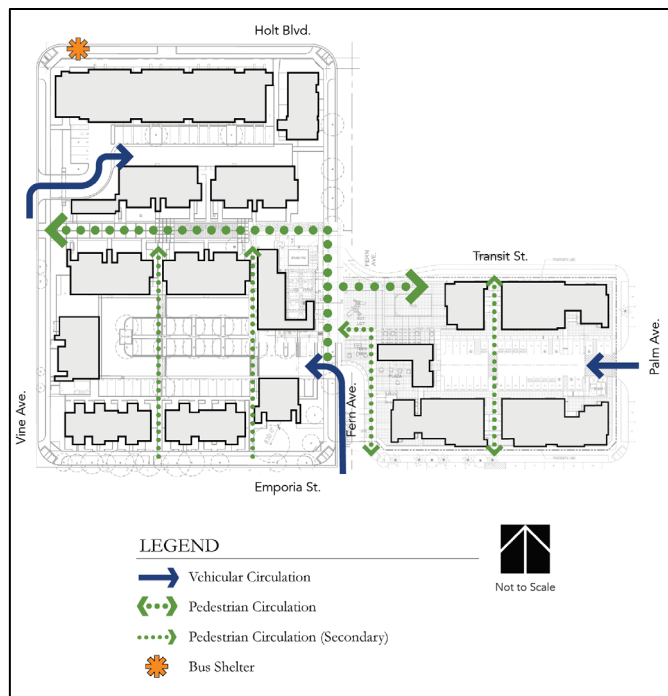


Figure 2—Circulation Plan

pursuing ministerial action under SB 35, it will provide 50-dwelling units that meet the affordable housing threshold and the Project site is located within the one-half mile of OmniTrans Bus Route 61 on Holt Boulevard. Parking standards for affordable housing should be customized to create alignment between parking needs and the requirements. Overall affordable housing projects tend to have fewer vehicles per household when compared to market rate housing. Efficiency-based parking, which also considers proximity to transit, can be tailored to the demand to minimize an oversupply of parking, an increase in development cost, and reduction of on-site amenities.

The PUD will continue to utilize a combination of on-site and on-street parking to provide an adequate parking supply to meet the anticipated parking demand for the project. All resident parking will be provided on site or on an off-street parking lot, while guest parking spaces will be provided on-street. New parking ratios and elimination of the covered parking requirement is proposed in the Amendment and are shown in the table below. A new off-street parking lot, comprised of 53 parking spaces, is located on the south side of Emporia Street. In addition, the use of tandem parking spaces is proposed, with the same dwelling unit assigned to both tandem spaces. Tandem parking for guest/visitor use is prohibited.

PUD Parking Requirements		
Type of Use	Current Parking Ratio	Proposed Parking Ratio
1-bedroom units	1.75 spaces per dwelling, including one space in a garage	1 space per dwelling
2 or more-bedroom units	2.0 spaces per dwelling, including one space in a garage or carport	1.65 spaces per dwelling
3 or more-bedroom units	N/A	2.0 spaces per dwelling
Visitor	1 Space Per 5	No change

(5) Landscaping —Street, parking lot, building perimeter, and patio area trees listed in PUD Table 4-5, Planting Palette, were replaced with a new planting palette to meet requirements of the Development Code and the California Department of Water Resources Model Water-Efficient Landscape Ordinance (MWELO). The existing Silk Oaks and California Fan Palms located within the Emporia Street parkway will remain in place and areas that are void of street trees will be planted with Silk Oak and California Fan Palm as not to disrupt or alter the existing historic street tree planting pattern.

(6) Historic Preservation — The Amendment identifies two new historic resources within the Project site. On August 10, 2010, the Historic Preservation Subcommittee approved a Tier III Determination, File No. PHP10-004, for a 4,000 square foot, one-story commercial/office building located at 201-215 South Fern Avenue. On January 11, 2011, the Historic Preservation Subcommittee approved a Tier III Determination, File No. PHP10-011, for a 10,000 square foot, one-story commercial building, commonly known as the

American Legion Building, located at 310 West Emporia Street. Issuance of a Certificate of Appropriateness is required prior to demolition of the historic buildings and as described in the "Background" section of this report, is being processed concurrently with this PUD Amendment request.

The proposed Amendment meets the goals and objectives as established in the Emporia Family Housing PUD. All proposed modifications to the PUD are identified by colored text in Attachment A of the resolution included with this staff report.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed Project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed Project are as follows:

(1) City Council Goals.

- Operate in a Businesslike Manner
- Focus Resources in Ontario's Commercial and Residential Neighborhoods

(2) Vision.

Distinctive Development:

- Commercial and Residential Development
 - Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

(3) Governance.

Decision Making:

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
 - G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision.

(4) Policy Plan (General Plan)

Land Use Element:

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.

- LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.

Housing Element:

- Goal H2: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.

- H2-1 Corridor Housing. We revitalize transportation corridors by encouraging the production of higher density residential and mixed-uses that are architecturally, functionally, and aesthetically suited to corridors.

- H2-2 Historic Downtown. We foster a vibrant historic downtown through facilitating a wide range of housing types and affordability levels for households of all ages, housing preferences, and income levels.

- H2-5 Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices, and other best practices.

- H2-6 Infill Development. We support the revitalization of neighborhoods through the construction of higher-density residential developments on underutilized residential and commercial sites.

Goal H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age, or other status.

- H5-2 Family Housing. We support the development of larger rental apartments that are appropriate for families with children, including, as feasible, the provision of services, recreation, and other amenities.

Community Economics Element:

- Goal CE1: A complete community that provides for all incomes and stages of life.

- CE1-6 Diversity of Housing. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life;

we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.

Community Design Element:

▪ Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

➤ CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.

➤ CD1-2 Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.

➤ CD1-3 Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.

▪ Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional, and distinct.

➤ CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:

- Building volume, massing, and height to provide appropriate scale and
- A true architectural style which is carried out in plan, section, and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

➤ CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping, and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.

➤ CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

➤ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➤ CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.

➤ CD2-11 Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.

▪ Goal CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

➤ CD3-1 Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.

➤ CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways, and plazas for pedestrians.

➤ CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.

HOUSING ELEMENT COMPLIANCE: The Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the proposed project exceeds the 31-dwelling units and density of 25.1 dwelling units per acre as specified in the Available Land Inventory.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino,

Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed Project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

ENVIRONMENTAL REVIEW: The environmental impacts of the Project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures are a condition of project approval and are incorporated herein by this reference.

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

	Existing Land Use	General Plan Designation	Zoning Designation
<i>Site</i>	Office, Commercial, Assembly, and Undeveloped	Mixed Use	LUA-3 (Holt Boulevard District) of MU-1 (Downtown Mixed Use)
<i>North</i>	Retail and Undeveloped	Mixed Use	LUA-3 (Holt Boulevard District) of MU-1 (Downtown Mixed Use)
<i>South</i>	Wholesale Machinery and Undeveloped	Mixed Use	LUA2-N (Arts District- North) of MU-1 (Downtown Mixed Use)
<i>East</i>	Parking Lot and Undeveloped	Mixed Use	LUA2-N (Arts District- North) of MU-1 (Downtown Mixed Use)
<i>West</i>	Multiple-Family Residential	Mixed Use	LUA-3 (Holt Boulevard District) of MU-1 (Downtown Mixed Use)

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO APPROVING THE USE OF AN ADDENDUM TO THE ONTARIO PLAN ENVIRONMENTAL IMPACT REPORT, PURSUANT TO THE REQUIREMENTS OF THE CALIFORNIA ENVIRONMENTAL QUALITY ACT, AS AMENDED, FOR FILE NOS. PUD21-001, PHP21-003, PMTT21-004, AND PDEV21-004.

WHEREAS, The Related Companies of California, LLC, (hereinafter referred to as "Applicant") has filed Applications for the approval of certain entitlements, including: [1] an Amendment to the Emporia Family Housing Planned Unit Development "PUD", File No. PUD21-001, which consists of amending certain provisions to: [a] expand the project boundary area from approximately 2.95-acres of land to 4.95-acres of land; [b] establish minimum building setbacks; [c] modify minimum parking requirements; [d] allow on-street loading; [e] update the planting palette; and [f] make various revisions necessary to the internal consistency of the PUD; [2] a Certificate of Appropriateness, File No. PHP21-003, for the demolition of two Tier III historic resources; [3] Tentative Parcel Map, File No. PMTT21-004 (TPM 20339), to consolidate four lots and the vacation of an adjoining section of Fern Avenue, for a total of 1.71 acres of land; and [4] a Development Plan, File No. PDEV21-008, to construct 50 multiple-family affordable housing dwelling units, along with the improvement of a 0.44-acre off-site parking facility, on approximately 4.95 acres of land, bordered by Holt Boulevard and Transit Street on the north, Vine Avenue on the west, Fern Avenue and Palm Avenue on the east, Emporia Street on the south, and two lots on the south side of Emporia Street, in the City of Ontario, California (hereinafter referred to as "Application" or "Project"); and

WHEREAS, The Ontario Plan Environmental Impact Report, State Clearinghouse No. 2008101140, was certified by City Council on January 27, 2010 (hereinafter referred to as "Certified EIR"), in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario has prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as "EIR Addendum") in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as "CEQA"); and

WHEREAS, the EIR Addendum concluded that implementation of the Project could result in a number of significant effects on the environment that were previously analyzed in the Certified EIR, and that the Certified EIR identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, pursuant to State CEQA Guidelines Section 15164(a), a lead agency shall prepare an addendum to a previously certified EIR if some changes or additions are necessary to a project, but the preparation of a subsequent or supplemental EIR is not required; and

WHEREAS, the City determined that none of the conditions requiring preparation of a subsequent or supplemental EIR would occur from the Project, and that preparation of an Addendum to the Certified EIR was appropriate; and

WHEREAS, the City of Ontario is the lead agency on the Project, and the Planning Commission is the recommending authority for the requested approval to construct and otherwise undertake the Project; and

WHEREAS, the Planning Commission has reviewed and considered the EIR Addendum for the Project, has concluded that none of the conditions requiring preparation of a subsequent of supplemental EIR have occurred, and intends to take actions on the Project in compliance with CEQA and state and local guidelines implementing CEQA; and

WHEREAS, the EIR Addendum for the Project are on file in the Planning Department, located at 303 East B Street, Ontario, CA 91764, are available for inspection by any interested person at that location and are, by this reference, incorporated into this Resolution as if fully set forth herein; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the recommending authority for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The environmental impacts of the Project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001.

(2) The EIR Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines.

(3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts.

(4) All previously adopted mitigation measures shall be a condition of Project approval, as they are applicable to the Project, and are incorporated herein by this reference.

(5) The EIR Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and

(6) There is no substantial evidence in the administrative record supporting a fair argument that the Project may result in significant environmental impacts.

SECTION 2: *Additional Environmental Review Not Required.* Based on the Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Environmental Impact Report is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

(d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: Planning Commission Action. Based upon the findings and conclusions set forth in Sections 1 and 2, above, the Planning Commission hereby recommends the City Council finds that based upon the entire record of proceedings before it, and all information received, that there is no substantial evidence that the Project will constitute substantial changes to the Certified EIR, and does hereby approve the EIR Addendum, attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 4: Indemnification. The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 5: Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 6: Certification to Adoption. The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of May 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 25, 2021, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**Addendum to The Ontario Plan
Environmental Impact Report**

(Addendum to follow this page)



City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

California Environmental Quality Act Addendum to The Ontario Plan Environmental Impact Report

Project Title/File Nos.: PUD21-001, PHP21-003, PMTT21-004 and PDEV21-008

Lead Agency: City of Ontario, 303 East "B" Street, Ontario, California 91764, (909) 395-2036

Contact Person: Diane Ayala, Senior Planner, (909) 395-2428

Project Sponsor: The Related Companies of California, LLC., c/o Randy Mai, 18201 Von Karmen Ave, Suite 900, Irvine, CA 92612; Phone: 510-421-2999; Email: Randy.Mai@related.com

Project Location: The Project site is located in southwestern San Bernardino County, within the City of Ontario. The City of Ontario is located approximately 40 miles from downtown Los Angeles, 20 miles from downtown San Bernardino, and 30 miles from Orange County. As illustrated on Figures 1 through 3, below, the Project site is generally located at the northwest corner of Emporia Street and Palm Avenue on Assessor Parcel Numbers ("APNs"): 1049-051-04, 1049-054-02, 1049-054-03, 1049-054-04, 1049-054-06, 1049-059-06, and 1049-059-07.

Figure 1: REGIONAL LOCATION MAP

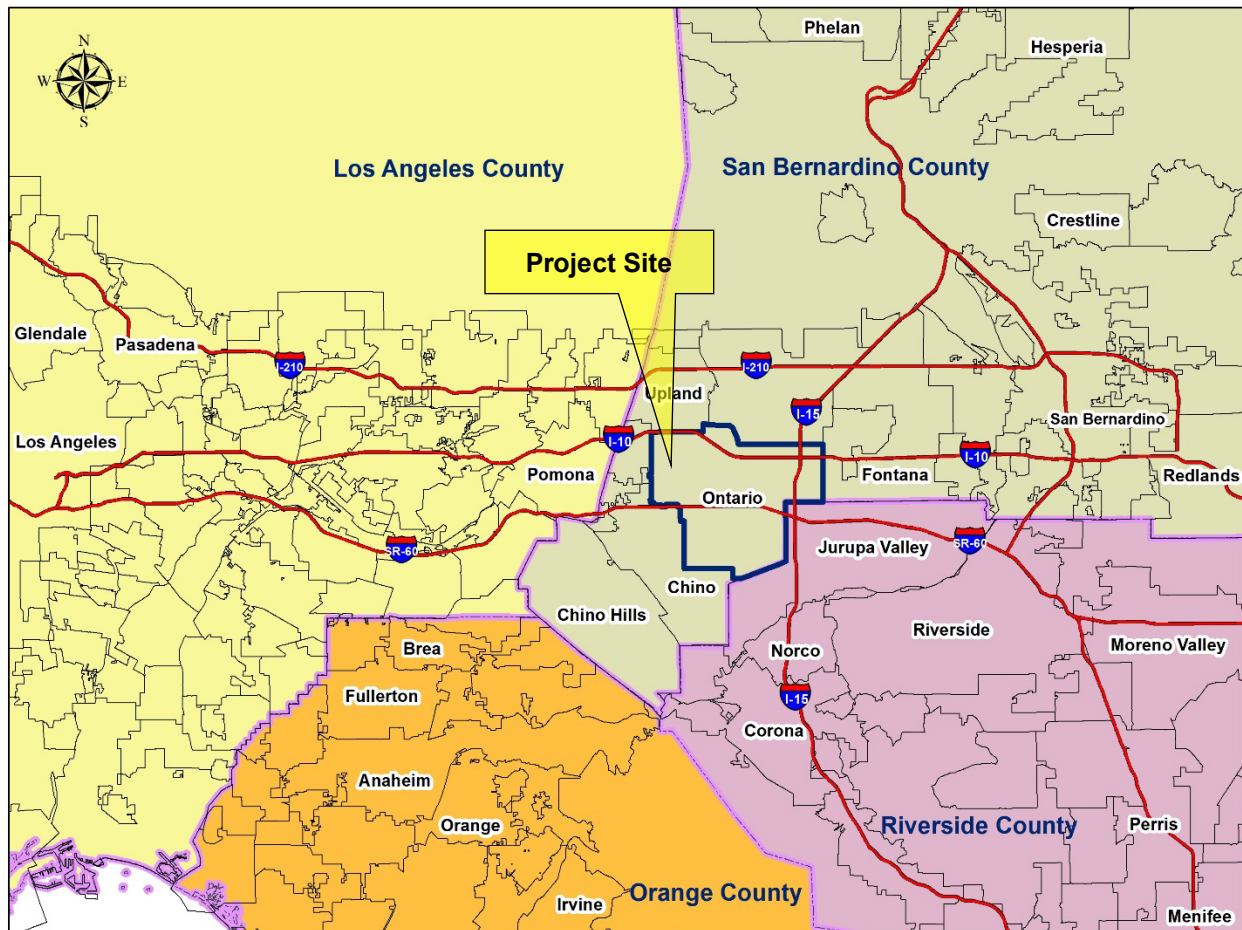


Figure 2: VICINITY MAP

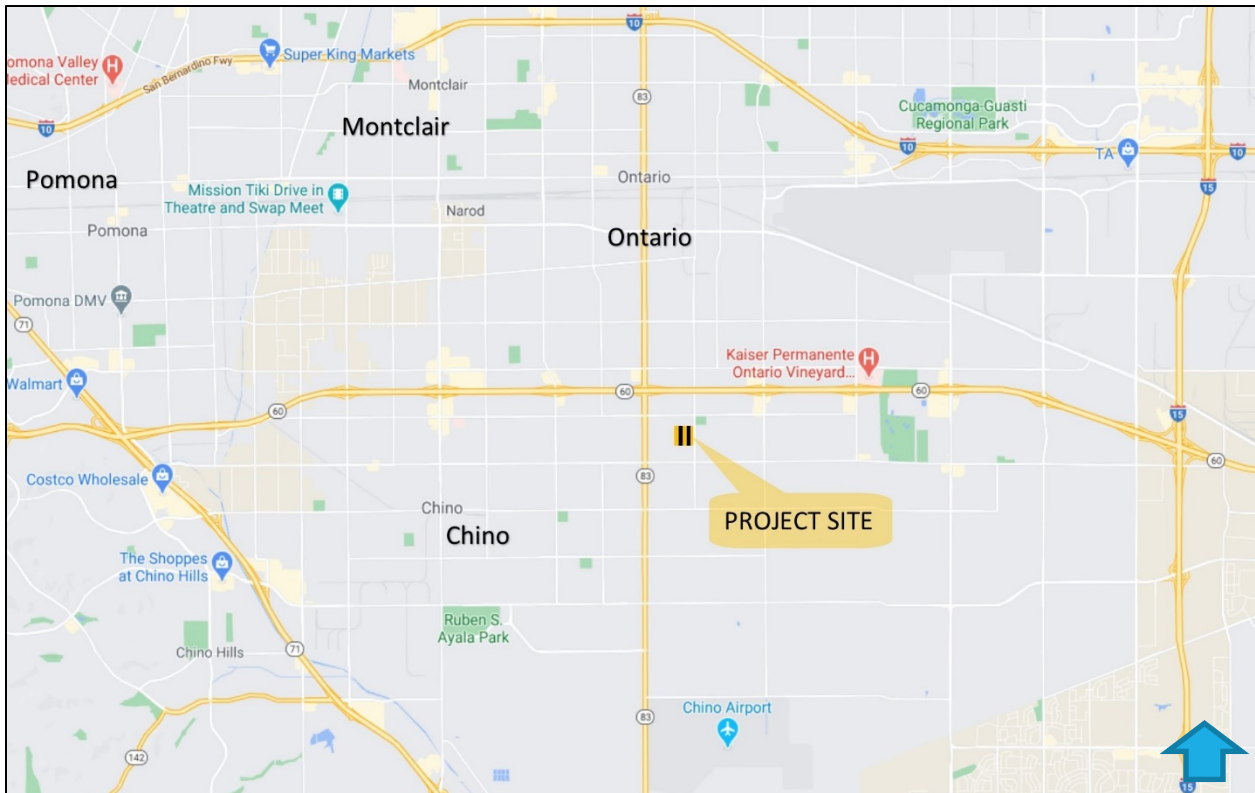


Figure 3: AERIAL PHOTOGRAPH



General Plan Designation: Mixed Use

Zoning: LUA2-N (Arts District- North) and LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district.

Description of Project: A request for approval of certain entitlements to facilitate the construction of an affordable multiple-family residential development project (Ontario Emporia Family Housing Project- Phase 2). The Project is proposed on approximately 2.15 acres of land within the Downtown Mixed Use District. Requested entitlements include the following:

1. An **Amendment to the Emporia Family Housing Planned Unit Development** (File No. PUD21-001) to expand the Project boundary area from approximately 2.80 acres of land to 4.95 acres of land, establish minimum building setbacks, modify minimum parking requirements, allow on-street loading, and update the planting palette.
2. A **Certificate of Appropriateness** (File No. PHP21-003) to demolish two historic Tier III buildings located within the Project site.
3. A **Tentative Parcel Map** (File No. PMTT21-004/ TPM 20339) to consolidate four lots and the vacation of an adjoining section of Fern Avenue, for a total of 1.71 acres of land.
4. A **Development Plan** (File No. PDEV21-008) to allow for the construction of 50 multiple-family affordable dwelling units on 2.15 acres of land, which includes the Tentative Parcel Map property in conjunction with an adjacent 0.44-acre property designated for off-street parking, located across Emporia Street, for a total project area of 2.15 acres of land.
5. A **Demolition Permit** to allow for the demolition of buildings located at 201 to 215 South Fern Avenue and 310 West Emporia Street, and determine appropriate hazardous materials (if any) and construction waste reduction measures; and
6. A **Street Vacation** to allow for the vacation of a segment of Fern Avenue, between Transit Street on the north and Emporia Street on the south , and a 30-foot wide storm drain easement will be established to allow an existing public storm drain in Fern Avenue to remain in place.
7. A **Disposition and Development Agreement (DDA)** with the Related Companies (Developer) for the development of this site as an affordable housing project. It is the intention of the parties that the Authority will sell the site to the Developer to develop a multifamily affordable rental housing project containing a total of the 50 dwelling units comprised of 6 one-bedroom units, 29 two-bedroom units, and 15 three-bedroom units. The units will be restricted to extremely low-income, very low-income, and low-income families, and will have a 55-year covenant recorded against the property to ensure the affordability and maintenance of the development.

Discretionary and Nondiscretionary Approvals Associated with Project: CEQA Guidelines Section 15124 states in pertinent part that if . . . “a public agency must make more than one decision on a project, all its decisions subject to CEQA should be listed . . .” Discretionary actions necessary to realize the project include, but are not limited to the following:

1. Planned Unit Development approval consistent with the requirements of The Ontario Plan and the Ontario Development Code;
2. Approval of a Development Plan subject to the requirements of the Development Code;
3. Approval of a Tentative and Final Parcel Map subject to the requirements of the Ontario Development Code;
4. Approval of a Certificate of Appropriateness Map subject to the requirements of the Ontario Development Code;

5. Approval of Demolition Permits;
6. Approval of rough and final grading plans;
7. Approval of infrastructure improvement plans, including but not limited to, roads, sewer, water, and storm water management systems;
8. Approval of architectural and structural building plans; and
9. Approval of landscape and irrigation plans.

Project Setting: The Project site encompasses a one-block area within the Mixed Use land use district of the Policy Plan (General Plan) and the MU-1 (Downtown Mixed Use) zoning district. The Project site is comprised of approximately 2.15 acres of land generally bordered by Transit Street to the north, Emporia Street, and the Southern Pacific Railroad Corridor to the south, Fern Avenue to the west and Palm Avenue to the east.

Existing land uses on the Project site includes unimproved land and two single-story commercial buildings that have been determined to be Tier III historic resources and are listed on the local register of historic properties. The existing properties south of Emporia Street, within the Project site, are unimproved.

Land uses surrounding the Project site are characterized by a mixture of legal nonconforming residential uses, light industrial, and commercial uses across Emporia Street to the south; retail, light industrial uses, and vacant property across Transit Street to the north; parking lot and unimproved land across Palm Avenue to the east; and a 75-unit affordable housing development (known as “Emporia Family Housing Phase 1”) to the west. The surrounding existing land uses, Policy Plan (General Plan), and zoning information are summarized below.

	<i>Existing Land Use</i>	<i>General Plan Designation</i>	<i>Zoning Designation</i>
Site:	Office, Commercial, Assembly, and Undeveloped	Mixed Use	LUA-3 (Holt Boulevard District) of MU-1 (Downtown Mixed Use)
North:	Retail and Undeveloped	Mixed Use	LUA-3 (Holt Boulevard District) of MU-1 (Downtown Mixed Use)
South:	Wholesale Machinery and Undeveloped	Mixed Use	LUA2-N (Arts District- North) of MU-1 (Downtown Mixed Use)
East:	Parking Lot and Undeveloped	Mixed Use	LUA2-N (Arts District- North) of MU-1 (Downtown Mixed Use)
West:	Multiple-Family Residential	Mixed Use	LUA-3 (Holt Boulevard District) of MU-1 (Downtown Mixed Use)

Cultural Resources Regulatory Setting: The City of Ontario Historic Preservation Ordinance addresses the alteration of historic resources under the “Certificate of Appropriateness” guidelines that regulate the proposed “demolition, in whole or part” of any historic building. Development Code Section 4.02.040 (Historic Preservation – Local Historic Landmark and Local District Designations, Historic Resource Tiering, and Architectural Conservation Areas) ranks historic properties under a three-tiered system to determine their significance.

1. Tier I properties should not be demolished or significantly altered under any circumstances. Tier I properties are considered Ontario’s most significant historical or cultural resources. In order for a property to be considered a Tier I it must meet the following criteria, [a] be listed as a local Eligible Historical Resource, [b] meet at least one of the criterion in the architecture category and three criteria in the historical category, or [c] be a contributor to a district and meet at least one architecture criterion and three historical criterion.
2. Demolition of Tier II properties should be avoided. In order for a property to be considered a Tier II

resource, the property must meet the following criteria, [a] be listed, or determined to be eligible for listing, in the National Register of Historic Places, or [b] be listed in the City's List of Eligible Historic Resources and determined to be eligible for listing in the California Register of Historic Places, or [c] be listed in the City's List of Eligible Historic Resources and meet at least two of the criterion in either architecture or history categories; and [4] be a contributing structure in a Eligible Historic District where the district meets at least two criterion on either the architecture or historic categories.

3. Tier III properties consist of [a] Designated Historic Landmarks; [b] contributing structures in Designated Historic Districts; or [c] historic resources listed in the Ontario Register, as defined in Development Code Section 4.02.045 (Historic Preservation – Rescind or Amend the Status of a Historic Resource). Demolition of these properties should be avoided where possible but may be appropriate under certain circumstances.

4. The City of Ontario Historic Preservation Commission makes recommendations to the Historic Preservation Subcommittee regarding the Tiers assigned to eligible historic properties. Pursuant to Development Code Section 4.02.040 (Historic Preservation – Local Historic Landmark and Local District Designations, Historic Resource Tiering, and Architectural Conservation Areas), the historic resource tiering criteria for individual properties is as follows:

a. Architecture/Form:

(i) The resource is prototypical, or one of the finest examples, of a period style, architectural movement, or construction in the City of a particular style of architecture, building type, or historical or archeological object. Only preeminent examples should be considered. Good representative examples of a style, period or method of construction are not appropriate; or

(ii) The resource is the first, last, only, or one of the finest examples, notable works, or the best surviving work by an architect or designer of major importance to the City, State or Nation.

b. History:

(i) The resource is the location of a historic event(s) that has significantly contributed to the history of the City, State, or Nation;

(ii) The resource is associated with a business, company, or individual that has made a significant cultural, social, or scientific contribution to the City, State, or Nation;

(iii) The resource is identified with a person(s) who has exerted a major influence on the heritage or history of the City, State, or Nation;

(iv) The resource embodies the ideals or principles of the "Model Colony" or furthers the ideals or principals established by the Chaffey Brothers;

(v) The resource has a direct relationship to one of the principal historic contexts in the City's history, including the "Model Colony," (includes the Chaffey Brothers, the Ontario Land and Improvement Company, or the citrus industry), the Guasti Winery or the wine industry, or the Dairy Preserve or the dairy industry;

(vi) The resource is related with a business, company, or individual significant in the agricultural history of the City; or

(vii) The resource is related to the archeological past of the region.

On August 10, 2010, the Historic Preservation Subcommittee approved a Tier III Determination (File No. PHP10-004) for the 4,000 square foot, one-story commercial building, a historic resource, located at 201-215 South Fern Avenue, within the Project site. On January 11, 2011, the Historic Preservation Subcommittee approved a Tier III Determination (File No. PHP10-011) for the 10,000 square foot, one-story commercial building, a historic resource, located at 310 West Emporia Street, also within the Project site.

Background: On January 27, 2010, the Ontario City Council adopted The Ontario Plan ("TOP"). TOP serves as the framework for the City's business plan and provides a foundation for the City to operate as a municipal corporation that consists of six (6) distinct components: 1) Vision; 2) Governance Manual; 3) Policy Plan; 4) Council Priorities; 5) Implementation; and 6) Tracking and Feedback. The Policy Plan

component of TOP meets the functional and legal mandate of a General Plan and contains nine elements: Land Use, Housing, Parks and Recreation, Environmental Resources, Community Economics, Safety, Mobility, Community Design and Social Resources.

An Environmental Impact Report ("EIR") was prepared for TOP (State Clearinghouse No. 2008101140) and certified by the City Council on January 27, 2010, that included Mitigation Findings and a Statement of Overriding Considerations pursuant to CEQA. The Certified TOP EIR analyzed the direct and physical changes in the environment that would be caused by TOP; focusing on changes to land use associated with the buildout of the proposed land use plan, in the Policy Plan and impacts resultant of population and employment growth in the City. The significant unavoidable adverse impacts that were identified in the EIR included agriculture resources, air quality, cultural resources, greenhouse gas emissions, noise, and transportation/traffic.

Analysis: According to the California Environmental Quality Act Guidelines Section 15164, an Addendum to a previously Certified EIR may be used if some changes or additions are necessary, but none of the conditions described in Section 15162 requiring the preparation of a subsequent Negative Declaration or EIR have occurred. The CEQA Guidelines require that a brief explanation be provided to support the findings that no subsequent EIR or Negative Declaration are needed for further discretionary approval. These findings are described below:

1. *Required Finding: Substantial changes are not proposed for the project that will require major revisions of the previous EIR due to the involvement of new, significant environmental effects or a substantial increase in the severity of previously identified effects.*

Substantial changes are not proposed by the Project and Project implementation will not require revisions to the Certified TOP EIR. The Certified TOP EIR analyzed the direct and physical changes in the environment that would be caused by TOP; focusing on changes to land use associated with the buildout of the proposed land use plan. In addition, all previously adopted mitigation measures are a condition of Project approval and are incorporated herein by reference. The attached Initial Study provides an analysis of the Project and verification that the Project will not cause environmental impacts such that any of the circumstances identified in State CEQA Guidelines Section 15162 are present.

2. *Required Finding: Substantial changes have not occurred with respect to the circumstances under which the project is undertaken, that would require major revisions of the previous Environmental Impact Report due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects.*

Substantial changes have not occurred with respect to the circumstances under which the Project was undertaken, that would not require major revisions to the Certified TOP EIR in that the proposed changes would be in keeping with the surrounding area. Therefore, no proposed changes or revisions to the EIR are required.

In addition, all previously adopted mitigation measures of the Certified TOP EIR are incorporated herein by reference. The attached Initial Study provides an analysis of the Project and verification that the Project will not cause environmental impacts such that any of the circumstances identified in State CEQA Guidelines Section 15162 are present.

3. *Required Finding: No new information has been provided that would indicate that the proposed project would result in one or more significant effects not discussed in the previous EIR.*

No new information has been provided that would indicate the proposed Project would result in any new significant effects not previously discussed in the Certified TOP EIR. Therefore, no proposed changes or revisions to the EIR are required. In addition, all previously adopted mitigation measures of the Certified TOP EIR are incorporated herein by reference. The attached Initial Study provides an analysis of the Project and verification that the Project will not cause environmental impacts such that any of the circumstances identified in State CEQA Guidelines Section 15162 are present.

CEQA Requirements for an Addendum: If changes to a Project or its circumstances occur or new information becomes available after adoption of a negative declaration, the lead agency may: (1) prepare a subsequent EIR if the criteria of State CEQA Guidelines Section 15162(a) are met, (2) prepare a subsequent negative declaration, (3) prepare an addendum, or (4) prepare no further documentation. (State CEQA Guidelines Section 15162(b)). When only minor technical changes or additions to the negative declaration are necessary and none of the conditions described in section 15162 calling for the preparation of a subsequent EIR or negative declaration have occurred, CEQA allows the lead agency to prepare and adopt an addendum. (State CEQA Guidelines Section 15164(b).)

Under Section 15162, a subsequent EIR or negative declaration is required only when:

1) Substantial changes are proposed in the project which will require major revisions of the previous negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;

2) Substantial changes occur with respect to the circumstances under which the project is undertaken which will require major revisions of the negative declaration due to the involvement of any new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or

3) New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the negative declaration was adopted, shows any of the following:

a) The project will have one or more significant effects not discussed in the previous negative declaration;

b) Significant effects previously examined will be substantially more severe than shown in the previous EIR;

c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or

d) Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative.

Thus, if the Project does not result in any of the circumstances listed in Section 15162 (i.e., no new or substantially greater significant impacts), the City may properly adopt an addendum to the Certified TOP EIR.

Conclusion: The Ontario Plan Environmental Impact Report (TOP EIR), certified by City Council on January 27, 2010, was prepared as a Program EIR in accordance with CEQA, the State CEQA Guidelines, and the City's Rules for the Implementation of CEQA and in accordance with Section 15121(a) of the State CEQA Guidelines (California Code of Regulations, Title 14, Division 6, Chapter 3). The Certified TOP EIR considered the direct physical changes and reasonably foreseeable indirect physical changes in the environment that would be caused by the Ontario Plan. Subsequent activities within the TOP Program EIR have been evaluated to determine whether an additional CEQA documents needs to be prepared.

Accordingly, and based on the findings and information contained in the Certified TOP EIR, the analysis above, the attached Initial Study, and CEQA statute and State CEQA Guidelines, including Sections 15164 and 15162, the Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified TOP EIR analyses are necessary, nor is there a need for any additional mitigation measures; therefore, pursuant to State CEQA Guidelines Section 15164, the Ontario City Council hereby adopts this Addendum to the Certified EIR.

Other public agencies whose approval is required (e.g., permits, financing approval or participation agreement): Permitting may be required by/through the Regional Water Quality Control Board (RWQCB) pursuant to requirements of the City's National Pollutant Discharge Elimination System (NPDES) Permit; Permitting may be required by/through the South Coast Air Quality Management District (SCAQMD) for certain equipment or land uses that may be implemented within the Project area; and Various construction, grading, and encroachment permits allowing implementation of the Project.

Tribal Consultation: Have California Native American tribes traditionally and culturally affiliated with the project area requested consultation pursuant to Public Resources Code section 21080.3.1?

Yes No N/A

If "yes," has consultation begun?

Yes No Completed

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

- | | | |
|--|---|---|
| <input type="checkbox"/> Aesthetics | <input type="checkbox"/> Agriculture/Forestry Resources | <input type="checkbox"/> Air Quality |
| <input type="checkbox"/> Biological Resources | <input type="checkbox"/> Cultural Resources | <input type="checkbox"/> Geology / Soils |
| <input type="checkbox"/> Greenhouse Gas Emissions | <input type="checkbox"/> Hazards & Hazardous Materials | <input type="checkbox"/> Hydrology / Water Quality |
| <input type="checkbox"/> Land Use / Planning | <input type="checkbox"/> Mineral Resources | <input type="checkbox"/> Noise |
| <input type="checkbox"/> Population / Housing | <input type="checkbox"/> Public Services | <input type="checkbox"/> Recreation |
| <input type="checkbox"/> Transportation | <input type="checkbox"/> Utilities / Service Systems | <input type="checkbox"/> Mandatory Findings of Significance |
| <input type="checkbox"/> Tribal Cultural Resources | <input type="checkbox"/> Wildfire | <input type="checkbox"/> Energy |

DETERMINATION (To be completed by the Lead Agency)

On the basis of this initial evaluation:

- I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
- I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
- I find that the proposed project MAY have a "potentially significant" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR pursuant to applicable standards; (b) have been avoided or mitigated pursuant to that earlier EIR, including revisions or mitigation measures that are imposed upon the proposed project; and (c) a Statement of Overriding Consideration was adopted for demolition of Tier III historic resources finding that the demolition of Tier III historic resources results in significant and unavoidable impacts that cannot be

fully mitigated to a level of less than significant, despite the imposition of all feasible mitigation measures identified in the EIR, and nothing further is required.

Signature	May13, 2021
	Date
Diane Ayala, Senior Planner	City of Ontario
Printed Name and Title	For

EVALUATION OF ENVIRONMENTAL IMPACTS

1. A brief explanation is required for all answers except "No Impact" answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g. the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general standards (e.g. the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).

2. All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.

3. Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. "Potentially Significant Impact" is appropriate if there is substantial evidence that an effect is significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.

4. "Negative Declaration: Less Than Significant with Mitigation Incorporated" applies where the incorporation of mitigation measures has reduced an effect from "Potentially Significant Impact" to a "Less than Significant Impact." The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from the "Earlier Analyses" Section may be cross-referenced).

5. Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or Negative Declaration. Section 15063(c)(3)(D). In this case, a brief discussion should identify the following:

a. Earlier Analyses Used. Identify and state where they are available for review.

b. Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.

c. Mitigation Measures. For effects that are "Less than Significant with Mitigation Measures Incorporated," describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.

6. Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g. general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.

7. Supporting Information Sources. A source list should be attached, and other sources used, or individuals contacted should be cited in the discussion.

8. This is only a suggested form, and lead agencies are free to use different formats; however, lead agencies should normally address the questions from this checklist that are relevant to a project's environmental effects in whatever format is selected.

9. The explanation of each issue should identify:

- a. The significance criteria or threshold, if any, used to evaluate each question; and
- b. The mitigation measure identified, if any, to reduce the impact to less than significance.

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
1. AESTHETICS. Would the project:				
a. Have a substantial adverse effect on a scenic vista?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. AGRICULTURE AND FOREST RESOURCES. In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest protocols adopted by the California Air Resources Board. Would the project:				
a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in the loss of forest land or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
e. Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3. AIR QUALITY. Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:				
a. Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in other emissions (such as those leading to odors adversely affecting a substantial number of people?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4. BIOLOGICAL RESOURCES. Would the project:				
a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
5. CULTURAL RESOURCES. Would the project:				
a. Cause a substantial adverse change in the significance of a historical resource pursuant to Section 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Disturb any human remains, including those interred outside of dedicated cemeteries?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
6. ENERGY. Would the project:				
a. Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
7. GEOLOGY AND SOILS. Would the project:				
a. Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury or death involving:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii. Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii. Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv. Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Be located on expansive soil, as defined in Table 18 1 B of the Uniform Building Code, creating substantial direct or indirect risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of waste water?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
8. GREENHOUSE GAS EMISSIONS. Would the project:				
a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emission of greenhouse gases?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
9. HAZARDS AND HAZARDOUS MATERIALS. Would the project:				
a. Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard or excessive noise for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g. Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
10. HYDROLOGY AND WATER QUALITY. Would the project:				
a. Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
i. result in substantial erosion or siltation on- or off-site;	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii. substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or offsite;	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii. create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff; or	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv. impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
11. LAND USE AND PLANNING. Would the project:				
a. Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
12. MINERAL RESOURCES. Would the project:				
a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
13. NOISE. Would the project result in:				
a. Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
14. POPULATION AND HOUSING. Would the project:				
a. Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of road or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
b. Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
15. PUBLIC SERVICES. Would the project:				
a. Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:				
i. Fire protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii. Police protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii. Schools?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv. Parks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
v. Other public facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
16. RECREATION. Would the project:				
a. Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Does the project include recreational facilities or require the construction or expansion of recreational facilities which have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
17. TRANSPORTATION. Would the project:				
a. Conflict with a program plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Would the project conflict or be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
18. TRIBAL CULTURAL RESOURCES. Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is				

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
<p>a. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k)?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>b. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>19. UTILITIES AND SERVICE SYSTEMS. Would the project:</p>				
<p>a. Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>b. Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>c. Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>d. Generate solid waste in excess of State or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>e. Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>20. WILDFIRES. If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:</p>				
<p>a. Substantially impair an adopted emergency response plan or emergency evacuation plan?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>b. Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>c. Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
d. Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
21. MANDATORY FINDINGS OF SIGNIFICANCE.				
a. Does the project have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current project, and the effects of probable future projects.)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Note: Authority cited: Public Resources Code sections 21083, 21083.05, 21083.09.

Reference: Gov. Code section 65088.4; Public Resources Code sections 21073, 21074, 21080(c), 21080.1, 21080.3, 21080.3.1, 21080.3.2, 21082.3, 21083, 21083.3, 21083.5, 21084.2, 21084.3, 21093, 21094, 21095 and 21151; *Sundstrom v. County of Mendocino* (1988) 202 Cal.App.3d 296; *Leonoff v. Monterey Board of Supervisors* (1990) 222 Cal.App.3d 1337; *Eureka Citizens for Responsible Govt. v. City of Eureka* (2007) 147 Cal.App.4th 357; *Protect the Historic Amador Waterways v. Amador Water Agency* (2004) 116 Cal.App.4th 1099, 1109; *San Franciscans Upholding the Downtown Plan v. City and County of San Francisco* (2002) 102 Cal.App.4th 656.

EXPLANATION OF ISSUES

1. AESTHETICS. Would the project:

a. Have a substantial adverse effect on a scenic vista?

Discussion of Effects: The Policy Plan (General Plan) does not identify scenic vistas within the City. However, TOP Policy Plan (Policy CD1-5) requires all major north-south streets be designed and redeveloped to feature views of the San Gabriel Mountains. The Project is not located adjacent to any major north-south arterial streets, as identified in the Functional Roadway Classification Plan (Figure M-2) of the Mobility Element within the Policy Plan. Consequently, the Project will not result in any new or substantially different aesthetic impacts than were previously addressed in TOP EIR, and no changes or additions to TOP EIR analyses are necessary.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

b. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings and historic buildings within a state scenic highway?

Discussion of Effects: The City of Ontario is served by three freeways: I-10, I-15, and SR-60. I-10 and SR-60 traverse the northern and central portion of the City, respectively, in an east–west direction. I-15 traverses the northeastern portion of the City in a north–south direction. These segments of I-10, I-15, and SR-60 have not been officially designated as scenic highways by the California Department of Transportation. In addition, there are no historic buildings, or any scenic resources identified on or in the vicinity of the Project site. Therefore, it will not result in adverse environmental impacts.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

c. In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?

Discussion of Effects: The Project would not degrade the existing visual character or quality of the site or its surroundings. The Project site is located in an area that is characterized by a mix of residential and commercial land uses and is surrounded by urban land uses.

The Project will substantially improve the visual quality of the area through development of the site with a high density residential development, which will be consistent with the policies of the Community Design Element of the Policy Plan (General Plan) and zoning designations on the property, as well as with the residential and commercial development in the surrounding area. Consequently, the Project will not result in any new or substantially different aesthetic impacts than were previously addressed in TOP EIR, and no changes or additions to TOP EIR analyses are necessary.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

d. Create a new source of substantial light or glare that would adversely affect day or nighttime views in the area?

Discussion of Effects: New lighting will be introduced to the site with the development of the proposed Project. Pursuant to the requirements of the City's Development Code and the Emporia Family Housing PUD, the Project's on-site lighting will be shielded, diffused, or indirect, to avoid glare to pedestrians or motorists. In addition, lighting fixtures will be selected and located to confine the area of illumination to within the project site and minimize light spillage.

Site lighting plans will be subject to review by the Planning Department and Police Department prior to issuance of building permits (pursuant to the City's Building Security Ordinance). Therefore, the Project will not result in any new or substantially different aesthetic impacts than were previously addressed in TOP EIR, and no changes or additions to TOP EIR analyses are necessary.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

2. AGRICULTURE AND FOREST RESOURCES. In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board. Would the project:

c. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?

Discussion of Effects: The site is presently improved with two commercial buildings and the unimproved land and does not contain any agricultural uses. Furthermore, the site is identified as “Urban and Built-Up Land” on the map prepared by the California Resources Agency, pursuant to the Farmland Mapping and Monitoring Program. Consequently, the Project will not result in any new or substantially different agriculture or forest resources impacts than were previously addressed in TOP EIR, and no changes or additions to TOP EIR analyses are necessary.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

d. Conflict with existing zoning for agricultural use, or a Williamson Act contract?

Discussion of Effects: The Project site is not zoned for agricultural use. Furthermore, there are no Williamson Act contracts in effect on the subject site. Therefore, no impacts to agricultural uses are anticipated, nor will there be any conflict with Williamson Act contracts.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

e. Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?

Discussion of Effects: The Project would not result in the rezoning of forest land, timberland, or timberland zoned Timberland Production because such land use designations do not exist within the City of Ontario. The Project site is zoned for Medium Density Residential development. The proposed project is consistent with the Land Use Element (Figure LU-6) of the Policy Plan (General Plan) and the development standards and allowed land uses of the MDR-18 (Medium Density Residential) zone. Therefore, no impacts to forest or timberland are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

f. Result in the loss of forest land or conversion of forest land to non-forest use?

Discussion of Effects: There is currently no land in the City of Ontario that qualifies as forest land as defined in Public Resources Code section 12220(g). Neither TOP nor the City’s Zoning Code provide designations for forest land. Consequently, the proposed Project would not result in the loss or conversion of forest land.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

g. Involve other changes in the existing environment, which, due to their location or nature, could individually or cumulatively result in loss of Farmland to non-agricultural use or conversion of forest land to non-forest use?

Discussion of Effects: The Project site is currently zoned MU-1 (Downtown Mixed Use) and is not designated as Farmland. The Project site is currently vacant and there are no agricultural uses occurring onsite. As a result, to the extent that the Project would result in changes to the existing environment those changes would not result in loss of Farmland to non-agriculture use.

Additionally, there is currently no land in the City of Ontario that qualifies as forest land as defined in Public Resources Code Section 12220(g). Neither TOP nor the City’s Zoning Code provide designations for forest land. Consequently, to the extent that the proposed Project would result in changes to the existing environment, those changes would not impact forest land.

Mitigation Required: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

3. AIR QUALITY. Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the Project:

a. Conflict with or obstruct implementation of the applicable air quality plan?

Discussion of Effects: The Project will not conflict with or obstruct implementation of any air quality plan. As noted in The Ontario Plan FEIR (Section 5.3), pollutant levels in the Ontario area already exceed Federal and State standards. To reduce pollutant levels, the City of Ontario is actively participating in efforts to enhance air quality by implementing Control Measures in the Air Quality Management Plan for local jurisdictions within the South Coast Air Basin.

The proposed Project is consistent with The Ontario Plan, for which the EIR was prepared and impacts evaluated. Furthermore, the Project is consistent with the City's participation in the Air Quality Management Plan and will not conflict with or obstruct implementation of the plan. Mitigation (Mitigation Measure 5.3-2) has been adopted by the City that requires fugitive dust control measures pursuant to SCAQMD's Rule 403, use of Tier 3 construction equipment, proper service and maintenance of construction equipment, limiting nonessential idling of construction equipment, and use of Super-Compliant VOC paints for coating and architectural surfaces. As a condition of approval, the project will comply with Mitigation Measure 5.3-2. No new impacts beyond those identified in the Certified TOP EIR that would result from Project implementation.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

b. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?

Discussion of Effects: The Project will not result in a cumulatively considerable net increase of any criteria pollutant for which the region is in non-attainment under an applicable federal or state ambient air quality because of the limited size and scope of the Project. Although no impacts are anticipated, the Project will still comply with the air quality standards of the TOP FEIR and the SCAQMD resulting in impacts that are less than significant [please refer to Sections 3(a) and 3(b)].

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

c. Expose sensitive receptors to substantial pollutant concentrations?

Discussion of Effects: Sensitive receptors are defined as populations that are more susceptible to the effects of pollution than the population at large. The SCAQMD identifies the following as sensitive receptors: long-term health care facilities, rehabilitation centers, convalescent centers, retirement homes, residences, schools, playgrounds, child-care centers, and athletic facilities. According to the SCAQMD, projects have the potential to create significant impacts if they are located within one-quarter mile of sensitive receptors and would emit toxic air contaminants identified in SCAQMD Rule 1401.

The Project proposes the construction of residential units, a sensitive receptor. There are not, however, any known hot spots or heavy concentrations of pollutants in the area that would expose residents to potential adverse impacts. Therefore, the Project will not result in any new or substantially different air quality impacts than were previously addressed in TOP EIR, and no changes or additions to TOP EIR analyses are necessary.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

d. Result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?

Discussion of Effects: The subject site was previously analyzed by TOP EIR as Mixed-Use (Downtown Mixed Use District). The residential use proposed on the subject site do not create objectionable odors. Further, the Project shall comply with the policies of the Ontario Municipal Code and the Policy Plan (General Plan). Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

4. BIOLOGICAL RESOURCES. Would the project:

a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?

Discussion of Effects: The Project site is located within an area that has not been identified as containing species identified as a candidate, sensitive, or special status species in local or regional plans, policies or regulations or by the California Department of Fish and Game or the U.S. Fish and Wildlife Service. Therefore, the Project will not result in any new or substantially different biological resources impacts than were previously addressed in TOP EIR, and no changes or additions to TOP EIR analyses are necessary.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?

Discussion of Effects: The subject site was previously analyzed by the Certified TOP EIR as residential uses. The site does not contain any riparian habitat or other sensitive natural community identified by the Department of Fish & Game or Fish & Wildlife Service. Therefore, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

c. Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?

Discussion of Effects: No wetland habitat is present on site. Therefore, Project implementation would have no impact on these resources.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?

Discussion of Effects: The site is part of a larger urbanized, mixed use area, and there are no wildlife corridors connecting this site to other areas. Therefore, the Project will not result in any new or substantially different biological resources impacts than were previously addressed in TOP EIR, and no changes or additions to TOP EIR analyses are necessary.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?

Discussion of Effects: The City of Ontario does not have any ordinances protecting biological resources; however, the Project site contains a number of mature trees necessitating the need for preservation consistent with the City's Tree Preservation Ordinance. Therefore, the Project will not result in any new or substantially different biological resources impacts than were previously addressed in TOP EIR, and no changes or additions to TOP EIR analyses are necessary.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

f. Conflict with the provisions of an adopted Habitat Conservation Plan (HCP), Natural Community Conservation Plan (NCCP), or other approved local, regional, or state habitat conservation plan?

Discussion of Effects: The site is not part of an adopted HCP, NCCP or other approved habitat conservation plan. As a result, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

5. CULTURAL RESOURCES. Would the project:

a. Cause a substantial adverse change in the significance of a historical resource pursuant to Section 15064.5?

Discussion of Effects: On January 27, 2010, The Ontario Plan (TOP), File No. PGPA06-001 for which an Environmental Impact Report (State Clearinghouse No. 2008101140) was adopted and certified by City Council, determining that demolition of Tier III historic resources results in significant and unavoidable impacts that cannot be fully mitigated to a level of less than significant, despite the imposition of all feasible mitigation measures identified in the EIR for which a Statement of Overriding Consideration was adopted. Two Tier III historic resources are located within the Project site:

(i) **Office Building located at 201-215 South Fern Avenue:** The building is a wide one-part commercial block building in the Art/Streamline Moderne architectural style and is approximately 4,000 square feet. The style is indicated by a flat roof, smooth wall surfaces, the use of glass-block windows, and a thin cantilevered roof running the entire length of the building which curves around the north-east corner. The building is situated in a rectangular building plan with a zero front setback and consists of eight commercial units. The original building addresses were 201 through 207 South Fern Avenue. It appears that there was a later addition constructed in the same style which added units in 1963 and 1978. The wall surfaces are made of concrete block covered by stucco and the entryways are recessed.

The 2003 Downtown intensive level survey suggests that the original portion of this commercial building appeared to be Eligible as a Contributor to a potential historic district stating that "the location, setting and design of the original building contributes to a historical record of the development of commerce in the City of Ontario." The design of the building preserves a sense of the historic character of the commercial core of the city of Ontario.

On August 10, 2010, the Historic Preservation Subcommittee approved a Tier III Determination (File No. PHP10-004) for this 4,000 square foot, one-story commercial building, a historic resource, located at 201-215 South Fern Avenue.

(ii) **American Legion Building located at 310 West Emporia Street:** Constructed in 1953, the American Legion Building is approximately 10,000 square feet and has a T-shape plan. The roof line is broken into two parts. The first portion is slightly angled and covered with composition. There is a window band under the eave that separates the two roof sections. The second portion of the roof is flat and intersects the wall just below the window band. The front façade has two vertical divisions. The wall, under

the angled roof, is a band of aluminum windows that increase in height towards the middle of the building. The wall below the window is covered with batten board and has vertical lines. The main entry door is centrally located and is under the flat roof section. There is a band of aluminum awning windows that sit below the eave and wrap the corner of the building. The building is constructed of hollow pumice concrete brick. This Modern Mid-Century building was designed by Jay Dewey Harnish of Harnish, Morgan and Causey (now referred to as HMC Architects, Inc.). HMC Architects is a nationally recognized firm with their main headquarters located in Ontario.

The American Legion is considered one of the Nation's most influential patriotic organizations. The American Legion was chartered by Congress in 1919 as a patriotic Veterans organization. Focusing on service to veterans, service members and communities, the Legion currently has about 2.4 million members in 14,000 posts worldwide. These posts are organized into 55 departments: one each for the 50 states, the District of Columbia, Puerto Rico, France, Mexico, and the Philippines.

The American Legion's national headquarters is in Indianapolis, with an office in Washington, D.C. The national organization has a full-time staff of about 300 employees. The Legion has several standing national commissions and committees that work with department, district, and post leadership to develop programs, increase membership, and recruit volunteers. Associated organizations are the American Legion Auxiliary and Sons of The American Legion. Over the years, The American Legion has founded many programs for children and youth, including American Legion Baseball and Boys Nation.

In 1919, a group of local veterans assembled in Archie Mitchell's office and formed the West End Service Men's club called "Cher- Ami". Archie Mitchell was an instrumental member in establishing the Ontario Municipal Airport. Later in 1920, the men's club was instituted as American Legion Post No. 112 with Dr. J.H. Titus serving as the first Commander. During the early 1920s, the Legion held regular meetings at the Hotpoint Clubhouse. During the late 1920s, John S. Armstrong donated the Armstrong residence to the Legion to be used as a new clubhouse. The residence was relocated from Euclid Avenue to 113 West E Street and was used as the clubhouse until 1953 when the new clubhouse was erected at the present location on Emporia Avenue. The Legion received top honors in 1929 for registering the highest percentage of voters in San Bernardino County. That same year the Legion Post took option on original thirty acres of the Ontario Airport. The Ontario Post has created and participated in several programs dedicated to promoting Americanism with the principals of justice, freedom, loyalty, and democracy. Aiding the veterans of all wars and their families is the foremost program as Ontario post works for a betterment of the community.

On January 11, 2011, the Historic Preservation Subcommittee approved a Tier III Determination (File No. PHP10-011) for the 10,000 square foot, one-story commercial building, a historic resource, located at 310 West Emporia Street. The following TOP mitigation measures are required Conditions of Approval for the Project:

(i) Prior to issuance of demolition building permit, every effort shall be made to relocate buildings. The buildings shall be offered at no cost for those who can relocate off site. Advertisements notifying the public of the opportunity to relocate the buildings shall be placed for a minimum of 45 days: on-site with temporary signage, in at least 3 local publications (newspapers, magazines, local organization newsletters), and on local bulletin boards (realtor's offices, local business). Applicant shall notify a minimum of 5 non-profit heritage organizations in writing of the building. A social media campaign including a dedicated web page (or post) with the building's information (description, square footage, photographs) and contact information should be incorporated into the building's advertisement.

(ii) Full documentation, including but not limited to as-built drawing, historical narrative and HABS photographs, of the historic resource pursuant to Historic American Building Survey (HABS) Level 3 standards shall be submitted to the Planning Department for review and approval and subsequent release to the Ovitt Family Community Library, Model Colony History Room prior to issuance of demolition building permit.

(iii) A mitigation fee pursuant to Section 7.01.030 of the Ontario Development Code shall be paid to the Planning Department prior to issuance of building permit for demolition. For Tier III structures, this mitigation fee is equal to 10 percent of the price per square foot construction cost as established in the most current ICC Building Valuation Data. The fee amount will be provided by the Planning Department at the time of payment.

(iv) A determination whether items within or on the resource should be salvaged shall be made by the Planning Department. The applicant shall be responsible for the removal, relocation and donation of such items selected for salvaging. An inventory of salvaged items shall be provided by the applicant to the Planning Department prior to be to issuance of demolition permit.

Therefore, the Project will not result in any new or substantially different cultural resources impacts than were previously addressed in the EIR, and no changes or additions to the EIR analyses are necessary.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

b. Cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5?

Discussion of Effects: The Ontario Plan FEIR (Section 5.5) indicates no archeological sites or resources have been recorded in the City with the Archeological Information Center at San Bernardino County Museum. However, only about 10 percent of the City of Ontario has been adequately surveyed for prehistoric or historic archaeology. While no adverse impacts to archeological resources are anticipated at this site due to its urbanized nature, standard conditions have been imposed on the Project that in the event of unanticipated archeological discoveries, construction activities will not continue or will moved to other parts of the Project site and a qualified archaeologist shall be contacted to determine significance of these resources. If the find is discovered to be historical or unique archaeological resources, as defined in Section 15064.5 of the CEQA Guidelines, avoidance or other appropriate measures shall be implemented. Therefore, the Project will not result in any new or substantially different cultural resources impacts than were previously addressed in TOP EIR, and no changes or additions to TOP EIR analyses are necessary.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

c. Disturb any human remains, including those interred outside of formal cemeteries?

Discussion of Effects: The proposed Project is in an area that has been previously disturbed by human activity. No known religious or sacred sites exist within the Project area. Thus, human remains are not expected to be encountered during any construction activities. However, in the unlikely event that human remains are discovered, existing regulations, including the California Public Resources Code Section 5097.98, would afford protection for human remains discovered during development activities. Furthermore, standard conditions have been imposed on the Project that in the event of unanticipated discoveries of human remains are identified during excavation, construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and/or Native American consultation has been completed, if deemed applicable.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

6. ENERGY Would the project:

a. Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?

b. Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?

Discussion of Effects: Energy was not analyzed in the Certified TOP EIR but has been included as part of the 2019 revisions to the State CEQA Guidelines. Implementation of the Project would not substantially increase the demand for electricity and natural gas at the Project site and gasoline consumption in the region during construction and operation. Implementation of the Project will require compliance with CALGreen Building Code (CCR Title 24, Part11). Moreover, the Project includes a sample Greenhouse Gas Reduction Measure Screening Table for Residential and Commercial Development. The Screening Table includes measures energy efficient development, indoor space efficiency measures, building efficiency measures, renewable energy measures, and water conservation measures. Measures that would reduce electricity consumption include, but are not limited to: greatly enhanced window

insulation, an enhanced cool-roof, an improved efficiency heating, ventilation, and air conditioning (“HVAC”) system, blower doors HERS verified Envelope leakage or equivalent, enhanced duct insulation, Energy Star commercial appliances, water efficient landscaping and irrigation systems, and water-efficient toilets and faucets.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts. No changes or additions to the Certified TOP EIR analyses are necessary.

7. GEOLOGY & SOILS. Would the project:

a. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury or death involving:

i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.

Discussion of Effects: There are no active faults known on the site and the Project site is located outside the Fault Rapture Hazard Zone (formerly Alquist-Priolo Zone). The Certified TOP EIR (Section 5.7/Figure 5.7-2) identifies eight active or potentially active fault zones near the City. Given that the closest fault zone is located more than ten miles from the Project site, fault rupture within the Project area is not likely. All development will comply with the Uniform Building Code seismic design standards to reduce geologic hazard susceptibility. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

ii. Strong seismic ground shaking?

Discussion of Effects: There are no active faults known on the site and the Project site is located outside the Fault Rapture Hazard Zone (formerly Alquist-Priolo Zone). The Certified TOP EIR (Section 5.7/Figure 5.7-2) identifies eight active or potentially active fault zones near the City. The closest fault zone is located more than ten miles from the Project site. The proximity of the site to the active faults will result in ground shaking during moderate to severe seismic events. All construction will comply with the California Building Code, the Ontario Municipal Code, The Ontario Plan, and all other ordinances adopted by the City related to construction and safety. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

iii. Seismic-related ground failure, including liquefaction?

Discussion of Effects: As identified in the Certified TOP EIR (Section 5.7), groundwater saturation of sediments is required for earthquake induced liquefaction. In general, groundwater depths shallower than 10 feet to the surface can cause the highest liquefaction susceptibility. Depth to ground water at the Project site during the winter months is estimated to be between 250 to 450 feet below ground surface. Therefore, the liquefaction potential within the Project area is minimal. Implementation of The Ontario Plan strategies, Uniform Building Code and Ontario Municipal code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

iv. Landslides?

Discussion of Effects: The Project would not expose people or structures to potential adverse effects, including the risk of loss, injury, or death involving landslides because the relatively flat topography of the Project site (less than 2 percent slope across the City) makes the chance of landslides remote. The allowed residential use will not create greater landslide potential impacts than were identified in the Certified

TOP EIR. Implementation of TOP EIR strategies, Uniform Building Code and Ontario Municipal Code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

b. Result in substantial soil erosion or the loss of topsoil?

Discussion of Effects: Implementation of the Project will not create greater erosion impacts than were identified in the Certified TOP EIR. Impacts will be less than significant with mitigation. The Project will not result in significant soil erosion or loss of topsoil because of the previously disturbed nature of the Project site and the limited size and scope of the Project. Grading increases the potential for erosion by removing protective vegetation, changing natural drainage patterns, and constructing slopes. However, compliance with the California Building Code and review of grading plans by the City Engineer will ensure no significant impacts will occur. In addition, the City requires an erosion/dust control plan for Projects located within this area. Implementation of a NPDES program, the Environmental Resource Element of the Policy Plan (General Plan) strategies, Uniform Building Code and Ontario Municipal code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse?

Discussion of Effects: Implementation of Project will not create greater landslide potential impacts than were identified in the Certified TOP EIR. Therefore, no adverse impacts are anticipated. In addition, the associated Project would not result in the location of development on a geologic unit or soil that is unstable, or that would become unstable because as previously discussed, the potential for liquefaction and landslides associated with the Project is less than significant. Certified TOP EIR (Section 5.7) indicates that subsidence is generally associated with large decreases or withdrawals of water from the aquifer. The Project would not withdraw water from the existing aquifer. Further, implementation of The Ontario Plan strategies, Uniform Building Code and Ontario Municipal Code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

d. Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?

Discussion of Effects: The majority of Ontario, including the Project site, is located on alluvial and eolian soil deposits. These types of soils are not considered to be expansive. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

e. Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?

Discussion of Effects: The area is served by the local sewer system and the use of alternative systems is not necessary. There will be no impact to the sewage system.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

f. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?

Discussion of Effects: The City of Ontario is underlain by deposits of Quaternary and Upper-Pleistocene sediments deposited during the Pliocene and early Pleistocene time, Quaternary Older Alluvial sediments may contain significant, nonrenewable, paleontological resources and are, therefore, considered to have high sensitivity at depths of 10 feet or more below ground surface. In addition, the Certified TOP EIR (Section 5.5) indicates that one paleontological resource has been discovered in the City. While no adverse impacts are anticipated, standard conditions have been imposed on the Project that in the event of unanticipated paleontological resources are identified during excavation, construction activities will not continue or will be moved to other parts of the Project site and a qualified paleontologist shall be contacted to determine significance of these resources. If the find is determined to be significant, avoidance or other appropriate measures shall be implemented.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR and TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

8. GREENHOUSE GAS EMISSIONS. Would the project:

a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?

Discussion of Effects: The Project site was previously analyzed by the Certified TOP EIR as a residential/commercial Mixed-Use that may have an impact on the environment at buildout of The Ontario Plan due to the emission of greenhouse gases ("GHGs"). According to the TOP EIR, this impact would be significant and unavoidable (Re-circulated Portions of the Ontario Plan Draft Environmental Impact Report, p. 2-118.) The TOP EIR was certified by the City on January 27, 2010, at which time a statement of overriding considerations was also adopted for The Ontario Plan's significant and unavoidable impacts, including that concerning the emission of greenhouse gases.

Implementation of Project will not create significantly greater impacts than were identified in the Certified TOP EIR. The Project includes a sample GHG Reduction Measures Screening Threshold Table, which provides guidance in measuring the reduction of greenhouse gas ("GHG") emissions attributable to certain design and construction measures incorporated into development projects. The analysis, methodology, and significance determination (thresholds) are based upon the City's Climate Action Plan ("CAP"), which includes GHG emission inventories (2008 and 2020 forecasts), a year 2020 emission reduction target, the goals and policies to reach the target, together with the Addendum prepared for the CAP. The Screening Table assigns points for each option incorporated into a project as mitigation or a project design feature (collectively referred to as "feature"). The point values correspond to the minimum emissions reduction expected from each feature. The menu of features allows maximum flexibility and options for how development projects can implement the GHG reduction measures. The point levels are based upon improvements compared to 2008 emission levels of efficiency. Projects that garner at least 100 points will be consistent with the reduction quantities anticipated in the City's CAP. As such, those projects that garner a total of 100 points or greater would not require quantification of project specific GHG emissions. Consistent with CEQA Guidelines, such projects would be determined to have a less than significant individual and cumulative impact for GHG emissions. As shown in the Project GHG Reduction Measures Screening Table, the Project garners a total of 103 points, and is therefore consistent with the reduction quantities anticipated in the City's CAP. Therefore, quantification of Project-specific GHG emissions is not required.

Additionally, pursuant to Public Resources Code Section 21083.3, this impact need not be analyzed further, because (1) the proposed project would result in an impact that was previously analyzed in the Certified TOP EIR; (2) the proposed project would not result in any greenhouse gas impacts that were not addressed in the Certified EIR; (3) the proposed project is consistent with The Ontario Plan. The proposed impacts of the project were already analyzed in the Certified EIR and the project will be built to current energy efficient standards. Potential impacts of project implementation will be less than significant with mitigation already required under the Certified TOP EIR and, CAP Screening Tables, and current energy efficiency standards. No changes or additions to the Certified TOP EIR analyses are necessary.

Mitigation Required: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to Certified TOP EIR analyses are necessary. The mitigation measures adopted as part of Certified TOP EIR adequately address any potential significant impacts and there is no need for any additional mitigation measures. The City has reviewed the emission reduction measures and concepts in TOP EIR's MM 6-2 and 6-3, and has determined that the following actions apply and shall be undertaken by the applicant in connection with the project: energy efficient design, efficient irrigation systems, and compliance with Title 24 of the California Code of Regulations.

b. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?

Discussion of Effects: The Project site was previously analyzed by the Certified TOP EIR as a residential/commercial mixed-use land use. The proposed Project is consistent with The Ontario Plan Goal ER 4 of improving air quality by, among other things, implementation of Policy ER4-3, regarding the reduction of greenhouse gas emissions in accordance with regional, State, and federal regulations. In addition, the proposed Project is consistent with the policies outlined in Section 5.6.4 of the TOP EIR, which aims to reduce the City's contribution of greenhouse gas emissions at build-out by fifteen (15 percent), because the project is upholding the applicable City's adopted mitigation measures as represented in 6-1 through 6-6 and energy efficient design, efficient irrigation systems, electric vehicle charging stations, and compliance with Title 24 of the California Code of Regulations. The Project is consistent with the City's Climate Action Plan. Therefore, the proposed Project does not conflict with an applicable plan, policy or regulation adopted for the purpose of reducing emissions of greenhouse gases.

Mitigation Required: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

9. HAZARDS & HAZARDOUS MATERIALS. Would the project:

a. Create a significant hazard to the public or the environment through the routine transport, use or disposal of hazardous materials?

Discussion of Effects: The Project site was previously analyzed by the TOP EIR for residential/commercial mixed-use land uses. The Project is not anticipated to involve the transport, use or disposal of hazardous materials during either construction or project implementation. Therefore, no adverse impacts are anticipated. However, in the unlikely event of an accident, implementation of the strategies included in The Ontario Plan will decrease the potential for health and safety risks from hazardous materials to a less than significant impact.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?

Discussion of Effects: The Project site was previously analyzed by the Certified TOP EIR for residential/commercial mixed-use land uses. The proposed Project does not include the use of hazardous materials or volatile fuels. In addition, there are no known stationary commercial or industrial land uses within close proximity to the subject site, which use/store hazardous materials to the extent that they would pose a significant hazard to visitors/occupants to the subject site, in the event of an upset condition resulting in the release of a hazardous material.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances or waste within one-quarter mile of an existing or proposed school?

Discussion of Effects: The Project does not include the use, emissions or handling of hazardous or acutely hazardous materials, substances, or waste. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?

Discussion of Effects: The Project site is not listed on the hazardous materials site compiled pursuant to Government Code Section 65962.5. Therefore, the Project would not create a hazard to the public or the environment and no impact is anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

e. For a project located within the safety zone of the airport land use compatibility plan for ONT or Chino Airports, would the project result in a safety hazard for people residing or working in the project area?

Discussion of Effects: The Project was reviewed and found to be located within the Airport Influence Area of Ontario International Airport ("ONT") and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan ("ALUCP") for ONT. The Project will not result in a safety hazard for people working or residing in the Project area because it will not obstruct aircraft maneuvering because of the Project's low elevation and the architectural style of the Project. Additionally, the Land Use Compatibility Guidelines for Noise Impacts (Table LU-08) shows the proposed use as normally accepted in the 65 CNEL. The proposed use will comply with standards for mitigating noise. Therefore, the Project will not result in any new or substantially different hazards and hazardous materials impacts than were previously addressed in TOP EIR, and no changes or additions to TOP EIR analyses are necessary.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

f. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?

Discussion of Effects: The City's Safety Element, as contained within The Ontario Plan, includes policies and procedures to be administered in the event of a disaster. The Ontario Plan seeks interdepartmental and inter-jurisdictional coordination and collaboration to be prepared for, respond to and recover from every day and disaster emergencies. In addition, the Project will comply with the requirements of the Ontario Fire Department and all City requirements for fire and other emergency access. Because the Project is required to comply with all applicable City codes, any impacts would be reduced to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

g. Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?

Discussion of Effects: The Project site is not located in or near wildlands. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

10. HYDROLOGY & WATER QUALITY. Would the project:

a. Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality?

Discussion of Effects: The Project site is served by City water and sewer service and will not affect water quality standards or waste discharge requirements. The site is required to comply with the statewide National Pollutant Discharge Elimination System (“NPDES”) General Industrial Activities Stormwater Permit, the San Bernardino County Area-Wide Urban Runoff Permit (MS4 permit) and the City of Ontario’s Municipal Code (Title 6, Chapter 6 (Stormwater Drainage System)). This would reduce any impacts to below a level of significance. Furthermore, the applicant for the subject site has submitted a Preliminary Water Quality Management Plan (PWQMP), which establishes the subject sites’ compliance with storm water discharge and water quality management requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development (LID) best management practices (BMPs), such as retention and infiltration, biotreatment and evapotranspiration. The PWQMP proposes the use of an underground stormwater infiltration system for the subject sites. Any overflow drainage will be conveyed to the public street by way of parkway culverts.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

b. Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?

Discussion of Effects: The Project site was previously analyzed by the Certified TOP EIR for residential/commercial mixed-use land uses. The water use associated with the proposed use of the property will be negligible, and the proposed Project will not deplete groundwater supplies, nor will it interfere with recharge. The water use associated with the proposed use of the property was included in the Certified TOP EIR analysis. The development of the site will require the grading of the site and excavation is expected to be less than three feet and would not affect the existing aquifer, estimated to be about 230 to 250 feet below the ground surface. No adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:

i. Result in substantial erosion or siltation on- or off-site?

Discussion of Effects: It is not anticipated that the Project would alter the drainage pattern of the site or area, in a manner that would result in erosion, siltation or flooding on-or-off site, nor will the proposed Project increase the erosion of the subject site or surrounding areas. The existing drainage pattern of the site will not be altered, and it will have no significant impact on downstream hydrology. Stormwater generated by the Project will be discharged in compliance with the statewide NPDES General Construction Activities Stormwater Permit and San Bernardino County MS4 permit requirements. With the full implementation of a Storm Water Pollution Prevention Plan developed in compliance with the General Construction Activities Permit requirements, the Best Management Practices included in the SWPPP, and a stormwater monitoring program would reduce any impacts to below a level of significance. No streams or streambeds are present on the site. No changes in erosion off-site are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

ii. Substantially increase the rate or amount of surface runoff water in a manner which would result in flooding on- or off-site?

Discussion of Effects: The Project is not anticipated to increase the flow velocity or volume of storm water runoff to cause environmental harm from the site and will not create a burden on existing infrastructure. Furthermore, with the implementation of an approved Water Quality Management Plan developed for the site, in compliance with the San Bernardino County MS4 Permit requirements, stormwater runoff volume shall be reduced to below a level of significance.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

iii. Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?

Discussion of Effects: It is not anticipated that the Project would create or contribute runoff water that would exceed the capacity of existing or planned stormwater drainage systems or create or contribute stormwater runoff pollutants during construction and/or post-construction activity. Pursuant to the requirements of The Ontario Plan, the City's Development Code, and the San Bernardino County MS4 Permit's Water Quality Management Plan ("WQMP"), individual developments must provide site drainage and WQMP plans according to guidelines established by the City's Engineering Department. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

iv. Impede or redirect flood flows?

Discussion of Effects: Urbanization in the areas surrounding the Project site have resulted in increased responsiveness of the basin to rainfall. The increase in impervious surfaces such as roofs, roads, and parking lots has resulted in a decrease in groundwater infiltration and larger storm surges. The Project site is not impacted by offsite flows. The Project site is not located in a FEMA Firm Panel designated Flood Zone Risk, and according to the United States Fish and Wildlife Service National Wetlands Inventory ("NWI") no wetlands exist on the property. An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. However, the Project will be conditioned to design and construct a storm water detention facility on site so that the 100-year post-development peak flow does not exceed 80 percent of pre-development peak flows.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

d. In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?

Discussion of Effects: Impacts associated with flooding are primarily related to the construction or placement of structures in areas prone to flooding including within an unprotected 100-year flood zone, and in areas susceptible to high tides, tsunamis, seiches, mudflows or sea level rise. Specifically, structures placed in flood prone areas, if flooded, would be damaged, and could subject people to injury or death. The National Flood Insurance Act of 1968 requires the identification of floodplain areas and establishment of flood-risk zones within those areas. FEMA administers the programs and coordinates with communities to establish effective floodplain management standards. According to FEMA, the Project is not located in a known floodplain. Furthermore, this area is not known to flood and is not typically subjected to flooding. The Project site is not located in a floodplain as shown in Figure S-2 of TOP. The Project site is in an urbanized area that is developed residential dwelling units. No wetlands have been mapped on the Project site according to the NWI. According to the FEMA, the Project is not located in an area that is subject to flood hazard, tsunami, or seiche zones. The Project site is located over 60 miles east of the Pacific Ocean and is not located in a mapped tsunami zone. Therefore, the Project would not have a significant risk of flood hazard, tsunami, seiche zones, release of pollutants due to Project inundation.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

e. Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?

Discussion of Effects: The Regional Water Quality Control Board's Basin Plan is designed to preserve and enhance water quality and protect the beneficial uses of all regional waters. Specifically, the Basin Plan (i) designates beneficial uses for surface and ground waters, (ii) sets narrative and numerical

objectives that must be attained or maintained to protect the designated beneficial uses and conform to the state's anti-degradation policy, and (iii) describes implementation programs to protect all waters in the region. The Project adheres to requirements of the water quality control plan, including all existing regulation and permitting requirements. This includes the incorporation of best management practices ("BMPs") to protect water quality during construction and operational periods. Development of the Project is subject to all existing water quality regulations and programs, as described in the regulatory section above, including all applicable construction permits. Existing General Plan policies related to water quality are also applicable to the Project. Implementation of these policies, in conjunction with compliance with existing regulatory programs, ensures that water quality impacts related to the Project are less than significant.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

11. LAND USE & PLANNING. Would the project:

a. Physically divide an established community?

Discussion of Effects: The Project site is in an area that is developed with urban land uses. The Project will be of similar design and size to surrounding development. The Project will become a part of the larger mixed-use community. Therefore, the Project will not result in any new or substantially different land use and planning impacts than were previously addressed in TOP EIR, and no changes or additions to TOP EIR analyses are necessary.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

b. Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?

Discussion of Effects: The subject site was previously analyzed by the Certified TOP EIR residential/commercial mixed-use land uses. Implementation of Project will not create greater impacts than were identified in the Certified TOP EIR. The proposed Project does not interfere with any policies for environmental protection. As such, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

12. MINERAL RESOURCES. Would the project:

a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?

Discussion of Effects: The Project site is located within a mostly developed area surrounded by urban land uses. There are no known mineral resources in the area; therefore, the project will not result in any new or substantially different mineral resources impacts than were previously addressed in TOP EIR, and no changes or additions to TOP EIR analyses are necessary.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

b. Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan, or other land use plan?

Discussion of Effects: There are no known mineral resources in the area. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

13. NOISE. Would the project result in:

a. Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?

Discussion of Effects: The Project site was previously analyzed by the Certified TOP EIR for residential/commercial mixed-use land uses. The Project will not expose people to or generate noise levels in excess of standards as established in The Ontario Plan FEIR (Section 5.12); therefore, the project will not result in any new or substantially different noise impacts than were previously addressed in TOP EIR, and no changes or additions to TOP EIR analyses are necessary.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

b. Generation of excessive groundborne vibration or groundborne noise levels?

Discussion of Effects: Implementation of the Project will not create greater impacts than were identified in the Certified TOP EIR. The uses associated with this proposed Project are required to comply with the environmental standards contained in the City of Ontario Development Code and as such, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

c. For a project located within the vicinity of a private airstrip or the noise impact zones of the airport land use compatibility plan for ONT and Chino Airports, would the project expose people residing or working in the project area to excessive noise levels?

Discussion of Effects: The Project was reviewed and found to be located within the Airport Influence Area of Ontario International Airport ("ONT") and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan ("ALUCP") for ONT. According to the Safety Element, the project is located outside of the 65 CNEL noise contour; therefore, the project will not result in any new or substantially different noise impacts than were previously addressed in TOP EIR, and no changes or additions to TOP EIR analyses are necessary. or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

14. POPULATION & HOUSING. Would the project:

a. Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of road or other infrastructure)?

Discussion of Effects: The Project site was previously analyzed by the Certified TOP EIR for residential/commercial mixed-use land uses and is consistent with General Plan land use designations and would not induce significant population growth. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

b. Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?

Discussion of Effects: The Project site does not contain existing housing. Implementation of the Project will result in the addition of 50 residential dwelling units.

Mitigation: No additional mitigation required. No changes or additions to the Certified TOP EIR analyses are necessary.

15. PUBLIC SERVICES. Would the project:

a. Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:

i. Fire protection?

Discussion of Effects: The Project site is in a developed area currently served by the Ontario Fire Department. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

ii. Police protection?

Discussion of Effects: The Project site is in a developed area, currently served by the Ontario Police Department. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

iii. Schools?

Discussion of Effects: Upon development, the Project proponent will be required to pay school fees as prescribed by state law prior to the issuance of building permits. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

iv. Parks?

Discussion of Effects: The Project site is in a developed area, currently served by the City of Ontario. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

v. Other public facilities?

Discussion of Effects: The Project site is in a developed area, currently served by the City of Ontario. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

16. RECREATION. Would the project:

a. Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?

Discussion of Effects: This Project is not proposing a significant number of new housing units that would result in the substantial physical deterioration of nearby existing parks. Implementation of the Project would result in the construction of 12,356 square feet of private recreational amenities on-site to include a pool, pool house and children's play area as required by the PUD for the development of 50 residential units. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

b. Does the project include recreational facilities or require the construction or expansion of recreational facilities that have an adverse physical effect on the environment?

Discussion of Effects: This Project is not proposing a significant number of new housing units or large employment generator that would require the construction of neighborhood parks or other recreational facilities. Furthermore, implementation of the Project includes construction of a recreational area and swimming pool for private use of residents. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

17. TRANSPORTATION. Would the project:

a. Conflict with a program plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle, and pedestrian facilities?

Discussion of Effects: The Project is in an area that is mostly developed with all street improvements existing. The number of vehicle trips per day is not expected to be increased significantly. Therefore, the Project will not create a substantial increase in the number of vehicle trips, traffic volume or congestion at intersections. Consequently, the Project will not result in any new or substantially different transportation/traffic impacts than were previously addressed in TOP EIR, and no changes or additions to TOP EIR analyses are necessary.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

b. Conflict or be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)?

Discussion of Effects: CEQA Guidelines Section 15064.3 subdivision (b) has been included in the 2018 CEQA Guidelines as part of the implementation of SB 743 which requires local jurisdictions to use Vehicle Miles Travelled (VMT) instead of Level of Service (LOS) methodologies for the purpose of determining the significance of traffic impacts under CEQA. Also, as part of the implementation of SB 743 local jurisdictions are required to develop and implement thresholds of significance criteria and methodologies for evaluating VMT. The City of Ontario has adopted and established a VMT analysis threshold or analysis methodology based on our Policy Plan (General Plan) baseline. However, the Project was submitted prior to the adoption of the threshold and therefore not subject to the adopted thresholds. Subsequently, TOP EIR analyzed VMT, as part of the GHG analysis. The Ontario Plan (TOP) is consistent with the RTP/SCS for the Southern California region. The SBTAM model has incorporated TOP buildout which was then incorporated into the SCAG model in developing the RTP/SCS for the region. The thresholds used in these models can be found in the tool created for SBCTA that analyzes the various threshold options. TOP established VMT thresholds as such this option has already been found to be consistent with the RTP/SCS and these land use assumptions have been incorporated into the SBTAM and SCAG's regional models. The screening tool created for use in San Bernardino County can be utilized for locations within Ontario where additional analysis is not required, and the City thresholds be used for Projects to determine if additional analysis is required. If mitigation measures are included for the Project and the VMT brought down below the established threshold (City average), then the Project can be

determined to have less than a significant impact on transportation (in terms of CEQA). Therefore, impacts with respect to CEQA Guidelines Section 15064.3(b) are less than significant.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

c. Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?

Discussion of Effects: The Project is in an area that is mostly developed, and street improvements are complete. The Project will not create a substantial increase in hazards due to a design feature. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

d. Result in inadequate emergency access?

Discussion of Effects: Development of the Project will be designed to provide access for all emergency vehicles and will therefore not create an inadequate emergency access. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

e. Result in inadequate parking capacity?

Discussion of Effects: The Project proposes to reduce the required parking standards that are established by the Emporia Family Housing PUD. The Project is proposing an average of 1.88 parking spaces per dwelling unit for a total of 94 parking spaces. On November 29, 2018, the California Department of Housing and Community Development (“HCD”) released the guidelines for the Streamlined Ministerial Approval Process created by Senate Bill No. 35 (SB 35 Guidelines). SB 35 provides a streamline ministerial approval process for eligible affordable housing projects. SB 35 allows for local jurisdiction to reduce parking requirements for eligible projects to a maximum of one space per dwelling unit or waive all parking requirements for Project sites located within ½ mile of public transit. Located within the ½ mile of Project site is OmniTrans Bus Route 61 on Holt Boulevard. Therefore, the Project will not create an inadequate parking capacity. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

18. TRIBAL CULTURAL RESOURCES. Would the Project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:

a. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k)?

b. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.

Discussion of Effects: The Project site has been highly disturbed by residential and commercial development since the early 1890s. Surrounding properties and area are highly urbanized. It is highly unlikely that tribal resources would be discovered at the Project site; therefore, it is concluded that the proposed Project will not impact Tribal Cultural Resources or Native American artifacts relating to TCRs and as such, no mitigation measures are recommended.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

19. UTILITIES AND SERVICE SYSTEMS. Would the project:

a. Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?

Discussion of Effects: The Project is served by the City of Ontario sewer system, which has waste treated by the Inland Empire Utilities Agency at the RP-1 or RP-5 treatment plant. The Project is required to meet the requirements of the Ontario Engineering Department regarding wastewater. Therefore, the Project will not result in any new or substantially different utilities and service systems impacts than were previously addressed in TOP EIR, and no changes or additions to TOP EIR analyses are necessary. As discussed in the energy section above, the Project will have no anticipated impacts with regards to electric power and natural gas. In addition, the Project will not have an impact on telecommunications facilities.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

b. Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry, and multiple dry years? In making this determination, the City shall consider whether the project is subject to the water supply assessment requirements of Water Code Section 10910, et seq. (SB 610), and the requirements of Government Code Section 664737 (SB 221).

Discussion of Effects: The Project site is served by the City of Ontario water system. There is currently sufficient water supply available to the City of Ontario to serve this Project as per the findings of the Certified TOP EIR. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

c. Result in a determination by the wastewater treatment provider that serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?

Discussion of Effects: The Project site is served by the City of Ontario water system. There is currently sufficient water supply available to the City of Ontario to serve this Project as per the findings of Certified TOP EIR. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

d. Generate solid waste in excess of state or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?

Discussion of Effects: City of Ontario serves the Project site. Currently, the City of Ontario contracts with a waste disposal company that transports trash to a landfill with sufficient capacity to handle the City's solid waste disposal needs. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to analyses are necessary.

e. Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?

Discussion of Effects: This Project complies with federal, state, and local statutes and regulations regarding solid waste. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

20. WILDFIRE. If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:

a. Substantially impair an adopted emergency response plan or emergency evacuation plan?

Discussion of Effects: The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

b. Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?

Discussion of Effects: The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

c. Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?

Discussion of Effects: The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

d. Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?

Discussion of Effects: The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

21. MANDATORY FINDINGS OF SIGNIFICANCE.

a. Does the project have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?

Discussion of Effects: The proposed Project does not have the potential to reduce wildlife habitat and threaten a wildlife species; therefore, no environmental impacts resulting from the Project are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

b. Does the project have the potential to achieve short-term environmental goals to the disadvantage of long-term environmental goals?

Discussion of Effects: The Project does not have the potential to achieve short-term environmental goals to the disadvantage of long-term environmental goals.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

c. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current project, and the effects of probable future projects.)

Discussion of Effects: The Project does not have impacts that are cumulatively considerable.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

d. Does the project have environmental effects that will cause substantial adverse effects on human beings, either directly or indirectly?

Discussion of Effects: The Project does not have environmental effects that will cause substantial adverse effects on human beings, either directly or indirectly.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

EARLIER ANALYSES

(Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, one or more effects have been adequately analyzed in an earlier EIR or Negative Declaration. Section 15063(c)(3)(D)):

- 1) **Earlier Analyses Used.** Identify earlier analyses used and state where they are available for review.
 - a) The Ontario Plan Final EIR
 - b) The Ontario Plan (TOP)
 - c) City of Ontario Official Zoning Map
 - d) City of Ontario Development Code
 - e) Ontario International Airport Land Use Compatibility Plan
 - f) Ontario International Airport Land Use Compatibility Plan Negative Declaration (SCH 2011011081)

All documents listed above are on file with the City of Ontario Planning Department, 303 East "B" Street, Ontario, California 91764, (909) 395-2036.

- 2) **Impacts Adequately Addressed.** Identify which effects from the above checklist were within the scope of, and adequately analyzed in, an earlier document pursuant to applicable legal standards.

MITIGATION MEASURES

(For effects that are “Less than Significant with Mitigation Incorporated,” describe the mitigation measures, which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.)

The Mitigation Measures contained in the Certified TOP EIR adequately mitigate the impacts of the proposed Project. These mitigation measures are contained in the attached Mitigation Monitoring Program.

No additional mitigation beyond that previously imposed is required.

Mitigation Monitoring Program for The Ontario Plan Environmental Impact Report

1. Introduction

1.1 PURPOSE OF MITIGATION MONITORING PROGRAM

This Mitigation Monitoring Program has been developed to provide a vehicle by which to monitor mitigation measures and conditions of approval outlined in The Ontario Plan Environmental Impact Report (EIR), State Clearinghouse No. 2008101140. The Mitigation Monitoring Program has been prepared in conformance with Section 21081.6 of the Public Resources Code and City of Ontario Monitoring Requirements. Section 21081.6 states:

(a) When making the findings required by paragraph (1) of subdivision subsection (a) of Section 21081 or when adopting a mitigated negative declaration pursuant to paragraph (2) of subdivision (c) of Section 21080, the following requirements shall apply:

(1) The public agency shall adopt a reporting or monitoring program for the changes made to the project or conditions of project approval, adopted in order to mitigate or avoid significant effects on the environment. The reporting or monitoring program shall be designed to ensure compliance during project implementation. For those changes which have been required or incorporated into the project at the request of a responsible agency or a public agency having jurisdiction by law over natural resources affected by the project, that agency shall, if so requested by the lead agency or a responsible agency, prepare and submit a proposed reporting or monitoring program.

(2) The lead agency shall specify the location and custodian of the documents or other material which constitute the record of proceedings upon which its decision is based.

(b) A public agency shall provide that measures to mitigate or avoid significant effects on the environment are fully enforceable through permit conditions, agreements, or other measures. Conditions of project approval may be set forth in referenced documents which address required mitigation measures or, in the case of the adoption of a plan, policy, regulation, or other public project, by incorporating the mitigation measures into the plan, policy, regulation, or project design.

(c) Prior to the close of the public review period for a draft environmental impact report or mitigated negative declaration, a responsible agency, or a public agency having jurisdiction over natural resources affected by the project, shall either submit to the lead agency complete and detailed performance objectives for mitigation measures which would address the significant effects on the environment identified by the responsible agency or agency having jurisdiction over natural resources affected by the project, or refer the lead agency to appropriate, readily available guidelines or reference documents. Any mitigation measures submitted to a lead agency by a responsible agency or an agency having jurisdiction over natural resources affected by the project shall be limited to measures which mitigate impacts to resources which are subject to the statutory authority of, and definitions applicable to, that agency. Compliance or noncompliance by a responsible agency or agency having jurisdiction over natural resources affected by a project with that requirement shall not limit the authority of the responsible agency or agency having jurisdiction over natural resources affected by a project, or the authority of the lead agency, to approve, condition, or deny projects as provided by this division or any other provision of law.

1.2 PROJECT SUMMARY

The proposed project is the preparation of The Ontario Plan, which consists of a Vision, Governance Manual, Policy Plan, City Council Priorities, Implementation Plans, and Tracking and

Feedback. The Ontario Plan integrates components of city governance documents into a single guidance system that shapes the community 20 years or more into the future.

(a) The Ontario **Vision** describes the future community of Ontario. Its basic purpose is to improve the quality of life for the people of Ontario. It is the rationale and motivation for everything the City does.

(b) The **Governance Manual** describes the foundation for conducting the public's business on behalf of the present and future people of Ontario. It explains how The Ontario Plan is a tool for decision-making and communication.

(c) **City Council Priorities** define the short-term direction in City actions and initiatives. They are the primary means for exercising leadership in carrying out The Plan and realizing the Vision.

(d) The **Policy Plan** connects intent with action through the broad range of Goals and Policies that would guide the long-term growth and development required for the City to achieve its Vision. It also satisfies the California Government Code requirement for a general plan. Figure 3-6, *Proposed Land Use Plan*, shows the proposed General Plan land use designations that guide and regulate land use patterns, distributions, densities and intensities in the City of Ontario, including residential employment, retail, recreation, and public uses.

(e) **Implementation** consists of actions taken to carry out Plan policies. This includes initiatives by the City and decisions on public and private development projects.

(f) **Tracking and Feedback** allows the City to learn from experience and redirect efforts.

Pursuant to CEQA Guidelines Section 15064(d), the EIR considers the direct physical changes and reasonably foreseeable indirect physical changes in the environment that would be caused by The Ontario Plan. Consequently, the EIR focuses on impacts from changes to land use associated with buildout of the Proposed Land Use Plan, within the Policy Plan, and impacts from the resultant population and employment growth in the City. The Ontario Plan Proposed Land Use Plan for the ultimate development of the City is not linked to a timeline. However, for the purpose of this environmental analysis, buildout of the Proposed Land Use Plan is forecast for the year 2035.

1.3 PROJECT LOCATION

The City of Ontario is in the southwestern corner of San Bernardino County and is surrounded by the Cities of Chino and Montclair, and unincorporated areas of San Bernardino County to the west; the Cities of Upland and Rancho Cucamonga to the north; the City of Fontana and unincorporated land in San Bernardino County to the east; and unincorporated Riverside County land to the south. The City is in the central part of the Upper Santa Ana River Valley. This portion of the valley is bounded by the San Gabriel Mountains to the north; the Chino Hills, Puente Hills, and San Jose Hills to the west; the Santa Ana River to the south; and Lytle Creek Wash on the east.

The City comprises approximately 50 square miles (31,958 acres), which includes the 8,200-acre New Model Colony (NMC) in the southern portion of the City (formerly the City's Sphere of Influence). The northern urbanized portion of the City is known as the Original Model Colony (OMC). The City is generally bounded by Benson Avenue and Euclid Avenue on the west; Interstate 10 (I-10), 8th Street, and 4th Street on the north; Etiwanda Avenue and Hamner Avenue on the east; and Merrill Avenue and the San Bernardino County/Riverside County boundary on the south. Regional circulation to and through the City is provided by I-10 and State Route 60 (SR-60) east-west, and by I-15 and SR-83 (Euclid Avenue) north-south.

1.4 ENVIRONMENTAL IMPACTS

The environmental document for this project is a “program EIR” as defined by State CEQA Guidelines (Section 15161, California Code of Regulations, Title 14, Division 6, Chapter 3). As provided in Section 15168 of the State CEQA Guidelines, a Program EIR may be prepared on a series of actions that may be characterized as one large project that are related either 1) geographically; 2) as logical parts of a chain of contemplated events; 3) in connection with issuance of rules, regulations, plans, or other general criteria to govern the conduct of a continuing program; or 4) as individual activities carried out under the same authorizing statutory or regulatory authority and have generally similar environmental effects that can be mitigated in similar ways.

Although the legally required contents of a Program EIR are the same as those of a Project EIR, Program EIRs are typically more conceptual and may contain a more general discussion of impacts, alternatives, and mitigation measures than a Project EIR. Once a Program EIR has been prepared, subsequent activities within the program must be evaluated to determine whether an additional CEQA document needs to be prepared. However, if the Program EIR addresses the program’s effects as specifically and comprehensively as possible, many subsequent activities could be found to be within the Program EIR scope and additional environmental documents may not be required (Guidelines Section 15168[c]). When a Program EIR is relied on for a subsequent activity, the lead agency must incorporate feasible mitigation measures and alternatives developed in the Program EIR into the subsequent activities (Guidelines Section 15168[c][1]). If a later activity would have effects that were not examined in the Program EIR, a new Initial Study would need to be prepared leading to either an EIR or a Negative Declaration. Pursuant to Sections 15126.2 and 15126.4 of the State CEQA Guidelines, the EIR should identify any potentially significant adverse impacts and recommend mitigation that would reduce or eliminate these impacts to levels of insignificance.

1.4.1 Impacts Considered Less Than Significant

Ten environmental categories are identified as having less than significant impacts that do not require mitigation. These categories are:

- Aesthetics
- Biological Resources
- Geology/Soils
- Hazards & Hazardous Materials
- Hydrology/Water Quality
- Land Use & Planning
- Mineral Resources
- Population and Housing
- Public Services
- Recreation

1.4.2 Potentially Significant Adverse Impacts That Can Be Mitigated, Avoided, or Substantially Lessened

The following have been identified as potentially resulting in significant adverse impacts that can be mitigated, avoided, or substantially lessened:

- Cultural Resources: Mitigation Measures 5-2 through 5-4 would reduce archeological and prehistoric cultural resource impacts to less than significant.
- Noise: Mitigation Measure 12-3 would ensure that any new vibration-sensitive structures near the Union Pacific Railroad or Southern California Regional Rail Authority right-of-way would be constructed so that train-related vibration would not be perceptible and operational vibration impacts would be less than significant.
- Utilities and Service Systems: Mitigation Measures 17-1 through 17-4 would reduce impacts on water supply and demand from buildout of The Ontario Plan to less than significant.

1.4.3 Unavoidable Significant Adverse Impacts

There are six environmental categories considered to have impacts that would be significant and unavoidable and would not be lessened through mitigation.

Agricultural Resources

Buildout of The Ontario Plan would convert 3,269.3 acres of California Resource Agency designated Prime Farmland, Unique Farmland, and Farmland of Statewide Importance to residential, commercial, mixed-use, and industrial land uses. Consequently, impacts to Farmland would remain significant and unavoidable.

There are a number of Williamson Act contracts within the City that have yet to expire. Buildout of The Ontario Plan would most likely require the cancellation or nonrenewal of these contracts. The current use of these contracts would slow the rate of conversion from agricultural to nonagricultural land, but it would not impede the conversion. Since there are some Williamson Act contracts still active in the New Model Colony, implementation of the proposed land use plan for The Ontario Plan would conflict with these contracts and cause a significant impact. Consequently, impacts to Williamson Act contracts would remain significant and unavoidable.

Development of the City in accordance with The Ontario Plan would increase the amount of nonagricultural land uses. When nonagricultural land uses are placed near agricultural uses, the odors, noises, and other hazards related to agriculture conflict with the activities and the quality of life of the people living and working in the surrounding areas. Consequently, conversion of agricultural uses in the city may cause farms and agricultural land uses outside the City to be converted to nonagricultural uses because of the nuisances related to agriculture and impacts would remain significant and unavoidable.

Air Quality

The project would not be consistent with the Air Quality Management Plan (AQMP) because air pollutant emissions associated with buildout of the City of Ontario would cumulatively contribute to the nonattainment designations in the South Coast Air Basin (SoCAB). Furthermore, buildout of the Proposed Land Use Plan would exceed current estimates of population, employment, and vehicle miles traveled for Ontario and therefore these emissions are not included in the current regional emissions inventory for the SoCAB. As both criteria must be met in order for a project to be considered consistent with the AQMP, the project would be considered inconsistent with the AQMP. Consequently, this impact would remain significant and unavoidable.

Construction activities associated with buildout of The Ontario Plan would generate short-term emissions that exceed the South Coast Air Quality Management District's (SCAQMD) regional significance thresholds; cumulatively contribute to the SoCAB's nonattainment designations for ozone (O₃), coarse inhalable particulate matter (PM₁₀), and fine inhalable particulate matter (PM_{2.5}); and potentially elevate concentrations of air pollutants at sensitive receptors. Mitigation Measure 3-1 would reduce The Ontario Plan's short-term construction-related volatile organic compounds (VOC), carbon monoxide (CO), oxides of nitrogen (NO_x), PM₁₀, and PM_{2.5} emissions but they would not be reduced to levels below the SCAQMD's regional thresholds and they would not reduce these impacts to less than significant. Consequently, construction air pollutant emissions generated by buildout of The Ontario Plan would remain significant and unavoidable.

Buildout of The Ontario Plan would generate long-term emissions that would exceed SCAQMD'S regional significance thresholds and cumulatively contribute to the SoCAB nonattainment designations for O₃, PM₁₀, and PM_{2.5}. Mitigation Measure 3-2 would reduce long-term operational emissions of VOC, CO, NO_x, PM₁₀, and PM_{2.5} related to the buildout of The Ontario Plan but they would not reduce these emissions to levels below the SCAQMD's regional significance thresholds and impacts would not be less than significant. Consequently, operational impacts from buildout of The Ontario Plan would remain significant and unavoidable.

Approval of residential and other sensitive land uses within 500 feet of Interstate-10, Interstate-15, or State Route-60 would result in exposure of persons to substantial concentrations of diesel particulate matter. Mitigation Measure 3-3 would reduce impacts related to the exposure of sensitive receptors (residential and other sensitive land uses) to diesel particulate matter because of their placement near freeways within the City. However, it would not reduce this impact to be less than significant.

Conversion of agricultural land to nonagricultural uses would temporarily expose residents to objectionable odors and impacts would remain significant and unavoidable.

Cultural Resources

Although protective regulations are in place and preservation policies are included in The Ontario Plan, implementation of the Proposed Land Use Plan, especially within growth focus areas, has the potential to impact Tier III historic resources. Mitigation Measure 5-1 would require a historical evaluation for properties within historic resources in the Focus Areas under the City's ordinance. However, the ordinance does not provide a high level of protection for Tier III resources. As a result, demolition of historical resources categorized under the Ordinance as Tier III could potentially be impacted with implementation of the Proposed Land Use Plan. Consequently, Tier III historic resource impacts would remain significant and unavoidable.

Global Climate Change

Buildout of the City of Ontario would generate greenhouse gas emissions that would significantly contribute to global climate change impacts in California. Greenhouse gas (GHG) emissions generated in the City would significantly contribute to climate change impacts in California as a result of the growth in population and employment in the City and scale of development activity associated with buildout of the Proposed Land Use Plan. Mitigation Measures 6-1 through 6-6 would act to reduce the contributions of The Ontario Plan to global climate change but they would not reduce the impacts to less than significant.

Noise

Buildout of the Proposed Land Use Plan would result in an increase in traffic on local roadways in the City of Ontario, which would substantially increase noise levels. Consequently, impacts would remain significant and unavoidable.

Noise-sensitive uses could be exposed to elevated noise levels from transportation sources. Any siting of new sensitive land uses within a noise environment that exceeds the normally acceptable land use compatibility criterion would result in a potentially significant impact and would require a separate noise study through the development review process to determine the level of impacts and required mitigation. Mitigation Measure 12-1 would decrease the exposure of sensitive receptors to excessive noise levels within 65 dBA CNEL contours, whether near Los Angeles/Ontario International Airport (LAONT) or other noise-producing areas such as freeways and railroads, but it would not reduce these impacts to less than significant.

Construction activities associated with buildout of the individual land uses associated with the Proposed Land Use Plan would expose sensitive uses to strong levels of groundborne vibration. Mitigation Measure 12-2 would reduce the impacts caused by construction-related vibrations on sensitive receptors but it would not reduce the impact to less than significant.

Impact 5.12-5. Significant. Construction activities associated with buildout of the individual land uses associated with the Proposed Land Use Plan would substantially elevate noise levels in the vicinity of sensitive land uses. Mitigation Measure 12-4 calls for the use of noise-reducing techniques during construction projects that would impact nearby sensitive receptors, such as the use of temporary sound walls and reduced unnecessary truck idling. However, these impacts would not be reduced to levels considered less than significant.

Noise-sensitive land uses within the 65 dBA CNEL contour of the Los Angeles/Ontario International Airport would be exposed to substantial levels of airport-related noise. Consequently, impacts would remain significant and unavoidable.

Transportation and Traffic

The increased development and population growth associated with the buildout of the Proposed Land Use Plan would cause deficient levels of service at area intersections without implementation of the recommended lane geometry improvements. In addition, buildout of the Proposed Land Use Plan would also cumulatively contribute to the cumulatively significant freeway level of service impact that is already projected to occur in the future. Mitigation Measure 16-1 would require the buildout of The Ontario Plan to be consistent with the traffic study prepared by Kimley-Horn and Associates. This traffic study indicates the appropriate lane geometry for area intersections. This would allow for intersections to have LOS values of E or above, but it would not improve the cumulative freeway LOS standards to appropriate levels. The City has no jurisdiction over Caltrans projects, such as freeway improvements. Therefore, the impacts related to cumulative LOS deficiencies on freeways would not be reduced to levels considered less than significant.

2. Mitigation Monitoring Process

2.1 MITIGATION MONITORING PROGRAM ORGANIZATION

CEQA requires that a reporting or monitoring program be adopted for the conditions of project approval that are necessary to mitigate or avoid significant effects on the environment (Public Resources Code 21081.6). The mitigation monitoring and reporting program is designed to ensure compliance with adopted mitigation measures during project implementation. For each mitigation measure recommended in the Environmental Impact Report, specifications are made herein that identify the action required and the monitoring that must occur. In addition, a responsible agency is identified for verifying compliance with individual conditions of approval contained in the Mitigation Monitoring Program (MMP). In order to effectively track and document the status of mitigation measures, a mitigation matrix has been prepared and includes:

- Responsibility for implementation
- Timing
- Responsibility for monitoring
- Monitor

Mitigation measure timing of verification has been apportioned into several specific timing increments. Of these, the most common are:

- Prior to project approval
- Prior to issuance of grading permit(s)
- During construction

Information pertaining to compliance with mitigation measures or any necessary modifications or refinements will be documented in the comments portion of the matrix.

2.2 MITIGATION MONITORING PROCEDURES

The City of Ontario Planning Department is the designated lead agency for the Mitigation Monitoring and Reporting Program. The City of Ontario includes the Mitigation Measures within the Special Conditions of Approval. The City is responsible for review of all monitoring reports, enforcement actions, and document disposition. The Planning Department shall designate a Project Mitigation Monitor for the proposed project.

2.2.1 In-Field Monitoring

The Responsible Monitoring Party shall exercise caution and professional practices at all times when monitoring construction. Protective wear (hard hats, glasses, etc.) shall be worn at all times in construction areas. Injuries shall be reported immediately to the Project Mitigation Monitor.

2.2.2 Coordination with Contractors

The construction manager/superintendent is responsible for coordination of contractors and for contractor completion of required measures in accordance with the provisions of this program.

2.2.3 Recognized Experts

The use of recognized experts as a component of the monitoring team is required to ensure compliance with scientific and engineering mitigation measures. While the recognized experts assess compliance with required mitigation measures, consultation with the City of Ontario planning staff shall take place in the event of a dispute.

2.2.4 Enforcement

Agencies may enforce conditions of approval through their existing police power, using stop-work orders, fines, infraction citations, loss of entitlements, refusal to issue building permits or certificates of use and occupancy or, in some cases, notice of violation for tax purposes. Criminal misdemeanor sanctions could be available where the agency has adopted an ordinance requiring compliance with the monitoring program, similar to the provision in many zoning ordinances that affirm the enforcement power to bring suit against violators of the ordinances.

3. Mitigation Monitoring Requirements

3.1 CATEGORIZED MITIGATION MEASURES/MATRIX

Project-specific mitigation measures have been categorized in matrix format, as shown in Table 3-1. The matrix identifies the environmental factor, specific mitigation measures, schedule, and responsible monitor. The mitigation matrix will serve as the basis for scheduling the implementation of, and compliance with, all mitigation measures.

3.2 IN-FIELD MONITORING

Project monitors and technical subconsultants shall exercise caution and professional practices at all times when monitoring implementation of mitigation measures. Protective wear (e.g., hard hat, glasses) shall be worn at all times in construction areas. Injuries shall be immediately reported to the mitigation monitoring committee.

3.3 DATABASE MANAGEMENT

All mitigation monitoring reports, letters, and memos shall be prepared using Microsoft Word software on IBM-compatible PCs and processed according to the City's Environmental Compliance Program.

3.4 COORDINATION WITH CONTRACTORS

The construction manager is responsible for coordination of contractors and for contractor completion of required mitigation measures.

3.5 LONG-TERM MONITORING

Long-term monitoring related to several mitigation measures will be required, including fire safety inspections. Post-construction fire inspections are conducted on a routine basis by the Ontario Fire Department.

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
5.3 AIR QUALITY				
<p>3-1 The City of Ontario Building Department shall require that all new construction projects incorporate feasible mitigation measures to reduce air quality emissions. Potential measures shall be incorporated as conditions of approval for a project and may include:</p> <ul style="list-style-type: none"> • Requiring fugitive dust control measures that exceed South Coast Air Quality Management District's Rule 403, such as: <ul style="list-style-type: none"> ○ Requiring use of nontoxic soil stabilizers to reduce wind erosion. ○ Applying water every four hours to active soil-disturbing activities. ○ Tarping and/or maintaining a minimum of 24 inches of freeboard on trucks hauling dirt, sand, soil, or other loose materials. • Using construction equipment rated by the United States Environmental Protection Agency as having Tier 3 or higher exhaust emission limits. • Ensuring construction equipment is properly serviced and maintained to the manufacturer's standards. • Limiting nonessential idling of construction equipment to no more than five consecutive minutes. • Using Super-Compliant VOC paints for coating of architectural surfaces whenever possible. A list of Super-Compliant architectural coating manufactures can be found on the South Coast Air Quality Management District's website at: 	<p>City of Ontario Building Department in coordination with the landowner/project applicant's construction contractor</p>	<p>During construction</p>	<p>City of Ontario Building and Department and Developer/Contractor</p>	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
http://www.aqmd.gov/prdas/brochures/Super-Compliant_AIM.pdf .				
3-2 The City of Ontario shall evaluate new development proposals within the City and require all developments to include access or linkages to alternative modes of transportation, such as transit stops, bike paths, and/or pedestrian paths (e.g., sidewalks).	City of Ontario Planning/Engineering Department in coordination with the landowner/project applicant	Prior to individual project approvals	City of Ontario Planning Department	
3-3 The City of Ontario shall evaluate new development proposals within the City for potential incompatibilities with regard to the California Air Resources Board's <i>Air Quality and Land Use Handbook: A Community Health Perspective</i> (April 2005). New development that is inconsistent with the recommended buffer distances shall only be approved if feasible mitigation measures, such as high efficiency Minimum Efficiency Reporting Value filters have been incorporated into the project design to protect future sensitive receptors from harmful concentrations of air pollutants as a result of proximity to existing air pollution sources.	City of Ontario Planning Department in coordination with the landowner/project applicant	Prior to individual project approvals	City of Ontario Planning Department	
5.5 CULTURAL RESOURCES				
5-1 Historic or potentially historic resources in the City shall be evaluated for historic significance through the City's tier system prior to the issuance of plan or development approvals.	City of Ontario Planning Department	Prior to individual project approvals	City of Ontario Planning Department	
5-2 In areas of documented or inferred archaeological and/or paleontological resource presence, City staff shall require applicants for development permits to provide studies to document the presence/absence of such resources. On properties where resources are identified, such studies shall provide a detailed mitigation plan, including a monitoring	City of Ontario Planning Department in coordination with the Landowner/Project Applicant	Prior to individual project approvals	City of Ontario Planning Department	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>program and recovery and/or in situ preservation plan, based on the recommendations of a qualified cultural preservation expert. The mitigation plan shall include the following requirements:</p> <ul style="list-style-type: none"> • Archaeologists and/or paleontologist shall be retained for the project and will be on call during grading and other significant ground-disturbing activities. • Should any cultural resources be discovered, no further grading shall occur in the area of the discovery until the Planning Director or designee is satisfied that adequate provisions are in place to protect these resources. • Unanticipated discoveries shall be evaluated for significance by a San Bernardino County Certified Professional Archaeologist/Paleontologist. If significance criteria are met, then the project shall be required to perform data recovery, professional identification, radiocarbon dates, and other special studies; submit materials to a museum for permanent curation; and provide a comprehensive final report including catalog with museum numbers. 				
<p>5-3 Upon receipt of an application for a Specific Plan or a project that requires a General Plan amendment subject to CEQA and is within the City's jurisdiction, the City's representative shall consult with the relevant tribe(s)' representative(s) to determine if the proposed project is within a culturally sensitive area to the tribe. If sufficient evidence is provided to reasonably ascertain that the site is within a [tribal] culturally sensitive area, then a cultural resources assessment prepared by an archaeologist shall be required. The findings of the cultural resources assessment shall be incorporated into the CEQA documentation. A copy of the report shall be forwarded to the tribe(s). If mitigation is recommended in the CEQA</p>	<p>City of Ontario Planning Department</p>	<p>Prior to individual project approvals</p>	<p>City of Ontario Planning Department</p>	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
document, the procedure described in Mitigation Measure 5-4 shall be followed.				
5-4 Prior to the issuance of grading permits for a Specific Plan or project that requires a General Plan amendment for which the CEQA document defines cultural resource mitigation for potential tribal resources, the project applicant shall contact the designated tribe(s) to notify them of the grading, excavation, and monitoring program. The applicant shall coordinate with the City of Ontario and the tribal representative(s) to develop mitigation measures that address the designation, responsibilities, and participation of tribal monitors during grading, excavation, and ground-disturbing activities; scheduling; terms of compensation; and treatment and final disposition of any cultural resources, sacred sites, and human remains discovered on the site. The City of Ontario shall be the final arbiter of the conditions for projects within the City's jurisdiction.	City of Ontario Planning Department in coordination with the Landowner/Project Applicant	Prior to issuance of grading permit(s)	City of Ontario Planning Department	
5.6 Global Climate Change				
6-1 The City of Ontario shall prepare a Climate Action Plan within 18 months after adopting The Ontario Plan. The goal of the Climate Action Plan shall be to reduce GHG emissions from all activities within the City boundaries to support the State's efforts under AB 32 and to mitigate the impact of climate change on the City, State, and world. Once completed, the City shall update The Ontario Plan and associated policies, as necessary, to be consistent with the Climate Action Plan and prepare a subsequent or supplemental Environmental Impact Report, if new significant impacts are identified. The Climate Action Plan shall include the following: <ul style="list-style-type: none"> • Emission Inventories: The City shall establish GHG emissions inventories including emissions from all sectors within the City, using methods approved by, or 	City of Ontario	Within 18 months of adopting The Ontario Plan	City of Ontario Planning Department/ Municipal Utilities Agency (MUA)	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>consistent with guidance from, the CARB; the City shall update inventories every 3 years or as determined by state standards to incorporate improved methods, better data, and more accurate tools and methods, and to assess progress. If the City is not on-schedule to achieve the GHG reduction targets, additional measured shall be implemented, as identified in the CAP.</p> <ul style="list-style-type: none"> ○ The City shall establish a baseline inventory of GHG emissions including municipal emissions, and emissions from all business sectors and the community. ○ The City shall define a “business as usual” scenario of municipal, economic, and community activities, and prepare a projected inventory for 2020 based on that scenario. <ul style="list-style-type: none"> ● Emission Targets: The City will develop Plans to reduce or encourage reductions in GHG emissions from all sectors within the City: <ul style="list-style-type: none"> ○ A Municipal Climate Action Plan which shall include measures to reduce GHG emissions from municipal activities by at least 30 percent by 2020 compared to the "business as usual" municipal emissions (including any reductions required by the California Air Resource Board under AB 32. ○ A Business Climate Action Plan in collaboration with the business community, which shall include measures to reduce GHG emissions from business activities, and which shall seek to reduce emissions by at least 30 percent by 2020 compared to "business as usual" business emissions. ○ A Community Climate Action Plan in collaboration with the stakeholders from the community at large, which shall include measures reduce GHG 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
emissions from community activities, and which shall seek to reduce emissions by at least 30 percent by 2020 compared to "business as usual" community emissions.				
<p>6-2 The Climate Action Plan shall include specific measures to achieve the GHG emissions reduction targets identified in Mitigation Measure 6-1. The Climate Action Plan shall quantify the approximate greenhouse gas emissions reductions of each measure and measures shall be enforceable. Measures listed below, along with others, shall be considered during the development of the Climate Action Plan (CAP):</p> <ul style="list-style-type: none"> • Require all new or renovated municipal buildings to seek Silver or higher Leadership in Energy and Environmental Design (LEED) standard, or compliance with similar green building rating criteria. • Require all municipal fleet purchases to be fuel efficient vehicles for their intended use based on the fuel type, design, size, and cost efficiency. • Require that new development projects in Ontario that require demolition prepare a demolition plan to reduce waste by recycling and/or salvaging a nonhazardous construction and demolition debris. • Require that new developments design buildings to be energy efficient by siting buildings to take advantage of shade, prevailing winds, landscaping, and sun screening to reduce energy required for cooling. • Require that cool roofs for non-residential development and cool pavement to be incorporated into the site/building design for new development where appropriate. 	City of Ontario Planning Department	Within 18 months of adopting The Ontario Plan	City of Ontario Planning Department	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Evaluate the feasibility of implementing a Public Transit Fee to support Omnitrans in developing additional transit service in the City. • Require diesel emission reduction strategies to eliminate and/or reduce idling at truck stops, warehouses, and distribution facilities throughout the City. • Install energy efficient lighting and lighting control systems in all municipal buildings. • Require all new traffic lights installed be energy efficient traffic signals. • Require the use of reclaimed water for landscape irrigation in all new development and on public property where such connections are within the service boundaries of the City's reclaimed water system. • Require all new landscaping irrigation systems installed within the City to be automated, high-efficient irrigation systems to reduce water use and require use of bubbler irrigation; low-angle, low-flow spray heads; or moisture sensors. • Conduct energy efficiency audits of existing municipal buildings by checking, repairing, and readjusting heating, ventilation, and air conditioning systems, lighting, water heating equipment, insulation, and weatherization. • Ensure that its local Climate Action, Land Use, Housing, and Transportation Plans are aligned with, support, and enhance any regional plans that have been developed consistent with state guidance to achieve reductions in GHG emissions. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Mitigate climate change by decreasing heat gain from pavement and other hard surfaces associated with infrastructure. • Reduce heat gain from pavement and other similar hardscaping. • Work with appropriate agencies to create an interconnected transportation system that allows a shift in travel from private passenger vehicles to alternative modes, including public transit, ride sharing, car-sharing, bicycling and walking. • Provide safe and convenient access for pedestrians and bicyclists to, across, and along major transit priority streets. • Facilitate employment opportunities that minimize the need for private vehicle trips, by: <ul style="list-style-type: none"> ○ Amending zoning ordinances and the Development Code to include live/work sites and satellite work centers in appropriate locations. ○ Encouraging telecommuting options with new and existing employers, through project review and incentives, as appropriate. • Establish policies and programs to reduce onsite parking demand and promote ride-sharing and public transit at large events. • Support and promote the use of low-and zero-emission vehicles, by: <ul style="list-style-type: none"> ○ Encouraging the necessary infrastructure to facilitate the use of zero- emission vehicles and clean alternative fuels, such as electric vehicle charging facilities and conveniently located alternative fueling stations. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Encouraging new construction to include vehicle access to properly wired outdoor receptacles to accommodate ZEV and/or plug in electric hybrids (PHEV). ○ Encouraging transportation fleet standards to achieve the lowest emissions possible, using a mix of alternate fuels, PZEV or better fleet mixes. ○ Establishing incentives, as appropriate, to taxicab owners to use alternative fuel or gas-electric hybrid vehicles. ● Establish green building requirements and standards for new development and redevelopment projects, and work to provide incentives for green building practices and remove barriers that impede their use. ● Allow increased height limits and/or flexibility in other standards for projects that incorporate energy efficient green building practices where not prohibited by Airport Land Use Compatibility Plan (ALUCP)/Federal Aviation Administration (FAA). ● Identify and remove regulatory or procedural barriers to implementing green building practices within its jurisdiction, such as updating codes, guidelines, and zoning, and ensure that all plan review and building inspection staff are trained in green building materials, practices, and techniques. ● Support the use of green building practices by: <ul style="list-style-type: none"> ○ Providing information, marketing, training, and technical assistance about green building practices. ○ Adopting a Green Building ordinance with guidelines for green building practices in residential and commercial development. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Adopt energy efficiency performance standards for buildings designed to achieve a greater reduction in energy and water use than currently required by state law, including: <ul style="list-style-type: none"> ○ Standards for the installation of "cool roofs". ○ Standards for improved overall efficiency of lighting systems. ○ Requirements for the use of Energy Star appliances and fixtures in discretionary new development. ● Encourage the performance of energy audits for residential and commercial buildings prior to completion of sale, and that audit results and information about opportunities for energy efficiency improvements be presented to the buyer. ● Establish policies and programs that facilitate the siting of new renewable energy generation. ● Require that any building constructed in whole or in part with City funds incorporate passive solar design features, such as daylighting and passive solar heating, where feasible. ● Prepare and implement a comprehensive plan to improve energy efficiency of municipal facilities, including: <ul style="list-style-type: none"> ○ Conducting energy audits. ○ Retrofitting municipal facilities for energy efficiency where feasible and when remodeling or replacing components, including increased insulation, installing green or reflective roofs and low-emissive window glass. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Implementing an energy tracking and management system for its municipal facilities. ○ Installing energy-efficient exit signs, street signs, and traffic lighting, subject to life/safety considerations. ○ Installing energy-efficient lighting retrofits and occupancy sensors, and institute a "lights out at night" policy, subject to life/safety considerations. ○ Retrofitting heating and cooling systems to optimize efficiency (e.g., replace chillers, boilers, fans, pumps, belts, etc.). ○ Installing Energy Star® appliances and energy-efficient vending machines. ○ Improving water use efficiency, including a schedule to replace or retrofit system components with high- efficiency units (i.e., ultra-low-flow toilets, fixtures, etc.). ○ Installing irrigation control systems which maximize water use efficiency and minimize off- peak use. ○ Adopting an accelerated replacement schedule for energy inefficient systems and components. ● Ensure that staff receives appropriate training and support to implement objectives and policies to reduce GHG emissions, including: <ul style="list-style-type: none"> ○ Providing energy efficiency training to design, engineering, building operations, and maintenance staff. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Providing information on energy use and management, including data from the tracking and management system, to managers and others making decisions that influence energy use. ○ Providing energy design review services to departments undertaking new construction or renovation projects, to facilitate compliance with LEED standards. • Maximize efficiency at drinking water treatment, pumping, and distribution facilities, including development of off-peak demand schedules for heavy commercial and industrial users. • Establish a replacement policy and schedule to replace fleet vehicles and equipment with the most fuel-efficient vehicles practical, including gasoline hybrid and alternative fuel or electric models. • Require the installation of outdoor electrical outlets on buildings to support the use, where practical, of electric lawn and garden equipment, and other tools that would otherwise be run with small gas engines or portable generators. • Implement measures to reduce employee vehicle trips and to mitigate emissions impacts from municipal travel. • Conduct a comprehensive inventory and analysis of the urban forest, and coordinate tree maintenance responsibilities with all responsible departments, consistent with best management practices. • Evaluate existing landscaping and options to convert reflective and impervious surfaces to landscaping and will install or replace vegetation with drought-tolerant, low- maintenance native species or edible 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>landscaping that can also provide shade and reduce heat-island effects.</p> <ul style="list-style-type: none"> • Implement enhanced programs to divert solid waste from landfill operations, by: <ul style="list-style-type: none"> ○ Establishing a diversion target which meets or exceeds AB 939 requirements. ○ Promoting and expanding recycling programs, purchasing policies, and employee education to reduce the amount of waste produced. • Reduce per capita water consumption consistent with state law by 2020. • Establish a water conservation plan that may include such policies and actions as: <ul style="list-style-type: none"> ○ Maintaining and refining the City's tiered rate structure for water use. ○ Establishing restrictions on time of use for landscape watering, or other demand management strategies. ○ Establishing performance standards for irrigation equipment and water fixtures, consistent with state law. • Establish programs and policies to increase the use of recycled water, including: <ul style="list-style-type: none"> ○ Promoting the use of recycled water for agricultural, industrial, and irrigation purposes, including grey water systems for residential irrigation. • Ensure that building standards and permit approval processes promote and support water conservation, by: 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Establishing building design guidelines and criteria to promote water-efficient building design, including minimizing the amount of non-roof impervious surfaces around the building(s). ○ Establishing menus and checklists for developers and contractors to ensure water-efficient infrastructure and technology are used in new construction, including low- flow toilets and shower heads, moisture-sensing irrigation, and other such advances. • Organize workshops on waste reduction activities for the home or business, such as backyard composting, or office paper recycling, and shall schedule recycling drop-off events and neighborhood chipping/mulching days. • Organize workshops on steps to increase energy efficiency in the home or business, such as weatherizing the home or building envelope, installing smart lighting systems, and how to conduct a self-audit for energy use and efficiency. 				
<p>6-3 The City of Ontario will amend the Municipal Code within 18 months after adopting The Ontario Plan, with provisions implementing the following GHG emission reduction concepts:</p> <ul style="list-style-type: none"> • Increase densities in urban core areas to support public transit, by, among other means: <ul style="list-style-type: none"> ○ Removing barriers to the development of accessory dwelling units in existing residential neighborhoods. • Reduce required road width standards wherever feasible to calm traffic and encourage alternative modes of transportation. 	City of Ontario Planning Department	Within 18 months of adopting The Ontario Plan	City of Ontario Planning Department	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Add bicycle facilities to city streets and public spaces, where feasible. • Promote infill, mixed-use, and higher density development, and provide incentives to support the creation of affordable housing in mixed use zones. • Plan for and create incentives for mixed-use development. • Identify sites suitable for mixed-use development and establish appropriate site- specific standards to accommodate mixed uses which could include: <ul style="list-style-type: none"> ○ Increasing allowable building height or allow height limit bonuses, in appropriate areas and where safe to do so. ○ Allowing flexibility in applying development standards (such as FAR2 and lot coverage) based on the location, type, and size of the units, and the design of the development. ○ Allowing reduced and shared parking based on the use mix, and availability of and proximity to public transit stops. ○ Allowing for tandem parking, shared parking and off-site parking leases. • Enable prototype mixed-use structures for use in neighborhood center zones that can be adapted to new uses over time with minimal internal remodeling. • Identify and facilitate the inclusion of complementary land uses not already present in local zoning districts, such as supermarkets, parks and recreational fields, schools in neighborhoods, and residential uses in business districts, to reduce the vehicle miles traveled and promote bicycling and walking to these uses. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Revise zoning ordinance(s) to allow local-serving businesses, such as childcare centers, restaurants, banks, family medical offices, drug stores, and other similar services near employment centers to minimize midday vehicle use. • Develop form-based community design standards to be applied to development projects and land use plans, for areas designated mixed-use. • Implement a Housing Overlay Zone for residential properties at transit centers and along transit corridors. This may include average minimum residential densities of 25 units per acre within one quarter miles of transit centers; average minimum densities of 15 units per acre within one quarter mile of transit corridors; and minimum FAR of 0.5:1 for non- residential uses within a quarter mile of transit centers or corridors. • Identify transit centers appropriate for mixed-use development, and promote transit-oriented, mixed-use development within these targeted areas, by: <ul style="list-style-type: none"> ○ Providing maximum parking standards and flexible building height limitations. ○ Providing density bonus programs. ○ Establishing guidelines for private and public spaces for transit-oriented and mixed-use development. ○ Discouraging auto-oriented development. • Ensure new development is designed to make public transit a viable choice for residents, including: <ul style="list-style-type: none"> ○ Locating medium to high density development near activity centers that can be served efficiently 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>by public transit and alternative transportation modes.</p> <ul style="list-style-type: none"> ○ Locating medium to high density development near streets served by public transit whenever feasible. ○ Linking neighborhoods to bus stops by continuous sidewalks or pedestrian paths. ● Develop form-based community design standards to be applied to development projects and land use plans, for areas designated mixed-use. ● Create and preserve distinct, identifiable neighborhoods whose characteristics support pedestrian travel, especially within, but not limited to, mixed-use and transit-oriented development areas, by: <ul style="list-style-type: none"> ○ Designing or maintaining neighborhoods where the neighborhood amenities can be reached in approximately five minutes of walking. ○ Encouraging pedestrian-only streets and/or plazas within developments, and destinations that may be reached conveniently by public transportation, walking, or bicycling. ○ Allowing flexible parking strategies in neighborhood activity centers to foster a pedestrian-oriented streetscape. ○ Providing continuous sidewalks with shade trees and landscape strips to separate pedestrians from traffic. ○ Encouraging neighborhood parks and recreational centers near concentrations of residential areas (preferably within one quarter 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>mile) and include pedestrian walkways and bicycle paths that encourage non- motorized travel.</p> <ul style="list-style-type: none"> • Ensure pedestrian access to activities and services, especially within, but not limited to, mixed-use and transit- oriented development areas, by: <ul style="list-style-type: none"> ○ Ensuring new development that provides pedestrian connections in as many locations as possible to adjacent development, arterial streets, and thoroughfares. ○ Ensuring a balanced mix of housing, workplaces, shopping, recreational opportunities, and institutional uses, including mixed-use structures. ○ Locating schools in neighborhoods, within safe and easy walking distances of residences served. ○ Encouraging new development in which primary entrances are pedestrian entrances, with automobile entrances and parking located to the rear. ○ Supporting development where automobile access to buildings does not impede pedestrian access, by consolidating driveways between buildings or developing alley access. ○ Utilizing street parking as a buffer between sidewalk pedestrian traffic and the automobile portion of the roadway. ○ Prioritizing the physical development of pedestrian connectors for existing areas that do not meet established connectivity standards. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Mitigate climate change by decreasing heat gain from pavement and other hard surfaces associated with infrastructure. • Reduce heat gain from pavement and other similar hardscaping, by: <ul style="list-style-type: none"> ○ Including low-water landscaping in place of hardscaping around transportation infrastructure and in parking areas. ○ Establishing standards that provide for pervious pavement options. ○ Removing obstacles to natural, drought tolerant landscaping and low-water landscaping. • Coordinate with appropriate agencies to create an interconnected transportation system that allows a shift in travel from private passenger vehicles to alternative modes, including public transit, ride sharing, car-sharing, bicycling and walking, including, but not limited to: <ul style="list-style-type: none"> ○ Providing safe and convenient access for pedestrians and bicyclists to, across, and along major transit priority streets. • Upgrade and maintain the following transit system infrastructure to enhance public use, including: <ul style="list-style-type: none"> ○ Ensuring transit stops and bus lanes are safe, convenient, clean and efficient. ○ Ensuring transit stops have clearly marked street-level designation and are accessible. ○ Ensuring transit stops are safe, sheltered, benches are clean, and lighting is adequate. ○ Working with transit providers to place transit stations along transit corridors within mixed-use or 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>transit- oriented development areas at intervals appropriate for the mode of transit.</p> <ul style="list-style-type: none"> • Facilitate employment opportunities that minimize the need for private vehicle trips, by: <ul style="list-style-type: none"> ○ Amending zoning ordinances and the Development Code to include live/work sites and satellite work centers in appropriate locations. ○ Encouraging telecommuting options with new and existing employers, through project review and incentives, as appropriate. • Establish standards for new development and redevelopment projects to support bicycle use, including: <ul style="list-style-type: none"> ○ Amending the Development Code to include standards for pedestrian and bicyclist accommodations, including: <ul style="list-style-type: none"> ➤ Providing access for pedestrians and bicyclist to public transportation through construction of dedicated paths, where feasible. ○ Requiring new development and redevelopment projects to include bicycle facilities, as appropriate with the new land use, including: <ul style="list-style-type: none"> ➤ Where feasible, promote the construction of weatherproof bicycle facilities and at a minimum, provide bicycle racks or covered, secure parking near the building entrances. • Establish a network of multi-use trails to facilitate direct off- street bicycle and pedestrian travel and will provide bike racks along these trails at secure, lighted locations. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Establish policies and programs to reduce onsite parking demand and promote ride-sharing and public transit at large events. • Require new commercial and retail developments to provide prioritized parking for electric vehicles and vehicles using alternative fuels. • Support and promote the use of low-and zero-emission vehicles (NEV), by: <ul style="list-style-type: none"> ○ Encouraging the necessary infrastructure to facilitate the use of zero- emission vehicles and clean alternative fuels, such as electric vehicle charging facilities and conveniently located alternative fueling stations. ○ Encouraging new construction to include vehicle access to properly wired outdoor receptacles to accommodate ZEV and/or plug in electric hybrids (PHEV). ○ Encouraging transportation fleet standards to achieve the lowest emissions possible, using a mix of alternate fuels, PZEV or better fleet mixes. ○ Establishing incentives, as appropriate, to taxicab owners to use alternative fuel or gas-electric hybrid vehicles. • Establish green building requirements and standards for new development and redevelopment projects, and work to provide incentives for green building practices and remove barriers that impede their use. • Allow increased height limits and/or flexibility in other standards for projects that incorporate energy efficient green building practices where not prohibited by ALUCP/FAA. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Identify and remove regulatory or procedural barriers to implementing green building practices within its jurisdiction, such as updating codes, guidelines, and zoning, and ensure that all plan review and building inspection staff are trained in green building materials, practices, and techniques. • Support the use of green building practices by: <ul style="list-style-type: none"> ○ Establishing guidelines for green building practices in residential and commercial development. ○ Providing incentives, which may include reduction in development fees, administrative fees, and/or expedited permit processing for projects that use green building practices. • Adopt energy efficiency performance standards for buildings that achieve a greater reduction in energy and water use than otherwise required by current state law, including: <ul style="list-style-type: none"> ○ Standards for the installation of "cool roofs". ○ Standards for improved overall efficiency of lighting systems. ○ Requirements for the use of Energy Star appliances and fixtures in discretionary new development. ○ Requirements for new residential lots and/or structures to be arranged and oriented to maximize effective use of passive solar energy. • Require that affordable housing development incorporate energy efficient design and features to the maximum extent feasible. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Identify possible sites for production of renewable energy (such as solar, wind, small hydro, and biogas). • Identify and remove or otherwise address barriers to renewable energy production, including: <ul style="list-style-type: none"> ○ Reviewing and revising building and development codes, design guidelines, and zoning ordinances to remove renewable energy production barriers. ○ Working with related agencies, such as fire, water, health and others that may have policies or requirements that adversely impact the development or use of renewable energy technologies. ○ Developing protocols for safe storage of renewable and alternative energy products with the potential to leak, ignite or explode, such as biodiesel, hydrogen, and/or compressed air. • Allow renewable energy projects in areas zoned for open space, where consistent with the Land Use element, and other uses and values. • Promote and encourage renewable energy generation, and co-generation projects where feasible and appropriate. • Require that, where feasible, all new buildings be constructed to allow for easy, cost-effective installation of solar energy systems in the future, using such “solar-ready” features as: <ul style="list-style-type: none"> ○ Optimal roof orientation (between 20 to 55 degrees from the horizontal), with sufficient south-sloped roof surface, where such buildings architecture and construction are designed for sloped roofs. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Clear access without obstructions (chimneys, heating and plumbing vents, etc.) on the south sloped roof. ○ Roof framing that will support the addition of solar panels. ○ Installation of electrical conduit to accept solar electric system wiring. ○ Installation of plumbing to support a solar hot water system and provision of space for a solar hot water storage tank. • Require that any building constructed in whole or in part with City funds incorporate passive solar design features, such as daylighting and passive solar heating, where feasible. • Prepare and implement a comprehensive plan to improve energy efficiency of municipal facilities, including: <ul style="list-style-type: none"> ○ Conducting energy audits. ○ Retrofitting municipal facilities for energy efficiency where feasible and when remodeling or replacing components, including increased insulation, installing green or reflective roofs and low-emissive window glass. ○ Implementing an energy tracking and management system for its municipal facilities. ○ Installing energy-efficient exit signs, street signs, and traffic lighting, subject to life/safety considerations. ○ Installing energy-efficient lighting retrofits and occupancy sensors, and institute a "lights out at night" policy, subject to life/safety considerations. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Retrofitting heating and cooling systems to optimize efficiency (e.g., replace chillers, boilers, fans, pumps, belts, etc.). ○ Installing Energy Star® appliances and energy-efficient vending machines. ○ Improving water use efficiency, including a schedule to replace or retrofit system components with high- efficiency units (i.e., ultra-low-flow toilets, fixtures, etc.). ○ Installing irrigation control systems maximizing water use efficiency and minimizing off- peak use. ○ Adopting an accelerated replacement schedule for energy inefficient systems and components. ● Require that any newly constructed, purchased, or leased municipal space meet minimum standards, such as: <ul style="list-style-type: none"> ○ The Energy Star® New Homes Program established by U.S. EPA. ○ The incorporation of passive solar design features in new buildings, including daylighting and passive solar heating. ● Reduce per capita water consumption consistent with state law by 2020. ● Establish a water conservation plan that may include such policies and actions as: <ul style="list-style-type: none"> ○ Maintaining and refining the City's tiered rate structure for water use. ○ Establishing restrictions on time of use for landscape watering, or other demand management strategies. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Establishing performance standards for irrigation equipment and water fixtures, consistent with State Law. • The City will establish programs and policies to increase the use of recycled water, including: <ul style="list-style-type: none"> ○ Promoting the use of recycled water for agricultural, industrial, and irrigation purposes, including grey water systems for residential irrigation. • Ensure that building standards and permit approval processes promote and support water conservation, by: <ul style="list-style-type: none"> ○ Establishing building design guidelines and criteria to promote water-efficient building design, including minimizing the amount of non-roof impervious surfaces around the building(s). ○ Establishing menus and check-lists for developers and contractors to ensure water-efficient infrastructure and technology are used in new construction, including low-flow toilets and shower heads, moisture-sensing irrigation, and other such advances. • Install water-efficient landscapes and irrigation, including: <ul style="list-style-type: none"> ○ Requiring planting drought-tolerant and native species and covering exposed dirt with moisture-retaining mulch or other materials such as decomposed granite. ○ Requiring the installation of water-efficient irrigation systems and devices, including advanced technology such as moisture-sensing irrigation controls. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Promote the planting of shade trees and establish shade tree guidelines and specifications, including: <ul style="list-style-type: none"> ○ Establishing guidelines for tree planting based on the land use (residential, commercial, parking lots, etc.). ○ Establishing guidelines for tree types based on species size, branching patterns, whether deciduous or evergreen, whether roots are invasive, etc. ○ Establishing tree guidelines for placement, including distance from structures, density of planting, and orientation relative to structures and the sun. • Develop an Urban Forestry Program to consolidate policies and ordinances regarding tree planting, maintenance, and removal, including: <ul style="list-style-type: none"> ○ Establishing guidelines for tree planting, including criteria for selecting deciduous or evergreen trees low-VOC- producing trees, and emphasizing the use of drought- tolerant native trees and vegetation. 				
6-4 Measures listed in Mitigation Measure 6-2 and 6-3 shall be considered by the City while reviewing all new development, as appropriate, between the time of adoption of The Ontario Plan and adoption of the Climate Action Plan (CAP).	City of Ontario Planning Department	Prior to adoption of the Climate Action Plan	City of Ontario Planning Department	
6-5 Pursuant to a goal of overall consistency with the Sustainable Communities Strategies, the City of Ontario shall evaluate new development for consistency with the development pattern set forth in the Sustainable Communities Strategies plan, upon adoption of the plan by the Southern California Association of Governments.	City of Ontario Planning Department	Prior to individual project approvals	City of Ontario Planning Department	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
6-6 The City of Ontario shall participate in the County of San Bernardino's Green Valley Initiative.	City of Ontario Planning Department	Prior to individual project approvals	City of Ontario Planning Department	
5.12 NOISE				
12-1 Prior to the issuance of building permits for any project that involves a noise-sensitive use within the 65 dBA CNEL contour along major roadways, freeways, railroads, or the Los Angeles/Ontario International Airport, the project property owner/developers shall retain an acoustical engineer to conduct an acoustic analysis and identify, where appropriate, site design features (e.g., setbacks, berms, or sound walls) and/or required building acoustical improvements (e.g., sound transmission class rated windows, doors, and attic baffling), to ensure compliance with the City's Noise Compatibility Criteria and the California State Building Code and California Noise Insulation Standards (Title 24 and 21 of the California Code of Regulations).	City of Ontario Planning/Building Department in coordination with the Landowner/Project Applicant	Prior to individual project approvals	City of Ontario Planning/Building Department	
12-2 Individual projects that involve vibration-intensive construction activities, such as pile drivers, jack hammers, and vibratory rollers, occurring near sensitive receptors shall be evaluated for potential vibration impacts. If construction-related vibration is determined to be perceptible at vibration-sensitive uses (i.e., exceed the Federal Transit Administration vibration-annoyance criteria of 78 VdB during the daytime), additional requirements, such as use of less vibration intensive equipment or construction techniques, shall be implemented during construction (e.g., drilled piles to eliminate use of vibration-intensive pile driver).	City of Ontario Planning/Building/MUA Department in coordination with the Landowner/Project Applicant's construction contractor	During construction	City of Ontario Building/MUA Department	
12-3 Prior to the issuance of building permits for any project that involves a vibration-sensitive use directly adjacent to the Union Pacific Railroad or Southern California Regional Rail Authority main lines shall retain an acoustical engineer to evaluate potential for trains to create perceptible levels of	City of Ontario Planning/Building Department with collaboration with the Landowner/Project	Prior to individual project approvals	City of Ontario Building Department	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
vibration indoors. If vibration- related impacts are found, mitigation measures, such as use of concrete, iron, or steel, or masonry materials to ensure that levels of vibration amplification are within acceptable limits to building occupants, shall be implemented. Pursuant to the Federal Transit Administration vibration-annoyance criteria, these acceptable limits are 78 VdB during the daytime and 72 VdB during the nighttime for residential uses, 84 VdB for office uses, and 90 VdB for workshops.	Applicant			
12-4 Construction activities associated with new development that occurs near sensitive receptors shall be evaluated for potential noise impacts. Mitigation measures such as installation of temporary sound barriers for adjacent construction activities that occur adjacent to occupied noise-sensitive structures, equipping construction equipment with mufflers, and reducing non-essential idling of construction equipment to no more than five minutes shall be incorporated into the construction operations to reduce construction-related noise to the extent feasible.	City of Ontario Building/Planning/MUA Department in coordination with the Landowner/Project Applicant's construction contractor	During construction	City of Ontario Building/Planning/MUA Department	
5.16 TRANSPORTATION AND CIRCULATION				
16-1 The Mobility Element of the Ontario Plan shall be consistent with the traffic study prepared by Kimley-Horn and Associates in 2009. Table 5.16-6 in Section 5.16, <i>Transportation and Traffic</i> , shows the recommended lane geometry for the Proposed Land Use Plan.	City of Ontario Engineering/Planning Department	Ongoing	City of Ontario Engineering/Planning Department	
5.17 UTILITIES AND SERVICE SYSTEMS				
17-1 The City shall include a policy in the Policy Plan that requires water conservation measures for development projects to improve water use efficiency and reduce overall water demand. Reduce potable water demand, through conservation measures, including but not limited to:	City of Ontario Planning/MUA Department	Ongoing	City of Ontario Planning/MUA/Engineering Department	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Work cooperatively with all developers to incorporate conservation measures into project designs (such as those recommended by the California Urban Water Conservation Council). • Continue to develop and implement drought contingency plans to assist citizens and businesses reduce water use during water shortages and emergencies. • Revise the City Code to include a Water-Efficient Landscape Ordinance to encourage or, as appropriate, require the use of water-efficient landscaping consistent with AB 1881. 				
<p>17-2 The City shall include a policy in the Policy Plan that maximizes the use of recycled water as an irrigation (nonpotable) source for landscaping, parks, and other irrigation opportunities in all areas of the City and requires use of recycled water in dual-system office and industrial uses in selected urban areas of the City, where available and feasible.</p>	<p>City of Ontario Planning Department in coordination with City of Ontario MUA/Engineering Department</p>	<p>Ongoing</p>	<p>City of Ontario Planning/MUA/Engineering Department</p>	
<p>17-3 The City shall include a policy in the Policy Plan that the City participate through the Chino Basin Water Master and the Inland Empire Utilities Agency in regional efforts to develop finding additional sources of water for groundwater recharge, such as capture of stormwater runoff, recycled water, or other sources to ensure that the Chino Basin stays in long-term hydraulic balance and sustainability and that adequate additional local water sources would be available to increase the flexibility of the City's water supply.</p>	<p>City of Ontario Planning Department in coordination with City of Ontario MUA/Engineering Department</p>	<p>Ongoing</p>	<p>City of Ontario Planning/MUA/Engineering Department</p>	

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING THAT THE CITY COUNCIL APPROVE FILE NO. PUD21-001, AN AMENDMENT TO THE EMPORIA FAMILY HOUSING PLANNED UNIT DEVELOPMENT ("PUD"), AMENDING CERTAIN PROVISIONS TO: [1] EXPAND THE PROJECT BOUNDARY AREA FROM APPROXIMATELY 2.80-ACRES OF LAND TO 4.95-ACRES OF LAND; [2] ESTABLISH MINIMUM BUILDING SETBACKS FROM PALM AVENUE AND TRANSIT STREET RIGHTS-OF-WAY; [3] MODIFY MINIMUM PARKING REQUIREMENTS; [4] ALLOW ON-STREET LOADING; [5] UPDATE THE PLANTING PALETTE; AND [6] MAKE VARIOUS REVISIONS NECESSARY TO THE INTERNAL CONSISTENCY OF THE PUD, AND MAKING FINDINGS IN SUPPORT THEREOF — APNS: 1049-051-04, 1049-054-02, 1049-054-03, 1049-054-04, 1049-054-06, 1049-059-06, AND 1049-059-07

WHEREAS, The Related Companies of California, LLC, (hereinafter referred to as "Applicant") has initiated a request for the approval of an amendment to the Emporia Family Housing Planned Unit Development, File No. PUD21-001, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, in June 2017, the City Council adopted Ordinance No. 3075 approving the Emporia Family Housing Planned Unit Development ("PUD"), which governs land use and development within a two-block area within the downtown, bordered by Holt Boulevard on the north, Emporia Street on the south, Vine Avenue on the west, and Fern Avenue on the east; and

WHEREAS, the Project site land uses include a 75-unit multiple-family apartment complex, two historic commercial buildings, and vacant land; and

WHEREAS, the surrounding properties are located within the MU-1 (Mixed Use Downtown) zoning districts and land uses are characterized by a mixture of legal nonconforming residential uses, residential, light industrial, and commercial uses; and

WHEREAS, the PUD Amendment primarily applies to establishing development standards for a one-block area bordered Transit Street on the north, Emporia Street on the south, Fern Avenue on the west, and Palm Avenue on the east, and 2 lots located on the south side of Emporia Street; and

WHEREAS, the PUD Amendment will expand the project boundary area from approximately 2.80 acres of land to 4.95 acres of land, establish minimum building setbacks from Palm Avenue and Transit Street rights-of-way, modify minimum parking requirements, allow on-street loading, and update the planting palette. Other minor

revisions proposed are intended to clarify or update standards which reflect applicable Development Code standards and overall industry best practices; and

WHEREAS, the Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001 (hereinafter referred to as "Certified EIR"), in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as "EIR Addendum") in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as "CEQA"); and

WHEREAS, the environmental impacts of this project were thoroughly analyzed in the EIR Addendum, which concluded that implementation of the Project could result in a number of significant effects on the environment that were previously analyzed in the Certified EIR, and that the Certified EIR identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an EIR Addendum has been prepared to determine possible environmental impacts; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and make recommendation to the City Council on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside,

and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, as the first action on the Project, on May 25, 2021, the Planning Commission issued a Resolution recommending the City Council adopt the EIR Addendum, finding that the proposed Project introduces no new significant environmental impacts and applying all previously adopted mitigation measures to the Project, which were incorporated by reference; and

WHEREAS, on May 25, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the recommending body for the Project, the Planning Commission has reviewed and considered the information contained in the Addendum, the initial study, and the administrative record for the Project, including all written and oral evidence provided during the comment period. Based upon the facts and information contained in the Addendum, the initial study, and the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The environmental impacts of the Project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001; and

(2) The EIR Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts; and

(4) All previously adopted mitigation measures shall be a condition of project approval, as they are applicable to the Project, and are incorporated herein by this reference; and

(5) The EIR Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and

(6) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts.

SECTION 2: *Subsequent or Supplemental Environmental Review Not Required.* Based on the EIR Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The Project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: *Housing Element Compliance.* Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the recommending authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the Project site is one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the Project exceeds the 31-dwelling units and density of 25.1 dwelling units per acre as specified in the Available Land Inventory.

SECTION 4: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (“ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the recommending authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 5: Concluding Facts and Reasons. Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Section 1 through 4, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed PUD, or amendment thereto, is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The Project is consistent with Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan. Section 1.1, The Ontario Plan Objectives of the PUD, describes the manner in which the PUD complies with the Policy Plan goals and policies.

(2) ***The proposed PUD, or amendment thereto, would not be detrimental to the public interest, health, safety, convenience, or general welfare of the City.*** The Ontario Plan Environmental Impact Report includes safeguards and impose certain mitigation measures to ensure that development within the PUD boundary area would not be detrimental to public interest, health, safety, or general welfare of the City.

(3) ***In the case of an application affecting specific property(ies), the proposed PUD, or amendment thereto, will not adversely affect the harmonious relationship with adjacent properties and land uses.*** The PUD area is situated in Ontario's downtown and is adjacent to residential and commercial land uses and development. The surrounding development supports the objective of the PUD to further commercial and residential mixed-use development. The Project will not affect the harmonious relationship with adjacent properties and land use.

(4) ***In the case of an application affecting specific property(ies), the subject site is physically suitable, including but not limited to parcel size, shape, access, and availability of utilities, for the request and anticipated development.*** In preparing the proposed PUD Amendment, a thorough review and analysis of the proposed Project and the subject site's physical suitability for the Project was completed, including analysis of the project size, shape, intensity of development, building height, building setbacks, site access, site landscaping and drainage, fences and walls, vehicle circulation, pedestrian connections, availability of mass transit, necessary street dedication and easements, public right-of-way improvements, availability of utilities and other infrastructure needs, off-street parking and circulation, building orientation and streetscapes, architectural character, building materials and color, and site signage. Based upon this review and analysis, the subject site has been deemed physically suitable, including but not limited to parcel size, shape, access, and availability of utilities, for the proposed PUD Amendment and the proposed development.

(5) ***The proposed PUD is superior to that which could be obtained through the application of the Development Code.*** The PUD Amendment will (a) expand the

Project area an additional 2.15 acres, from approximately 2.80-acres of land to 4.95-acres of land; (b) establish minimum building setbacks from Palm Avenue and Transit Street rights-of-way; (c) modify minimum parking requirements; (d) allow on-street loading, and (e) update the PUD's planting palette. These changes to the PUD will facilitate the development of Emporia Family Housing – Phase 2, consisting of 50 affordable multiple-family dwellings, related parking facilities and private and common open space areas. Additionally, the PUD Amendment will allow for the construction of additional recreation amenities that are available to all residents within the Emporia Family Housing Project (Phases 1 and 2). The proposed PUD Amendment will allow for subsequent development that is superior to that which could be obtained through the application of the Development Code by itself.

SECTION 6: Planning Commission Action. Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby RECOMMENDS THE CITY COUNCIL APPROVE the herein described Amendment to the Emporia Family Housing Planned Unit Development, File No. PUD21-001, as depicted in Attachment A of this resolution and incorporated herein by this reference.

SECTION 7: Indemnification. The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 8: Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 9: Certification to Adoption. The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of May 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 25, 2021, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**File No. PUD21-001,
Emporia Family Housing Planned Unit Development-
Draft Amendment**

Related Emporia Planned Unit Development

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1 – Introduction

This document has been prepared to establish development regulations for the Emporia Ontario Family Project site located in the City of Ontario. The overall project site is approximately 4.62 net acres. Emporia I is comprised of ~~project site is~~ approximately 2.81 net acres and Emporia II is approximately 2.15 net acres in size. The project site is ~~containing~~ contained a within a ~~two~~-three block area that is generally bounded by Holt Boulevard and Transit Street to the north, Emporia Street to the south, ~~Fern-Palm~~ Avenue to the east and Vine Avenue to the west and includes 2 lots bound by Emporia Street and the railroad right-of-way. Additionally, this site is bisected east/west by Transit Street and bisected north/south by Fern Avenue (see Exhibit 1-1: Project Location Map).

The project site is located within the Ontario Plan Downtown Mixed-Use District which requires the establishment of a Planned Unit Development (PUD) prior to development. The PUD fulfills the requirements of a Downtown Mixed-Use District Planned Unit Development (“PUD”), as set forth in Ontario Development Code Section 4.01.030 (Planned Unit Developments (“PUD”). Upon approval, this PUD will establish the land use and development standards for the project site. Unless otherwise defined herein, definitions and interpretations contained in the Development Code shall apply to this PUD.

City staff and private developers will rely upon this PUD to determine whether precise plans for development (“Development Plans”) will adequately meet the City’s land use and design objectives for this key part of Downtown Ontario. These objectives have been defined over the last 20+ years in the Redevelopment Plan for the Center City Redevelopment Project (1983) and The Ontario Plan (2010) and are summarized in the proceeding chapters.

Exhibit 1-1: Project Location Map



Not to Scale



Not to Scale



2 – Objectives

2.1 – The Ontario Plan Objectives

The Ontario Plan, which includes the City’s General Plan, designates the project area as part of the Downtown Mixed-Use District. The Downtown Mixed Use District designation requires approval of an Area Plan or Planned Unit Development to develop the property. Additionally, the designation specifies a residential density range of 25 to 75 units per acre and a maximum floor area ratio (FAR) of 2.0 for office and retail uses.

The intent of this designation is to:

- Create “an intensive vertical and horizontal mixture of retail, office, and residential uses in a pedestrian friendly atmosphere;
- Ensure the historic character is enhanced; and
- Concentrate the most intensive uses along Euclid and Holt Avenues.

The Ontario Plan goals and policies furthered by this Planned Unit Development are as follows:

2.1.1 – Land Use Element Goals & Policies

- LU1 A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.
 - ❖ LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.

2.1.2 – Community Design Element Goals & Policies

- CD1 A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.
 - ❖ CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.
 - ❖ CD1-2 Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.
- CD2 A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.
 - ❖ CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:
 - building volume, massing, and height to provide appropriate scale and proportion;
 - a true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and

- exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.
- ❖ CD2-5 Streetscapes. We design new and, when necessary, retrofit existing streets to improve walkability, bicycling and transit integration, strengthen connectivity, and enhance community identity through improvements to the public right of way such as sidewalks, street trees, parkways, curbs, street lighting and street furniture.
- ❖ CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.
- CD3 Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.
 - ❖ CD3-1 Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics. (Link to Bicycle and Pedestrians Section of the Mobility Element and Policies M2-3 and M2-4)
- CD4 Historic buildings, streets, landscapes and neighborhoods, as well as the story of Ontario’s people, businesses, and social and community organizations, that have been preserved and serve as a focal point for civic pride and identity.

2.1.3 – Housing Element Goals & Policies

- H2 Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.
 - ❖ H2-1 Corridor Housing. We revitalize transportation corridors by encouraging the production of higher density residential and mixed-uses that are architecturally, functionally and aesthetically suited to corridors.
 - ❖ H2-2 Historic Downtown. We foster a vibrant historic downtown through facilitating a wide range of housing types and affordability levels for households of all ages, housing preferences, and income levels.
 - ❖ H2-5 Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices and other best practices.
 - ❖ H2-6 Infill Development. We support the revitalization of neighborhoods through the construction of higher-density residential developments on underutilized residential and commercial sites.
- H4 Increased opportunities for low and moderate income households and families to afford and maintain quality ownership and rental housing opportunities, including move-up opportunities
 - ❖ H4-3 Rental Assistance. We support the provision of rental assistance for individuals and families earning extremely low, very low, and low income with funding from the state and federal government.

- H5 A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.
 - ❖ H5-2 Family Housing. We support the development of larger rental apartments that are appropriate for families with children, including, as feasible, the provision of services, recreation and other amenities.

2.1.4 – Community Economics Element Goals & Policies

- CE1 A complete community that provides for all incomes and stages of life
 - ❖ CE1-6 Diversity of Housing. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.

2.2 – Center City Redevelopment Plan Objectives

The Center City Redevelopment Plan was established to provide a framework and process to guide the development of projects within the study area of the Redevelopment Plan. The following objectives have been identified from the Redevelopment Plan as pertaining to the Planned Unit Development:

- Create a healthy and exciting urban variety, the ability to work, live, shop and play within a small area, combine daytime and nighttime use, and conserve energy and resources through mixed-use land development;
- Create an attractive and pleasant environment in the Project Area through the use of proper design, open space, and other amenities to enhance the aesthetic quality;
- Encourage and facilitate medium and high-density development, including, but not limited to, condominiums, townhouses, apartments and similar compatible uses.
- Maximize the housing opportunities of the residential areas; and
- Provide improvements necessary for the elimination of blight and to provide for the orderly development of the commercial industrial and residential areas included in the redevelopment project area.

2.3 – Related Emporia Project Objectives

This Planned Unit Development provides guidelines of development for the project area. The objectives below have been established to provide a general framework for establishing development standards to ensure proper development of the project area.

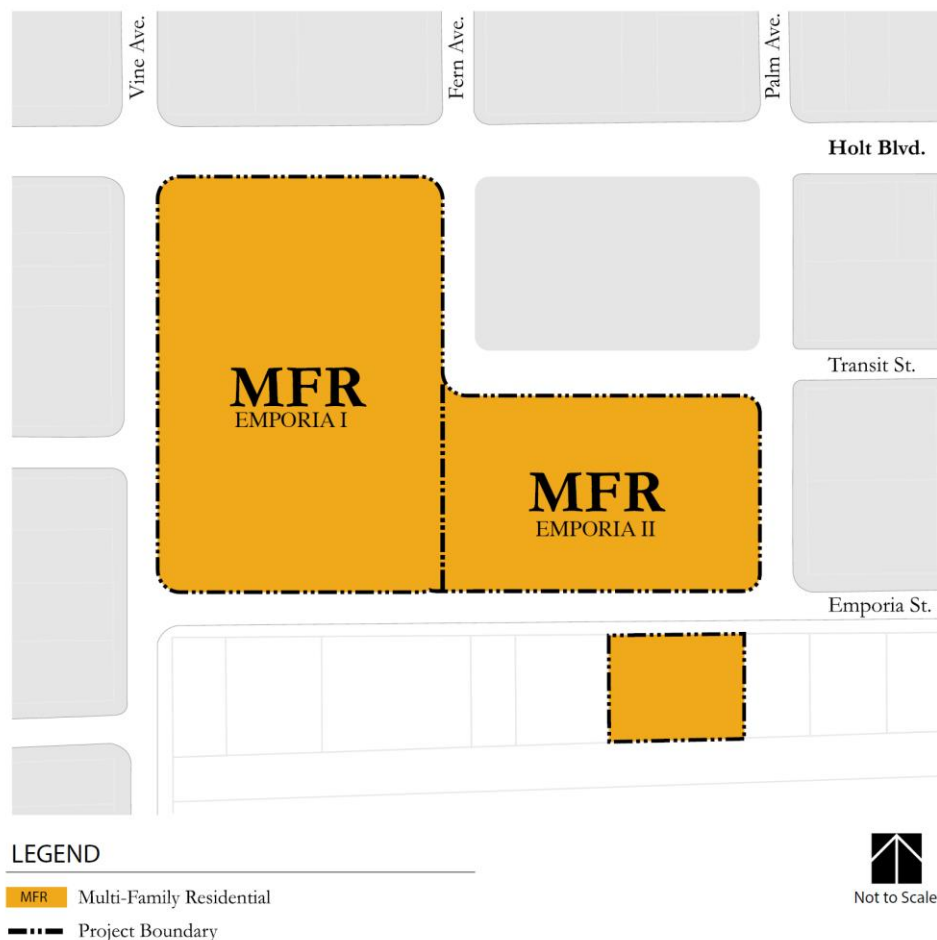
- Develop high quality, -affordable rental housing;
- Establish appropriate relationships among new residential neighborhoods as well as with existing adjacent land use; and
- Provide new housing designed for families with children.

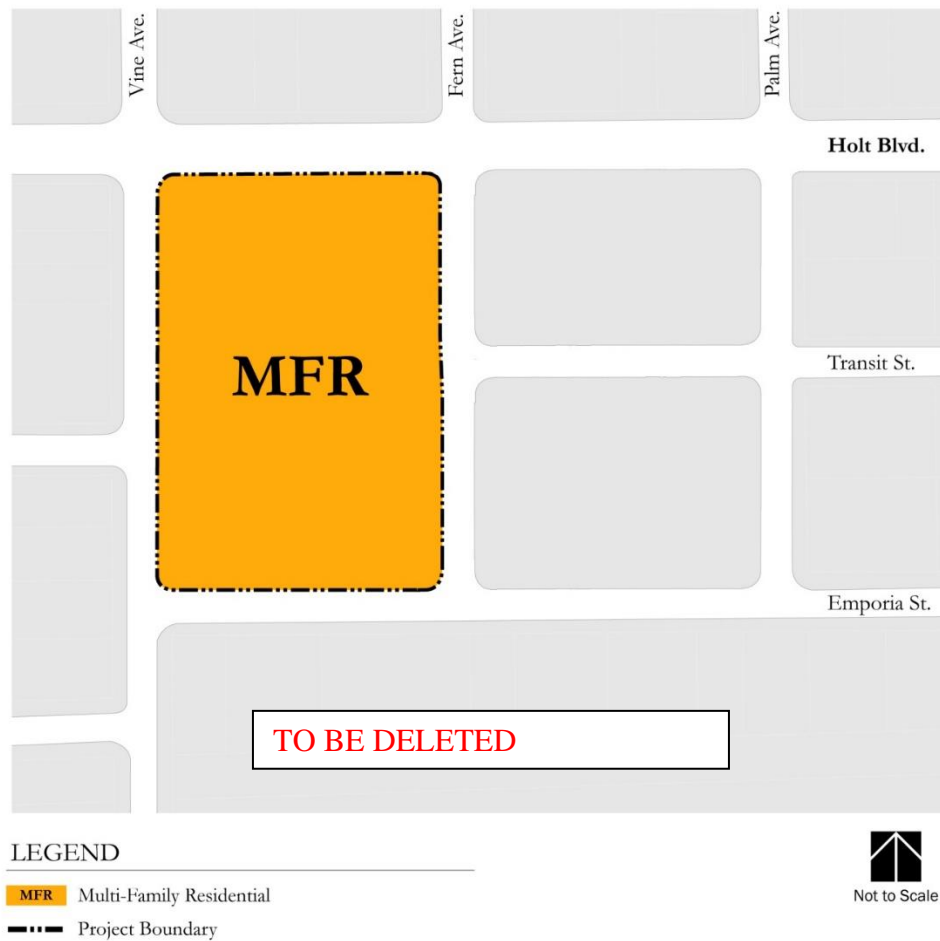
3 – Allowable Uses

The project site is located within the Downtown Mixed Use Area Policy Plan (General Plan) land use district, and the MU-1 (Downtown Mixed-Use) zoning district, which implements the Downtown Mixed-Use Area. The MU-1 zoning district is established to accommodate an intensive mixture of vertical and horizontal retail and office uses at a development intensity of up to 2.0 FAR, and residential uses at a density of 25 to 75 dwellings units per acre. Development projects within the MU-1 zoning district are intended to maintain a pedestrian friendly atmosphere, while at the same time enhancing the historic character of the area. The most intensive uses within this district are envisioned along Euclid Avenue and Holt Boulevard.

Consistent with the intent of the Downtown Mixed Use Area and the MU-1 zoning district, the project site is designated for Multiple-Family Residential (MFR) land uses. Exhibit 3-1 (Land Use Plan), below, depicts the Planned Unit Development boundary and land use designation.

Exhibit 3-1: Land Use Plan





3.1 – Residential Use

The project site will be developed with high quality, attached housing units, with either two or three levels. The buildings will be a combination of podium-style stacked flat units, with parking beneath, [and](#) townhome style units, with first floor garage access, [two and three story walk-ups with stacked flat units](#).

3.2 – Permitted Use Table/List

The permitted uses within the Related Emporia PUD include the following:

- Multiple-family ([Affordable Low-Income](#)) dwelling;
- Recreation facilities ancillary to multiple-family residential development projects, for use by residents and their guests, including but not limited to pool, spas, tennis courts, clubhouse or recreation building, playgrounds or tot lots, and other similar amenities appropriate to serve the project;
- Temporary Uses (As permitted within the residential districts of the City of Ontario Development Code subject to an Administrative Permit);

- Leasing/Administrative Office (for on-site property and facilities management only);
- Motor vehicle parking ancillary to a multiple-family residential development project; and
- Other land use compatible with multiple-family residential development projects, as determined appropriate by the Zoning Administrator.

Uses may be prohibited through rental contracts/agreements as provided by the project management, unless prohibited by Federal, State, or local laws.

4 – Development Regulations

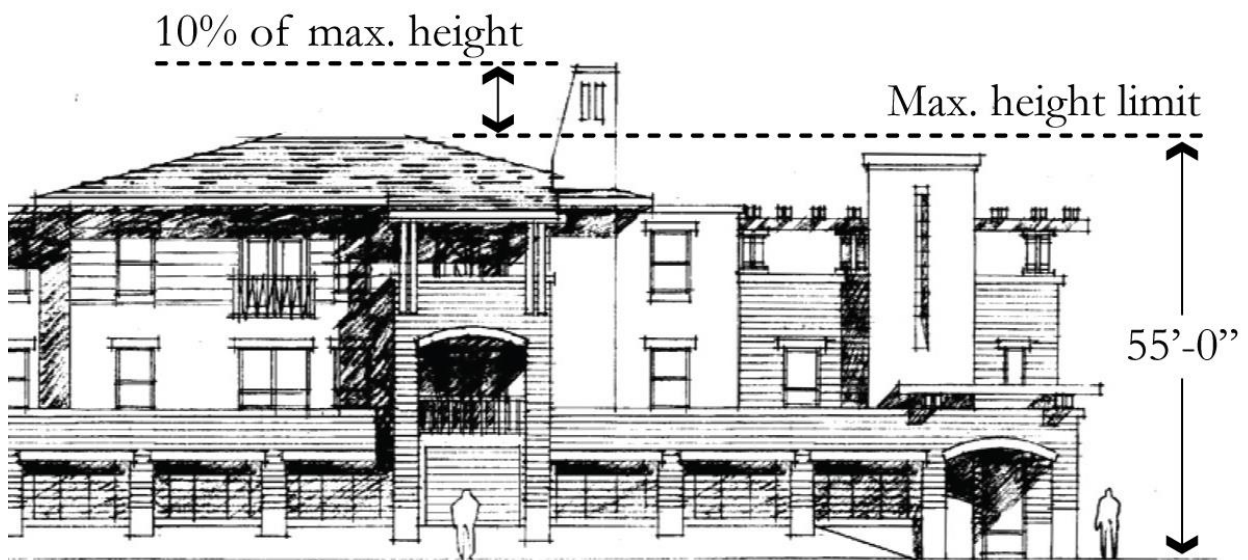
4.1 – Residential Density

The residential density shall range from 25.1 dwelling units per acre to a maximum of 75.0 dwelling units per acre.

4.2 – Building Height

No structure shall exceed 55 feet in height, except that the maximum height may be exceeded by roof mounted equipment, architectural projections, chimneys, elevator towers, parapet walls and any other roof top structures, by up to 10% of the allowed building height. No rooftop equipment shall be visible from anywhere on the project site, public streets, or adjacent properties, and shall be fully screened with appropriate architectural parapet walls or appropriate roof treatments. Roof-mounted equipment shall not exceed the height of the structures and appurtenances used to screen the equipment.

Exhibit 4-1: Height Diagram



4.3 – Setbacks

Building setbacks shall be as set forth in Table 4-1: (Minimum Building Setbacks), and shall comply with Exhibit 4-2 (Setback Diagram).

Table 4-1: Minimum Building Setbacks

Setback	Distance
Street Setback Holt	9 feet
Emporia	5 feet
Vine	5 feet
Fern	<u>5 feet</u>
<u>Palm</u>	<u>5 feet</u>
<u>Transit</u>	5 feet
Easement Setback**	2 feet
Building Separation	9 feet

** 30 foot wide storm drain easement located along vacated Transit Street

Certain types of features and equipment are allowed to encroach into the required setback areas in accordance with Table 4-2 (Encroachments into Setbacks) including but not limited to architectural projections, balconies, and underground utility structures such as electrical transformer vaults. Utility structures may encroach into the public right-of-way and/or public utility easement, subject to the approval of the City Engineer.

Exhibit 4-2: Setback Diagram

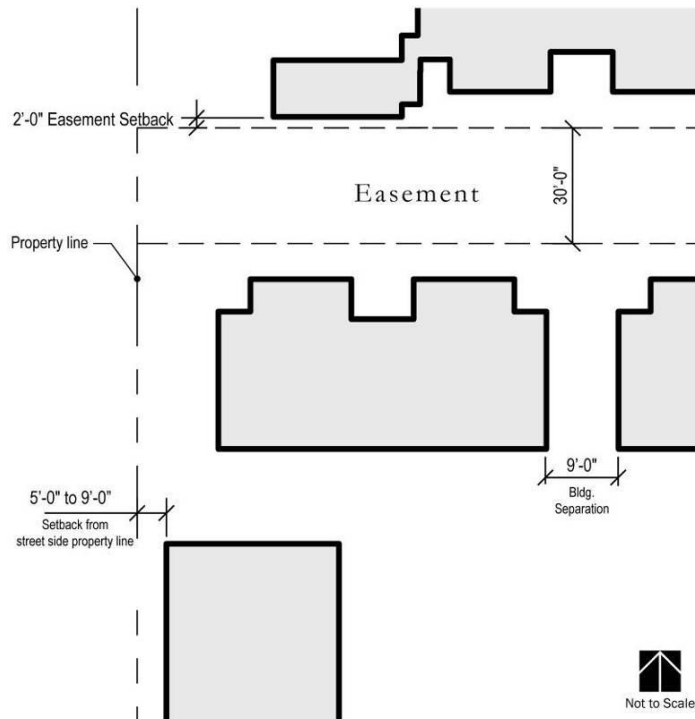
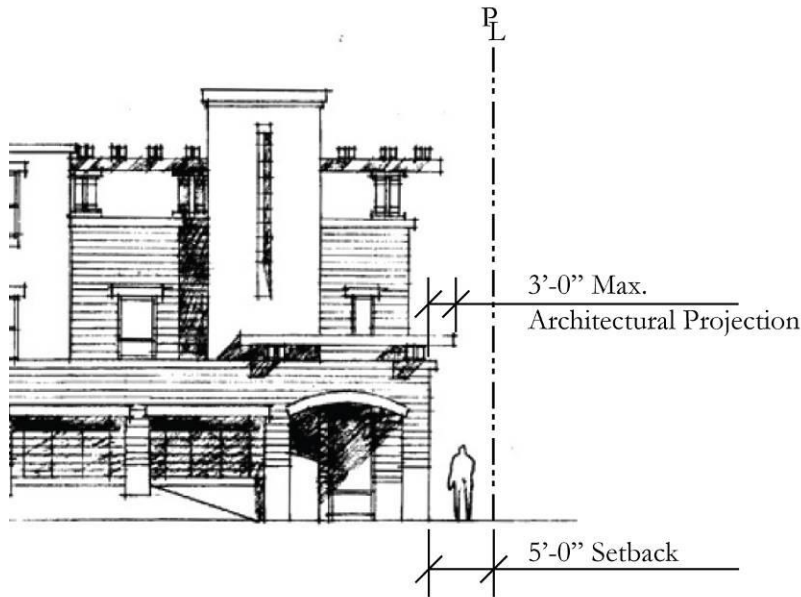


Table 4-2: Maximum Encroachments into Building Setbacks

Projection	Encroachment
Architctural Features**	3 feet
Patios**	3 feet
Balconies/Decks**	3 feet
Porches**	3 feet

** No projection may encroach into the storm drain easement

Exhibit 4-3: Projection Diagram



4.4 – Open Space

Open space shall be provided for passive and active recreation opportunities within the project site. Two types of open space are required, private and common open space. These open space areas are for the use of the on-site residents and their guests. Required street setback areas are not counted towards common open space requirements.

4.4.1 – Private Open Space

Private open space shall be provided for all two and three-bedroom each-residential units and one-bedroom ground units fronting Holt Avenue and Vine Street in order to provide private outdoor areas which can be enjoyed for the exclusive use by the occupant of the residential unit and their guests. Types of areas considered private open space include balconies, decks, and enclosed patios and yards.

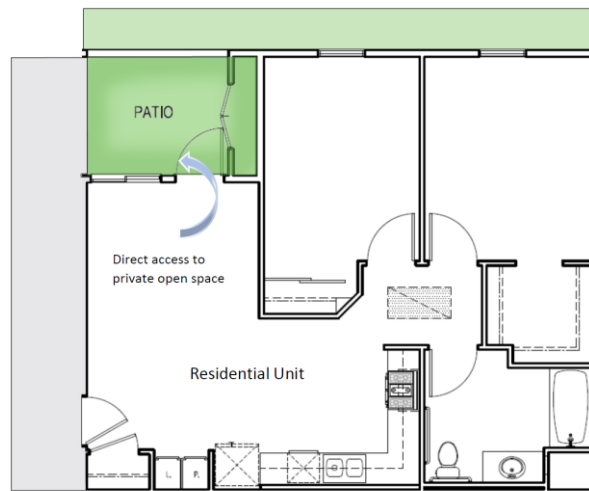
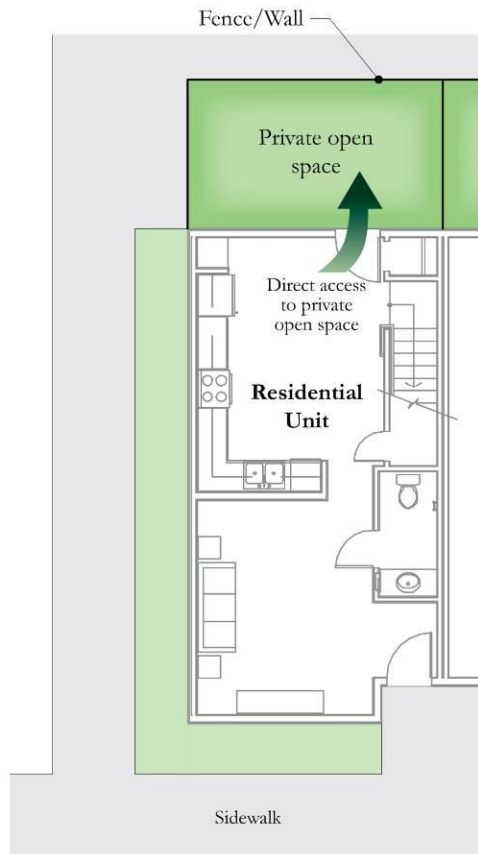
Private open space shall be provided for every residential unit and shall have direct access from the residential unit. Each residential unit shall have a minimum size of private open space as defined in Table 4-3 (Private Open Space). The space may be provided in multiple areas (e.g. two balconies, a yard and a balcony, etc.), as long as the total area of the spaces meets the minimum private open space requirement for the unit. The minimum dimension for private open space shall be no less than 6 feet in any direction.

Table 4-3: Private Open Space

Number of Bedrooms	Open Space (SF)
1	0**
2	50
3	100
4	160

~~**20 square feet for one bedroom units with balconies fronting on to Holt Boulevard, and 50 square feet for one bedroom units fronting on to Vine Avenue.~~

Exhibit 4-4: Private Open Space



4.4.2 – Common Open Space

Common open space shall be provided to allow for passive and active types of recreation, along with site landscaping amenities. These areas are for use by project residents and their guests and may include indoor recreational facilities. Recreational facilities provided pursuant to Section 4.4.3, shall be provided in common open space areas. The amount of required

common open space is based on the number of dwellings developed and shall be provided at the rate of 235 square feet of common open space for each dwelling unit.

Areas not considered common open space include the following: ~~(1) parking lots; (2) walkways along or between buildings; (3) parking lot landscaping; (4) street setback areas; and (5) areas not intended for active or passive recreation~~

- Parking lots;
- Walkways along or between buildings;
- Parking lot landscaping;
- Street side setback areas; and
- Other areas not intended for active or passive recreation.

Exhibit 4-5: Common Open Space



TO BE DELETED

LEGEND

- Common Open Space
- Active Open Space
- Passive Open Space
- Holt Blvd. Dedication
- Setback Areas (Not included)





LEGEND

- Common Open Space
- Active Open Space
- Passive Open Space
- Holt Blvd. Dedication
- Setback Areas (Not included)



4.4.3 – Recreational Facilities

Access to recreational facilities is important in a multi-family residential project as they provide needed facilities for the residents of the community. These facilities shall be centrally located on the site to minimize the distance of these facilities to all residential units within the project. A total of at least three recreational facilities (indoor or outdoor) shall be provided on-site. The facilities can be comprised of a combination of both similar and different recreational facilities to meet the recreational facilities requirement (e.g. two pools and one play ground or one clubhouse, one pool, and one tot lot, etc.). Recreational facilities can be indoor or outdoor and may include the following:

- Clubhouse;

- Teen Area; This area will be open between 6:00am and 10:00pm on a daily basis. The area will be designed with chairs and seating. Security lighting in this area will also add an additional safety factor.
- Pools;
- Community Building;
- Playground or tot lot; and
- Other indoor or outdoor recreation deemed appropriate by the City.

All recreational uses shall be for the exclusive use of the on-site residents and their guests.

4.5 – Parking Supply and Demand

4.5.1 – On-Site Parking Requirements

The number of parking spaces provide is based upon number of bedrooms contained within each dwelling unit, and is subject to the following requirements:

- All required resident parking shall be provided on-site;
- Each unit shall have at least one reserved parking space that is clearly marked;
- ~~Reserved parking spaces shall be located within 150-200 feet of the dwelling it serves;~~
- ~~At least one covered parking space shall be provided for each unit in Emporia I. This can be achieved through use of garages, carports, or tuck under parking;~~
- Covered parking spaces provided through use of garages, carports, and tuck under parking are encouraged.
- Resident’s assigned parking space(s) shall be used for the storage of the occupant’s operable automobile(s), only;
- ~~Tandem parking is prohibited;~~
- Tandem parking spaces shall be assigned to the same unit and may not be used as guest parking
- Required guest parking spaces shall ~~be~~ provide the necessary parking for the project leasing office. No separate parking is required for the leasing office; and
- ~~Each dwelling unit is intended for occupancy by one family, regardless of bedroom size(s) provide. Paring for dwellings shall be provided on-site, pursuant to the requirements of Table 4-4 (Minimum Parking Requirements), below, except that guest parking may be provided on-street, pursuant to Section 4.5.2 (On-Street Guest Parking Requirements) of this PUD.~~

Table 4-4: Minimum Parking Requirements

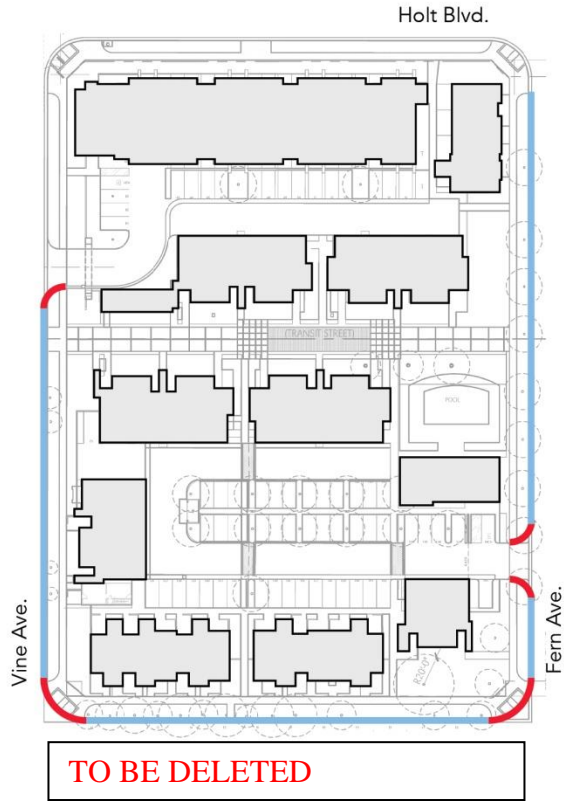
Use	Parking Spaces
Multiple Family Dwellings One Bedroom Unit	1.75 <u>1</u> spaces per dwelling (at least one space shall be in a garage or carport)
Two or more Bedroom Unit	2.0 <u>1.65</u> spaces per dwelling (at least one space shall be in a garage or carport)
<u>Three Bedroom Unit</u>	<u>2 spaces per dwelling</u>
Guest Parking	1 space per 5 dwellings

4.5.2 – On-Street Guest Parking Requirements



On-street parking may be utilized to satisfy guest-parking requirements subject to the following:

- On-street parking may be used to satisfy guest parking requirements only, and shall not be used for required resident parking;
- On-street parking shall only be counted along the streets surrounding the project site on the side of the street adjoining the property, excluding Holt Boulevard and any other areas deemed necessary by the City Engineer, for corner sight distance as shown in Exhibit 4-6 (On-Street Parking Locations Map)
- All on-street parking locations shall be subject to approval by the City Engineer; and
- On-street parallel parking spaces shall be calculated by the length of unobstructed curb adjacent to the project site.

Exhibit 4-6: Off-Site Parking Locations



LEGEND

-  On-Street Parking Locations
-  Estimated Corner Site Distances





Not to Scale

Note: Proper clearance shall be maintained around fire hydrants. Fire hydrant clearance may impact availability to provide on-street parking.



LEGEND

-  On-Street Parking Locations
-  Estimated Corner Site Distances



Not to Scale

Note: Proper clearance shall be maintained around fire hydrants. Fire hydrant clearance may impact availability to provide on-street parking.

4.5.3 – Parking Standards

All parking space size and location requirements shall conform to the standards set forth in Section 6.03.045 (Off-Street Parking Standards) of the Ontario Development Code.

4.6 – Vehicular Access

Vehicular access to the site from Holt Boulevard is prohibited. All points of access shall be approved by the City.

4.7 – Service Facilities

- Loading/unloading for residential uses and trash pickup service shall be limited to [a designated street curb staging location consistent with an approved Solid Waste Handling Plan](#). ~~on-site private drive aisles~~. Location, signage and any hourly restrictions to be imposed on such areas shall be subject to approval by the City.
- ~~On-street loading shall be prohibited.~~
- The number of enclosures, and their precise locations, dimensions, and design shall be provided consistent with City standards.
- Trash enclosures shall be designed to contain separate containers for the collection of refuse and recyclable materials, with an adequate number of containers provided to allow for the collection of both refuse and recyclable materials generated by the development, pursuant to standards established by the Ontario Municipal Utilities Company.
- Trash enclosures shall meet the minimum design standards depicted in the standard drawings adopted by the City, which shall include: [1] a minimum 6-FT high decorative masonry wall, with appropriate view-obstructing gates for container access, [2] separate pedestrian access that is designed to screen the interior of the enclosure from view from the exterior and prevent refuse dispersion, and [3] a decorative overhead roof structure to protect bins containing recyclable materials from adverse environmental conditions, which might render the collected materials unusable, and screen trash bins from view of the upper floors of adjacent dwellings. Furthermore, trash enclosures shall be architecturally enhanced, and shall be consistent with the architectural design of adjacent buildings.
- Trash enclosure dimensions shall be of adequate size to accommodate containers consistent with the City's current methods of collection within the area in which the project is located.
- Signs clearly identifying all recycling and refuse collection areas, and the materials accepted for recycling shall be posted adjacent to all points of access to each trash enclosure.
- Trash enclosures shall be located a minimum of 10 feet from the interior project boundary/property line.
- Particular care shall be given when placing trash enclosures immediately adjacent to dwelling units; however, no trash enclosure shall be located within 10 feet of the livable portion of a structure.
- Trash enclosures shall be bordered by a minimum 5-foot wide planter, and shall be screened with landscaping on all exposed sides, excluding the side with bin access gates.
- All service facilities shall be screened in accordance with the Section 4.9 (Screening) of this Planned Unit Development.

4.8 – Landscaping

4.8.1 – Site Landscaping

A conceptual landscape ~~and irrigation~~ plan shall be submitted along with the Development Plan for this Planned Unit Development. The plan shall specify all landscaping and hardscape for the entire project site. Detailed Landscape and Irrigation Plans shall be required prior to the issuance of building permits. The detailed plans shall show location of ground mounted utility boxes and equipment, along with the methods of screening for these items from the public right-of-way and adjacent residences where possible. The irrigation system shall be designed to utilize recycled water pursuant to State and local codes, ordinances, and laws, and applicable building and plumbing codes.

The landscape and irrigation plan shall be designed with water conservation in mind, utilizing “California friendly” species and drought tolerant planting materials. The landscaping and irrigation shall comply with AB 1881 and any other laws and regulations related to planting materials.

All trees which are to remain on-site shall be properly protected in place during construction to ensure tree health is preserved.

Tree plantings along the Transit Street storm drain easement shall be restricted to those species allowed by the San Bernardino Flood Control District.

4.8.2 – Planting Palette

The planting palette shall be comprised of “California friendly” drought tolerant planting materials compatible with the overall architectural style of the Planned Unit Development. The following palette has been established to meet the needs of this Planned Unit Development:

Table 4-5: Planting Palette

Botanical Name	Common Name	Size	Comments
Trees			
Vine Street			
Cedrus deodara		Existing	Preserve in Place
Lagerstroemia indica ‘Watermelon Red’	Crape Myrtle	24” Box	Matching Standards
Washingtonia filifera	California Fan	18 FT B Existing	Preserve in Place Skinned Trunk
Ginko Biloba	Maiden Hair Tree	Existing	Preserve in Place
Palm Street			
Washingtonia filifera	California Fan	Existing	Preserve in Place
Koelreuteria paniculata	Goldenrain Tree	24” Box	Matching Standards

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<u>Transit Street</u>			
Chilopsis linearis “Bubba”	Bubba Desert willow	24” Box	Matching Standard
Emporia Street			
Washingtonia filifera	California Fan Palm	Existing	Preserve in Place
Grevillea robusta	Silk Oak	Existing 24” box	Preserve in Place Matching Standards
Oak		Existing	Preserve in Place
Grevillea robusta	Silk Oak	24” Box	Matching Standards
Washingtonia filifera	California Fan	18 FT BT	Skinned Trunk
Fern Avenue			
Cinnamomum camphora	Camphor Tree	48” Box Existing	Matching Standards Preserve in Place
Quercus agrifolia	Coast Live Oak	Existing	Preserve in Place
Quercus agrifolia	Coast Live Oak	48” Box	Matching Standards
Holt Boulevard			
Frainus o. ‘Raywood’	Raywood Ash	24” Box	Matching Standards
Pedestrian Promenade			
Lagerstroemia hybrid ‘Tuscarora’ Chitalpa tashkentensis	Chitalpa Crape Myrtle	24” Box Existing	Matching Standards Preserve in Place
Magnolia grandiflora ‘Samuel Sommer’ Olea Europea ‘Swan Hill’	Samuel Sommer Southern Magnolia Swan Hill Fruitless Olive	36” Box Existing	Preserve in Place Matching Standards
Lagerstroemia hybrid ‘Tuscarora’	Crape Myrtle	36” 24” Box	Multi Branch
Auto Court			
Quercus virginiana	Southern Live Oak	36” Box	Matching Standards
Platanus acerifolia ‘Bloodgood’	London Plane Tree	36” Box	Matching Standards
Quercus ilex	Holly Oak	36” Box	Matching Standard
Accent Trees			
Cercidium ‘Azt’ (Thornless)	Hybrid Desert Museum	48” Box	Multi Branch
Cupressus sempervirens	Tiny Towers Italian Cypress	24” Box	Match Height
Schinus molle	California Pepper	36” Box	Multi Branch
Tipuana tipu	Tipu Tree	48” Box	Matching Standards
Olea europaea ‘Swan Hill’	Swan Hill Olive	48” 36” Box	Multi Branch
Phoenix dactylifera	Date Palm	20 FT BT	Diamond Cut trunk

**DEVELOPMENT
REGULATIONS**

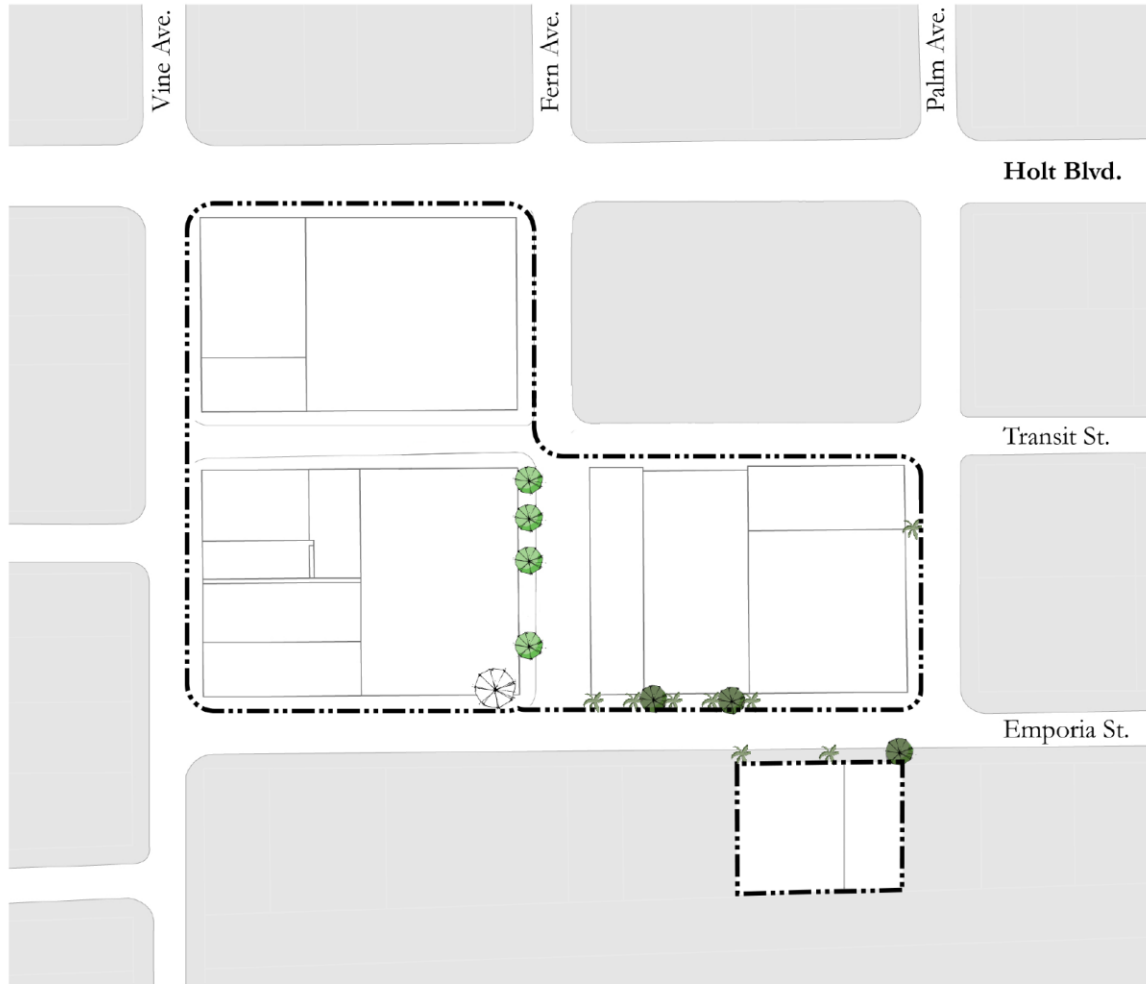
Chilopsis 'Bubba	Bubba Desert Willow	24" Box	Matching Standard
Cercis canadensis 'Mexican'	Mexican Redbud	24" Box	Matching Standards
Patio Areas			
Langerstroemia indica "Natchez"	Crape Myrtle	24" Box	Matching Standards
Prosopis chilensis	Chilean Mesquite	24" Box	Matching Standards
Cercis canadensis "Mexicana" Pyrus calleryana 'Bradford'	Ornamental Pear Mexican Redbud	24" Box	Matching Standards
Building Perimeter			
Tristania conferta	Brisbane Box	15 Gallon	Matching Standards
Pinus eldarica	Afghan Pine	15 Gallon	Matching Standards
Washingtonia robusta filifera	Mexican California Fan Palm	18 FT BT	Skinned Trunk
Tristania laurina	Water Gum	24" Box	Standard
Parking Lot			
Rhus lancea	African Sumac	24" Box	Matching Standards
Quercus ilex	Holly Oak	36" Box	Standard
Cercidium 'AZT'	Thornless Palo Verde	48" Box	Standard
Cupressus S. 'Tiny Towers'	Tiny Towers Italian Cypress	15 Gal	Match Height
Vines			
Parthenocissus tricuspidata	Boston Ivy	1 Gallon/5 Gal Staked	None Attach to Wall
Disticus buccinatoria	Blood Red Trumpet Vine	1 Gallon/5 Gal Staked	None Attach to Wall
Jasminum polyanthum	Pink Jasmine	1 Gallon	None
Gelsemium sempervirens Maefadyena unguis-cati	Cat's Claw "Yellow Trumpet Vine" Carolina Jassamine	1 Gallon	None Attach to Wall
Solanum jasminoides	Potato Vine	5 Gal. Staked	Attach to Wall
Shrubs			
Laurus nobilis 'Saratogo'	Saratoga Laurel	5 Gal.	None
Westringia 'Morning Light'	Dwarf Coast Rosemary	5 Gal.	None
Ligustrum J. 'Texanum'	Texas Privet	5 Gal.	None
Ilex vomitoria 'Stokes'	Stokes Dwarf Holly	5 Gal.	None
Trachelospermum jasminoides	Star Jasmin	1 Gal.	None
Diets bicolor	Yellow Morea	5 Gal.	None

<u>Duranta R. ‘Sapphire Skies’</u>	<u>Sapphire Sky Flower</u>	<u>5 Gal.</u>	<u>None</u>
<u>Agave parryi</u>	<u>Parry’s Agave</u>	<u>5 Gal.</u>	<u>None</u>
<u>Muhlenbergia capillaris</u>	<u>Pink Muhlygrass</u>	<u>5 Gal.</u>	<u>None</u>
<u>Dianella R. ‘Cassa Blue’</u>	<u>Cassa Blue Flax Lily</u>	<u>1 Gal.</u>	<u>None</u>
<u>Carex divulsa</u>	<u>Berkeley Sedge</u>	<u>1 Gal.</u>	<u>None</u>
<u>Callistemon ‘Little John’</u>	<u>Little John Callistemon</u>	<u>5 Gal.</u>	<u>None</u>
<u>Myoporum P. ‘Putah Creek’</u>	<u>Putah Creek Myoporum</u>	<u>5 Gal.</u>	<u>None</u>
<u>Senecio mandraliscae</u>	<u>Blue Chalk Sticks</u>	<u>1 Gal.</u>	<u>None</u>
<u>Hesperaloe parvifolia</u>	<u>Red Yucca</u>	<u>1 Gal.</u>	<u>None</u>
<u>Euonymus fortunei “moomshadow”</u>	<u>Moonshadow Euonymus</u>	<u>5 Gal.</u>	<u>None</u>
<u>Baccharis ‘Centennial’</u>	<u>Centennial Coyote Brush</u>	<u>1 Gal.</u>	<u>None</u>






4.8.3 – Tree Removal

As part of Emporia I Eight mature Camphor trees and one mature Cork Oak tree on site shall remain and be incorporated into the overall design of the development plan. The Camphor trees are currently located in the right-of-way on Fern Street and Transit Street and The existing Cork Oak is located at the northwest corner of Emporia Street and Fern Street shall be preserved in place. Four existing Coast Live Oak trees located on the west side of Fern Avenue are to remain and shall be preserved in place. Approximate location of trees is shown in Exhibit 4-7 (Existing Trees Plan). These trees have been in place prior to the demolition of the Casa Blanca hotel and have been preserved in accordance with the Casa Blanca Hotel Demolition Environmental Impact Report. An arborist report shall be prepared on all other existing trees to determine the health and where feasible existing healthy trees in the project area shall be preserved in place.

Exhibit 4-7: Existing Trees Plan ~~Exhibit 4-7: Existing Trees Plan~~

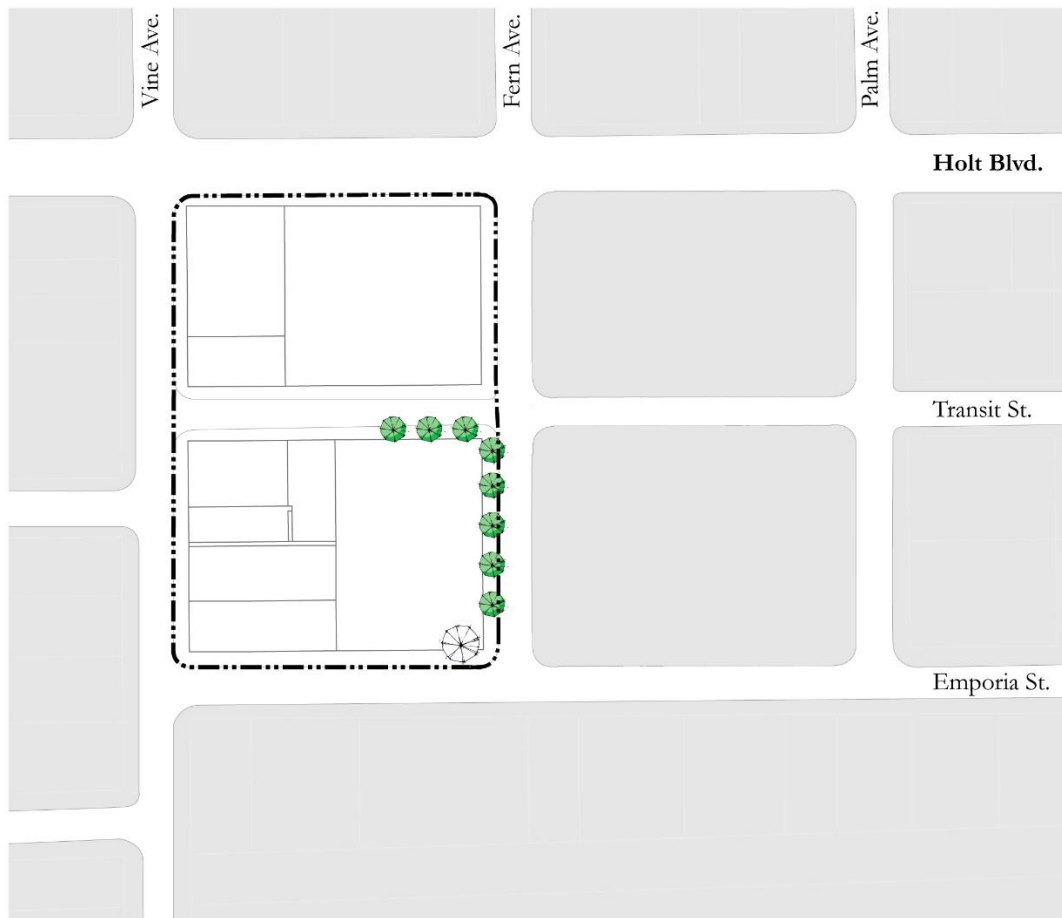


LEGEND




-  Existing California Fan Palm Trees to Remain
-  Existing Silk Oak Tree to Remain
-  Existing Coast Live Oak to Remain
-  Existing Cork Oak to Remain (Emporia I)
-  Project Boundary



Not to Scale



LEGEND

-  Existing Camphor Trees to Remain
-  Existing Cork Oak Tree to Remain
-  Project Boundary

TO BE DELETED



4.8.4 – Parking Lot Landscaping

Parking lot landscaping shall be provided pursuant to Subsection D (Landscaping of Off-Street Parking Facilities) of Ontario Development Code Section 6.05.030 (Required Landscape Areas).

4.8.5 – Compliance with State and Federal Laws

Provide landscaping and an irrigation system, which promotes the conservation of water as required by the Water Conservation in Landscaping Act of 2006 (AB 1881), commencing with California Government Code Section 65591.

4.9 – Screening

- All roof and ground mounted equipment shall be screened pursuant to the requirements of the Ontario Development Code.
- Trash enclosures shall be screened with enclosures that are architecturally compatible with the surrounding buildings.
- Screening shall include plant and building materials compatible with the project design so it is well integrated and hidden within the project area.
- Building and plant materials used for screening shall be compatible with the architectural style and planting palette used on the project area.
- All ground level screening shall comply with the requirements of the Ontario Development Code Section 6.02.030 (Protection of Intersection Visibility).

4.9.1 – Fences, Walls, and Hedges

Fences, walls, and hedges shall comply with Section 6.02.030 (Protection of Intersection Visibility) of the Ontario Development Code, Engineering corner sight distance standards, and all other applicable city standards. Fences and walls shall be made of decorative materials which are compatible with or enhance the overall architectural character of the project. All fences, walls, and hedges shall be in scale with the development and shall be used for screening, site enhancement and creating a safer living environment for residents and their guests. All decorative walls, monuments and/or other similar features shall not encroach in to the public street right-of-way.

5 – Circulation

5.1 – Site Accessibility

The site shall be designed to promote safety for residents by only allowing limited vehicular and pedestrian access into and across the site. This can be done through the use of building orientation and placement to minimize the use of gates and fencing; however, amenities shall be properly gated with limited access for residents and their guests.

5.2 – Vehicular Circulation

The site should be designed to reduce the number of dead end aisles in the parking lot areas and provide all guest parking outside of gated areas (on or off-site) to ensure accessibility. Location of drive aisles and entries shall be approved by the City Engineer. Vehicular circulation shall be designed in a way that promotes pedestrian safety and proper access to all parking areas.

5.3 – Pedestrian Circulation

Site design must provide for safe pedestrian circulation across the project site by separating pedestrian areas from areas with vehicular access. This includes, but is not limited to accessibility from parking lot areas to unit entries, site amenities, and perimeter sidewalk areas. Fencing and gates may be used to limit public access to resident-only areas.

The vacation of Transit Street [in Emporia I](#) provides a unique opportunity to create a pedestrian promenade that will connect residential foot traffic between residential units, parking, and community spaces throughout the site. The space should be welcoming and designed with landscape features that help define the walking path across the project area. Since no vehicular traffic will be allowed on this promenade, it is expected that children will be able to play safely throughout the site. Lighting and visibility for enhanced security shall be taken into consideration designing this area and shall be compatible with the architectural theme. Buildings should have windows that look on to this open space area to enhance the views of the residences and help create defensible spaces. [Emporia II will vacate Fern Avenue to create pedestrian connectivity and linkage between Emporia I and Emporia II as shown in Exhibit 5-1 \(Circulation Plan\).](#)

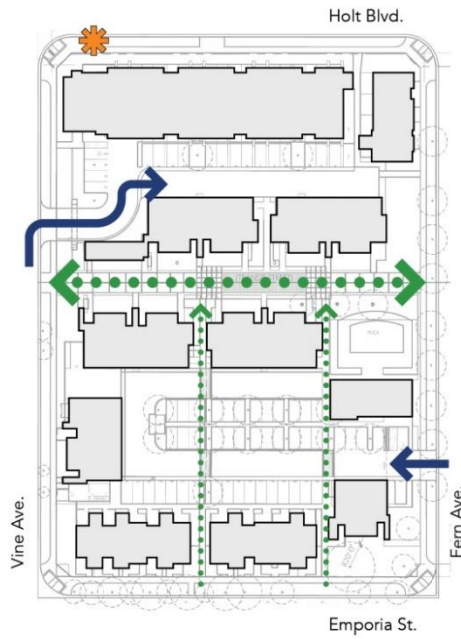
5.4 – Access to Mass Transit

An existing bus stop is located on the south side Holt Boulevard just east of Vine Avenue. A new decorative bus shelter with seating and lighting shall be installed at this location upon development. The bus shelter design shall be consistent with the design of other shelters in the downtown area (see Figure 5-1 (Bus Shelter)), and shall meet ADA access requirements per City of Ontario and/or OmniTrans Standards and to the satisfaction of the City Engineer. Additionally, a concrete bus pad for this bus stop shall be incorporated into the public improvements required for the development of the project site.





Figure 5-1: Bus Shelter



Exhibit 5-1: Circulation Plan

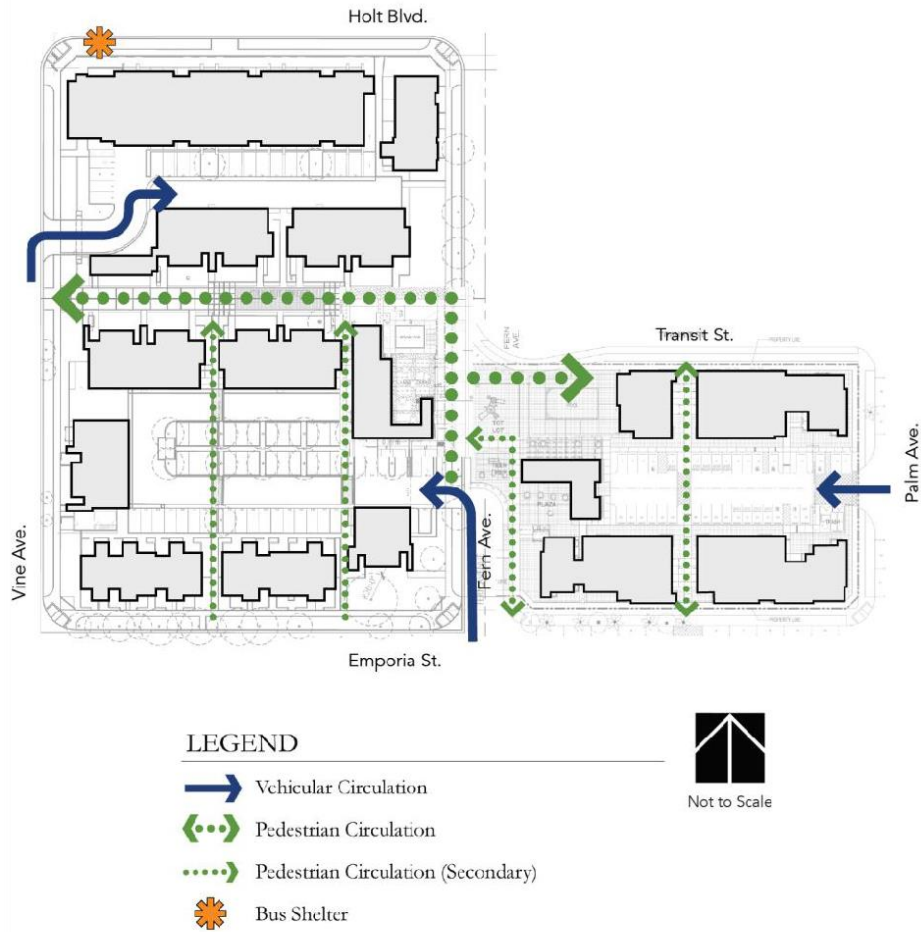


LEGEND

-  Vehicular Circulation
-  Pedestrian Circulation
-  Pedestrian Circulation (Secondary)
-  Bus Shelter



TO BE DELETED



6 – Design Guidelines

The project area is located within Ontario’s historic downtown area and shall comply with the Downtown Ontario Design Guidelines. The Downtown Ontario Design Guidelines were adopted in 1998 to guide the physical revitalization of Ontario’s historic downtown. The Guidelines provide architectural and design principals, as well as design concepts for downtown districts. The project area is located within the Educational District, a mixed use area with an educational theme. The Guidelines do not provide specific architectural or design guidance within the Educational District but do require that development be context sensitive.

The design guidelines provided in this section are designed to be compatible with the Downtown Ontario Design Guidelines and provide site specific guidance for the development of the project area.

6.1 – Building Orientation

Building orientation shall be designed to minimize potential noise impacts, aide in providing property site safety, create proper accessibility to site amenities and parking, and to maximize views from residential units. Design should also be done in a way to create defensible spaces improving site safety.

Exterior: Orientation towards the exterior of the project area shall be carefully evaluated to ensure a proper relationship.

Interior: Buildings should be clustered in a way that creates defensible spaces providing views of interior open spaces and amenities. For units without garage spaces buildings shall be oriented to create minimal distances from assigned parking spaces to primary or secondary residential unit entries.

6.2 – Architectural Character

The architectural design leans toward a current interpretation of the Craftsman style. The design will make use of open ended beams, gabled and hipped roofs and trellis construction throughout the site. Large areas of masonry and wood siding with plaster accents will be provided to help enhance this overall architectural theme. Buildings along Holt Blvd will have a linear design with enhanced areas of design and color to differentiate units along this street. Street fronting podium parking will be shielded from view by intensified landscaping and podium walls with screened openings running alongside the north boundary of the site. Buildings along Emporia Avenue will be built with a cottages-style Craftsman -feel. A sample rendering and photographic examples of the architectural style are provided in Figure 6-1 and Figure 6-2, on the proceeding page.

Figure 6-1: Architectural Renderings

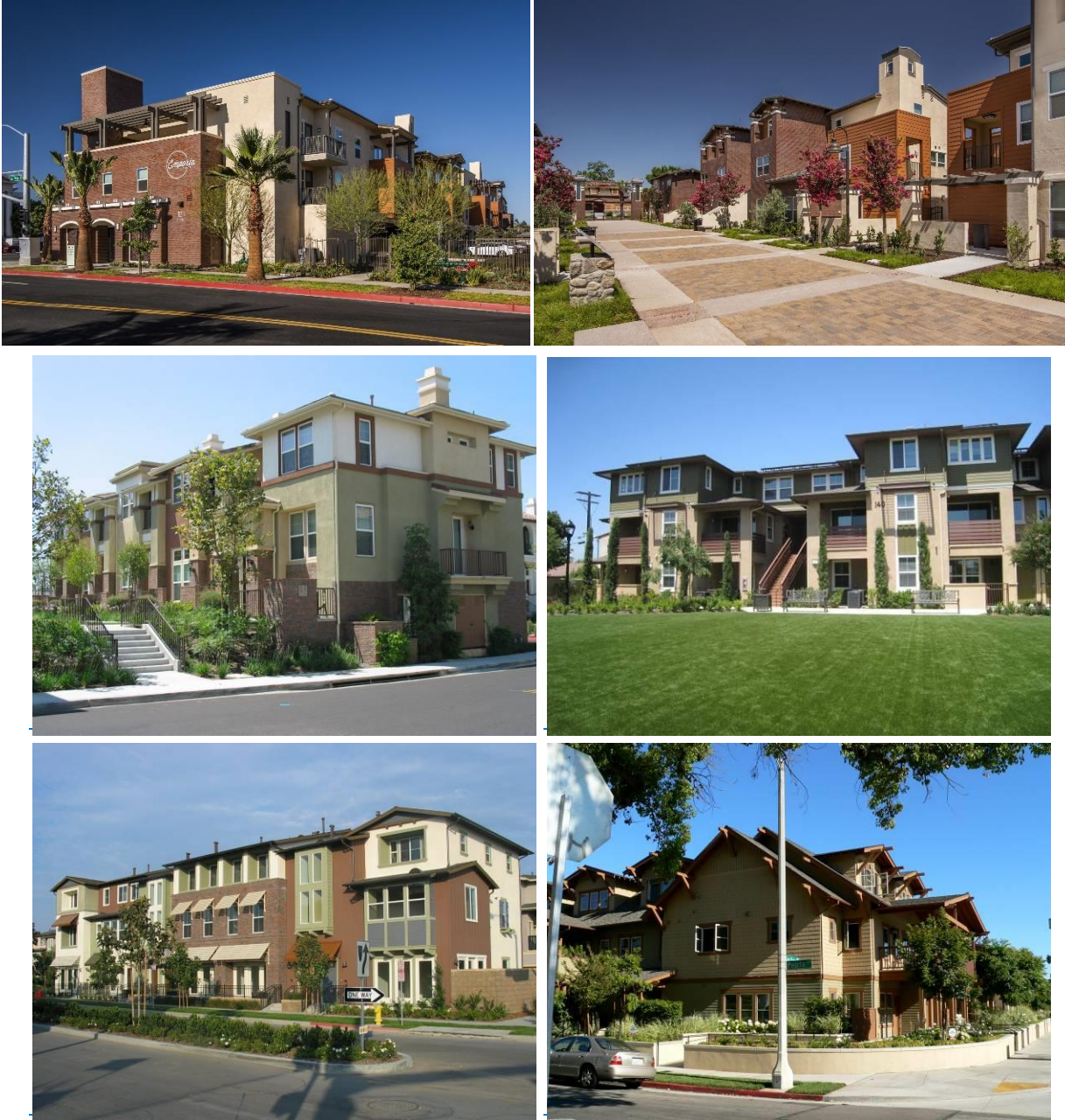
Emporia I



Emporia II



Figure 6-2: Architectural Examples



6.3 – Massing and Scale

All buildings shall be in scale with surrounding development with a majority of the building placement to occur towards the boundary of the project site. A prominent building edge shall

face Holt Boulevard. Buildings should be 2-3 stories in height containing a varied amount of units in each building with varying façade planes to decrease creating a “monotonous” look.

Amenities and site design shall be developed with the pedestrian scale in mind.

6.4 – Architectural Details

Architectural variations should occur between buildings to help distinguish each building and create variety within the contemporary craftsman style. Architectural details should be consistent with the architectural style of each individual building. Details should complement and enhance openings and accentuate the overall design of each building. Detailing can be achieved through the use of different colors and materials (brick, siding, etc.) to create interesting elevations.

Awnings should be placed where feasible over south facing windows and adequate shade should be provided for outdoor spaces. Coverings should use materials compatible with the building roofing and façade materials in creating a comprehensive building design. Exposed rafter beams should be integrated into building design along with trellises to help define outdoor spaces.

6.5 – Materials and Color

Materials and colors shall be compatible with and complementary to the overall architectural style of the development project. Larger building masses should use a combination of dark and light colors to break up building planes and use light colored trim to help accentuate roof features. All colors should be complementary and utilize primarily “earth tone” type colors. Materials should be a mixture of stucco, wood and brick siding in a variety of complementary colors and a concrete tile roof material should be used.

6.6 – On-Site Streetscapes and Pedestrian Walkways

Street furniture and decorative paving should be used throughout the project area to enhance architectural design. The use of complementary materials and textures should be used to create a continuity and consistency that is desired throughout the project area. A variety of decorative elements should be incorporated into the design of the project area including, but not limited to, benches, tree grates, bike racks, enhanced paving, trash receptacles and bollards.

A selection of appropriate elements shall consider sidewalk widths, size and spacing of street trees, importance of the pedestrian path, and the requirement to maintain a minimum 4 feet of clear space for pedestrian movement in all areas. All streetscape features shall comply with the Americans with Disabilities Act (ADA), along with accessibility requirements for public services, such as police and fire.

Figure 6-3: Streetscape Examples



6.7 – On-Site Lighting

Decorative light fixtures compatible with the architectural style of the buildings shall be provided on-site.

A minimum of one-foot-candle of light shall be provided within parking areas and 0.5 foot-candles within pedestrian pathways, in compliance with the City of Ontario Police Department standards.



6.8 – Signs

Signs within the project area shall comply with the Division 8.0 (Sign Regulations) of the Ontario Development Code.

6.9 – Off-Site Improvements

All off-site improvements shall be installed in accordance with City standards and to the satisfaction of the City Engineer.

7 – Infrastructure and Utilities

7.1 – Infrastructure

All improvements and alterations to public infrastructure (sewer, water, recycled water, storm drain, etc.) shall obtain approval from all appropriate agencies. Public sewer and water lines within the project area on Transit Street that will only serve the project area once the street is vacated will not be the maintenance responsibility of the City. Public sewer and water lines within the project area on Fern Avenue that will only serve the proposed project area is to be vacated and a 30' wide public utilities easement (PUE) shall be granted to the City of Ontario.

If infiltration is deemed permissible by a licensed geotechnical engineer, on-site storm water drainage facilities shall be provided for capture and infiltration of a 2-yr, 24-hour storm event, consistent with the San Bernardino County Storm Water Program's Water Quality Management Plan (WQMP) requirements for new residential development. Storm-water capture and infiltration facilities may will include the utilization of ~~pervious concrete pavers in enhanced paving areas and MaxWell Drywells~~ or underground storm water infiltration chambers and pretreatment devices, for the remainder of the site.

Upon development of the project area, ~~existing waterlines smaller than 8 inches in diameter fronting the project site along Emporia Street and Vine Avenue shall be upgraded~~ all existing main sizes will be determine per the fire flow test results and will apply to the following: to a minimum of 8 inches.

- a. Transit Street: From Fern Avenue to Laurel Avenue.
- b. Emporia Street: From Fern Avenue to Euclid Avenue.
- c. Palm Avenue: From Holt Boulevard to Emporia Street.
- d. Laurel Avenue: From Holt Boulevard to Emporia Street

Upon water line upgrade, the existing water line in Transit Street through the project area may be abandoned. An 8-inch water line shall be installed within Fern Avenue from Transit Street north to Holt Boulevard.

Any connection into the existing storm drain located within Transit Street shall require approval from the County of San Bernardino Flood Control District. See Exhibit 7-1 (Easement & Dedication Locations) for the approximate storm drain location.

The project area is a Priority Land Use (PLU and shall comply with the statewide Trash Provisions adopted by the State Water Resources Control Board (SWRCB).

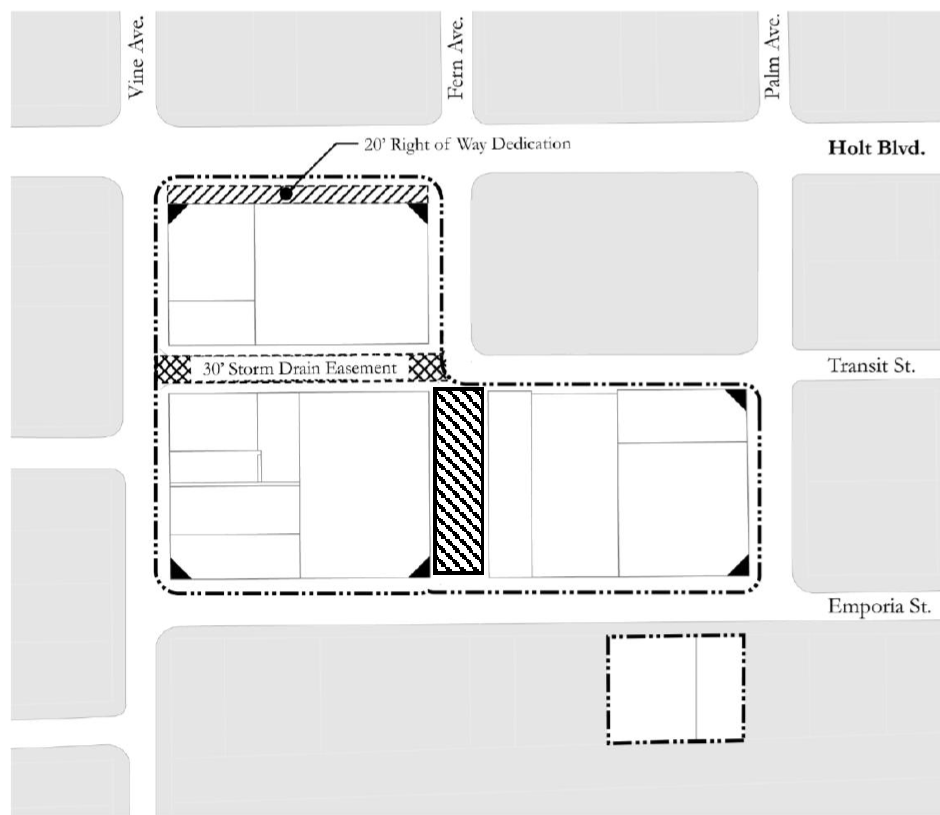
7.2 – Street Improvements

Street improvements (street, sidewalk, parkway, curb, gutter, traffic signal, street lights, etc.) shall be constructed with the development of the project site. Improvements will be required on the streets adjacent to the project area including Holt Boulevard, Emporia Street, Fern Avenue, Transit Street, Palm Avenue, and Vine Avenue. Design and construction of street

improvements shall be in accordance with the City standards, and to the satisfaction of the City Engineer. Exhibit 7-2 (Street Sections) identifies the improvements for each street adjacent to the project site.

King standard lighting (City of Ontario Standard Drawing No. 5103) shall be required within the public right-of-way. Upon development of the project site, traffic signal alterations will need to occur to allow for safe and proper traffic movement. Traffic signal modifications shall be required at the intersection of Holt Boulevard and Vine Avenue.

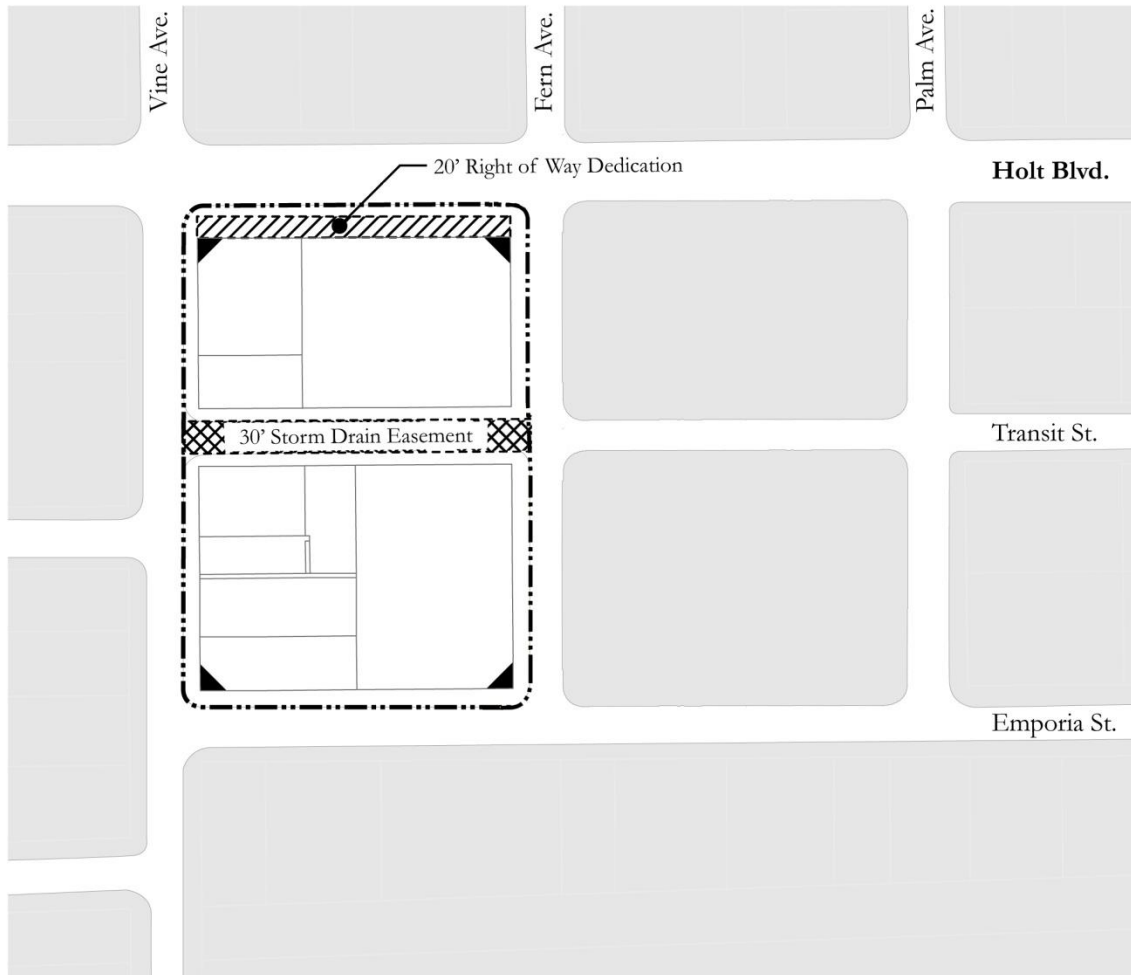
Exhibit 7-1: Easement & Dedication Locations







LEGEND

- Right of Way Dedication
- Proposed Storm Drain Easement
- Corner Cut Back Dedication
- Project Boundary
- Proposed Street Vacation (13,654 Sq.Ft.) with PUE





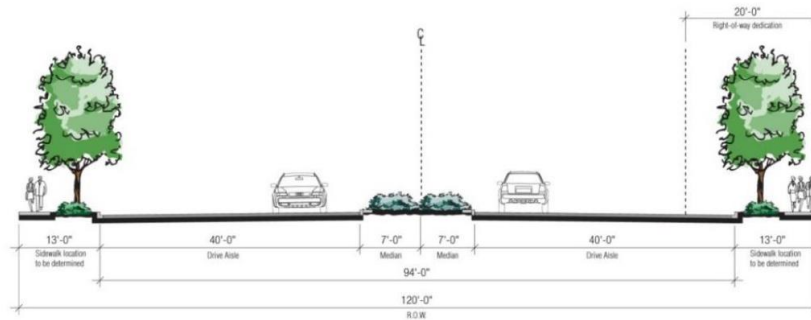
LEGEND

-  Right of Way Dedication
-  Proposed Storm Drain Easement
-  Corner Cut Back Dedication
-  Project Boundary

TO BE DELETED

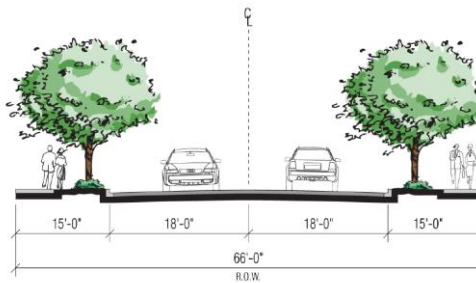


Exhibit 7-2: Street Sections



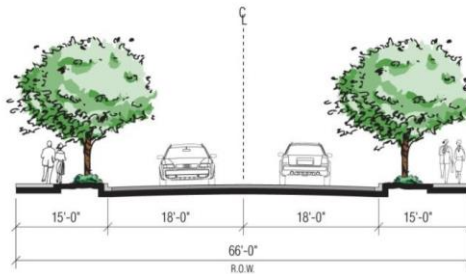
Divided Arterial Section

Holt Boulevard
not to scale



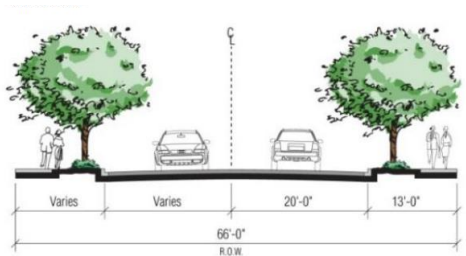
Local Street Section

Palm Avenue and Emporia Street
not to scale



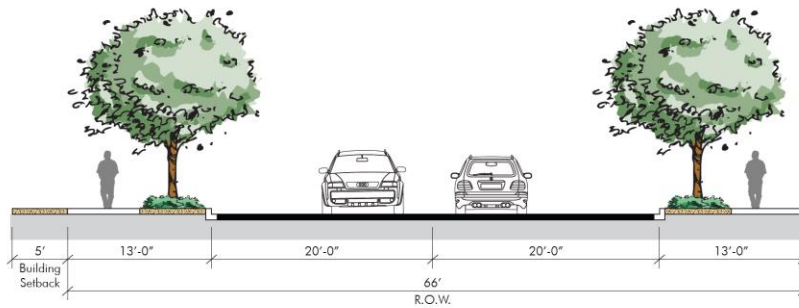
Local Street Section

Fern Avenue (Fern Avenue is vacated from Transit Street to Emporia Street)
not to scale

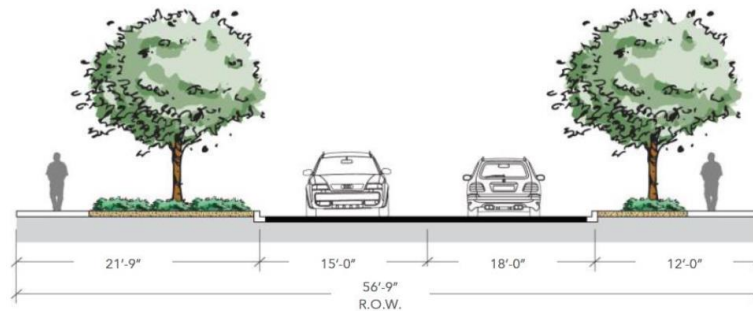


Local Street Section

Vine Avenue
not to scale



Palm Avenue



Transit Street (Transit Street is vacated from Vine Avenue to Fern Avenue)

7.3 – Public Utilities

All existing dry utilities (electricity, cable, telephone, gas, etc.) along [the south side of Transit Street \(Vine Avenue to Fern Avenue\) and within Emporia I shall be](#) relocated and re-routed to the streets bounding the project area, as required and approved by the affected utility agencies/companies. [All public utilities along the proposed vacated portions of Fern Avenue \(Transit Street to Emporia Street\) and as part of Emporia II shall be relocated outside of the abovementioned street and undergrounded along the adjoining streets, as required and approved by the affected utility companies/agencies prior to recordation of PM-20339. Written approvals from the affected utility companies/agencies shall be submitted to the Engineering Department indicating that such utilities will be relocated and re-routed to adjacent streets and](#)

easements are not required on Fern Avenue. A public utility easement shall be reserved for the City of Ontario for the existing sewer and water mains located in Fern Avenue. In the event that relocation of a utility is not feasible, a Public Utility Easement (PUE) shall be reserved for the existing utility(s) prior to the vacation of Transit Street.

7.4 – Street Dedication, Vacation & Easements

In order to create a single developable project site for the proposed Planned Unit Development, a portion of the street that bisects the project site in an east-west direction in Emporia I (Transit Street from Vine Avenue to Fern Avenue) will be required to be vacated. The existing sewer lines located within this section of Transit Street to be vacated, may be abandoned upon vacation of the street. Fern Avenue from Emporia Street to Transit Street will be required to be vacated and privatized as part of Emporia II to allow connectivity between the overall project.

The ultimate Holt Boulevard right-of-way is 60 feet, measured from centerline, and will require an additional 20 feet of street dedication. Corner cut-off areas within the project area shall be dedicated at the intersection of Holt Boulevard and -Fern Avenue, Fern Avenue and Emporia Street, Fern Avenue and Transit Street, Emporia Street and Vine Avenue, Emporia Street and Palm Avenue, Palm Avenue and Transit Street, and Vine Avenue and Holt Boulevard per Engineering Standard Drawing #13011209. The ultimate half-width street section, measured from the centerline, shall be widened to 18 feet along the project frontage of Transit Street General location of the street dedication and corner cut-off areas are shown on Exhibit 7-1: Easement & Dedication Locations.

The proposed 30-foot wide storm drain easement in Emporia I runs along Transit Street (Vine Avenue to Fern Avenue) to facilitate an existing 7.5 foot by 8 foot reinforced Concrete Box (RCB). Furthermore, a 30 foot wide storm drain easement, which runs along Fern Avenue north of Transit Street, is require to facilitate a reinforced concrete pipe (RCP). The easements will be reserved and accommodated with the development of the project site. The easement locations are shown on Exhibit 7-1 (Easement & Dedication Locations).

8 – Historic Preservation

All historic structures are subject to the requirements of Ontario Development Code Sections 4.02.040 through 4.02.065, and Division 7.01 (Historic Preservation) of the Ontario Development Code, as applicable.

8.1 – Historic Background

The ~~two~~three-two block area that comprises Emporia I of this Planned Unit Development, was once home to the Casa Blanca Hotel and Developer’s Row. During Ontario’s early settlement period, several homes were constructed for prominent city pioneers along Emporia Avenue. This stretch of development was referred to as “Developer’s Row.” All of the homes from this development have been demolished, with the exception of the Ford-Collins House which was moved to its current location at 227 West Main Street sometime after 1915, to facilitate the construction of the Casa Blanca Hotel. An Environmental Impact Report was certified by City Council in 1998 for the demolition of the Casa Blanca Hotel.

In 2007, the property at 205 and 205 ½ was acquired by the City’s Redevelopment Agency. A Mitigated Negative Declaration was prepared for the demolition of both buildings. A request to defer the Certificate of Appropriateness (File No. PHP07-012) for the replacement structure was approved by the Historic Preservation Commission on September 25, 2007. Development of ~~the project site will~~Emporia I required both Development Plan approval pursuant to Ontario Development Code Section 4.02.025 (Development Plans) and the approval of a Certificate of Appropriateness pursuant to the requirements of Ontario Development Code Section 4.02.050 (Historic Preservation – Certificates of Appropriateness and Demolition of Historic Resources).

The one block area bound by Transit Street to the north, Emporia Street to the south, Fern Avenue to the west and Palm Avenue that comprises of Emporia II of this Planned Unit Development includes the American Legion Building located at 310 West Emporia Avenue and a single-story office building located at 201-215 South Fern Avenue and is shown on Exhibit 8-1 (Historic Resources Map).The buildings were reviewed for historic significance and determined to be eligible for listing on the local inventory of historic resources.

~~8.2 – Existing Historic Structures~~

~~Remaining within the area of the project is site are historic properties located at 201 through 215 South Fern Avenue, The Fallis House, and 310 West Emporia Avenue (American Legion Hall). Location of these properties is shown on Exhibit 8-1 (Historic Resources Map).~~

~~8.3 –~~8.2 – Certificate of Appropriateness

~~As mentioned in Section 8.1 (Historic Background), above, a Pursuant to the Ontario Development Code Section 4.02.050 (Historic Preservation – Certificates of Appropriateness and Demolition of Historic Resources, Certificate of Appropriateness approval is was is required for the demolition of the existing historic resources within the PUD area. Prior to~~

~~issuance of an approval, to be submitted along with a Development Plan to construct replacement buildings, structures or objects shall be approved. , for Emporia I, to implement this Planned Unit Development. Approval of a Certificate of Appropriateness is to ensure a compatible project that does not detract from the historic site nor adversely affect adjacent historic properties, such as the Fallis House (Local Landmark No. 1), located at 122 South Vine Avenue, across the street to the west. A Certificate of Appropriateness is required for Emporia II for the American Legion building located at 310 West Emporia Avenue and office building at 201-215 South Fern Avenue. Approval of a Certificate of Appropriateness is to determine if certain findings can be made to determine if the demolition is appropriate and to ensure impose previously approved mitigation measures. a compatible project that does not detract from the historic site nor adversely affect adjacent historic properties, such as the Fallis House (Local Landmark No. 1), located at 122 South Vine Avenue, across the street to the west. Locations of adjacent historic properties are shown on Exhibit 8-1 (Historic Resources Map).~~

Exhibit 8-1: Historic Resources Map



~~There are several policies in The Ontario Plan and regulations in the Ontario Development Code which support and encourage preservation of historic resources. More specifically, TOP contains policies for the management of the City's cultural resources through the updating and maintenance of the City's historic sites and buildings inventory compiled in the Ontario Register. In order to support the preservation goals, the Ontario City Council adopted a tier system with standard criteria and procedures for evaluating the significance of historic or potentially historic resources threatened by major modifications or demolition through a regulatory process. The Ontario Development Code establishes criteria for Tier I, Tier II, or Tier III historic resources, with Tier I and II being the most historically significant. The tier system identifies those historic resources that have the highest preservation value in terms of their architectural and/or historical contribution to the City and establishes a method to evaluate the impacts of their loss in the case of major modification or demolition. Major modification or demolition should not occur to Tier I or Tier II historic resources and preservation and/or avoidance of such historical resources to prevent demolition is strongly encouraged. Whereas, Tier III historic resources may be modified or demolished under certain circumstances with appropriate mitigation measures in place.~~

~~On January 27, 2010, environmental impacts were analyzed in an Environmental Impact Report (State Clearinghouse No. 2008101140) in conjunction with File No. PGPA06-001 adopted by City Council, whereas a statement of overriding considerations for demolition of Tier III historic resources was also adopted. Prior to demolition of the Tier III historic resource, those mitigation measures listed in the Environmental Impact Report and in Section 4.02.050, Certificate of Appropriateness of the Ontario Development Code, will be implemented. As such, the following mitigation measures shall be imposed and completed prior to issuance of demolition permits of Tier III historic resources located within the project site:~~

~~The following mitigation measures shall be imposed on the project prior to the issuance of demolition permits for all Tier III designated historic properties located on the project site:~~

- ~~An effort to relocate structure must be made (such as an advertisement notifying the public of the opportunity to relocate the home to interested parties running a new paper ad making the structure available to interested parties).~~
- ~~HABS/HAER documentation of the historic resource (photos interior and exterior, written physical description of property, historical narrative, plans with dimensioned floor plan, site plan, elevations, and detailed drawings of any character defining-feature).~~
- ~~Mitigation fees for non-residential structures (\$6.50 per square foot with a cap of \$32,500) shall be paid to the Historic Preservation Mitigation Trust Fund.~~
- ~~Planning Department to identify salvageable features from the building or site to be reused either in the new project or donated to a local preservation group.~~

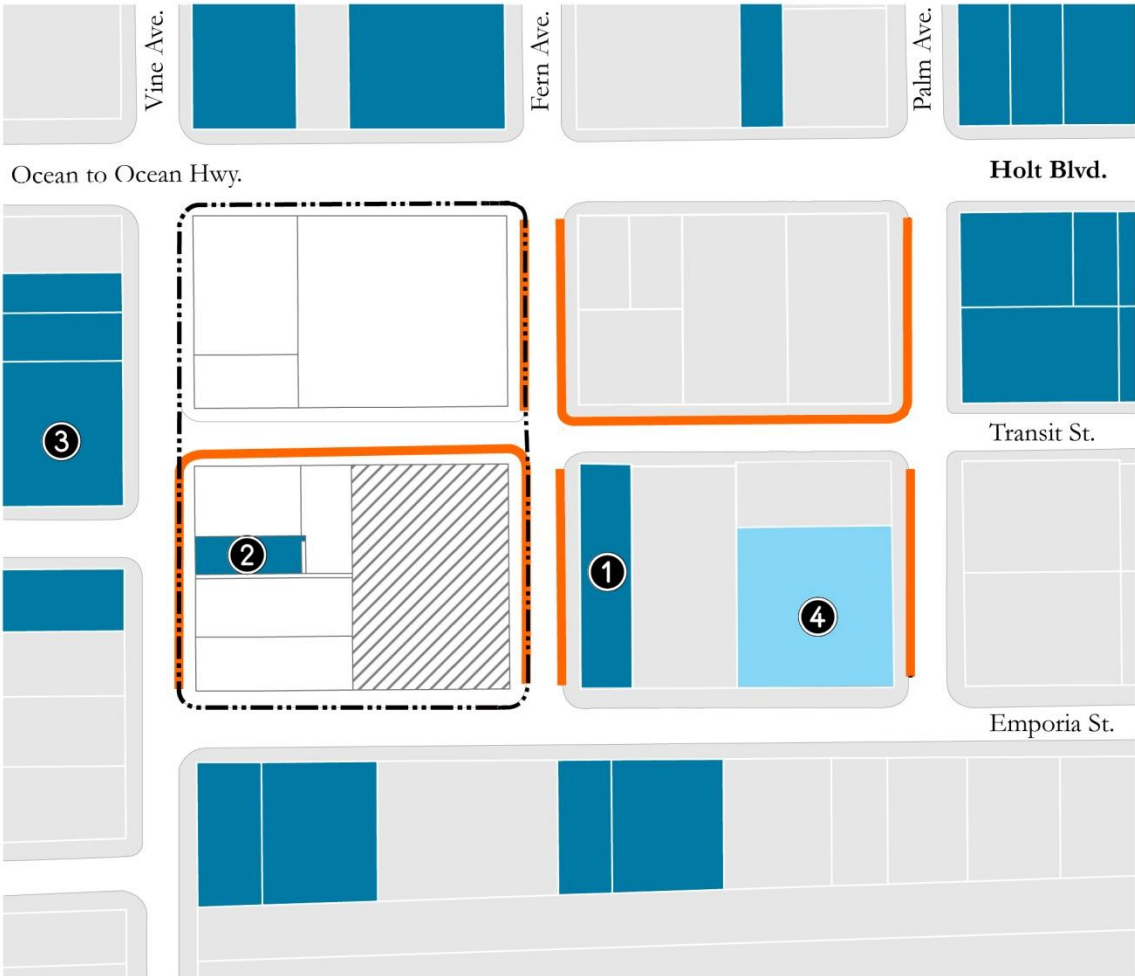
~~Demolition of any historic building within the project site shall not occur until the Approving Authority has approved a Certificate of Appropriateness for the replacement structure. The Historic Preservation Commission, upon the recommendation of the Historic Preservation Subcommittee, may waive the requirement for replacement structures if the ultimate project proposed for the site of the demolition provides an exceptional benefit to the community. Minimum findings must be made to waive the replacement structure requirements pursuant to~~

~~the requirements of Ontario Development Code Section 4.02.050 (Historic Preservation—Certificates of Appropriateness and Demolition of Historic Resources).~~

~~8.4~~8.3 – Rock Curb

Split Cobble Stone Curb (Rock Curb) exists along areas of Vine Avenue, Fern Avenue, and Transit Street in the project area. All rock curb locations in the project area are considered the lowest priority of rock curb classification. Due to this classification, the rock curb can be removed. The rocks shall be clean to the extent possible (removal of concrete) and temporarily stored on the project site or at a City facility to be determined and arranged by the Housing Agency. The rocks acquired shall be reused into the project site within pedestrian corridors, at entry points into the site (e.g. drive aisles), or within the open space areas, subject to review and approval of the Planning Department. Rock curb locations can be replaced with standard curb and gutters pursuant to City standards. Locations of the rock curb are shown on Exhibit 8-1 (Historic Resources Map).

Incorporating an interpretative plan should be considered and submitted with the Development Plan submitted to implement this Planned Unit Development. Interpretative elements should be coordinated with the design of the landscape and hardscape plans, to achieve maximum compatibility and functionality. The purpose of the interpretative plan is to convey the historic background and historic significance (such as Developer’s Row, Fallis House, Casa Blanca, [American Legion](#), and Ocean to Ocean Highway- Holt Blvd.) of the project site and surrounding area through narrative plaques and photo displays. The salvaged rock curb should be part of the interpretative plan.



LEGEND

- Historic Sites
- Potential Historic Sites
- Former Casa Blanca Hotel Site
- Existing Rock Face Curb
- Project Boundary
- 1 201 - 215 S. Fern Avenue
- 2 205 - 205 1/2 S. Vine Avenue
- 3 The Fallis House
- 4 310 W. Emporia Avenue (American Legion)



Not to Scale

9 – CEQA Compliance

An Addendum to The Ontario Plan Environmental Impact Report (SCH No. 2008101140), prepared in conjunction with File No. PGPA06-001, and certified by the City of Ontario City Council on January 27, 2010, was prepared for the project.

The Approving Authority for the Project reviewed and considered the information contained in the Addendum, the initial study, and the administrative record for the Project, including all submitted written and oral evidence. Based upon the facts and information contained in the Addendum, the initial study, and the administrative record, including all written and oral evidence, the Approving Authority found as follows:

- The Addendum and administrative record have been completed in compliance with the California Environmental Quality Act (CEQA), the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and
- The Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Approving Authority; and
- There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts; and
- The project will not introduce any new significant environmental impacts beyond those previously analyzed in the Environmental Impact Report, and all mitigation measures previously adopted by the Environmental Impact Report, are incorporated herein by this reference.

9.1 – Environmental Performance Standards

Due to the proximity of the project area to rail lines and Holt Avenue which are both generators of noise and emissions, standards have been established to mitigate these environmental impacts.

9.1.1 – Noise

To ensure a proper standard of living, noise levels shall not exceed 65 dBA for exterior noise levels and 45 dBA for interior noise levels. In order to achieve these noise levels the following mitigations shall be followed:

- MM 5.12-2: Prior to the issuance of building permits, the developer/owner shall retain an acoustical engineer to evaluate the potential for trains to create perceptible levels of vibration indoors. If vibration-related impacts are found, mitigation measures, such as use of concrete, iron, steel, or masonry materials to ensure that levels of vibration amplification are within acceptable limits to building occupants, shall be implemented. Pursuant to the Federal Transit Administration vibration-annoyance criteria, these acceptable limits are 78 VdB during the daytime and 72 VdB during the nighttime for residential uses.

Listed below are standards for implementation of MM 5.12-2:

- Construction activities shall only occur between the hours of 7:00 AM and 6:00 PM on weekdays, and 9:00 AM and 6:00 PM on Saturdays and Sundays;
- Structural noise attenuation requirements contained in Ontario Municipal Code Title 8, Chapter 15, Article 3 (Building Requirements for New Residential Construction in the 65 CNEL to 70 CNEL Noise Zone) are required to be incorporated into exterior façade units located along the eastern, western, and northern project perimeter.
- Structural noise attenuation requirements contained in Ontario Municipal Code Title 8, Chapter 15, Article 2 (Building Requirements for New Residential Construction in the 70 CNEL to 75 CNEL Noise Zone) are required to be incorporated into exterior façade units located along the southern project perimeter;
- Balconies on exterior façade units located along the project’s east, west and north perimeter street frontages shall have a solid balcony barrier of at least 5 feet above the deck height. On exterior façade units located along the project’s south perimeter street frontage, ~~no~~ balconies or yard areas shall be designed to provide noise reducing measures permitted; and
- A 6-foot high solid decorative masonry block wall or clear barrier with self closing, sound insulated gates shall be constructed between the buildings located along the southern edge of the project site where outdoor recreation is located. This will ensure exterior noise for outdoor facilities within the project will not exceed 65 dB CNEL.

9.1.2 – Air Quality

Due to the proximity of the active rail lines to the south of the project area, air quality is a concern for the project site resulting from the diesel emissions from trains. Listed below are standards for reducing the air quality impacts of the diesel emissions.

- All residential living areas shall be equipped with air filtration systems operating under a positive pressure rated at MERV 12 or higher;
- The HOA or property management will replace all unit filters on a routine basis, determined by industry standards for the filter and air conditioning HVAC systems selected;
- The active outdoor recreation areas should be shifted northward where feasible, to provide the greatest possible distance setback from the closest railroad tracks; and
- A dense tree canopy shall be established along the southern site boundary to act as a living bio-filter for particulate air pollution.

10 – Administration

10.1 – Items Not Addressed in PUD

Any terms or regulations pertaining to design, development, subdivision, administration and interpretation, and nonconforming use, structures and, which are not addressed in this PLANNED UNIT DEVELOPMENT, shall be governed by the City of Ontario Development Code

10.2 – Development Applications

Development Plan approval, pursuant to the requirements of Ontario Development Code Section 4.02.025 (Development Plans), shall be required for the physical alteration of a lot, the construction of a building, or the addition or significant alteration of an existing building. A Development Plan application shall be submitted to the Planning Department on a City application form pursuant to the requirements of Ontario Development Code Division 2.02 (Application Filing and Processing), commencing with Subsection B (Discretionary Permits and Actions) of Section 2.02.015 (Application Processing Procedures).

10.3 – Administrative Exceptions

Minor exceptions to the development standards set forth in this document may be granted by the Zoning Administrator in accordance with Section 4.02.020.C (Administrative Exceptions) of the Ontario Development Code, not to exceed 10 percent from minimum residential setback and separation requirements.

10.4 – Severability

If any portion of this Planned Unit Development is held to be invalid, unconstitutional, or unenforceable by a court of competent jurisdiction, the determination shall not affect the validity of the remaining portions of this Planned Unit Development. Moreover, the decision shall not affect, impair, or nullify this Planned Unit Development, either in whole or in part, and the remainder of this Planned Unit Development shall continue in full force and effect.

FILE NOS: PHP21-003, PMTT21-004 and PDEV21-008

SUBJECT: A Certificate of Appropriateness (File No. PHP21-003) to demolish two historic Tier III buildings located within the Project site and a Tentative Parcel Map (File No. PMTT21-004/TPM 20339) to consolidate four lots and the vacation of an adjoining section of Fern Avenue, for a total of 2.15 acres of land, in conjunction with a Development Plan (File No. PDEV21-008) to construct 50 multiple-family affordable housing dwelling units, generally located at the northwest and southwest corners of Emporia Street and Palm Avenue, within LUA2-N (Arts District- North) and LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district. (APNs:1049-054-02, 1049-054-03, 1049-054-04, 1049-054-06, 1049-059-06, and 1049-059-07) **submitted by The Related Companies of California, LLC.**


PROPERTY OWNER: City of Ontario Housing Authority and American Legion Post No. 112

RECOMMENDED ACTION: That the Planning Commission and the Historic Preservation Commission consider and adopt an Addendum to The Ontario Plan (File No. PGPA06-001) EIR (SCH# 2008101140), certified by City Council on January 27, 2010 and approve File Nos. PHP21-003, PMTT21-004, and PDEV21-008, pursuant to the facts and reasons contained in the staff report and attached resolutions, and subject to the conditions of approval contained in the attached departmental reports.

PROJECT SETTING: The Project site encompasses a one-block area within the Mixed-Use Land Use District of the Policy Plan (General Plan) and the MU-1 (Downtown Mixed Use) zoning district. The Project site is comprised of approximately 2.15 acres of land generally bordered by Transit Street to the north, Emporia Street and the Southern Pacific Railroad Corridor to the south, Fern Avenue to the west and Palm Avenue to the east. Existing land uses on



Figure 1: Project Location

Case Planner:	Diane Ayala
Planning Director Approval:	
Submittal Date:	2/23/2021

Hearing Body	Date	Decision	Action
DAB	5/17/2021	Approval	Recommend
PC	5/25/2021		Final
CC			

the Project site includes unimproved land and 2 single story commercial buildings that have been determined to be Tier III historic resources and are listed on the local register of historic properties. The existing land uses south of Emporia Street within the Project site are unimproved.

Land uses surrounding the Project site are characterized by a mixture of legal nonconforming residential uses, light industrial, and commercial uses across Emporia Street to the south; retail, light industrial uses, and vacant property across Transit Street to the north; parking lot and unimproved land across Palm Avenue to the east; and a 75-unit affordable housing development known as Emporia Family Housing Phase 1 to the west. The existing surrounding land uses, zoning, and general plan and land use designations are summarized in the "Surrounding Zoning & Land Uses" table located in the Technical Appendix of this report.

PROJECT ANALYSIS:

(1) Background — On May 23, 2017, the Planning Commission approved a Tentative Parcel Map (File No. PMTT17-005) to consolidate lots and vacate a portion of Transit Street and Development Plan (File No. PDEV17-017) to construct 75 multiple-family residential affordable apartment units on 2.80 acres of land, generally located south of Holt Boulevard, east of Fern Avenue, north of Emporia Street, and west of Vine Avenue, contingent upon City Council approval the Emporia Family Housing Planned Unit Development ("PUD"). On June 6, 2017, the City Council approved the Emporia Family Housing PUD. Construction of the 75 multiple-family dwelling units (known as "Ontario Emporia Family Housing Project - Phase 1") was completed in 2020.

On February 23, 2021, the Applicant submitted the following applications:

- A Certificate of Appropriateness (File No. PHP21-003) to allow for the demolition of two Tier III historic resources to accommodate a second phase of development to the Ontario Emporia Family Housing Project;
- A Tentative Parcel Map (File No. PMTT21-004) which proposes the consolidation of 4 parcels of land into a single parcel and the vacation of an adjoining portion of Fern Avenue, between Transit Street and Emporia Street, for a total area of 1.71 acres of land; and
- A Development Plan application (File No. PDEV21-008) proposing the expansion of the Ontario Emporia Family Housing Project by an additional 2.15-acres of land, consolidation of parcels of land, vacation of Fern Avenue, between Transit Street and Emporia Street, and the construction of 50 multiple-family affordable dwelling units (known as "Ontario Emporia Family Housing Project - Phase 2").

An Amendment to the Emporia Family Housing Planned Unit Development (File No. PUD21-001), adding a second phase of development to the Ontario Emporia Family Housing Project was also submitted for concurrent processing. Final approval of the Certificate of Appropriateness, Tentative Parcel Map, and Development Plan is

contingent upon the approval of the Amendment to the Emporia Family Housing Planned Unit Development by the City Council.

On May 13, 2021, the Historic Preservation Subcommittee recommended approval of the Certificate of Appropriateness application to the Planning/Historic Preservation Commission. On May 17, 2021, the Development Advisory Board recommended approval of the Tentative Parcel Map and Development Plan applications to the Planning Commission.

(2) Certificate of Appropriateness — A Certificate of Appropriateness is required for the demolition of historic resources. The Applicant is proposing to demolish two Tier III historic resources located within the Project area, to facilitate the development, as described below.

(i) **Office Building (201-215 South Fern Avenue):** The building is a wide one-part commercial block building in the Art/Streamline Moderne architectural style and is approximately 4,000 square feet in area. The style is indicated by a flat roof, smooth wall surfaces, the use of glass-block windows, and a thin cantilevered roof running the entire length of the building, which curves around the north-east corner. The building is situated in a rectangular building plan with a zero front setback and consists of eight commercial units. The original building addresses were 201 through 207 South Fern Avenue. It appears that there were later additions constructed in the same style, which added units in 1963 and 1978. The wall surfaces are made of concrete block covered by stucco and the entryways are recessed.

The 2003 Downtown intensive level survey suggests that the original portion of this commercial building appeared to be Eligible as a Contributor to a potential historic district, stating that "the location, setting and design of the original building contributes to a historical record of the development of commerce in the City of Ontario." The design of the building preserves a sense of the historic character of the commercial core of the City of Ontario.

(ii) **American Legion Building (310 West Emporia Street):** Constructed in 1953, the American Legion Building is approximately 10,000 square feet in area and has a T-shape plan. The roof line is broken into two parts. The first portion is slightly angled and covered with composition. There is a window band under the eave that separates the two roof sections. The second portion of the roof is flat and intersects the wall just below the window band. The front façade has two vertical divisions. The wall, under the angled roof, is a band of aluminum windows that increase in height towards the middle of the building. The wall below the window is covered with batten board and has vertical lines. The main entry door is centrally located and is under the flat roof section. There is a band of aluminum awning windows that sit below the eave and wrap the corner of the building. The building is constructed of hollow pumice concrete brick. This Modern Mid-Century building was designed by Jay Dewey Harnish of Harnish, Morgan and Causey (now referred to as HMC Architects, Inc.). HMC Architects is a nationally recognized firm with their main headquarters located in Ontario.

The American Legion is considered one of the Nation's most influential patriotic organizations and was chartered by Congress in 1919 as a patriotic Veterans organization. Focusing on service to veterans, service members, and communities, the Legion currently has about 2.4 million members in 14,000 posts worldwide. These posts are organized into 55 departments: one each for the 50 states, the District of Columbia, Puerto Rico, France, Mexico, and the Philippines.

The American Legion's national headquarters is in Indianapolis, with an office in Washington, D.C. The national organization has a full-time staff of about 300 employees. The Legion has several standing national commissions and committees that work with department, district, and post leadership to develop programs, increase membership, and recruit volunteers. Associated organizations are the American Legion Auxiliary and Sons of The American Legion. Over the years, The American Legion has founded many programs for children and youth, including American Legion Baseball and Boys Nation.

In 1919, a group of local veterans assembled in Archie Mitchell's office and formed the West End Service Men's club called "Cher-Ami". Archie Mitchell was an instrumental member in establishing the Ontario Municipal Airport. Later in 1920, the men's club was instituted as American Legion Post No. 112, with Dr. J.H. Titus serving as the first Commander. During the early 1920s, the Legion held regular meetings at the Hotpoint Clubhouse. During the late 1920s, John S. Armstrong donated the Armstrong residence to the Legion to be used as a new clubhouse. The residence was relocated from Euclid Avenue to 113 West E Street and was used as the clubhouse until 1953, when the new clubhouse was erected at the present location on Emporia Street. The Legion received top honors in 1929 for registering the highest percentage of voters in San Bernardino County. That same year, the Legion Post took option on original thirty acres of the Ontario Airport. The Ontario Post has created and participated in several programs dedicated to promoting Americanism with the principals of justice, freedom, loyalty, and democracy. Aiding the veterans of all wars and their families is the foremost program as Ontario post works for a betterment of the community.

There are several policies in The Ontario Plan ("TOP") and regulations in the Ontario Development Code that support and encourage preservation of historic resources. More specifically, TOP contains policies for the management of the City's cultural resources through the updating and maintenance of the City's historic sites and buildings inventory compiled in the Ontario Register. In order to support the preservation goals, the Ontario City Council adopted a tier system with standard criteria and procedures for evaluating the significance of historic or potentially historic resources threatened by major modifications or demolition through a regulatory process. The Ontario Development Code establishes criteria for Tier I, Tier II, or Tier III historic resources, with Tiers I and II being the most historically significant. The tier system identifies those historic resources that have the highest preservation value in terms of their architectural and/or historical contribution to the City and establishes a method to evaluate the impacts of their loss in the case of major modification or demolition. Major modification or demolition should not occur to Tier I or Tier II historic resources and preservation and/or avoidance of such historical resources to prevent demolition is strongly encouraged. Whereas, Tier III historic resources

may be modified or demolished under certain circumstances, provided appropriate mitigation measures in place.

On January 27, 2010, the City Council certified The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) and adopted a Statement of Overriding Considerations for the demolition of Tier III historic resources. Prior to demolition of the Tier III historic resource, mitigation measures listed in the Environmental Impact Report and in Section 4.02.050 (Certificate of Appropriateness) of the Ontario Development Code, will be implemented through project conditions of approval.

On August 10, 2010, the Historic Preservation Subcommittee approved a Tier III Determination, File No. PHP10-004, for the 4,000 square foot, one-story commercial office building. On January 11, 2011, the Historic Preservation Subcommittee approved a Tier III Determination, File No. PHP10-011, for the 10,000 square foot, one-story commercial American Legion building, located at 310 West Emporia Street, which are depicted in Exhibit F—Historic Resources, attached.

(3) Tentative Parcel Map — The proposed Tentative Parcel Map will consolidate four parcels of land into a single parcel and vacate the adjoining portion of Fern Avenue, between Transit and Emporia Streets, for a total area of 1.71 acres of land. This will facilitate the construction of the 50 multiple-family apartment complex and recreation building and area connecting Emporia Family Housing Phase 1 with Phase 2 (see Exhibit A—Tentative Parcel Map, attached).

(4) Site Design/Building Layout — The Project is being developed with 50 multiple-family units at a density of 29 dwelling units per acre, and is depicted in Exhibit B—Site Plan, attached. The Project is designed in a Motorcourt layout, situated on approximately 1.71 acres of land. Two detached, 2 and 3 story buildings containing 50 dwelling units in a stacked-flat configuration, are oriented towards a centrally located 26-foot wide drive aisle and two rows of parking spaces. At the end of the drive aisle is a 1,921 square foot, single-story community building containing a community room, laundry room, bathrooms, and an open barbecue area. West of the community building is a children's play area with play structure, pool, a dedicated outdoor gathering area furnished with benches, seating and tables for teens, a plaza area equipped with tables, chairs, shade trellis, and a mailroom. A 5-foot landscaped setback is proposed around the perimeter of the Project.

The North residential building is in two-parts and includes a 3-story (39.5 feet tall) module and a 2-story (25 feet tall) module, which are connected by a pedestrian bridge. The South residential building is in 3-parts and is predominately 3-story (40 feet tall), except on the east and west ends of the building, which are 2-story. Three floor plans with one-, 2-, and 3-bedroom units ranging from 563 square feet to 1,250 square feet in area are proposed, which is depicted in Exhibit D—Floor Plans, attached. All 2- and 3- bedroom units will have a private patio or balcony, which range from 76 to 100 square feet in area. Entry access to units is provided from the sides of the building, which are accessible from

the second and third floors by a pedestrian bridge or an enclosed stair well. Balconies and patios will face onto the surrounding streets or toward the interior drive aisle.

(5) Site Access/Circulation — Vehicular access onto the site is from Palm Avenue via a 26-foot wide drive aisle that runs west-east and terminates at the community building. Pedestrian access to the site is through 6 gated entries located on Transit Street, Palm Avenue, and Emporia Street. Pedestrian circulation runs north-south and east-west through a series of paved pathways and sidewalks.

A second off-site parking lot, which is situated on a 0.44-acre property and located across Emporia Street to the south, adjacent to the railroad right-of-way, has been provided. The parking lot takes vehicular access from Emporia Street and each parking space will be assigned to residents for their exclusive use.

(6) Parking — Consistent with the requirements of the Emporia Family Housing Planned Unit Development, the Project utilizes a combination of on-site and on-street, as-well-as adjacent off-site parking. All resident parking will be provided on-site or on the adjacent off-site parking lot, while guest parking spaces will be provided on-street, along Emporia Street, Transit Street, and Palm Avenue. Eighty-four tenant and 10 guest parking spaces are required by the Emporia Family Housing Planned Unit Development, for a total of 94 parking spaces. As demonstrated in the Parking Summary Table below, the project will provide a total of 94 parking spaces, consistent with the Emporia Family Housing Planned Unit Development parking requirements.

Parking Summary Table			
Product Type (No. of Units)	Number of Bedrooms	Req. Parking Per Unit	Total Provided
50 - Stacked- Flats Units	6 1-Bedroom	1 Space per One Bedroom (6 Spaces)	6
	29 2-Bedroom	1.65 Spaces per Two Bedrooms (48 spaces)	48
	15 3-Bedroom	2 Spaces per Three Bedrooms (30 spaces)	30
		Guest one space per 5 units (10 spaces)	10 Guest Parking Spaces
Totals	50 Units	94 Spaces	94 Spaces (1.88 Per Unit)

(7) Architecture — The architectural style proposed for the Project is a modern interpretation of the Prairie style that is exemplified in the hipped roof, wide eaves, brackets, rafter tails, trellis, and rectangular form, and materials, including brick veneer, horizontal and vertical cement board lap siding, smooth stucco, wood, and metal panels and is depicted in Exhibit C – Elevations, attached. Hung style windows are placed in a single or double pattern throughout the project. Staircase towers have flat roofs, are covered in stucco, and features an inset banding of vertical windows. The linear design is enhanced with vertical oriented recessed wall and window panels.

The North Building is distinguished by a combination of gable and flat roofs. The gable ends feature heavy wood beam accents and rafter tails along the eaves. Vertical siding and brick veneer accentuate projection elements on all building elevations. The South Building is distinguished predominately by hipped roofs with large overhanging eaves. The community building is covered by a metal standing seam gable and cross gable roof. Vertical banding of windows and recessed wall/window panels are continued. A combination of stucco, horizontal cement lap siding, and brick veneer accents have been provided to enhance the architectural theme. The community building is single-story and has multiple gable roofs that vary in heights. The roof is covered in a standing seam metal. The building features a series of vertical bandings of windows, a large storefront window system and 2 garage-style roll up doors. The building is finished in cement plaster and plank siding and has a wood trellis attached.

(8) Landscaping —Perimeter landscaping in the front, side, and rear yards, and throughout the interior of the Project, provides for an overall landscape coverage of 23 percent, meeting the minimum Emporia Family Housing PUD landscape requirements. The proposed on-site and public right-of-way landscape improvements will assist towards creating a walkable, safe area for pedestrians to access and circulate through the site, and is depicted in Exhibit E—Landscape Plan, attached. The landscape plan incorporates a combination of 15-gallon, 24-inch, 36-inch, and 48-inch box trees along the Project street frontage and throughout the site, which includes a diverse mix of Watergum, Holly Oak, Coast Live Oak, Bubba Desert Willow, California Fan Palm, and Paleo Verde trees. A variety of shrubs and groundcovers are also being provided, which are low or moderate water usage.

A total of 11,750 square feet of private/common open space is required for the Project and a total of 20,500 has been provided. The community building, recreational area and pool will be constructed on the west portion of the site connecting the Project to Emporia Family Housing-Phase 1. The balance of the required common area is dispersed throughout the Project site in the form of passive landscaped areas.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed Project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

(1) City Council Goals.

- Operate in a Businesslike Manner
- Focus Resources in Ontario's Commercial and Residential Neighborhoods

(2) Vision.

Distinctive Development:

- Commercial and Residential Development
 - Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

(3) Governance.

Decision Making:

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
 - G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision.

(4) Policy Plan (General Plan)

Land Use Element:

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.
 - LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.

Housing Element:

- Goal H2: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.

➤ H2-1 Corridor Housing. We revitalize transportation corridors by encouraging the production of higher density residential and mixed-uses that are architecturally, functionally and aesthetically suited to corridors.

➤ H2-2 Historic Downtown. We foster a vibrant historic downtown through facilitating a wide range of housing types and affordability levels for households of all ages, housing preferences, and income levels.

➤ H2-5 Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices, and other best practices.

➤ H2-6 Infill Development. We support the revitalization of neighborhoods through the construction of higher-density residential developments on underutilized residential and commercial sites.

Goal H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.

➤ H5-2 Family Housing. We support the development of larger rental apartments that are appropriate for families with children, including, as feasible, the provision of services, recreation and other amenities.

Community Economics Element:

▪ Goal CE1: A complete community that provides for all incomes and stages of life.

➤ CE1-6 Diversity of Housing. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.

Community Design Element:

▪ Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

➤ CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.

➤ CD1-2 Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.

➤ CD1-3 Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.

▪ Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional, and distinct.

➤ CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:

- Building volume, massing, and height to provide appropriate scale and proportion;
- A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

➤ CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.

➤ CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

➤ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➤ CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.

➤ CD2-11 Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use

areas, industrial developments, and public places that reinforce them as uniquely identifiable places.

- Goal CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

- CD3-1 Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.

- CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.

- CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.

HOUSING ELEMENT COMPLIANCE: The Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the proposed project exceeds the 31-dwelling units and density of 25.1 dwelling units per acre as specified in the Available Land Inventory.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

ENVIRONMENTAL REVIEW: The environmental impacts of the Project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by the Ontario City Council on January 27, 2010,

in conjunction with File No. PGPA06-001. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures are a condition of project approval and are incorporated herein by this reference.

CONDITIONS OF APPROVAL: See attached department reports.

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

	Existing Land Use	General Plan Designation	Zoning Designation
Site	Office, commercial, assembly, and undeveloped	Mixed Use	LUA-3 (Holt Boulevard District) of MU-1 (Downtown Mixed-Use)
North	Retail and undeveloped	Mixed Use	LUA-3 (Holt Boulevard District) of MU-1 (Downtown Mixed-Use)
South	Wholesale machinery and undeveloped	Mixed Use	LUA2-N (Arts District- North) of MU-1 (Downtown Mixed-Use)
East	Parking lot and undeveloped	Mixed Use	LUA2-N (Arts District- North) of MU-1 (Downtown Mixed-Use)
West	Multi-family residential	Mixed Use	LUA-3 (Holt Boulevard District) of MU-1 (Downtown Mixed-Use)

General Site & Building Statistics

Item	Required Min./Max.	Provided (Ranges)	Meets Y/N
Project area (in acres):	N/A	2.15	-
Maximum project density (dwelling units/ac):	25.1 /75 DUs/AC	29	Y
Maximum coverage (in %):	N/A	67 percent	-
Minimum lot size (in SF):	N/A	93,671 Sq. Ft.	-
Minimum lot depth (in FT):	N/A	210 FT	-
Minimum lot width (in FT):	N/A	356 FT	-
Front yard setback (in FT):	5 FT	14-5 FT	Y
Side yard setback (in FT):	5 FT	14-5 FT	Y
Rear yard setback (in FT):	5 FT	14-5 FT	Y
Drive aisle setback (in FT):	N/A	N/A	-
Parking setback (in FT):	N/A	8.5- 11 FT	-
Structure setbacks (in FT):	9 FT	13 FT	Y
Maximum height (in FT):	55 FT	40- 19 FT	Y
Parking – resident:	1 bd- 1 Space 2 bd- 1.65 Spaces 3 bd- 2 Spaces	6 (1 bd)- 6 Spaces 29 (2 bd)- 48 Spaces 15 (3 bd)- 30 Spaces = 84 Spaces	Y
Parking – guest:	1 per 5 units	.2 x 50 units= 10 Spaces	Y
Open space – private:	1 bd- 0 2 bd- 50 Sq. Ft.	1 bd- 0 2 bd- 76 Sq. Ft.	Y

<i>Item</i>	<i>Required Min./Max.</i>	<i>Provided (Ranges)</i>	<i>Meets Y/N</i>
	3 bd- 100 Sq. Ft.	3 Bd- 100 Sq. Ft.	
<i>Open space – common:</i>	235 Sq. Ft. per unit	11,750 Sq. Ft.	Y

Dwelling Unit Statistics:

<i>Unit Count</i>	<i>Size (in SF)</i>	<i>No. Bedrooms</i>	<i>No. Bathrooms</i>	<i>No. Stories</i>	<i>Private Open Space (in FT)</i>
6	563-579	1	1	Flat	0
29	865	2	1	Flat	76
15	1,250	3	2	Flat	100

Exhibit A—TENTATIVE PARCEL MAP

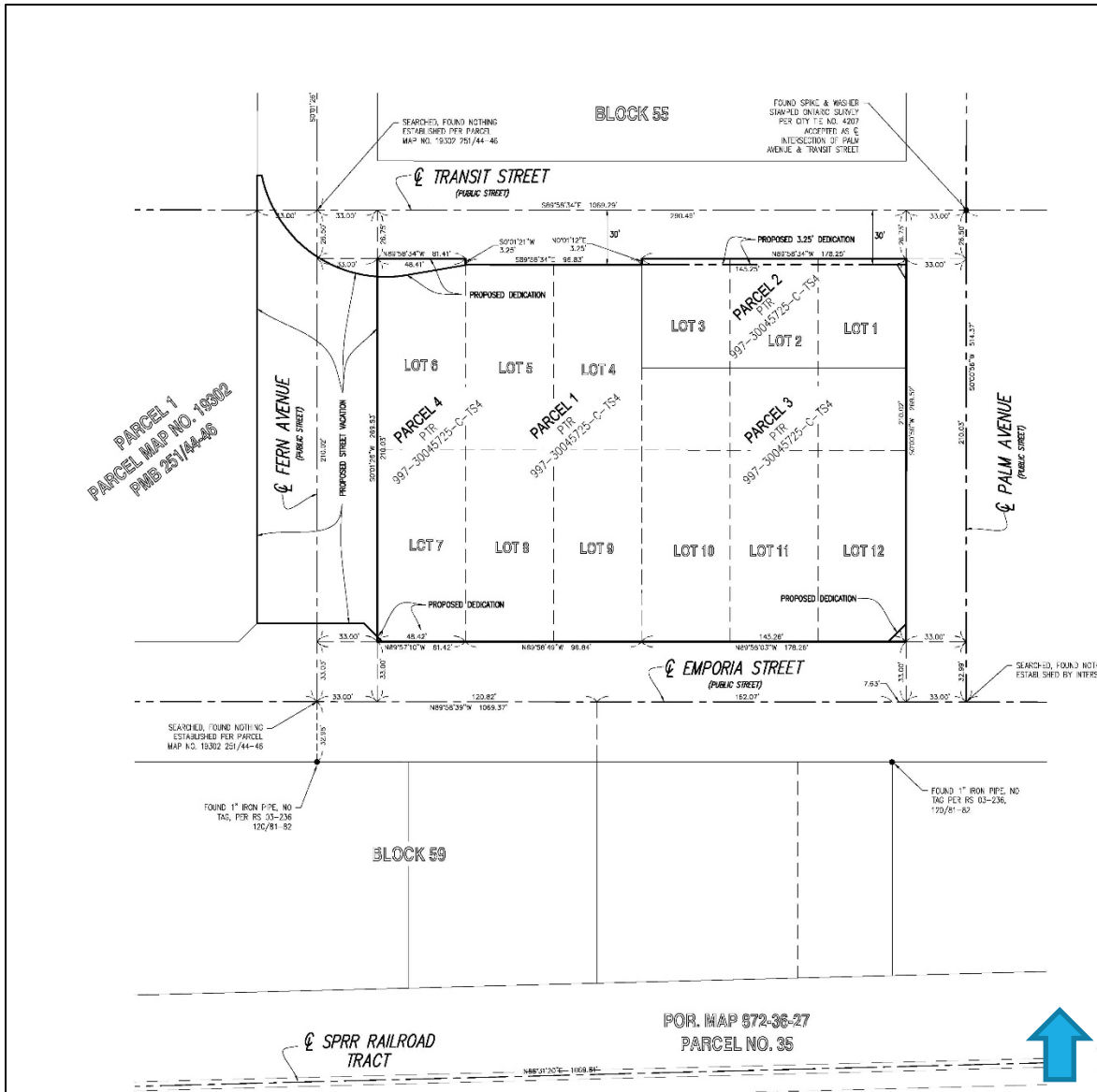


Exhibit B—SITE PLAN

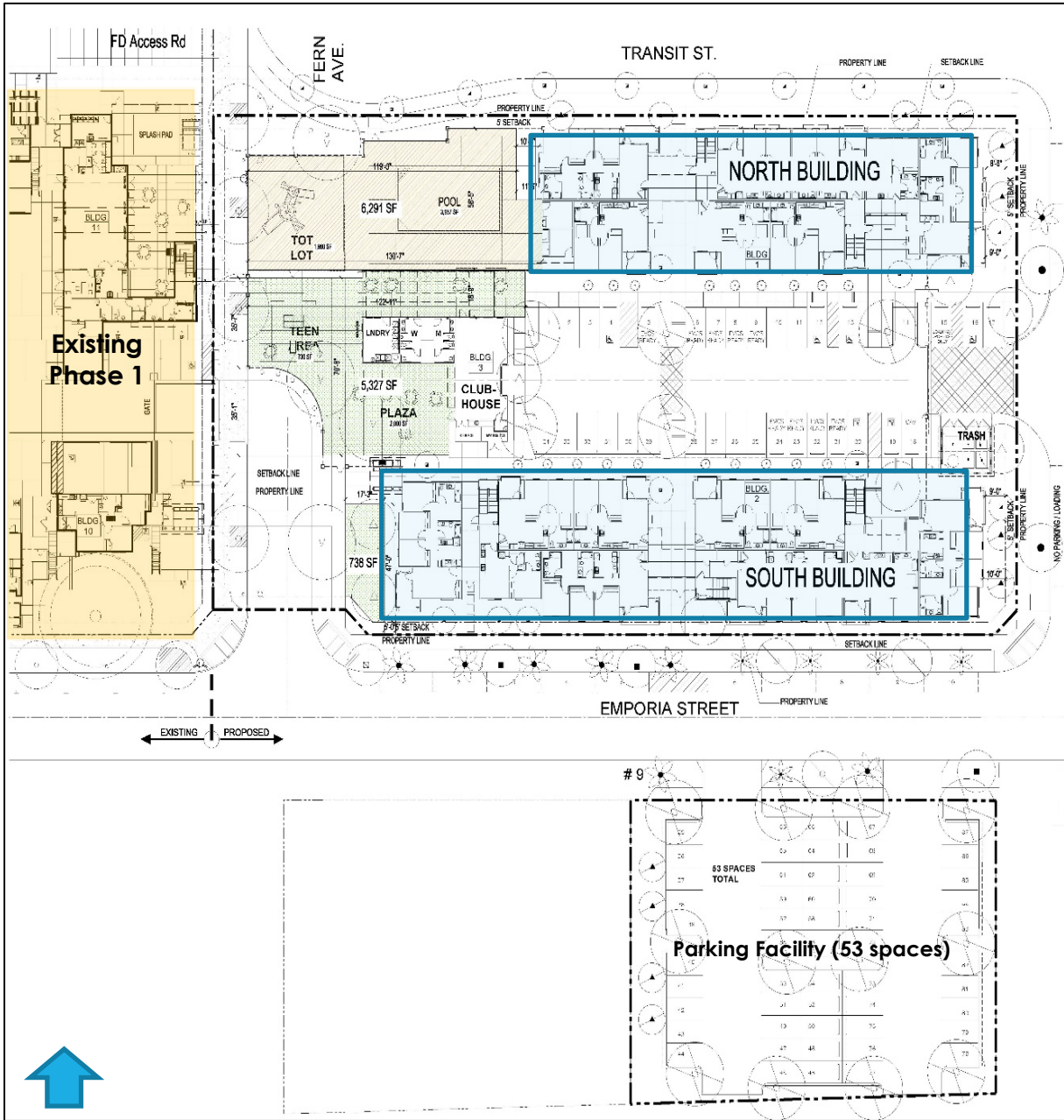


Exhibit C—ELEVATIONS



North Building



North Building

Exhibit C—ELEVATIONS (CONTINUED)



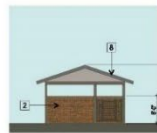
- ELEVATION KEY NOTES**
- 1 1" CEMENT PLASTER
 - 2 THIN BRICK VENEER OF 7/8" CEMENT PLASTER
 - 3 CEMENTITIOUS PLANK LAP SIDING OF MIN. 1/2" SOLID BREATHING
 - 4 CEMENTITIOUS PANEL - SMOOTH
 - 5 FOAM TRIM WITH COLOR COAT CEMENT PLASTER FINISH
 - 6 RAFTER TAILS
 - 7 CONCRETE FLAT TILE ROOF
 - 8 STANDING SEAM METAL ROOF
 - 9 VINYL WINDOW - MIN. SFC 35-49
 - 10 STONEFRONT SYSTEM
 - 11 FOLD UP GARAGE DOOR
 - 12 METAL QUADRANT - 42"
 - 13 WOOD TRELLIS
 - 14 DECORATIVE METAL PANEL (PARASOLEL)
 - 15 FOAM CORNELL WITH COLOR COAT CEMENT PLASTER FINISH
 - 16 8" SOLID PACTY BALCONY WALL - 42" H. SOLID WALL PER ELEV. WITH 1/4" EMERGED GL. VIEW PANEL ABOVE.



South Building



TRASH ENCLOSURE -
COURTYARD ELEVATION



TRASH ENCLOSURE -
WEST ELEVATION



- ELEVATION KEY NOTES**
- 1 1" CEMENT PLASTER
 - 2 THIN BRICK VENEER OF 7/8" CEMENT PLASTER
 - 3 CEMENTITIOUS PLANK LAP SIDING OF MIN. 1/2" SOLID BREATHING
 - 4 CEMENTITIOUS PANEL - SMOOTH
 - 5 FOAM TRIM WITH COLOR COAT CEMENT PLASTER FINISH
 - 6 RAFTER TAILS
 - 7 CONCRETE FLAT TILE ROOF
 - 8 STANDING SEAM METAL ROOF
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 - 10 STONEFRONT SYSTEM
 - 11 FOLD UP GARAGE DOOR
 - 12 METAL QUADRANT - 42"
 - 13 WOOD TRELLIS
 - 14 DECORATIVE METAL PANEL (PARASOLEL)
 - 15 FOAM CORNELL WITH COLOR COAT CEMENT PLASTER FINISH
 - 16 8" SOLID PACTY BALCONY WALL - 42" H. SOLID WALL PER ELEV. WITH 1/4" EMERGED GL. VIEW PANEL ABOVE.



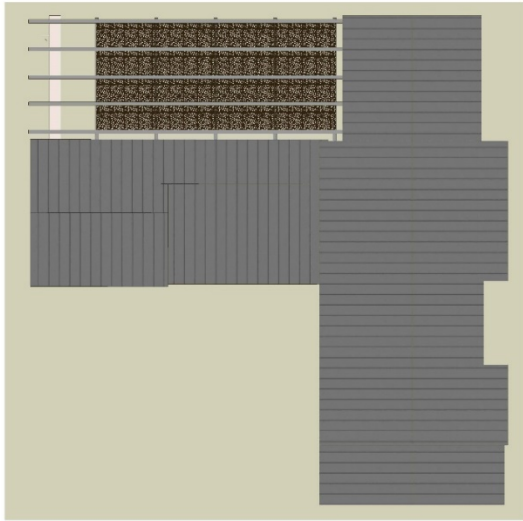
South Building

Exhibit C—ELEVATIONS (CONTINUED)

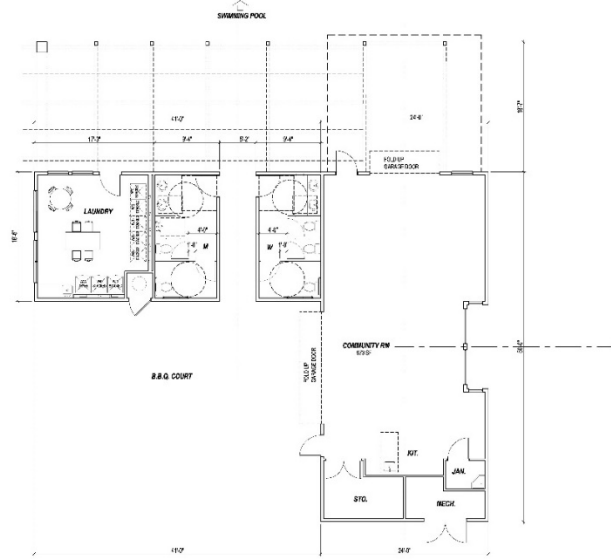


Community Building

Exhibit D—FLOOR PLANS



ROOF PLAN



FIRST FLOOR



20093
 EMPORIA PLACE PHASE 2
 ONTARIO, CA
 The Related Companies of California, LLC

COMMUNITY BUILDING FIRST FLOOR AND ROOF PLANS

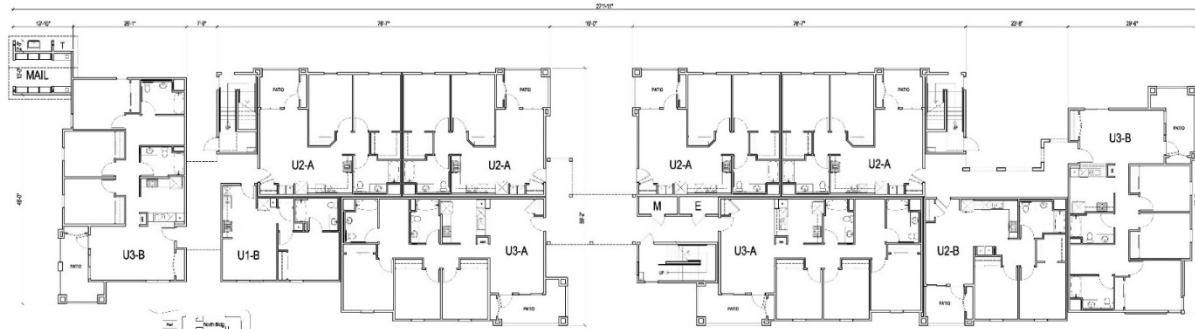


A.14 4/13/2021
DANIELIAN
 ASSOCIATES
 ARCHITECTS
 www.danielian.com 951.471.4288

Exhibit D—FLOOR PLANS (CONTINUED)



SECOND FLOOR

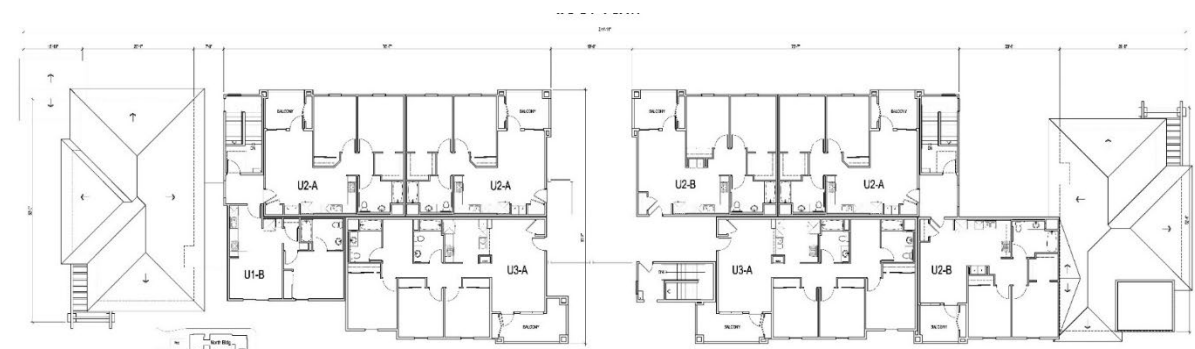


FIRST FLOOR

20093
EMPORIA PLACE PHASE 2
ONTARIO, CA
The Related Companies of California, LLC

SOUTH BUILDING- FIRST & SECOND FLOOR PLANS

A.09 4/13/2021
DANIELIAN ASSOCIATES
SCALE 1/8" = 1'-0"



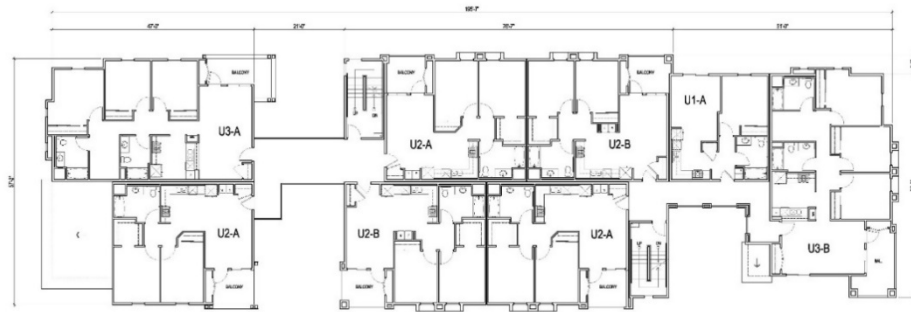
THIRD FLOOR

20093
EMPORIA PLACE PHASE 2
ONTARIO, CA
The Related Companies of California, LLC

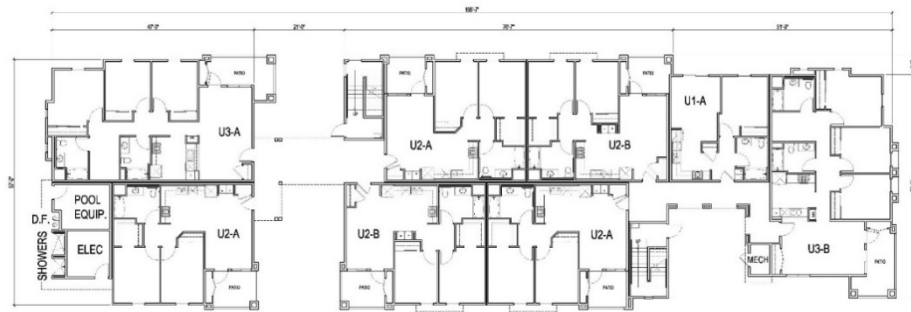
SOUTH BUILDING- THIRD FLOOR & ROOF PLANS

A.10 4/13/2021
DANIELIAN ASSOCIATES
SCALE 1/8" = 1'-0"

Exhibit D—FLOOR PLANS (CONTINUED)



SECOND FLOOR



FIRST FLOOR

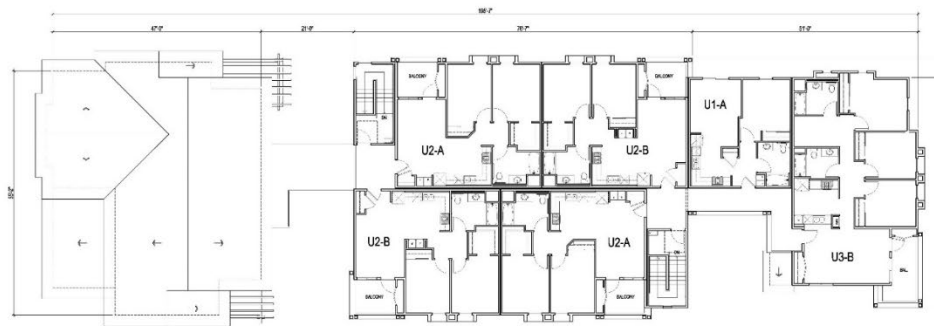


20093
 EMPORIA PLACE PHASE 2
 ONTARIO, CA
 The Related Companies of California, LLC

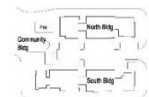
NORTH BUILDING- FIRST & SECOND FLOOR PLANS



A.05 4/13/2021
DANIELIAN ASSOCIATES
 ARCHITECTS



THIRD FLOOR



20093
 EMPORIA PLACE PHASE 2
 ONTARIO, CA
 The Related Companies of California, LLC

NORTH BUILDING- THIRD FLOOR & ROOF PLANS



A.06 4/13/2021
DANIELIAN ASSOCIATES
 ARCHITECTS

Exhibit E—LANDSCAPE PLAN



10093

Exhibit F—HISTORIC RESOURCES



Office Building- 201-215 S. Fern Ave- Tier III

Exhibit F—HISTORIC RESOURCES (CONTINUED)



American Legion Building- 310 West Emporia Street- Tier III

RESOLUTION NO.

A RESOLUTION OF THE HISTORIC PRESERVATION COMMISSION OF THE CITY OF ONTARIO, APPROVING FILE NO. PHP21-003, A CERTIFICATE OF APPROPRIATENESS FOR THE DEMOLITION OF TWO TIER III HISTORIC SINGLE-STORY COMMERCIAL BUILDINGS LOCATED AT 201-215 SOUTH FERN AVENUE AND 310 WEST EMPORIA AVENUE, TO FACILITATE THE CONSTRUCTION OF A 50-UNIT APARTMENT COMPLEX, GENERALLY LOCATED AT THE NORTHWEST CORNER OF EMPORIA STREET AND PALM AVENUE, WITHIN LUA2-N (ARTS DISTRICT- NORTH) AND LUA-3 (HOLT BOULEVARD DISTRICT) OF THE MU-1 (DOWNTOWN MIXED-USE) ZONING DISTRICT AND MAKING FINDINGS IN SUPPORT THEREOF – APNS: 1049-05-404 AND 1049-05-406

WHEREAS, Related Companies of California, LLC (“Applicant”) has filed an application for the approval of a Certificate of Appropriateness, File No. PHP21-003, as described in the title of this Resolution (hereinafter referred to as “Project”); and

WHEREAS, the City’s character and history are reflected in its cultural, historical, and architectural heritage, with an emphasis on the “Model Colony” as declared by an act of the Congress of the United States and presented at the St. Louis World’s Fair in 1904; and

WHEREAS, the City’s historical foundations should be preserved as living parts of community life and development in order to foster an understanding of the City’s past, so that future generations may have a genuine opportunity to appreciate, enjoy, and understand Ontario’s rich heritage; and

WHEREAS, the Community Development and the Aesthetic, Cultural, Open Space, and Recreational Resources Elements of the Policy Plan component of The Ontario Plan sets forth Goals and Policies to conserve Ontario’s historic buildings and districts; and

WHEREAS, the Application applies to 1.71-acres of land generally located on a one-block area bordered by Transit Street on the north, Emporia Street on the south, Palm Avenue on the east and Fern Avenue on the west, at 201-215 South Fern Avenue and 310 West Emporia Street within the LUA2-N (Arts District- North) and LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district and the Emporia Family Housing Planned Unit Development “PUD,” and is presently improved with a 2 historic single story commercial buildings and the remainder of land is undeveloped; and

WHEREAS, the property to the north of the Project site is within the LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district and is partially developed with retail. The property to the east is within the LUA-3 (Holt Boulevard District)

of the MU-1 (Downtown Mixed-Use) zoning district and is partially developed with office. The property to the south is within the LUA2-N (Arts District- North) of the MU-1 (Downtown Mixed-Use) zoning district and is partially developed with wholesale machinery. The property to the west is within the LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district, and is developed with 75-multiple-family apartment complex (Emporia Family Housing-Phase 1); and

WHEREAS, on August 10, 2010, the Historic Preservation Subcommittee approved a Tier III Determination (File No. PHP10-004) for the 4,000 square foot, one-story office building, a historic resource, located at 201-215 South Fern Avenue; and

WHEREAS, on January 11, 2011, the Historic Preservation Subcommittee approved a Tier III Determination (File No. PHP10-011) for the 10,000 square foot, one-story assembly building, the American Legion Building, located at 310 West Emporia Street; and

WHEREAS, Ontario Development Code Section 4.02.050 requires approval of a replacement structure or development plan by the City of Ontario prior to approval and issuance of a Certificate of Appropriateness for demolition; and

WHEREAS, on February 23, 2021, a Development Plan (File No. PDEV21-008) to construct 50 multiple-family dwelling units was submitted in conjunction with the Project; and

WHEREAS, the Project is contingent upon Planning Commission approval of the submitted Development Plan (File No. PDEV21-008); and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA"); and

WHEREAS, on January 27, 2010, The Ontario Plan (TOP), File No. PGPA06-001 for which an Environmental Impact Report (State Clearinghouse No. 2008101140) was adopted by City Council, determining that demolition of Tier III historic resources results in significant and unavoidable impacts that cannot be fully mitigated to a level of less than significant, despite the imposition of all feasible mitigation measures identified in the EIR for which a Statement of Overriding Consideration was documented; and

WHEREAS, the Planning Director of the City of Ontario prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as "EIR Addendum") in accordance with the requirements of

the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as "CEQA"); and

WHEREAS, the environmental impacts of this project were thoroughly analyzed in the EIR Addendum, which concluded that implementation of the Project could result in a number of significant effects on the environment that were previously analyzed in the Certified EIR, and that the Certified EIR identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Historic Preservation Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the proposed Project is located within the Airport Influence Area of Ontario International Airport ("ONT") and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan ("ALUCP") for ONT; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 13, 2021, the Historic Preservation Subcommittee of the City of Ontario conducted a hearing and issued Decision No. HPSC21-005, recommending the Historic Preservation Commission approve the Application; and

WHEREAS, as the first action on the Project, on May 25, 2021, the Planning Commission issued a Resolution recommending the City Council adopt the EIR Addendum, finding that the proposed Project introduces no new significant environmental

impacts and applying all previously adopted mitigation measures to the Project, which were incorporated by reference; and

WHEREAS, on May 25, 2021, the Historic Preservation Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, BE IT RESOLVED, that the Historic Preservation Commission of the City of Ontario as follows:

SECTION 1: Environmental Determination and Findings. As the decision-making body for the Project, the Historic Preservation Commission has reviewed and considered the information contained in the Addendum, the initial study, and the administrative record for the Project, including all written and oral evidence provided during the comment period. Based upon the facts and information contained in the Addendum, the initial study, and the administrative record, including all written and oral evidence presented to the Historic Preservation Commission, the Historic Preservation Commission finds as follows:

(1) The environmental impacts of the Project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001; and

(2) The EIR Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts; and

(4) All previously adopted mitigation measures shall be a condition of project approval, as they are applicable to the Project, and are incorporated herein by this reference; and

(5) The EIR Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Historic Preservation Commission; and

(6) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts.

SECTION 2: Subsequent or Supplemental Environmental Review Not Required. Based on the EIR Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and.

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The Project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: Housing Element Compliance. Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Historic Preservation Commission finds that based on the facts and information contained in the Application and supporting

documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the Project exceeds the 31-dwelling units and density of 25.1 dwelling units per acre as specified in the Available Land Inventory.

SECTION 4: Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance. The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as “ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making body for the Project, the Historic Preservation Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the HISTORIC PRESERVATION COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 5: Concluding Facts and Reasons. Based upon the substantial evidence presented to the Historic Preservation Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 through 4, above, the Historic Preservation Commission hereby concludes as follows:

(1) ***The proposed demolition is necessary because all efforts to restore, rehabilitate, and/or relocate the resource have been exhausted.*** The site is located within the LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district which offers the most opportunity for development of market-rate and attainable housing. Avoidance of demolition through restoration, rehabilitation, or adaptive reuse of the historic resources would render the proposed multiple-family residential development not feasible. However, relocation of the historic resource may be possible under certain conditions. Prior to demolition, the Project conditions of approval require advertisements

be placed offering the buildings at no cost for those who can relocate the buildings off site; and

(2) ***The proposed demolition is necessary because restoration/rehabilitation is not practical because the extensive alterations required would render the resource not worthy of preservation.*** The proposed multi-family residential development at the Project site is consistent with existing surrounding development and land use. Rehabilitation of the single-family residential building and incorporation into the multi-family residential development is not feasible due to the minimum development standards required for the HDR-45 zoning district; and

(3) ***The proposed demolition is necessary because failure to demolish the resource would adversely affect or detract from the character of the District.*** The proposed multiple-family residential development at the project site would implement phase 2 of the Emporia Family Housing project. The historic buildings are one-story commercial buildings. Adaptive reuse of buildings to support an increase of residential units at the site would result in major alterations to the buildings by which building height and additions to the existing floor area would dramatically alter character-defining features; and

(4) ***The resource proposed to be demolished has been assigned a Tier III designation.*** On August 10, 2010, the Historic Preservation Subcommittee approved a Tier III Determination (File No. PHP10-004) for the 4,000 square foot, one-story office building, a historic resource, located at 201-215 South Fern Avenue. On January 11, 2011, the Historic Preservation Subcommittee approved a Tier III Determination (File No. PHP10-011) for the 10,000 square foot, one-story assembly building, the American Legion Building, located at 310 West Emporia Street.

SECTION 6: Historic Preservation Commission Action. Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Historic Preservation Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A" and incorporated herein by this reference. If the City Council does not approve the Amendment to the Emporia Family Housing Planned Unit Development (File No. PUD21-001) this resolution should become null and void and of no further force and effect.

SECTION 7: Indemnification. The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the Applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate

fully in the defense.

SECTION 8: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East “B” Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 9: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Historic Preservation Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Historic Preservation Commission of the City of Ontario at a regular meeting thereof held on the 25th day of May 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage
Historic Preservation Commission
Chairman

ATTEST:

Rudy Zeledon
Planning Director and Secretary of the
Historic Preservation Commission

Historic Preservation Commission Resolution
File No. PHP21-003
May 25, 2021
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STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Historic Preservation Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____, was duly passed and adopted by the Historic Preservation Commission of the City of Ontario at their regular meeting held on May 25, 2021 by the following roll call vote, to wit:

AYES:

NOES:

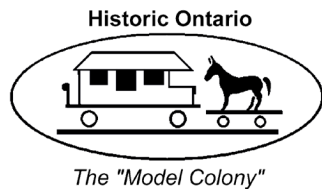
ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:
File No. PHP21-003
Departmental Conditions of Approval

(Departmental conditions of approval follow this page)



CERTIFICATE OF APPROPRIATENESS- DEMOLITION

CONDITIONS OF APPROVAL

Date: May 13, 2021
File No.: PHP21-003 (Related File Nos. PMTT21-004 & PDEV21-008)
Location: 201-215 S. Fern Avenue and 310 West Emporia Avenue (APNS: 1049-054-04 & 1049-054-06)
Prepared By: Diane Ayala, Senior Planner

Description:

A Certificate of Appropriateness for the demolition of 2 Tier III historic single-story commercial buildings located at 201-215 South Fern Avenue and 310 West Emporia Avenue to facilitate the construction of 50 multiple-family affordable housing dwelling units, generally located at the northwest corner of Emporia Street and Palm Avenue, within LUA2-N (Arts District- North) and LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district.

Conditions:

1. The Certificate of Appropriateness shall become void twenty-four (24) months from the date of approval unless a building permit has been issued and work authorized by this approval has commenced prior to the expiration date and is diligently pursued to completion.
2. Approval of this request is contingent upon City Council approval of PUD Amendment, File No. PUD21-001, and Planning Commission approval of related Tentative Tract Map, File No. PMTT21-004 and Development Plan, File No. PDEV21-008.
3. Prior to issuance of demolition building permit, every effort shall be made to relocate buildings. The buildings shall be offered at no cost for those who can relocate off site. Advertisements notifying the public of the opportunity to relocate the buildings shall be placed for a minimum of 45 days: on-site with temporary signage, in at least 3 local publications (newspapers, magazines, local organization newsletters), and on local bulletin boards (realtor's offices, local business). Applicant shall notify a minimum of 5 non-profit heritage organizations in writing of the building. A social media campaign including a dedicated web page (or post) with the building's information (description, square footage, photographs) and contact information should be incorporated into the building's advertisement.
4. Full documentation, including but not limited to as-built drawing, historical narrative and HABS photographs, of the historic resource pursuant to Historic American Building Survey (HABS) Level 3 standards shall be submitted to the Planning

Conditions of Approval

File No.: PHP21-003

May 13, 2021

Department for review and approval and subsequent release to the Ovitt Family Community Library, Model Colony History Room prior to issuance of demolition building permit.

5. A mitigation fee pursuant to Section 7.01.030 of the Ontario Development Code shall be paid to the Planning Department prior to issuance of building permit for demolition. For Tier III structures, this mitigation fee is equal to 10% of the price per square foot construction cost as established in the most current ICC Building Valuation Data. The fee amount will be provided by the Planning Department at the time of payment.
6. A determination whether items within or on the resource should be salvaged shall be made by the Planning Department. The applicant shall be responsible for the removal, relocation and donation of such items selected for salvaging. An inventory of salvaged items shall be provided by the applicant to the Planning Department prior to be to issuance of demolition permit.
7. An interpretative plan highlighting the American Legion history, architect of record history and Mid-Century Modern architectural style of the American Legion Building located at 310 West Emporia Avenue shall be constructed onsite. Rock curb within project area may be reused as in the Phase 1 Emporia Family Housing project. Content of plan shall be completed by a professional who meets the U.S. Department of the Interior Standards qualifications in the categories of history or architectural history. The Plan shall be implemented and constructed prior to final occupancy of buildings.
8. The applicant shall obtain a building permit prior to any demolition, relocation, or construction.
9. Any deviation from the approved plans shall require approval of the Planning Department and, if necessary, the Historic Preservation Commission.
10. Conditions of Approval table shall be reproduced onto the all plans submitted for permits.

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PMTT21-004, A TENTATIVE PARCEL MAP TO CONSOLIDATE 4 LOTS AND THE VACATION OF AN ADJOINING SECTION OF FERN AVENUE, FOR A TOTAL OF 1.71 ACRES OF LAND, GENERALLY BORDERED BY TRANSIT STREET ON THE NORTH AND EMPORIA STREET ON THE SOUTH, TO FACILITATE THE DEVELOPMENT OF A 50-UNIT, THREE-STORY APARTMENT COMPLEX, GENERALLY LOCATED AT THE NORTHWEST AND SOUTHWEST CORNERS OF EMPORIA STREET AND PALM AVENUE, WITHIN LUA2-N (ARTS DISTRICT- NORTH) AND LUA-3 (HOLT BOULEVARD DISTRICT) OF THE MU-1 (DOWNTOWN MIXED-USE) ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF—APNS: 1049-054-02, 1049-054-03, 1049-054-04, AND 1049-054-06.

WHEREAS, the Related Companies of California, LLC. (hereinafter referred to as "Applicant") has filed an Application for the approval of a Tentative Parcel Map, File No. PMTT21-004, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 1.71 acres of land generally located south of Transit Street, north of Emporia Street, east of Palm Avenue and vacated portion of Fern Avenue (0.31 acre of land) to the west LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district, and is presently improved with commercial, office and undeveloped land; and

WHEREAS, the property to the north of the Project site is within the LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district and is partially developed with retail. The property to the east is within the LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district and is partially developed with office. The property to the south is within the LUA2-N (Arts District- North) of the MU-1 (Downtown Mixed-Use) zoning district and is partially developed with wholesale machinery. The property to the west is within the LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district, and is developed with 75-multiple-family apartment complex (Emporia Family Housing-Phase 1); and

WHEREAS, the proposed Tentative Parcel Map will consolidate 4 parcels into a single parcel and vacate an adjoining section of Fern Avenue bordered by Transit Street on the north and Emporia Street on the south; and

WHEREAS, the proposed Tentative Parcel Map will facilitate the development of a 50-unit apartment complex (Emporia Family Housing -Phase 2); and

WHEREAS, the proposed Tentative Parcel Map will connect Emporia Family Housing Project Phase 1 with Phase 2 for a single development; and

WHEREAS, The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001 (hereinafter referred to as "Certified EIR"), in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as "EIR Addendum") in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as "CEQA"); and

WHEREAS, the environmental impacts of this project were thoroughly analyzed in the EIR Addendum, which concluded that implementation of the Project could result in a number of significant effects on the environment that were previously analyzed in the Certified EIR, and that the Certified EIR identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an EIR Addendum has been prepared to determine possible environmental impacts; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside,

and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (“ALUCP”), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, an Amendment to the Emporia Family Housing PUD (File No. PUD21-001), Tentative Parcel Map (File No. PMTT21-004), Development Plan (File No. PDEV21-008) to expand the Project site area by 2.15-acres of land, consolidate parcels of land, vacate portion of Fern Avenue south of Transit Street and north of Emporia Street, and construct 50 multiple-family affordable apartment units, known as Emporia Family Housing- Phase 2, and a Certificate of Appropriateness (File No. PHP21-003) to allow the demolition of 2 Tier III historic resources to accommodate the proposed multiple-family residential development were submitted on February 23, 2021, and are being processed concurrently; and

WHEREAS, on May 17, 2021, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Addendum and the Project, and concluded said hearing on that date, voting to issue Decision No. DAB21-024, recommending that the Planning Commission recommend the City Council approve the Addendum and Decision No. DAB21-025, recommending that the Planning Commission approve the Tentative Parcel Map (File No. PMTT21-004); and

WHEREAS, as the first action on the Project, on May 25, 2021, the Planning Commission issued a Resolution recommending the City Council adopt the EIR Addendum, finding that the proposed Project introduces no new significant environmental impacts and applying all previously adopted mitigation measures to the Project, which were incorporated by reference; and

WHEREAS, on May 25, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the decision-making body for the Project, the Planning Commission has reviewed and considered the information contained in the Addendum, the initial study, and the administrative record for the Project, including all written and oral evidence provided during the comment period. Based upon the facts and information contained in the Addendum, the initial study, and the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The environmental impacts of the Project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. SCH# 2008101140), certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001; and

(2) The EIR Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts; and

(4) All previously adopted mitigation measures shall be a condition of project approval, as they are applicable to the Project, and are incorporated herein by this reference; and

(5) The EIR Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and

(6) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts.

SECTION 2: Subsequent or Supplemental Environmental Review Not Required. Based on the EIR Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and.

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The Project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: *Housing Element Compliance.* Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the Project exceeds the 31-dwelling units and density of 25.1 dwelling units per acre as specified in the Available Land Inventory.

SECTION 4: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as “ONT”), which encompasses lands

within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making body for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 5: Concluding Facts and Reasons. Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 through 4, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed Tentative Parcel Map is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and applicable area and specific plans, and planned unit developments.*** The proposed Tentative Parcel Map is located within the Mixed-Use land use district of the Policy Plan Land Use Map, and the LUA2-N (Arts District- North) and LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district and the Emporia Family Housing Planned Unit Development "PUD." The proposed subdivision is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the project will contribute to providing "a spectrum of housing types and price ranges that match the jobs in the City, and that make it possible for people to live and work in Ontario and maintain a quality of life" (Goal LU1). Furthermore, the project will promote the City's policy to "incorporate a variety of land uses and building types that contribute to a complete community where residents at all stages of life, employers, workers, and visitors, have a wide spectrum of choices of where they can live, work, shop, and recreate within Ontario" (Policy LU1-6 *Complete Community*).

(2) ***The design or improvement of the proposed Tentative Parcel Map is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and applicable specific plans and planned unit developments.*** The proposed Tentative Parcel Map is located within the Mixed-Use land use district of the Policy Plan Land Use Map, and the LUA2-N (Arts District- North) and LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district and the PUD. The proposed design or

improvement of the subdivision is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the project will contribute to providing “[a] high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct” (Goal CD2). Furthermore, the Project will promote the City’s policy to “create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:

- A pattern of smaller, walkable blocks that promote access, activity and safety;
- Variable setbacks and parcel sizes to accommodate a diversity of housing types;
- Traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows;
- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the “outdoor living room”), as appropriate; and
- Landscaped parkways, with sidewalks separated from the curb.” (Policy CD2-2 *Neighborhood Design*).

(3) ***The site is physically suitable for the type of development proposed.*** The Project site meets the minimum lot area and dimensions of the LUA2-N (Arts District-North) and LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district and the PUD, and is physically suitable for the type of residential development proposed in terms of zoning, land use and development activity proposed, and existing and proposed site conditions.

(4) ***The site is physically suitable for the density/intensity of development proposed.*** The Project site is proposed for residential development at a density of 29 DUs/acre. The project site meets the minimum lot area and dimensions of the LUA2-N (Arts District- North) and LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district and PUD, and is physically suitable for this proposed density.

(5) ***The design of the subdivision or the proposed improvements thereon, are not likely to cause substantial environmental damage, or substantially and avoidably injure fish or wildlife, or their habitat.*** The Project site is not located in an area that has been identified as containing species identified as a candidate, sensitive, or special status species in local or regional plans, policies or regulations or by the California Department of Fish and Wildlife or the U.S. Fish and Wildlife Service, nor does the site contain any riparian habitat or other sensitive natural community, and no wetland habitat is present on site; therefore, the design of the subdivision, or improvements

proposed thereon, are not likely to cause substantial environmental damage, or substantially and avoidably injure fish or wildlife, or their habitat.

(6) ***The design of the subdivision, or the type of improvements thereon, are not likely to cause serious public health problems.*** The design of the proposed subdivision, and the right-of-way and utility improvements existing or proposed on the project site, are not likely to cause serious public health problems, as The Project is not anticipated to involve the transport, use, or disposal of hazardous materials during either construction or project implementation, include the use of hazardous materials or volatile fuels, nor are there any known stationary commercial or industrial land uses within close proximity to the subject site that use/store hazardous materials to the extent that they would pose a significant hazard to visitors or occupants to the Project site.

(7) ***The design of the subdivision, or the type of improvements thereon, will not conflict with easements acquired by the public at large for access through, or use of property within, the proposed subdivision.*** The proposed subdivision has provided for all necessary public easements and dedications for access through, or use of property within, the proposed subdivision. Furthermore, all such public easements and dedications have been designed pursuant to: (a) the requirements of the Policy Plan component of The Ontario Plan and applicable area plans; (b) applicable specific plans or planned unit developments; (c) applicable provisions of the City of Ontario Development Code; (d) applicable master plans and design guidelines of the City; and (e) applicable Standard Drawings of the City.

SECTION 6: Planning Commission Action. Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference. If the City Council does not approve the Amendment to the Emporia Family Housing Planned Unit Development (File No. PUD21-001) this resolution should become null and void and of no further force and effect.

SECTION 7: Indemnification. The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 8: Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 9: **Certification to Adoption.** The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of May 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 25, 2021, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**File No. PMTT21-004
Departmental Conditions of Approval**

(Departmental conditions of approval to follow this page)



City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

Planning Department Land Development Division Conditions of Approval

Meeting Date: May 25, 2021
File No: PMTT21-004/ TPM 20339
Related Files: File No. PDEV21-008, PUD21-001, and PHP21-003.

Project Description: A Tentative Parcel Map (File No. PMTT21-004/ TPM 20339) to consolidate 4 parcels of land, approximately 1.39 acres, into a single parcel and vacate a section of Fern Avenue, bound by Transit Street on the north and Emporia Street on the south, with a Development Plan (File No. PDEV21-008) to construct 50 multiple-family affordable housing dwellings, generally located at the northwest and southwest corners of Emporia Street and Palm Avenue, within LUA2-N and LUA-3 of the MU-1 (Downtown Mixed-Use) zoning district. (APN(s): 1049-054-02, 1049-054-03, 1049-054-04, 1049-054-06, 1049-059-06, and 1049-059-07; **submitted by The Related Companies of California, LLC.**

Prepared By: Diane Ayala, Senior Planner
Phone: 909.395.2428 (direct)
Email: dayala@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits.

(a) Tentative Parcel Map approval shall become null and void 2 years following the effective date of application approval, unless the final parcel map has been recorded, or a time extension has been approved by the Planning Commission pursuant to Development Code Section 2.02.025 (Time Limits and Extensions). This Permit does not supersede any individual time limits specified herein for performance of specific conditions or improvements.

2.2 Subdivision Map.

(a) The Final Parcel Map shall be in conformance with the approved Tentative Parcel Map on file with the City. Variations from the approved Tentative Parcel Map may be reviewed and approved by the Planning Department. A substantial variation from the approved Tentative Parcel Map may require review and approval by the Planning Commission, as determined by the Planning Director.

(b) Tentative Parcel Map approval shall be subject to all conditions, requirements and recommendations from all other departments/agencies provided on the attached reports/memorandums.

(c) The subject Tentative Parcel Map for condominium purposes shall require the recordation of a condominium plan concurrent with the recordation of the Final Parcel Map and CC&Rs.

(d) Pursuant to California Government Section 66474.9, the subdivider agrees that it will defend, indemnify, and hold harmless the City of Ontario or its agents, officers and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer of this subdivision, which action is brought within the time period provided for in Government Code Section 66499.37. The City of Ontario shall promptly notify the subdivider of any such claim, action or proceeding and the City of Ontario shall cooperate fully in the defense.

2.3 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.4 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b) Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

(c) Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(d) Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

2.5 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

2.6 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b) All drive approaches shall be provided with an enhanced pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.

(c) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(d) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

(e) Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).

(f) Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11).

2.7 Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

2.8 Signs. All Project signage shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations).

2.9 Disclosure Statements.

(a) A copy of the Public Report from the Department of Real Estate, prepared for the subdivision pursuant to Business and Professions Code Section 11000 et seq., shall be provided to each prospective buyer of the residential units and shall include a statement to the effect that:

(i) This tract is subject to noise from the Ontario International Airport and may be more severely impacted in the future.

2.10 Environmental Review.

(a) The environmental impacts of this project were reviewed in conjunction with an **Addendum to The Ontario Plan Environmental Impact Report**, certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001 (City Council Resolution No. 2010-006). This application introduces no new significant environmental impacts. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures are a condition of project approval, and are incorporated herein by this reference. All previously adopted mitigation measures shall be a condition of project approval, as they are applicable, and are incorporated herein by this reference.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

2.11 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.12 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

2.13 Additional Requirements.

(a) Project approval is contingent upon City Council approval of an Amendment to the Emporia Family Housing PUD, File No. PUD21-001.



**ENGINEERING DEPARTMENT
CONDITIONS OF APPROVAL**

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Broadband Operations & Investment and Revenue Resources Department Conditions incorporated)

<input checked="" type="checkbox"/> DEVELOPMENT PLAN <input type="checkbox"/> OTHER	<input checked="" type="checkbox"/> PARCEL MAP <input type="checkbox"/> TRACT MAP <input type="checkbox"/> FOR CONDOMINIUM PURPOSES
PROJECT FILE NO. PM-20339 RELATED FILE NO(S). PUD21-001, PMTT21-004, PDEV21-008	
<input checked="" type="checkbox"/> ORIGINAL <input type="checkbox"/> REVISED: __/__/__	

CITY PROJECT ENGINEER & PHONE NO: Miguel Sotomayor (909) 395-2108 *MS*

CITY PROJECT PLANNER & PHONE NO: Diane Ayala (909) 395-2428

DAB MEETING DATE: May 17, 2021

PROJECT NAME / DESCRIPTION: A Tentative Parcel Map and Development Plan to subdivide 4 lots into a single parcel and construct a 50-unit, 3 story apartment complex on 2.15 acres, within LUA2N and LUA3 of the MU-1 (Downtown Mixed Use) zoning district.

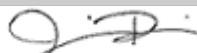
LOCATION: Emporia Avenue between Fern and Palm Avenues

APPLICANT: The Related Companies of California, LLC

REVIEWED BY: *Raymond Lee* *5/11/21*
 Raymond Lee, P.E. Date
 Assistant City Engineer

APPROVED BY: *Khoi Do* *5-11-21*
 Khoi Do, P.E. Date
 City Engineer

CITY OF ONTARIO
LANDSCAPE PLANNING DIVISION
 303 East "B" Street, Ontario, CA 91764

DAB CONDITIONS OF APPROVAL	
Sign Off	
	5/11/2021
Jamie Richardson, Sr. Landscape Planner	Date

Reviewer's Name: Jamie Richardson, Sr. Landscape Planner	Phone: (909) 395-2615
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D.A.B. File No.: PMTT21-004	Related Files:	Case Planner: Diane Ayala
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Project Name and Location: Related Emporia NW and SW corners of Emporia Street and Palm Avenue
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Applicant/Representative: The Related Companies of California, LLC randy.mai@related.com 18201 Von Karman Avenue, Suite 900 Irvine CA, 92612
--

<input checked="" type="checkbox"/>	A Tentative Tract Map (dated 4/20/2021) has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.
<input type="checkbox"/>	A Tentative Tract Map (dated 6/5/18) has not been approved. Corrections noted below are required prior to DAB approval.

CORRECTIONS REQUIRED

On Grading or Utility Construction Plans:

- Note for compaction to not be greater than 85% at landscape areas; all finished grades 1 ½" below finished surfaces; landscaped slopes to be max 3:1.
- Show or note transformers shall be located in planter areas, and set back 3' from paving for small transformers less than 4' high and 5' setback for large transformer greater than 4' high. Locate on level grade. Coordinate with landscape plans.
- Show or note backflow devices shall be located in planter areas, and set back min 3' from paving. Locate on level grade. Coordinate with landscape plans.
- Storm water infiltration devices located in landscape areas shall be reviewed and approved by the Landscape Planning Division prior to installation.
- Provide a tree inventory for existing trees include genus, species, trunk diameter, canopy width and condition. Show and note existing trees in good condition to remain and note trees proposed to be removed. Include existing trees within 15' of adjacent property that would be affected by new walls, footings or on-site tree planting. Add tree protection notes on construction and demo plans.

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT



Project File No.: PUD21-001, PMTT21-004, PDEV21-008 & PHP21-003

Address: SEC of Holt Blvd. & Fern Avenue

APN: 1049-054-02, 1049-054-03, 1049-054-06, 1049-059-07 & 1049-059-06

Existing Land Use: Vacant Lots and commercial buildings

Proposed Land Use: A PUD to establish development standards, Parcel Map to consolidate 2.15 acres into 1 parcel, a Development Plan to construct 50 Multi-family residential units

Site Acreage: 2.15 Proposed Structure Height: 55 FT

ONT-IAC Project Review: n/a

Airport Influence Area: ONT

Reviewed By: Lorena Mejia

Contact Info: 909-395-2276

Project Planner: Diane Ayala

Date: 5/11/2021

CD No.: 2021-016

PALU No.: n/a

The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="checkbox"/> High Terrain Zone	<input type="checkbox"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input checked="" type="checkbox"/> FAA Notification Surfaces	<input checked="" type="checkbox"/> Recorded Overflight Notification
<input type="radio"/> Zone 2	<input type="checkbox"/> 65 - 70 dB CNEL	<input checked="" type="checkbox"/> Airspace Obstruction Surfaces	<input type="checkbox"/> Real Estate Transaction Disclosure
<input type="checkbox"/> Zone 3	<input checked="" type="checkbox"/> 60 - 65 dB CNEL	<input type="checkbox"/> Airspace Avigation Easement Area	
<input type="radio"/> Zone 4		Allowable Height: <u>90 FT</u>	
<input type="radio"/> Zone 5			

The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1
 Zone 2
 Zone 3
 Zone 4
 Zone 5
 Zone 6

Allowable Height: _____

CONSISTENCY DETERMINATION

This proposed Project is: Exempt from the ALUCP
 Consistent
 Consistent with Conditions
 Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

See attached condition.

Airport Planner Signature: _____

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT

CD No.: 2021-016
PALU No.: _____

PROJECT CONDITIONS

New Residential land uses are required to have a Recorded Overflight Notification appearing on the Property Deed and Title incorporating the following language:

NOTICE OF AIRPORT IN VICINITY: This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.



THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.

1. PRIOR TO PARCEL MAP APPROVAL, APPLICANT SHALL:	Check When Complete
---	---------------------

- | | | | |
|-------------------------------------|------|--|--------------------------|
| <input checked="" type="checkbox"/> | 1.01 | Dedicate to the City of Ontario, the right-of-way, described below:

3.25 feet on Transit Street to achieve an ultimate centerline ROW of 30 feet and additional right of way for the proposed Knuckle.

Property line corner 'cut-back' required at the intersection of Transit Street and Palm Avenue, Emporia Street and Palm Avenue, Emporia Street and Fern Avenue. | <input type="checkbox"/> |
|-------------------------------------|------|--|--------------------------|
 - | | | | |
|--------------------------|------|--|--------------------------|
| <input type="checkbox"/> | 1.02 | Dedicate to the City of Ontario, the following easement(s): _____
_____ | <input type="checkbox"/> |
|--------------------------|------|--|--------------------------|
 - | | | | |
|--------------------------|------|---|--------------------------|
| <input type="checkbox"/> | 1.03 | Restrict vehicular access to the site as follows: _____ | <input type="checkbox"/> |
|--------------------------|------|---|--------------------------|
 - | | | | |
|-------------------------------------|------|---|--------------------------|
| <input checked="" type="checkbox"/> | 1.04 | Vacate the following street(s) and/or easement(s): Fern Avenue between Transit Street and Emporia Street via separate instrument. Reserve a 30' Public Utility Easement for the existing City utilities. | <input type="checkbox"/> |
|-------------------------------------|------|---|--------------------------|
 - | | | | |
|--------------------------|------|--|--------------------------|
| <input type="checkbox"/> | 1.05 | Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles. | <input type="checkbox"/> |
|--------------------------|------|--|--------------------------|
 - | | | | |
|--------------------------|------|---|--------------------------|
| <input type="checkbox"/> | 1.06 | Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards. | <input type="checkbox"/> |
|--------------------------|------|---|--------------------------|
 - | | | | |
|--------------------------|------|--|--------------------------|
| <input type="checkbox"/> | 1.07 | For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at http://tceplumecleanup.com/), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658 . | <input type="checkbox"/> |
|--------------------------|------|--|--------------------------|
 - | | | | |
|--------------------------|------|---|--------------------------|
| <input type="checkbox"/> | 1.08 | File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Financial Services Department at (909) 395-2124 regarding this requirement. | <input type="checkbox"/> |
|--------------------------|------|---|--------------------------|
- (1) _____
- (2) _____



- 1.09 Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.
- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ci.ontario.ca.us) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.
- 1.13 New Model Colony (NMC) Developments:
 - 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
 - 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
 - 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions: _____

2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:

**A. GENERAL
(Permits includes Grading, Building, Demolition and Encroachment)**

- 2.01 Record Parcel Map No. 20339 pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 Submit a PDF of the recorded map to the City Engineer's office.
- 2.03 Note that the subject parcel will be a recognized parcel in the City of Ontario per PM-20339.
- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of _____ .



- 2.05 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.

- 2.06 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.

- 2.07 **Submit a soils/geology report.**

- 2.08 **Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:**
 - State of California Department of Transportation (Caltrans)
 - San Bernardino County Road Department (SBCRD)
 - San Bernardino County Flood Control District (SBCFCD) – for proposed storm drain connection along Transit Street**
 - Federal Emergency Management Agency (FEMA)
 - Cucamonga Valley Water District (CVWD) for sewer/water service
 - United States Army Corps of Engineers (USACE)
 - California Department of Fish & Game
 - Inland Empire Utilities Agency (IEUA)
 - Other:

- 2.09 Dedicate to the City of Ontario the right-of-way described below:

_____ feet on _____

Property line corner 'cut-back' required at the intersection of _____ and _____.

- 2.10 Dedicate to the City of Ontario the following easement(s): _____

- 2.11 New Model Colony (NMC) Developments:
 - 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.
 - 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.
 - 3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a



maximum 3-foot high retaining wall.

- 2.12 Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at _____% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.
- 2.13 **The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.**
- 2.14 **Pay all Development Impact Fees (DIF) to the Building Department. Final fee shall be determined based on the approved site plan.**
- 2.15 **Other conditions:**
 - a. **The applicant/developer shall process a Lot Line Adjustment for the proposed parking lot south of Emporia Street to eliminate the existing parcel line and consolidate the site into one (1) parcel. The Lot Line Adjustment and conforming grant deed shall be recorded prior to issuance of a building permit.**
 - b. **Final Utilities Systems Map (USM): As part of the precise grading plans submittal, provide a Final Utilities Systems Map that shows all existing and proposed Utilities (Potable Water, Recycled Water, Sewer, Storm Drain, and other utilities) including each of the City's public utilities' points of connection to the existing systems.**



B. PUBLIC IMPROVEMENTS
 (See attached Exhibit 'A' for plan check submittal requirements.)

2.16 Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):

Improvement	Transit Street	Palm Avenue	Emporia Avenue (both sides of street, along apartments and parking lot frontages)	Laurel Avenue
Curb and Gutter	<input checked="" type="checkbox"/> New; 18 ft. from C/L <input type="checkbox"/> Replace damaged <input checked="" type="checkbox"/> Remove Existing	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
AC Pavement	<input type="checkbox"/> Replacement <input checked="" type="checkbox"/> Widen one additional foot along frontage, including pavm't Transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Drive Approach^(a)	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input checked="" type="checkbox"/> Remove	<input type="checkbox"/> New <input type="checkbox"/> Remove
Sidewalk	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
ADA Access Ramp^(b)	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Parkway	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
Raised Landscaped Median	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace



Fire Hydrant (see Sec. 2.D)	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Sewer (see Sec. 2.C)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Water (see Sec. 2.D)	<input checked="" type="checkbox"/> Main <input type="checkbox"/> Service	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Service	<input checked="" type="checkbox"/> Main <input type="checkbox"/> Service	<input checked="" type="checkbox"/> Main <input type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Street Light (see Sec. 2.F)	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2G)	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Fiber Optics (see Sec. 2K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input checked="" type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
Removal of Improvements	_____	_____	_____	_____
Other Improvements	_____	_____	_____	_____



Specific notes for improvements listed in item no. 2.17, above:

- a. **The applicant/developer shall design and construct sidewalk, curb, gutter and landscape parkway including irrigation where existing drive approaches are being removed.**
- b. **The applicant/developer shall design and construct an access ramp per City Standard on the south side of Emporia Avenue across from the access ramp on the NWC of Emporia Street and Palm Avenue.**

- 2.17 Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s):**
 - a. **Palm Avenue from Emporia Street to Holt Boulevard (curb to curb)**
 - b. **Laurel Avenue from Emporia Street to Holt Boulevard (curb to curb)**
 - c. **Transit Street from Fern Avenue to Laurel Avenue (curb to curb)**
 - d. **Emporia Avenue from Fern Avenue to end of cul-de-sac east of Laurel Avenue (curb to curb)**
 - e. **Fern Avenue from Transit Street to Holt Boulevard (east half of street)**
- 2.18 Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.**
- 2.19 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide water service sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.**
- 2.20 Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately _____, for undergrounding of utilities in accordance with Section 7-7.303.e of the City's Municipal Code.**
- 2.21 Other conditions:**
 - a. **The applicant/developer shall salvage the existing street lights located along the portion of Fern Avenue being vacated and provide them to the City of Ontario.**

C. SEWER

- 2.22 A 8 inch sewer main is available for connection by this project in Emporia Avenue (Ref: Sewer plan bar code: S12738)**
- 2.23 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately _____ feet away.**
- 2.24 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.**
- 2.25 Other conditions:**
 - a. **The applicant/developer shall relocate the existing sewer manhole to be installed outside of the proposed gutter on the knuckle along Fern Avenue and Transit Street. New location shall be in accordance with City Standards.**



D. WATER

- 2.26 A _____ inch water main is available for connection by this project in _____.
(Ref: Water plan bar code: _____)
- 2.27 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.28 **Other conditions:**
 - a. **Existing Water Mains Upsizing: Upgrade all existing water mains in the area to 12” water mains and connect and construct all services, fire hydrants (with 300’ maximum spacing between fire hydrants), backflow, DCDA and all other appurtenances to city’s current standards and guidelines in additions to what have been mentioned above. The construction of all water main will be eligible for reimbursement by the City of Ontario under separate agreement.**
The limits of water mains upgrade as follow:
 - i. **Transit Street: From Fern Avenue to Laurel Avenue.**
 - ii. **Emporia Street: From Fern Avenue to Euclid Avenue.**
 - iii. **Palm Avenue: From Holt Boulevard to Emporia Street.**
 - iv. **Laurel Avenue: From Holt Boulevard to Emporia Street**
 - b. **Irrigation Service: Provide a separate irrigation service with a backflow device and a direct connection to the public water main.**
 - c. **Domestic Water Service: Provide a separate domestic water service with a backflow device and a direct connection to the public water main.**
 - d. **Existing Fire Hydrant: Upgrade the existing fire hydrant at the northwest corner of Emporia and Palm to City’s current standards.**
 - e. **Proposed Fire Hydrants: Construct one fire hydrant at the northeast corner of Emporia and Fern, one at the southwest corner of Palm and Transit, and one at the southeast corner of Fern and Transit.**

E. RECYCLED WATER

- 2.29 A _____ inch recycled water main is available for connection by this project in _____.
(Ref: Recycled Water plan bar code: _____)
- 2.30 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.
- 2.31 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.
- 2.32 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.

Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.
- 2.33 Other conditions: _____



F. TRAFFIC / TRANSPORTATION

- 2.34 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer:
1. On-site and off-site circulation
 2. Traffic level of service (LOS) at 'build-out' and future years
 3. Impact at specific intersections as selected by the City Engineer
- 2.35 New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.
- 2.36 **Other conditions:**
- a. **The applicant/developer shall be responsible to design and construct in-fill public street lights along the property frontages of Transit Street, Emporia Street, Palm Avenue and Fern Avenue, in accordance with City of Ontario Traffic and Transportation Design Guidelines.**
 - b. **The applicant/developer shall design driveways in accordance with City Standard Drawing No. 1204.**
 - c. **Parking shall be restricted 40 feet on either side of the project driveway on Emporia Street with "No Parking Any Time" signs with arrows (R28(CA)(L) & (R)). Signs shall be in accordance with all applicable standards and to the satisfaction of the City Engineer.**
 - d. **The applicant/developer shall install a R28(L) (CA) sign on Palm Avenue to restrict parking for the required solid waste bins. Designated striping of the street for the solid waste bins will not be allowed.**
 - e. **The applicant/developer's engineer-of-record shall meet with City Engineering staff prior to starting street lighting design.**

G. DRAINAGE / HYDROLOGY

- 2.37 A _____ inch storm drain main is available to accept flows from this project in _____.
(Ref: Storm Drain plan bar code: _____)
- 2.38 **Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.**
- 2.39 **An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100 year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans. However, the applicant/developer may be able to obtain approval from the County of San Bernardino Flood Control District to drain overflow of site into existing storm drain on Transit Street.**
- 2.40 Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.



- 2.41 Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.

- 2.42 Other conditions: _____

H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)

- 2.43 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels.
If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.
Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.

- 2.44 **Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: <http://www.sbcounty.gov/dpw/land/npdes.asp>.**

- 2.45 **Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.**

- 2.46 Other conditions: _____

J. SPECIAL DISTRICTS

- 2.47 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.

- 2.48 Other conditions: _____



K. FIBER OPTIC

- 2.49 Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole.

- 2.50 Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Broadband Operations Department at (909) 395-2000, regarding this requirement.

L. INTEGRATED WASTE

- 2.51 Onsite solid waste shall be designed in accordance with the City's Refuse & Recycling Planning Manual located at:

<https://www.ontarioca.gov/OMUC/IntegratedWaste>

- 2.52 Other conditions:
 - a. **Trash Enclosures (TEs):** The proposed single trash enclosure with six 3-cy bins is adequate. However, it is agreed between the city and the developer, that property management will roll out all the bins on trash pickup day(s) along Palm Avenue with no parking signs (per 2.36(d.) above) to assist with bins staging.

 - b. **Integrated Waste Management Report (IWMR):** Prior to approval of the Precise Grading Plan, submit to OMUC for review and approval, an Integrated Waste Management Report (IWMR) that complies with the IWMR Requirements.

 - c. **Final Solid Waste Handling Plan (SWHP):** Prior to approval of any building permits, submit a final SWHP with the Precise Grading Plan for review and approval of Ontario Municipal Utilities Company.



3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:

- 3.01 **Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.**

- 3.02 Complete all requirements for recycled water usage.
 - 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.
 - 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.
 - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.

- 3.03 **The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.**

- 3.04 NMC Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.

- 3.05 **Confirm payment of all Development Impact Fees (DIF) to the Building Department.**

- 3.06 **Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).**

4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:

- 4.01 Complete all Conditions of Approval listed under Sections 1-3 above.

- 4.02 Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.

- 4.03 **The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall include a completed Acceptance and Bond Release Checklist, state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.**

- 4.04 **Submit record drawings (PDF) for all public improvements identified within Section 2 of these Conditions of Approval.**



EXHIBIT 'A'

ENGINEERING DEPARTMENT First Plan Check Submittal Checklist

Project Number: PDEV21-008, and Parcel Map No. 20339

The following items are required to be included with the first plan check submittal:

1. **A copy of this check list**
2. **Payment of fee for Plan Checking**
3. **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4. **One (1) copy of project Conditions of Approval**
5. **Include a PDF (electronic submittal) of each required improvement plan at every submittal.**
6. **Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).**
7. **Three (3) sets of Public Street improvement plan with street cross-sections**
8. **Three (3) sets of Private Street improvement plan with street cross-sections**
9. **Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)**
10. **Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)**
11. **Four (4) sets of Public Sewer improvement plan**
12. **Five (5) sets of Public Storm Drain improvement plan**
13. **Three (3) sets of Public Street Light improvement plan**
14. **Three (3) sets of Signing and Striping improvement plan**
15. **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
16. **Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)**
17. **Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.**
18. **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**
19. **One (1) copy of Hydrology/Drainage study**
20. **One (1) copy of Soils/Geology report**



21. **Payment for Parcel Map processing fee**
22. **Three (3) copies of Parcel Map**
23. **One (1) copy of approved Tentative Map**
24. **One (1) copy of Preliminary Title Report (current within 30 days)**
25. **One (1) copy of Traverse Closure Calculations**
26. **One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.**
27. Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
28. **Other: Lot Line Adjustment**

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PDEV21-008, A DEVELOPMENT PLAN TO CONSTRUCT A 50-UNIT, THREE-STORY APARTMENT COMPLEX ON 1.71 ACRES OF LAND BORDERED BY TRANSIT STREET ON THE NORTH, PALM AVENUE ON THE EAST, FERN AVENUE ON THE WEST AND EMPORIA STREET ON THE SOUTH, AND TO CONSTRUCT AN ADJACENT OFF-STREET PARKING FACILITY ON 0.44-ACRE OF LAND LOCATED AT 303 AND 309 WEST EMPORIA STREET, FOR A TOTAL PROJECT AREA OF 2.15 ACRES OF LAND WITHIN THE LUA2-N (ARTS DISTRICT- NORTH) AND LUA-3 (HOLT BOULEVARD DISTRICT) OF THE MU-1 (DOWNTOWN MIXED-USE) ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF—APNS: 1049-054-02, 1049-054-03, 1049-054-04, AND 1049-054-06.

WHEREAS, the Related Companies of California, LLC. (hereinafter referred to as "Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV21-008, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 2.15 acres of land generally located south of Transit Street, north of Emporia Street, east of Palm Avenue, the adjoining vacated section of Fern Avenue (0.31 acre of land), and 2 lots located at 303 and 309 West Emporia Street within the LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district, and is presently improved with commercial, office and undeveloped land; and

WHEREAS, the property to the north of the Project site is within the LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district and is partially developed with retail. The property to the east is within the LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district and is developed with partially developed with office. The property to the south is within the LUA2-N (Arts District- North) of the MU-1 (Downtown Mixed-Use) zoning district and is partially developed with wholesale machinery. The property to the west is within the LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district, and is developed with 75-multiple-family apartment complex (Emporia Family Housing-Phase 1); and

WHEREAS, the Project will develop approximately 1.71 acres of land with a 50-unit, three-story apartment complex that includes on-site parking, a recreation area equipped with a 1,921 square foot community building, containing a laundry room, bathrooms, and an open barbeque area, and a children's play area with play structure,

pool, a dedicated outdoor gathering area furnished with benches, seating and tables for teens, a plaza area equipped with tables, chairs, shade trellis, and a mailroom; and

WHEREAS, the Project will construct 6 one-bedroom units (563 to 579 square feet in area), 29 two-bedroom units (865 square feet in area), and 15 three-bedroom units (1,250 square feet in area) in a modern Prairie style of architecture; and

WHEREAS, the Project will develop an adjacent off-street parking lot with 53 vehicle parking spaces for residents on 0.44-acre of land located at 303 and 309 West Emporia Street; and

WHEREAS, The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001 (hereinafter referred to as "Certified EIR"), in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as "EIR Addendum") in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as "CEQA"); and

WHEREAS, the environmental impacts of this project were thoroughly analyzed in the EIR Addendum, which concluded that implementation of the Project could result in a number of significant effects on the environment that were previously analyzed in the Certified EIR, and that the Certified EIR identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an EIR Addendum has been prepared to determine possible environmental impacts; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (“ALUCP”), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, an Amendment to the Emporia Family Housing PUD (File No. PUD21-001), Tentative Parcel Map (File No. PMTT21-004), Development Plan (File No. PDEV21-008) to expand the project site area by 2.15-acres of land, consolidate parcels of land, vacate portion of Fern Avenue south of Transit Street and north of Emporia Street, and construct 50 multiple-family affordable apartment units, known as Emporia Family Housing- Phase 2, and a Certificate of Appropriateness (File No. PHP21-003) to allow the demolition of two Tier III historic resources to accommodate the proposed multiple-family residential development were submitted on February 23, 2021, and are being processed concurrently; and

WHEREAS, on May 17, 2021, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Addendum and the Project, and concluded said hearing on that date, voting to issue Decision No. DAB21-024, recommending that the Planning Commission recommend the City Council approve the Addendum and Decision No. DAB21-026 recommending that the Planning Commission approve the Development Plan (File No. PDEV21-008); and

WHEREAS, as the first action on the Project, on May 25, 2021, the Planning Commission issued a Resolution recommending the City Council adopt the EIR Addendum, finding that the proposed Project introduces no new significant environmental impacts and applying all previously adopted mitigation measures to the Project, which were incorporated by reference; and

WHEREAS, on May 25, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the decision-making body for the Project, the Planning Commission has reviewed and considered the information contained in the Addendum, the initial study, and the administrative record for the Project, including all written and oral evidence provided during the comment period. Based upon the facts and information contained in the Addendum, the initial study, and the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The environmental impacts of the Project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. SCH# 2008101140), certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001; and

(2) The EIR Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts; and

(4) All previously adopted mitigation measures shall be a condition of project approval, as they are applicable to the Project, and are incorporated herein by this reference; and

(5) The EIR Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and

(6) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts.

SECTION 2: *Subsequent or Supplemental Environmental Review Not Required.* Based on the EIR Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and.

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The Project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: *Housing Element Compliance.* Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the Project site is one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the Project

exceeds the 31-dwelling units and density of 25.1 dwelling units per acre as specified in the Available Land Inventory.

SECTION 4: Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance. The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as “ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making body for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 5: Concluding Facts and Reasons. Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 through 4, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Project is located within the Mixed-Use land use district of the Policy Plan Land Use Map, and the LUA2-N (Arts District- North) and LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district and the Emporia Family Housing Planned Unit Development “PUD.” The development standards and conditions under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.

(2) ***The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in***

which the site is located. The Project has been designed consistent with the requirements of the City of Ontario Development Code and the LUA2-N (Arts District-North) and LUA-3 (Holt Boulevard District) of the MU-1 (Downtown Mixed-Use) zoning district and Emporia Family Housing PUD, including standards relative to the particular land use proposed (residential), as-well-as building intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions.

(3) ***The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed project.*** The Development Advisory Board has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: [i] the purposes of the Development Code and Emporia Family Housing PUD are maintained; [ii] the project will not endanger the public health, safety or general welfare; [iii] the project will not result in any significant environmental impacts; [iv] the project will be in harmony with the area in which it is located; and [v] the project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan, and the Emporia Family Housing PUD.

The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development. The proposed Project has been reviewed for consistency with the general development standards and guidelines of the Development Code and Emporia Family Housing PUD that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking and loading spaces, parking lot dimensions, design and landscaping, bicycle parking, on-site landscaping, and fences and walls, as-well-as those development standards and guidelines specifically related to the particular land use being proposed ([insert land use]). As a result of this review, the Development Advisory Board has determined that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the development standards and guidelines described in the Development Code and Emporia Family Housing PUD.

SECTION 6: Planning Commission Action. Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as “Attachment A,” and incorporated herein by this reference. If the City Council does not approve the Amendment to the Emporia Family Housing Planned Unit Development (File No. PUD21-001) this resolution should become null and void and of no further force and effect.

SECTION 7: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 8: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 9: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of May 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 25, 2021, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**File No. PDEV21-008
Departmental Conditions of Approval**

(Departmental conditions of approval to follow this page)



City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

Planning Department Land Development Division Conditions of Approval

Meeting Date: May 25, 2021
File No: PDEV21-008
Related Files: PUD21-001, PHP21-003, and PMTT21-004

Project Description: A Development Plan (File No. PDEV21-008) to construct 50 multiple-family affordable housing dwellings, generally located at the northwest and southwest corners of Emporia Street and Palm Avenue, within LUA2-N and LUA-3 of the MU-1 (Downtown Mixed-Use) zoning district. (APN(s): 1049-054-02, 1049-054-03, 1049-054-04, 1049-054-06, 1049-059-06, and 1049-059-07); **submitted by The Related Companies of California, LLC.**

Prepared By: Diane Ayala, Senior Planner
Phone: 909.395.2428 (direct)
Email: dayala@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits.

(a) Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

2.2 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.3 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping) and the Emporia Family Housing PUD.

(b) New street trees planted along Emporia Avenue shall continue the existing street tree pattern and species (Silk Oak and CA Fan Palm).

(c) Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

(d) Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(e) Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

2.4 Walls and Fences.

(a) All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions) and the Emporia Family Housing PUD.

(b) Enclosing Tot lot area with barrier, fencing, and walls should be avoided.

(c) Noise control measures are required for project at all outdoor recreational areas to not exceed the 65 db CNEL noise level threshold. The fence along the west perimeter beginning from the northwest corner of the tot lot and terminating at the west building wall of the mail center shall have a minimum 6 FT solid barrier wall constructed of decorative materials such as split face, slump stone, board foam concrete, wrought iron with plexi-glass or any other combination of fence/wall considered to be decorative by the Planning Director.

(d) Controlled gated access along driveway/aisle accessing development is not allowed due to inadequate space to maneuver and "turn-around" a standard size vehicle within the 26 FT wide drive aisle.

(e) Wrought iron or tubular steel fence may be installed around the project perimeter with decorative pilasters at logical key locations such corners, entries and approximately every 50 FT to break up long expanses of wall/fence.

(f) A 6 FT tall tubular steel fence or decorative wall shall be constructed along the South property boundary adjacent to the railroad ROW in the parking lot.

2.5 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading) and the Emporia Family Housing PUD.

(b) All drive approaches shall be provided with an enhanced pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first planter approximately 44 FT into the site, and at the 12 FT wide pedestrian crossing located at midpoint of drive aisle and at pedestrian crossing in front of Clubhouse located at the end of the drive aisle.

(c) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(d) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

(e) Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).

(f) Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11).

(g) Tandem parking spaces shall be assigned to tenants living in the same unit.

2.6 Outdoor Loading and Storage Areas.

(a) Areas designated for off-street parking, loading, and vehicular circulation and maneuvering, shall not be used for the outdoor storage of materials or equipment.

(b) Outdoor loading and storage areas are prohibited.

2.7 Site Lighting.

(a) All off-street parking facilities shall be provided with nighttime security lighting pursuant to Ontario Municipal Code Section 4-11.08 (Special Residential Building Provisions) and Section 4-11.09 (Special Commercial/Industrial Building Provisions), designed to confine emitted light to the parking areas. Parking facilities shall be lighted from sunset until sunrise, daily, and shall be operated by a photocell switch.

(b) Unless intended as part of a master lighting program, no operation, activity, or lighting fixture shall create illumination on any adjacent property.

2.8 Mechanical and Rooftop Equipment.

(a) All exterior roof-mounted mechanical, heating and air conditioning equipment, and all appurtenances thereto, shall be completely screened from public view by parapet walls or roof screens that are architecturally treated so as to be consistent with the building architecture.

(b) All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.

2.9 Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

2.10 Signs. All Project signage shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations).

2.11 Sound Attenuation.

(a) The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noise levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

(b) Private balconies and patios which face west and east shall provide a minimum 5 FT tall solid wall (noise barrier) above the deck height. Private balconies and patios which face Emporia Street on the south elevation of the south building shall be enclosed with fully operational window or door as noise measure controls to not exceed exterior noise levels of 65 db CNEL at the Project. Third story balconies on the south building facing Emporia Street shall have solid roofs.

2.12 Disclosure Statements.

(a) A copy of the Public Report from the Department of Real Estate, prepared for the subdivision pursuant to Business and Professions Code Section 11000 et seq., shall be provided to each prospective buyer of the residential units and shall include a statement to the effect that:

(i) This tract is subject to noise from the Ontario International Airport and may be more severely impacted in the future.

2.13 Environmental Review.

(a) The environmental impacts of this project were reviewed in conjunction with an **Addendum to The Ontario Plan Environmental Impact Report**, certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001 (City Council Resolution No. 2010-006). This application introduces no new significant environmental impacts. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures are a condition of project approval, and are incorporated herein by this reference. All previously adopted mitigation measures shall be a condition of project approval, as they are applicable, and are incorporated herein by this reference.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

2.14 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.15 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

2.16 Additional Requirements.

(a) A minimum 2-inch recess on windows/glass shall be constructed at locations with brick veneer is used and on the vertical window bands located on stair towers.

(b) A minimum inset of 2 inches on plaster wall panels with one or two windows that are "stacked" and where the plaster wall meets a wall finished with Cementous plank siding shall be constructed.

(c) Windows shall be fixed or hung style.

(d) Project approval is contingent upon City Council approval of an Amendment to the Emporia Family Housing PUD, File No. PUD21-001.

(e) A parking management plan and a security plan shall be submitted to the Planning Department for review and approval prior to issuance of building permits.

(f) New sidewalks located within the public right-of way shall be designed with a score pattern to match the existing.



**ENGINEERING DEPARTMENT
CONDITIONS OF APPROVAL**

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Broadband Operations & Investment and Revenue Resources Department Conditions incorporated)

<input checked="" type="checkbox"/> DEVELOPMENT PLAN <input type="checkbox"/> OTHER	<input checked="" type="checkbox"/> PARCEL MAP <input type="checkbox"/> TRACT MAP <input type="checkbox"/> FOR CONDOMINIUM PURPOSES
PROJECT FILE NO. PM-20339 RELATED FILE NO(S). PUD21-001, PMTT21-004, PDEV21-008	
<input checked="" type="checkbox"/> ORIGINAL <input type="checkbox"/> REVISED: __/__/__	

CITY PROJECT ENGINEER & PHONE NO: Miguel Sotomayor (909) 395-2108 *MS*

CITY PROJECT PLANNER & PHONE NO: Diane Ayala (909) 395-2428

DAB MEETING DATE: May 17, 2021

PROJECT NAME / DESCRIPTION: A Tentative Parcel Map and Development Plan to subdivide 4 lots into a single parcel and construct a 50-unit, 3 story apartment complex on 2.15 acres, within LUA2N and LUA3 of the MU-1 (Downtown Mixed Use) zoning district.

LOCATION: Emporia Avenue between Fern and Palm Avenues

APPLICANT: The Related Companies of California, LLC

REVIEWED BY: *Raymond Lee* *5/11/21*
 Raymond Lee, P.E. Date
 Assistant City Engineer

APPROVED BY: *Khoi Do* *5-11-21*
 Khoi Do, P.E. Date
 City Engineer



THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.

1. PRIOR TO PARCEL MAP APPROVAL, APPLICANT SHALL:	Check When Complete
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- | | | | |
|-------------------------------------|-------------|---|--------------------------|
| <input checked="" type="checkbox"/> | 1.01 | Dedicate to the City of Ontario, the right-of-way, described below:

3.25 feet on Transit Street to achieve an ultimate centerline ROW of 30 feet and additional right of way for the proposed Knuckle.

Property line corner 'cut-back' required at the intersection of Transit Street and Palm Avenue, Emporia Street and Palm Avenue, Emporia Street and Fern Avenue. | <input type="checkbox"/> |
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 - | | | | |
|--------------------------|------|--|--------------------------|
| <input type="checkbox"/> | 1.02 | Dedicate to the City of Ontario, the following easement(s): _____
_____ | <input type="checkbox"/> |
|--------------------------|------|--|--------------------------|
 - | | | | |
|--------------------------|------|---|--------------------------|
| <input type="checkbox"/> | 1.03 | Restrict vehicular access to the site as follows: _____ | <input type="checkbox"/> |
|--------------------------|------|---|--------------------------|
 - | | | | |
|-------------------------------------|-------------|---|--------------------------|
| <input checked="" type="checkbox"/> | 1.04 | Vacate the following street(s) and/or easement(s): Fern Avenue between Transit Street and Emporia Street via separate instrument. Reserve a 30' Public Utility Easement for the existing City utilities. | <input type="checkbox"/> |
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 - | | | | |
|--------------------------|------|--|--------------------------|
| <input type="checkbox"/> | 1.05 | Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles. | <input type="checkbox"/> |
|--------------------------|------|--|--------------------------|
 - | | | | |
|--------------------------|------|---|--------------------------|
| <input type="checkbox"/> | 1.06 | Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards. | <input type="checkbox"/> |
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 - | | | | |
|--------------------------|------|--|--------------------------|
| <input type="checkbox"/> | 1.07 | For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at http://tceplumecleanup.com/), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658 . | <input type="checkbox"/> |
|--------------------------|------|--|--------------------------|
 - | | | | |
|--------------------------|------|---|--------------------------|
| <input type="checkbox"/> | 1.08 | File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Financial Services Department at (909) 395-2124 regarding this requirement. | <input type="checkbox"/> |
|--------------------------|------|---|--------------------------|
- (1) _____

(2) _____



- 1.09 Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.
- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ci.ontario.ca.us) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.
- 1.13 New Model Colony (NMC) Developments:
 - 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
 - 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
 - 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions: _____

2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:

**A. GENERAL
 (Permits includes Grading, Building, Demolition and Encroachment)**

- 2.01 Record Parcel Map No. 20339 pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 Submit a PDF of the recorded map to the City Engineer's office.
- 2.03 Note that the subject parcel will be a recognized parcel in the City of Ontario per PM-20339.
- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of _____ .



- 2.05 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.

- 2.06 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.

- 2.07 **Submit a soils/geology report.**

- 2.08 **Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:**
 - State of California Department of Transportation (Caltrans)
 - San Bernardino County Road Department (SBCRD)
 - San Bernardino County Flood Control District (SBCFCD) – for proposed storm drain connection along Transit Street**
 - Federal Emergency Management Agency (FEMA)
 - Cucamonga Valley Water District (CVWD) for sewer/water service
 - United States Army Corps of Engineers (USACE)
 - California Department of Fish & Game
 - Inland Empire Utilities Agency (IEUA)
 - Other:

- 2.09 Dedicate to the City of Ontario the right-of-way described below:

_____ feet on _____

Property line corner 'cut-back' required at the intersection of _____ and _____.

- 2.10 Dedicate to the City of Ontario the following easement(s): _____

- 2.11 New Model Colony (NMC) Developments:
 - 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.
 - 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.
 - 3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a



maximum 3-foot high retaining wall.

- 2.12 Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at _____% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.
- 2.13 **The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.**
- 2.14 **Pay all Development Impact Fees (DIF) to the Building Department. Final fee shall be determined based on the approved site plan.**
- 2.15 **Other conditions:**
 - a. **The applicant/developer shall process a Lot Line Adjustment for the proposed parking lot south of Emporia Street to eliminate the existing parcel line and consolidate the site into one (1) parcel. The Lot Line Adjustment and conforming grant deed shall be recorded prior to issuance of a building permit.**
 - b. **Final Utilities Systems Map (USM): As part of the precise grading plans submittal, provide a Final Utilities Systems Map that shows all existing and proposed Utilities (Potable Water, Recycled Water, Sewer, Storm Drain, and other utilities) including each of the City's public utilities' points of connection to the existing systems.**



B. PUBLIC IMPROVEMENTS
 (See attached Exhibit 'A' for plan check submittal requirements.)

2.16 Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):

Improvement	Transit Street	Palm Avenue	Emporia Avenue (both sides of street, along apartments and parking lot frontages)	Laurel Avenue
Curb and Gutter	<input checked="" type="checkbox"/> New; 18 ft. from C/L <input type="checkbox"/> Replace damaged <input checked="" type="checkbox"/> Remove Existing	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
AC Pavement	<input type="checkbox"/> Replacement <input checked="" type="checkbox"/> Widen one additional foot along frontage, including pavm't Transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Drive Approach^(a)	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input checked="" type="checkbox"/> Remove	<input type="checkbox"/> New <input type="checkbox"/> Remove
Sidewalk	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
ADA Access Ramp^(b)	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Parkway	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
Raised Landscaped Median	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace



Fire Hydrant (see Sec. 2.D)	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Sewer (see Sec. 2.C)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Water (see Sec. 2.D)	<input checked="" type="checkbox"/> Main <input type="checkbox"/> Service	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Service	<input checked="" type="checkbox"/> Main <input type="checkbox"/> Service	<input checked="" type="checkbox"/> Main <input type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Street Light (see Sec. 2.F)	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2G)	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Fiber Optics (see Sec. 2K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input checked="" type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
Removal of Improvements	_____	_____	_____	_____
Other Improvements	_____	_____	_____	_____



Specific notes for improvements listed in item no. 2.17, above:

- a. **The applicant/developer shall design and construct sidewalk, curb, gutter and landscape parkway including irrigation where existing drive approaches are being removed.**
- b. **The applicant/developer shall design and construct an access ramp per City Standard on the south side of Emporia Avenue across from the access ramp on the NWC of Emporia Street and Palm Avenue.**

- 2.17 Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s):**
 - a. **Palm Avenue from Emporia Street to Holt Boulevard (curb to curb)**
 - b. **Laurel Avenue from Emporia Street to Holt Boulevard (curb to curb)**
 - c. **Transit Street from Fern Avenue to Laurel Avenue (curb to curb)**
 - d. **Emporia Avenue from Fern Avenue to end of cul-de-sac east of Laurel Avenue (curb to curb)**
 - e. **Fern Avenue from Transit Street to Holt Boulevard (east half of street)**
- 2.18 Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.**
- 2.19 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide water service sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.**
- 2.20 Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately _____, for undergrounding of utilities in accordance with Section 7-7.303.e of the City's Municipal Code.**
- 2.21 Other conditions:**
 - a. **The applicant/developer shall salvage the existing street lights located along the portion of Fern Avenue being vacated and provide them to the City of Ontario.**

C. SEWER

- 2.22 A 8 inch sewer main is available for connection by this project in Emporia Avenue (Ref: Sewer plan bar code: S12738)**
- 2.23 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately _____ feet away.**
- 2.24 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.**
- 2.25 Other conditions:**
 - a. **The applicant/developer shall relocate the existing sewer manhole to be installed outside of the proposed gutter on the knuckle along Fern Avenue and Transit Street. New location shall be in accordance with City Standards.**



D. WATER

- 2.26 A _____ inch water main is available for connection by this project in _____.
 (Ref: Water plan bar code: _____)
- 2.27 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.28 **Other conditions:**
 - a. **Existing Water Mains Upsizing: Upgrade all existing water mains in the area to 12" water mains and connect and construct all services, fire hydrants (with 300' maximum spacing between fire hydrants), backflow, DCDA and all other appurtenances to city's current standards and guidelines in additions to what have been mentioned above. The construction of all water main will be eligible for reimbursement by the City of Ontario under separate agreement.**
The limits of water mains upgrade as follow:
 - i. **Transit Street: From Fern Avenue to Laurel Avenue.**
 - ii. **Emporia Street: From Fern Avenue to Euclid Avenue.**
 - iii. **Palm Avenue: From Holt Boulevard to Emporia Street.**
 - iv. **Laurel Avenue: From Holt Boulevard to Emporia Street**
 - b. **Irrigation Service: Provide a separate irrigation service with a backflow device and a direct connection to the public water main.**
 - c. **Domestic Water Service: Provide a separate domestic water service with a backflow device and a direct connection to the public water main.**
 - d. **Existing Fire Hydrant: Upgrade the existing fire hydrant at the northwest corner of Emporia and Palm to City's current standards.**
 - e. **Proposed Fire Hydrants: Construct one fire hydrant at the northeast corner of Emporia and Fern, one at the southwest corner of Palm and Transit, and one at the southeast corner of Fern and Transit.**

E. RECYCLED WATER

- 2.29 A _____ inch recycled water main is available for connection by this project in _____.
 (Ref: Recycled Water plan bar code: _____)
 - 2.30 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.
 - 2.31 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.
 - 2.32 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.
- Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.
- 2.33 Other conditions: _____



F. TRAFFIC / TRANSPORTATION

- 2.34 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer:
1. On-site and off-site circulation
 2. Traffic level of service (LOS) at 'build-out' and future years
 3. Impact at specific intersections as selected by the City Engineer
- 2.35 New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.
- 2.36 **Other conditions:**
- a. **The applicant/developer shall be responsible to design and construct in-fill public street lights along the property frontages of Transit Street, Emporia Street, Palm Avenue and Fern Avenue, in accordance with City of Ontario Traffic and Transportation Design Guidelines.**
 - b. **The applicant/developer shall design driveways in accordance with City Standard Drawing No. 1204.**
 - c. **Parking shall be restricted 40 feet on either side of the project driveway on Emporia Street with "No Parking Any Time" signs with arrows (R28(CA)(L) & (R)). Signs shall be in accordance with all applicable standards and to the satisfaction of the City Engineer.**
 - d. **The applicant/developer shall install a R28(L) (CA) sign on Palm Avenue to restrict parking for the required solid waste bins. Designated striping of the street for the solid waste bins will not be allowed.**
 - e. **The applicant/developer's engineer-of-record shall meet with City Engineering staff prior to starting street lighting design.**

G. DRAINAGE / HYDROLOGY

- 2.37 A _____ inch storm drain main is available to accept flows from this project in _____.
(Ref: Storm Drain plan bar code: _____)
- 2.38 **Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.**
- 2.39 **An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100 year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans. However, the applicant/developer may be able to obtain approval from the County of San Bernardino Flood Control District to drain overflow of site into existing storm drain on Transit Street.**
- 2.40 Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.



- 2.41 Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.

- 2.42 Other conditions: _____

H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)

- 2.43 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels.
If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.
Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.

- 2.44 **Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: <http://www.sbcounty.gov/dpw/land/npdes.asp>.**

- 2.45 **Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.**

- 2.46 Other conditions: _____

J. SPECIAL DISTRICTS

- 2.47 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.

- 2.48 Other conditions: _____



K. FIBER OPTIC

- 2.49 Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole.
- 2.50 Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Broadband Operations Department at (909) 395-2000, regarding this requirement.

L. INTEGRATED WASTE

- 2.51 Onsite solid waste shall be designed in accordance with the City's Refuse & Recycling Planning Manual located at:

<https://www.ontarioca.gov/OMUC/IntegratedWaste>
- 2.52 Other conditions:
 - a. Trash Enclosures (TEs): The proposed single trash enclosure with six 3-cy bins is adequate. However, it is agreed between the city and the developer, that property management will roll out all the bins on trash pickup day(s) along Palm Avenue with no parking signs (per 2.36(d.) above) to assist with bins staging.
 - b. Integrated Waste Management Report (IWMR): Prior to approval of the Precise Grading Plan, submit to OMUC for review and approval, an Integrated Waste Management Report (IWMR) that complies with the IWMR Requirements.
 - c. Final Solid Waste Handling Plan (SWHP): Prior to approval of any building permits, submit a final SWHP with the Precise Grading Plan for review and approval of Ontario Municipal Utilities Company.



3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:

- 3.01 **Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.**

- 3.02 Complete all requirements for recycled water usage.
 - 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.
 - 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.
 - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.

- 3.03 **The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.**

- 3.04 NMC Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.

- 3.05 **Confirm payment of all Development Impact Fees (DIF) to the Building Department.**

- 3.06 **Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).**

4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:

- 4.01 Complete all Conditions of Approval listed under Sections 1-3 above.

- 4.02 Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.

- 4.03 **The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall include a completed Acceptance and Bond Release Checklist, state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.**

- 4.04 **Submit record drawings (PDF) for all public improvements identified within Section 2 of these Conditions of Approval.**



EXHIBIT 'A'

ENGINEERING DEPARTMENT First Plan Check Submittal Checklist

Project Number: PDEV21-008, and Parcel Map No. 20339

The following items are required to be included with the first plan check submittal:

1. **A copy of this check list**
2. **Payment of fee for Plan Checking**
3. **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4. **One (1) copy of project Conditions of Approval**
5. **Include a PDF (electronic submittal) of each required improvement plan at every submittal.**
6. **Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).**
7. **Three (3) sets of Public Street improvement plan with street cross-sections**
8. **Three (3) sets of Private Street improvement plan with street cross-sections**
9. **Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)**
10. **Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)**
11. **Four (4) sets of Public Sewer improvement plan**
12. **Five (5) sets of Public Storm Drain improvement plan**
13. **Three (3) sets of Public Street Light improvement plan**
14. **Three (3) sets of Signing and Striping improvement plan**
15. **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
16. **Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)**
17. **Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.**
18. **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**
19. **One (1) copy of Hydrology/Drainage study**
20. **One (1) copy of Soils/Geology report**



21. **Payment for Parcel Map processing fee**
22. **Three (3) copies of Parcel Map**
23. **One (1) copy of approved Tentative Map**
24. **One (1) copy of Preliminary Title Report (current within 30 days)**
25. **One (1) copy of Traverse Closure Calculations**
26. **One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.**
27. Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
28. **Other: Lot Line Adjustment**



CITY OF ONTARIO

MEMORANDUM

TO: Diana Ayala, Senior Planner
Planning Department

FROM: Mike Gerken, Deputy Fire Chief/Fire Marshal
Fire Department

DATE: March 29, 2021

SUBJECT: PDEV21-008 - A Development Plan approval to construct 50 multiple-family affordable housing dwellings on approximately 2.15 acres of land located at the northwest and southwest corners of Emporia Street and Palm Avenue, within LUA2N and LUA3 of the MU-1 (Downtown Mixed Use) zoning district (APN(s): 1049-054-02, 1049-054-03, 1049-054-06, 1049-059-07 & 1049-059-06). Related File(s): PUD-21-001, PMTT21-004 & PHP-21-003.

-
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.
-

SITE AND BUILDING FEATURES:

- A. 2019 CBC Type of Construction: Type V
- B. Type of Roof Materials: Ordinary
- C. Ground Floor Area(s): Varies
 - North Building – 9,473 Sq. Ft.
 - South Building – 12,470 Sq. Ft.
 - Community Building – 1,921 Sq. Ft.
- D. Number of Stories: 3
- E. Total Square Footage: Varies
 - North Building – 24,893 Sq. Ft.
 - South Building – 33,805 Sq. Ft.
 - Community Building – 1,921 Sq. Ft.
 - Per Unit – 563 to 1,250 Sq. Ft.
- F. 2019 CBC Occupancy Classification(s): R-2 & A-3

CONDITIONS OF APPROVAL:

1.0 GENERAL

- ☒ 1.1 The following are the Ontario Fire Department (“Fire Department”) requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards (“Standards.”) It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at www.ontarioca.gov/Fire/Prevention.
- ☒ 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

2.0 FIRE DEPARTMENT ACCESS

- ☒ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- ☒ 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25’) inside and forty-five feet (45’) outside turning radius per Standard #B-005.
- ☒ 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150’) in length shall have an approved turn-around per Standard #B-002.
- ☒ 2.4 Access drive aisles which cross property lines shall be provided with CC&Rs, access easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of building plan check.
- ☒ 2.5 "No Parking-Fire Lane" signs and /or red painted curbs with lettering are required to be installed in interior access roadways, in locations where vehicle parking would obstruct the minimum clear width requirement. Installation shall be per Standard #B-001.
- ☒ 2.6 Security gates or other barriers on fire access roadways shall be provided with a Knox brand key switch or padlock to allow Fire Department access. See Standards #B-003, B-004 and H-001.
- ☒ 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-four (24) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by fire department and other emergency services.

3.0 WATER SUPPLY

- ☒ 3.1 The required fire flow per Fire Department standards, based on the 2019 California Fire Code, Appendix B, is 2500 gallons per minute (g.p.m.) for 4 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.
- ☒ 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.
- ☒ 3.4 The water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

4.0 FIRE PROTECTION SYSTEMS

- ☒ 4.2 Underground fire mains which cross property lines shall be provided with CC & R, easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of fire department plan check. The shared use of private fire mains or fire pumps is allowable only between immediately adjacent properties and shall not cross any public street.
- ☒ 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 13 R. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- ☒ 4.4 Wood frame buildings that are to be sprinkled shall have these systems in service (but not necessarily finalized) before the building is enclosed.
- ☒ 4.7 Portable fire extinguishers are required to be installed prior to occupancy per Standard #C-001. Please contact the Fire Prevention Bureau to determine the exact number, type and placement required.

5.0 BUILDING CONSTRUCTION FEATURES

- ☒ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ☒ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.

- ☒ 5.3 Single station smoke alarms and carbon monoxide alarms are required to be installed per the California Building Code and the California Fire Code.
- ☒ 5.4 Multiple unit building complexes shall have building directories provided at the main entrances. The directories shall be designed to the requirements of the Fire Department, see Section 9-1 6.06 of the Ontario Municipal Code and Standard #H-003.
- ☒ 5.5 All residential chimneys shall be equipped with an approved spark arrester meeting the requirements of the California Building Code.
- ☒ 5.6 Knox ® brand key-box(es) shall be installed in location(s) acceptable to the Fire Department. All Knox boxes shall be monitored for tamper by the building fire alarm system. See Standard #H-001 for specific requirements.



CITY OF ONTARIO

MEMORANDUM

TO: Diana Ayala, Senior Planner

FROM: Officer Bill Lee, Police Department

DATE: April 1, 2021

SUBJECT: PDEV21-008: A DEVELOPMENT PLAN TO A CONSTRUCT 50 MULTIPLE-FAMILY AFFORDABLE HOUSING DWELLINGS AT THE NORTHWEST AND SOUTHWEST CORNERS OF EMPORIA STREET AND PALM AVENUE. RELATED FILES: PUD-21-001 AND PMTT21-004.

The “Standard Conditions of Approval” contained in Resolution No. 2017-027 apply. The applicant shall read and be thoroughly familiar with these conditions, including but not limited to, the requirements listed below.

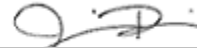
- Areas outside apartments, duplexes, and condominiums, are to be provided with a photocell operated exterior lighting system, which switches on at sunset and switches off at sunrise. This is to include walkways, doorways, and other areas used by the public and common areas. Minimum 0.5 foot-candle of light shall be maintained in all common areas. Planned landscaping shall not obstruct lighting.
- During hours of darkness, all parking lots and carports shall be provided with minimum one foot-candle of light, measured on the parking surface. Lighting devices shall be fully protected with weather and vandalism resistant covers.
- First floor and all common stairwells shall be constructed to either allow for visibility through the stairwell risers or to prohibit public access to the areas behind stairwells. Prevention of public access shall not be constructed to allow for easy disregard of barrier and shall fully prohibit access to area.
- The Applicant shall comply with all construction site security requirements as stated in the Standard Conditions. This includes the provisions for perimeter lighting, site lighting, fencing and/or uniformed security.
- Rooftop addresses shall be installed on the buildings as stated in the Standard Conditions. The numbers shall be at a minimum 3 feet tall and 1 foot wide, in reflective white paint on a flat black background, and oriented with the bottom of the numbers towards the addressed street. Each building shall be labeled with the corresponding address and letter if applicable.

The Applicant is invited to call Bill Lee at (909) 408-1672 with any questions regarding these conditions.

CITY OF ONTARIO
LANDSCAPE PLANNING DIVISION
 303 East "B" Street, Ontario, CA 91764

CONDITIONS OF APPROVAL

Sign Off



Jamie Richardson, Sr. Landscape Planner

5/11/2021

Date

Reviewer's Name:

Jamie Richardson, Sr. Landscape Planner

Phone:

(909) 395-2615

D.A.B. File No.:

PDEV21-008

Case Planner:

Diane Ayala

Project Name and Location:

Related Emporia

NW and SW corners of Emporia Street and Palm Avenue

Applicant/Representative:

The Related Companies of California, LLC randy.mai@related.com

18201 Von Karman Avenue, Suite 900

Irvine CA, 92612



Preliminary Plans (dated 4/20/2021) meets the Standard Conditions for New Development and has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.



Preliminary Plans (dated) has not been approved. Corrections noted below are required prior to Preliminary Landscape Plan approval.

A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED AS INCOMPLETE.

Landscape construction plans with plan check number may be emailed to: landscapeplancheck@ontarioca.gov

DIGITAL SUBMITTALS MUST BE 10MB OR LESS.

Civil/ Site Plans

1. Show on demo plans and landscape construction plans trees to be preserved, removed or mitigation measures for trees removed, such as:
 - a. New 15 gallon trees min 1" diameter trunk, in addition to trees required.
 - b. New 24" box trees min 1.5" diameter trunk, in addition to trees required.
 - c. Upsizing trees on the plan one size larger such as 15 gallon to 24" box, or 24" to 36" box size.
 - d. Monetary value of the trees removed as identified in the "Guide for Plant Appraisal", approved certified arborist plant appraiser, or may be equal to the value of the installation cost of planting, fertilizing, staking and irrigating 15 gallon trees, (100\$ each) to the City of Ontario Historic Preservation Fund for city tree planting or city approved combination of the above items.
2. Storm water infiltration devices located in landscape areas shall be reviewed and plans approved by the Landscape Planning Division prior to permit issuance. Any storm water devices in parkway areas shall not displace street trees.
3. Show/Note transformers set back 5' from paving all sides. Coordinate with landscape plans.
4. Show/Note backflow devices set back 4' from paving all sides. Locate on level grade
5. Locate utilities including light standards, fire hydrants, water, drain and sewer lines to not conflict with required tree locations. Coordinate civil plans with landscape plans.
6. Note for compaction to be no greater than 85% at landscape areas. All finished grades at 1 1/2" below finished surfaces. Slopes to be maximum 3:1.
7. Dimension, show and call out for step-outs at parking spaces adjacent to planters; a 12" wide monolithic concrete curb, DG paving or pavers with edging.

8. Add Note to Grading and Landscape Plans: Landscape areas where compaction has occurred due to grading activities and where trees or storm water infiltration areas are located shall be loosened by soil fracturing. For trees a 12'x12'x18" deep area; for storm water infiltration the entire area shall be loosened. Add the following information on the plans: The back hoe method of soil fracturing shall be used to break up compaction. A 4" layer of Compost is spread over the soil surface before fracturing is begun. The back hoe shall dig into the soil lifting and then drop the soil immediately back into the hole. The bucket then moves to the adjacent soil and repeats. The Compost falls into the spaces between the soil chunks created. Fracturing shall leave the soil surface quite rough with large soil clods. These must be broken by additional tilling. Tilling in more Compost to the surface after fracturing per the soil report will help create an A horizon soil. Imported or reused Topsoil can be added on top of the fractured soil as needed for grading. The Landscape Architect shall be present during this process and provide certification of the soil fracturing. For additional reference see Urban Tree Foundation – Planting Soil Specifications.

Landscape Plans

9. Show backflow devices with 36" high strappy leaf shrub screening and trash enclosures and transformers, a 4'-5' high evergreen hedge screening. Do not encircle utility, show as masses and duplicate masses in other locations on regular intervals.
10. Locate light standards, fire hydrants, water and sewer lines to not conflict with required tree locations. Coordinate civil plans with landscape plans
11. Show all utilities on the landscape plans. Coordinate so utilities are clear of tree locations.
12. After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections at a rate established by resolution of the City Council.

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT



Project File No.: PUD21-001, PMTT21-004, PDEV21-008 & PHP21-003

Address: SEC of Holt Blvd. & Fern Avenue

APN: 1049-054-02, 1049-054-03, 1049-054-06, 1049-059-07 & 1049-059-06

Existing Land Use: Vacant Lots and commercial buildings

Proposed Land Use: A PUD to establish development standards, Parcel Map to consolidate 2.15 acres into 1 parcel, a Development Plan to construct 50 Multi-family residential units

Site Acreage: 2.15 Proposed Structure Height: 55 FT

ONT-IAC Project Review: n/a

Airport Influence Area: ONT

Reviewed By: Lorena Mejia

Contact Info: 909-395-2276

Project Planner: Diane Ayala

Date: 5/11/2021

CD No.: 2021-016

PALU No.: n/a

The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="checkbox"/> High Terrain Zone	<input type="checkbox"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input checked="" type="checkbox"/> FAA Notification Surfaces	<input checked="" type="checkbox"/> Recorded Overflight Notification
<input type="radio"/> Zone 2	<input type="checkbox"/> 65 - 70 dB CNEL	<input checked="" type="checkbox"/> Airspace Obstruction Surfaces	<input type="checkbox"/> Real Estate Transaction Disclosure
<input type="checkbox"/> Zone 3	<input checked="" type="checkbox"/> 60 - 65 dB CNEL	<input type="checkbox"/> Airspace Avigation Easement Area	
<input type="radio"/> Zone 4		Allowable Height: 90 FT	
<input type="radio"/> Zone 5			

The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1
 Zone 2
 Zone 3
 Zone 4
 Zone 5
 Zone 6

Allowable Height: _____

CONSISTENCY DETERMINATION

This proposed Project is: Exempt from the ALUCP
 Consistent
 Consistent with Conditions
 Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

See attached condition.

Airport Planner Signature: _____

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT

CD No.: 2021-016
PALU No.: _____

PROJECT CONDITIONS

New Residential land uses are required to have a Recorded Overflight Notification appearing on the Property Deed and Title incorporating the following language:

NOTICE OF AIRPORT IN VICINITY: This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

FILE NOS: PSPA20-003 and PDEV20-008

SUBJECT: An Amendment to the California Commerce Center Specific Plan (File No. PSPA20-003), changing the land use designation on 10.64 acres of land from Commercial/Food/Hotel to Light Industrial, to be consistent with The Ontario Plan Policy Plan (General Plan) Industrial (0.55 FAR) land use designation, in conjunction with a Development Plan (File No. PDEV20-008) to construct a 200,291-square foot industrial building located at the northeast corner of Haven Avenue and Airport Drive; (APN: 0211-222-66) **submitted by Vogel Properties, Inc.**

PROPERTY OWNER: Vogel Properties, Inc.

RECOMMENDED ACTION: That the Planning Commission consider and adopt the following:

[1] A resolution recommending that the City Council approve the use of an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140); and


[2] A resolution recommending that the City Council approve the amendment to the California Commerce Center Specific Plan, File No. PSPA20-003, pursuant to the facts and reasons contained in this staff report and the attached resolution; and

[3] A resolution approving the Development Plan, File No. PDEV20-008, pursuant to the facts and reasons contained in this staff report, the resolution, and subject to the conditions of approval.

PROJECT SETTING: The project site is comprised of 10.64 acres of land located at the northeast corner of Haven Avenue and Airport Drive, within the proposed



Figure 1: Project Location

Case Planner:	Jeanie Irene Aguiló
Planning Director Approval:	
Submittal Date:	03/27/2020

Hearing Body	Date	Decision	Action
DAB	05/17/2021	Approval	Recommend
PC	05/25/2021		Final
CC			

Light Industrial land use district of the California Commerce Center Specific Plan, and is depicted in Figure 1: Project Location, above. The project site is currently developed as a parking lot. Land uses immediately surrounding the project site include commercial to the north; industrial to the east, and vacant land to the south and west. The existing surrounding land uses, zoning, and general plan and specific plan land use designations are summarized in the "Surrounding Zoning & Land Uses" table located in the Technical Appendix of this report.

PROJECT ANALYSIS:

(1) Background — The project site was developed in 1999 as a privately-owned long-term parking lot to accommodate customers from Ontario International Airport. The site is presently developed with two structures totaling 1,500 square feet, including a toll booth and a modular office building. The Applicant will no longer operate the site as a parking lot.

On March 27, 2020, the Applicant submitted an amendment to the California Commerce Center Specific Plan (File No. PSPA20-003) to change the land use designation of the Project site from Commercial/Food/Hotel to Light Industrial, to be consistent with The Ontario Plan Policy Plan (General Plan), which designates the subject site for Industrial (0.55 FAR) land uses, in conjunction with a Development Plan application (File No. PDEV20-008), which proposes to develop the Project site with an industrial warehouse building.

On May 17, 2021, the Development Advisory Board ("DAB") conducted a hearing to consider the Development Plan, and concluded the hearing, voting to recommend that the Planning Commission approve the Application subject to conditions of approval, which are included as attachments to the Planning Commission resolutions.

(2) Amendment to the California Commerce Center Specific Plan (File No. PSPA20-003) — To date, the City has been processing land use changes within Specific Plans to achieve consistency with the Policy Plan Land Use Plan on an as needed basis, when new development projects are proposed, and land use inconsistencies must be addressed. The subject site presently has a Specific Plan land use designation of Commercial/Food/Hotel, which is inconsistent with the Industrial land use designation assigned to the property by the Policy Plan Land Use Plan. To establish consistency between the two land use plans and facilitate the proposed Development Plan application, the Specific Plan Amendment will change the land use designation from Commercial/Food/Hotel to Light Industrial, (See Figures 2 and 3: Existing and Proposed California Commerce Center Specific Plan Land Use, on the proceeding page). Furthermore, the Policy Plan Land Use Map designates properties surrounding the project site (immediately to east and south) for Industrial land uses, providing further land use consistency within the immediate vicinity of the project site.

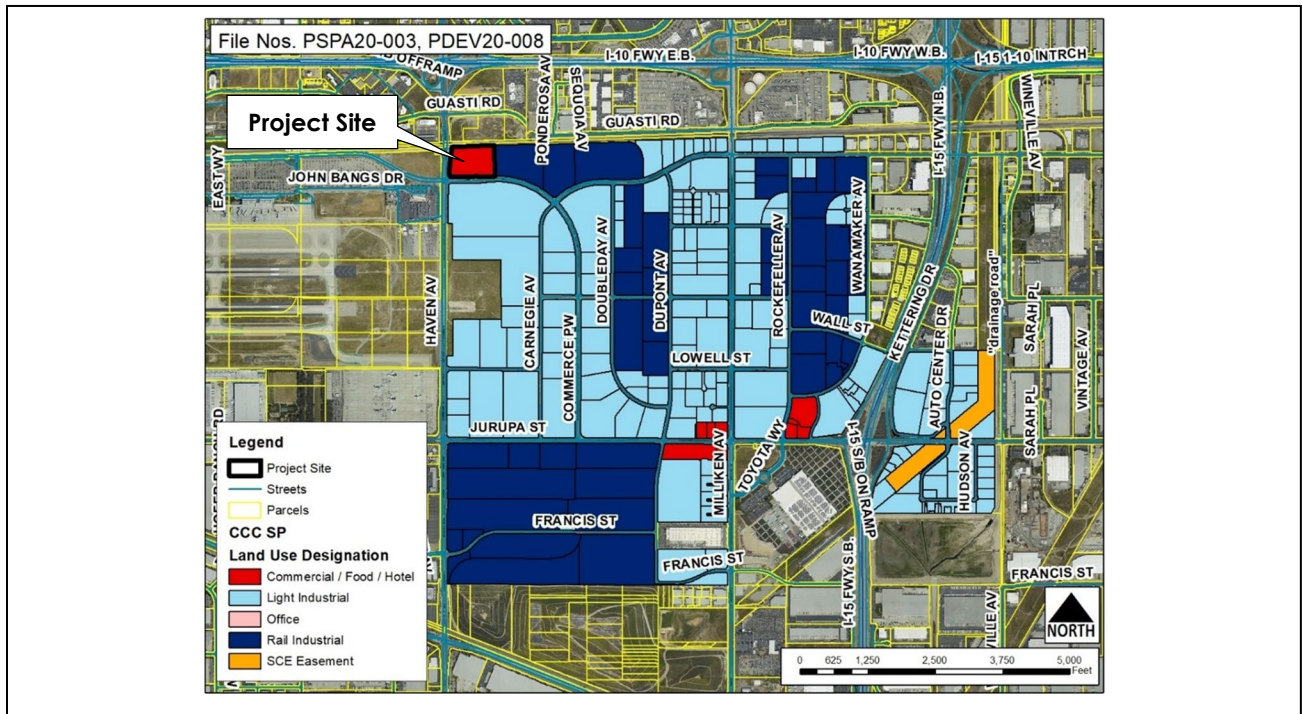


Figure 2: Existing – California Commerce Center Specific Plan Land Use Plan

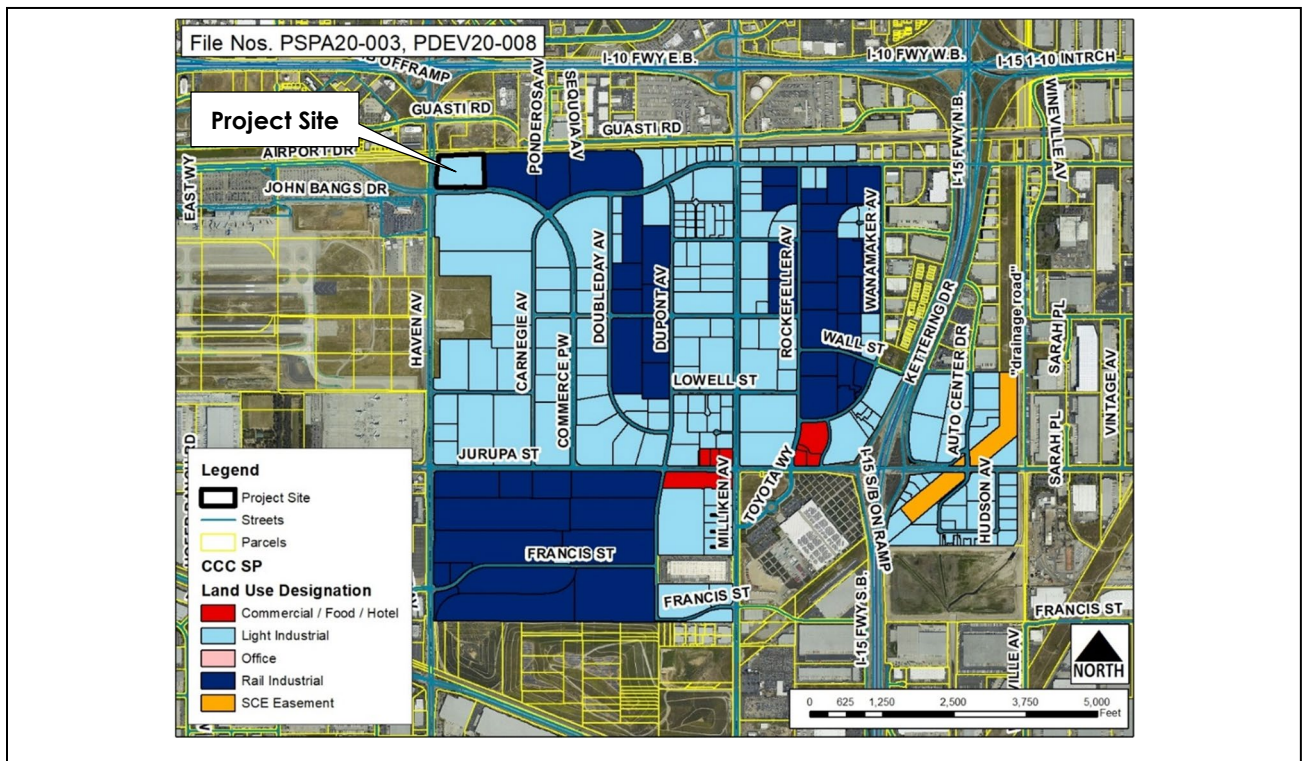


Figure 3: Proposed – California Commerce Center Specific Plan Land Use Plan

(3) Development Plan (File No. PDEV20-008)

(a) Site Design/Building Layout — The applicant is proposing to construct a 200,291-square foot industrial building with a floor area ratio ("FAR") of 0.43. The rectangular-shaped building is located along the northern portion of the site, with the front of the building and office entry located at the southwest corner of the building and oriented to the west, facing Haven Avenue. The building is setback approximately 95 feet from the north (rear) property line, approximately 140 feet from the south (Airport Drive) property line, 73 feet from the west (Haven Avenue) property line, and 3 feet from the east (interior) property line. The project will provide off-street parking along the northern, western, and southern portions of the site, in addition to a smaller parking area located at the southeast corner of the site to serve warehouse employees (see Exhibit B—Site Plan, attached).

A yard area designed for tractor-trailer parking, truck maneuvering, loading activities, and outdoor staging, is located on the south side of the proposed building. The yard area will be screened from public street views by a combination of landscaping and 14-foot high tilt-up screen walls with view-obstructing gates that have been designed to match the architecture and color scheme of the proposed building (see Exhibit C—Elevations – Industrial Warehouse Building, attached). An outdoor employee patio area has been provided on-site, located adjacent to the building's western office entry.

(b) Site Access/Circulation — The Project has two points of vehicular access along Airport Drive, including a 30-foot wide driveway located near the southwest corner of the site and a 50-foot wide driveway located near the southeast corner of the site, which will be shared by both standard vehicles and tractor-trailers accessing the yard area and parking lot. A 24-foot wide drive-aisle is proposed along the western, northern, and southern portions of the site, connecting the two points of street access and two emergency access drives located on the northeast and southeast portions of the site. The emergency access drives will be gated and will connect to the adjoining property's north-south running drive aisle. To provide access to the emergency drive aisle from the adjoining property, the project has been conditioned to provide a reciprocal access agreement between the two property owners (see Exhibit B—Site Plan, attached).

(c) Parking — The Project has provided off-street parking pursuant to the "Warehouse and Distribution" parking standards specified in the Development Code. The industrial building requires a total of 111 off-street parking spaces, and 140 spaces have been provided. In addition, a minimum of one tractor-trailer parking space for each 4 dock-high loading spaces is required to be provided. There are 25 dock-high loading doors proposed, requiring 7 tractor-trailer parking spaces. The project is providing 22 tractor-trailer parking spaces, exceeding the minimum requirement.

(d) Traffic Impact Analysis — A focused Traffic Analysis was prepared for the project site by TJW Engineering (Dated: August 27, 2020), that compared the trip generation between the existing California Commerce Center Specific Plan

Commercial/Food/Hotel land use designation (shopping center/commercial) versus the proposed Light Industrial land use at a FAR of 0.55 (warehouse/small ancillary office). The trip generation analysis utilized the Industry standard Institute of Transportation Engineers ("ITE") Trip Generation Manual (10th Edition, 2017) to determine trip generation rates for the existing and proposed land uses and represents the amount of traffic, both inbound and outbound, produced by each land use. The Traffic Analysis concluded that proposed change in land use from Commercial/Food/Hotel to Light Industrial would result in a net difference of 7,183 fewer Average Daily Trips ("ADT") trips, including 146 fewer A.M. peak hour trips, and 719 fewer P.M. peak hour trips (see Table 1: Net Difference in Trip Generation, below).

Table 1: Net Difference in Trip Generation

Land Use ¹	Qty	Unit ²	Daily Trips (ADTs)		AM Peak Hour					PM Peak Hour				
			Rate	Volume	Rate	In:Out Split	Volume			Rate	In:Out Split	Volume		
							In	Out	Total			In	Out	Total
Proposed Project														
Warehousing (150)	196.49	TSF	1.74	342	0.17	77:23	25	8	33	0.19	27:73	10	27	37
Small Office (712)	5.00	TSF	16.19	81	1.92	83:17	8	2	10	2.45	32:68	4	8	12
Total		TSF		423			33	10	43			14	35	49
Designated Land Use														
Shopping Center/Commercial (820)	201.49	TSF	37.75	7,606	0.94	62:38	117	72	189	3.81	48:52	369	399	768
Net Difference				-7,183			-84	-62	-146			-355	-364	-719

1: Rates from ITE Trip Generation (10th Edition, 2017)
 2: TSF = Thousand Square Feet

(e) Architecture — The proposed industrial warehouse building will be of concrete tilt-up construction. Architecturally, the building incorporates smooth-painted concrete, horizontal reveals, color blocking, clerestory windows with clear anodized aluminum mullions and blue glazing, and steel canopies over the main office entries and first story windows (see Exhibit C—Elevations, attached). The mechanical equipment will be roof-mounted and obscured from public view by parapet walls and equipment screens, if necessary, which will be incorporated into the design of the building architecture. Staff believes that the proposed project illustrates the type of high-quality architecture that is promoted by the Development Code. This is exemplified through the use of:

- Articulation in the building footprint, incorporating a combination of recessed and popped-out wall areas;
- Articulation in the building parapet/roof line, which serves to accentuate the building's entries and breaks up large expanses of building wall;
- A mix of exterior materials, finishes and fixtures;

- Base and top treatments defined by changes in color, materials, and recessed wall areas; and
- An architectural design that ensures that the building's massing, proportion, color palette, and architectural detailing are consistent throughout all four building elevations.

(f) Landscaping — The project provides landscaping along the Haven Avenue and Airport Drive frontages, around the project perimeter, and tractor-trailer yard area. The Development Code requires that the project provide a minimum 15 percent landscape coverage, which has been provided. Moreover, a combination of 24-inch, 36-inch, and 48-inch box accent and shade trees will be provided throughout the project site, in addition to a variety of shrubs and groundcovers that are low water usage and drought tolerant. The proposed on-site and off-site landscape improvements will assist towards creating a walkable, safe area for pedestrians to access the project site (see Exhibit D—Landscape Plan, attached).

(g) Utilities (drainage, sewer) — Public utilities (water and sewer) are available to serve the project. Furthermore, the Applicant has submitted a Preliminary Water Quality Management Plan ("PWQMP"), which establishes the project's compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development ("LID") best management practices ("BMPs"), such as retention and infiltration, biotreatment, and evapotranspiration. The PWQMP proposes on-site run-off will be collected by a catch basin and conveyed to an underground infiltration system located within the tractor-trailer courtyard area. Any overflow drainage will be conveyed to 30-inch on-site storm drain that connects to a 60-inch storm drain located within Airport Drive.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

(1) City Council Goals.

- Invest in the Growth and Evolution of the City's Economy
- Maintain the Current High Level of Public Safety
- Operate in a Businesslike Manner
- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)

(2) Vision.

Distinctive Development:

- Commercial and Residential Development
 - Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

(3) Governance.

Decision Making:

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
 - G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

(4) Policy Plan (General Plan).

Land Use Element:

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.
 - LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.
 - LU1-6 Complete Community: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).
- Goal LU2: Compatibility between a wide range of uses.
 - LU2-6: Infrastructure Compatibility: We require infrastructure to be aesthetically pleasing and in context with the community character.

Community Economics Element:

- Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.
 - CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.
 - CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.
 - CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.
 - CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Safety Element:

- Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.
 - S1-1 Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

Community Design Element:

- Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.
 - CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:
 - Building volume, massing, and height to provide appropriate scale and proportion;
 - A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
 - Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

- CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.
- CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.
- CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.
- CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.
- CD2-11 Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.
- CD2-12 Site and Building Signage. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.
- CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.
 - Goal CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.
- CD3-1 Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort, and aesthetics.

- CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.
- CD3-3 Building Entrances. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces.
- CD3-5 Paving. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.
- CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.
 - Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.
- CD5-1 Maintenance of Buildings and Property. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.
- CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

HOUSING ELEMENT COMPLIANCE: The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

ENVIRONMENTAL REVIEW: Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140). The Addendum concluded that the Specific Plan Amendment and the Development Plan introduces no new significant environmental impacts.

Approval of the Development Plan is contingent upon City Council approval of the and the Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) and Amendment to the California Commerce Center Specific Plan (File No. PSPA20-003).

CONDITIONS OF APPROVAL: See attached department reports.

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

	Existing Land Use	General Plan Designation	Zoning Designation	Specific Plan Land Use
Site	Parking Lot	Industrial	California Commerce Specific Plan	Proposed Light Industrial
North	Retail (Costco and Starbucks)	Office/Commercial	Ontario Gateway Specific Plan	Mixed Use and Office
South	Vacant	Industrial	California Commerce Specific Plan	Light Industrial
East	Industrial Warehouse	Industrial	California Commerce Specific Plan	Rail Industrial
West	Vacant	Airport	ONT – Ontario International Airport	N/A

General Site & Building Statistics

Item	Proposed	Min./Max. Standard	Meets Y/N
Project Area:	10.64 ac	N/A	Y
Lot/Parcel Size:	463,478 SF	43,560 SF (Min.)	Y
Building Area:	200,291 SF	N/A	Y
Floor Area Ratio:	0.43	0.55 (Max.)	Y
Building Height:	50 FT	120 FT (Max.)	Y

Off-Street Parking:

Type of Use	Building Area	Parking Ratio	Spaces Required	Spaces Provided
Warehouse/Distribution	200,291 SF	One space per 1,000 SF (0.001/SF) for portion of GFA < 20,000 SF, plus 0.5 space per 1,000 SF (0.0005/SF) for GFA > 20,000 SF;	111	140
		One tractor-trailer parking space per 4 dock-high loading doors (25 dock-high loading doors proposed)	7	22
Office	2,500 SF + 2,500 Mezzanine	Parking required parking for "general business offices" and other associated uses, when those uses exceed 10 percent of the building GFA.	0	0
TOTAL (excludes tractor-trailer spaces)			111	140

Exhibit A—PROJECT LOCATION MAP

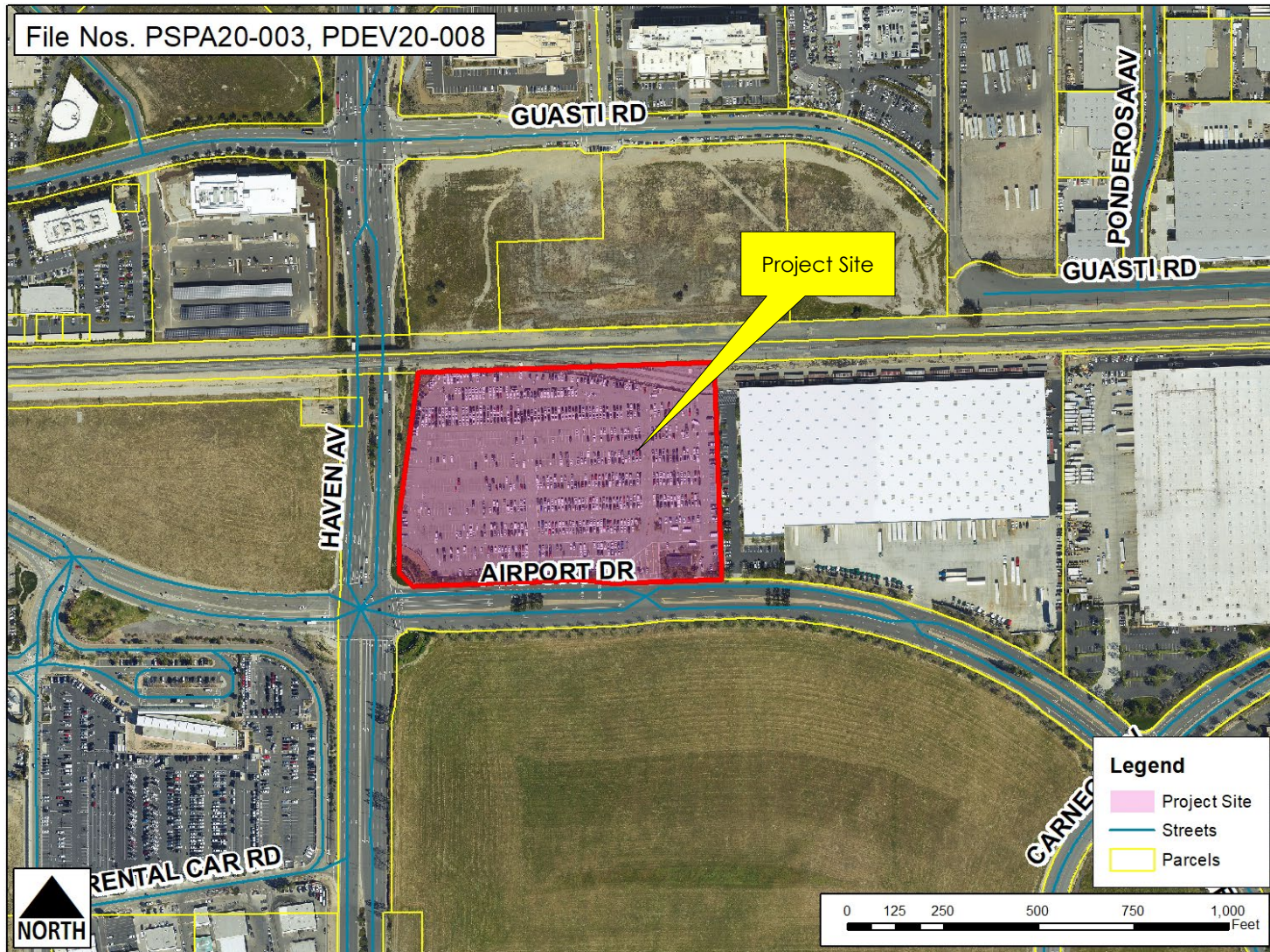


Exhibit B—SITE PLAN

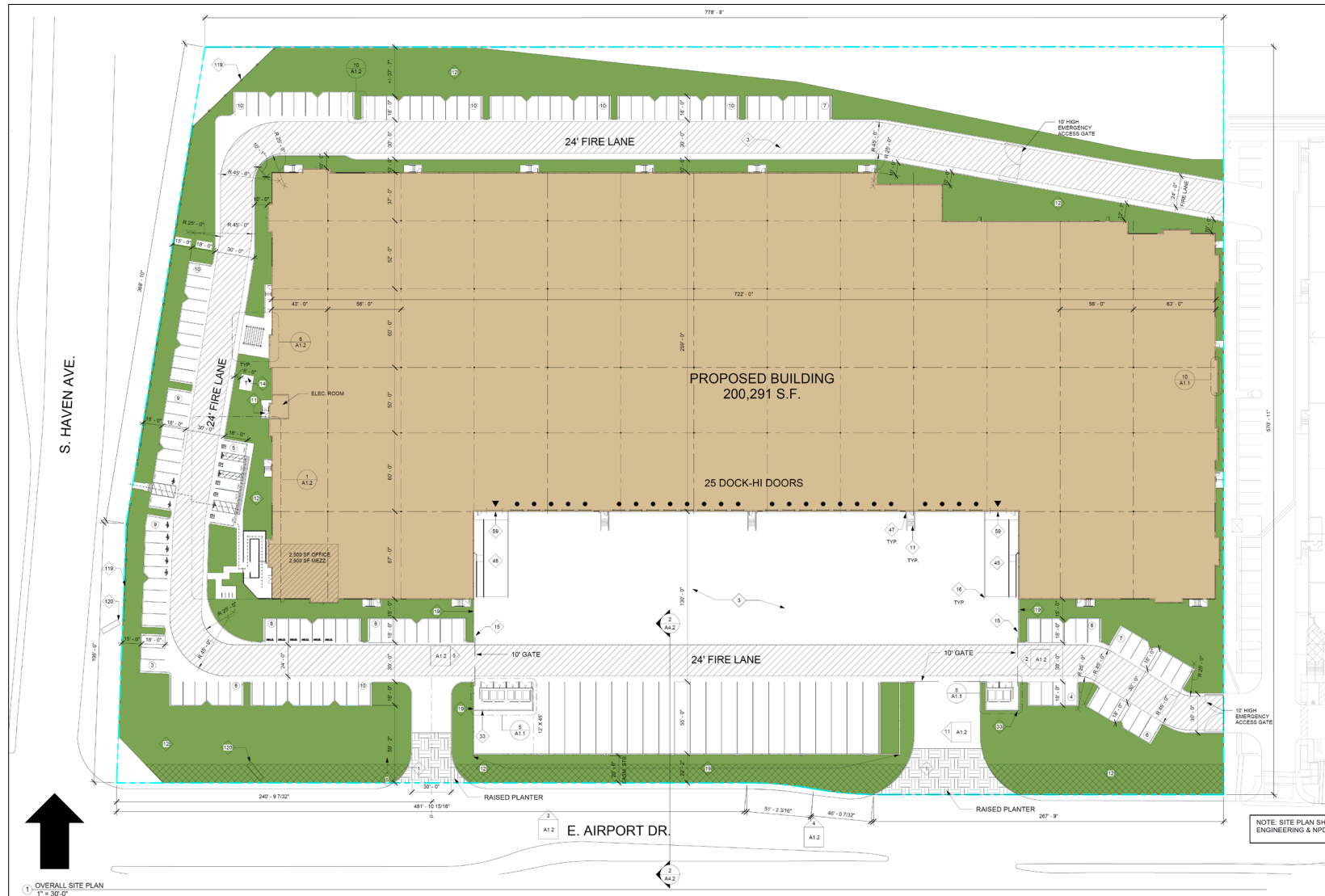


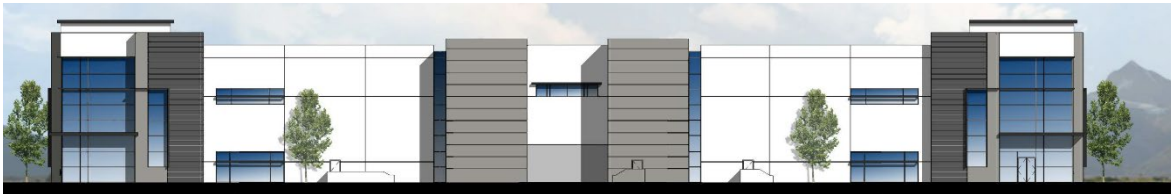
Exhibit C—EXTERIOR ELEVATIONS



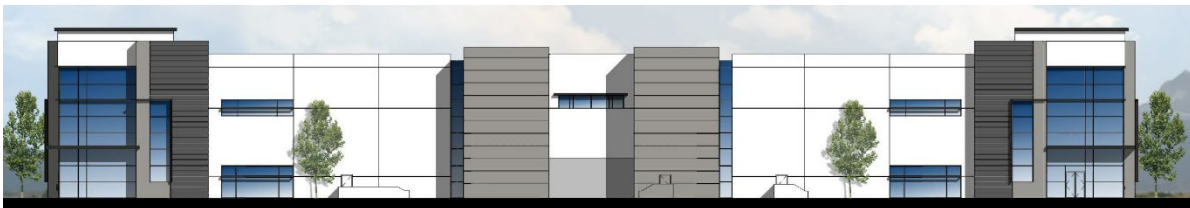
North Elevation



South Elevation

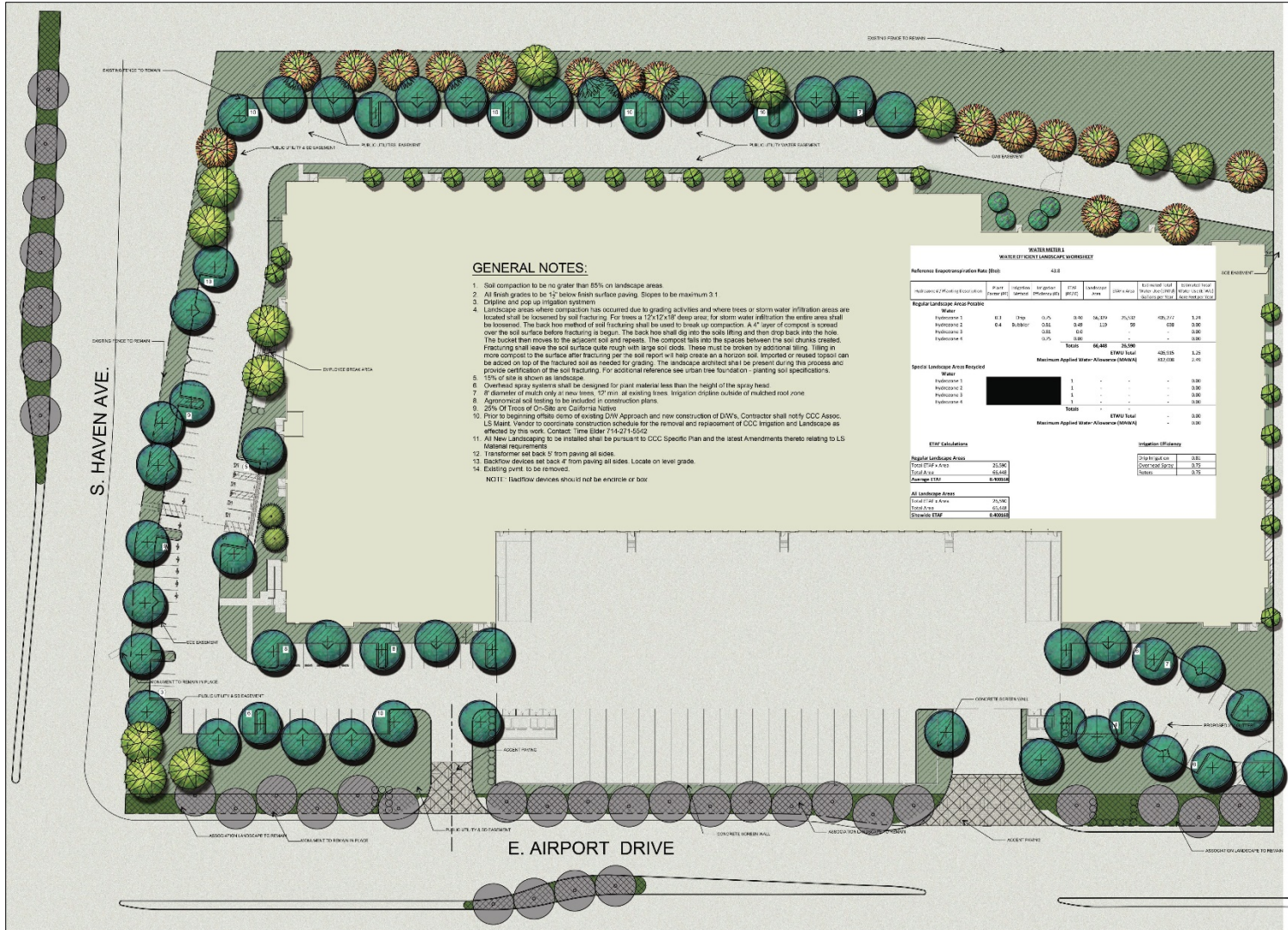


East Elevation



West Elevation

Exhibit D—LANDSCAPE PLAN



RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO RECOMMENDING CITY COUNCIL APPROVE THE USE OF AN ADDENDUM TO THE ONTARIO PLAN ENVIRONMENTAL IMPACT REPORT, PURSUANT TO THE REQUIREMENTS OF THE CALIFORNIA ENVIRONMENTAL QUALITY ACT, AS AMENDED, FOR FILE NOS. PSPA20-003 AND PDEV20-008

WHEREAS, VOGEL PROPERTIES, INC. (hereinafter referred to as "Applicant") has filed an Application for the approval of an Amendment to the California Commerce Center Specific Plan (File No. PSPA20-003), changing the land use designation on 10.64 acres of land from Commercial/Food/Hotel to Light Industrial, to be consistent with The Ontario Plan Policy Plan (General Plan) Industrial (0.55 FAR) land use designation, in conjunction with a Development Plan (File No. PDEV20-008) to construct a 200,291 square foot industrial building located at the northeast corner of Haven Avenue and Airport Drive, in the City of Ontario, California (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the property to the north of the Project site is within the Office/Commercial land use designation of the Ontario Gateway Specific Plan and is developed with retail land uses (Costco and Starbucks). The property to the east is within the Rail Industrial land use designation of the California Commerce Center Specific Plan and is developed with industrial land uses. The property to the south is within the Light Industrial land use designation of the California Commerce Center Specific Plan and is currently vacant. The property to the west is within the ONT (Ontario International Airport) zoning district and is currently vacant; and

WHEREAS, The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was certified on January 27, 2010, (hereinafter referred to as "Certified EIR") in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario has prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as "EIR Addendum") in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as "CEQA"); and

WHEREAS, the EIR Addendum concluded that implementation of the Project could result in a number of significant effects on the environment that were previously analyzed in the Certified EIR, and that the Certified EIR identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, pursuant to CEQA Guidelines Section 15164(a), a lead agency shall prepare an addendum to a previously certified EIR if some changes or additions are necessary to a project, but the preparation of a subsequent or supplemental EIR is not required; and

WHEREAS, the City determined that none of the conditions requiring preparation of a subsequent or supplemental EIR would occur from the Project, and that preparation of an Addendum to the Certified EIR was appropriate; and

WHEREAS, the City of Ontario is the lead agency on the Project, and the Planning Commission is the recommending authority for the requested approval to construct and otherwise undertake the Project; and

WHEREAS, the Planning Commission has reviewed and considered the EIR Addendum for the Project, has concluded that none of the conditions requiring preparation of a subsequent or supplemental EIR have occurred, and intends to take actions on the Project in compliance with CEQA and state and local guidelines implementing CEQA; and

WHEREAS, the EIR Addendum for the Project are on file in the Planning Department, located at 303 East B Street, Ontario, CA 91764, are available for inspection by any interested person at that location and are, by this reference, incorporated into this Resolution as if fully set forth herein; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the recommending authority for the Project, The Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The environmental impacts of the Project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was certified on January 27, 2010 in conjunction with File No. PGPA06-001; and

(2) The EIR Addendum and administrative record have been completed in compliance with CEQA, the CEQA Guidelines, and the City of Ontario Local CEQA Guidelines.

(3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts.

(4) All previously adopted mitigation measures shall be a condition of project approval, as they are applicable to the Project, and are incorporated herein by this reference.

(5) The EIR Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and

(6) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts.

SECTION 2: *Additional Environmental Review Not Required.* Based on the Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Environmental Impact Report is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

(d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: *Planning Commission Action.* Based upon the findings and conclusions set forth in Sections 1 and 2, above, the Planning Commission hereby recommends the City Council finds that based upon the entire record of proceedings before it, and all information received, that there is no substantial evidence that the Project will constitute substantial changes to the Certified EIR, and does hereby approve the EIR Addendum, attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 4: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 5: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 6: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of May 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 25, 2021, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:
Addendum to The Ontario Plan
Environmental Impact Report

(Addendum to follow this page)



City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

California Environmental Quality Act Addendum to The Ontario Plan Environmental Impact Report

Project Title/File Nos.: PSPA20-003 and PDEV20-008

Lead Agency: City of Ontario, 303 East "B" Street, Ontario, California 91764, (909) 395-2036

Contact Person: Jeanie Irene Aguilo, Associate Planner, 909-395-2418

Project Sponsor: William Vogel, Vogel Properties, Inc., 3000 Paseo Tesoro, Walnut, CA 91789

Project Location: The project site is located in southwestern San Bernardino County, within the City of Ontario. The City of Ontario is located approximately 40 miles from downtown Los Angeles, 20 miles from downtown San Bernardino, and 30 miles from Orange County. As illustrated on Figures 1 and 2, below, the project site is located on Assessor Parcel Number (APN): 0211-222-66, which is comprised of 10.64 acres of land located on the northeast corner of Airport Drive and Haven Avenue.

Figure 1: REGIONAL LOCATION MAP

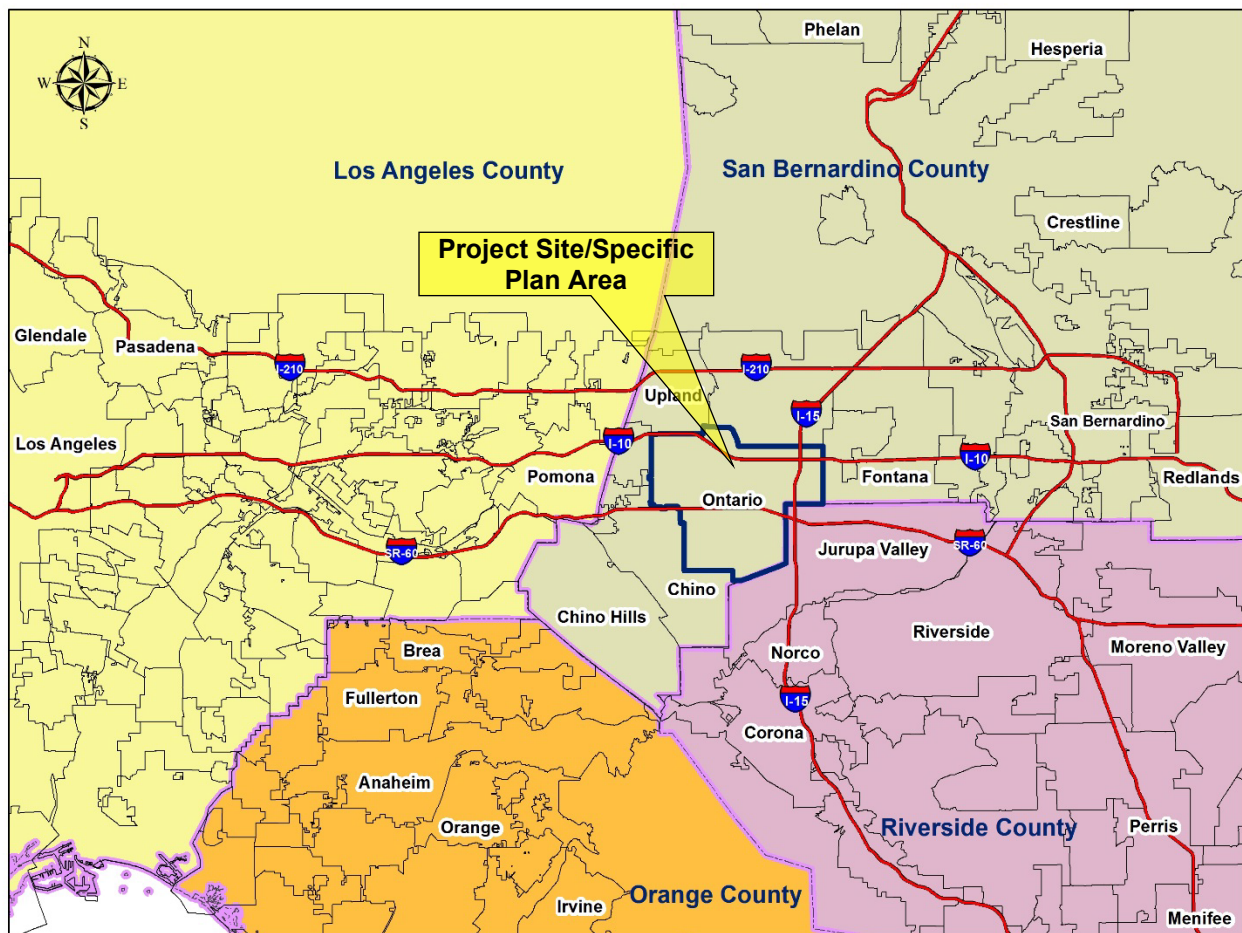
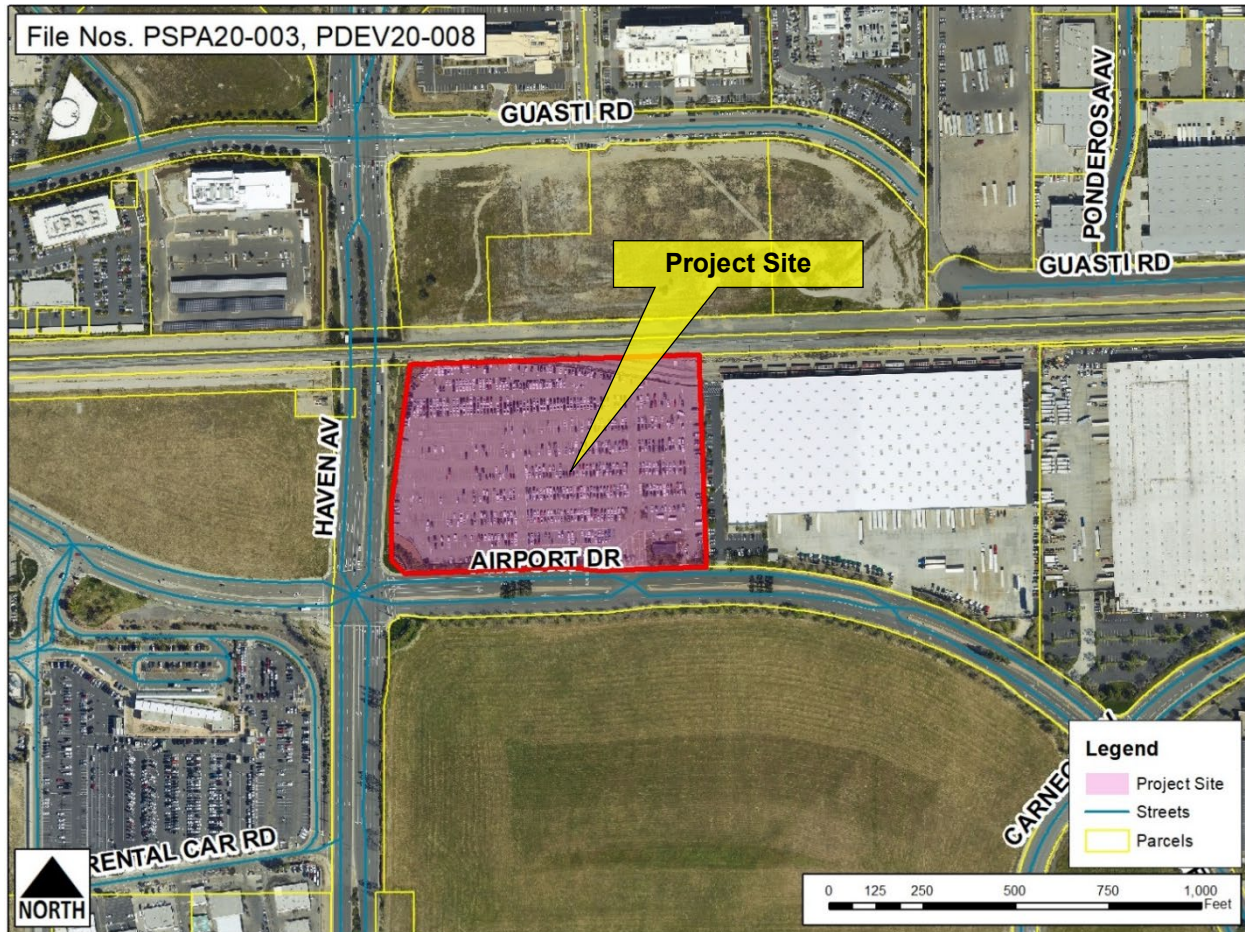


Figure 2: VICINITY MAP



General Plan Designation: Industrial (0.55 FAR)

Zoning:

1. Existing – California Commerce Center Specific Plan, Commercial/Food/Hotel land use district
2. Proposed – California Commerce Center Specific Plan, Light Industrial land use district

Description of Project: The project proposes the following entitlements:

1. An Amendment to the California Commerce Center Specific Plan, changing the land use designation on 10.64 acres of land from Commercial/Food/Hotel to Light Industrial, to be consistent with The Ontario Plan Policy Plan (General Plan) Industrial (0.55 FAR) land use designation; and
2. A Development Plan to construct a 200,291-square foot industrial building on 10.64 acres of land located at the northeast corner of Haven Avenue and Airport Drive, within the proposed Light Industrial land use district of the California Commerce Center Specific Plan.

The California Commerce Center Specific Plan and Environmental Impact Report No. 81-4 (EIR) was adopted and certified by the City Council on May 17, 1984, with a Statement of Overriding Considerations. The Specific Plan encompasses approximately 1,500 acres of land generally located north of the SR-60 Pomona Freeway, South of Mission, Boulevard, east of Haven Avenue and west of Doubleday Street. The Specific Plan Amendment will include the following revisions to the Specific Plan:

- Any graphic, table, and/or text that currently identifies the Project Site of 10.64 acres of land from Commercial/Food/Hotel, will be changed to Light Industrial.

Project Setting: The project site is comprised of 10.64 acres of land located at the northeast corner of Haven Avenue and Airport Drive. The site slopes to the south and is presently developed as a parking lot with two structures totaling 1,500 square feet that consist of a toll booth and modular office building. The site is surrounded by commercial development to the north, vacant land to the south and west, and industrial warehouse to the east, as shown in the table below.

Surrounding Land Uses:

	Existing Land Use	General Plan Designation	Zoning Designation	Specific Plan Land Use
Site:	Parking Lot	Industrial	California Commerce Specific Plan	Proposed Light Industrial
North:	Retail (Costco and Starbucks)	Office/Commercial	Ontario Gateway Specific Plan	Mixed Use and Office
South:	Vacant	Industrial	California Commerce Specific Plan	Light Industrial
East:	Industrial Warehouse	Industrial	California Commerce Specific Plan	Rail Industrial
West:	Vacant	Airport	ONT – Ontario International Airport	N/A

Background: On January 27, 2010, the Ontario City Council adopted The Ontario Plan (“TOP”). TOP serves as the framework for the City’s business plan and provides a foundation for the City to operate as a municipal corporation that consists of six (6) distinct components: 1) Vision; 2) Governance Manual; 3) Policy Plan; 4) Council Priorities; 5) Implementation; and 6) Tracking and Feedback. The Policy Plan component of TOP meets the functional and legal mandate of a General Plan and contains nine elements: Land Use, Housing, Parks and Recreation, Environmental Resources, Community Economics, Safety, Mobility, Community Design and Social Resources.

An Environmental Impact Report (“EIR”) was prepared for TOP (State Clearinghouse No. 2008101140) and certified by the City Council on January 27, 2010 (hereinafter referred to as “Certified EIR”), which included Mitigation, Findings and a Statement of Overriding Considerations pursuant to the requirements of the California Environmental Quality Act, commencing with Public Resources Code Section 21000 (“CEQA”). The Certified EIR analyzed the direct and physical changes in the environment that would be caused by TOP; focusing on changes to land use associated with the buildout of the proposed land use plan, and in the Policy Plan and impacts resultant of population and employment growth in the City. The subject site was analyzed in the Certified EIR as Industrial (see Exhibit A: Existing – California Commerce Center Specific Plan Land Use Plan) to be consistent with the industrial uses to the south, east, and west of the subject site. The significant unavoidable adverse impacts that were identified in Certified EIR included agriculture resources, air quality, cultural resources, greenhouse gas emissions, noise, and transportation/traffic.

Analysis: According to the California Environmental Quality Act Guidelines Section 15164, an Addendum to a previously certified EIR may be used if some changes or additions are necessary, but none of the conditions described in Section 15162 requiring the preparation of a subsequent Negative Declaration or EIR have occurred. The CEQA Guidelines require that a brief explanation be provided to support the findings that no subsequent EIR or Negative Declaration are needed for further discretionary approval. These findings are described below:

- Required Finding: Substantial changes are not proposed for the project that will require major revisions of the previous EIR due to the involvement of new, significant environmental effects or a substantial*

increase in the severity of previously identified effects.

Substantial changes are not proposed by the project and project implementation will not require revisions to TOP EIR. TOP EIR analyzed the direct and physical changes in the environment that would be caused by TOP; focusing on changes to land use associated with the buildout of the proposed land use plan. The proposed California Commerce Center Specific Plan land use Amendment from Commercial/Food/Hotel to Light Industrial will bring the project site in conformance with The Ontario Plan Policy Plan (General Plan) Industrial (0.55 FAR) land use designation.

A focused Traffic Analysis was prepared for the project site by TJW Engineering (Dated: August 27, 2020), that compared the trip generation between the existing California Commerce Center Specific Plan Commercial/Food/Hotel land use designation (shopping center/commercial) versus the proposed Light Industrial land use at a FAR of 0.55 (warehouse/small ancillary office). The trip generation analysis utilized the Industry standard Institute of Transportation Engineers (ITE) Trip Generation Manual (10th Edition, 2017) to determine trip generation rates for the existing and proposed land uses and represents the amount of traffic, both inbound and outbound, produced by each land use. The Traffic Analysis concluded that proposed change in land use from Commercial/Food/Hotel to Light Industrial would result in a net difference of 7,183 fewer Average Daily Trips (ADT) trips, including 146 fewer AM peak hour trips, and 719 fewer PM peak hour trips, as shown in Table1: Net Difference in Trip Generation, below.

Table1: Net Difference in Trip Generation

Land Use ¹	Qty	Unit ²	Daily Trips (ADTs)		AM Peak Hour					PM Peak Hour				
			Rate	Volume	Rate	In:Out Split	Volume			Rate	In:Out Split	Volume		
							In	Out	Total			In	Out	Total
Proposed Project														
Warehousing (150)	196.49	TSF	1.74	342	0.17	77:23	25	8	33	0.19	27:73	10	27	37
Small Office (712)	5.00	TSF	16.19	81	1.92	83:17	8	2	10	2.45	32:68	4	8	12
Total		TSF		423			33	10	43			14	35	49
Designated Land Use														
Shopping Center/Commercial (820)	201.49	TSF	37.75	7,606	0.94	62:38	117	72	189	3.81	48:52	369	399	768
Net Difference				-7,183			-84	-62	-146			-355	-364	-719

1: Rates from ITE Trip Generation (10th Edition, 2017)
 2: TSF = Thousand Square Feet

Since the proposed Specific Plan Amendment will bring the project site in conformance with TOP and planned buildout and the proposed project will be consistent with the impacts originally analyzed in TOP EIR, no revisions to TOP EIR are required. In addition, all previously adopted mitigation measures are a condition of project approval and are incorporated herein by reference. The attached Initial Study provides an analysis of the Project and verification that the Project will not cause environmental impacts such that any of the circumstances identified in State CEQA Guidelines Section 15162 are present.

- 2) *Required Finding: Substantial changes have not occurred with respect to the circumstances under which the project is undertaken, that would require major revisions of the previous Environmental Impact Report due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects.*

Substantial changes have not occurred with respect to the circumstances under which the project was undertaken, that would require major revisions to TOP EIR. The proposed California Commerce Center Specific Plan land use Amendment from Commercial/Food/Hotel to Light Industrial will bring the project site in conformance with The Ontario Plan Policy Plan (General Plan) Industrial (0.55 FAR) land use

designation. Therefore, no proposed changes or revisions to the EIR are required. In addition, all previously adopted mitigation measures are a condition of project approval and are incorporated herein by reference. The attached Initial Study provides an analysis of the Project and verification that the Project will not cause environmental impacts such that any of the circumstances identified in State CEQA Guidelines Section 15162 are present.

- 3) *Required Finding. No new information has been provided that would indicate that the proposed project would result in one or more significant effects not discussed in the previous EIR.*

No new information has been provided that would indicate the proposed project would result in any new significant effects not previously discussed in TOP EIR. Therefore, no proposed changes or revisions to the EIR are required. In addition, all previously adopted mitigation measures are a condition of project approval and are incorporated herein by reference. The attached Initial Study provides an analysis of the Project and verification that the Project will not cause environmental impacts such that any of the circumstances identified in State CEQA Guidelines Section 15162 are present.

CEQA Requirements for an Addendum: If changes to a project or its circumstances occur or new information becomes available after adoption of a negative declaration, the lead agency may: (1) prepare a subsequent EIR if the criteria of State CEQA Guidelines Section 15162(a) are met, (2) prepare a subsequent negative declaration, (3) prepare an addendum, or (4) prepare no further documentation. (State CEQA Guidelines Section 15162(b)). When only minor technical changes or additions to the negative declaration are necessary and none of the conditions described in section 15162 calling for the preparation of a subsequent EIR or negative declaration have occurred, CEQA allows the lead agency to prepare and adopt an addendum. (State CEQA Guidelines Section 15164(b).)

Under Section 15162, a subsequent EIR or negative declaration is required only when:

1) Substantial changes are proposed in the project which will require major revisions of the previous negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;

2) Substantial changes occur with respect to the circumstances under which the project is undertaken which will require major revisions of the negative declaration due to the involvement of any new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or

3) New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the negative declaration was adopted, shows any of the following:

a) The project will have one or more significant effects not discussed in the previous negative declaration;

b) Significant effects previously examined will be substantially more severe than shown in the previous EIR;

c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or

d) Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative.

Thus, if the Project does not result in any of the circumstances listed in Section 15162 (i.e., no new or substantially greater significant impacts), the City may properly adopt an addendum to the Certified EIR.

Conclusion: TOP EIR, certified by City Council on January 27, 2010, was prepared as a Program EIR in accordance with CEQA, the State CEQA Guidelines, and the City's Rules for the Implementation of CEQA and in accordance with Section 15121(a) of the State CEQA Guidelines (California Code of Regulations, Title 14, Division 6, Chapter 3). TOP EIR considered the direct physical changes and reasonably foreseeable indirect physical changes in the environment that would be caused by The Ontario Plan. Consequently, TOP EIR focused on impacts from changes to land use associated with buildout of the City's Land Use Plan, within the Policy Plan, and impacts from the resulting population and employment growth in the City. The proposed California Commerce Center Specific Plan land use Amendment from Commercial/Food/Hotel to Light Industrial will bring the project site in conformance with The Ontario Plan Policy Plan (General Plan) Industrial (0.55 FAR) land use designation. As described on page 4, the amount of development anticipated at buildout will be consistent with the Certified EIR. Subsequent activities within TOP Program EIR have been evaluated to determine whether an additional CEQA document needs to be prepared.

Accordingly, and based on the findings and information contained in the Certified EIR, the analysis above, the attached Initial Study, and CEQA statute and State CEQA Guidelines, including Sections 15164 and 15162, the Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary, nor is there a need for any additional mitigation measures; therefore, pursuant to State CEQA Guidelines Section 15164, the Ontario City Council hereby adopts this Addendum to the Certified EIR.

Other public agencies whose approval is required (e.g., permits, financing approval or participation agreement): None

Tribal Consultation: Have California Native American tribes traditionally and culturally affiliated with the project area requested consultation pursuant to Public Resources Code section 21080.3.1? Yes No

If "yes," has consultation begun? Yes No Completed

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

- | | | |
|--|---|---|
| <input type="checkbox"/> Aesthetics | <input type="checkbox"/> Agriculture/Forestry Resources | <input type="checkbox"/> Air Quality |
| <input type="checkbox"/> Biological Resources | <input type="checkbox"/> Cultural Resources | <input type="checkbox"/> Geology / Soils |
| <input type="checkbox"/> Greenhouse Gas Emissions | <input type="checkbox"/> Hazards & Hazardous Materials | <input type="checkbox"/> Hydrology / Water Quality |
| <input type="checkbox"/> Land Use / Planning | <input type="checkbox"/> Mineral Resources | <input type="checkbox"/> Noise |
| <input type="checkbox"/> Population / Housing | <input type="checkbox"/> Public Services | <input type="checkbox"/> Recreation |
| <input type="checkbox"/> Transportation | <input type="checkbox"/> Utilities / Service Systems | <input type="checkbox"/> Mandatory Findings of Significance |
| <input type="checkbox"/> Tribal Cultural Resources | <input type="checkbox"/> Wildfire | <input type="checkbox"/> Energy |

DETERMINATION (To be completed by the Lead Agency)

On the basis of this initial evaluation:

- I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.

- I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
- I find that the proposed project MAY have a "potentially significant" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.


Signature

Jeanie Irene Aguilo, Associate Planner
Printed Name and Title

May 10, 2021
Date

City of Ontario – Planning Department
For

EVALUATION OF ENVIRONMENTAL IMPACTS

1) A brief explanation is required for all answers except "No Impact" answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g. the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general standards (e.g. the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).

2) All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.

3) Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. "Potentially Significant Impact" is appropriate if there is substantial evidence that an effect is significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.

4) "Negative Declaration: Less Than Significant with Mitigation Incorporated" applies where the incorporation of mitigation measures has reduced an effect from "Potentially Significant Impact" to a "Less than Significant Impact." The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from the "Earlier Analyses" Section may be cross-referenced).

5) Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or Negative Declaration. Section 15063(c)(3)(D). In this case, a brief discussion should identify the following:

a) Earlier Analyses Used. Identify and state where they are available for review.

b) Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.

c) Mitigation Measures. For effects that are "Less than Significant with Mitigation Measures Incorporated," describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.

6) Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g. general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.

7) Supporting Information Sources. A source list should be attached, and other sources used, or individuals contacted should be cited in the discussion.

8) This is only a suggested form, and lead agencies are free to use different formats; however, lead agencies should normally address the questions from this checklist that are relevant to a project's environmental effects in whatever format is selected.

9) The explanation of each issue should identify:

- a) The significance criteria or threshold, if any, used to evaluate each question; and
- b) The mitigation measure identified, if any, to reduce the impact to less than significance.

<i>Issues</i>	<i>Potentially Significant Impact</i>	<i>Less Than Significant with Mitigation</i>	<i>Less Than Significant Impact</i>	<i>Impacts Previously Analyzed in TOP EIR</i>
1. AESTHETICS. Would the project:				
a. Have a substantial adverse effect on a scenic vista?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. AGRICULTURE AND FOREST RESOURCES. In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest protocols adopted by the California Air Resources Board. Would the project:				

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in the loss of forest land or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3. AIR QUALITY. Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:				
a. Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in other emissions (such as those leading to odors adversely affecting a substantial number of people)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4. BIOLOGICAL RESOURCES. Would the project:				
a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
c. Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
5. CULTURAL RESOURCES. Would the project:				
a. Cause a substantial adverse change in the significance of a historical resource pursuant to Section 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Disturb any human remains, including those interred outside of dedicated cemeteries?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
6. ENERGY. Would the project:				
a. Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
7. GEOLOGY AND SOILS. Would the project:				
a. Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury or death involving:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii. Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii. Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv. Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
b. Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Be located on expansive soil, as defined in Table 18 1 B of the Uniform Building Code, creating substantial direct or indirect risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of waste water?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
8. GREENHOUSE GAS EMISSIONS. Would the project:				
a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emission of greenhouse gases?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
9. HAZARDS AND HAZARDOUS MATERIALS. Would the project:				
a. Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard or excessive noise for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
f. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g. Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
10. HYDROLOGY AND WATER QUALITY. Would the project:				
a. Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i. result in substantial erosion or siltation on- or off-site;	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii. substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or offsite;	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii. create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff; or	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv. impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
11. LAND USE AND PLANNING. Would the project:				
a. Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
12. MINERAL RESOURCES. Would the project:				
a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
13. NOISE. Would the project result in:				
a. Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
14. POPULATION AND HOUSING. Would the project:				
a. Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of road or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
15. PUBLIC SERVICES. Would the project:				
a. Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:				
i. Fire protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii. Police protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii. Schools?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv. Parks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
v. Other public facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
16. RECREATION. Would the project:				
a. Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Does the project include recreational facilities or require the construction or expansion of recreational facilities which have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
17. TRANSPORTATION. Would the project:				
a. Conflict with a program plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Would the project conflict or be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
18. TRIBAL CULTURAL RESOURCES. Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is				
a. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
19. UTILITIES AND SERVICE SYSTEMS. Would the project:				
a. Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Generate solid waste in excess of State or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
e. Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
20. WILDFIRES. If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:				
a. Substantially impair an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
21. MANDATORY FINDINGS OF SIGNIFICANCE.				
a. Does the project have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current project, and the effects of probable future projects.)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>Note: Authority cited: Public Resources Code sections 21083, 21083.05, 21083.09.</p> <p>Reference: Gov. Code section 65088.4; Public Resources Code sections 21073, 21074, 21080(c), 21080.1, 21080.3, 21080.3.1, 21080.3.2, 21082.3, 21083, 21083.3, 21083.5, 21084.2, 21084.3, 21093, 21094, 21095 and 21151; <i>Sundstrom v. County of Mendocino</i> (1988) 202 Cal.App.3d 296; <i>Leonoff v. Monterey Board of Supervisors</i> (1990) 222 Cal.App.3d 1337; <i>Eureka Citizens for Responsible Govt. v. City of Eureka</i> (2007) 147 Cal.App.4th 357; <i>Protect the Historic Amador Waterways v. Amador Water Agency</i> (2004) 116 Cal.App.4th 1099, 1109; <i>San Franciscans Upholding the Downtown Plan v. City and County of San Francisco</i> (2002) 102 Cal.App.4th 656.</p>				

EXPLANATION OF ISSUES

1. AESTHETICS. Would the project:

a. Have a substantial adverse effect on a scenic vista?

Discussion of Effects: The Policy Plan (General Plan) does not identify scenic vistas within the City. However, the Policy Plan (Policy CD1-5) requires all major north-south streets be designed and redeveloped to feature views of the San Gabriel Mountains. The project site is located at the northeast corner of Haven Avenue and Airport Drive, a major north-south principal arterial street and east-west arterial street, respectfully, as identified in the Functional Roadway Classification Plan (Figure M-2) of the Mobility Element within the Policy Plan. The proposed Amendment to California Commerce Center Specific Plan to change the land use designations for 10.64 acres of land, from Commercial/Food/Hotel to Light Industrial, will not result in adverse environmental impacts with regard to views of the San Gabriel Mountains. Therefore, no adverse impacts are anticipated in relation to the project.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings and historic buildings within a state scenic highway?

Discussion of Effects: The City of Ontario is served by three freeways: I-10, I-15, and SR-60. I-10 and SR-60 traverse the northern and central portion of the City, respectively, in an east to west direction. I-15 traverses the northeastern portion of the City in a north-south direction. These segments of I-10, I-15, and SR-60 have not been officially designated as scenic highways by the California Department of Transportation. In addition, there are no historic buildings, or any scenic resources identified on or in the vicinity of the project site. Therefore, it will not result in adverse environmental impacts.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Substantially degrade the existing visual character or quality of the site and its surroundings?

Discussion of Effects: The project would not degrade the existing visual character or quality of the site or its surroundings. The project site is located in an area that is characterized by Industrial and commercial development and is surrounded by urban land uses.

The proposed project will substantially improve the visual quality of the area through development of the site with an Industrial Warehouse/Distribution facility, which will be consistent with the policies of the Community Design Element of the Policy Plan (General Plan), as well as with the Industrial development in the surrounding area. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analyses are necessary.

d. Create a new source of substantial light or glare that would adversely affect day or nighttime views in the area?

Discussion of Effects: The proposed land use change itself will not cause lighting to be installed in the Project. New lighting will be introduced to the site with the development of a proposed 200,291-square foot industrial building. Pursuant to the requirements of the City's Development Code, on-site lighting will be shielded, diffused or indirect, to avoid glare to pedestrians or motorists. In addition, lighting fixtures will be selected and located to confine the area of illumination to within the project site and minimize light spillage.

Site lighting plans will be subject to review by the Planning Department and Police Department prior to issuance of building permits (pursuant to the City's Building Security Ordinance). Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

2. AGRICULTURE AND FOREST RESOURCES. In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board. Would the project:

a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?

Discussion of Effects: The site is presently developed as a parking lot and does not contain any agricultural uses. Further, the site is identified as Urban and Built-up Land on the map prepared by the California Resources Agency, pursuant to the Farmland Mapping and Monitoring Program. As a result, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Conflict with existing zoning for agricultural use, or a Williamson Act contract?

Discussion of Effects: The project site is not zoned for agricultural use. The proposed Development Plan to construct a 200,291-square foot industrial building is consistent with the Light Industrial development standards of California Commerce Specific Plan. Furthermore, there are no Williamson Act contracts in effect on the subject site. Therefore, no impacts to agricultural uses are anticipated, nor will there be any conflict with Williamson Act contracts.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?

Discussion of Effects: The project proposes an Amendment to California Commerce Center Specific Plan changing the land use designation on 10.64 acres of land, from Commercial/Food/Hotel to Light Industrial. This would not result in the rezoning of forest land, timberland, or timberland zoned Timberland Production because such land use designations do not exist within the City of Ontario. Therefore, no impacts to forest or timberland are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Result in the loss of forest land or conversion of forest land to non-forest use?

Discussion of Effects: There is currently no land in the City of Ontario that qualifies as forest land as defined in Public Resources Code section 12220(g). Neither TOP nor the City's Zoning Code provide designations for forest land. Consequently, the proposed project would not result in the loss or conversion of forest land.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified EIR analyses are necessary.

e. Involve other changes in the existing environment, which, due to their location or nature, could individually or cumulatively result in loss of Farmland to non-agricultural use or conversion of forest land to non-forest use?

Discussion of Effects: The project proposes an Amendment to California Commerce Center Specific Plan changing the land use designation on 10.64 acres of land, from Commercial/Food/Hotel to Light Industrial, therefore is not designated as Farmland. The project site is presently developed as a parking lot and there are no agricultural uses occurring onsite. As a result, to the extent that the project would result in changes to the existing environment those changes would not result in loss of Farmland to non-agricultural use.

Additionally, there is currently no land in the City of Ontario that qualifies as forest land as defined in Public Resources Code Section 12220(g). Neither TOP nor the City's Zoning Code provide designations for forest land. Consequently, to the extent that the proposed project would result in changes to the existing environment, those changes would not impact forest land.

Mitigation Required: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified EIR analyses are necessary.

3. AIR QUALITY. Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:

a. Conflict with or obstruct implementation of the applicable air quality plan?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as Industrial and is surrounded on the north by commercial development, industrial development to the east, and vacant land to the west and south. The project will not conflict with or obstruct implementation of any air quality plan. As noted in the Certified EIR (Section 5.3), pollutant levels in the Ontario area already exceed Federal and State standards. To reduce pollutant levels, the City of Ontario is actively participating in efforts to enhance air quality by implementing Control Measures in the Air Quality Management Plan for local jurisdictions within the South Coast Air Basin.

The proposed project is consistent with The Ontario Plan, for which the EIR was prepared and impacts evaluated. Furthermore, the project is consistent with the City's participation in the Air Quality Management Plan and, because of the project's limited size and scope, will not conflict with or obstruct implementation of the plan. Mitigation (Mitigation Measure 5.3-2) has been adopted by the City that requires fugitive dust control measures pursuant to SCAQMD's Rule 403, use of Tier 3 construction equipment, proper service and maintenance of construction equipment, limiting nonessential idling of construction equipment, and use of Super-Compliant VOC paints for coating and architectural surfaces. Any future development proposals on the project site will be required to comply with Mitigation Measure 5.3-2. No new impacts beyond those identified in the Certified EIR would result from Project implementation.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?

Discussion of Effects: The project will not result in a cumulatively considerable net increase of any criteria pollutant for which the region is in non-attainment under an applicable federal or state ambient air quality because the Project will provide land use consistency with the Official Land Use Map (Exhibit LU-01) of the Policy Plan component of TOP. The proposed California Commerce Center Specific Plan land use Amendment from Commercial/Food/Hotel to Light Industrial will bring the project site in conformance with the Industrial land use designation of TOP. Mitigation (Mitigation Measure 5.3-1) has already been adopted by the City that would facilitate continued City cooperation with the SCAQMD and SCAG to achieve regional air quality improvement goals, promote energy conservation design and development techniques, encourage alternative modes of transportation, and implement transportation demand strategies. The project will comply with the air quality standards of the Certified EIR and the SCAQMD resulting in impacts that are less than significant [please refer to Sections 3(a) and 3(b)].

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Expose sensitive receptors to substantial pollutant concentrations?

Discussion of Effects: Sensitive receptors are defined as populations that are more susceptible to the effects of pollution than the population at large. The SCAQMD identifies the following as sensitive receptors: long-term health care facilities, rehabilitation centers, convalescent centers, retirement homes, residences, schools, playgrounds, child care centers, and athletic facilities. According to the SCAQMD, projects have the potential to create significant impacts if they are located within one-quarter mile of sensitive receptors and would emit toxic air contaminants identified in SCAQMD Rule 1401.

The project will not expose sensitive receptors to any increase in pollutant concentrations because there are no sensitive receptors located within close proximity of the project site. Further, there is limited potential for sensitive receptors to be located within close proximity of the site because the project site will be zoned Light Industrial within the California Commerce Center Specific Plan at the time of project approval. The types of uses that would potentially impact sensitive receptors would not be supported on the property pursuant to the Land Use Element (Figure LU-6) of the Policy Plan (General Plan) and zoning designations on the property. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?

Discussion of Effects: The subject site was previously analyzed by TOP EIR as Industrial and is surrounded on the north by commercial development, industrial development to the east, and vacant land to the west and south. The uses proposed on the subject site, as well as those permitted within the proposed Light Industrial land use district of the California Commerce Center Specific Plan, do not create objectionable odors. Further, the project shall comply with the policies of the Ontario Municipal Code and the Policy Plan (General Plan). Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified EIR analyses are necessary.

4. BIOLOGICAL RESOURCES. Would the project:

a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as Industrial and is surrounded on the north by commercial development, industrial development to the east, and vacant land to the west and south. The project site is located within an area that has not been identified as containing species identified as a candidate, sensitive, or special status species in local or regional plans, policies or regulations or by the California Department of Fish and Game or the U.S. Fish and Wildlife Service. Therefore, no adverse impacts are anticipated beyond those previously identified in the TOP EIR are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified analyses are necessary.

b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?

Discussion of Effects: The site does not contain any riparian habitat or other sensitive natural community identified by the Department of Fish & Game or Fish & Wildlife Service. Therefore, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?

Discussion of Effects: No wetland habitat is present on site. Therefore, project implementation would have no impact on these resources.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?

Discussion of Effects: The site is presently developed as a parking lot that is surrounded on the north by commercial development, industrial development to the east, and vacant land to the west and south. As a result, there are no wildlife corridors connecting this site to other areas. Therefore, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?

Discussion of Effects: The City of Ontario does not have any ordinances protecting biological resources. Further, the site does not contain any mature trees necessitating the need for preservation. As a result, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

f. Conflict with the provisions of an adopted Habitat Conservation Plan (HCP), Natural Community Conservation Plan (NCCP), or other approved local, regional, or state habitat conservation plan?

Discussion of Effects: The site is not part of an adopted HCP, NCCP or other approved habitat conservation plan. As a result, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

5. CULTURAL RESOURCES. Would the project:

a. Cause a substantial adverse change in the significance of a historical resource pursuant to Section 15064.5?

Discussion of Effects: The subject site was previously analyzed by TOP EIR as Industrial and is surrounded by commercial development to the north, industrial development to the east, and vacant land to the west and south. The subject site is presently developed as a parking lot and does not contain any buildings or structures constructed more than 50 years ago and cannot be considered for eligibility for listing in the California Register of Historic Resources. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5?

Discussion of Effects: The Certified EIR (Section 5.5) indicates no archeological sites or resources have been recorded in the City with the Archeological Information Center at San Bernardino County Museum. Figure 5.5-2 of the Certified EIR shows that the Project site has not been surveyed for archeological resources. While no adverse impacts to archeological resources are anticipated at this site due to its urbanized nature, standard conditions have been imposed on the project that in the event of unanticipated archeological discoveries, construction activities will not continue or will be moved to other parts of the project site and a qualified archaeologist shall be contacted to determine significance of these resources. If the find is discovered to be historical or unique archaeological resources, as defined in Section 15064.5 of the CEQA Guidelines, avoidance or other appropriate measures shall be implemented.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Disturb any human remains, including those interred outside of formal cemeteries?

Discussion of Effects: The proposed project is in an area that has been previously disturbed by human activity. No known religious or sacred sites exist within the Project area. Thus, human remains are not expected to be encountered during any construction activities. However, in the unlikely event that human remains are discovered, existing regulations, including the California Public Resources Code Section 5097.98, would afford protection for human remains discovered during development activities. Furthermore, standard conditions have been imposed on the project that in the event of unanticipated discoveries of human remains are identified during excavation, construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and/or Native American consultation has been completed, if deemed applicable.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

6. ENERGY Would the project:

a. Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?

b. Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?

Discussion of Effects: Energy was not analyzed in the Certified TOP EIR but has been included as part of the 2019 revisions to the State CEQA Guidelines. Implementation of the Project would not substantially increase the demand for electricity and natural gas at the Project site and gasoline consumption in the region during construction and operation. Implementation of the Project will require compliance with CALGreen Building Code (CCR Title 24, Part 11). Moreover, the Project includes a sample Greenhouse Gas Reduction Measure Screening Table for Commercial and Industrial Development. The Screening Table includes measures energy efficient development, indoor space efficiency measures, building efficiency measures, renewable energy measures, and water conservation measures. Measures that would reduce electricity consumption include, but are not limited to: greatly enhanced window insulation, an enhanced cool-roof, an improved efficiency heating, ventilation, and air conditioning ("HVAC") system, blower doors HERS verified Envelope leakage or equivalent, enhanced duct insulation, Energy Star commercial appliances, water efficient landscaping and irrigation systems, and water-efficient toilets and faucets.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts. No changes or additions to the Certified EIR analyses are necessary.

7. GEOLOGY & SOILS. Would the project:

a. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury or death involving:

i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.

Discussion of Effects: There are no active faults known on the site and the project site is located outside the Fault Rapture Hazard Zone (formerly Alquist-Priolo Zone). The Certified EIR (Section 5.7/Figure 5.7-2) identifies eight active or potentially active fault zones near the City. Given that the closest fault zone is located more than ten miles from the project site, fault rupture within the project area is not likely. All development will comply with the Uniform Building Code seismic design standards to reduce geologic hazard susceptibility. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

ii. Strong seismic ground shaking?

Discussion of Effects: There are no active faults known on the site and the project site is located outside the Fault Rapture Hazard Zone (formerly Alquist-Priolo Zone). The Certified EIR (Section 5.7/Figure 5.7-2) identifies eight active or potentially active fault zones near the City. The closest fault zone is located more than ten miles from the project site. The proximity of the site to the active faults will result in ground shaking during moderate to severe seismic events. All construction will comply with the California Building Code, the Ontario Municipal Code, The Ontario Plan and all other ordinances adopted by the City related to construction and safety. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

iii. Seismic-related ground failure, including liquefaction?

Discussion of Effects: As identified in the Certified EIR (Section 5.7), groundwater saturation of sediments is required for earthquake induced liquefaction. In general, groundwater depths shallower than 10 feet to the surface can cause the highest liquefaction susceptibility. Depth to ground water at the project site during the winter months is estimated to be between 250 to 450 feet below ground surface. Therefore, the liquefaction potential within the project area is minimal. Implementation of The Ontario Plan strategies, Uniform Building Code and Ontario Municipal code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

iv. Landslides?

Discussion of Effects: The project would not expose people or structures to potential adverse effects, including the risk of loss, injury, or death involving landslides because the relatively flat topography of the project site (less than 2 percent slope across the City) makes the chance of landslides remote. Implementation of The Ontario Plan strategies, Uniform Building Code and Ontario Municipal Code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Result in substantial soil erosion or the loss of topsoil?

Discussion of Effects: The project will not result in significant soil erosion or loss of topsoil because of the previously disturbed and developed nature of the project site and the limited size and scope of the

project. Grading increases the potential for erosion by removing protective vegetation, changing natural drainage patterns, and constructing slopes. However, compliance with the California Building Code and review of grading plans by the City Engineer will ensure no significant impacts will occur. In addition, the City requires an erosion/dust control plan for projects located within this area. Implementation of a NPDES program, the Environmental Resource Element of the Policy Plan (General Plan) strategies, Uniform Building Code and Ontario Municipal code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?

Discussion of Effects: The project would not result in the location of development on a geologic unit or soil that is unstable, or that would become unstable because as previously discussed, the potential for liquefaction and landslides associated with the project is less than significant. The Ontario Plan FEIR (Section 5.7) indicates that subsidence is generally associated with large decreases or withdrawals of water from the aquifer. The project would not withdraw water from the existing aquifer. Further, implementation of The Ontario Plan strategies, Uniform Building Code and Ontario Municipal code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?

Discussion of Effects: The majority of Ontario, including the project site, is located on alluvial and eolian soil deposits. These types of soils are not considered to be expansive. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

e. Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?

Discussion of Effects: The area is served by the local sewer system and the use of alternative systems is not necessary. There will be no impact to the sewage system.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

f. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?

Discussion of Effects: The City of Ontario is underlain by deposits of Quaternary and Upper-Pleistocene sediments deposited during the Pliocene and early Pleistocene time, Quaternary Older Alluvial sediments may contain significant, nonrenewable, paleontological resources and are, therefore, considered to have high sensitivity at depths of 10 feet or more below ground surface. In addition, the Certified TOP EIR (Section 5.5) indicates that one paleontological resource has been discovered in the City. While no adverse impacts are anticipated, standard conditions have been imposed on the Project that in the event of unanticipated paleontological resources are identified during excavation, construction activities will not continue or will be moved to other parts of the Project site and a qualified paleontologist shall be contacted to determine significance of these resources. If the find is determined to be significant, avoidance or other appropriate measures shall be implemented.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

8. GREENHOUSE GAS EMISSIONS. Would the project:

a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial use. Additionally, the impact of buildout of The Ontario Plan on the environment due to the emission of greenhouse gases ("GHGs") was analyzed in the Certified EIR. According to the EIR, this impact would be significant and unavoidable (Re-circulated Portions of the Ontario Plan Draft Environmental Impact Report, p. 2-118.) This EIR was certified by the City on January 27, 2010, at which time a statement of overriding considerations was also adopted for The Ontario Plan's significant and unavoidable impacts, including that concerning the emission of greenhouse gases. The proposed Amendment to California Commerce Center Specific Plan to change the land use designations for 10.64 acres of land, from Commercial/Food/Hotel to Light Industrial will not create significantly greater impacts than were identified in the Certified EIR. The Project includes a sample GHG Reduction Measures Screening Threshold Table, which provides guidance in measuring the reduction of greenhouse gas ("GHG") emissions attributable to certain design and construction measures incorporated into development projects. The analysis, methodology, and significance determination (thresholds) are based upon the City's Climate Action Plan ("CAP"), which includes GHG emission inventories (2008 and 2020 forecasts), a year 2020 emission reduction target, the goals and policies to reach the target, together with the Addendum prepared for the CAP. The Screening Table assigns points for each option incorporated into a project as mitigation or a project design feature (collectively referred to as "feature"). The point values correspond to the minimum emissions reduction expected from each feature. The menu of features allows maximum flexibility and options for how development projects can implement the GHG reduction measures. The point levels are based upon improvements compared to 2008 emission levels of efficiency. Projects that garner at least 100 points will be consistent with the reduction quantities anticipated in the City's CAP. As such, those projects that garner a total of 100 points or greater would not require quantification of project specific GHG emissions. Consistent with CEQA Guidelines, such projects would be determined to have a less than significant individual and cumulative impact for GHG emissions. As shown in the Project GHG Reduction Measures Screening Table, the Project garners a total of 116 points, and is therefore consistent with the reduction quantities anticipated in the City's CAP. Therefore, quantification of Project-specific GHG emissions is not required.

Additionally, pursuant to Public Resources Code Section 21083.3, this impact need not be analyzed further, because (1) the proposed project would result in an impact that was previously analyzed in the Certified EIR, which was certified by the City; (2) the proposed project would not result in any greenhouse gas impacts that were not addressed in the Certified EIR; (3) the proposed project is consistent with The Ontario Plan. The proposed impacts of the project were already analyzed in the Certified EIR and the project will be built to current energy efficient standards. Potential impacts of project implementation will be less than significant with mitigation already required under the Certified EIR and current energy efficiency standards. No changes or additions to the Certified EIR analyses are necessary.

Mitigation Required: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to TOP EIR analyses are necessary. The mitigation measures adopted as part of TOP EIR adequately address any potential significant impacts and there is no need for any additional mitigation measures. The City has reviewed the emission reduction measures and concepts in The Ontario Plan EIR's MM 6-2 and 6-3, and has determined that the following actions apply and shall be undertaken by the applicant in connection with the project: energy efficient design, efficient irrigation systems, electric vehicle charging stations, and compliance with Title 24 of the California Code of Regulations.

b. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial use. The proposed Project is consistent with The Ontario Plan Goal ER 4 of improving air quality by, among

other things, implementation of Policy ER4-3, regarding the reduction of greenhouse gas emissions in accordance with regional, State, and federal regulations. In addition, the proposed Project is consistent with the policies outlined in Section 5.6.4 of the Environmental Impact Report for The Ontario Plan, which aims to reduce the City's contribution of greenhouse gas emissions at build-out by fifteen (15 percent), because the project is upholding the applicable City's adopted mitigation measures as represented in 6-1 through 6-6 and energy efficient design, efficient irrigation systems, electric vehicle charging stations, and compliance with Title 24 of the California Code of Regulations. The Project is consistent with the City's Climate Action Plan. Therefore, the proposed Project does not conflict with an applicable plan, policy or regulation adopted for the purpose of reducing emissions of greenhouse gases.

Mitigation Required: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

9. HAZARDS & HAZARDOUS MATERIALS. Would the project:

a. Create a significant hazard to the public or the environment through the routine transport, use or disposal of hazardous materials?

Discussion of Effects: The subject site was previously analyzed by TOP EIR as an industrial use. The project is not anticipated to involve the transport, use or disposal of hazardous materials during either construction or project implementation. Therefore, no adverse impacts are anticipated. However, in the unlikely event of an accident, implementation of the strategies included in The Ontario Plan will decrease the potential for health and safety risks from hazardous materials to a less than significant impact.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial use. The proposed project does not include the use of hazardous materials or volatile fuels. In addition, there are no known stationary commercial or industrial land uses within close proximity to the subject site, which use/store hazardous materials to the extent that they would pose a significant hazard to visitors/occupants to the subject site, in the event of an upset condition resulting in the release of a hazardous material.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances or waste within one-quarter mile of an existing or proposed school?

Discussion of Effects: The proposed project does not include the use, emissions or handling of hazardous or acutely hazardous materials, substances, or waste. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?

Discussion of Effects: The proposed project site is not listed on the hazardous materials site compiled pursuant to Government Code Section 65962.5. Therefore, the project would not create a hazard to the public or the environment and no impact is anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

e. For a project located within the safety zone of the airport land use compatibility plan for ONT or Chino Airports, would the project result in a safety hazard for people residing or working in the project area?

Discussion of Effects: The Project was reviewed and found to be located within the Airport Influence Area of Ontario International Airport (“ONT”) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (“ALUCP”) for ONT. The Project will not result in a safety hazard for people working or residing in the project area because it will not obstruct aircraft maneuvering because of the project’s low elevation and the architectural style of the project. Additionally, the Land Use Compatibility Guidelines for Noise Impacts (Table LU-08) shows the proposed use as normally accepted in the 65 CNEL. The proposed use will comply with standards for mitigating noise. Therefore, the project will not result in any new or substantially different hazards and hazardous materials impacts than were previously addressed in The Ontario Plan EIR, and no changes or additions to The Ontario Plan EIR analyses are necessary.

The entire City is located within the Airport Influence Area (AIA) of ONT and the location of the Safety Impact Zones are reflected in Policy Map 2-2 of the ONT ALUCP. The project site is located outside the ONT Safety Zones. The project site is also located outside of the Chino Airport Influence Area. The Project is consistent with the policies and criteria of the ONT ALUCP, and therefore, would not result in a safety hazard for people residing or working in the project area. Consequently, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

f. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?

Discussion of Effects: The City's Safety Element, as contained within The Ontario Plan, includes policies and procedures to be administered in the event of a disaster. The Ontario Plan seeks interdepartmental and inter-jurisdictional coordination and collaboration to be prepared for, respond to and recover from every day and disaster emergencies. In addition, the project will comply with the requirements of the Ontario Fire Department and all City requirements for fire and other emergency access. Because the project is required to comply with all applicable City codes, any impacts would be reduced to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

g. Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?

Discussion of Effects: The project site is not located in or near wildlands. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

10. HYDROLOGY & WATER QUALITY. Would the project:

a. Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality?

Discussion of Effects: The project site is served by City water and sewer service and will not affect water quality standards or waste discharge requirements. Discharge of storm water pollutants from areas of materials storage, vehicle or equipment fueling, vehicle or equipment maintenance (including washing, waste handling, hazardous materials handling or storage, delivery areas or loading docks, or other outdoor work) areas could result in a temporary increase in the amount of suspended solids, trash and debris, oil and grease, organic compounds, pesticides, nutrients, heavy metals and bacteria pathogens in surface flows during a concurrent storm event, thus resulting in surface water quality impacts. The site is required to comply with the statewide National Pollutant Discharge Elimination System (“NPDES”) General Industrial

Activities Stormwater Permit, the San Bernardino County Area-Wide Urban Runoff Permit (MS4 permit) and the City of Ontario's Municipal Code (Title 6, Chapter 6 (Stormwater Drainage System)). This would reduce any impacts to below a level of significance. Furthermore, any future applicant to develop the site would be required to submit a Preliminary Water Quality Management Plan ("PWQMP"), which would establish the site's compliance with storm water discharge and water quality management requirements. The PWQMP will include site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development ("LID") best management practices ("BMPs"), such as retention and infiltration, biotreatment and evapotranspiration. The PWQMP would include the use of an underground stormwater infiltration system for the site. Any overflow drainage from future development of the site will be conveyed to the public street by way of parkway culverts.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial use. No increases in the current amount of water flow to the project site are anticipated, and the proposed project will not deplete groundwater supplies, nor will it interfere with recharge. The water use associated with the proposed use of the property was included in the Certified EIR analysis. The development of the site will require the grading of the site and excavation is expected to be less than three feet and would not affect the existing aquifer, estimated to be about 230 to 250 feet below the ground surface. No adverse impacts are anticipated.

Mitigation: No additional mitigation required.

c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:

i. Result in substantial erosion or siltation on- or off-site?

Discussion of Effects: It is not anticipated that the Project would alter the drainage pattern of the site or area, in a manner that would result in erosion, siltation or flooding on-or-off site, nor will the proposed Project increase the erosion of the subject site or surrounding areas. The existing drainage pattern of the site will not be altered, and it will have no significant impact on downstream hydrology. Stormwater generated by the Project will be discharged in compliance with the statewide NPDES General Construction Activities Stormwater Permit and San Bernardino County MS4 permit requirements. With the full implementation of a Storm Water Pollution Prevention Plan developed in compliance with the General Construction Activities Permit requirements, the Best Management Practices included in the SWPPP, and a stormwater monitoring program would reduce any impacts to below a level of significance. No streams or streambeds are present on the site. No changes in erosion off-site are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

ii. Substantially increase the rate or amount of surface runoff water in a manner which would result in flooding on- or off-site?

Discussion of Effects: The proposed Project is not anticipated to increase the flow velocity or volume of storm water runoff to cause environmental harm from the site and will not create a burden on existing infrastructure. Furthermore, with the implementation of an approved Water Quality Management Plan developed for the site, in compliance with the San Bernardino County MS4 Permit requirements, stormwater runoff volume shall be reduced to below a level of significance.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

iii. Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?

Discussion of Effects: It is not anticipated that the project would create or contribute runoff water that would exceed the capacity of existing or planned stormwater drainage systems or create or contribute stormwater runoff pollutants during construction and/or post-construction activity. The stormwater flows will enter an existing storm pipe in Jurupa Street. Pursuant to the requirements of The Ontario Plan, the City's Development Code, and the San Bernardino County MS4 Permit's "Water Quality Management Plan" ("WQMP"), individual developments must provide site drainage and WQMP plans according to guidelines established by the City's Engineering Department. If master drainage facilities are not in place at the time of project development, then standard engineering practices for controlling post-development runoff may be required, which could include the construction of on-site storm water detention and/or retention/infiltration facilities. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

iv. Impede or redirect flood flows?

Discussion of Effects: Urbanization in the areas surrounding the project site have resulted in increased responsiveness of the basin to rainfall. The increase in impervious surfaces such as roofs, roads, and parking lots has resulted in a decrease in groundwater infiltration and larger storm surges. The project site is not impacted by offsite flows. The project site is not located in a FEMA Firm Panel designated Flood Zone Risk, and according to the United States Fish and Wildlife Service National Wetlands Inventory ("NWI") no wetlands exist on the property. The Project could lead to the conversion of permeable surfaces to impermeable surfaces such as parking areas and building foundation areas. Any future development on the Project site would discharge onsite flows into an existing storm drain facility. As such, the proposed project would not impede or redirect flood flows. With adherence to existing federal, state, and local regulation no changes to the existing flood flows would occur.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?

Discussion of Effects: Impacts associated with flooding are primarily related to the construction or placement of structures in areas prone to flooding including within an unprotected 100-year flood zone, and in areas susceptible to high tides, tsunamis, seiches, mudflows or sea level rise. Specifically, structures placed in flood prone areas, if flooded, would be damaged, and could subject people to injury or death. The National Flood Insurance Act of 1968 requires the identification of floodplain areas and establishment of flood-risk zones within those areas. FEMA administers the programs and coordinates with communities to establish effective floodplain management standards. According to FEMA, the Project is not located in a known floodplain. Furthermore, this area is not known to flood and is not typically subjected to flooding. The Project site is not located in a floodplain as shown in Figure S-2 of TOP. The Project site is in an urbanized area that is developed with commercial and industrial buildings. No wetlands have been mapped on the project site according to the NWI. According to the FEMA, the Project is not located in an area that is subject to flood hazard, tsunami, or seiche zones. The project site is located over 60 miles east of the Pacific Ocean and is not located in a mapped tsunami zone. Therefore, the project would not have a significant risk of flood hazard, tsunami, seiche zones, release of pollutants due to project inundation.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

e. Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?

Discussion of Effects: The Regional Water Quality Control Board's Basin Plan is designed to preserve and enhance water quality and protect the beneficial uses of all regional waters. Specifically, the

Basin Plan (i) designates beneficial uses for surface and ground waters, (ii) sets narrative and numerical objectives that must be attained or maintained to protect the designated beneficial uses and conform to the state's anti-degradation policy, and (iii) describes implementation programs to protect all waters in the region. Development allowed by the Project would be required to adhere to requirements of the water quality control plan, including all existing regulation and permitting requirements. This would include the incorporation of best management practices ("BMPs") to protect water quality during construction and operational periods. Development of the Project would be subject to all existing water quality regulations and programs, as described in the regulatory section above, including all applicable construction permits. Existing General Plan policies related to water quality would also be applicable to the Project. Implementation of these policies, in conjunction with compliance with existing regulatory programs, would ensure that water quality impacts related to the Project would be less than significant.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

11. LAND USE & PLANNING. Would the project:

a. Physically divide an established community?

Discussion of Effects: The project site is located in an area that is currently developed with urban land uses. This project will be of similar design and size to surrounding developments. No adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial use. The proposed project does not interfere with any policies for environmental protection. As such, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

12. MINERAL RESOURCES. Would the project:

a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?

Discussion of Effects: The project site is located within a mostly developed area surrounded by urban land uses. There are no known mineral resources in the area; therefore, the project will not result in any new or substantially different mineral resources impacts than were previously addressed in The Ontario Plan EIR, and no changes or additions to The Ontario Plan EIR analyses are necessary.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?

Discussion of Effects: There are no known mineral resources in the area. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

13. NOISE. Would the project result in:

a. Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR an industrial use and the Project will not expose people to or generate noise levels in excess of standards as established in The Ontario Plan FEIR (Section 5.12); therefore, the project will not result in any new or substantially different noise impacts than were previously addressed in The Ontario Plan EIR, and no changes or additions to The Ontario Plan EIR analyses are necessary.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Generation of excessive groundborne vibration or groundborne noise levels?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial use and the uses associated with this project normally do not induce groundborne vibrations. As such, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. For a project located within the vicinity of a private airstrip or the noise impact zones of the airport land use compatibility plan for ONT and Chino Airports, would the project expose people residing or working in the project area to excessive noise levels?

Discussion of Effects: The proposed Amendment was reviewed and found to be located within the Airport Influence Area of Ontario International Airport (“ONT”) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (“ALUCP”) for ONT. According to the Safety Element, the project is located within the 60-70CNEL noise contour; therefore, the project will not result in any new or substantially different noise impacts than were previously addressed in The Ontario Plan EIR, and no changes or additions to The Ontario Plan EIR analyses are necessary. or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified TOP EIR analyses are necessary.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

14. POPULATION & HOUSING. Would the project:

a. Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of road or other infrastructure)?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR an industrial use and is consistent with General Plan land use designations and would not induce significant population growth. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?

Discussion of Effects: The project site does not contain existing housing. The project site is presently developed as a parking lot. Implementation of the project will result in the development of an industrial building; therefore no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

15. PUBLIC SERVICES. Would the project:

a. Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:

i. Fire protection?

Discussion of Effects: The site is in a developed area currently served by the Ontario Fire Department. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

ii. Police protection?

Discussion of Effects: The site is in a developed area, currently served by the Ontario Police Department. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

iii. Schools?

Discussion of Effects: Upon development, the Project will be required to pay school fees as prescribed by state law prior to the issuance of building permits. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

iv. Parks?

Discussion of Effects: The site is in a developed area, currently served by the City of Ontario. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

v. Other public facilities?

Discussion of Effects: The site is in a developed area, currently served by the City of Ontario. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

16. RECREATION. Would the project:

a. Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?

Discussion of Effects: This project is not proposing any new housing or large employment generator that would cause an increase in the use of neighborhood parks or other recreational facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Does the project include recreational facilities or require the construction or expansion of recreational facilities that have an adverse physical effect on the environment?

Discussion of Effects: This project is not proposing any new housing or large employment generator that would require the construction of neighborhood parks or other recreational facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

17. TRANSPORTATION. Would the project:

a. Conflict with a program plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an Industrial land use and is in an area that is mostly developed with all street improvements existing. A focused Traffic Analysis was prepared for the project site that compared the trip generation between the existing California Commerce Center Specific Plan Commercial/Food/Hotel land use designation (shopping center/commercial) versus the proposed Light Industrial land use at an FAR of 0.55 (warehouse/small ancillary office). The Traffic Analysis concluded that proposed change in land use from Commercial/Food/Hotel to Light Industrial would result in a net difference of 7,183 fewer Average Daily Trips (ADT) trips, including 146 fewer AM peak hour trips, and 719 fewer PM peak hour trips.

Therefore, the traffic impacts will be consistent with and less than the traffic impacts projected and analyzed under the Certified EIR. The project will not create a substantial increase in the number of vehicle trips, traffic volume or congestion at intersections. Less than significant impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Conflict or be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)?

Discussion of Effects: CEQA Guidelines Section 15064.3 subdivision (b) has been included in the 2018 CEQA Guidelines as part of the implementation of SB 743 which requires local jurisdictions to use Vehicle Miles Travelled (VMT) instead of Level of Service (LOS) methodologies for the purpose of determining the significance of traffic impacts under CEQA. Also, as part of the implementation of SB 743 local jurisdiction were given until July 1, 2020 to develop and implement thresholds of significance criteria and methodologies for evaluating VMT under the new SB 743 requirements. The City of Ontario has adopted and established a VMT analysis threshold or analysis methodology based on our Policy Plan (General Plan) baseline. However, the project was submitted prior to the adoption of the threshold and therefore not subject to the adopted thresholds. Subsequently, The Ontario Plan EIR analyzed VMT, as part of the GHG analysis. The Ontario Plan (TOP) is consistent with the RTP/SCS for the Southern California region. The SBTAM model has incorporated TOP buildout which was then incorporated into the SCAG model in developing the RTP/SCS for the region. The thresholds used in these models can be found in the tool created for SBCTA that analyzes the various threshold options. TOP to establish VMT thresholds since this option has already been found to be consistent with the RTP/SCS and these land use assumptions have been incorporated into the SBTAM and SCAG's regional models. The screening tool created for use in San Bernardino County can be utilized for locations within Ontario where additional

analysis is not required, and the City thresholds be used for projects where additional analysis is required. If mitigation measures are included for the project and the VMT brought down below the established threshold (City average), then the project can be determined to have less than a significant impact on transportation (in terms of CEQA).

Subsequently, a focused Traffic Analysis was prepared for the project site that compared the trip generation between the existing California Commerce Center Specific Plan Commercial/Food/Hotel land use designation (shopping center/commercial) versus the proposed Light Industrial land use at a FAR of 0.55 (warehouse/small ancillary office). The Traffic Analysis concluded that proposed change in land use from Commercial/Food/Hotel to Light Industrial would result in a net difference of 7,183 fewer Average Daily Trips (ADT) trips, including 146 fewer AM peak hour trips, and 719 fewer PM peak hour trips. Therefore, impacts with respect to CEQA Guidelines Section 15064.3(b) are less than significant.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?

Discussion of Effects: The Project is in an area that is mostly developed. All street improvements are complete, and the Development Plan (File No. PDEV20-008) will be required to construct right-of-way improvements along the project frontages. The Project will, therefore, not create a substantial increase in hazards due to a design feature. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Result in inadequate emergency access?

Discussion of Effects: Development of the Project will be designed to provide access for all emergency vehicles and will therefore not create an inadequate emergency access. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

e. Result in inadequate parking capacity?

Discussion of Effects: The project is required to meet parking standards established by the Ontario Development Code and will therefore not create an inadequate parking capacity. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

18. TRIBAL CULTURAL RESOURCES. Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:

a. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k)?

Discussion of Effects: The subject site was previously analyzed by the TOP FEIR as an industrial use and is not listed in the California Register of Historic Resources or local register of historical resources. Development of the site will not create greater impacts than were identified in the Certified TOP FEIR.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.

Discussion of Effects: The subject site is not listed in the California Register of Historic Resources. No consultation had been initiated and no impacts are anticipated through Project implementation.

Mitigation: No new mitigation measures are required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

19. UTILITIES AND SERVICE SYSTEMS. Would the project:

a. Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?

Discussion of Effects: The proposed project is served by the City of Ontario sewer system, which has waste treated by the Inland Empire Utilities Agency at the RP-1 treatment plant. The project is required to meet the requirements of the Ontario Engineering Department regarding wastewater. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years? In making this determination, the City shall consider whether the project is subject to the water supply assessment requirements of Water Code Section 10910, et seq. (SB 610), and the requirements of Government Code Section 664737 (SB 221).

Discussion of Effects: The Project site is served by the City of Ontario water system. There is currently sufficient water supply available to the City of Ontario to serve this Project as per the findings of TOP EIR. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Result in a determination by the wastewater treatment provider that serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?

Discussion of Effects: The Project site is served by the City of Ontario water system. There is currently sufficient water supply available to the City of Ontario to serve this Project as per the findings of Certified TOP EIR. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Generate solid waste in excess of state or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?

Discussion of Effects: City of Ontario serves the Project site. Currently, the City of Ontario contracts with a waste disposal company that transports trash to a landfill with sufficient capacity to handle the City's solid waste disposal needs. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to analyses are necessary.

e. Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?

Discussion of Effects: This Project complies with federal, state, and local statutes and regulations regarding solid waste. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

20. WILDFIRE. If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:

a. Substantially impair an adopted emergency response plan or emergency evacuation plan?

Discussion of Effects: The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?

Discussion of Effects: The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?

Discussion of Effects: The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?

Discussion of Effects: The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

21. MANDATORY FINDINGS OF SIGNIFICANCE.

a. Does the project have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?

Discussion of Effects: The proposed Project does not have the potential to reduce wildlife habitat and threaten a wildlife species; therefore, no environmental impacts resulting from the Project are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Does the project have the potential to achieve short-term environmental goals to the disadvantage of long-term environmental goals?

Discussion of Effects: The Project does not have the potential to achieve short-term environmental goals to the disadvantage of long-term environmental goals.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current project, and the effects of probable future projects.)

Discussion of Effects: The Project does not have impacts that are cumulatively considerable.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Does the project have environmental effects that will cause substantial adverse effects on human beings, either directly or indirectly?

Discussion of Effects: The Project does not have environmental effects that will cause substantial adverse effects on human beings, either directly or indirectly.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

EARLIER ANALYSES

(Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, one or more effects have been adequately analyzed in an earlier EIR or Negative Declaration. Section 15063(c)(3)(D)):

- 1) **Earlier Analyses Used.** Identify earlier analyses used and state where they are available for review.
 - a) The Ontario Plan Final EIR
 - b) The Ontario Plan
 - c) California Commerce Center Specific Plan (File No. 2591-SP) Adopted May 17, 1983
 - d) California Commerce Center Specific Plan EIR No. 81-4 Certified May 17, 1983
 - e) City of Ontario Official Zoning Map
 - f) City of Ontario Development Code
 - g) Ontario International Airport Land Use Compatibility Plan
 - h) Ontario International Airport Land Use Compatibility Plan Negative Declaration (SCH 2011011081)
 - i) TJW Engineering, Inc. – 3555 E. Airport Drive Focused Traffic Impact Analysis – August 27, 2020, included as Attachment B, Traffic Impact Analysis, of this Addendum)

All documents listed above are on file with the City of Ontario Planning Department, 303 East "B" Street, Ontario, California 91764, (909) 395-2036.

2) Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of, and adequately analyzed in, an earlier document pursuant to applicable legal standards.

MITIGATION MEASURES

(For effects that are "Less than Significant with Mitigation Incorporated," describe the mitigation measures, which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.)

The Mitigation Measures contained in the Certified EIR adequately mitigate the impacts of the proposed Project. These mitigation measures are contained in the Mitigation Monitoring Program, included as Attachment A of this Addendum.

No additional mitigation beyond that previously imposed is required.

Exhibit A: PSPA20-003 EXISTING – CALIFORNIA COMMERCE CENTER SPECIFIC PLAN LAND USE PLAN

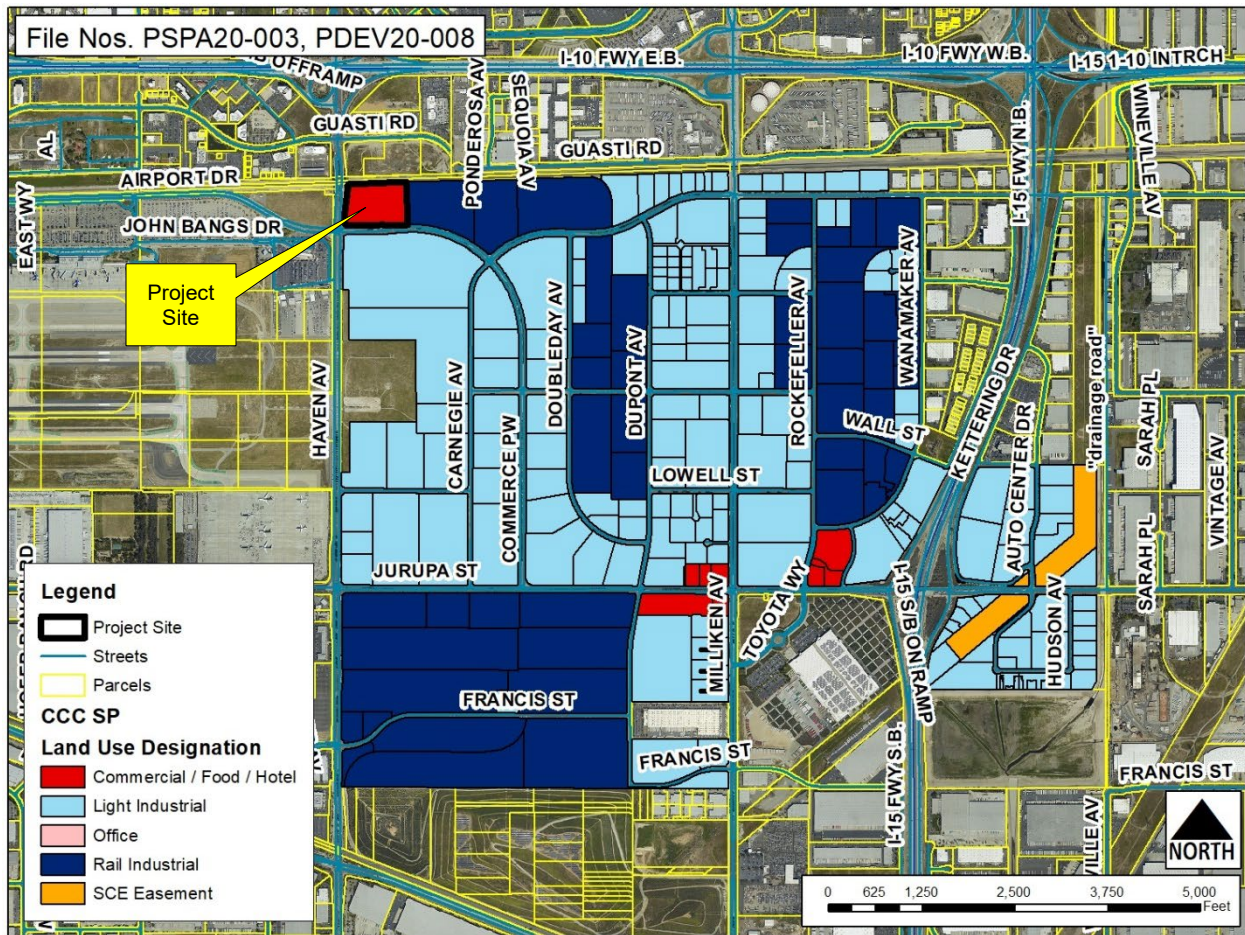
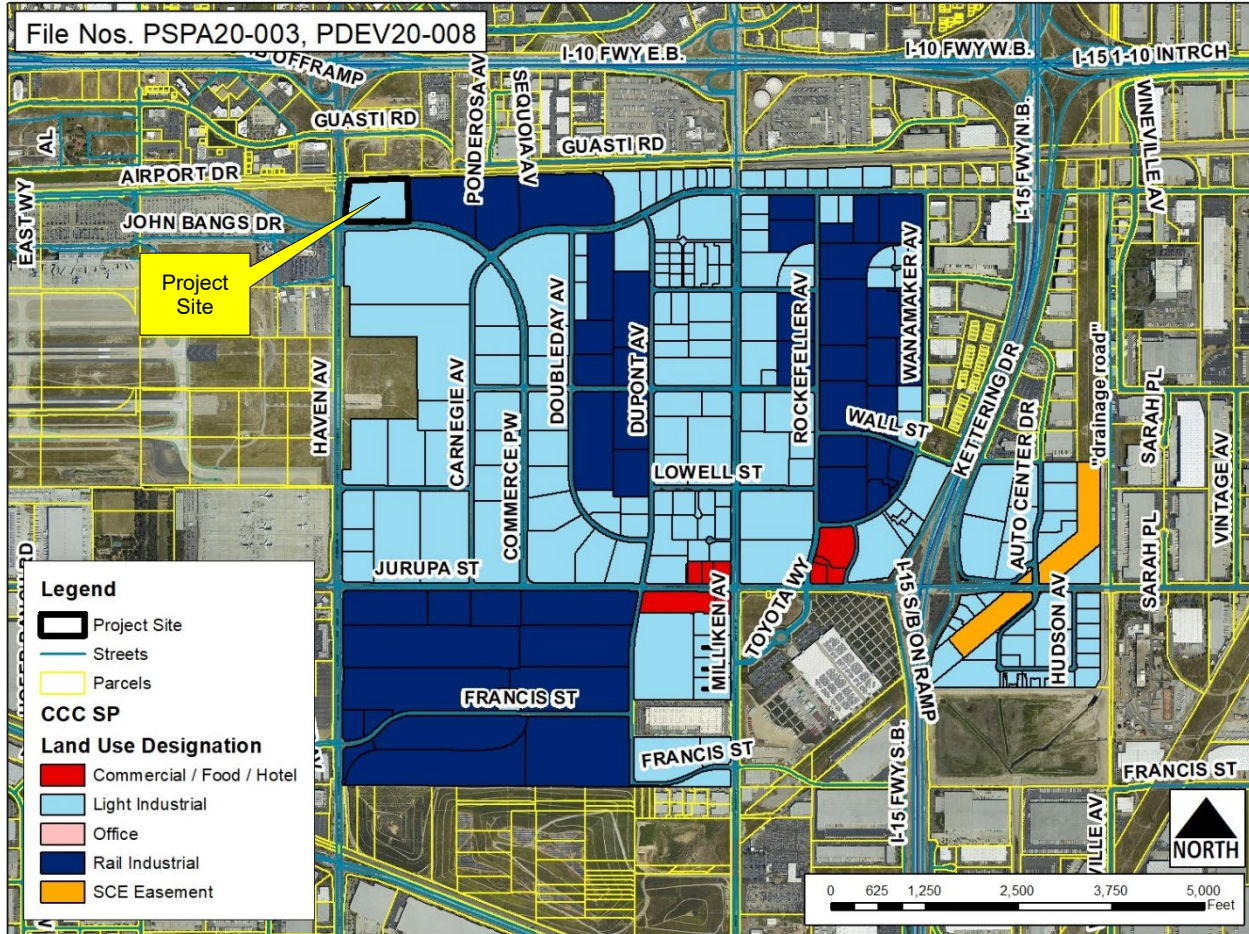


Exhibit B: PSPA20-003 PROPOSED – CALIFORNIA COMMERCE CENTER SPECIFIC PLAN LAND USE PLAN



Attachment A: **Mitigation Monitoring Program for** **The Ontario Plan Environmental Impact Report**

1. Introduction

1.1 PURPOSE OF MITIGATION MONITORING PROGRAM

This Mitigation Monitoring Program has been developed to provide a vehicle by which to monitor mitigation measures and conditions of approval outlined in The Ontario Plan Environmental Impact Report (EIR), State Clearinghouse No. 2008101140. The Mitigation Monitoring Program has been prepared in conformance with Section 21081.6 of the Public Resources Code and City of Ontario Monitoring Requirements. Section 21081.6 states:

(a) When making the findings required by paragraph (1) of subdivision subsection (a) of Section 21081 or when adopting a mitigated negative declaration pursuant to paragraph (2) of subdivision (c) of Section 21080, the following requirements shall apply:

(1) The public agency shall adopt a reporting or monitoring program for the changes made to the project or conditions of project approval, adopted in order to mitigate or avoid significant effects on the environment. The reporting or monitoring program shall be designed to ensure compliance during project implementation. For those changes which have been required or incorporated into the project at the request of a responsible agency or a public agency having jurisdiction by law over natural resources affected by the project, that agency shall, if so requested by the lead agency or a responsible agency, prepare and submit a proposed reporting or monitoring program.

(2) The lead agency shall specify the location and custodian of the documents or other material which constitute the record of proceedings upon which its decision is based.

(b) A public agency shall provide that measures to mitigate or avoid significant effects on the environment are fully enforceable through permit conditions, agreements, or other measures. Conditions of project approval may be set forth in referenced documents which address required mitigation measures or, in the case of the adoption of a plan, policy, regulation, or other public project, by incorporating the mitigation measures into the plan, policy, regulation, or project design.

(c) Prior to the close of the public review period for a draft environmental impact report or mitigated negative declaration, a responsible agency, or a public agency having jurisdiction over natural resources affected by the project, shall either submit to the lead agency complete and detailed performance objectives for mitigation measures which would address the significant effects on the environment identified by the responsible agency or agency having jurisdiction over natural resources affected by the project, or refer the lead agency to appropriate, readily available guidelines or reference documents. Any mitigation measures submitted to a lead agency by a responsible agency or an agency having jurisdiction over natural resources affected by the project shall be limited to measures which mitigate impacts to resources which are subject to the statutory authority of, and definitions applicable to, that agency. Compliance or noncompliance by a responsible agency or agency having jurisdiction over natural resources affected by a project with that requirement shall not limit the authority of the responsible agency or agency having jurisdiction over natural resources affected by a project, or the authority of the lead agency, to approve, condition, or deny projects as provided by this division or any other provision of law.

1.2 PROJECT SUMMARY

The proposed project is the preparation of The Ontario Plan, which consists of a Vision, Governance Manual, Policy Plan, City Council Priorities, Implementation Plans, and Tracking and Feedback. The Ontario Plan integrates components of city governance documents into a single guidance system that shapes the community 20 years or more into the future.

(a) The Ontario **Vision** describes the future community of Ontario. Its basic purpose is to improve the quality of life for the people of Ontario. It is the rationale and motivation for everything the City does.

(b) The **Governance Manual** describes the foundation for conducting the public's business on behalf of the present and future people of Ontario. It explains how The Ontario Plan is a tool for decision-making and communication.

(c) **City Council Priorities** define the short-term direction in City actions and initiatives. They are the primary means for exercising leadership in carrying out The Plan and realizing the Vision.

(d) The **Policy Plan** connects intent with action through the broad range of Goals and Policies that would guide the long-term growth and development required for the City to achieve its Vision. It also satisfies the California Government Code requirement for a general plan. Figure 3-6, *Proposed Land Use Plan*, shows the proposed General Plan land use designations that guide and regulate land use patterns, distributions, densities and intensities in the City of Ontario, including residential employment, retail, recreation, and public uses.

(e) **Implementation** consists of actions taken to carry out Plan policies. This includes initiatives by the City and decisions on public and private development projects.

(f) **Tracking and Feedback** allows the City to learn from experience and redirect efforts.

Pursuant to CEQA Guidelines Section 15064(d), the EIR considers the direct physical changes and reasonably foreseeable indirect physical changes in the environment that would be caused by The Ontario Plan. Consequently, the EIR focuses on impacts from changes to land use associated with buildout of the Proposed Land Use Plan, within the Policy Plan, and impacts from the resultant population and employment growth in the City. The Ontario Plan Proposed Land Use Plan for the ultimate development of the City is not linked to a timeline. However, for the purpose of this environmental analysis, buildout of the Proposed Land Use Plan is forecast for the year 2035.

1.3 PROJECT LOCATION

The City of Ontario is in the southwestern corner of San Bernardino County and is surrounded by the Cities of Chino and Montclair, and unincorporated areas of San Bernardino County to the west; the Cities of Upland and Rancho Cucamonga to the north; the City of Fontana and unincorporated land in San Bernardino County to the east; and unincorporated Riverside County land to the south. The City is in the central part of the Upper Santa Ana River Valley. This portion of the valley is bounded by the San Gabriel Mountains to the north; the Chino Hills, Puente Hills, and San Jose Hills to the west; the Santa Ana River to the south; and Lytle Creek Wash on the east.

The City comprises approximately 50 square miles (31,958 acres), which includes the 8,200-acre New Model Colony (NMC) in the southern portion of the City (formerly the City's Sphere of Influence). The northern urbanized portion of the City is known as the Original Model Colony (OMC). The City is generally bounded by Benson Avenue and Euclid Avenue on the west; Interstate 10 (I-10), 8th Street, and 4th Street on the north; Etiwanda Avenue and Hamner Avenue on the east; and Merrill Avenue and the San Bernardino County/Riverside County boundary on the south. Regional circulation to and through the City is provided by I-10 and State Route 60 (SR-60) east-west, and by I-15 and SR-83 (Euclid Avenue) north-south.

1.4 ENVIRONMENTAL IMPACTS

The environmental document for this project is a “program EIR” as defined by State CEQA Guidelines (Section 15161, California Code of Regulations, Title 14, Division 6, Chapter 3). As provided in Section 15168 of the State CEQA Guidelines, a Program EIR may be prepared on a series of actions that may be characterized as one large project that are related either 1) geographically; 2) as logical parts of a chain of contemplated events; 3) in connection with issuance of rules, regulations, plans, or other general criteria to govern the conduct of a continuing program; or 4) as individual activities carried out under the same authorizing statutory or regulatory authority and have generally similar environmental effects that can be mitigated in similar ways.

Although the legally required contents of a Program EIR are the same as those of a Project EIR, Program EIRs are typically more conceptual and may contain a more general discussion of impacts, alternatives, and mitigation measures than a Project EIR. Once a Program EIR has been prepared, subsequent activities within the program must be evaluated to determine whether an additional CEQA document needs to be prepared. However, if the Program EIR addresses the program’s effects as specifically and comprehensively as possible, many subsequent activities could be found to be within the Program EIR scope and additional environmental documents may not be required (Guidelines Section 15168[c]). When a Program EIR is relied on for a subsequent activity, the lead agency must incorporate feasible mitigation measures and alternatives developed in the Program EIR into the subsequent activities (Guidelines Section 15168[c][1]). If a later activity would have effects that were not examined in the Program EIR, a new Initial Study would need to be prepared leading to either an EIR or a Negative Declaration. Pursuant to Sections 15126.2 and 15126.4 of the State CEQA Guidelines, the EIR should identify any potentially significant adverse impacts and recommend mitigation that would reduce or eliminate these impacts to levels of insignificance.

1.4.1 Impacts Considered Less Than Significant

Ten environmental categories are identified as having less than significant impacts that do not require mitigation. These categories are:

- Aesthetics
- Biological Resources
- Geology/Soils
- Hazards & Hazardous Materials
- Hydrology/Water Quality
- Land Use & Planning
- Mineral Resources
- Population and Housing
- Public Services
- Recreation

1.4.2 Potentially Significant Adverse Impacts That Can Be Mitigated, Avoided, or Substantially Lessened

The following have been identified as potentially resulting in significant adverse impacts that can be mitigated, avoided, or substantially lessened:

- Cultural Resources: Mitigation Measures 5-2 through 5-4 would reduce archeological and prehistoric cultural resource impacts to less than significant.
- Noise: Mitigation Measure 12-3 would ensure that any new vibration-sensitive structures near the Union Pacific Railroad or Southern California Regional Rail Authority right-of-way would be constructed so that train-related vibration would not be perceptible and operational vibration impacts would be less than significant.
- Utilities and Service Systems: Mitigation Measures 17-1 through 17-4 would reduce impacts on water supply and demand from buildout of The Ontario Plan to less than significant.

1.4.3 Unavoidable Significant Adverse Impacts

There are six environmental categories considered to have impacts that would be significant and unavoidable and would not be lessened through mitigation.

Agricultural Resources

Buildout of The Ontario Plan would convert 3,269.3 acres of California Resource Agency designated Prime Farmland, Unique Farmland, and Farmland of Statewide Importance to residential, commercial, mixed-use, and industrial land uses. Consequently, impacts to Farmland would remain significant and unavoidable.

There are a number of Williamson Act contracts within the City that have yet to expire. Buildout of The Ontario Plan would most likely require the cancellation or nonrenewal of these contracts. The current use of these contracts would slow the rate of conversion from agricultural to nonagricultural land, but it would not impede the conversion. Since there are some Williamson Act contracts still active in the New Model Colony, implementation of the proposed land use plan for The Ontario Plan would conflict with these contracts and cause a significant impact. Consequently, impacts to Williamson Act contracts would remain significant and unavoidable.

Development of the City in accordance with The Ontario Plan would increase the amount of nonagricultural land uses. When nonagricultural land uses are placed near agricultural uses, the odors, noises, and other hazards related to agriculture conflict with the activities and the quality of life of the people living and working in the surrounding areas. Consequently, conversion of agricultural uses in the city may cause farms and agricultural land uses outside the City to be converted to nonagricultural uses because of the nuisances related to agriculture and impacts would remain significant and unavoidable.

Air Quality

The project would not be consistent with the Air Quality Management Plan (AQMP) because air pollutant emissions associated with buildout of the City of Ontario would cumulatively contribute to the nonattainment designations in the South Coast Air Basin (SoCAB). Furthermore, buildout of the Proposed Land Use Plan would exceed current estimates of population, employment, and vehicle miles traveled for Ontario and therefore these emissions are not included in the current regional emissions inventory for the SoCAB. As both criteria must be met in order for a project to be considered consistent with the AQMP, the project would be considered inconsistent with the AQMP. Consequently, this impact would remain significant and unavoidable.

Construction activities associated with buildout of The Ontario Plan would generate short-term emissions that exceed the South Coast Air Quality Management District's (SCAQMD) regional significance thresholds; cumulatively contribute to the SoCAB's nonattainment designations for ozone (O₃), coarse inhalable particulate matter (PM₁₀), and fine inhalable particulate matter (PM_{2.5}); and potentially elevate concentrations of air pollutants at sensitive receptors. Mitigation Measure 3-1 would reduce The Ontario Plan's short-term construction-related volatile organic compounds (VOC), carbon monoxide (CO), oxides of nitrogen (NO_x), PM₁₀, and PM_{2.5} emissions but they would not be reduced to levels below the SCAQMD's regional thresholds and they would not reduce these impacts to less than significant. Consequently, construction air pollutant emissions generated by buildout of The Ontario Plan would remain significant and unavoidable.

Buildout of The Ontario Plan would generate long-term emissions that would exceed SCAQMD'S regional significance thresholds and cumulatively contribute to the SoCAB nonattainment designations for O₃, PM₁₀, and PM_{2.5}. Mitigation Measure 3-2 would reduce long-term operational emissions of VOC, CO, NO_x, PM₁₀, and PM_{2.5} related to the buildout of The Ontario Plan but they would not reduce these emissions to levels below the SCAQMD's regional significance thresholds and impacts would not be less than significant. Consequently, operational impacts from buildout of The Ontario Plan would remain significant and unavoidable.

Approval of residential and other sensitive land uses within 500 feet of Interstate-10, Interstate-15, or State Route-60 would result in exposure of persons to substantial concentrations of diesel particulate matter. Mitigation Measure 3-3 would reduce impacts related to the exposure of sensitive receptors (residential and other sensitive land uses) to diesel particulate matter because of their placement near freeways within the City. However, it would not reduce this impact to be less than significant.

Conversion of agricultural land to nonagricultural uses would temporarily expose residents to objectionable odors and impacts would remain significant and unavoidable.

Cultural Resources

Although protective regulations are in place and preservation policies are included in The Ontario Plan, implementation of the Proposed Land Use Plan, especially within growth focus areas, has the potential to impact Tier III historic resources. Mitigation Measure 5-1 would require a historical evaluation for properties within historic resources in the Focus Areas under the City's ordinance. However, the ordinance does not provide a high level of protection for Tier III resources. As a result, demolition of historical resources categorized under the Ordinance as Tier III could potentially be impacted with implementation of the Proposed Land Use Plan. Consequently, Tier III historic resource impacts would remain significant and unavoidable.

Global Climate Change

Buildout of the City of Ontario would generate greenhouse gas emissions that would significantly contribute to global climate change impacts in California. Greenhouse gas (GHG) emissions generated in the City would significantly contribute to climate change impacts in California as a result of the growth in population and employment in the City and scale of development activity associated with buildout of the Proposed Land Use Plan. Mitigation Measures 6-1 through 6-6 would act to reduce the contributions of The Ontario Plan to global climate change but they would not reduce the impacts to less than significant.

Noise

Buildout of the Proposed Land Use Plan would result in an increase in traffic on local roadways in the City of Ontario, which would substantially increase noise levels. Consequently, impacts would remain significant and unavoidable.

Noise-sensitive uses could be exposed to elevated noise levels from transportation sources. Any siting of new sensitive land uses within a noise environment that exceeds the normally acceptable land use compatibility criterion would result in a potentially significant impact and would require a separate noise study through the development review process to determine the level of impacts and required mitigation. Mitigation Measure 12-1 would decrease the exposure of sensitive receptors to excessive noise levels within 65 dBA CNEL contours, whether near Los Angeles/Ontario International Airport (LAONT) or other noise-producing areas such as freeways and railroads, but it would not reduce these impacts to less than significant.

Construction activities associated with buildout of the individual land uses associated with the Proposed Land Use Plan would expose sensitive uses to strong levels of groundborne vibration. Mitigation Measure 12-2 would reduce the impacts caused by construction-related vibrations on sensitive receptors but it would not reduce the impact to less than significant.

Impact 5.12-5. Significant. Construction activities associated with buildout of the individual land uses associated with the Proposed Land Use Plan would substantially elevate noise levels in the vicinity of sensitive land uses. Mitigation Measure 12-4 calls for the use of noise-reducing techniques during construction projects that would impact nearby sensitive receptors, such as the use of temporary sound walls and reduced unnecessary truck idling. However, these impacts would not be reduced to levels considered less than significant.

Noise-sensitive land uses within the 65 dBA CNEL contour of the Los Angeles/Ontario International Airport would be exposed to substantial levels of airport-related noise. Consequently, impacts would remain significant and unavoidable.

Transportation and Traffic

The increased development and population growth associated with the buildout of the Proposed Land Use Plan would cause deficient levels of service at area intersections without implementation of the recommended lane geometry improvements. In addition, buildout of the Proposed Land Use Plan would also cumulatively contribute to the cumulatively significant freeway level of service impact that is already projected to occur in the future. Mitigation Measure 16-1 would require the buildout of The Ontario Plan to be consistent with the traffic study prepared by Kimley-Horn and Associates. This traffic study indicates the appropriate lane geometry for area intersections. This would allow for intersections to have LOS values of E or above, but it would not improve the cumulative freeway LOS standards to appropriate levels. The City has no jurisdiction over Caltrans projects, such as freeway improvements. Therefore, the impacts related to cumulative LOS deficiencies on freeways would not be reduced to levels considered less than significant.

2. Mitigation Monitoring Process

2.1 MITIGATION MONITORING PROGRAM ORGANIZATION

CEQA requires that a reporting or monitoring program be adopted for the conditions of project approval that are necessary to mitigate or avoid significant effects on the environment (Public Resources Code 21081.6). The mitigation monitoring and reporting program is designed to ensure compliance with adopted mitigation measures during project implementation. For each mitigation measure recommended in the Environmental Impact Report, specifications are made herein that identify the action required and the monitoring that must occur. In addition, a responsible agency is identified for verifying compliance with individual conditions of approval contained in the Mitigation Monitoring Program (MMP). In order to effectively track and document the status of mitigation measures, a mitigation matrix has been prepared and includes:

- Responsibility for implementation
- Timing
- Responsibility for monitoring
- Monitor

Mitigation measure timing of verification has been apportioned into several specific timing increments. Of these, the most common are:

- Prior to project approval
- Prior to issuance of grading permit(s)
- During construction

Information pertaining to compliance with mitigation measures or any necessary modifications or refinements will be documented in the comments portion of the matrix.

2.2 MITIGATION MONITORING PROCEDURES

The City of Ontario Planning Department is the designated lead agency for the Mitigation Monitoring and Reporting Program. The City of Ontario includes the Mitigation Measures within the Special Conditions of Approval. The City is responsible for review of all monitoring reports, enforcement actions, and document disposition. The Planning Department shall designate a Project Mitigation Monitor for the proposed project.

2.2.1 In-Field Monitoring

The Responsible Monitoring Party shall exercise caution and professional practices at all times when monitoring construction. Protective wear (hard hats, glasses, etc.) shall be worn at all times in construction areas. Injuries shall be reported immediately to the Project Mitigation Monitor.

2.2.2 Coordination with Contractors

The construction manager/superintendent is responsible for coordination of contractors and for contractor completion of required measures in accordance with the provisions of this program.

2.2.3 Recognized Experts

The use of recognized experts as a component of the monitoring team is required to ensure compliance with scientific and engineering mitigation measures. While the recognized experts assess

compliance with required mitigation measures, consultation with the City of Ontario planning staff shall take place in the event of a dispute.

2.2.4 Enforcement

Agencies may enforce conditions of approval through their existing police power, using stop-work orders, fines, infraction citations, loss of entitlements, refusal to issue building permits or certificates of use and occupancy or, in some cases, notice of violation for tax purposes. Criminal misdemeanor sanctions could be available where the agency has adopted an ordinance requiring compliance with the monitoring program, similar to the provision in many zoning ordinances that affirm the enforcement power to bring suit against violators of the ordinances.

3. Mitigation Monitoring Requirements

3.1 CATEGORIZED MITIGATION MEASURES/MATRIX

Project-specific mitigation measures have been categorized in matrix format, as shown in Table 3-1. The matrix identifies the environmental factor, specific mitigation measures, schedule, and responsible monitor. The mitigation matrix will serve as the basis for scheduling the implementation of, and compliance with, all mitigation measures.

3.2 IN-FIELD MONITORING

Project monitors and technical subconsultants shall exercise caution and professional practices at all times when monitoring implementation of mitigation measures. Protective wear (e.g., hard hat, glasses) shall be worn at all times in construction areas. Injuries shall be immediately reported to the mitigation monitoring committee.

3.3 DATABASE MANAGEMENT

All mitigation monitoring reports, letters, and memos shall be prepared using Microsoft Word software on IBM-compatible PCs and processed according to the City's Environmental Compliance Program.

3.4 COORDINATION WITH CONTRACTORS

The construction manager is responsible for coordination of contractors and for contractor completion of required mitigation measures.

3.5 LONG-TERM MONITORING

Long-term monitoring related to several mitigation measures will be required, including fire safety inspections. Post-construction fire inspections are conducted on a routine basis by the Ontario Fire Department.

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
5.3 AIR QUALITY				
<p>3-1 The City of Ontario Building Department shall require that all new construction projects incorporate feasible mitigation measures to reduce air quality emissions. Potential measures shall be incorporated as conditions of approval for a project and may include:</p> <ul style="list-style-type: none"> • Requiring fugitive dust control measures that exceed South Coast Air Quality Management District's Rule 403, such as: <ul style="list-style-type: none"> ○ Requiring use of nontoxic soil stabilizers to reduce wind erosion. ○ Applying water every four hours to active soil-disturbing activities. ○ Tarping and/or maintaining a minimum of 24 inches of freeboard on trucks hauling dirt, sand, soil, or other loose materials. • Using construction equipment rated by the United States Environmental Protection Agency as having Tier 3 or higher exhaust emission limits. • Ensuring construction equipment is properly serviced and maintained to the manufacturer's standards. • Limiting nonessential idling of construction equipment to no more than five consecutive minutes. • Using Super-Compliant VOC paints for coating of architectural surfaces whenever possible. A list of Super-Compliant architectural coating manufactures can be found on the South Coast Air Quality Management District's website at: http://www.aqmd.gov/prdas/brochures/Super-Compliant_AIM.pdf . 	<p>City of Ontario Building Department in coordination with the landowner/project applicant's construction contractor</p>	<p>During construction</p>	<p>City of Ontario Building and Department and Developer/Contractor</p>	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
3-2 The City of Ontario shall evaluate new development proposals within the City and require all developments to include access or linkages to alternative modes of transportation, such as transit stops, bike paths, and/or pedestrian paths (e.g., sidewalks).	City of Ontario Planning/Engineering Department in coordination with the landowner/project applicant	Prior to individual project approvals	City of Ontario Planning Department	
3-3 The City of Ontario shall evaluate new development proposals within the City for potential incompatibilities with regard to the California Air Resources Board's <i>Air Quality and Land Use Handbook: A Community Health Perspective</i> (April 2005). New development that is inconsistent with the recommended buffer distances shall only be approved if feasible mitigation measures, such as high efficiency Minimum Efficiency Reporting Value filters have been incorporated into the project design to protect future sensitive receptors from harmful concentrations of air pollutants as a result of proximity to existing air pollution sources.	City of Ontario Planning Department in coordination with the landowner/project applicant	Prior to individual project approvals	City of Ontario Planning Department	
5.5 CULTURAL RESOURCES				
5-1 Historic or potentially historic resources in the City shall be evaluated for historic significance through the City's tier system prior to the issuance of plan or development approvals.	City of Ontario Planning Department	Prior to individual project approvals	City of Ontario Planning Department	
5-2 In areas of documented or inferred archaeological and/or paleontological resource presence, City staff shall require applicants for development permits to provide studies to document the presence/absence of such resources. On properties where resources are identified, such studies shall provide a detailed mitigation plan, including a monitoring program and recovery and/or in situ preservation plan, based on the recommendations of a qualified cultural preservation expert. The mitigation plan shall include the following requirements:	City of Ontario Planning Department in coordination with the Landowner/Project Applicant	Prior to individual project approvals	City of Ontario Planning Department	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Archaeologists and/or paleontologist shall be retained for the project and will be on call during grading and other significant ground-disturbing activities. • Should any cultural resources be discovered, no further grading shall occur in the area of the discovery until the Planning Director or designee is satisfied that adequate provisions are in place to protect these resources. • Unanticipated discoveries shall be evaluated for significance by a San Bernardino County Certified Professional Archaeologist/Paleontologist. If significance criteria are met, then the project shall be required to perform data recovery, professional identification, radiocarbon dates, and other special studies; submit materials to a museum for permanent curation; and provide a comprehensive final report including catalog with museum numbers. 				
<p>5-3 Upon receipt of an application for a Specific Plan or a project that requires a General Plan amendment subject to CEQA and is within the City's jurisdiction, the City's representative shall consult with the relevant tribe(s)' representative(s) to determine if the proposed project is within a culturally sensitive area to the tribe. If sufficient evidence is provided to reasonably ascertain that the site is within a [tribal] culturally sensitive area, then a cultural resources assessment prepared by an archaeologist shall be required. The findings of the cultural resources assessment shall be incorporated into the CEQA documentation. A copy of the report shall be forwarded to the tribe(s). If mitigation is recommended in the CEQA document, the procedure described in Mitigation Measure 5-4 shall be followed.</p>	City of Ontario Planning Department	Prior to individual project approvals	City of Ontario Planning Department	
<p>5-4 Prior to the issuance of grading permits for a Specific Plan or project that requires a General Plan amendment for which the CEQA document defines cultural resource mitigation for potential tribal resources, the project applicant</p>	City of Ontario Planning Department in coordination with the	Prior to issuance of grading permit(s)	City of Ontario Planning Department	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
shall contact the designated tribe(s) to notify them of the grading, excavation, and monitoring program. The applicant shall coordinate with the City of Ontario and the tribal representative(s) to develop mitigation measures that address the designation, responsibilities, and participation of tribal monitors during grading, excavation, and ground-disturbing activities; scheduling; terms of compensation; and treatment and final disposition of any cultural resources, sacred sites, and human remains discovered on the site. The City of Ontario shall be the final arbiter of the conditions for projects within the City's jurisdiction.	Landowner/Project Applicant			
5.6 Global Climate Change				
<p>6-1 The City of Ontario shall prepare a Climate Action Plan within 18 months after adopting The Ontario Plan. The goal of the Climate Action Plan shall be to reduce GHG emissions from all activities within the City boundaries to support the State's efforts under AB 32 and to mitigate the impact of climate change on the City, State, and world. Once completed, the City shall update The Ontario Plan and associated policies, as necessary, to be consistent with the Climate Action Plan and prepare a subsequent or supplemental Environmental Impact Report, if new significant impacts are identified. The Climate Action Plan shall include the following:</p> <ul style="list-style-type: none"> • Emission Inventories: The City shall establish GHG emissions inventories including emissions from all sectors within the City, using methods approved by, or consistent with guidance from, the CARB; the City shall update inventories every 3 years or as determined by state standards to incorporate improved methods, better data, and more accurate tools and methods, and to assess progress. If the City is not on-schedule to achieve the GHG reduction targets, additional measured shall be implemented, as identified in the CAP. 	City of Ontario	Within 18 months of adopting The Ontario Plan	City of Ontario Planning Department/ Municipal Utilities Agency (MUA)	

**Table 3-1
 Mitigation Monitoring Requirements**

Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ The City shall establish a baseline inventory of GHG emissions including municipal emissions, and emissions from all business sectors and the community. ○ The City shall define a “business as usual” scenario of municipal, economic, and community activities, and prepare a projected inventory for 2020 based on that scenario. ● Emission Targets: The City will develop Plans to reduce or encourage reductions in GHG emissions from all sectors within the City: <ul style="list-style-type: none"> ○ A Municipal Climate Action Plan which shall include measures to reduce GHG emissions from municipal activities by at least 30 percent by 2020 compared to the "business as usual" municipal emissions (including any reductions required by the California Air Resource Board under AB 32. ○ A Business Climate Action Plan in collaboration with the business community, which shall include measures to reduce GHG emissions from business activities, and which shall seek to reduce emissions by at least 30 percent by 2020 compared to "business as usual" business emissions. ○ A Community Climate Action Plan in collaboration with the stakeholders from the community at large, which shall include measures reduce GHG emissions from community activities, and which shall seek to reduce emissions by at least 30 percent by 2020 compared to "business as usual" community emissions. 				
<p>6-2 The Climate Action Plan shall include specific measures to achieve the GHG emissions reduction targets identified in Mitigation Measure 6-1. The Climate Action Plan shall quantify the approximate greenhouse gas emissions reductions of each measure and measures shall be</p>	<p>City of Ontario Planning Department</p>	<p>Within 18 months of adopting The Ontario Plan</p>	<p>City of Ontario Planning Department</p>	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>enforceable. Measures listed below, along with others, shall be considered during the development of the Climate Action Plan (CAP):</p> <ul style="list-style-type: none"> • Require all new or renovated municipal buildings to seek Silver or higher Leadership in Energy and Environmental Design (LEED) standard, or compliance with similar green building rating criteria. • Require all municipal fleet purchases to be fuel efficient vehicles for their intended use based on the fuel type, design, size, and cost efficiency. • Require that new development projects in Ontario that require demolition prepare a demolition plan to reduce waste by recycling and/or salvaging a nonhazardous construction and demolition debris. • Require that new developments design buildings to be energy efficient by siting buildings to take advantage of shade, prevailing winds, landscaping, and sun screening to reduce energy required for cooling. • Require that cool roofs for non-residential development and cool pavement to be incorporated into the site/building design for new development where appropriate. • Evaluate the feasibility of implementing a Public Transit Fee to support Omnitrans in developing additional transit service in the City. • Require diesel emission reduction strategies to eliminate and/or reduce idling at truck stops, warehouses, and distribution facilities throughout the City. • Install energy efficient lighting and lighting control systems in all municipal buildings. • Require all new traffic lights installed be energy efficient traffic signals. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Require the use of reclaimed water for landscape irrigation in all new development and on public property where such connections are within the service boundaries of the City's reclaimed water system. • Require all new landscaping irrigation systems installed within the City to be automated, high-efficient irrigation systems to reduce water use and require use of bubbler irrigation; low-angle, low-flow spray heads; or moisture sensors. • Conduct energy efficiency audits of existing municipal buildings by checking, repairing, and readjusting heating, ventilation, and air conditioning systems, lighting, water heating equipment, insulation, and weatherization. • Ensure that its local Climate Action, Land Use, Housing, and Transportation Plans are aligned with, support, and enhance any regional plans that have been developed consistent with state guidance to achieve reductions in GHG emissions. • Mitigate climate change by decreasing heat gain from pavement and other hard surfaces associated with infrastructure. • Reduce heat gain from pavement and other similar hardscaping. • Work with appropriate agencies to create an interconnected transportation system that allows a shift in travel from private passenger vehicles to alternative modes, including public transit, ride sharing, car-sharing, bicycling and walking. • Provide safe and convenient access for pedestrians and bicyclists to, across, and along major transit priority streets. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Facilitate employment opportunities that minimize the need for private vehicle trips, by: <ul style="list-style-type: none"> ○ Amending zoning ordinances and the Development Code to include live/work sites and satellite work centers in appropriate locations. ○ Encouraging telecommuting options with new and existing employers, through project review and incentives, as appropriate. • Establish policies and programs to reduce onsite parking demand and promote ride-sharing and public transit at large events. • Support and promote the use of low-and zero-emission vehicles, by: <ul style="list-style-type: none"> ○ Encouraging the necessary infrastructure to facilitate the use of zero- emission vehicles and clean alternative fuels, such as electric vehicle charging facilities and conveniently located alternative fueling stations. ○ Encouraging new construction to include vehicle access to properly wired outdoor receptacles to accommodate ZEV and/or plug in electric hybrids (PHEV). ○ Encouraging transportation fleet standards to achieve the lowest emissions possible, using a mix of alternate fuels, PZEV or better fleet mixes. ○ Establishing incentives, as appropriate, to taxicab owners to use alternative fuel or gas-electric hybrid vehicles. • Establish green building requirements and standards for new development and redevelopment projects, and work to provide incentives for green building practices and remove barriers that impede their use. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Allow increased height limits and/or flexibility in other standards for projects that incorporate energy efficient green building practices where not prohibited by Airport Land Use Compatibility Plan (ALUCP)/Federal Aviation Administration (FAA). • Identify and remove regulatory or procedural barriers to implementing green building practices within its jurisdiction, such as updating codes, guidelines, and zoning, and ensure that all plan review and building inspection staff are trained in green building materials, practices, and techniques. • Support the use of green building practices by: <ul style="list-style-type: none"> ○ Providing information, marketing, training, and technical assistance about green building practices. ○ Adopting a Green Building ordinance with guidelines for green building practices in residential and commercial development. ○ Adopt energy efficiency performance standards for buildings designed to achieve a greater reduction in energy and water use than currently required by state law, including: <ul style="list-style-type: none"> ○ Standards for the installation of "cool roofs". ○ Standards for improved overall efficiency of lighting systems. ○ Requirements for the use of Energy Star appliances and fixtures in discretionary new development. • Encourage the performance of energy audits for residential and commercial buildings prior to completion of sale, and that audit results and information about opportunities for energy efficiency improvements be presented to the buyer. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Establish policies and programs that facilitate the siting of new renewable energy generation. • Require that any building constructed in whole or in part with City funds incorporate passive solar design features, such as daylighting and passive solar heating, where feasible. • Prepare and implement a comprehensive plan to improve energy efficiency of municipal facilities, including: <ul style="list-style-type: none"> ○ Conducting energy audits. ○ Retrofitting municipal facilities for energy efficiency where feasible and when remodeling or replacing components, including increased insulation, installing green or reflective roofs and low-emissive window glass. ○ Implementing an energy tracking and management system for its municipal facilities. ○ Installing energy-efficient exit signs, street signs, and traffic lighting, subject to life/safety considerations. ○ Installing energy-efficient lighting retrofits and occupancy sensors, and institute a "lights out at night" policy, subject to life/safety considerations. ○ Retrofitting heating and cooling systems to optimize efficiency (e.g., replace chillers, boilers, fans, pumps, belts, etc.). ○ Installing Energy Star® appliances and energy-efficient vending machines. ○ Improving water use efficiency, including a schedule to replace or retrofit system components 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>with high- efficiency units (i.e., ultra-low-flow toilets, fixtures, etc.).</p> <ul style="list-style-type: none"> ○ Installing irrigation control systems which maximize water use efficiency and minimize off- peak use. ○ Adopting an accelerated replacement schedule for energy inefficient systems and components. • Ensure that staff receives appropriate training and support to implement objectives and policies to reduce GHG emissions, including: <ul style="list-style-type: none"> ○ Providing energy efficiency training to design, engineering, building operations, and maintenance staff. ○ Providing information on energy use and management, including data from the tracking and management system, to managers and others making decisions that influence energy use. ○ Providing energy design review services to departments undertaking new construction or renovation projects, to facilitate compliance with LEED standards. • Maximize efficiency at drinking water treatment, pumping, and distribution facilities, including development of off-peak demand schedules for heavy commercial and industrial users. • Establish a replacement policy and schedule to replace fleet vehicles and equipment with the most fuel-efficient vehicles practical, including gasoline hybrid and alternative fuel or electric models. • Require the installation of outdoor electrical outlets on buildings to support the use, where practical, of 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>electric lawn and garden equipment, and other tools that would otherwise be run with small gas engines or portable generators.</p> <ul style="list-style-type: none"> • Implement measures to reduce employee vehicle trips and to mitigate emissions impacts from municipal travel. • Conduct a comprehensive inventory and analysis of the urban forest, and coordinate tree maintenance responsibilities with all responsible departments, consistent with best management practices. • Evaluate existing landscaping and options to convert reflective and impervious surfaces to landscaping and will install or replace vegetation with drought-tolerant, low- maintenance native species or edible landscaping that can also provide shade and reduce heat-island effects. • Implement enhanced programs to divert solid waste from landfill operations, by: <ul style="list-style-type: none"> ○ Establishing a diversion target which meets or exceeds AB 939 requirements. ○ Promoting and expanding recycling programs, purchasing policies, and employee education to reduce the amount of waste produced. • Reduce per capita water consumption consistent with state law by 2020. • Establish a water conservation plan that may include such policies and actions as: <ul style="list-style-type: none"> ○ Maintaining and refining the City's tiered rate structure for water use. ○ Establishing restrictions on time of use for landscape watering, or other demand management strategies. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Establishing performance standards for irrigation equipment and water fixtures, consistent with state law. ● Establish programs and policies to increase the use of recycled water, including: <ul style="list-style-type: none"> ○ Promoting the use of recycled water for agricultural, industrial, and irrigation purposes, including grey water systems for residential irrigation. ● Ensure that building standards and permit approval processes promote and support water conservation, by: <ul style="list-style-type: none"> ○ Establishing building design guidelines and criteria to promote water-efficient building design, including minimizing the amount of non-roof impervious surfaces around the building(s). ○ Establishing menus and checklists for developers and contractors to ensure water-efficient infrastructure and technology are used in new construction, including low-flow toilets and shower heads, moisture-sensing irrigation, and other such advances. ● Organize workshops on waste reduction activities for the home or business, such as backyard composting, or office paper recycling, and shall schedule recycling drop-off events and neighborhood chipping/mulching days. ● Organize workshops on steps to increase energy efficiency in the home or business, such as weatherizing the home or building envelope, installing smart lighting systems, and how to conduct a self-audit for energy use and efficiency. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>6-3 The City of Ontario will amend the Municipal Code within 18 months after adopting The Ontario Plan, with provisions implementing the following GHG emission reduction concepts:</p> <ul style="list-style-type: none"> • Increase densities in urban core areas to support public transit, by, among other means: <ul style="list-style-type: none"> ○ Removing barriers to the development of accessory dwelling units in existing residential neighborhoods. • Reduce required road width standards wherever feasible to calm traffic and encourage alternative modes of transportation. • Add bicycle facilities to city streets and public spaces, where feasible. • Promote infill, mixed-use, and higher density development, and provide incentives to support the creation of affordable housing in mixed use zones. • Plan for and create incentives for mixed-use development. • Identify sites suitable for mixed-use development and establish appropriate site- specific standards to accommodate mixed uses which could include: <ul style="list-style-type: none"> ○ Increasing allowable building height or allow height limit bonuses, in appropriate areas and where safe to do so. ○ Allowing flexibility in applying development standards (such as FAR2 and lot coverage) based on the location, type, and size of the units, and the design of the development. ○ Allowing reduced and shared parking based on the use mix, and availability of and proximity to public transit stops. 	City of Ontario Planning Department	Within 18 months of adopting The Ontario Plan	City of Ontario Planning Department	

**Table 3-1
 Mitigation Monitoring Requirements**

Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Allowing for tandem parking, shared parking and off-site parking leases. • Enable prototype mixed-use structures for use in neighborhood center zones that can be adapted to new uses over time with minimal internal remodeling. • Identify and facilitate the inclusion of complementary land uses not already present in local zoning districts, such as supermarkets, parks and recreational fields, schools in neighborhoods, and residential uses in business districts, to reduce the vehicle miles traveled and promote bicycling and walking to these uses. • Revise zoning ordinance(s) to allow local-serving businesses, such as childcare centers, restaurants, banks, family medical offices, drug stores, and other similar services near employment centers to minimize midday vehicle use. • Develop form-based community design standards to be applied to development projects and land use plans, for areas designated mixed-use. • Implement a Housing Overlay Zone for residential properties at transit centers and along transit corridors. This may include average minimum residential densities of 25 units per acre within one quarter miles of transit centers; average minimum densities of 15 units per acre within one quarter mile of transit corridors; and minimum FAR of 0.5:1 for non- residential uses within a quarter mile of transit centers or corridors. • Identify transit centers appropriate for mixed-use development, and promote transit-oriented, mixed-use development within these targeted areas, by: <ul style="list-style-type: none"> ○ Providing maximum parking standards and flexible building height limitations. ○ Providing density bonus programs. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Establishing guidelines for private and public spaces for transit-oriented and mixed-use development. ○ Discouraging auto-oriented development. • Ensure new development is designed to make public transit a viable choice for residents, including: <ul style="list-style-type: none"> ○ Locating medium to high density development near activity centers that can be served efficiently by public transit and alternative transportation modes. ○ Locating medium to high density development near streets served by public transit whenever feasible. ○ Linking neighborhoods to bus stops by continuous sidewalks or pedestrian paths. • Develop form-based community design standards to be applied to development projects and land use plans, for areas designated mixed-use. • Create and preserve distinct, identifiable neighborhoods whose characteristics support pedestrian travel, especially within, but not limited to, mixed-use and transit-oriented development areas, by: <ul style="list-style-type: none"> ○ Designing or maintaining neighborhoods where the neighborhood amenities can be reached in approximately five minutes of walking. ○ Encouraging pedestrian-only streets and/or plazas within developments, and destinations that may be reached conveniently by public transportation, walking, or bicycling. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Allowing flexible parking strategies in neighborhood activity centers to foster a pedestrian-oriented streetscape. ○ Providing continuous sidewalks with shade trees and landscape strips to separate pedestrians from traffic. ○ Encouraging neighborhood parks and recreational centers near concentrations of residential areas (preferably within one quarter mile) and include pedestrian walkways and bicycle paths that encourage non- motorized travel. ● Ensure pedestrian access to activities and services, especially within, but not limited to, mixed-use and transit- oriented development areas, by: <ul style="list-style-type: none"> ○ Ensuring new development that provides pedestrian connections in as many locations as possible to adjacent development, arterial streets, and thoroughfares. ○ Ensuring a balanced mix of housing, workplaces, shopping, recreational opportunities, and institutional uses, including mixed-use structures. ○ Locating schools in neighborhoods, within safe and easy walking distances of residences served. ○ Encouraging new development in which primary entrances are pedestrian entrances, with automobile entrances and parking located to the rear. ○ Supporting development where automobile access to buildings does not impede pedestrian access, by consolidating driveways between buildings or developing alley access. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Utilizing street parking as a buffer between sidewalk pedestrian traffic and the automobile portion of the roadway. ○ Prioritizing the physical development of pedestrian connectors for existing areas that do not meet established connectivity standards. ● Mitigate climate change by decreasing heat gain from pavement and other hard surfaces associated with infrastructure. ● Reduce heat gain from pavement and other similar hardscaping, by: <ul style="list-style-type: none"> ○ Including low-water landscaping in place of hardscaping around transportation infrastructure and in parking areas. ○ Establishing standards that provide for pervious pavement options. ○ Removing obstacles to natural, drought tolerant landscaping and low-water landscaping. ● Coordinate with appropriate agencies to create an interconnected transportation system that allows a shift in travel from private passenger vehicles to alternative modes, including public transit, ride sharing, car-sharing, bicycling and walking, including, but not limited to: <ul style="list-style-type: none"> ○ Providing safe and convenient access for pedestrians and bicyclists to, across, and along major transit priority streets. ● Upgrade and maintain the following transit system infrastructure to enhance public use, including: <ul style="list-style-type: none"> ○ Ensuring transit stops and bus lanes are safe, convenient, clean and efficient. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Ensuring transit stops have clearly marked street-level designation and are accessible. ○ Ensuring transit stops are safe, sheltered, benches are clean, and lighting is adequate. ○ Working with transit providers to place transit stations along transit corridors within mixed-use or transit- oriented development areas at intervals appropriate for the mode of transit. ● Facilitate employment opportunities that minimize the need for private vehicle trips, by: <ul style="list-style-type: none"> ○ Amending zoning ordinances and the Development Code to include live/work sites and satellite work centers in appropriate locations. ○ Encouraging telecommuting options with new and existing employers, through project review and incentives, as appropriate. ● Establish standards for new development and redevelopment projects to support bicycle use, including: <ul style="list-style-type: none"> ○ Amending the Development Code to include standards for pedestrian and bicyclist accommodations, including: <ul style="list-style-type: none"> ➤ Providing access for pedestrians and bicyclist to public transportation through construction of dedicated paths, where feasible. ○ Requiring new development and redevelopment projects to include bicycle facilities, as appropriate with the new land use, including: <ul style="list-style-type: none"> ➤ Where feasible, promote the construction of weatherproof bicycle facilities and at a minimum, provide bicycle racks or covered, secure parking near the building entrances. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Establish a network of multi-use trails to facilitate direct off- street bicycle and pedestrian travel and will provide bike racks along these trails at secure, lighted locations. • Establish policies and programs to reduce onsite parking demand and promote ride-sharing and public transit at large events. • Require new commercial and retail developments to provide prioritized parking for electric vehicles and vehicles using alternative fuels. • Support and promote the use of low-and zero-emission vehicles (NEV), by: <ul style="list-style-type: none"> ○ Encouraging the necessary infrastructure to facilitate the use of zero- emission vehicles and clean alternative fuels, such as electric vehicle charging facilities and conveniently located alternative fueling stations. ○ Encouraging new construction to include vehicle access to properly wired outdoor receptacles to accommodate ZEV and/or plug in electric hybrids (PHEV). ○ Encouraging transportation fleet standards to achieve the lowest emissions possible, using a mix of alternate fuels, PZEV or better fleet mixes. ○ Establishing incentives, as appropriate, to taxicab owners to use alternative fuel or gas-electric hybrid vehicles. • Establish green building requirements and standards for new development and redevelopment projects, and work to provide incentives for green building practices and remove barriers that impede their use. • Allow increased height limits and/or flexibility in other standards for projects that incorporate energy efficient 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>green building practices where not prohibited by ALUCP/FAA.</p> <ul style="list-style-type: none"> • Identify and remove regulatory or procedural barriers to implementing green building practices within its jurisdiction, such as updating codes, guidelines, and zoning, and ensure that all plan review and building inspection staff are trained in green building materials, practices, and techniques. • Support the use of green building practices by: <ul style="list-style-type: none"> ○ Establishing guidelines for green building practices in residential and commercial development. ○ Providing incentives, which may include reduction in development fees, administrative fees, and/or expedited permit processing for projects that use green building practices. • Adopt energy efficiency performance standards for buildings that achieve a greater reduction in energy and water use than otherwise required by current state law, including: <ul style="list-style-type: none"> ○ Standards for the installation of "cool roofs". ○ Standards for improved overall efficiency of lighting systems. ○ Requirements for the use of Energy Star appliances and fixtures in discretionary new development. ○ Requirements for new residential lots and/or structures to be arranged and oriented to maximize effective use of passive solar energy. • Require that affordable housing development incorporate energy efficient design and features to the maximum extent feasible. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Identify possible sites for production of renewable energy (such as solar, wind, small hydro, and biogas). • Identify and remove or otherwise address barriers to renewable energy production, including: <ul style="list-style-type: none"> ○ Reviewing and revising building and development codes, design guidelines, and zoning ordinances to remove renewable energy production barriers. ○ Working with related agencies, such as fire, water, health and others that may have policies or requirements that adversely impact the development or use of renewable energy technologies. ○ Developing protocols for safe storage of renewable and alternative energy products with the potential to leak, ignite or explode, such as biodiesel, hydrogen, and/or compressed air. • Allow renewable energy projects in areas zoned for open space, where consistent with the Land Use element, and other uses and values. • Promote and encourage renewable energy generation, and co-generation projects where feasible and appropriate. • Require that, where feasible, all new buildings be constructed to allow for easy, cost-effective installation of solar energy systems in the future, using such “solar-ready” features as: <ul style="list-style-type: none"> ○ Optimal roof orientation (between 20 to 55 degrees from the horizontal), with sufficient south-sloped roof surface, where such buildings architecture and construction are designed for sloped roofs. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Clear access without obstructions (chimneys, heating and plumbing vents, etc.) on the south sloped roof. ○ Roof framing that will support the addition of solar panels. ○ Installation of electrical conduit to accept solar electric system wiring. ○ Installation of plumbing to support a solar hot water system and provision of space for a solar hot water storage tank. • Require that any building constructed in whole or in part with City funds incorporate passive solar design features, such as daylighting and passive solar heating, where feasible. • Prepare and implement a comprehensive plan to improve energy efficiency of municipal facilities, including: <ul style="list-style-type: none"> ○ Conducting energy audits. ○ Retrofitting municipal facilities for energy efficiency where feasible and when remodeling or replacing components, including increased insulation, installing green or reflective roofs and low-emissive window glass. ○ Implementing an energy tracking and management system for its municipal facilities. ○ Installing energy-efficient exit signs, street signs, and traffic lighting, subject to life/safety considerations. ○ Installing energy-efficient lighting retrofits and occupancy sensors, and institute a "lights out at night" policy, subject to life/safety considerations. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Retrofitting heating and cooling systems to optimize efficiency (e.g., replace chillers, boilers, fans, pumps, belts, etc.). ○ Installing Energy Star® appliances and energy-efficient vending machines. ○ Improving water use efficiency, including a schedule to replace or retrofit system components with high- efficiency units (i.e., ultra-low-flow toilets, fixtures, etc.). ○ Installing irrigation control systems maximizing water use efficiency and minimizing off- peak use. ○ Adopting an accelerated replacement schedule for energy inefficient systems and components. ● Require that any newly constructed, purchased, or leased municipal space meet minimum standards, such as: <ul style="list-style-type: none"> ○ The Energy Star® New Homes Program established by U.S. EPA. ○ The incorporation of passive solar design features in new buildings, including daylighting and passive solar heating. ● Reduce per capita water consumption consistent with state law by 2020. ● Establish a water conservation plan that may include such policies and actions as: <ul style="list-style-type: none"> ○ Maintaining and refining the City's tiered rate structure for water use. ○ Establishing restrictions on time of use for landscape watering, or other demand management strategies. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Establishing performance standards for irrigation equipment and water fixtures, consistent with State Law. • The City will establish programs and policies to increase the use of recycled water, including: <ul style="list-style-type: none"> ○ Promoting the use of recycled water for agricultural, industrial, and irrigation purposes, including grey water systems for residential irrigation. • Ensure that building standards and permit approval processes promote and support water conservation, by: <ul style="list-style-type: none"> ○ Establishing building design guidelines and criteria to promote water-efficient building design, including minimizing the amount of non-roof impervious surfaces around the building(s). ○ Establishing menus and check-lists for developers and contractors to ensure water-efficient infrastructure and technology are used in new construction, including low- flow toilets and shower heads, moisture-sensing irrigation, and other such advances. • Install water-efficient landscapes and irrigation, including: <ul style="list-style-type: none"> ○ Requiring planting drought-tolerant and native species and covering exposed dirt with moisture-retaining mulch or other materials such as decomposed granite. ○ Requiring the installation of water-efficient irrigation systems and devices, including advanced technology such as moisture-sensing irrigation controls. • Promote the planting of shade trees and establish shade tree guidelines and specifications, including: 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Establishing guidelines for tree planting based on the land use (residential, commercial, parking lots, etc.). ○ Establishing guidelines for tree types based on species size, branching patterns, whether deciduous or evergreen, whether roots are invasive, etc. ○ Establishing tree guidelines for placement, including distance from structures, density of planting, and orientation relative to structures and the sun. ● Develop an Urban Forestry Program to consolidate policies and ordinances regarding tree planting, maintenance, and removal, including: <ul style="list-style-type: none"> ○ Establishing guidelines for tree planting, including criteria for selecting deciduous or evergreen trees low-VOC- producing trees, and emphasizing the use of drought- tolerant native trees and vegetation. 				
6-4 Measures listed in Mitigation Measure 6-2 and 6-3 shall be considered by the City while reviewing all new development, as appropriate, between the time of adoption of The Ontario Plan and adoption of the Climate Action Plan (CAP).	City of Ontario Planning Department	Prior to adoption of the Climate Action Plan	City of Ontario Planning Department	
6-5 Pursuant to a goal of overall consistency with the Sustainable Communities Strategies, the City of Ontario shall evaluate new development for consistency with the development pattern set forth in the Sustainable Communities Strategies plan, upon adoption of the plan by the Southern California Association of Governments.	City of Ontario Planning Department	Prior to individual project approvals	City of Ontario Planning Department	
6-6 The City of Ontario shall participate in the County of San Bernardino's Green Valley Initiative.	City of Ontario Planning Department	Prior to individual project approvals	City of Ontario Planning Department	
5.12 NOISE				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
12-1 Prior to the issuance of building permits for any project that involves a noise-sensitive use within the 65 dBA CNEL contour along major roadways, freeways, railroads, or the Los Angeles/Ontario International Airport, the project property owner/developers shall retain an acoustical engineer to conduct an acoustic analysis and identify, where appropriate, site design features (e.g., setbacks, berms, or sound walls) and/or required building acoustical improvements (e.g., sound transmission class rated windows, doors, and attic baffling), to ensure compliance with the City's Noise Compatibility Criteria and the California State Building Code and California Noise Insulation Standards (Title 24 and 21 of the California Code of Regulations).	City of Ontario Planning/Building Department in coordination with the Landowner/Project Applicant	Prior to individual project approvals	City of Ontario Planning/Building Department	
12-2 Individual projects that involve vibration-intensive construction activities, such as pile drivers, jack hammers, and vibratory rollers, occurring near sensitive receptors shall be evaluated for potential vibration impacts. If construction-related vibration is determined to be perceptible at vibration-sensitive uses (i.e., exceed the Federal Transit Administration vibration-annoyance criteria of 78 VdB during the daytime), additional requirements, such as use of less vibration intensive equipment or construction techniques, shall be implemented during construction (e.g., drilled piles to eliminate use of vibration-intensive pile driver).	City of Ontario Planning/Building/MUA Department in coordination with the Landowner/Project Applicant's construction contractor	During construction	City of Ontario Building/MUA Department	
12-3 Prior to the issuance of building permits for any project that involves a vibration-sensitive use directly adjacent to the Union Pacific Railroad or Southern California Regional Rail Authority main lines shall retain an acoustical engineer to evaluate potential for trains to create perceptible levels of vibration indoors. If vibration-related impacts are found, mitigation measures, such as use of concrete, iron, or steel, or masonry materials to ensure that levels of vibration amplification are within acceptable limits to building occupants, shall be implemented. Pursuant to the Federal Transit Administration vibration-annoyance criteria, these acceptable limits are 78 VdB during the daytime and 72 VdB	City of Ontario Planning/Building Department with collaboration with the Landowner/Project Applicant	Prior to individual project approvals	City of Ontario Building Department	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
during the nighttime for residential uses, 84 VdB for office uses, and 90 VdB for workshops.				
12-4 Construction activities associated with new development that occurs near sensitive receptors shall be evaluated for potential noise impacts. Mitigation measures such as installation of temporary sound barriers for adjacent construction activities that occur adjacent to occupied noise-sensitive structures, equipping construction equipment with mufflers, and reducing non-essential idling of construction equipment to no more than five minutes shall be incorporated into the construction operations to reduce construction-related noise to the extent feasible.	City of Ontario Building/Planning/MUA Department in coordination with the Landowner/Project Applicant's construction contractor	During construction	City of Ontario Building/Planning/MUA Department	
5.16 TRANSPORTATION AND CIRCULATION				
16-1 The Mobility Element of the Ontario Plan shall be consistent with the traffic study prepared by Kimley-Horn and Associates in 2009. Table 5.16-6 in Section 5.16, <i>Transportation and Traffic</i> , shows the recommended lane geometry for the Proposed Land Use Plan.	City of Ontario Engineering/Planning Department	Ongoing	City of Ontario Engineering/Planning Department	
5.17 UTILITIES AND SERVICE SYSTEMS				
17-1 The City shall include a policy in the Policy Plan that requires water conservation measures for development projects to improve water use efficiency and reduce overall water demand. Reduce potable water demand, through conservation measures, including but not limited to: <ul style="list-style-type: none"> • Work cooperatively with all developers to incorporate conservation measures into project designs (such as those recommended by the California Urban Water Conservation Council). • Continue to develop and implement drought contingency plans to assist citizens and businesses 	City of Ontario Planning/MUA Department	Ongoing	City of Ontario Planning/MUA/Engineering Department	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>reduce water use during water shortages and emergencies.</p> <ul style="list-style-type: none"> Revise the City Code to include a Water-Efficient Landscape Ordinance to encourage or, as appropriate, require the use of water-efficient landscaping consistent with AB 1881. 				
<p>17-2 The City shall include a policy in the Policy Plan that maximizes the use of recycled water as an irrigation (nonpotable) source for landscaping, parks, and other irrigation opportunities in all areas of the City and requires use of recycled water in dual-system office and industrial uses in selected urban areas of the City, where available and feasible.</p>	<p>City of Ontario Planning Department in coordination with City of Ontario MUA/Engineering Department</p>	<p>Ongoing</p>	<p>City of Ontario Planning/MUA/Engineering Department</p>	
<p>17-3 The City shall include a policy in the Policy Plan that the City participate through the Chino Basin Water Master and the Inland Empire Utilities Agency in regional efforts to develop finding additional sources of water for groundwater recharge, such as capture of stormwater runoff, recycled water, or other sources to ensure that the Chino Basin stays in long-term hydraulic balance and sustainability and that adequate additional local water sources would be available to increase the flexibility of the City's water supply.</p>	<p>City of Ontario Planning Department in coordination with City of Ontario MUA/Engineering Department</p>	<p>Ongoing</p>	<p>City of Ontario Planning/MUA/Engineering Department</p>	

ATTACHMENT B:
Traffic Impact Analysis

(Document to follow this page)



TJW ENGINEERING, INC.
TRAFFIC ENGINEERING &
TRANSPORTATION PLANNING
CONSULTANTS

August 27, 2020

Mr. William D. Vogel
VOGEL PROPERTIES, INC.
300 Paseo Tesoro
Walnut, CA 91789

Subject: 3555 E Airport Drive Focused Traffic Analysis, City of Ontario

Dear Mr. Vogel,

TJW ENGINEERING, INC. (TJW) is pleased to present you with this focused traffic analysis for the proposed 3555 E. Airport Drive Project. The proposed project is located at the northeast corner of the intersection of Airport Drive and Haven Avenue in the City of Ontario.

This focused traffic analysis has been prepared to analyze project trip generation and assess the performance of a single westbound right turn lane compared to dual westbound right turn lanes at the intersection of Airport Drive and Haven Avenue. This report is being submitted to you for review and forwarding to the City of Ontario.

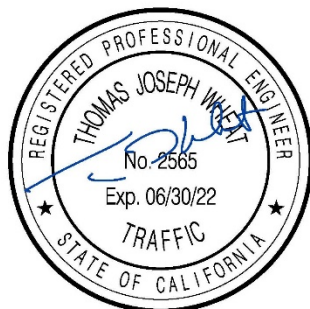
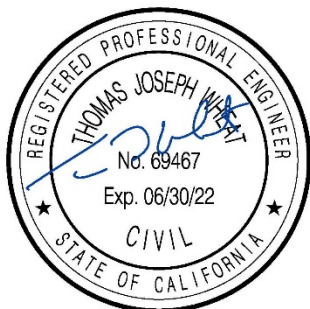
Please contact us at (949) 878-3509 if you have any questions regarding this analysis.

Sincerely,

Thomas Wheat, PE, TE
President

David Chew, PTP
Transportation Planner

Registered Civil Engineer #69467
Registered Traffic Engineer #2565



Jeffrey Chinchilla, PE
Project Engineer

6 Venture, Suite 225 | Irvine, California 92618 | t: (949) 878-3509
www.tjwengineering.com

1.1 PROJECT DESCRIPTION

The project is located at the northeast corner of the intersection of Airport Drive and Haven Avenue in the City of Ontario. According to the California Commerce Center Specific Plan, the site's current designated land use is Commercial/Food/Hotel. The proposed project would change the designated land use to Industrial.

The proposed project consists of 201,491 square foot building consisting of 198,991 square feet of warehouse use and 5,000 square feet of office use. Site access is planned along Airport Drive via one full-access driveway and one right-in-right-out driveway. The proposed project is anticipated to be built and generating trips in 2022. The proposed project location and project site plan are provided in the appendix.

1.2 PROJECT TRIP GENERATION ANALYSIS

Trip generation represents the amount of traffic, both inbound and outbound, produced by a development. Determining trip generation for a proposed project is based on projecting the amount of traffic that the specific land uses being proposed will produce. Industry standard *Institute of Transportation Engineers (ITE) Trip Generation Manual (10th Edition, 2017)* trip generation rates were used to determine trip generation for the proposed project land uses.

Table 1 summarizes the projected AM peak hour, PM peak hour and daily trip generation of the proposed project. The proposed project is projected to generate 423 daily trips, 43 AM peak hour trips, and 49 PM peak hour trips.

According to the California Commerce Center Specific Plan, the site's current designated land use is Commercial/Food/Hotel. The proposed project would change the designated land use to Industrial. Due to the change in land use, a trip generation analysis has been prepared to determine net differences in trip generation forecasts. **Table 2** summarizes the projected net difference between the proposed project trip generation and the California Commerce Center Specific Plan land use designation trip generation. A projected net difference of -7,183 daily trips, -146 AM peak hour trips, and -719 PM peak hour trips is expected between designated and proposed land uses.

Table 1
Proposed Project Trip Generation

Proposed Land Use ¹	Qty	Unit ²	Daily Trips (ADTs)		AM Peak Hour					PM Peak Hour				
			Rate	Volume	Rate	In:Out Split	Volume			Rate	In:Out Split	Volume		
							In	Out	Total			In	Out	Total
Warehousing (150)	196.49	TSF	1.74	342	0.17	77:23	25	8	33	0.19	27:73	10	27	37
Small Office (712)	5.00	TSF	16.19	81	1.92	83:17	8	2	10	2.45	32:68	4	8	12
Total	201.49	TSF		423			33	10	43			14	35	49

1: Rates from ITE Trip Generation (10th Edition, 2017)

2: TSF = Thousand Square Feet

Table 2
Net Difference in Trip Generation

Land Use ¹	Qty	Unit ²	Daily Trips (ADTs)		AM Peak Hour					PM Peak Hour				
			Rate	Volume	Rate	In:Out Split	Volume			Rate	In:Out Split	Volume		
							In	Out	Total			In	Out	Total
Proposed Project														
Warehousing (150)	196.49	TSF	1.74	342	0.17	77:23	25	8	33	0.19	27:73	10	27	37
Small Office (712)	5.00	TSF	16.19	81	1.92	83:17	8	2	10	2.45	32:68	4	8	12
Total		TSF		423			33	10	43			14	35	49
Designated Land Use														
Shopping Center/Commercial (820)	201.49	TSF	37.75	7,606	0.94	62:38	117	72	189	3.81	48:52	369	399	768
Net Difference				-7,183			-84	-62	-146			-355	-364	-719

1: Rates from ITE Trip Generation (10th Edition, 2017)

2: TSF = Thousand Square Feet

1.2 INTERSECTION ANALYSIS METHODOLOGY

Level of Service (LOS) is commonly used to describe the quality of flow on roadways and at intersections using a range of LOS from LOS A (free flow with little congestion) to LOS F (severely congested conditions). The definitions for LOS for interruption of traffic flow differ depending on the type of traffic control (traffic signal, unsignalized intersection with side street stops, unsignalized intersection with all-way stops). The *Highway Capacity Manual (HCM) 6* (Transportation Research Board, 2016) methodology expresses the LOS of an intersection in terms of delay time for the intersection approaches. The HCM methodology utilizes different procedures for different types of intersection control.

The City of Ontario and Caltrans traffic impact study guidelines require signalized intersection operations be analyzed utilizing the HCM 6th Edition methodology. Intersection LOS for signalized intersections is based on the intersections average control delay for all movements at the intersection during the peak hour. Control delay includes initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay.

Table 3 describes the general characteristics of traffic flow and accompanying delay ranges at signalized intersections.

Table 3
HCM – LOS & Delay Ranges – Signalized Intersections

Level Of Service	Description	Delay (in seconds)
A	Very favorable progression; most vehicles arrive during green signal and do not stop. Short cycle lengths.	0 – 10.00
B	Good progression, short cycle lengths. More vehicles stop than for LOS A.	10.01 – 20.00
C	Fair progression; longer cycle lengths. Individual cycle failures may begin to appear. The number of vehicles stopping is significant, though many vehicles still pass through without stopping.	20.01 – 35.00
D	Progression less favorable, longer cycle length and high flow/capacity ratio. The proportion of vehicles that pass through without stopping diminishes. Individual cycle failures are obvious.	35.01 – 55.00
E	Severe congestion with some long standing queues on critical approaches. Poor progression, long cycle lengths and high flow/capacity ratio. Individual cycle failures are frequent.	55.01 – 80.00
F	Very poor progression, long cycle lengths and many individual cycle failures. Arrival flow rates exceed capacity of intersection.	> 80.01

Source: Transportation Research Board, *Highway Capacity Manual*, HCM6 Edition (Washington D.C., 2016).

Collected peak hour traffic volumes have been adjusted using a peak hour factor (PHF) to reflect peak 15-minute volumes. It is a common practice in LOS analysis to conservatively use a peak 15-minute flow rate applied to the entire hour to derive flow rates in vehicles per hour that are used in the LOS analysis. The PHF is the relationship between the peak 15-minute flow rate and the full hourly volume. $PHF = \frac{\text{Hourly Volume}}{\text{Peak 15-minute flow rate}}$

[4 * Peak 15-Minute Volume]. The use of a 15-minute PHF produces a more detailed and conservative analysis compared to analyzing vehicles per hour. Existing PHFs, obtained from the existing traffic counts have been used for all analysis scenarios in this study.

1.3 TRAFFIC COUNTS AND VOLUME DEVELOPMENT

Due to the ongoing effects of COVID-19, traffic volumes have been lower than average. To account for abnormal traffic volumes, a 2% growth rate was applied to historical traffic counts from November of 2019 to establish existing 2020 volumes. A growth rate of 2% was applied to existing 2020 volumes to develop project opening year and buildout year volumes.

Existing (2020) = [Historical (2019) Counts * 1.02¹]

Project Opening Year (2022) = [Existing (2020) Volumes * 1.02²]

Buildout Year (2040) = [Existing (2020 Volumes) * 1.02²⁰]

Traffic volumes were converted into Passenger Car Equivalent (PCEs) utilizing the following factors:

- 2-axle trucks: 2.0 PCE
- 3-axle trucks: 2.5 PCE
- 4+ axle trucks: 3.0 PCE

1.4 PROJECT OPENING YEAR (2022) CONDITIONS LANE CONFIGURATION ANALYSIS

The *City of Ontario General Plan Update Transportation Technical Report* recommends the following westbound approach configuration for the intersection of Airport Drive and Haven Avenue:

- 2 left turn lanes, 3 through lanes, and 2 right turn lanes.

Due to the decrease in projected trip generation, a lane configuration analysis was conducted to determine the need for two right turn lanes as recommended in the *Transportation Technical Report*.

Project opening year conditions AM and PM peak hour intersection analysis is shown in **Table 4**. Calculations are based on the lane configuration shown in **Exhibit 1**. **Exhibit 1** also shows *project opening year* AM and PM peak hour volumes at the study intersection of Airport Drive and Haven Avenue. HCM analysis sheets are provided in the appendix.

Traffic operations are evaluated for the following time periods:

- Weekday AM Peak Hour occurring within 7:00 AM to 9:00 AM; and
- Weekday PM Peak Hour occurring within 4:00 PM to 6:00 PM.

Table 4
Intersection Analysis – Opening Year Conditions

Intersection		Control Type	Peak Hour	Opening Year Conditions				
				1 WBR Lanes		2 WBR Lanes		
				Delay ¹	LOS	Delay ¹	LOS	
1	Airport Dr	Haven Ave	Signal	AM	32.5	C	32.1	C
				PM	53.8	D	44.2	D

1: Delay shown in seconds per vehicle. Per the Highway Capacity Manual 6th Edition, overall average delay and LOS are shown for signalized intersections.
Note: WBR = Westbound Right

Policy 12.2 of the *City of Ontario General Plan Infrastructure Element* indicates that LOS E should be maintained at intersections. As shown in **Table 4**, the study intersection is projected to operate at an acceptable LOS during the AM and PM peak hours for *opening year* conditions for both single and dual westbound right lane configurations.

1.5 BUILDOUT YEAR (2040) CONDITIONS LANE CONFIGURATION ANALYSIS

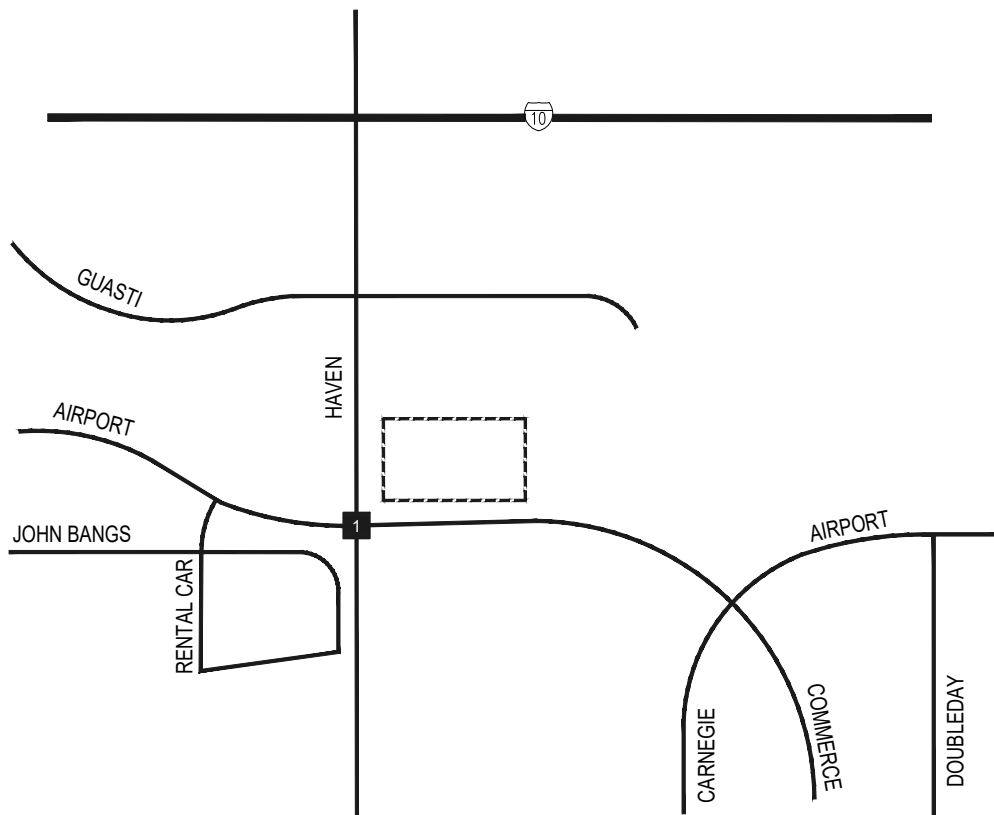
Buildout year conditions AM and PM peak hour intersection analysis is shown in **Table 5**. Calculations are based on the lane geometry shown in **Exhibit 2**. **Exhibit 2** also shows *buildout year* AM and PM peak hour volumes at the study intersection. HCM analysis sheets are provided in the appendix.

Table 5
Intersection Analysis – Buildout Year Conditions

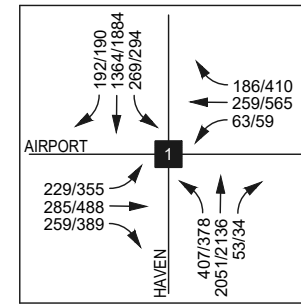
Intersection		Control Type	Peak Hour	Buildout Year Conditions				
				1 WBR Lanes		2 WBR Lanes		
				Delay ¹	LOS	Delay ¹	LOS	
1	Airport Dr	Haven Ave	Signal	AM	43.9	D	40.5	D
				PM	75.8	E	67.4	E

1: Delay shown in seconds per vehicle. Per the Highway Capacity Manual 6th Edition, overall average delay and LOS are shown for signalized intersections.
Note: WBR = Westbound Right

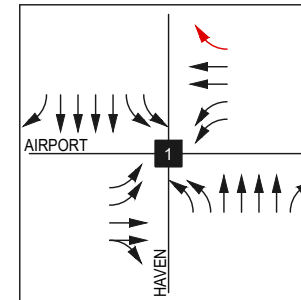
Policy 12.2 of the *City of Ontario General Plan Infrastructure Element* indicates that LOS E should be maintained at intersections. As shown in **Table 5**, the study intersection is projected to operate at an acceptable LOS during the AM and PM peak hours for *buildout year* conditions for both single and dual westbound right lane configurations.



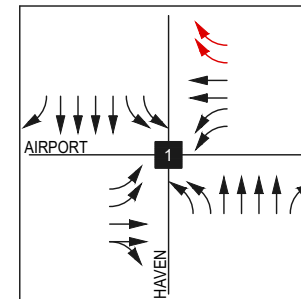
- Legend:
- XX/XX AM/PM Peak Hour Volumes
 - ← Existing Lane
 - ↪ Westbound Right Turn Lane
 - Signal-Controlled Intersection
 - Project Site



TRAFFIC VOLUMES



LANE GEOMETRY (1 WBR)

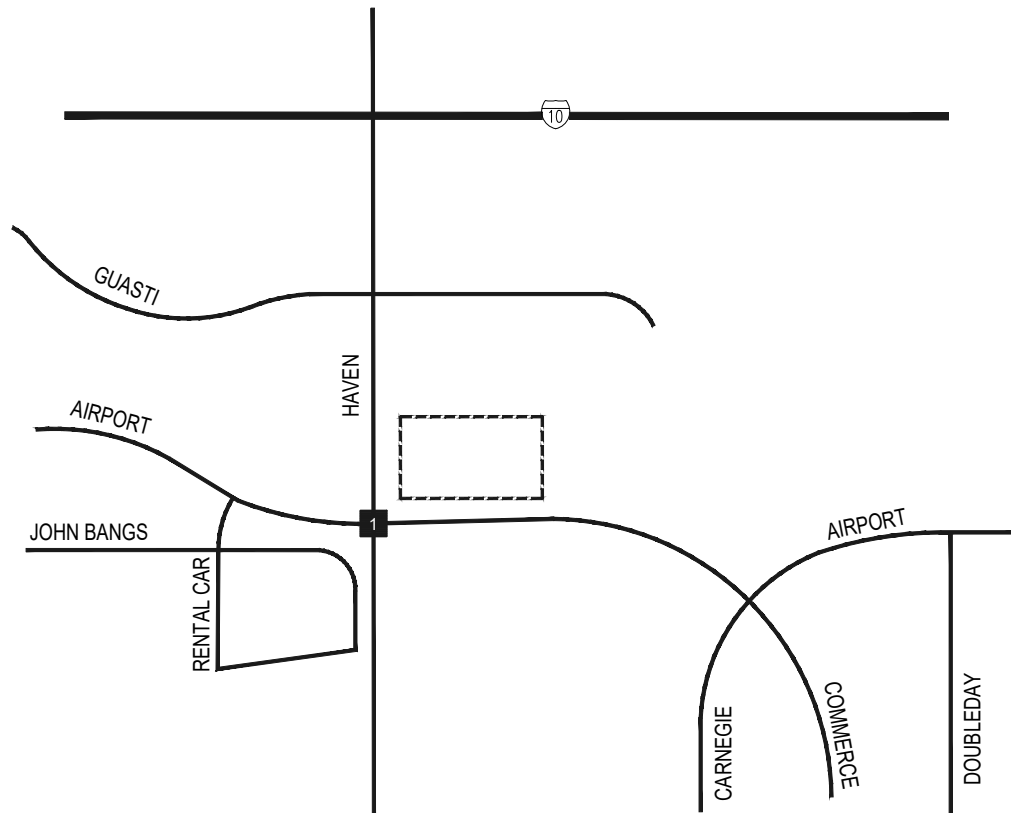


LANE GEOMETRY (2 WBR)

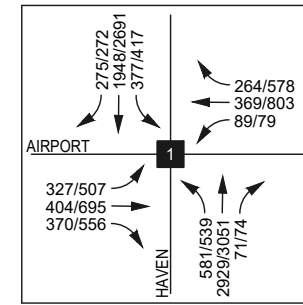


Exhibit 1: Project Opening Year (2022) AM/PM Peak Hour Volumes and Lane Geometry

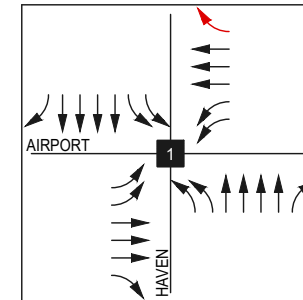




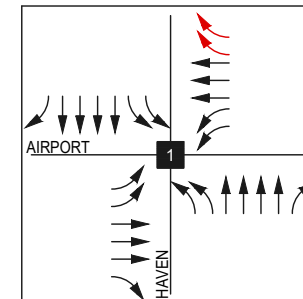
- Legend:
- XX/XX AM/PM Peak Hour Volumes
 - ← Existing Lane
 - ↪ Westbound Right Turn Lane
 - Signal-Controlled Intersection
 - Project Site



TRAFFIC VOLUMES



LANE GEOMETRY (1 WBR)



LANE GEOMETRY (2 WBR)



Exhibit 2: Buildout Year (2040) AM/PM Peak Hour Volumes and Lane Geometry

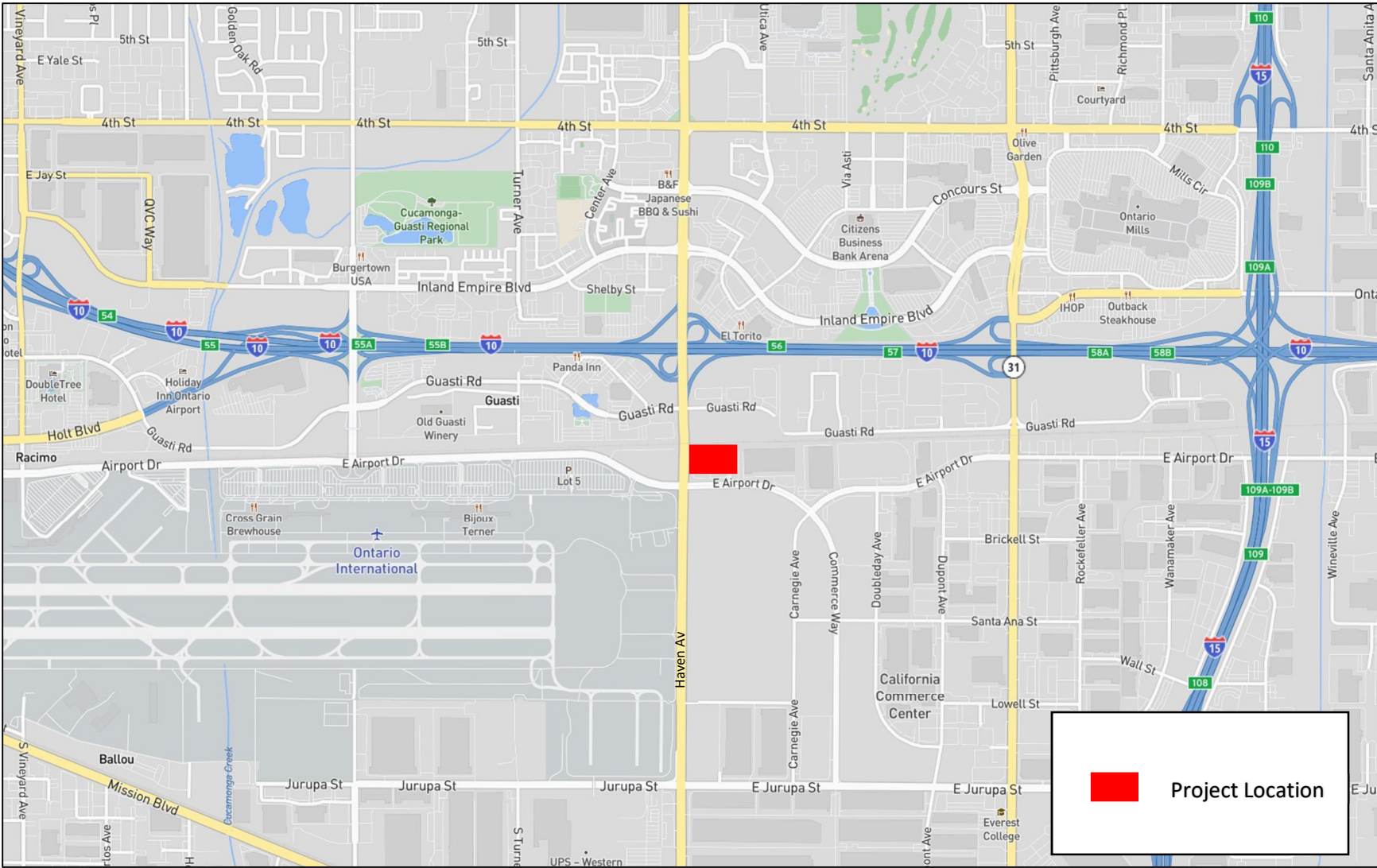


1.6 CONCLUSION

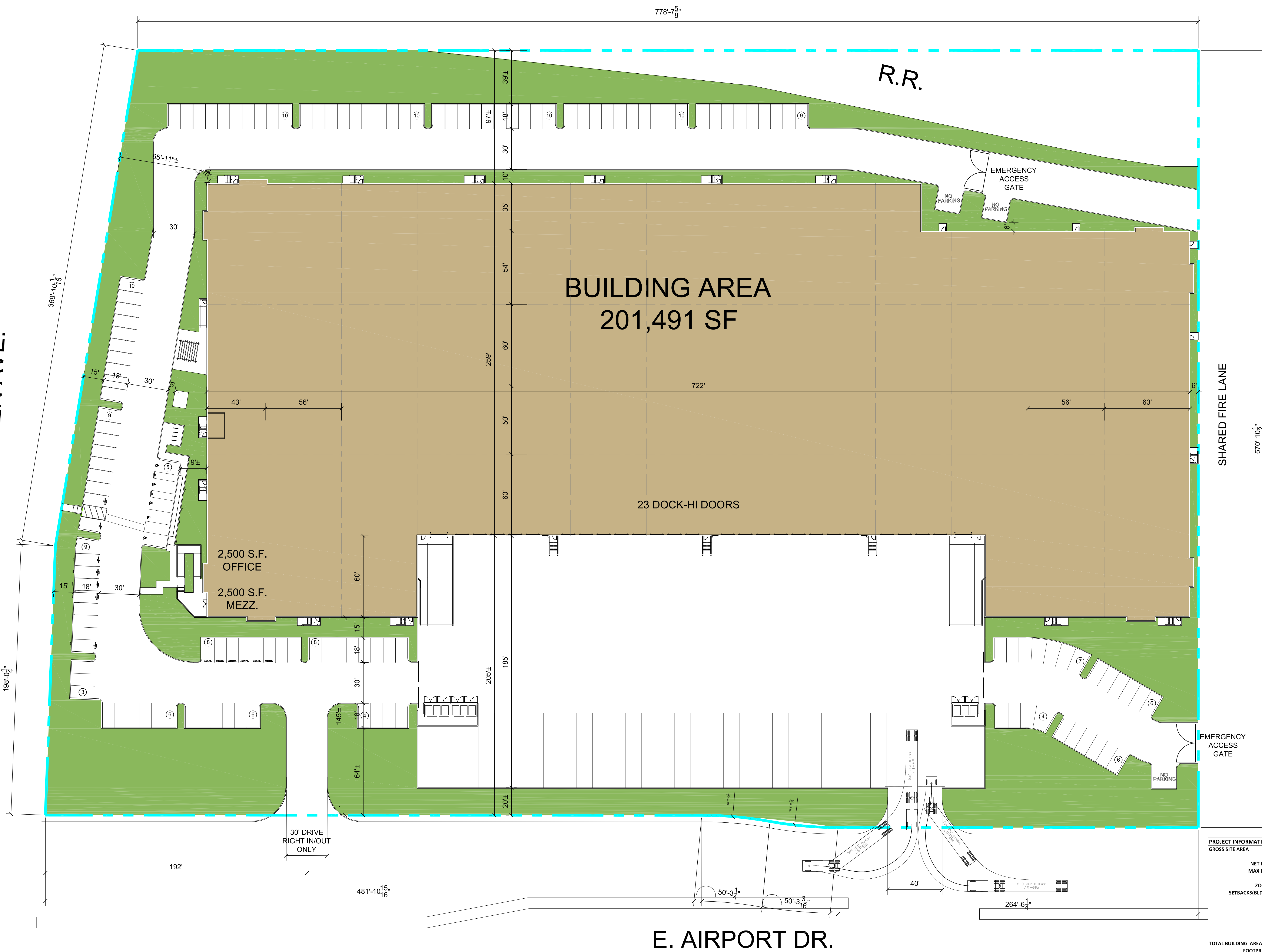
The proposed project would change the designated land use from Commercial/Food/Hotel to Industrial. A projected net difference of -7,183 daily trips, -146 AM peak hour trips, and -719 PM peak hour trips is anticipated between designated and proposed land uses.

The *City of Ontario General Plan Update Transportation Technical Report* recommends westbound dual right turn lanes at the intersection of Airport Drive/Haven Avenue. To determine if the decrease in projected trip generation would continue to warrant a second right turn lane, the intersection was analyzed with single and dual westbound right turn lane configurations. Lane configurations were analyzed for *project opening year* and *buildout year* conditions. The analysis results indicate that the intersection will operate at an acceptable LOS for *project opening year* and *buildout year* conditions with either single or dual westbound right turn lanes.

APPENDIX



S. HAVEN AVE.



E. AIRPORT DR.

A CONCEPTUAL SITE PLAN
SCALE: 1" = 60'

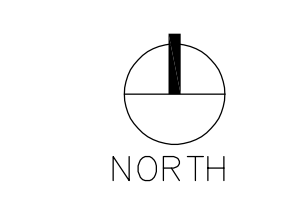
PROJECT VOGEL HAVEN - 3525 E AIRPORT DR
ONTARIO, CA
SCHEME 2
SITE PLAN

PROJECT INFORMATION		SCH 2	07.13.2020
GROSS SITE AREA	10.65 AC	469,978	
NET FAR		43.4%	
MAX FAR		55.0%	
ZONE:		(I-1) INDUSTRIAL LIGHT	
SETBACKS(BLDG):			
	STREET FRONT BLDG. SETBACK (E. AIRPORT DR.)	35'	
	STREET SIDE BLDG. SETBACK (S. HAVEN AVE.)	45'	
	STREET FRONT LANDSCAPE SETBACK	20'	
	STREET SIDE LANDSCAPE SETBACK (S. HAVEN AVE.)	15'	
	INT/REAR SETBACK	0'	
TOTAL BUILDING AREA		201,491 SF	
FOOTPRINT		198,991 SF	
	WAREHOUSE	196,491	
	OFFICE	2,500	
MEZZANINE		2,500	
	OFFICE	2,500	
TOTAL PARKING REQUIRED (9' x 18')		131	
	WAREHOUSE @ 1/1,000 UP TO 20K	20	
	WAREHOUSE @ 1/2,000 OVER 20K	91	
	OFFICE @ 1/250 IF OVER 10%	20	
PARKING PROVIDED (9' x 18')		140	
	STANDARD	122	
	ADA	5	
	EV VAN ADA	1	
	EV ADA	1	
	EV	5	
	CLEAN AIR	6	
	TRAILER PROVIDED (12X45)	25	
LANDSCAPE	REQUIRED	15.0%	69,597 SF
	PROVIDED	18.1%	83,752 SF

HERDMAN
ARCHITECTURE + DESIGN

16201 Scientific Way
Irvine, CA 92618
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714.389.2800
info@HerdmanRierson.com

A19-2147
07.13.2020



CONCEPTUAL
SITE PLAN

TRAFFIC COUNTS

City of Ontario
 N/S: South Haven Avenue
 E/W: East Airport Drive
 Weather: Clear

File Name : 12_ONT_S Haven_Airport AM
 Site Code : 99919787
 Start Date : 11/7/2019
 Page No : 1

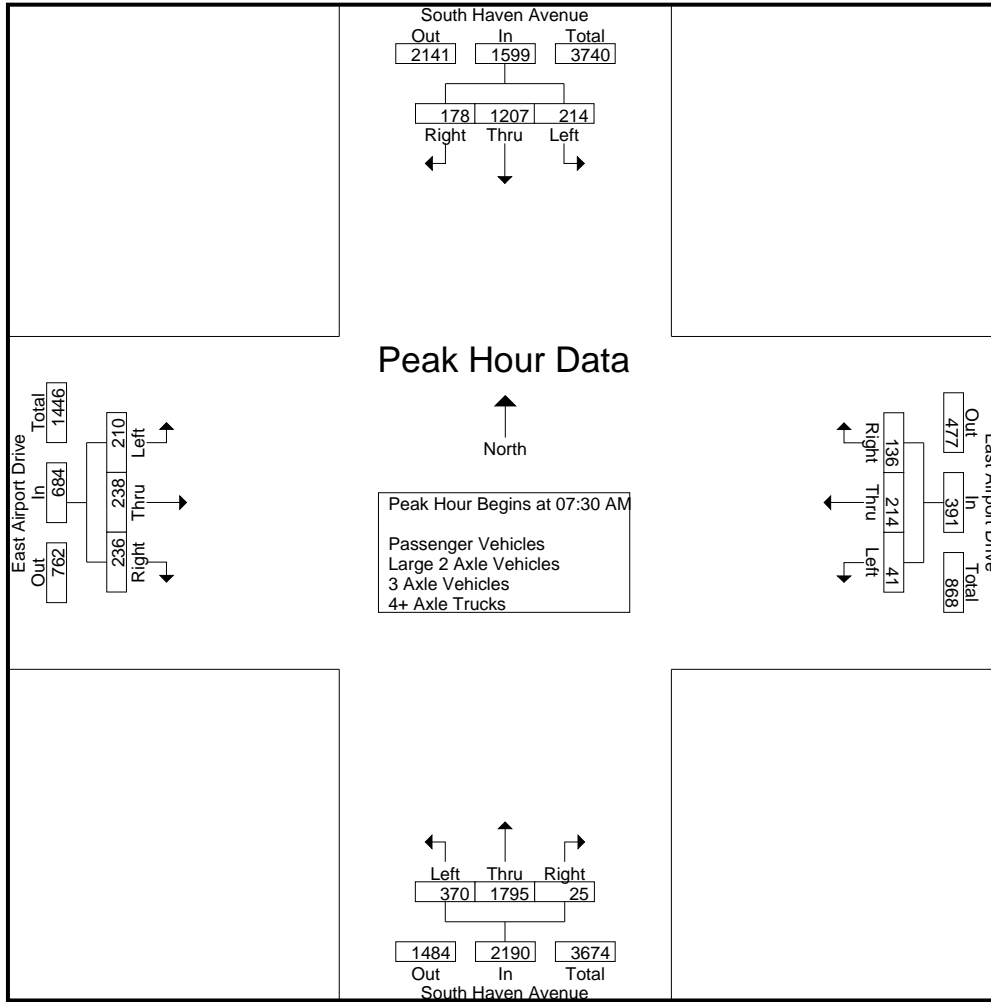
Groups Printed- Passenger Vehicles - Large 2 Axle Vehicles - 3 Axle Vehicles - 4+ Axle Trucks

Start Time	South Haven Avenue Southbound				East Airport Drive Westbound				South Haven Avenue Northbound				East Airport Drive Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
06:00 AM	30	170	43	243	4	30	7	41	27	126	6	159	25	20	15	60	503
06:15 AM	24	204	34	262	3	23	14	40	42	198	2	242	30	23	25	78	622
06:30 AM	37	270	35	342	8	23	14	45	43	236	6	285	24	37	43	104	776
06:45 AM	53	271	39	363	5	46	23	74	54	281	6	341	36	46	47	129	907
Total	144	915	151	1210	20	122	58	200	166	841	20	1027	115	126	130	371	2808
07:00 AM	40	313	33	386	7	30	29	66	70	303	5	378	24	32	41	97	927
07:15 AM	35	294	44	373	3	36	19	58	60	346	4	410	35	44	49	128	969
07:30 AM	45	316	36	397	14	60	32	106	89	439	6	534	57	79	61	197	1234
07:45 AM	80	306	48	434	13	52	35	100	88	474	6	568	60	77	70	207	1309
Total	200	1229	161	1590	37	178	115	330	307	1562	21	1890	176	232	221	629	4439
08:00 AM	45	328	53	426	10	62	31	103	93	449	4	546	47	43	54	144	1219
08:15 AM	44	257	41	342	4	40	38	82	100	433	9	542	46	39	51	136	1102
08:30 AM	47	268	50	365	6	40	38	84	104	454	6	564	53	36	48	137	1150
08:45 AM	39	260	47	346	7	37	24	68	77	422	12	511	64	36	53	153	1078
Total	175	1113	191	1479	27	179	131	337	374	1758	31	2163	210	154	206	570	4549
Grand Total	519	3257	503	4279	84	479	304	867	847	4161	72	5080	501	512	557	1570	11796
Apprch %	12.1	76.1	11.8		9.7	55.2	35.1		16.7	81.9	1.4		31.9	32.6	35.5		
Total %	4.4	27.6	4.3	36.3	0.7	4.1	2.6	7.3	7.2	35.3	0.6	43.1	4.2	4.3	4.7	13.3	
Passenger Vehicles	487	3097	495	4079	63	424	251	738	832	3883	53	4768	494	464	547	1505	11090
% Passenger Vehicles	93.8	95.1	98.4	95.3	75	88.5	82.6	85.1	98.2	93.3	73.6	93.9	98.6	90.6	98.2	95.9	94
Large 2 Axle Vehicles	9	57	5	71	4	37	11	52	7	133	1	141	2	28	3	33	297
% Large 2 Axle Vehicles	1.7	1.8	1	1.7	4.8	7.7	3.6	6	0.8	3.2	1.4	2.8	0.4	5.5	0.5	2.1	2.5
3 Axle Vehicles	7	24	0	31	2	6	6	14	2	26	1	29	3	11	2	16	90
% 3 Axle Vehicles	1.3	0.7	0	0.7	2.4	1.3	2	1.6	0.2	0.6	1.4	0.6	0.6	2.1	0.4	1	0.8
4+ Axle Trucks	16	79	3	98	15	12	36	63	6	119	17	142	2	9	5	16	319
% 4+ Axle Trucks	3.1	2.4	0.6	2.3	17.9	2.5	11.8	7.3	0.7	2.9	23.6	2.8	0.4	1.8	0.9	1	2.7

Start Time	South Haven Avenue Southbound				East Airport Drive Westbound				South Haven Avenue Northbound				East Airport Drive Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 06:00 AM to 08:45 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:30 AM																	
07:30 AM	45	316	36	397	14	60	32	106	89	439	6	534	57	79	61	197	1234
07:45 AM	80	306	48	434	13	52	35	100	88	474	6	568	60	77	70	207	1309
08:00 AM	45	328	53	426	10	62	31	103	93	449	4	546	47	43	54	144	1219
08:15 AM	44	257	41	342	4	40	38	82	100	433	9	542	46	39	51	136	1102
Total Volume	214	1207	178	1599	41	214	136	391	370	1795	25	2190	210	238	236	684	4864
% App. Total	13.4	75.5	11.1		10.5	54.7	34.8		16.9	82	1.1		30.7	34.8	34.5		
PHF	.669	.920	.840	.921	.732	.863	.895	.922	.925	.947	.694	.964	.875	.753	.843	.826	.929

City of Ontario
 N/S: South Haven Avenue
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 Weather: Clear

File Name : 12_ONT_S Haven_Airport AM
 Site Code : 99919787
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Peak Hour Analysis From 06:00 AM to 08:45 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:15 AM				07:30 AM				07:45 AM				07:30 AM			
+0 mins.	35	294	44	373	14	60	32	106	88	474	6	568	57	79	61	197
+15 mins.	45	316	36	397	13	52	35	100	93	449	4	546	60	77	70	207
+30 mins.	80	306	48	434	10	62	31	103	100	433	9	542	47	43	54	144
+45 mins.	45	328	53	426	4	40	38	82	104	454	6	564	46	39	51	136
Total Volume	205	1244	181	1630	41	214	136	391	385	1810	25	2220	210	238	236	684
% App. Total	12.6	76.3	11.1		10.5	54.7	34.8		17.3	81.5	1.1		30.7	34.8	34.5	
PHF	.641	.948	.854	.939	.732	.863	.895	.922	.925	.955	.694	.977	.875	.753	.843	.826

City of Ontario
 N/S: South Haven Avenue
 E/W: East Airport Drive
 Weather: Clear

File Name : 12_ONT_S Haven_Airport AM
 Site Code : 99919787
 Start Date : 11/7/2019
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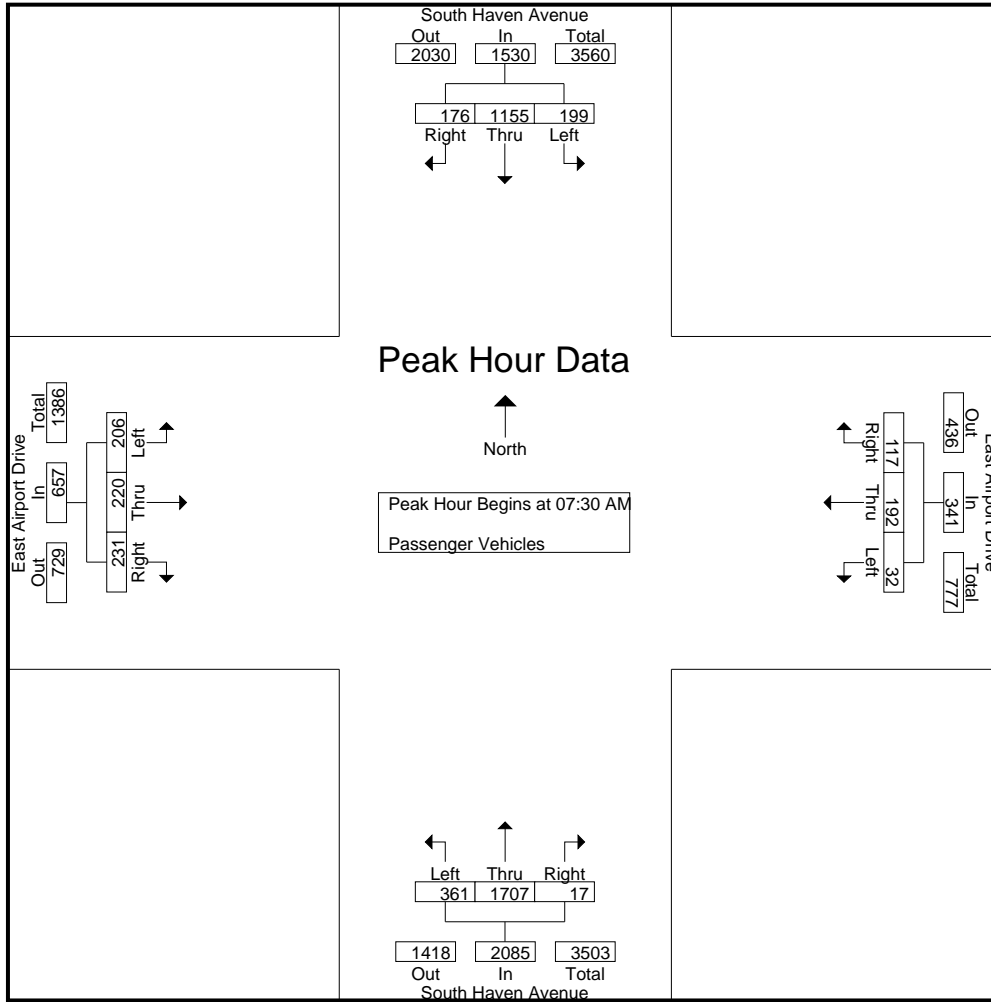
Groups Printed- Passenger Vehicles

Start Time	South Haven Avenue Southbound				East Airport Drive Westbound				South Haven Avenue Northbound				East Airport Drive Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
06:00 AM	27	159	43	229	2	28	5	35	27	110	4	141	24	17	14	55	460
06:15 AM	22	193	33	248	2	20	9	31	42	174	0	216	30	22	25	77	572
06:30 AM	37	255	34	326	6	19	11	36	41	210	5	256	23	30	43	96	714
06:45 AM	50	259	37	346	5	38	19	62	52	259	5	316	36	44	46	126	850
Total	136	866	147	1149	15	105	44	164	162	753	14	929	113	113	128	354	2596
07:00 AM	39	304	33	376	6	28	26	60	70	286	5	361	24	29	41	94	891
07:15 AM	34	281	44	359	2	34	14	50	60	326	4	390	35	42	48	125	924
07:30 AM	40	308	35	383	11	53	26	90	84	417	5	506	57	76	59	192	1171
07:45 AM	76	298	48	422	10	47	30	87	86	454	2	542	59	71	68	198	1249
Total	189	1191	160	1540	29	162	96	287	300	1483	16	1799	175	218	216	609	4235
08:00 AM	41	311	52	404	8	56	28	92	91	426	3	520	46	37	53	136	1152
08:15 AM	42	238	41	321	3	36	33	72	100	410	7	517	44	36	51	131	1041
08:30 AM	47	246	48	341	2	34	33	69	102	417	3	522	53	29	46	128	1060
08:45 AM	32	245	47	324	6	31	17	54	77	394	10	481	63	31	53	147	1006
Total	162	1040	188	1390	19	157	111	287	370	1647	23	2040	206	133	203	542	4259
Grand Total	487	3097	495	4079	63	424	251	738	832	3883	53	4768	494	464	547	1505	11090
Apprch %	11.9	75.9	12.1		8.5	57.5	34		17.4	81.4	1.1		32.8	30.8	36.3		
Total %	4.4	27.9	4.5	36.8	0.6	3.8	2.3	6.7	7.5	35	0.5	43	4.5	4.2	4.9	13.6	

Start Time	South Haven Avenue Southbound				East Airport Drive Westbound				South Haven Avenue Northbound				East Airport Drive Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:30 AM to 08:15 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:30 AM																	
07:30 AM	40	308	35	383	11	53	26	90	84	417	5	506	57	76	59	192	1171
07:45 AM	76	298	48	422	10	47	30	87	86	454	2	542	59	71	68	198	1249
08:00 AM	41	311	52	404	8	56	28	92	91	426	3	520	46	37	53	136	1152
08:15 AM	42	238	41	321	3	36	33	72	100	410	7	517	44	36	51	131	1041
Total Volume	199	1155	176	1530	32	192	117	341	361	1707	17	2085	206	220	231	657	4613
% App. Total	13	75.5	11.5		9.4	56.3	34.3		17.3	81.9	0.8		31.4	33.5	35.2		
PHF	.655	.928	.846	.906	.727	.857	.886	.927	.903	.940	.607	.962	.873	.724	.849	.830	.923

City of Ontario
 N/S: South Haven Avenue
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 Weather: Clear

File Name : 12_ONT_S Haven_Airport AM
 Site Code : 99919787
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Peak Hour Analysis From 07:30 AM to 08:15 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:30 AM				07:30 AM				07:30 AM				07:30 AM			
+0 mins.	40	308	35	383	11	53	26	90	84	417	5	506	57	76	59	192
+15 mins.	76	298	48	422	10	47	30	87	86	454	2	542	59	71	68	198
+30 mins.	41	311	52	404	8	56	28	92	91	426	3	520	46	37	53	136
+45 mins.	42	238	41	321	3	36	33	72	100	410	7	517	44	36	51	131
Total Volume	199	1155	176	1530	32	192	117	341	361	1707	17	2085	206	220	231	657
% App. Total	13	75.5	11.5		9.4	56.3	34.3		17.3	81.9	0.8		31.4	33.5	35.2	
PHF	.655	.928	.846	.906	.727	.857	.886	.927	.903	.940	.607	.962	.873	.724	.849	.830

City of Ontario
 N/S: South Haven Avenue
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File Name : 12_ONT_S Haven_Airport AM
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 Page No : 1

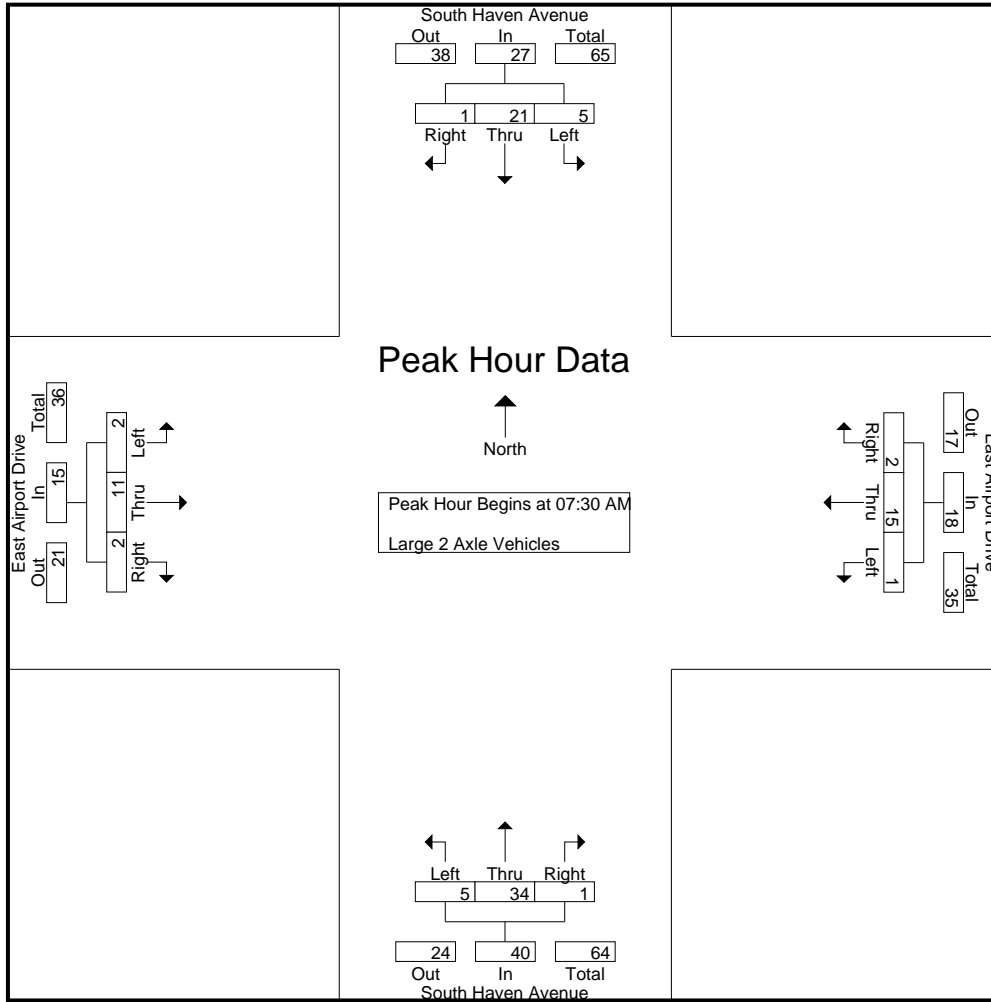
Groups Printed- Large 2 Axle Vehicles

Start Time	South Haven Avenue Southbound				East Airport Drive Westbound				South Haven Avenue Northbound				East Airport Drive Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
06:00 AM	1	1	0	2	0	2	0	2	0	6	0	6	0	3	0	3	13
06:15 AM	0	3	0	3	1	1	0	2	0	9	0	9	0	0	0	0	14
06:30 AM	0	4	0	4	1	4	0	5	1	13	0	14	0	3	0	3	26
06:45 AM	0	5	2	7	0	5	0	5	0	11	0	11	0	2	0	2	25
Total	1	13	2	16	2	12	0	14	1	39	0	40	0	8	0	8	78
07:00 AM	0	3	0	3	0	1	2	3	0	5	0	5	0	2	0	2	13
07:15 AM	0	7	0	7	0	1	4	5	0	10	0	10	0	2	0	2	24
07:30 AM	0	5	1	6	0	5	0	5	2	9	0	11	0	3	0	3	25
07:45 AM	1	4	0	5	0	2	0	2	1	7	1	9	0	4	2	6	22
Total	1	19	1	21	0	9	6	15	3	31	1	35	0	11	2	13	84
08:00 AM	3	3	0	6	0	5	1	6	2	11	0	13	0	2	0	2	27
08:15 AM	1	9	0	10	1	3	1	5	0	7	0	7	2	2	0	4	26
08:30 AM	0	9	2	11	1	6	1	8	1	28	0	29	0	3	1	4	52
08:45 AM	3	4	0	7	0	2	2	4	0	17	0	17	0	2	0	2	30
Total	7	25	2	34	2	16	5	23	3	63	0	66	2	9	1	12	135
Grand Total	9	57	5	71	4	37	11	52	7	133	1	141	2	28	3	33	297
Apprch %	12.7	80.3	7		7.7	71.2	21.2		5	94.3	0.7		6.1	84.8	9.1		
Total %	3	19.2	1.7	23.9	1.3	12.5	3.7	17.5	2.4	44.8	0.3	47.5	0.7	9.4	1	11.1	

Start Time	South Haven Avenue Southbound				East Airport Drive Westbound				South Haven Avenue Northbound				East Airport Drive Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:30 AM to 08:15 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:30 AM																	
07:30 AM	0	5	1	6	0	5	0	5	2	9	0	11	0	3	0	3	25
07:45 AM	1	4	0	5	0	2	0	2	1	7	1	9	0	4	2	6	22
08:00 AM	3	3	0	6	0	5	1	6	2	11	0	13	0	2	0	2	27
08:15 AM	1	9	0	10	1	3	1	5	0	7	0	7	2	2	0	4	26
Total Volume	5	21	1	27	1	15	2	18	5	34	1	40	2	11	2	15	100
% App. Total	18.5	77.8	3.7		5.6	83.3	11.1		12.5	85	2.5		13.3	73.3	13.3		
PHF	.417	.583	.250	.675	.250	.750	.500	.750	.625	.773	.250	.769	.250	.688	.250	.625	.926

City of Ontario
 N/S: South Haven Avenue
 E/W: East Airport Drive
 Weather: Clear

File Name : 12_ONT_S Haven_Airport AM
 Site Code : 99919787
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Peak Hour Analysis From 07:30 AM to 08:15 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:30 AM				07:30 AM				07:30 AM				07:30 AM			
+0 mins.	0	5	1	6	0	5	0	5	2	9	0	11	0	3	0	3
+15 mins.	1	4	0	5	0	2	0	2	1	7	1	9	0	4	2	6
+30 mins.	3	3	0	6	0	5	1	6	2	11	0	13	0	2	0	2
+45 mins.	1	9	0	10	1	3	1	5	0	7	0	7	2	2	0	4
Total Volume	5	21	1	27	1	15	2	18	5	34	1	40	2	11	2	15
% App. Total	18.5	77.8	3.7		5.6	83.3	11.1		12.5	85	2.5		13.3	73.3	13.3	
PHF	.417	.583	.250	.675	.250	.750	.500	.750	.625	.773	.250	.769	.250	.688	.250	.625

City of Ontario
 N/S: South Haven Avenue
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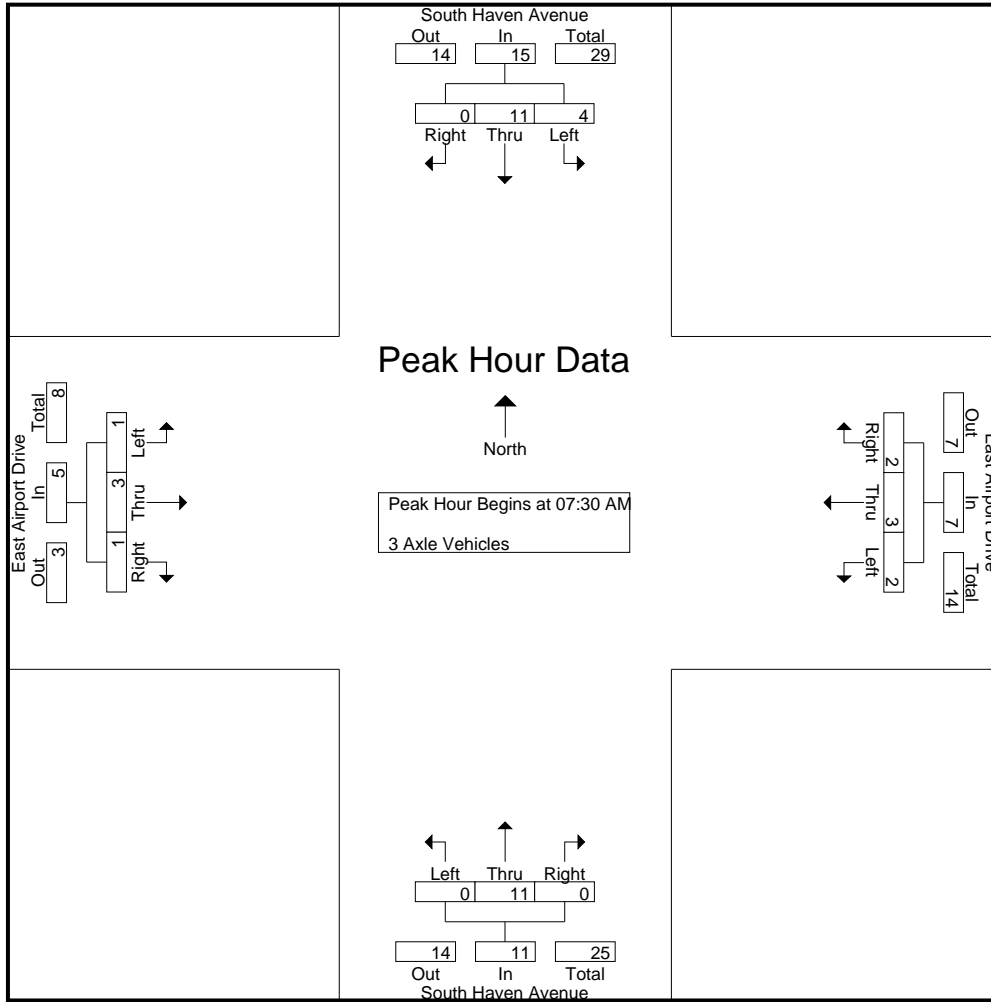
Groups Printed- 3 Axle Vehicles

Start Time	South Haven Avenue Southbound				East Airport Drive Westbound				South Haven Avenue Northbound				East Airport Drive Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
06:00 AM	1	1	0	2	0	0	0	0	0	1	0	1	1	0	1	2	5
06:15 AM	0	2	0	2	0	1	1	2	0	2	0	2	0	0	0	0	6
06:30 AM	0	1	0	1	0	0	0	0	0	6	0	6	0	4	0	4	11
06:45 AM	0	2	0	2	0	2	1	3	2	3	1	6	0	0	0	0	11
Total	1	6	0	7	0	3	2	5	2	12	1	15	1	4	1	6	33
07:00 AM	0	1	0	1	0	0	0	0	0	1	0	1	0	1	0	1	3
07:15 AM	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	1
07:30 AM	3	1	0	4	0	0	0	0	0	0	0	0	0	0	1	1	5
07:45 AM	1	1	0	2	1	1	1	3	0	6	0	6	0	1	0	1	12
Total	4	3	0	7	1	1	1	3	0	8	0	8	0	2	1	3	21
08:00 AM	0	6	0	6	1	1	0	2	0	3	0	3	1	1	0	2	13
08:15 AM	0	3	0	3	0	1	1	2	0	2	0	2	0	1	0	1	8
08:30 AM	0	2	0	2	0	0	0	0	0	1	0	1	0	1	0	1	4
08:45 AM	2	4	0	6	0	0	2	2	0	0	0	0	1	2	0	3	11
Total	2	15	0	17	1	2	3	6	0	6	0	6	2	5	0	7	36
Grand Total	7	24	0	31	2	6	6	14	2	26	1	29	3	11	2	16	90
Apprch %	22.6	77.4	0		14.3	42.9	42.9		6.9	89.7	3.4		18.8	68.8	12.5		
Total %	7.8	26.7	0	34.4	2.2	6.7	6.7	15.6	2.2	28.9	1.1	32.2	3.3	12.2	2.2	17.8	

Start Time	South Haven Avenue Southbound				East Airport Drive Westbound				South Haven Avenue Northbound				East Airport Drive Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:30 AM to 08:15 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:30 AM																	
07:30 AM	3	1	0	4	0	0	0	0	0	0	0	0	0	0	1	1	5
07:45 AM	1	1	0	2	1	1	1	3	0	6	0	6	0	1	0	1	12
08:00 AM	0	6	0	6	1	1	0	2	0	3	0	3	1	1	0	2	13
08:15 AM	0	3	0	3	0	1	1	2	0	2	0	2	0	1	0	1	8
Total Volume	4	11	0	15	2	3	2	7	0	11	0	11	1	3	1	5	38
% App. Total	26.7	73.3	0		28.6	42.9	28.6		0	100	0		20	60	20		
PHF	.333	.458	.000	.625	.500	.750	.500	.583	.000	.458	.000	.458	.250	.750	.250	.625	.731

City of Ontario
 N/S: South Haven Avenue
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 Weather: Clear

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 Site Code : 99919787
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Peak Hour Analysis From 07:30 AM to 08:15 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:30 AM				07:30 AM				07:30 AM				07:30 AM			
+0 mins.	3	1	0	4	0	0	0	0	0	0	0	0	0	0	1	1
+15 mins.	1	1	0	2	1	1	1	3	0	6	0	6	0	1	0	1
+30 mins.	0	6	0	6	1	1	0	2	0	3	0	3	1	1	0	2
+45 mins.	0	3	0	3	0	1	1	2	0	2	0	2	0	1	0	1
Total Volume	4	11	0	15	2	3	2	7	0	11	0	11	1	3	1	5
% App. Total	26.7	73.3	0		28.6	42.9	28.6		0	100	0		20	60	20	
PHF	.333	.458	.000	.625	.500	.750	.500	.583	.000	.458	.000	.458	.250	.750	.250	.625

City of Ontario
 N/S: South Haven Avenue
 E/W: East Airport Drive
 Weather: Clear

File Name : 12_ONT_S Haven_Airport AM
 Site Code : 99919787
 Start Date : 11/7/2019
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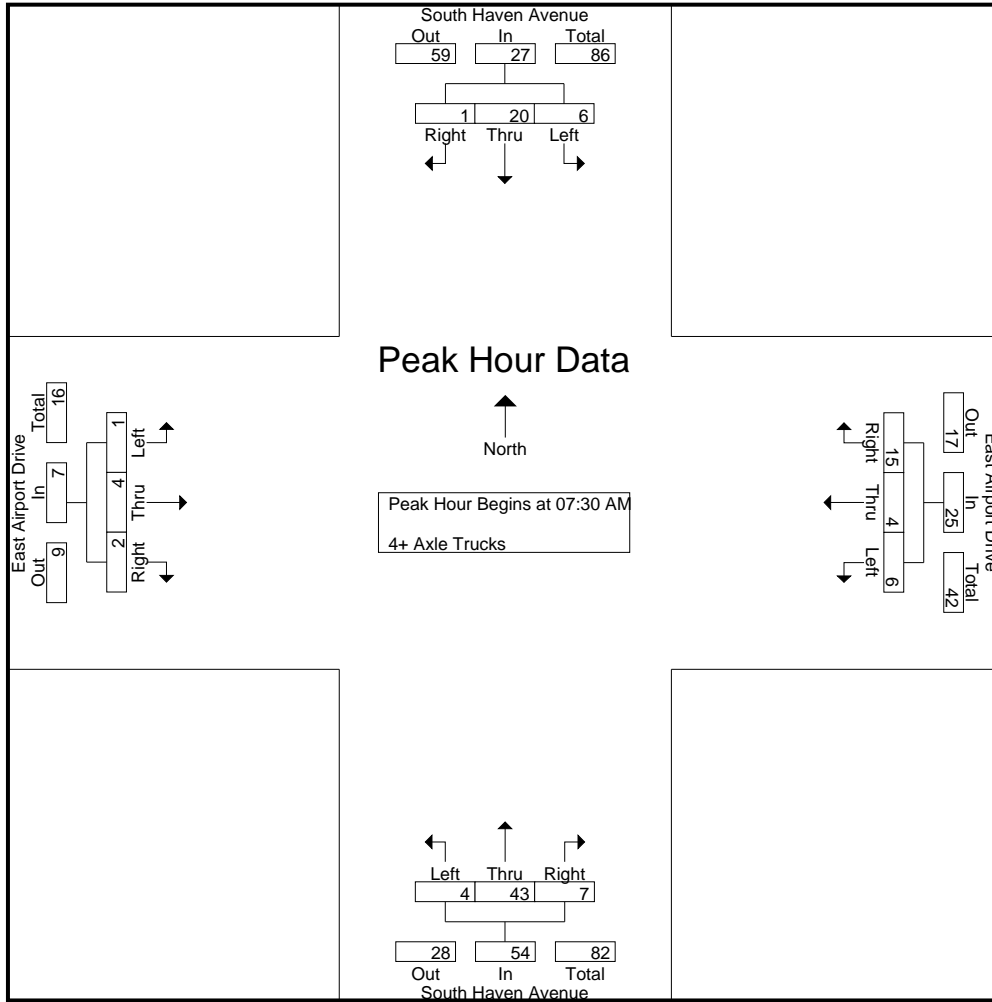
Groups Printed- 4+ Axle Trucks

Start Time	South Haven Avenue Southbound				East Airport Drive Westbound				South Haven Avenue Northbound				East Airport Drive Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
06:00 AM	1	9	0	10	2	0	2	4	0	9	2	11	0	0	0	0	25
06:15 AM	2	6	1	9	0	1	4	5	0	13	2	15	0	1	0	1	30
06:30 AM	0	10	1	11	1	0	3	4	1	7	1	9	1	0	0	1	25
06:45 AM	3	5	0	8	0	1	3	4	0	8	0	8	0	0	1	1	21
Total	6	30	2	38	3	2	12	17	1	37	5	43	1	1	1	3	101
07:00 AM	1	5	0	6	1	1	1	3	0	11	0	11	0	0	0	0	20
07:15 AM	1	6	0	7	1	1	1	3	0	9	0	9	0	0	1	1	20
07:30 AM	2	2	0	4	3	2	6	11	3	13	1	17	0	0	1	1	33
07:45 AM	2	3	0	5	2	2	4	8	1	7	3	11	1	1	0	2	26
Total	6	16	0	22	7	6	12	25	4	40	4	48	1	1	2	4	99
08:00 AM	1	8	1	10	1	0	2	3	0	9	1	10	0	3	1	4	27
08:15 AM	1	7	0	8	0	0	3	3	0	14	2	16	0	0	0	0	27
08:30 AM	0	11	0	11	3	0	4	7	1	8	3	12	0	3	1	4	34
08:45 AM	2	7	0	9	1	4	3	8	0	11	2	13	0	1	0	1	31
Total	4	33	1	38	5	4	12	21	1	42	8	51	0	7	2	9	119
Grand Total	16	79	3	98	15	12	36	63	6	119	17	142	2	9	5	16	319
Apprch %	16.3	80.6	3.1		23.8	19	57.1		4.2	83.8	12		12.5	56.2	31.2		
Total %	5	24.8	0.9	30.7	4.7	3.8	11.3	19.7	1.9	37.3	5.3	44.5	0.6	2.8	1.6	5	

Start Time	South Haven Avenue Southbound				East Airport Drive Westbound				South Haven Avenue Northbound				East Airport Drive Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 07:30 AM to 08:15 AM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 07:30 AM																	
07:30 AM	2	2	0	4	3	2	6	11	3	13	1	17	0	0	1	1	33
07:45 AM	2	3	0	5	2	2	4	8	1	7	3	11	1	1	0	2	26
08:00 AM	1	8	1	10	1	0	2	3	0	9	1	10	0	3	1	4	27
08:15 AM	1	7	0	8	0	0	3	3	0	14	2	16	0	0	0	0	27
Total Volume	6	20	1	27	6	4	15	25	4	43	7	54	1	4	2	7	113
% App. Total	22.2	74.1	3.7		24	16	60		7.4	79.6	13		14.3	57.1	28.6		
PHF	.750	.625	.250	.675	.500	.500	.625	.568	.333	.768	.583	.794	.250	.333	.500	.438	.856

City of Ontario
 N/S: South Haven Avenue
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 Weather: Clear

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Peak Hour Analysis From 07:30 AM to 08:15 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:30 AM				07:30 AM				07:30 AM				07:30 AM			
+0 mins.	2	2	0	4	3	2	6	11	3	13	1	17	0	0	1	1
+15 mins.	2	3	0	5	2	2	4	8	1	7	3	11	1	1	0	2
+30 mins.	1	8	1	10	1	0	2	3	0	9	1	10	0	3	1	4
+45 mins.	1	7	0	8	0	0	3	3	0	14	2	16	0	0	0	0
Total Volume	6	20	1	27	6	4	15	25	4	43	7	54	1	4	2	7
% App. Total	22.2	74.1	3.7		24	16	60		7.4	79.6	13		14.3	57.1	28.6	
PHF	.750	.625	.250	.675	.500	.500	.625	.568	.333	.768	.583	.794	.250	.333	.500	.438

City of Ontario
 N/S: South Haven Avenue
 E/W: East Airport Drive
 Weather: Clear

File Name : 12_ONT_S Haven_Airport PM
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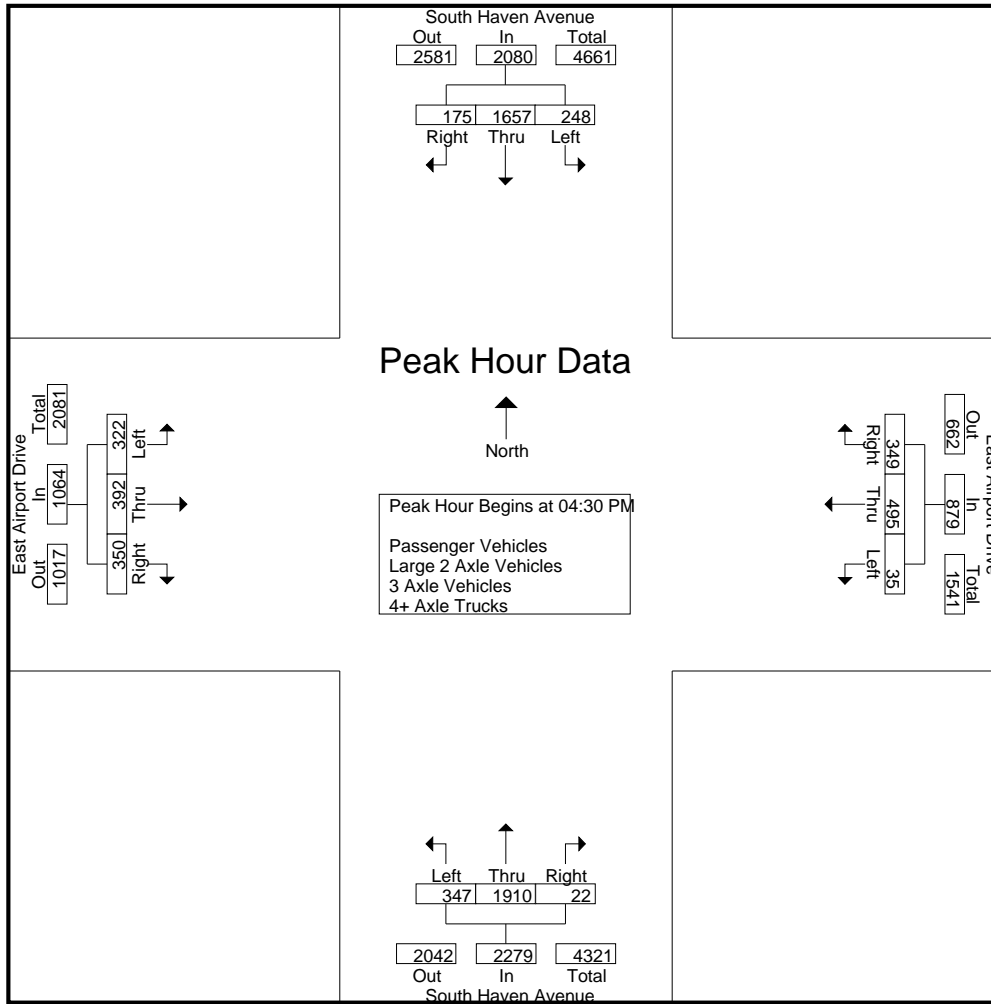
Groups Printed- Passenger Vehicles - Large 2 Axle Vehicles - 3 Axle Vehicles - 4+ Axle Trucks

Start Time	South Haven Avenue Southbound				East Airport Drive Westbound				South Haven Avenue Northbound				East Airport Drive Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
04:00 PM	40	357	37	434	24	106	95	225	88	546	10	644	76	95	52	223	1526
04:15 PM	52	339	37	428	4	99	73	176	91	508	15	614	61	88	103	252	1470
04:30 PM	55	386	45	486	8	120	97	225	77	511	2	590	79	92	63	234	1535
04:45 PM	53	413	31	497	7	121	76	204	82	497	4	583	71	98	91	260	1544
Total	200	1495	150	1845	43	446	341	830	338	2062	31	2431	287	373	309	969	6075
05:00 PM	75	389	49	513	5	125	91	221	101	452	7	560	71	87	97	255	1549
05:15 PM	65	469	50	584	15	129	85	229	87	450	9	546	101	115	99	315	1674
05:30 PM	36	355	45	436	16	115	56	187	87	522	7	616	55	126	109	290	1529
05:45 PM	42	340	42	424	18	91	63	172	65	390	1	456	84	101	63	248	1300
Total	218	1553	186	1957	54	460	295	809	340	1814	24	2178	311	429	368	1108	6052
Grand Total	418	3048	336	3802	97	906	636	1639	678	3876	55	4609	598	802	677	2077	12127
Apprch %	11	80.2	8.8		5.9	55.3	38.8		14.7	84.1	1.2		28.8	38.6	32.6		
Total %	3.4	25.1	2.8	31.4	0.8	7.5	5.2	13.5	5.6	32	0.5	38	4.9	6.6	5.6	17.1	
Passenger Vehicles	390	2878	328	3596	85	863	613	1561	658	3721	42	4421	585	722	648	1955	11533
% Passenger Vehicles	93.3	94.4	97.6	94.6	87.6	95.3	96.4	95.2	97.1	96	76.4	95.9	97.8	90	95.7	94.1	95.1
Large 2 Axle Vehicles	10	78	4	92	6	22	4	32	10	47	3	60	9	45	19	73	257
% Large 2 Axle Vehicles	2.4	2.6	1.2	2.4	6.2	2.4	0.6	2	1.5	1.2	5.5	1.3	1.5	5.6	2.8	3.5	2.1
3 Axle Vehicles	6	14	2	22	2	6	2	10	5	21	2	28	0	18	4	22	82
% 3 Axle Vehicles	1.4	0.5	0.6	0.6	2.1	0.7	0.3	0.6	0.7	0.5	3.6	0.6	0	2.2	0.6	1.1	0.7
4+ Axle Trucks	12	78	2	92	4	15	17	36	5	87	8	100	4	17	6	27	255
% 4+ Axle Trucks	2.9	2.6	0.6	2.4	4.1	1.7	2.7	2.2	0.7	2.2	14.5	2.2	0.7	2.1	0.9	1.3	2.1

Start Time	South Haven Avenue Southbound				East Airport Drive Westbound				South Haven Avenue Northbound				East Airport Drive Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:30 PM																	
04:30 PM	55	386	45	486	8	120	97	225	77	511	2	590	79	92	63	234	1535
04:45 PM	53	413	31	497	7	121	76	204	82	497	4	583	71	98	91	260	1544
05:00 PM	75	389	49	513	5	125	91	221	101	452	7	560	71	87	97	255	1549
05:15 PM	65	469	50	584	15	129	85	229	87	450	9	546	101	115	99	315	1674
Total Volume	248	1657	175	2080	35	495	349	879	347	1910	22	2279	322	392	350	1064	6302
% App. Total	11.9	79.7	8.4		4	56.3	39.7		15.2	83.8	1		30.3	36.8	32.9		
PHF	.827	.883	.875	.890	.583	.959	.899	.960	.859	.934	.611	.966	.797	.852	.884	.844	.941

City of Ontario
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Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	04:30 PM				04:30 PM				04:00 PM				04:45 PM			
+0 mins.	55	386	45	486	8	120	97	225	88	546	10	644	71	98	91	260
+15 mins.	53	413	31	497	7	121	76	204	91	508	15	614	71	87	97	255
+30 mins.	75	389	49	513	5	125	91	221	77	511	2	590	101	115	99	315
+45 mins.	65	469	50	584	15	129	85	229	82	497	4	583	55	126	109	290
Total Volume	248	1657	175	2080	35	495	349	879	338	2062	31	2431	298	426	396	1120
% App. Total	11.9	79.7	8.4		4	56.3	39.7		13.9	84.8	1.3		26.6	38	35.4	
PHF	.827	.883	.875	.890	.583	.959	.899	.960	.929	.944	.517	.944	.738	.845	.908	.889

City of Ontario
 N/S: South Haven Avenue
 E/W: East Airport Drive
 Weather: Clear

File Name : 12_ONT_S Haven_Airport PM
 Site Code : 99919787
 Start Date : 11/7/2019
 Page No : 1

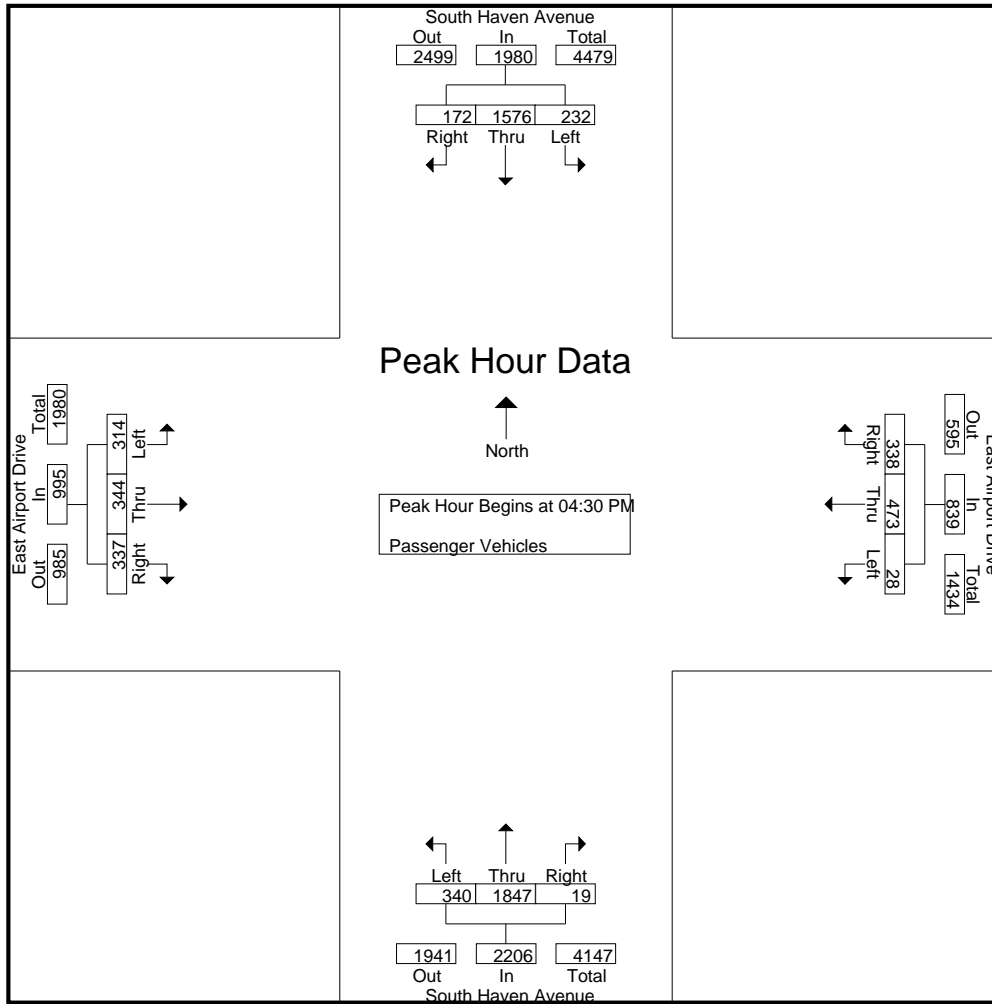
Groups Printed- Passenger Vehicles

Start Time	South Haven Avenue Southbound				East Airport Drive Westbound				South Haven Avenue Northbound				East Airport Drive Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
04:00 PM	40	341	36	417	24	100	93	217	86	520	4	610	72	87	51	210	1454
04:15 PM	45	318	35	398	4	94	71	169	83	481	12	576	61	82	100	243	1386
04:30 PM	49	369	44	462	7	114	92	213	75	496	2	573	75	83	62	220	1468
04:45 PM	50	387	30	467	5	114	73	192	80	489	4	573	71	89	88	248	1480
Total	184	1415	145	1744	40	422	329	791	324	1986	22	2332	279	341	301	921	5788
05:00 PM	73	369	48	490	5	121	89	215	100	432	5	537	68	74	93	235	1477
05:15 PM	60	451	50	561	11	124	84	219	85	430	8	523	100	98	94	292	1595
05:30 PM	35	330	44	409	15	110	51	176	84	500	6	590	54	117	103	274	1449
05:45 PM	38	313	41	392	14	86	60	160	65	373	1	439	84	92	57	233	1224
Total	206	1463	183	1852	45	441	284	770	334	1735	20	2089	306	381	347	1034	5745
Grand Total	390	2878	328	3596	85	863	613	1561	658	3721	42	4421	585	722	648	1955	11533
Apprch %	10.8	80	9.1		5.4	55.3	39.3		14.9	84.2	1		29.9	36.9	33.1		
Total %	3.4	25	2.8	31.2	0.7	7.5	5.3	13.5	5.7	32.3	0.4	38.3	5.1	6.3	5.6	17	

Start Time	South Haven Avenue Southbound				East Airport Drive Westbound				South Haven Avenue Northbound				East Airport Drive Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:30 PM to 05:15 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:30 PM																	
04:30 PM	49	369	44	462	7	114	92	213	75	496	2	573	75	83	62	220	1468
04:45 PM	50	387	30	467	5	114	73	192	80	489	4	573	71	89	88	248	1480
05:00 PM	73	369	48	490	5	121	89	215	100	432	5	537	68	74	93	235	1477
05:15 PM	60	451	50	561	11	124	84	219	85	430	8	523	100	98	94	292	1595
Total Volume	232	1576	172	1980	28	473	338	839	340	1847	19	2206	314	344	337	995	6020
% App. Total	11.7	79.6	8.7		3.3	56.4	40.3		15.4	83.7	0.9		31.6	34.6	33.9		
PHF	.795	.874	.860	.882	.636	.954	.918	.958	.850	.931	.594	.962	.785	.878	.896	.852	.944

City of Ontario
 N/S: South Haven Avenue
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Peak Hour Analysis From 04:30 PM to 05:15 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	04:30 PM				04:30 PM				04:30 PM				04:30 PM			
+0 mins.	49	369	44	462	7	114	92	213	75	496	2	573	75	83	62	220
+15 mins.	50	387	30	467	5	114	73	192	80	489	4	573	71	89	88	248
+30 mins.	73	369	48	490	5	121	89	215	100	432	5	537	68	74	93	235
+45 mins.	60	451	50	561	11	124	84	219	85	430	8	523	100	98	94	292
Total Volume	232	1576	172	1980	28	473	338	839	340	1847	19	2206	314	344	337	995
% App. Total	11.7	79.6	8.7		3.3	56.4	40.3		15.4	83.7	0.9		31.6	34.6	33.9	
PHF	.795	.874	.860	.882	.636	.954	.918	.958	.850	.931	.594	.962	.785	.878	.896	.852

City of Ontario
 N/S: South Haven Avenue
 E/W: East Airport Drive
 Weather: Clear

File Name : 12_ONT_S Haven_Airport PM
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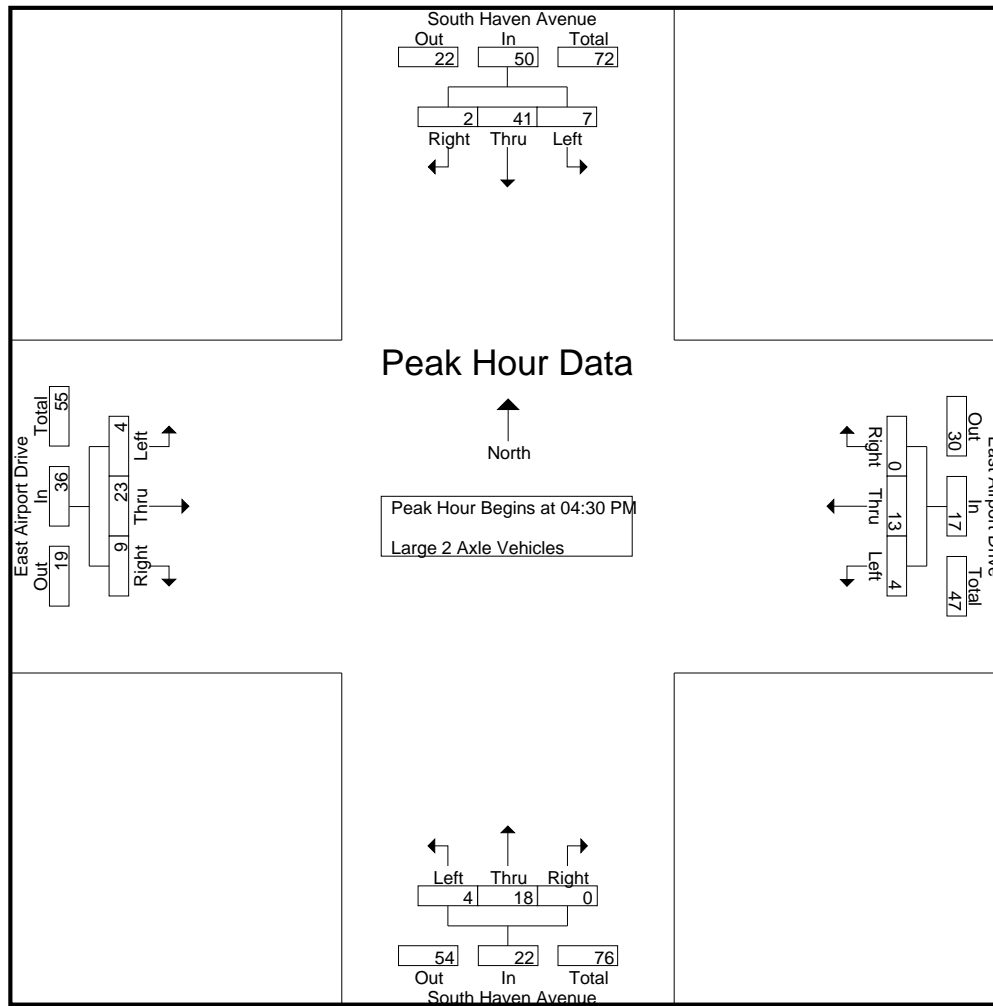
Groups Printed- Large 2 Axle Vehicles

Start Time	South Haven Avenue Southbound				East Airport Drive Westbound				South Haven Avenue Northbound				East Airport Drive Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
04:00 PM	0	4	0	4	0	2	1	3	1	8	3	12	4	5	1	10	29
04:15 PM	3	6	0	9	0	3	0	3	4	8	0	12	0	5	0	5	29
04:30 PM	3	8	1	12	1	4	0	5	1	4	0	5	1	5	0	6	28
04:45 PM	1	9	0	10	2	4	0	6	2	5	0	7	0	4	3	7	30
Total	7	27	1	35	3	13	1	17	8	25	3	36	5	19	4	28	116
05:00 PM	0	15	1	16	0	2	0	2	0	2	0	2	2	9	2	13	33
05:15 PM	3	9	0	12	1	3	0	4	1	7	0	8	1	5	4	10	34
05:30 PM	0	13	1	14	1	2	1	4	1	5	0	6	1	5	6	12	36
05:45 PM	0	14	1	15	1	2	2	5	0	8	0	8	0	7	3	10	38
Total	3	51	3	57	3	9	3	15	2	22	0	24	4	26	15	45	141
Grand Total	10	78	4	92	6	22	4	32	10	47	3	60	9	45	19	73	257
Apprch %	10.9	84.8	4.3		18.8	68.8	12.5		16.7	78.3	5		12.3	61.6	26		
Total %	3.9	30.4	1.6	35.8	2.3	8.6	1.6	12.5	3.9	18.3	1.2	23.3	3.5	17.5	7.4	28.4	

Start Time	South Haven Avenue Southbound				East Airport Drive Westbound				South Haven Avenue Northbound				East Airport Drive Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:30 PM to 05:15 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:30 PM																	
04:30 PM	3	8	1	12	1	4	0	5	1	4	0	5	1	5	0	6	28
04:45 PM	1	9	0	10	2	4	0	6	2	5	0	7	0	4	3	7	30
05:00 PM	0	15	1	16	0	2	0	2	0	2	0	2	2	9	2	13	33
05:15 PM	3	9	0	12	1	3	0	4	1	7	0	8	1	5	4	10	34
Total Volume	7	41	2	50	4	13	0	17	4	18	0	22	4	23	9	36	125
% App. Total	14	82	4		23.5	76.5	0		18.2	81.8	0		11.1	63.9	25		
PHF	.583	.683	.500	.781	.500	.813	.000	.708	.500	.643	.000	.688	.500	.639	.563	.692	.919

City of Ontario
 N/S: South Haven Avenue
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Peak Hour Analysis From 04:30 PM to 05:15 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	04:30 PM				04:30 PM				04:30 PM				04:30 PM			
+0 mins.	3	8	1	12	1	4	0	5	1	4	0	5	1	5	0	6
+15 mins.	1	9	0	10	2	4	0	6	2	5	0	7	0	4	3	7
+30 mins.	0	15	1	16	0	2	0	2	0	2	0	2	2	9	2	13
+45 mins.	3	9	0	12	1	3	0	4	1	7	0	8	1	5	4	10
Total Volume	7	41	2	50	4	13	0	17	4	18	0	22	4	23	9	36
% App. Total	14	82	4		23.5	76.5	0		18.2	81.8	0		11.1	63.9	25	
PHF	.583	.683	.500	.781	.500	.813	.000	.708	.500	.643	.000	.688	.500	.639	.563	.692

City of Ontario
 N/S: South Haven Avenue
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 Weather: Clear

File Name : 12_ONT_S Haven_Airport PM
 Site Code : 99919787
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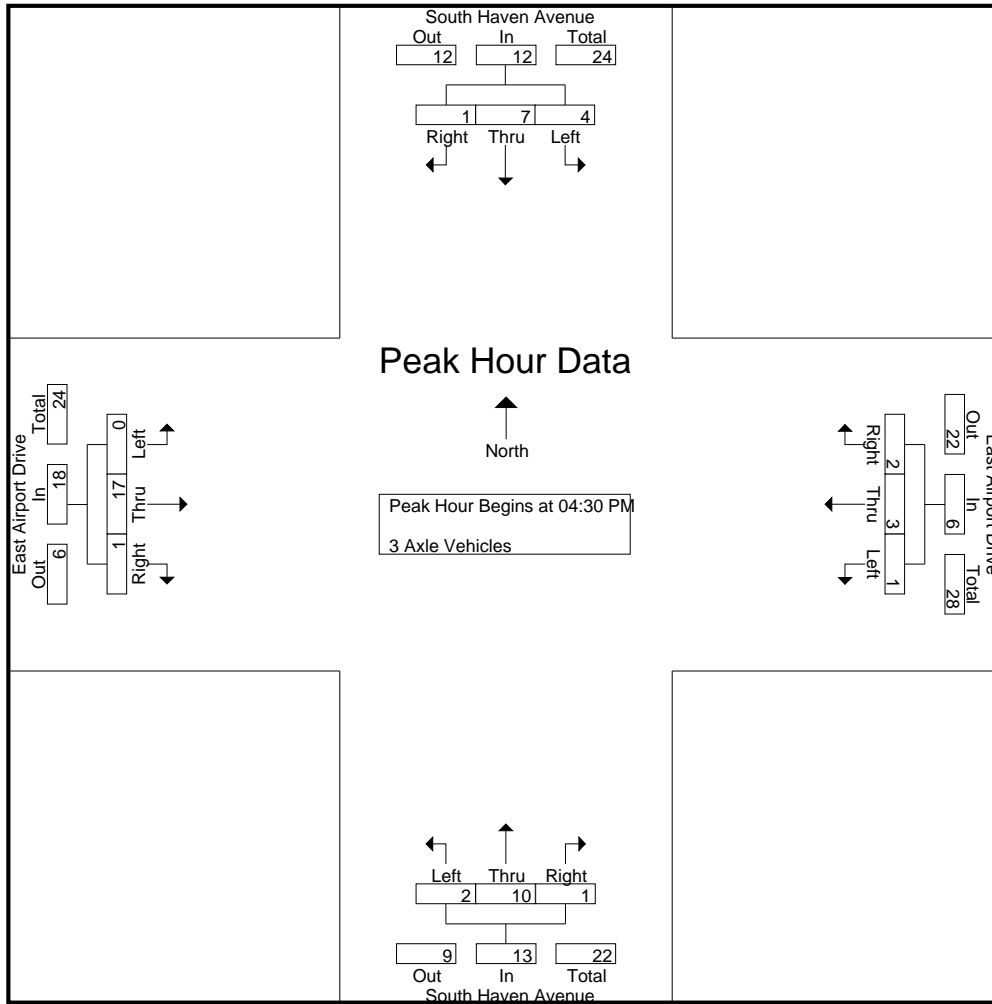
Groups Printed- 3 Axle Vehicles

Start Time	South Haven Avenue Southbound				East Airport Drive Westbound				South Haven Avenue Northbound				East Airport Drive Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
04:00 PM	0	1	0	1	0	2	0	2	1	5	0	6	0	1	0	1	10
04:15 PM	2	1	1	4	0	0	0	0	2	0	1	3	0	0	1	1	8
04:30 PM	1	2	0	3	0	2	1	3	1	3	0	4	0	2	0	2	12
04:45 PM	2	3	1	6	0	0	0	0	0	0	0	0	0	2	0	2	8
Total	5	7	2	14	0	4	1	5	4	8	1	13	0	5	1	6	38
05:00 PM	1	0	0	1	0	0	1	1	1	4	1	6	0	2	1	3	11
05:15 PM	0	2	0	2	1	1	0	2	0	3	0	3	0	11	0	11	18
05:30 PM	0	0	0	0	0	0	0	0	0	6	0	6	0	0	0	0	6
05:45 PM	0	5	0	5	1	1	0	2	0	0	0	0	0	0	2	2	9
Total	1	7	0	8	2	2	1	5	1	13	1	15	0	13	3	16	44
Grand Total	6	14	2	22	2	6	2	10	5	21	2	28	0	18	4	22	82
Apprch %	27.3	63.6	9.1		20	60	20		17.9	75	7.1		0	81.8	18.2		
Total %	7.3	17.1	2.4	26.8	2.4	7.3	2.4	12.2	6.1	25.6	2.4	34.1	0	22	4.9	26.8	

Start Time	South Haven Avenue Southbound				East Airport Drive Westbound				South Haven Avenue Northbound				East Airport Drive Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:30 PM to 05:15 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:30 PM																	
04:30 PM	1	2	0	3	0	2	1	3	1	3	0	4	0	2	0	2	12
04:45 PM	2	3	1	6	0	0	0	0	0	0	0	0	0	2	0	2	8
05:00 PM	1	0	0	1	0	0	1	1	1	4	1	6	0	2	1	3	11
05:15 PM	0	2	0	2	1	1	0	2	0	3	0	3	0	11	0	11	18
Total Volume	4	7	1	12	1	3	2	6	2	10	1	13	0	17	1	18	49
% App. Total	33.3	58.3	8.3		16.7	50	33.3		15.4	76.9	7.7		0	94.4	5.6		
PHF	.500	.583	.250	.500	.250	.375	.500	.500	.500	.625	.250	.542	.000	.386	.250	.409	.681

City of Ontario
 N/S: South Haven Avenue
 E/W: East Airport Drive
 Weather: Clear

File Name : 12_ONT_S Haven_Airport PM
 Site Code : 99919787
 Start Date : 11/7/2019
 Page No : 2



Peak Hour Analysis From 04:30 PM to 05:15 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	04:30 PM				04:30 PM				04:30 PM				04:30 PM			
+0 mins.	1	2	0	3	0	2	1	3	1	3	0	4	0	2	0	2
+15 mins.	2	3	1	6	0	0	0	0	0	0	0	0	0	2	0	2
+30 mins.	1	0	0	1	0	0	1	1	1	4	1	6	0	2	1	3
+45 mins.	0	2	0	2	1	1	0	2	0	3	0	3	0	11	0	11
Total Volume	4	7	1	12	1	3	2	6	2	10	1	13	0	17	1	18
% App. Total	33.3	58.3	8.3		16.7	50	33.3		15.4	76.9	7.7		0	94.4	5.6	
PHF	.500	.583	.250	.500	.250	.375	.500	.500	.500	.625	.250	.542	.000	.386	.250	.409

City of Ontario
 N/S: South Haven Avenue
 E/W: East Airport Drive
 Weather: Clear

File Name : 12_ONT_S Haven_Airport PM
 Site Code : 99919787
 Start Date : 11/7/2019
 Page No : 1

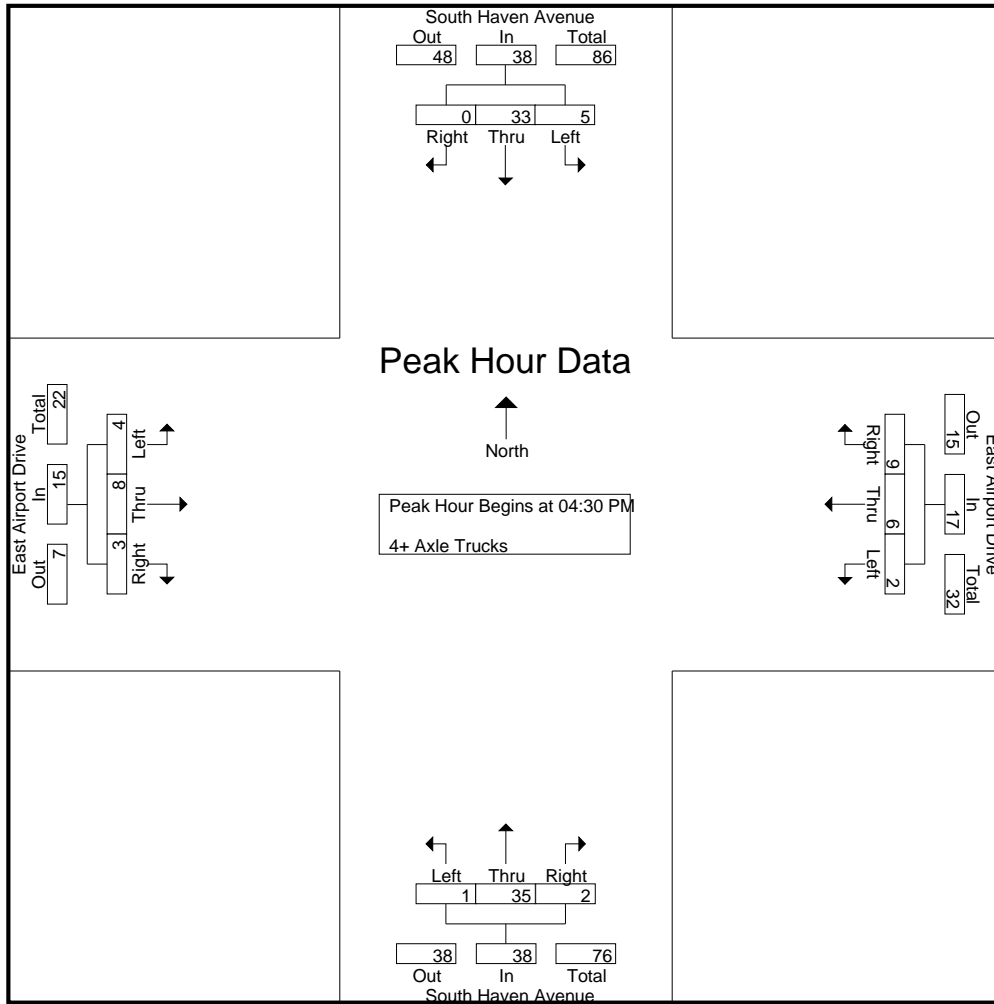
Groups Printed- 4+ Axle Trucks

Start Time	South Haven Avenue Southbound				East Airport Drive Westbound				South Haven Avenue Northbound				East Airport Drive Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
04:00 PM	0	11	1	12	0	2	1	3	0	13	3	16	0	2	0	2	33
04:15 PM	2	14	1	17	0	2	2	4	2	19	2	23	0	1	2	3	47
04:30 PM	2	7	0	9	0	0	4	4	0	8	0	8	3	2	1	6	27
04:45 PM	0	14	0	14	0	3	3	6	0	3	0	3	0	3	0	3	26
Total	4	46	2	52	0	7	10	17	2	43	5	50	3	8	3	14	133
05:00 PM	1	5	0	6	0	2	1	3	0	14	1	15	1	2	1	4	28
05:15 PM	2	7	0	9	2	1	1	4	1	10	1	12	0	1	1	2	27
05:30 PM	1	12	0	13	0	3	4	7	2	11	1	14	0	4	0	4	38
05:45 PM	4	8	0	12	2	2	1	5	0	9	0	9	0	2	1	3	29
Total	8	32	0	40	4	8	7	19	3	44	3	50	1	9	3	13	122
Grand Total	12	78	2	92	4	15	17	36	5	87	8	100	4	17	6	27	255
Apprch %	13	84.8	2.2		11.1	41.7	47.2		5	87	8		14.8	63	22.2		
Total %	4.7	30.6	0.8	36.1	1.6	5.9	6.7	14.1	2	34.1	3.1	39.2	1.6	6.7	2.4	10.6	

Start Time	South Haven Avenue Southbound				East Airport Drive Westbound				South Haven Avenue Northbound				East Airport Drive Eastbound				Int. Total
	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	Left	Thru	Right	App. Total	
Peak Hour Analysis From 04:30 PM to 05:15 PM - Peak 1 of 1																	
Peak Hour for Entire Intersection Begins at 04:30 PM																	
04:30 PM	2	7	0	9	0	0	4	4	0	8	0	8	3	2	1	6	27
04:45 PM	0	14	0	14	0	3	3	6	0	3	0	3	0	3	0	3	26
05:00 PM	1	5	0	6	0	2	1	3	0	14	1	15	1	2	1	4	28
05:15 PM	2	7	0	9	2	1	1	4	1	10	1	12	0	1	1	2	27
Total Volume	5	33	0	38	2	6	9	17	1	35	2	38	4	8	3	15	108
% App. Total	13.2	86.8	0		11.8	35.3	52.9		2.6	92.1	5.3		26.7	53.3	20		
PHF	.625	.589	.000	.679	.250	.500	.563	.708	.250	.625	.500	.633	.333	.667	.750	.625	.964

City of Ontario
 N/S: South Haven Avenue
 E/W: East Airport Drive
 Weather: Clear

File Name : 12_ONT_S Haven_Airport PM
 Site Code : 99919787
 Start Date : 11/7/2019
 Page No : 2




































Peak Hour Analysis From 04:30 PM to 05:15 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:


































	04:30 PM				04:30 PM				04:30 PM				04:30 PM			
+0 mins.	2	7	0	9	0	0	4	4	0	8	0	8	3	2	1	6
+15 mins.	0	14	0	14	0	3	3	6	0	3	0	3	0	3	0	3
+30 mins.	1	5	0	6	0	2	1	3	0	14	1	15	1	2	1	4
+45 mins.	2	7	0	9	2	1	1	4	1	10	1	12	0	1	1	2
Total Volume	5	33	0	38	2	6	9	17	1	35	2	38	4	8	3	15
% App. Total	13.2	86.8	0		11.8	35.3	52.9		2.6	92.1	5.3		26.7	53.3	20	
PHF	.625	.589	.000	.679	.250	.500	.563	.708	.250	.625	.500	.633	.333	.667	.750	.625

HCM ANALYSIS WORKSHEETS

3555 E Airport Drive Focused Traffic Analysis Project Opening Year AM Peak Hour Conditions
 1: Haven Ave & Airport Dr HCM 6th Signalized Intersection Summary




































												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	 	 		 	 		 	  		 	  	
Traffic Volume (veh/h)	229	285	259	63	259	186	407	2051	53	269	1364	192
Future Volume (veh/h)	229	285	259	63	259	186	407	2051	53	269	1364	192
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	246	306	278	68	278	200	438	2205	57	289	1467	206
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	333	403	357	164	629	281	428	2843	700	305	2616	644
Arrive On Green	0.09	0.22	0.22	0.05	0.17	0.17	0.12	0.43	0.43	0.09	0.40	0.40
Sat Flow, veh/h	3510	1811	1605	3510	3610	1610	3510	6536	1610	3510	6536	1610
Grp Volume(v), veh/h	246	305	279	68	278	200	438	2205	57	289	1467	206
Grp Sat Flow(s),veh/h/ln	1755	1805	1611	1755	1805	1610	1755	1634	1610	1755	1634	1610
Q Serve(g_s), s	5.9	13.6	14.0	1.6	5.9	10.1	10.5	24.8	1.8	7.1	15.0	7.6
Cycle Q Clear(g_c), s	5.9	13.6	14.0	1.6	5.9	10.1	10.5	24.8	1.8	7.1	15.0	7.6
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	333	402	359	164	629	281	428	2843	700	305	2616	644
V/C Ratio(X)	0.74	0.76	0.78	0.42	0.44	0.71	1.02	0.78	0.08	0.95	0.56	0.32
Avail Cap(c_a), veh/h	517	775	691	204	1227	547	428	2843	700	305	2616	644
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	38.0	31.3	31.5	40.0	31.8	33.6	37.9	20.8	14.3	39.2	20.0	17.8
Incr Delay (d2), s/veh	3.2	3.0	3.7	1.7	0.5	3.4	49.9	2.1	0.2	37.4	0.9	1.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.5	5.8	5.4	0.7	2.5	4.0	7.1	8.4	0.6	4.5	5.3	2.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	41.2	34.3	35.2	41.6	32.3	36.9	87.7	22.9	14.5	76.5	20.9	19.1
LnGrp LOS	D	C	D	D	C	D	F	C	B	E	C	B
Approach Vol, veh/h		830			546			2700			1962	
Approach Delay, s/veh		36.6			35.2			33.2			28.9	
Approach LOS		D			D			C			C	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	12.0	42.0	8.5	23.7	15.0	39.0	12.7	19.5				
Change Period (Y+Rc), s	4.5	4.5	4.5	4.5	4.5	4.5	4.5	4.5				
Max Green Setting (Gmax), s	7.5	37.5	5.0	37.0	10.5	34.5	12.7	29.3				
Max Q Clear Time (g_c+I1), s	9.1	26.8	3.6	16.0	12.5	17.0	7.9	12.1				
Green Ext Time (p_c), s	0.0	8.8	0.0	3.2	0.0	9.9	0.3	2.1				
Intersection Summary												
HCM 6th Ctrl Delay				32.5								
HCM 6th LOS				C								

3555 E Airport Drive Focused Traffic Analysis Project Opening Year PM Peak Hour Conditions
 1: Haven Ave & Airport Dr HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	 	 		 	 		 	  		 	  	
Traffic Volume (veh/h)	355	488	389	59	565	410	378	2136	34	294	1884	190
Future Volume (veh/h)	355	488	389	59	565	410	378	2136	34	294	1884	190
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	378	519	414	63	601	436	402	2272	36	313	2004	202
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	445	673	536	142	960	428	403	2362	582	254	2085	514
Arrive On Green	0.13	0.35	0.35	0.04	0.27	0.27	0.11	0.36	0.36	0.07	0.32	0.32
Sat Flow, veh/h	3510	1909	1522	3510	3610	1610	3510	6536	1610	3510	6536	1610
Grp Volume(v), veh/h	378	491	442	63	601	436	402	2272	36	313	2004	202
Grp Sat Flow(s),veh/h/ln	1755	1805	1626	1755	1805	1610	1755	1634	1610	1755	1634	1610
Q Serve(g_s), s	10.9	25.1	25.1	1.8	15.2	27.6	11.9	35.3	1.5	7.5	31.2	10.1
Cycle Q Clear(g_c), s	10.9	25.1	25.1	1.8	15.2	27.6	11.9	35.3	1.5	7.5	31.2	10.1
Prop In Lane	1.00		0.94	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	445	636	573	142	960	428	403	2362	582	254	2085	514
V/C Ratio(X)	0.85	0.77	0.77	0.44	0.63	1.02	1.00	0.96	0.06	1.23	0.96	0.39
Avail Cap(c_a), veh/h	487	644	580	169	960	428	403	2362	582	254	2085	514
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	44.3	29.9	29.9	48.6	33.5	38.1	45.9	32.4	21.6	48.1	34.7	27.5
Incr Delay (d2), s/veh	12.5	5.7	6.3	2.2	1.3	48.1	44.4	11.5	0.2	134.3	12.5	2.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	5.3	11.0	10.0	0.8	6.5	16.0	7.4	14.4	0.6	7.9	13.4	4.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	56.8	35.6	36.2	50.8	34.8	86.1	90.3	43.9	21.8	182.5	47.1	29.8
LnGrp LOS	E	D	D	D	C	F	F	D	C	F	D	C
Approach Vol, veh/h		1311			1100			2710			2519	
Approach Delay, s/veh		41.9			56.1			50.5			62.6	
Approach LOS		D			E			D			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	12.0	42.0	8.7	41.1	16.4	37.6	17.6	32.1				
Change Period (Y+Rc), s	4.5	4.5	4.5	4.5	4.5	4.5	4.5	4.5				
Max Green Setting (Gmax), s	7.5	37.5	5.0	37.0	11.9	33.1	14.4	27.6				
Max Q Clear Time (g_c+I1), s	9.5	37.3	3.8	27.1	13.9	33.2	12.9	29.6				
Green Ext Time (p_c), s	0.0	0.2	0.0	3.9	0.0	0.0	0.2	0.0				
Intersection Summary												
HCM 6th Ctrl Delay			53.8									
HCM 6th LOS			D									




































3555 E Airport Drive Focused Traffic Analysis
1: Haven Ave & Airport Dr

HCM 6th Signalized Intersection Summary
Project Opening Year AM Peak Hour Conditions (2WBR)

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	 	 		 	 	 	 	  		  	  	
Traffic Volume (veh/h)	229	285	259	63	259	186	407	2051	53	269	1364	192
Future Volume (veh/h)	229	285	259	63	259	186	407	2051	53	269	1364	192
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	246	306	278	68	278	200	438	2205	57	289	1467	206
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	333	403	357	164	629	740	428	2843	700	305	2616	644
Arrive On Green	0.09	0.22	0.22	0.05	0.17	0.17	0.12	0.43	0.43	0.09	0.40	0.40
Sat Flow, veh/h	3510	1811	1605	3510	3610	2834	3510	6536	1610	3510	6536	1610
Grp Volume(v), veh/h	246	305	279	68	278	200	438	2205	57	289	1467	206
Grp Sat Flow(s),veh/h/ln	1755	1805	1611	1755	1805	1417	1755	1634	1610	1755	1634	1610
Q Serve(g_s), s	5.9	13.6	14.0	1.6	5.9	4.8	10.5	24.8	1.8	7.1	15.0	7.6
Cycle Q Clear(g_c), s	5.9	13.6	14.0	1.6	5.9	4.8	10.5	24.8	1.8	7.1	15.0	7.6
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	333	402	359	164	629	740	428	2843	700	305	2616	644
V/C Ratio(X)	0.74	0.76	0.78	0.42	0.44	0.27	1.02	0.78	0.08	0.95	0.56	0.32
Avail Cap(c_a), veh/h	517	775	691	204	1227	1210	428	2843	700	305	2616	644
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	38.0	31.3	31.5	40.0	31.8	25.3	37.9	20.8	14.3	39.2	20.0	17.8
Incr Delay (d2), s/veh	3.2	3.0	3.7	1.7	0.5	0.2	49.9	2.1	0.2	37.4	0.9	1.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.5	5.8	5.4	0.7	2.5	1.5	7.1	8.4	0.6	4.5	5.3	2.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	41.2	34.3	35.2	41.6	32.3	25.5	87.7	22.9	14.5	76.5	20.9	19.1
LnGrp LOS	D	C	D	D	C	C	F	C	B	E	C	B
Approach Vol, veh/h		830			546			2700			1962	
Approach Delay, s/veh		36.6			31.0			33.2			28.9	
Approach LOS		D			C			C			C	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	12.0	42.0	8.5	23.7	15.0	39.0	12.7	19.5				
Change Period (Y+Rc), s	4.5	4.5	4.5	4.5	4.5	4.5	4.5	4.5				
Max Green Setting (Gmax), s	7.5	37.5	5.0	37.0	10.5	34.5	12.7	29.3				
Max Q Clear Time (g_c+I1), s	9.1	26.8	3.6	16.0	12.5	17.0	7.9	7.9				
Green Ext Time (p_c), s	0.0	8.8	0.0	3.2	0.0	9.9	0.3	2.3				
Intersection Summary												
HCM 6th Ctrl Delay				32.1								
HCM 6th LOS				C								





































3555 E Airport Drive Focused Traffic Analysis
1: Haven Ave & Airport Dr

HCM 6th Signalized Intersection Summary
Project Opening Year PM Peak Hour Conditions (2WBR)

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	 	 		 	 	 	 	  		  	  	
Traffic Volume (veh/h)	355	488	389	59	565	410	378	2136	34	294	1884	190
Future Volume (veh/h)	355	488	389	59	565	410	378	2136	34	294	1884	190
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	378	519	414	63	601	436	402	2272	36	313	2004	202
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	451	602	480	147	826	648	426	2500	616	269	2207	544
Arrive On Green	0.13	0.32	0.32	0.04	0.23	0.23	0.12	0.38	0.38	0.08	0.34	0.34
Sat Flow, veh/h	3510	1909	1522	3510	3610	2834	3510	6536	1610	3510	6536	1610
Grp Volume(v), veh/h	378	491	442	63	601	436	402	2272	36	313	2004	202
Grp Sat Flow(s),veh/h/ln	1755	1805	1626	1755	1805	1417	1755	1634	1610	1755	1634	1610
Q Serve(g_s), s	10.3	25.1	25.1	1.7	15.1	13.7	11.1	32.3	1.4	7.5	28.7	9.3
Cycle Q Clear(g_c), s	10.3	25.1	25.1	1.7	15.1	13.7	11.1	32.3	1.4	7.5	28.7	9.3
Prop In Lane	1.00		0.94	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	451	570	513	147	826	648	426	2500	616	269	2207	544
V/C Ratio(X)	0.84	0.86	0.86	0.43	0.73	0.67	0.94	0.91	0.06	1.17	0.91	0.37
Avail Cap(c_a), veh/h	516	681	614	179	1016	798	426	2500	616	269	2207	544
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	41.7	31.5	31.5	45.8	35.0	34.5	42.7	28.6	19.1	45.3	31.0	24.6
Incr Delay (d2), s/veh	10.5	9.6	10.5	2.0	2.1	1.6	29.6	6.2	0.2	107.2	6.9	1.9
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	4.9	11.5	10.5	0.8	6.5	4.6	6.3	12.2	0.5	7.1	11.5	3.6
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	52.2	41.1	42.1	47.8	37.0	36.1	72.4	34.9	19.3	152.5	37.9	26.5
LnGrp LOS	D	D	D	D	D	D	E	C	B	F	D	C
Approach Vol, veh/h		1311			1100			2710			2519	
Approach Delay, s/veh		44.6			37.3			40.2			51.2	
Approach LOS		D			D			D			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	12.0	42.0	8.6	35.4	16.4	37.6	17.1	26.9				
Change Period (Y+Rc), s	4.5	4.5	4.5	4.5	4.5	4.5	4.5	4.5				
Max Green Setting (Gmax), s	7.5	37.5	5.0	37.0	11.9	33.1	14.4	27.6				
Max Q Clear Time (g_c+I1), s	9.5	34.3	3.7	27.1	13.1	30.7	12.3	17.1				
Green Ext Time (p_c), s	0.0	3.0	0.0	3.9	0.0	2.2	0.3	4.0				
Intersection Summary												
HCM 6th Ctrl Delay			44.2									
HCM 6th LOS			D									





































3555 E Airport Drive Focused Traffic Analysis
1: Haven Ave & Airport Dr

Buildout Year AM Peak Hour Conditions
HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	 	  		 	  		 	  		 	  	
Traffic Volume (veh/h)	327	404	370	89	369	264	581	2929	71	377	1948	275
Future Volume (veh/h)	327	404	370	89	369	264	581	2929	71	377	1948	275
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	327	404	370	89	369	264	581	2929	71	377	1948	275
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	394	1246	684	138	868	449	648	3018	744	391	2539	625
Arrive On Green	0.11	0.24	0.24	0.04	0.17	0.17	0.18	0.46	0.46	0.11	0.39	0.39
Sat Flow, veh/h	3510	5187	1610	3510	5187	1610	3510	6536	1610	3510	6536	1610
Grp Volume(v), veh/h	327	404	370	89	369	264	581	2929	71	377	1948	275
Grp Sat Flow(s),veh/h/ln	1755	1729	1610	1755	1729	1610	1755	1634	1610	1755	1634	1610
Q Serve(g_s), s	11.1	7.8	21.0	3.1	7.8	17.3	19.8	53.4	3.0	13.1	31.7	15.4
Cycle Q Clear(g_c), s	11.1	7.8	21.0	3.1	7.8	17.3	19.8	53.4	3.0	13.1	31.7	15.4
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	394	1246	684	138	868	449	648	3018	744	391	2539	625
V/C Ratio(X)	0.83	0.32	0.54	0.64	0.42	0.59	0.90	0.97	0.10	0.96	0.77	0.44
Avail Cap(c_a), veh/h	543	1571	785	144	981	484	730	3018	744	391	2539	625
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	53.1	38.2	26.2	57.8	45.6	38.0	48.7	32.1	18.5	54.0	32.5	27.5
Incr Delay (d2), s/veh	7.7	0.1	0.7	9.0	0.3	1.6	12.8	10.8	0.3	36.2	2.3	2.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	5.2	3.2	7.8	1.5	3.3	6.8	9.4	21.2	1.1	7.6	12.3	6.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	60.8	38.4	26.9	66.8	45.9	39.6	61.5	42.9	18.8	90.2	34.8	29.8
LnGrp LOS	E	D	C	E	D	D	E	D	B	F	C	C
Approach Vol, veh/h		1101			722			3581			2600	
Approach Delay, s/veh		41.2			46.2			45.4			42.3	
Approach LOS		D			D			D			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	18.1	60.9	9.3	33.8	27.1	51.9	18.2	24.9				
Change Period (Y+Rc), s	4.5	4.5	4.5	4.5	4.5	4.5	4.5	4.5				
Max Green Setting (Gmax), s	13.6	56.4	5.0	37.0	25.4	44.6	18.9	23.1				
Max Q Clear Time (g_c+I1), s	15.1	55.4	5.1	23.0	21.8	33.7	13.1	19.3				
Green Ext Time (p_c), s	0.0	1.0	0.0	3.2	0.8	8.8	0.6	1.2				
Intersection Summary												
HCM 6th Ctrl Delay			43.9									
HCM 6th LOS			D									
Notes												
User approved changes to right turn type.												

3555 E Airport Drive Focused Traffic Analysis
1: Haven Ave & Airport Dr

Buildout Year PM Peak Hour Conditions
HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	 	  		 	  		 	  		 	  	
Traffic Volume (veh/h)	507	695	556	79	803	578	539	3051	47	417	2691	272
Future Volume (veh/h)	507	695	556	79	803	578	539	3051	47	417	2691	272
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	507	695	556	79	803	578	539	3051	47	417	2691	272
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	510	1432	682	123	861	459	517	2852	703	419	2668	657
Arrive On Green	0.15	0.28	0.28	0.04	0.17	0.17	0.15	0.44	0.44	0.12	0.41	0.41
Sat Flow, veh/h	3510	5187	1610	3510	5187	1610	3510	6536	1610	3510	6536	1610
Grp Volume(v), veh/h	507	695	556	79	803	578	539	3051	47	417	2691	272
Grp Sat Flow(s),veh/h/ln	1755	1729	1610	1755	1729	1610	1755	1634	1610	1755	1634	1610
Q Serve(g_s), s	19.5	15.1	37.3	3.0	20.6	22.4	19.9	58.9	2.3	16.0	55.1	16.2
Cycle Q Clear(g_c), s	19.5	15.1	37.3	3.0	20.6	22.4	19.9	58.9	2.3	16.0	55.1	16.2
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	510	1432	682	123	861	459	517	2852	703	419	2668	657
V/C Ratio(X)	0.99	0.49	0.82	0.64	0.93	1.26	1.04	1.07	0.07	1.00	1.01	0.41
Avail Cap(c_a), veh/h	510	1432	682	130	861	459	517	2852	703	419	2668	657
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	57.6	40.9	34.3	64.3	55.6	48.2	57.5	38.0	22.1	59.4	40.0	28.5
Incr Delay (d2), s/veh	38.4	0.3	7.6	9.5	16.8	133.1	50.8	39.2	0.2	42.9	19.6	1.9
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	11.1	6.3	16.8	1.5	10.1	31.9	12.1	29.4	0.9	9.5	24.6	6.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	96.1	41.1	41.9	73.8	72.3	181.4	108.4	77.3	22.3	102.3	59.5	30.4
LnGrp LOS	F	D	D	E	E	F	F	F	C	F	F	C
Approach Vol, veh/h		1758			1460			3637			3380	
Approach Delay, s/veh		57.2			115.6			81.2			62.5	
Approach LOS		E			F			F			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	20.6	63.4	9.2	41.8	24.4	59.6	24.1	26.9				
Change Period (Y+Rc), s	4.5	4.5	4.5	4.5	4.5	4.5	4.5	4.5				
Max Green Setting (Gmax), s	16.1	58.9	5.0	37.0	19.9	55.1	19.6	22.4				
Max Q Clear Time (g_c+I1), s	18.0	60.9	5.0	39.3	21.9	57.1	21.5	24.4				
Green Ext Time (p_c), s	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0				
Intersection Summary												
HCM 6th Ctrl Delay			75.8									
HCM 6th LOS			E									




































3555 E Airport Drive Focused Traffic Analysis
1: Haven Ave & Airport Dr

HCM 6th Signalized Intersection Summary
Buildout Year AM Peak Hour Conditions (2 WBR)

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	327	404	370	89	369	264	581	2929	71	377	1948	275
Future Volume (veh/h)	327	404	370	89	369	264	581	2929	71	377	1948	275
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	327	404	370	89	369	264	581	2929	71	377	1948	275
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	396	1143	654	140	765	742	652	3099	763	401	2632	648
Arrive On Green	0.11	0.22	0.22	0.04	0.15	0.15	0.19	0.47	0.47	0.11	0.40	0.40
Sat Flow, veh/h	3510	5187	1610	3510	5187	2834	3510	6536	1610	3510	6536	1610
Grp Volume(v), veh/h	327	404	370	89	369	264	581	2929	71	377	1948	275
Grp Sat Flow(s),veh/h/ln	1755	1729	1610	1755	1729	1417	1755	1634	1610	1755	1634	1610
Q Serve(g_s), s	10.8	7.8	21.1	3.0	7.8	9.0	19.2	50.8	2.9	12.7	30.2	14.6
Cycle Q Clear(g_c), s	10.8	7.8	21.1	3.0	7.8	9.0	19.2	50.8	2.9	12.7	30.2	14.6
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	396	1143	654	140	765	742	652	3099	763	401	2632	648
V/C Ratio(X)	0.83	0.35	0.57	0.64	0.48	0.36	0.89	0.95	0.09	0.94	0.74	0.42
Avail Cap(c_a), veh/h	558	1613	800	148	1007	874	750	3099	763	401	2632	648
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	51.6	39.2	27.2	56.3	46.5	35.7	47.3	29.8	17.2	52.3	30.2	25.6
Incr Delay (d2), s/veh	7.0	0.2	0.8	8.1	0.5	0.3	11.8	7.6	0.2	30.0	1.9	2.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	5.0	3.2	7.9	1.4	3.3	3.1	9.0	19.4	1.1	7.1	11.6	5.7
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	58.6	39.4	28.0	64.4	47.0	36.0	59.0	37.5	17.4	82.2	32.2	27.6
LnGrp LOS	E	D	C	E	D	D	E	D	B	F	C	C
Approach Vol, veh/h		1101			722			3581			2600	
Approach Delay, s/veh		41.3			45.1			40.6			38.9	
Approach LOS		D			D			D			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	18.1	60.9	9.2	30.7	26.6	52.4	17.9	22.0				
Change Period (Y+Rc), s	4.5	4.5	4.5	4.5	4.5	4.5	4.5	4.5				
Max Green Setting (Gmax), s	13.6	56.4	5.0	37.0	25.4	44.6	18.9	23.1				
Max Q Clear Time (g_c+I1), s	14.7	52.8	5.0	23.1	21.2	32.2	12.8	11.0				
Green Ext Time (p_c), s	0.0	3.5	0.0	3.1	0.9	9.8	0.6	2.6				
Intersection Summary												
HCM 6th Ctrl Delay			40.5									
HCM 6th LOS			D									

3555 E Airport Drive Focused Traffic Analysis
1: Haven Ave & Airport Dr

HCM 6th Signalized Intersection Summary
Buildout Year PM Peak Hour Conditions (2 WBR)

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	 	  		 	  	 	 	  		 	 	
Traffic Volume (veh/h)	507	695	556	79	803	578	539	3051	47	417	2691	272
Future Volume (veh/h)	507	695	556	79	803	578	539	3051	47	417	2691	272
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	507	695	556	79	803	578	539	3051	47	417	2691	272
Peak Hour Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Percent Heavy Veh, %	0	0	0	0	0	0	0	0	0	0	0	0
Cap, veh/h	510	1432	682	123	861	796	517	2881	710	403	2668	657
Arrive On Green	0.15	0.28	0.28	0.04	0.17	0.17	0.15	0.44	0.44	0.11	0.41	0.41
Sat Flow, veh/h	3510	5187	1610	3510	5187	2834	3510	6536	1610	3510	6536	1610
Grp Volume(v), veh/h	507	695	556	79	803	578	539	3051	47	417	2691	272
Grp Sat Flow(s),veh/h/ln	1755	1729	1610	1755	1729	1417	1755	1634	1610	1755	1634	1610
Q Serve(g_s), s	19.5	15.1	37.3	3.0	20.6	22.4	19.9	59.5	2.3	15.5	55.1	16.2
Cycle Q Clear(g_c), s	19.5	15.1	37.3	3.0	20.6	22.4	19.9	59.5	2.3	15.5	55.1	16.2
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	510	1432	682	123	861	796	517	2881	710	403	2668	657
V/C Ratio(X)	0.99	0.49	0.82	0.64	0.93	0.73	1.04	1.06	0.07	1.03	1.01	0.41
Avail Cap(c_a), veh/h	510	1432	682	130	861	796	517	2881	710	403	2668	657
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	57.6	40.9	34.3	64.3	55.6	43.9	57.5	37.7	21.7	59.7	40.0	28.5
Incr Delay (d2), s/veh	38.4	0.3	7.6	9.5	16.8	3.3	50.8	35.1	0.2	54.0	19.6	1.9
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	11.1	6.3	16.8	1.5	10.1	8.9	12.1	28.8	0.9	9.8	24.6	6.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	96.1	41.1	41.9	73.8	72.3	47.2	108.4	72.8	21.9	113.8	59.5	30.4
LnGrp LOS	F	D	D	E	E	D	F	F	C	F	F	C
Approach Vol, veh/h		1758			1460			3637			3380	
Approach Delay, s/veh		57.2			62.5			77.4			63.9	
Approach LOS		E			E			E			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	20.0	64.0	9.2	41.8	24.4	59.6	24.1	26.9				
Change Period (Y+Rc), s	4.5	4.5	4.5	4.5	4.5	4.5	4.5	4.5				
Max Green Setting (Gmax), s	15.5	59.5	5.0	37.0	19.9	55.1	19.6	22.4				
Max Q Clear Time (g_c+I1), s	17.5	61.5	5.0	39.3	21.9	57.1	21.5	24.4				
Green Ext Time (p_c), s	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0				
Intersection Summary												
HCM 6th Ctrl Delay			67.4									
HCM 6th LOS			E									

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING CITY COUNCIL APPROVE FILE NO. PSPA20-003, AN AMENDMENT TO THE CALIFORNIA COMMERCE CENTER SPECIFIC PLAN, CHANGING THE LAND USE DESIGNATION ON 10.64 ACRES OF LAND FROM COMMERCIAL/FOOD/HOTEL TO LIGHT INDUSTRIAL, TO BE CONSISTENT WITH THE ONTARIO PLAN POLICY PLAN (GENERAL PLAN) INDUSTRIAL (0.55 FAR) LAND USE DESIGNATION, LOCATED AT THE NORTHEAST CORNER OF HAVEN AVENUE AND AIRPORT DRIVE, WITHIN THE CALIFORNIA COMMERCE CENTER SPECIFIC PLAN, AND MAKING FINDINGS IN SUPPORT THEREOF—APN: 0211-222-66.

WHEREAS, VOGEL PROPERTIES, INC. (hereinafter referred to as "Applicant") has filed an Application for the approval of a Specific Plan Amendment, File No. PSPA20-003, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 10.64 acres of land generally located on the southeast corner of Haven Avenue and Airport Drive, within the proposed Light Industrial land use designation of the California Commerce Center Specific Plan and is developed as a parking lot; and

WHEREAS, the property to the north of the Project site is within the Office/Commercial land use designation of the Ontario Gateway Specific Plan and is developed with a retail land uses (Costco and Starbucks). The property to the east is within the Rail Industrial land use designation of the California Commerce Center Specific Plan and is developed with industrial land uses. The property to the south is within the Light Industrial land use designation of the California Commerce Center Specific Plan and is currently vacant. The property to the west is within the ONT (Ontario International Airport) zoning district and is currently vacant; and

WHEREAS, the Specific Plan Amendment will change the land use designation on the subject site from Commercial/Food/Hotel to Light Industrial within the California Commerce Center Specific Plan, to be consistent with The Ontario Plan Policy Plan (General Plan), which designates the subject site for Industrial (maximum 0.55 FAR) land uses; and

WHEREAS, a focused Traffic Impact Analysis was prepared for the project site by TJW Engineering (Dated: August 27, 2020), which compared the trip generation between the existing California Commerce Center Specific Plan Commercial/Food/Hotel land use designation (shopping center/commercial) versus the proposed Light Industrial land use

at a FAR of 0.55 (warehouse/small ancillary office). The trip generation analysis utilized the Industry standard Institute of Transportation Engineers (“ITE”) Trip Generation Manual (10th Edition, 2017) to determine trip generation rates for the existing and proposed land uses and represents the amount of traffic, both inbound and outbound, produced by each land use. The Traffic Impact Analysis concluded that proposed change in land use from Commercial/Food/Hotel to Light Industrial would result in a net difference of 7,183 fewer Average Daily Trips (ADT) trips, including 146 fewer AM peak hour trips, and 719 fewer PM peak hour trips; and

WHEREAS, the Applicant submitted a Development Plan (File No. PDEV20-008) in conjunction with the Amendment to the Specific Plan Amendment to facilitate the construction of a 200,291-square foot industrial building and approval of the Development Plan would not be final and conclusive until such time that the City Council approves the subject Specific Plan Amendment and related Addendum to The Ontario Plan Environmental Impact Report; and

WHEREAS, The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was certified on January 27, 2010 (hereinafter referred to as “Certified EIR”), in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as “EIR Addendum”) in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as “CEQA”); and

WHEREAS, the environmental impacts of this project were thoroughly analyzed in the EIR Addendum, which concluded that implementation of the Project could result in a number of significant effects on the environment that were previously analyzed in the Certified EIR, and that the Certified EIR identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an EIR Addendum has been prepared to determine possible environmental impacts; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and make recommendation to the City Council on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 17, 2021, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Addendum and the Project, and concluded said hearing on that date, voting to issue Decision No. DAB21-002, recommending that the Planning Commission recommend the City Council approve the Addendum and Decision No. DAB21-023, recommending the Planning Commission approve the Project (File No. PDEV20-008); and

WHEREAS, as the first action on the Project, on May 25, 2021, the Planning Commission issued a Resolution recommending the City Council adopt the EIR Addendum, finding that the proposed Project introduces no new significant environmental impacts and applying all previously adopted mitigation measures to the Project, which were incorporated by reference; and

WHEREAS, on May 25, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the recommending body for the Project, the Planning Commission has reviewed and considered the information contained in the Addendum, the initial study, and the administrative record for the Project, including all written and oral evidence provided during the comment period. Based upon the facts and information contained in the Addendum, the initial study, and the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The environmental impacts of this project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report — State Clearinghouse No. 2008101140 (“Certified EIR”), which was certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001; and

(2) The EIR Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts; and

(4) All previously adopted mitigation measures shall be a condition of project approval, as they are applicable to the Project, and are incorporated herein by this reference; and

(5) The EIR Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and

(6) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts.

SECTION 2: Subsequent or Supplemental Environmental Review Not Required. Based on the EIR Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and.

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The Project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: *Housing Element Compliance.* Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the recommending authority for the Project, the Planning Commission finds that based upon the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 4: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted

Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as "ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the recommending authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 5: Concluding Facts and Reasons. Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 and 4, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed Specific Plan, or amendment thereto, is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed California Commerce Center Specific Plan amendment will provide consistency between the Policy Plan Land Use Plan and the California Commerce Center Specific Plan Light Industrial land use designation. The proposed amendment will accommodate the construction of industrial land uses within the Specific Plan which is consistent with goals, policies, plans and City Council priorities of The Ontario Plan.

(2) ***The proposed Specific Plan, or amendment thereto, would not be detrimental to the public interest, health, safety, convenience, or general welfare of the City.*** The proposed amendment to the California Commerce Center Specific Plan will provide consistency between the Policy Plan Land Use Plan and the California Commerce Center Specific Plan proposed Light Industrial land use designation. The proposed Specific Plan Amendment will not be detrimental to the public interest, health, safety, convenience, or general welfare of the City. The land use change will provide Light Industrial uses within the California Commerce Center Specific Plan, which is consistent with the type and intensity of development specified in The Ontario Plan and evaluated by The Ontario Plan Environmental Impact Report.

(3) ***In the case of an application affecting specific property, the proposed Specific Plan, or amendment thereto, will not adversely affect the harmonious***

relationship with adjacent properties and land uses. The proposed amendment will accommodate the construction of industrial land uses. Furthermore, the properties surrounding the project site (immediately to east and south) are currently assigned the Industrial (0.55 FAR) land use designation, providing further land use consistency within the immediate vicinity of the project site which will establish a harmonious relationship between the existing surrounding land uses and planned uses within the specific plan.

(4) ***In the case of an application affecting specific property, the subject site is physically suitable, including, but not limited to, parcel size, shape, access, and availability of utilities, for the request and anticipated development.*** The subject site is physically suitable to accommodate the proposed light industrial land use. The California Commerce Center Specific Plan amendment includes development standards to facilitate the proposed industrial land use, which will be developed with an adequate lot size, access, and utilities to serve the project site.

SECTION 6: Planning Commission Action. Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby RECOMMENDS THE CITY COUNCIL APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 7: Indemnification. The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 8: Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 9: Certification to Adoption. The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of May 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 25, 2021, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**File No. PSPA20-003
Departmental Conditions of Approval**

(Departmental conditions of approval to follow this page)



City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

Planning Department Land Development Division Conditions of Approval

Meeting Date: May 25, 2021

File No: PSPA20-003

Related Files: PDEV20-008

Project Description: An Amendment to the California Commerce Center Specific Plan, changing the land use designation on 10.64 acres of land from Commercial/Food/Hotel to Light Industrial, to be consistent with The Ontario Plan Policy Plan (General Plan) Industrial (0.55 FAR) land use designation, located at the northeast corner of Haven Avenue and Airport Drive, within the California Commerce Center Specific Plan (APN: 0211-222-66); **submitted by Vogel Properties, Inc.**

Prepared By: Jeanie Irene Aguilo, Associate Planner
Phone: 909.395.2418 (direct)
Email: jaguilo@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Specific Plan/Specific Plan Amendment. The following shall be submitted to the Planning Department within 30 days following City Council approval of the Specific Plan/Specific Plan Amendment:

- (a) Fifteen copies of the final Specific Plan document;
- (b) One complete, unbound copy of the final Specific Plan document;
- (c) One CD containing a complete Microsoft Word copy of the final Specific Plan document, including all required revisions;
- (d) Five CDs, each containing a complete PDF copy of the final Specific Plan document, including all required revisions; and
- (e) One CD containing a complete electronic website version of the final Specific Plan document, including all required revisions.

2.2 Environmental Review.

(a) The environmental impacts of this project were reviewed in conjunction with an **Addendum to The Ontario Plan Environmental Impact Report**, certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001 (City Council Resolution No. 2010-006). This application introduces no new significant environmental impacts. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures are a condition of project approval, and are incorporated herein by this reference. All previously adopted mitigation measures shall be a condition of project approval, as they are applicable, and are incorporated herein by this reference.

2.3 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.4 Tribal Consultation Conditions.

(a) Prior to the commencement of any ground disturbing activity at the project site, the project applicant shall retain a Native American Monitor approved by the Gabrieleno Band of Mission Indians-Kizh Nation – the tribe that consulted on this project pursuant to Assembly Bill A52 – SB18 (the “Tribe” or the “Consulting Tribe”). A copy of the executed contract shall be submitted to the City of Ontario Planning and Building Department prior to the issuance of any permit necessary to commence a ground-disturbing activity. The Tribal monitor will only be present on-site during the construction phases that involve ground-disturbing activities. Ground disturbing activities are defined by the Tribe as activities that may include, but are not limited to, pavement removal, potholing or auguring, grubbing, tree removals, boring, grading, excavation, drilling, and trenching, within the project area. The Tribal Monitor will complete daily monitoring logs that will provide descriptions of the day's activities, including construction activities, locations, soil, and any cultural materials identified. The on-site monitoring shall end when all ground-disturbing activities on the Project Site are completed, or when the Tribal Representatives and Tribal Monitor have indicated that all upcoming ground-disturbing activities at the Project Site have little to no potential for impacting Tribal Cultural Resources. Upon discovery of any Tribal Cultural Resources, construction activities shall cease in the immediate vicinity of the find (not less than the surrounding 100 feet) until the find can be assessed. All Tribal Cultural Resources unearthed by project activities shall be evaluated by the qualified archaeologist and Tribal monitor approved by the Consulting Tribe. If the resources are Native American in origin, the Consulting Tribe will retain it/them in the form and/or manner the Tribe deems appropriate, for educational, cultural and/or historic purposes. If human remains and/or grave goods are discovered or recognized at the Project Site, all ground disturbance shall immediately cease, and the county coroner shall be notified per Public Resources Code Section 5097.98, and Health & Safety Code Section 7050.5. Human remains and grave/burial goods shall be treated alike per California Public Resources Code section 5097.98(d)(1) and (2). Work may continue on other parts of the Project Site while evaluation and, if necessary, mitigation takes place (CEQA Guidelines Section 15064.5[f]). If a non-Native American resource is determined by the qualified archaeologist to constitute a “historical resource” or “unique archaeological resource,” time allotment and funding sufficient to allow for implementation of avoidance measures, or appropriate mitigation, must be available. The treatment plan established for the resources shall be in accordance with CEQA Guidelines Section 15064.5(f) for historical resources and PRC Sections 21083.2(b) for unique archaeological resources.

(b) Preservation in place (i.e., avoidance) is the preferred manner of treatment. If preservation in place is not feasible, treatment may include implementation of archaeological data recovery

excavations to remove the resource along with subsequent laboratory processing and analysis. Any historic archaeological material that is not Native American in origin shall be curated at a public, non-profit institution with a research interest in the materials, such as the Natural History Museum of Los Angeles County or the Fowler Museum, if such an institution agrees to accept the material. If no institution accepts the archaeological material, it shall be offered to a local school or historical society in the area for educational purposes.

2.5 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PDEV20-008, A DEVELOPMENT PLAN TO CONSTRUCT A 200,291-SQUARE FOOT INDUSTRIAL BUILDING ON 10.64 ACRES OF LAND, LOCATED AT THE NORTHEAST CORNER OF HAVEN AVENUE AND AIRPORT DRIVE, WITHIN THE PROPOSED LIGHT INDUSTRIAL LAND USE DISTRICT OF THE CALIFORNIA COMMERCE CENTER SPECIFIC PLAN, AND MAKING FINDINGS IN SUPPORT THEREOF—APN: 0211-222-66.

WHEREAS, VOGEL PROPERTIES, INC. (hereinafter referred to as "Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV20-008, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 10.64 acres of land generally located on the southeast corner of Haven Avenue and Airport Drive, within the proposed Light Industrial land use designation of the California Commerce Center Specific Plan and is developed as a parking lot; and

WHEREAS, the property to the north of the Project site is within the Office/Commercial land use designation of the Ontario Gateway Specific Plan and is developed with a retail land uses (Costco and Starbucks). The property to the east is within the Rail Industrial land use designation of the California Commerce Center Specific Plan and is developed with industrial land uses. The property to the south is within the Light Industrial land use designation of the California Commerce Center Specific Plan and is currently vacant. The property to the west is within the ONT zoning district and is currently vacant; and

WHEREAS, the project site is comprised of 10.64 acres of land located at the northeast corner of Haven Avenue and Airport Drive, within the proposed Light Industrial land use district of the California Commerce Center Specific Plan zoning district. The project site is currently developed as a parking lot. Land uses immediately surrounding the project site include commercial to the north; industrial to the east, and vacant land to the south and west; and

WHEREAS, project site was developed in 1999 as a privately-owned long-term parking lot to accommodate customers from Ontario International Airport. The site is presently developed with two structures totaling 1,500 square feet that consist of a toll booth and modular office building; and

WHEREAS, March 27, 2020, the Applicant submitted a Development Plan Application (File No. PDEV20-008), to develop the project site with an industrial

warehouse building, in conjunction with an amendment to the California Commerce Center Specific Plan (File No. PSPA20-003) to change the land use designation of the project site from Commercial/Food/Hotel to Light Industrial, to be consistent with The Ontario Plan Policy Plan (General Plan) Industrial (0.55 FAR) land use designation; and

WHEREAS, approval of the Development Plan would not be final and conclusive until such time that the City Council approves the related Specific Plan Amendment and the related Addendum to The Ontario Plan Environmental Impact Report; and

WHEREAS, the applicant is proposing to construct a 200,291-square foot industrial building with a floor area ratio ("FAR") of 0.43. The rectangular-shaped building is located along the northern portion of the site, with the front of the building and office entry located at the southwest corner of the building, and oriented to the west, facing Haven Avenue; and

WHEREAS, the building is setback approximately 95 feet from the north (rear) property line, approximately 140 feet from the south (Airport Drive) property line, 73 feet from the west (Haven Avenue) property line, and 3 feet from the east (interior) property line; and

WHEREAS, the project will provide off-street parking along the northern, western, and southern portions of the site, in addition to a smaller parking area located at the southeast corner of the site to serve warehouse employees; and

WHEREAS, a yard area, designed for tractor-trailer parking, truck maneuvering, loading activities, and outdoor staging, is centrally located on the project site immediately south of the proposed building; and

WHEREAS, the Project has two points of vehicular access along Airport Drive, including a 30-foot wide driveway located near the southwest corner of the site and a 50-foot wide driveway located near the southeast corner of the site, which will be shared by both standard vehicles and tractor-trailers accessing the yard area and parking lot; and

WHEREAS, the Project has provided off-street parking pursuant to the "Warehouse and Distribution" parking standards specified in the Development Code. The industrial building requires a total of 111 off-street parking spaces, and 140 spaces have been provided. In addition, a minimum of 7 tractor-trailer parking spaces are required and 22 tractor-trailer parking spaces have been provided, exceeding the minimum requirements; and

WHEREAS, the proposed industrial warehouse building will be of concrete tilt-up construction. Architecturally, the building incorporates smooth-painted concrete, horizontal reveals, color blocking, clerestory windows with clear anodized aluminum

mullions and blue glazing, and steel canopies over the main office entries and first story windows. Additionally, mechanical equipment will be roof-mounted and obscured from public view by parapet walls; and

WHEREAS, the project provides landscaping along the Haven Avenue and Airport Drive frontages, around the project perimeter, and tractor-trailer yard area. The Development Code requires that the project provide a minimum 15 percent landscape coverage, which has been provided; and

WHEREAS, public utilities (water and sewer) are available to serve the project. Furthermore, the Applicant has submitted a Preliminary Water Quality Management Plan ("PWQMP"), which establishes the project's compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development ("LID") best management practices ("BMPs"), such as retention and infiltration, biotreatment, and evapotranspiration. The PWQMP proposes on-site run-off will be collected by a catch basin and conveyed to an underground infiltration system located within the tractor-trailer courtyard area. Any overflow drainage will be conveyed to 30-inch on-site storm drain that connects to a 60-inch storm drain located within Airport Drive; and

WHEREAS, The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was certified on January 27, 2010 (hereinafter referred to as "Certified EIR"), in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as "EIR Addendum") in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as "CEQA"); and

WHEREAS, the environmental impacts of this project were thoroughly analyzed in the EIR Addendum, which concluded that implementation of the Project could result in a number of significant effects on the environment that were previously analyzed in the Certified EIR, and that the Certified EIR identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an EIR Addendum has been prepared to determine possible environmental impacts; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 17, 2021, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Addendum and the Project, and concluded said hearing on that date, voting to issue Decision No. DAB21-022, recommending that the Planning Commission recommend the City Council approve the Application and Decision No. DAB21-023, recommending the Planning Commission approve the Project, File No. PDEV20-008; and

WHEREAS, as the first action on the Project, on May 25, 2021, the Planning Commission issued a Resolution recommending the City Council adopt the EIR Addendum, finding that the proposed Project introduces no new significant environmental impacts and applying all previously adopted mitigation measures to the Project, which were incorporated by reference; and

WHEREAS, on May 25, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the decision-making body for the Project, the Planning Commission has reviewed and considered the information contained in the Addendum, the initial study, and the administrative record for the Project, including all written and oral evidence provided during the comment period. Based upon the facts and information contained in the Addendum, the initial study, and the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The environmental impacts of this project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report — State Clearinghouse No. 2008101140 (“Certified EIR”), which was certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001; and

(2) The EIR Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts; and

(4) All previously adopted mitigation measures shall be a condition of project approval, as they are applicable to the Project, and are incorporated herein by this reference; and

(5) The EIR Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and

(6) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts.

SECTION 2: *Subsequent or Supplemental Environmental Review Not Required.* Based on the EIR Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and.

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The Project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: *Housing Element Compliance.* Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based upon the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 4: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as “ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making body for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 5: *Concluding Facts and Reasons.* Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 through 4, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Project is located within the Industrial land use district of the Policy Plan Land Use Map, and the proposed Light Industrial land use district of the California Commerce Center Specific Plan. The development standards and conditions under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan; and

(2) ***The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located.*** The Project has been designed consistent with the requirements of the City of Ontario Development Code and the proposed Light Industrial land use district of the California Commerce Center Specific Plan zoning district, including standards relative to the particular land use proposed (industrial), as-well-as building

intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions; and

(3) ***The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed project.*** The Development Advisory Board has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: [i] the purposes of the California Commerce Center Specific Plan are maintained; [ii] the project will not endanger the public health, safety or general welfare; [iii] the project will not result in any significant environmental impacts; [iv] the project will be in harmony with the area in which it is located; and [v] the project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan, and the California Commerce Center Specific Plan; and

(4) ***The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development.*** The proposed Project has been reviewed for consistency with the general development standards and guidelines of the California Commerce Center Specific Plan that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking and loading spaces, parking lot dimensions, design and landscaping, bicycle parking, on-site landscaping, and fences and walls, as-well-as those development standards and guidelines specifically related to the particular land use being proposed (industrial). As a result of this review, the Development Advisory Board has determined that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the development standards and guidelines described in the California Commerce Center Specific Plan.

SECTION 6: Planning Commission Action. Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as “Attachment A,” and incorporated herein by this reference. If the City Council does not approve the Amendment to the California Commerce Center Specific Plan (File No. PSPA20-003), this resolution should become null and void and of no further force and effect.

SECTION 7: Indemnification. The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 8: Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 9: Certification to Adoption. The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of May 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 25, 2021, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**File No. PDEV20-008
Departmental Conditions of Approval**

(Departmental conditions of approval to follow this page)



City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

Planning Department Land Development Division Conditions of Approval

Meeting Date: May 17, 2021

File No: PDEV20-008

Related Files: PSPA20-003

Project Description: A Development Plan to construct a 200,291-square foot industrial building on 10.64 acres of land located at the northeast corner of Haven Avenue and Airport Drive, within the proposed Light Industrial land use district of the California Commerce Center Specific Plan. (APN: 0211-222-66); **submitted by Vogel Properties, Inc.**

Prepared By: Jeanie Irene Aguilo, Associate Planner
Phone: 909.395.2418 (direct)
Email: jaguilo@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits. Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

2.2 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.3 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b) Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

(c) Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(d) Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

2.4 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

2.5 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b) All drive approaches shall be provided with an enhanced pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.

(c) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(d) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

(e) Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).

(f) Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11).

2.6 Outdoor Loading and Storage Areas.

(a) Loading facilities shall be designed and constructed pursuant to Development Code Division 6.03 (Off-Street Parking and Loading).

(b) Areas designated for off-street parking, loading, and vehicular circulation and maneuvering, shall not be used for the outdoor storage of materials or equipment.

(c) Outdoor loading and storage areas, and loading doors, shall be screened from public view pursuant to the requirements of Development Code Paragraph 6.02.025.A.2 (Screening of Outdoor Loading and Storage Areas, and Loading Doors) Et Seq.

(d) Outdoor loading and storage areas shall be provided with gates that are view-obstructing by one of the following methods:

(i) Construct gates with a perforated metal sheet affixed to the inside of the gate surface (50 percent screen); or

(ii) Construct gates with minimum one-inch square tube steel pickets spaced at maximum 2-inches apart.

(e) The minimum gate height for screen wall openings shall be established based upon the corresponding wall height, as follows:

<i>Screen Wall Height</i>	<i>Minimum Gate Height</i>
14 feet:	10 feet
12 feet:	9 feet
10 feet:	8 feet
8 feet:	8 feet
6 feet:	6 feet

2.7 Site Lighting.

(a) All off-street parking facilities shall be provided with nighttime security lighting pursuant to Ontario Municipal Code Section 4-11.08 (Special Residential Building Provisions) and Section 4-11.09 (Special Commercial/Industrial Building Provisions), designed to confine emitted light to the parking areas. Parking facilities shall be lighted from sunset until sunrise, daily, and shall be operated by a photocell switch.

(b) Unless intended as part of a master lighting program, no operation, activity, or lighting fixture shall create illumination on any adjacent property.

2.8 Mechanical and Rooftop Equipment.

(a) All exterior roof-mounted mechanical, heating and air conditioning equipment, and all appurtenances thereto, shall be completely screened from public view by parapet walls or roof screens that are architecturally treated so as to be consistent with the building architecture.

(b) All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.

2.9 Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

2.10 Signs. All Project signage shall comply with the requirements of Ontario Development Code Division 8.0 (Sign Regulations).

2.11 Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

2.12 Covenants, Conditions and Restrictions (CC&Rs)/Mutual Access and Maintenance Agreements.

(a) Reciprocal Easement Agreement shall ensure reciprocal access between the project site and the adjacent parcel.

(b) Reciprocal Easement Agreement shall be prepared for the Project and shall be recorded prior to the issuance of a building permit.

2.13 Environmental Review.

(a) The environmental impacts of this project were reviewed in conjunction with an **Addendum to The Ontario Plan Environmental Impact Report**, certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001 (City Council Resolution No. 2010-006). This application introduces no new significant environmental impacts. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures are a condition of project approval, and are incorporated herein by this reference. All previously adopted mitigation measures shall be a condition of project approval, as they are applicable, and are incorporated herein by this reference.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

2.14 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.15 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

2.16 Tribal Consultation Conditions.

(a) Prior to the commencement of any ground disturbing activity at the project site, the project applicant shall retain a Native American Monitor approved by the Gabrieleno Band of Mission Indians-Kizh Nation – the tribe that consulted on this project pursuant to Assembly Bill A52 – SB18 (the “Tribe” or the “Consulting Tribe”). A copy of the executed contract shall be submitted to the City of Ontario Planning and Building Department prior to the issuance of any permit necessary to commence a ground-disturbing activity. The Tribal monitor will only be present on-site during the construction phases that involve ground-disturbing activities. Ground disturbing activities are defined by the Tribe as activities that may include, but are not limited to, pavement removal, potholing or auguring, grubbing, tree removals, boring, grading, excavation, drilling, and trenching, within the project area. The Tribal Monitor will complete daily monitoring logs that will provide descriptions of the day's activities, including construction activities, locations, soil, and any cultural materials identified. The on-site monitoring shall end when all ground-disturbing activities on the Project Site are completed, or when the Tribal Representatives and Tribal Monitor have indicated that all upcoming ground-disturbing activities at the Project Site have little to no potential for impacting Tribal Cultural Resources. Upon discovery of any Tribal Cultural Resources, construction activities shall cease in the immediate vicinity of the find (not less than the surrounding 100 feet) until the find can be assessed. All Tribal Cultural Resources unearthed by project activities shall be evaluated by the qualified archaeologist and Tribal monitor approved by the Consulting Tribe. If the resources are Native American in origin, the Consulting Tribe will retain it/them in the form and/or manner the Tribe deems appropriate, for educational, cultural and/or historic purposes. If human remains and/or grave goods are discovered or recognized at the Project Site, all ground disturbance shall immediately cease, and the county coroner shall be notified per Public Resources Code Section 5097.98, and Health & Safety Code Section 7050.5. Human remains and grave/burial goods shall be treated alike per California Public Resources Code section 5097.98(d)(1) and (2). Work may continue on other parts of the Project Site while evaluation and, if necessary, mitigation takes place (CEQA Guidelines Section 15064.5[f]). If a non-Native American resource is determined by the qualified archaeologist to constitute a “historical resource” or “unique archaeological resource,” time allotment and funding sufficient to allow for implementation of avoidance measures, or appropriate mitigation, must be available. The treatment plan established for the resources shall be in accordance with CEQA Guidelines Section 15064.5(f) for historical resources and PRC Sections 21083.2(b) for unique archaeological resources.

(b) Preservation in place (i.e., avoidance) is the preferred manner of treatment. If preservation in place is not feasible, treatment may include implementation of archaeological data recovery excavations to remove the resource along with subsequent laboratory processing and analysis. Any historic archaeological material that is not Native American in origin shall be curated at a public, non-profit institution with a research interest in the materials, such as the Natural History Museum of Los Angeles County or the Fowler Museum, if such an institution agrees to accept the material. If no institution accepts the archaeological material, it shall be offered to a local school or historical society in the area for educational purposes.

2.17 Additional Requirements.

(a) The approval of File No. PDEV20-008 shall be final and conclusive upon the approval of File No. PSPA20-003 by the City Council.



**ENGINEERING DEPARTMENT
CONDITIONS OF APPROVAL**

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Broadband Operations & Investment and Revenue Resources Department Conditions incorporated)

<input checked="" type="checkbox"/> DEVELOPMENT PLAN <input type="checkbox"/> OTHER	<input type="checkbox"/> PARCEL MAP <input type="checkbox"/> TRACT MAP <input type="checkbox"/> FOR CONDOMINIUM PURPOSES
PROJECT FILE NO. <u>PDEV20-008</u> RELATED FILE NO(S). _____	
<input checked="" type="checkbox"/> ORIGINAL <input type="checkbox"/> REVISED: <u> </u> / <u> </u> / <u> </u>	

CITY PROJECT ENGINEER & PHONE NO: Eric Woosley, PE (909) 395-2134 *EW*

CITY PROJECT PLANNER & PHONE NO: Jeanie Aguilo (909) 395-2418

DAB MEETING DATE: May 17, 2021

PROJECT NAME / DESCRIPTION: PDEV20-008 a Development Plan to construct a 237,398-square feet industrial building on 10.64 acres of land within the Commercial/Food/Hotel land use district of the California Commerce Center Specific Plan

LOCATION: Northeast corner of Haven Avenue and Airport Drive

APPLICANT: Vogel Haven

REVIEWED BY: Raymond Lee 5/6/21
 Raymond Lee, P.E. Date
 Assistant City Engineer

APPROVED BY: Khoi Do 5-6-21
 Khoi Do, P.E. Date
 City Engineer



THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.

1. PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, APPLICANT SHALL: Check When Complete

- 1.01 Dedicate to the City of Ontario, the right-of-way, described below:
_____ feet on _____
Property line corner 'cut-back' required at the intersection of _____ and _____.

- 1.02 Dedicate to the City of Ontario, the following easement(s): _____

- 1.03 Restrict vehicular access to the site as follows: _____

- 1.04 Vacate the following street(s) and/or easement(s): _____

- 1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.

- 1.06 Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.

- 1.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.

- 1.08 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Financial Services Department at (909) 395-2124 regarding this requirement.
 - (1) _____
 - (2) _____

- 1.09 Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.



- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ci.ontario.ca.us) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.
- 1.13 New Model Colony (NMC) Developments:
 - 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
 - 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
 - 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions: _____

2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:

**A. GENERAL
(Permits includes Grading, Building, Demolition and Encroachment)**

- 2.01 Record Parcel Map/Tract Map No. _____ pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 Submit a PDF of the recorded map to the City Engineer's office.
- 2.03 **Note that the subject parcel is a recognized parcel in the City of Ontario Per Parcel Map No. 15162 in Map Book 185, Pages 84-85.**
- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of _____:
- 2.05 Apply for a: Certificate of Compliance with a Record of Survey; Lot Line Adjustment
 Make a Dedication of Easement.
- 2.06 **Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.**



- 2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.
- 2.08 **Submit a soils/geology report.**
- 2.09 **Other Agency Permit/Approval: Submit a copy of the approved permit, non-interference letter and/or other form of approval of the project from the following agency or agencies:**
- State of California Department of Transportation (Caltrans)
 - San Bernardino County Road Department (SBCRD)
 - San Bernardino County Flood Control District (SBCFCD)
 - Federal Emergency Management Agency (FEMA)
 - Cucamonga Valley Water District (CVWD) for sewer/water service
 - United States Army Corps of Engineers (USACE)
 - California Department of Fish & Game
 - Inland Empire Utilities Agency (IEUA)
 - Utility Companies (Southern California Gas, etc.) for any easements within the limits of the subject project.**
- 2.10 Dedicate to the City of Ontario the right-of-way described below:
- _____ feet on _____
- Property line corner 'cut-back' required at the intersection of _____
and _____.
- 2.11 **Dedicate to the City of Ontario the following easement(s):**
1. **A 20' Public Utilities Easement (PUE) for the existing 18" water main along the northern portion of the property.**
 2. **A 20' PUE for the existing public sewer main along the northerly property limits.**
- Each easement shall follow the alignment of the utility with reference to the centerline.**
- 2.12 **New Model Colony (NMC) Developments:**
- 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.
 - 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.
 - 3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall).
- 2.13 **Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at 100% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.**



- 2.14 The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.
- 2.15 **Pay all Development Impact Fees (DIF) to the Building Department. Storm Drain Development Impact Fee, approximately \$238,229.60, shall be paid to the Building Department. Final fee shall be determined based on the approved site plan.**
- 2.16 Other conditions: _____



B. PUBLIC IMPROVEMENTS

(See attached Exhibit 'A' for plan check submittal requirements.)

- 2.17 Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):

Improvement	Haven Avenue	Airport Drive	Street 3	Street 4
Curb and Gutter	<input type="checkbox"/> New; ___ ft. from C/L <input checked="" type="checkbox"/> Replace damaged <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input checked="" type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
AC Pavement	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Drive Approach	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Sidewalk	<input checked="" type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
ADA Access Ramp	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Parkway	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
Raised Landscaped Median	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Fire Hydrant	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation



Sewer (see Sec. 2.C)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Water (see Sec. 2.D)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Street Light (see Sec. 2.F)	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2.G)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Fiber Optics (see Sec. 2.K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
Removal of Improvements	_____	_____	_____	_____
Other Improvements	_____	_____	_____	_____

Specific notes for improvements listed in item no. 2.17, above: _____

- 2.18 Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s): _____
- 2.19 Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.



- 2.20 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide water service sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.
- 2.21 Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately _____, for undergrounding of utilities in accordance with Section 7-7.303.e of the City's Municipal Code.
- 2.22 Other conditions: _____

C. SEWER

- 2.23 **An 8-inch sewer main is available for connection by this project in Airport Drive. (Ref: Sewer plan bar code: S13223)**
- 2.24 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.25 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.
- 2.26 **Other conditions:**
 - 1. **The proposed sewer monitoring manhole shall be located on private property immediately behind the property line in place of the proposed sewer cleanout.**
 - 2. **The Occupant/Applicant shall apply for a Wastewater Discharge Permit for their Establishment, and shall comply will all the requirements of the Wastewater Discharge Permit (https://www.ontarioca.gov/sites/default/files/Ontario-Files/Municipal-Utilities-Company/industrial_wastewater_permit_application.pdf). Requirements of the Wastewater Discharge Permit may include, but not limited to: Installation of wastewater pretreatment equipment, such as clarifiers. For wastewater permit application questions, please contact: Michael Birmelin, Environmental Programs Manager omucenvironmental@ontarioca.gov Phone: (909) 395-2661**
 - 3. **As part of the precise grading plans submittal, provide a Final Utilities Systems Map that shows all existing and proposed Utilities (Potable Water, Recycled Water, Sewer, Storm Drain, and other utilities) including each of the City's public utilities' points of connection to the existing systems.**

D. WATER

- 2.27 **A 16-inch water main is available for connection by this project in Airport Drive. (Ref: Water plan bar code: W10280)**
- 2.28 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.29 **Other conditions:**
 - 1. **Upgrade the existing two fire hydrants to current city standard with breakaway check valves on Airport Drive.**
 - 2. **Construct one new fire hydrant near the intersection of Haven Avenue and Airport Drive on Airport Drive.**
 - 3. **The proposed fire hydrant, east of the proposed easterly driveway of the project, must be connected the directly to the public water main in the street and cannot be attached to the downstream of the proposed DCDA.**
 - 4. **Construct two new fire hydrants along Haven Avenue.**



E. RECYCLED WATER

- 2.30 **No recycled water main is available for connection by this project in Airport Drive or Haven Avenue.**
(Ref: Recycled Water plan bar code: N/A)
- 2.31 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.
- 2.32 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.
- 2.33 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.

Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.
- 2.34 Other conditions: _____

F. TRAFFIC / TRANSPORTATION

- 2.35 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer:
 - 1. On-site and off-site circulation
 - 2. Traffic level of service (LOS) at 'build-out' and future years
 - 3. Impact at specific intersections as selected by the City Engineer
- 2.36 New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.
- 2.37 **Other conditions:**
 - 1. **The Applicant/Developer shall construct concrete approach for all lanes in the westbound direction along Airport Drive since Airport Drive is a truck route in accordance with the City of Ontario Standard Drawing No. 1207.**
 - 2. **The Applicant/Developer shall pay an in-lieu fee, approximately \$51,168, for the construction of one-fourth of the concrete intersection of Haven Avenue and Airport Drive.**
 - 3. **The Applicant/Developer shall be responsible to design and construct a bus turnout on the east side of Haven Avenue north of Airport Drive for the existing bus stop to the satisfaction of the City Engineer and Omnitrans. The design shall be in accordance with Omnitrans' Bus Stop Design Guidelines.**
 - 4. **The Applicant/Developer shall be responsible to replace any existing streetlight fixtures along the property frontages of Haven Avenue, including the median and Airport Drive with the current City-approved LED equivalent fixture. Please refer to the Traffic and Transportation Design Guidelines Section 1.4-Street Light Plans.**



G. DRAINAGE / HYDROLOGY

- 2.38 **A 72-inch storm drain main is available to accept flows from this project in Airport Drive. (Ref: Storm Drain plan bar code: D10145)**
- 2.39 Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.
- 2.40 An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100-year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.
- 2.41 Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.
- 2.42 Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100-year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.
- 2.43 Other conditions: _____

H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)

- 2.44 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year-round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels.
 If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.
 Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.
- 2.45 **Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: <http://www.sbcounty.gov/dpw/land/npdes.asp>.**
- 2.46 **Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.**
- 2.47 Other conditions: _____



J. SPECIAL DISTRICTS

- 2.48 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.
- 2.49 Other conditions: _____

K. FIBER OPTIC

- 2.50 Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole.
- 2.51 Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Broadband Operations Department at (909) 395-2000, regarding this requirement.

L. INTEGRATED WASTE

- 2.52 Onsite solid waste shall be designed in accordance with the City's Refuse & Recycling Planning Manual located at:
<https://www.ontarioca.gov/OMUC/IntegratedWaste>
- 2.53 Other conditions:
1. **Final Solid Waste Handling Plan (SWHP):** Prior to approval of any building permits, submit a final SWHP with the Precise Grading Plan for review and approval of Ontario Municipal Utilities Company.
 2. **Trash Enclosures (TE):** Per the proposed building square footage, this site is required to construct three TEs. Each TE to accommodate three 4-cy yard bins. Each bin is 5'x7' in dimensions.
 3. **Integrated Waste Management Report (IWMR):** Prior to approval of the Precise Grading Plan, submit to OMUC for review and approval, an Integrated Waste Management Report (IWMR) that complies with the IWMR Requirements.



3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:

- 3.01 Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.
- 3.02 Complete all requirements for recycled water usage.
 - 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.
 - 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.
 - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.
- 3.03 The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.
- 3.04 NMC Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.
- 3.05 Confirm payment of all Development Impact Fees (DIF) to the Building Department.
- 3.06 Submit electronic copies (PDF) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).

4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:

- 4.01 Complete all Conditions of Approval listed under Sections 1-3 above.
- 4.02 Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.
- 4.03 The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall include a completed Acceptance and Bond Release Checklist, state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.
- 4.04 Submit record drawings (PDF) for all public improvements identified within Section 2 of these Conditions of Approval.



EXHIBIT 'A'

ENGINEERING DEPARTMENT First Plan Check Submittal Checklist

Project Number: PDEV20-008

The following items are required to be included with the first plan check submittal:

1. **A copy of this check list**
2. **Payment of fee for Plan Checking**
3. **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4. **One (1) copy of project Conditions of Approval**
5. **Include a PDF (electronic submittal) of each required improvement plan at every submittal.**
6. Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).
7. **Three (3) sets of Public Street improvement plan with street cross-sections**
8. Three (3) sets of Private Street improvement plan with street cross-sections
9. Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)
10. Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)
11. Four (4) sets of Public Sewer improvement plan
12. Five (5) sets of Public Storm Drain improvement plan
13. Three (3) sets of Public Street Light improvement plan
14. Three (3) sets of Signing and Striping improvement plan
15. **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
16. Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)
17. Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.
18. **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**
19. One (1) copy of Hydrology/Drainage study
20. **One (1) copy of Soils/Geology report**



- 21. Payment for Final Map/Parcel Map processing fee
- 22. Three (3) copies of Final Map/Parcel Map
- 23. One (1) copy of approved Tentative Map
- 24. **One (1) copy of Preliminary Title Report (current within 30 days)**
- 25. One (1) copy of Traverse Closure Calculations
- 26. **One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.**
- 27. Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
- 28. Other: _____



UTILITIES SYSTEMS MAP (USM) REQUIREMENTS:

The USM shall meet, at a minimum, the following requirements:

1. **USM Content and Format:** The Utilities Systems Maps shall show all existing and proposed Utilities (Potable Water, Recycled Water, Sewer, Storm Drain, and other utilities) including each of the City's public utilities' points of connection to the existing systems. This plan should include:
 - a. **Format:** The Utilities Systems plan at a minimum 1:100 scale (or large engineering scale as appropriate to show needed details) that clearly shows each existing and proposed utility and its relative location. This includes property lines, right-of-way, public utility easements, but should not include underlying existing topography, just proposed general grades. Use appropriate colors for each Utility type: blue for Potable Water; purple for Recycled Water; green for Sanitary Sewer; yellow-brown for storm Drain.
 - b. **Services and Laterals:** All Proposed Utility Service laterals for each parcel (potable water domestic, recycled water irrigation, potable/recycled water for process water, and sewer) and any associated appurtenances.
 - i. **Meter and Backflow Device Locations:** Show all proposed meters and required backflow devices located per City Standards (Water Services and Meters; Backflow Devices). Meters should be located in public rights-of-way or PUEs; either at the R/W (or PUE) line for curb adjacent sidewalks or at back of curb for all other cases. All water connections that serve more than one residential unit are required to have a backflow device installed behind the meter.
 - c. **Cross Sections (if applicable, for project construction new public mains):** Scaled cross sections showing the utility layout on the Utility Systems Map (Utility Plan) for each public street, private street and Public Utility Easement (PUE). The cross sections shall show the location and size of each utility and annotate the property/ROW lines, the type of finished surface material, the distance of each utility from centerline, the depth from finished surface to top of pipe, and the distance between utilities (outside wall to outside wall).
 - d. **Points of Connections:** The locations of the points of connections to the existing utility systems, which can include breaks between the map area and the connection points with descriptions of the pipe size, type, use (pressure zone for water), and distance. An inset map can be used in addition to this to help provide clarity.
 - e. **Water Demand Table (if applicable, for projects within Ontario Ranch/NMC):** Add a Water Demand Table to the Utility Systems Map (Utility Plan) that calculates the project's domestic water use based on land use category (residential, commercial, and OS-R/Parks) and the number of units. The table shall state demand in terms of Average Daily Demand (ADD from Table 4-8 of the Water Master Plan) and Water Demand Equivalents (WDE / Net MDD from Exhibit C-2R of the NMC Construction Agreement; WDEs only if NMC). It should also identify the quantity of units in each category and the specific lots that are included in that category. Please Note that master planned lines are designed using gross acreage densities for all projected water use from residential categories.
 - i. See Attached Sheet for WDT Example.
 - f. **Phasing Plan (if applicable):** As separate exhibits, provide a proposed phasing plan showing the phasing of the infrastructure and the number and type (TOP land use category) of units in each phase.
 - i. All phases must have: a connection to public sewer; a two separate looped connections to the potable water system, where no one closing of a main segment results in any part of any of any phase being without potable water.



- ii. For public water mains in all phases, dead-end water lines (temporary or permanent) are limited to serving 28 dwelling units or a maximum of 600 linear feet, whichever comes first. Otherwise a looped water system with at least two (2) points of connection to the primary public system is required.

- g. Private Onsite Systems versus Public Systems within PUEs for Residential Tract Map Project *if applicable*: the following requirements apply when delineating between Private and Public Systems:
 - i. Current Standard Drawing No. 1304 remains applicable and minimum health separation must be met.
 - ii. Public water mains will be accepted in longer alleys when it serves more than 6 meters.
 - iii. Public sewer mains will be accepted in alleys where the water is public.
 - iv. Public dead-end water mains will require a blow-off at the end and the alley should be designed to accommodate runoff from required water main flushing operations.
 - v. Public sewer mains in alleys will require a manhole at both ends of the main.
 - vi. Public meters serving more than one single family residential unit are considered as multifamily service with master meter and require: a backflow device after the meter, private HOA sub-metering for each unit, and a separate Fire Service with DCDA to provide private onsite fire service.



SOLID WASTE HANDLING PLAN (SWHP) REQUIREMENTS:

The SWHP shall meet, at a minimum, the following requirements:

1. **SWHP Content and Format:** The Solid Waste Handling Plan shall demonstrate compliance with the Services Standards in the City's Solid Waste Planning Manual (available online at: <http://www.ontarioca.gov/government-departments-municipal-utilities-company/integrated-waste>) and shall contain, at a minimum, the following elements:
 - a. A statement identifying the Service Requirements being used (e.g. Single Family Detached with automated cans, Multi-family/ Commercial/Industrial with bins and enclosures, etc.) and describing the solid waste handling operation (*for instance, will there be scouting services, etc.*).
 - b. A table utilizing the metrics on Page 8 of the Planning Manual and calculating the volume (gallons or cubic yards), quantity, and service schedule for each type of can and bin required for each Service Category (refuse, recycled, etc.).
 - c. An Engineering Site Plan drawn to scale that shows:
 - i. Minimum plan scale of scale of 1:100. Larger scales are preferred and should be scaled to fill the sheet and show as much detail as clearly as possible on one sheet; multiple sheets may be used if entire project area cannot fit on one sheet at 1:100 scale.
 - ii. A detail of the Solid Waste Vehicle with dimensions and annotation that states the minimum turning radii and path of travel widths actually being used on the plan.
 - iii. The Solid Waste Vehicle turning movements and paths of travel in each direction of travel and at all intersections. All paths of travel shall be 15 feet wide minimum.
 - iv. All parking stalls and parallel parking spaces along all streets, alleys, or aisles.
 - v. All proposed curbs and areas designated and striped/signed as "No Parking".
 - vi. All proposed trash enclosures and the ADA paths of travel from the buildings.
 - vii. A detail for each enclosure footprint delineating the number and size of the bins in order to demonstrate that the enclosure is adequately sized and oriented, if enclosures and bins are proposed.
 - viii. All proposed locations of automated cans shown as a 26-inch by 26-inch can pad with 20-inches between can pads and 40-inches between can pads and Parking spaces, mailboxes and other obstructions (Can Collection Area). Can Collection Areas shall be located along designated paths of travel and cannot be located along dead end alleys, motor courts, driveways, or private streets; use multi-family standards for enclosures in these cases.
2. **Can Collection Area (CCA) Locations:** If CCAs are being proposed in lieu of bin enclosures for residential units located along dead end alleys, motor courts, driveways, or private streets, then the SWHP shall comply with the following requirements:
 - a. CCAs cannot conflict or compete with potential parking areas. Proposed CCAs must be designated as "no parking" at all times with appropriate striping and signage.
 - b. Each residential unit must have a designated CCA and each CCA must delineated with markings so that its location and the unit it is designated for are easily identifiable.
 - c. Solid Waste Handling Plan shall include a detail showing how the CCAs will be delineated and identifiable.



3. **Private Third Party Hauler:** If any Solid Waste Collections are going to be provided by a private third party hauler, include on the SWHP:
 - a. A statement describing the service.
 - b. The names, contact information, and City of Ontario Commercial Recycler Collection Permit numbers of all private third party haulers. Note: all private third party organics and recycling haulers must be formally approved and permitted by the City of Ontario and meet City Code requirements, otherwise the City must provide the collection services. If the third party haulers do not have a City of Ontario Commercial Recycler Collection Permit, state if the third party hauler is charging a fee for service.
 - c. Show & label staging and collection areas for private third party haulers.

**CITY OF ONTARIO
LANDSCAPE PLANNING
DIVISION**

303 East "B" Street, Ontario, CA 91764

DAB CONDITIONS OF APPROVAL

Sign Off

	02/24/2021
Jamie Richardson, Sr. Landscape Planner	Date

Reviewer's Name: Jamie Richardson, Sr. Landscape Planner	Phone: (909) 395-2615
--	---------------------------------

D.A.B. File No.: PDEV20-008	Case Planner: Jeanie Aguilo
--------------------------------	--------------------------------

Project Name and Location: Vogel Industrial Bldg. NEC Haven Ave and Airport Drive	
Applicant/Representative: Herdman Architecture and Design, Inc. 16201 Scientific Way Irvine, CA 92618	

<input checked="" type="checkbox"/>	A Preliminary Landscape Plan (dated 02/09/2021) meets the Standard Conditions for New Development and has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.
<input type="checkbox"/>	A Preliminary Landscape Plan (dated) has not been approved. Corrections noted below are required prior to Preliminary Landscape Plan approval.

A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED AS INCOMPLETE.
Landscape construction plans with plan check number may be emailed landscapeplancheck@ontarioca.gov
DIGITAL SUBMITTALS MUST BE 10MB OR LESS.

Civil/ Site Plans

1. Show on demo plans and landscape construction plans trees to be preserved, removed or mitigation measures for trees removed, such as:
 - a. New 15 gallon trees min 1" diameter trunk, in addition to trees required.
 - b. New 24" box trees min 1.5" diameter trunk, in addition to trees required.
 - c. Upsizing trees on the plan one size larger such as 15 gallon to 24" box, or 24" to 36" box size.
 - d. Monetary value of the trees removed as identified in the "Guide for Plant Appraisal", approved certified arborist plant appraiser, or may be equal to the value of the installation cost of planting, fertilizing, staking and irrigating 15 gallon trees, (100\$ each) to the City of Ontario Historic Preservation Fund for city tree planting or city approved combination of the above items.
2. ~~Increase the landscape planter along the east.~~ There are adjacent trees within 15' of this building and will be impacted by construction. **Include these trees in the inventory. Identify mitigation/protection measures. Landscape, trees and irrigation will be required to be replaced and repaired along the eastern property line. Parking lot trees will be required to be installed on adjacent property if removed.**

Landscape Plans

3. Do not encircle or box in the backflow devices, show as masses and duplicate masses in other locations on regular intervals.
4. Dimension all planters to have a minimum 5' wide inside dimension with 6" curbs and 12" wide

curbs where parking spaces are adjacent to planters.

5. Provide additional trees throughout the landscape planter along the northern property line.
6. Locate trees 50% of canopy width from walls, buildings, and existing trees.
7. Show 25% of trees as California native (*Platanus racemosa*, *Quercus agrifolia*, *Quercus wislizenii*, *Quercus douglasii*, *Cercis occidentalis* etc.) in appropriate locations.
8. Landscape construction plans shall meet the requirements of the Landscape Development Guidelines. See <http://www.ontarioca.gov/landscape-planning/standards>
9. After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections at a rate established by resolution of the City Council. Landscape construction plans with building permit number for plan check may be emailed to: landscapeplancheck@ontarioca.gov

CITY OF ONTARIO

MEMORANDUM

TO: PLANNING DEPARTMENT, Jeanie Irene Aguilo
FROM: BUILDING DEPARTMENT, Kevin Shear
DATE: April 3, 2020
SUBJECT: PDEV20-008

-
- The plan **does** adequately address the departmental concerns at this time.
 No comments
 Report below.

Conditions of Approval

1. Project Address will be 3525 E Airport Dr.
2. Standard Conditions of Approval apply.

KS:lr



CITY OF ONTARIO

MEMORANDUM

TO: Jeanie Irene Aguilo, Associate Planner
Planning Department

FROM: Paul Ehrman, Deputy Fire Chief/Fire Marshal
Fire Department

DATE: April 8, 2020

SUBJECT: PDEV20-008 – A Development Plan to construct a 237,398-square foot industrial building on 10.64 acres of land located at the northeast corner Haven Avenue and Airport Drive, within the Commercial/Food/Hotel land use district of the California Commerce Center Specific Plan (APN: 0211-222-66). Related File: PSPA20-003.

-
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.
-

SITE AND BUILDING FEATURES:

- A. 2016 CBC Type of Construction: III B
- B. Type of Roof Materials: Panelized
- C. Ground Floor Area(s): 237,398 Sq. Ft.
- D. Number of Stories: 1 with Mezzanine
- E. Total Square Footage: 237,398 Sq. Ft.
- F. 2016 CBC Occupancy Classification(s): S

CONDITIONS OF APPROVAL:

1.0 GENERAL

- ☒ 1.1 The following are the Ontario Fire Department (“Fire Department”) requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards (“Standards.”) It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at www.ontarioca.gov, click on “Fire Department” and then on “Standards and Forms.”
- ☒ 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

2.0 FIRE DEPARTMENT ACCESS

- ☒ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- ☒ 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25’) inside and forty-five feet (45’) outside turning radius per Standard #B-005.
- ☒ 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150’) in length shall have an approved turn-around per Standard #B-002.
- ☒ 2.4 Access drive aisles which cross property lines shall be provided with CC&Rs, access easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of building plan check.
- ☒ 2.5 "No Parking-Fire Lane" signs and /or red painted curbs with lettering are required to be installed in interior access roadways, in locations where vehicle parking would obstruct the minimum clear width requirement. Installation shall be per Standard #B-001.
- ☒ 2.6 Security gates or other barriers on fire access roadways shall be provided with a Knox brand key switch or padlock to allow Fire Department access. See Standards #B-003, B-004 and H-001.
- ☒ 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-four (24) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by Fire Department and other emergency services.

3.0 WATER SUPPLY

- ☒ 3.1 The required fire flow per Fire Department standards, based on the 2016 California Fire Code, Appendix B, is 4000 gallons per minute (g.p.m.) for 4 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.
- ☒ 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.
- ☒ 3.3 Buildings that exceed 100,000 square feet in floor area shall provide an onsite looped fire protection water line around the building(s.) The loops shall be required to have two or more points of connection from a public circulating water main.
- ☒ 3.4 The water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

4.0 FIRE PROTECTION SYSTEMS

- ☒ 4.1 On-site private fire hydrants are required per Standard #D-005, and identified in accordance with Standard #D-002. Installation and location(s) are subject to the approval of the Fire Department. An application with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- ☒ 4.2 Underground fire mains which cross property lines shall be provided with CC & R, easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of Fire Department plan check. The shared use of private fire mains or fire pumps is allowable only between immediately adjacent properties and shall not cross any public street.
- ☒ 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- ☒ 4.5 Fire Department Connections (FDC) shall be located on the address side of the building within one hundred fifty feet (150') of a public fire hydrant on the same side of the street. Provide identification for all fire sprinkler control valves and Fire Department connections per Standard #D-007. Raised curbs adjacent to Fire Department connection(s) shall be painted red, five feet either side, per City standards.
- ☒ 4.6 A fire alarm system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 72. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.

- ☒ 4.7 Portable fire extinguishers are required to be installed prior to occupancy per Standard #C-001. Please contact the Fire Prevention Bureau to determine the exact number, type and placement required.

5.0 BUILDING CONSTRUCTION FEATURES

- ☒ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ☒ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- ☒ 5.6 Knox ® brand key-box(es) shall be installed in location(s) acceptable to the Fire Department. All Knox boxes shall be monitored for tamper by the building fire alarm system. See Standard #H-001 for specific requirements.
- ☒ 5.7 Placards shall be installed in acceptable locations on buildings that store, use or handle hazardous materials in excess of the quantities specified in the CFC. Placards shall meet the requirements of National Fire Protection Association (NFPA) Standard 704.

6.0 OTHER SPECIAL USES

- ☒ 6.1 The storage, use, dispensing, or handling of any hazardous materials shall be approved by the Fire Department, and adequate fire protection features shall be required. If hazardous materials are proposed, a Fire Department Hazardous Materials Information Packet, including Disclosure Form and Information Worksheet, shall be completed and submitted with Material Safety Data Sheets to the Fire Department along with building construction plans.
- ☒ 6.2 Any High Piled Storage, or storage of combustible materials greater than twelve (12') feet in height for ordinary (Class I-IV) commodities or storage greater than six feet (6') in height of high hazard (Group A plastics, rubber tires, flammable liquids, etc.) shall be approved by the Fire Department, and adequate fire protection features shall be required. If High Piled Storage is proposed, a Fire Department High Piled Storage Worksheet shall be completed and detailed racking plans or floor plans submitted prior to occupancy of the building.
- ☒ 6.3 Underground fuel tanks, their associated piping and dispensers shall be reviewed, approved, and permitted by Ontario Building Department, Ontario Fire Department, and San Bernardino County Fire Department Hazardous Materials Division. In fueling facilities, an exterior emergency pump shut-off switch shall be provided.



CITY OF ONTARIO

MEMORANDUM

TO: Jeanie Irene Aguilo, Associate Planner

FROM: Officer Emily Hernandez, Police Department

DATE: April 6, 2020

SUBJECT: PDEV20-008- A DEVELOPMENT PLAN TO CONSRUCT ONE INDUSTRIAL BUILDING TOTALING 237,398 SQUARE FEET LOCATED AT THE NORTHEAST CORNER OF HAVEN AVENUE AND AIRPORT DRIVE.

The “Standard Conditions of Approval” contained in Resolution No. 2017-027 apply. The applicant shall read and be thoroughly familiar with these conditions, including, but not limited to, the requirements below.

- Required lighting for all walkways, driveways, doorways, parking lots, hallways and other areas used by the public shall be provided. Lights shall operate via photosensor. Photometrics shall be provided to the Police Department and include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- Rooftop addresses shall be installed on the buildings as stated in the Standard Conditions. The numbers shall be at a minimum 6 feet tall and 2 foot wide, in reflective white paint on a flat black background, and oriented with the bottom of the numbers towards the addressed street. Associated letters shall also be included.
- First floor common stairwells shall be constructed to either allow for visibility through the stairwell risers or to prohibit public access to the areas behind stairwells.
- The Applicant shall comply with construction site security requirements as stated in the Standard Conditions.

The Applicant is invited to contact Officer Emily Hernandez at (909) 408-1755 with any questions or concerns regarding these conditions.

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT



Project File No.: PSPA20-003 & PDEV20-008

Address: Northeast Corner of Haven Avenue & Airport Drive

APN: 0211-222-66

Existing Land Use: Parking Lot

Proposed Land Use: An SPA to change the Commercial/Food/Hotel land use to Industrial and Dev. Plan to construct 1 industrial building totaling 237, 398 SF

Site Acreage: 10.64 Proposed Structure Height: 50 FT

ONT-IAC Project Review: n/a

Airport Influence Area: ONT

Reviewed By: Lorena Mejia

Contact Info: 909-395-2276

Project Planner: Jeanie Aguilo

Date: 12/23/2020

CD No.: 2020-021

PALU No.: n/a

The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="radio"/> High Terrain Zone	<input type="radio"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input checked="" type="checkbox"/> FAA Notification Surfaces	<input checked="" type="checkbox"/> Recorded Overflight Notification
<input type="radio"/> Zone 2	<input checked="" type="checkbox"/> 65 - 70 dB CNEL	<input checked="" type="checkbox"/> Airspace Obstruction Surfaces	<input checked="" type="checkbox"/> Real Estate Transaction Disclosure
<input type="radio"/> Zone 3	<input checked="" type="checkbox"/> 60 - 65 dB CNEL	<input checked="" type="checkbox"/> Airspace Avigation Easement Area	
<input type="radio"/> Zone 4		Allowable Height: <u>120 FT</u>	
<input type="radio"/> Zone 5			

The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1
 Zone 2
 Zone 3
 Zone 4
 Zone 5
 Zone 6

Allowable Height: _____

CONSISTENCY DETERMINATION

This proposed Project is: Exempt from the ALUCP Consistent Consistent with Conditions Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

Airport Planner Signature: _____

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT

CD No.: 2020-021
PALU No.: _____

PROJECT CONDITIONS

1. The maximum height limit for the project site is 120 feet and as such, any construction equipment such as cranes or any other equipment exceeding 120 feet in height will need a determination of "No Hazard" from the FAA. An FAA Form 7460-1 for any temporary objects will need be filed and approved by the FAA prior to operating such equipment on the project site during construction.

FILE NOS: PSPA20-006 and PMTT20-012

SUBJECT: An Amendment to the Subarea 29 Specific Plan (File No. PSPA20-006) to increase the overall density within Planning Area 27 (Cluster Homes – 7-14 du/ac) from 4.8 to 4.9 dwelling units per gross acre and establish a new residential product type (Motorcourt Cluster D – 8-Plex), in conjunction with a Tentative Tract Map (File No. PMTT20-012 / TTM 20389) to subdivide 5.99 acres of land into one numbered lot and three lettered lots for condominium purposes. The project is bounded by Merrill Avenue to the north, Southern California Edison easement to the west, and the San Bernardino County Flood Control District and the City of Eastvale to the south; (APN: 0218-331-42) **submitted by SL Ontario Development Company, LLC. City Council action is required.**

PROPERTY OWNER: SL Ontario Development Company, LLC

RECOMMENDED ACTION: That the Planning Commission consider and adopt the following:

[1] A resolution recommending that the City Council approve the use of an Addendum to the Subarea 29 Specific Plan Environmental Impact Report (State Clearinghouse No. 2004011009); and


[2] A resolution recommending that the City Council approve File No. PSPA20-006 pursuant to the facts and reasons contained in this staff report and the attached resolution; and

[3] A resolution approving File No. PMTT20-012 (TTM 20389) pursuant to the facts and reasons contained in this staff report and the resolution, and subject to the conditions of approval.

PROJECT SETTING: The project site is comprised of 5.99 acres of land bounded by Merrill Avenue to the north, Southern California Edison easement to the west,



Figure 1: Project Location

Case Planner:	Jeanie Irene Aguilo
Planning Director Approval:	
Submittal Date:	12/15/2020

Hearing Body	Date	Decision	Action
DAB	05/17/2021	Approval	Recommend
PC	05/25/2021		Final
CC			

and the San Bernardino County Flood Control District and the City of Eastvale to the south, and is depicted in Figure 1: Project Location, above. The project site was historically utilized for agricultural dairy purposes. The site has been cleared of any structures and is presently vacant. The natural vegetation and soil conditions that once occurred throughout the project area have been significantly altered through agricultural activities, leaving little to no native vegetation. There is an existing 255-foot-wide SCE Easement located along the western property line that contain 180-foot-tall transmission towers (115kV). The existing surrounding land uses, zoning, and general plan and specific plan land use designations are summarized in the "Surrounding Zoning & Land Uses" table located in the Technical Appendix of this report.

PROJECT ANALYSIS:

(1) Background — The Subarea 29 Specific Plan and related Environmental Impact Report (EIR; State Clearinghouse No. 2004011009) were approved by the City Council on October 17, 2006. The Specific Plan established the land use designations, development standards, and design guidelines, which included the potential development of 2,470 dwelling units and up to 87,000 square feet of commercial uses for the Specific Plan Area.

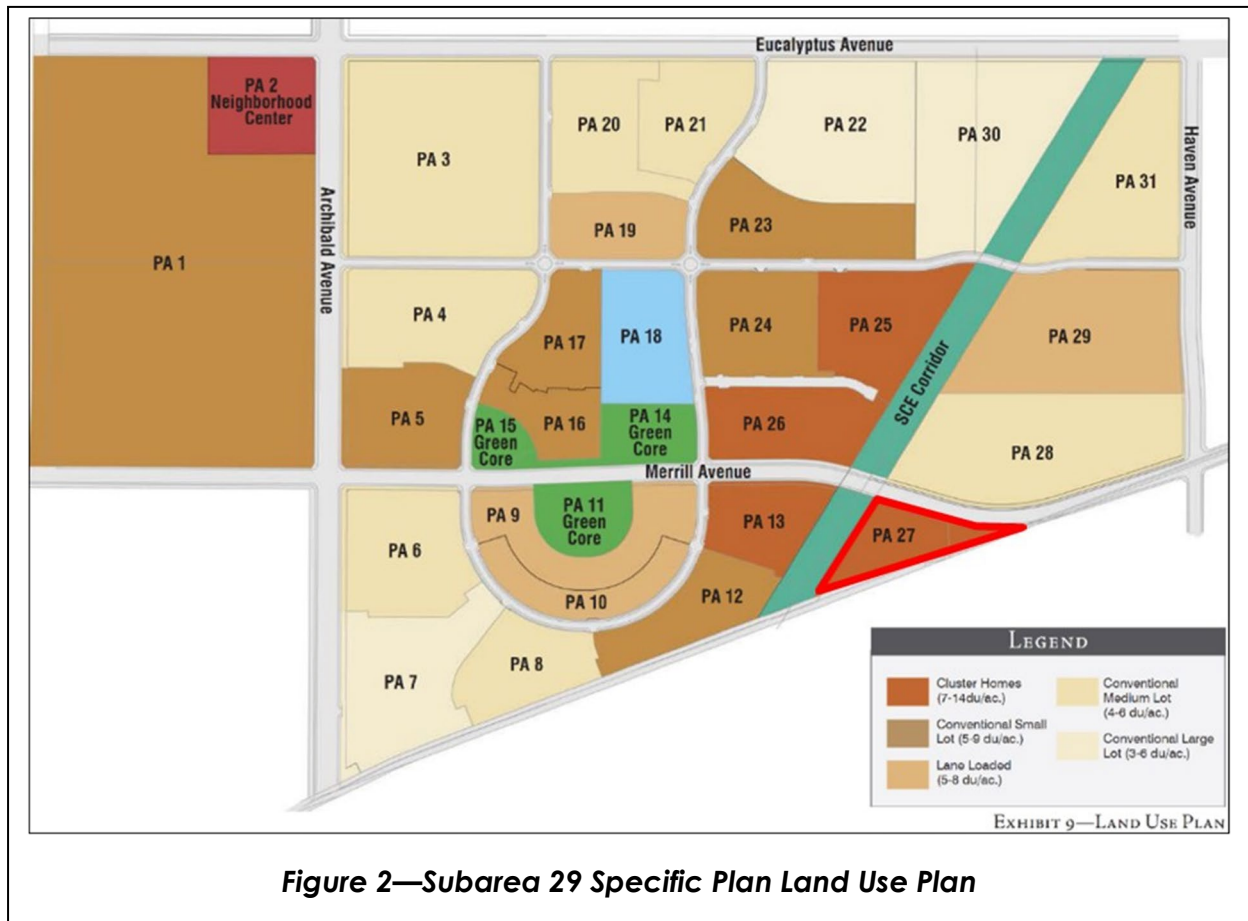
On December 15, 2020, the Applicant submitted an Amendment to the Subarea 29 Specific Plan (File No. PSPA20-006) to increase the overall density from 4.8 to 4.9 dwelling units per gross acre and establish a new residential product type (Motorcourt Cluster D – 8-Plex). The proposed change will increase the number of units within Planning Area 27 (Cluster Homes – 7-14 du/ac) from 47 to 73 dwelling units, for a total increase of 2,392 dwelling units to 2,418 dwelling units (See Figure 2—Subarea 29 Specific Plan Land Use Plan).

In addition, to the Subarea 29 Specific Plan Amendment, the Applicant has submitted a Tentative Tract Map application (File No. PMTT20-012/TTM 20389) to subdivide 5.99 acres of land into one numbered lot and three lettered lots for condominium purposes.

On May 17, 2021, the Development Advisory Board ("DAB") conducted a hearing to consider the Tentative Tract Map, and concluded the hearing, voting to recommend that the Planning Commission approve the Application subject to conditions of approval, which are included as attachments to the Planning Commission resolutions.

(2) Amendment to the Subarea 29 Specific Plan ("SPA") (File No. PSPA20-006) — To accommodate the proposed Tentative Tract Map application, the SPA proposes to increase the overall residential density of the Subarea 29 Specific Plan from 4.8 to 4.9 dwelling units per gross acre. Adjusting the total unit count implements the Low-Density land use designation of the General Plan while maintaining the neighborhood planning and architectural design standards of the original Specific Plan. This Specific Plan Amendment increases the total number of allocated dwelling units from 2,392 dwelling units to 2,418 dwelling units. Specifically, the planned gross density within Planning Area 27 (Cluster Homes – 7-14 du/ac) will increase from 6.2 to 9.7 dwelling units per gross acre

resulting in an increase from 47 to 73 dwelling units (See Exhibit D: Subarea 29 Specific Plan Land Use Summary). In addition, the Applicant has proposed to establish a new residential product type (Motorcourt Cluster D – 8-Plex) to Planning Area 27.



The Specific Plan currently allows for a Motorcourt Cluster A (6- or 8- Plex), Paseo Cluster Lots, Motorcourt Cluster B (6-Plex), Motorcourt Cluster C (5-Plex), and Green Courts. The new Motorcourt Cluster D (8-Plex) product will allow for a cluster of up to 8 single-family homes, with a smaller building footprint. The single-family homes will be clustered around a motorcourt (See Exhibit D: Motorcourt Cluster D (8-Plex)) with vehicle access taken from the shared motorcourt and pedestrian access will be taken from a paseo. Parking for residents will be provided in garages or uncovered parking spaces and each unit will be provided private open space within the side yard.

All changes and additions to the Specific Plan (exhibits, tables, development standards and design guidelines) are contained within the revised Specific Plan document accompanying this report. All additions to the Specific Plan have been highlighted in red and all deletions have been highlighted in red with a strikethrough.

Approval of the related Tentative Tract Map, discussed below, is contingent upon the approval of the proposed Specific Plan Amendment and related Addendum to the Subarea 29 Specific Plan Environmental Impact Report (State Clearinghouse No. 2004011009) certified by the City Council on April 21, 2015.

(3) Tentative Tract Map (File No. PMTT20-012 / TTM 20389) — Tentative Tract Map 20389 proposes to subdivide 5.99 acres of land into one numbered lot for condominium purposes and three lettered lots for common areas, private streets, public utility easements and neighborhood landscape edges (see Exhibit B—Tentative Tract Map, attached). The map will implement the required infrastructure improvements to serve the site. These improvements include the backbone infrastructure and the internal street circulation for residential neighborhoods (Private Streets A and B and Private Drives A through I). The Tentative Tract Map will accommodate the proposed Motorcourt Cluster D – 8-Plex product type and allow up to 73 units (see, Exhibit C—Conceptual Site Plan, attached). The future development of the site will require review and approval of a Development Plan.

(4) Site Access/Circulation — The project will have two access points from Merrill Avenue, which runs east-west along the northern frontage. The developer is responsible for the construction of the remaining backbone street improvements and all interior neighborhood streets necessary to serve the project.

(5) Architecture — Future development of the site will be required to meet all Development Code and Subarea 29 Specific Plan standards regarding architecture, including style, decorative elements and enhancements.

(6) Landscaping — Future development of the site will be required to meet all Development Code and Subarea 29 Specific Plan standards regarding landscaping. The Policy Plan (Policy PR1-6) requires new developments to provide a minimum of 2 acres of private park per 1,000 residents. The proposed project is required to provide a 0.56-acre park to meet the minimum Policy Plan private park requirement. The site will include landscaped parkways along the public and private streets, and homes fronting the private streets will have a small front landscape area maintained by the Homeowners Association. The final landscape plan, including the park amenities and planting plan, will be reviewed and approved as part of the Development Plan process.

(7) Utilities (drainage, sewer) — The project will be required to undergo a more extensive Preliminary Water Quality Management Plan (“PWQMP”) review as part of the Development Plan process. The PWQMP establishes project compliance with storm water discharge/water quality requirements and includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development (“LID”) best management practices (“BMPs”), such as retention and infiltration, biotreatment, and evapotranspiration. Additionally, the Project is consistent with the previously approved Development Agreement (File No. PDA16-001)

that required all major backbone infrastructure improvements within the Subarea 29 Specific Plan.

(8) Covenants, Conditions and Restrictions ("CCR&Rs") — As a Condition of Approval, CC&R's will be required to be prepared and recorded with the final map. The CC&R's will outline the maintenance responsibilities for the open space areas, recreation amenities, drive aisles, utilities, and upkeep of the entire site to ensure the on-going maintenance of the common areas and facilities.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

(1) City Council Goals.

- Invest in the Growth and Evolution of the City's Economy
- Maintain the Current High Level of Public Safety
- Operate in a Businesslike Manner
- Focus Resources in Ontario's Commercial and Residential Neighborhoods
- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)
- Ensure the Development of a Well Planned, Balanced, and Self-Sustaining Community in the New Model Colony

(2) Vision.

Distinctive Development:

- Commercial and Residential Development
 - Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

(3) Governance.

Decision Making:

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
 - G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

(4) Policy Plan (General Plan).

Land Use Element:

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.
 - LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.
 - LU1-6 Complete Community: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).
- Goal LU2: Compatibility between a wide range of uses.
 - LU2-6: Infrastructure Compatibility: We require infrastructure to be aesthetically pleasing and in context with the community character.

Housing Element:

- Goal H2: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.
 - H2-4 New Model Colony. We support a premier lifestyle community in the New Model Colony distinguished by diverse housing, highest design quality, and cohesive and highly amenitized neighborhoods.
 - H2-5 Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices and other best practices.
- Goal H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.
 - H5-2 Family Housing. We support the development of larger rental apartments that are appropriate for families with children, including, as feasible, the provision of services, recreation and other amenities.

Community Economics Element:

- Goal CE1: A complete community that provides for all incomes and stages of life.

- CE1-6 Diversity of Housing. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.

- Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.

- CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.

- CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.

- CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.

- CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Safety Element:

- Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.

- S1-1 Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

Community Design Element:

- Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

- CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.

➤ CD1-2 Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.

➤ CD1-3 Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.

▪ Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.

➤ CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:

- Building volume, massing, and height to provide appropriate scale and proportion;
- A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

➤ CD2-2 Neighborhood Design. We create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:

- A pattern of smaller, walkable blocks that promote access, activity and safety;
- Variable setbacks and parcel sizes to accommodate a diversity of housing types;
- Traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows;
- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the "outdoor living room"), as appropriate; and
- Landscaped parkways, with sidewalks separated from the curb.

➤ CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.

➤ CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding

physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

➤ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➤ CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.

➤ CD2-11 Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.

➤ CD2-12 Site and Building Signage. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.

➤ CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

▪ Goal CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

➤ CD3-1 Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.

➤ CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.

➤ CD3-3 Building Entrances. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces.

➤ CD3-5 Paving. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.

➤ CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.

▪ Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.

➤ CD5-1 Maintenance of Buildings and Property. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.

➤ CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

HOUSING ELEMENT COMPLIANCE: The project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and with approval of the related Specific Plan Amendment (File No. PSPA20-006) the proposed project will be consistent with the number of dwelling units and density (7-14 DU/AC) within Planning Area 27, as specified in the Available Land Inventory.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. The project site is also located within the Airport Influence area of Chino Airport and is consistent with policies and criteria set forth within the 2011 California Airport Land Use Planning Handbook published by the California Department of Transportation, Division of Aeronautics. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

ENVIRONMENTAL REVIEW: An Addendum to the Subarea 29 Specific Plan Environmental Impact Report (State Clearinghouse No. 2004011009) has been prepared. The Addendum concluded that the amendment to the Subarea 29 Specific Plan and Tentative Tract Map introduces no new significant environmental impacts.

CONDITIONS OF APPROVAL: See attached department reports.

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

	<i>Existing Land Use</i>	<i>General Plan Designation</i>	<i>Zoning Designation</i>	<i>Specific Plan Land Use</i>
<i>Site</i>	Mass Graded	Low Density Residential	Subarea 29 Specific Plan	Planning Area 27 (Cluster Homes)
<i>North</i>	Residential	Low Density Residential	Subarea 29 Specific Plan	Planning Areas 28 (Conventional Medium Lot)
<i>South</i>	Residential	City of Eastvale – Medium Density Residential	City of Eastvale – R-1 One Family Dwellings	N/A
<i>East</i>	Residential	City of Eastvale – Medium Density Residential	City of Eastvale – PRD-Planned Residential Developments	N/A
<i>West</i>	Southern California Edison Easement	Open Space Non-Residential (OS-NR)	Subarea 29 Specific Plan	SCE Corridor

Exhibit A—PROJECT LOCATION MAP

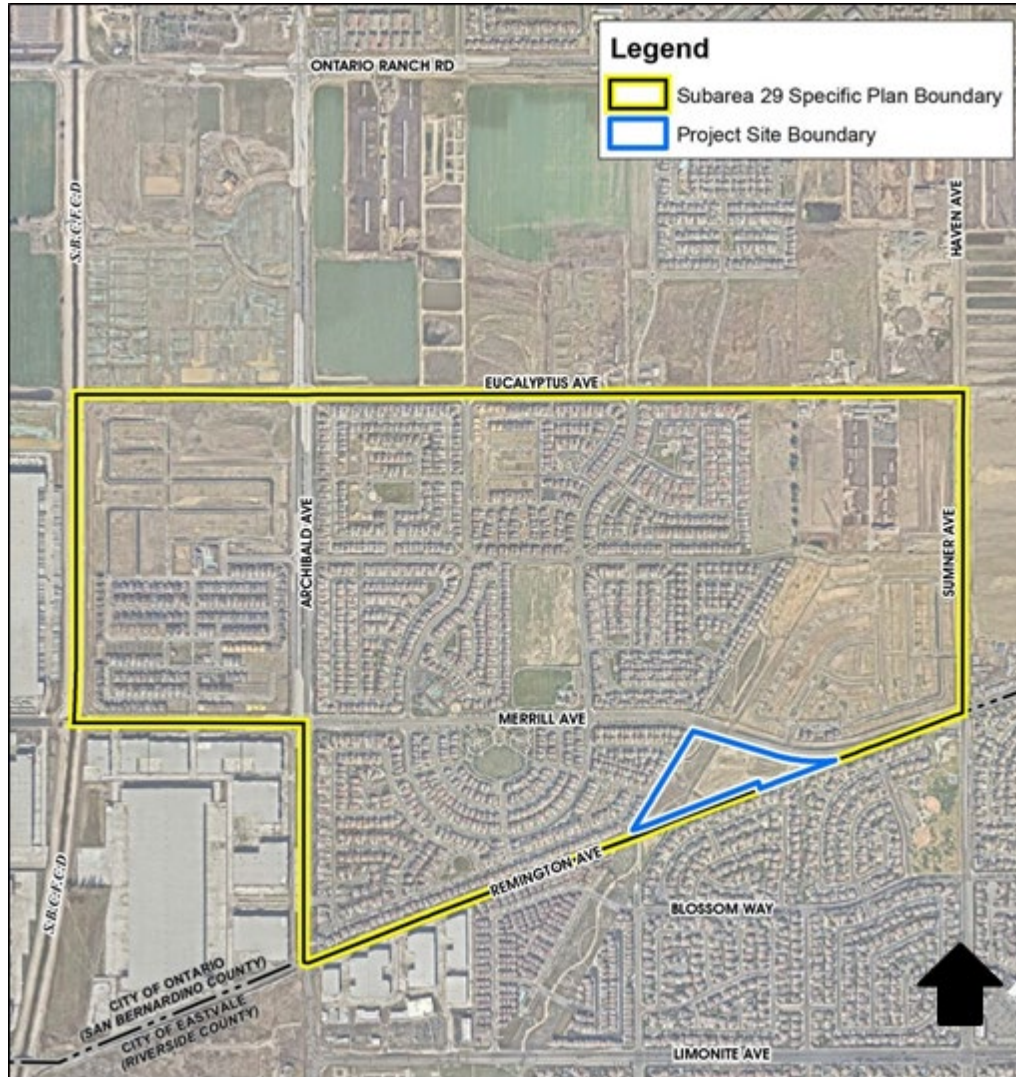


Exhibit B—TENTATIVE TRACT MAP

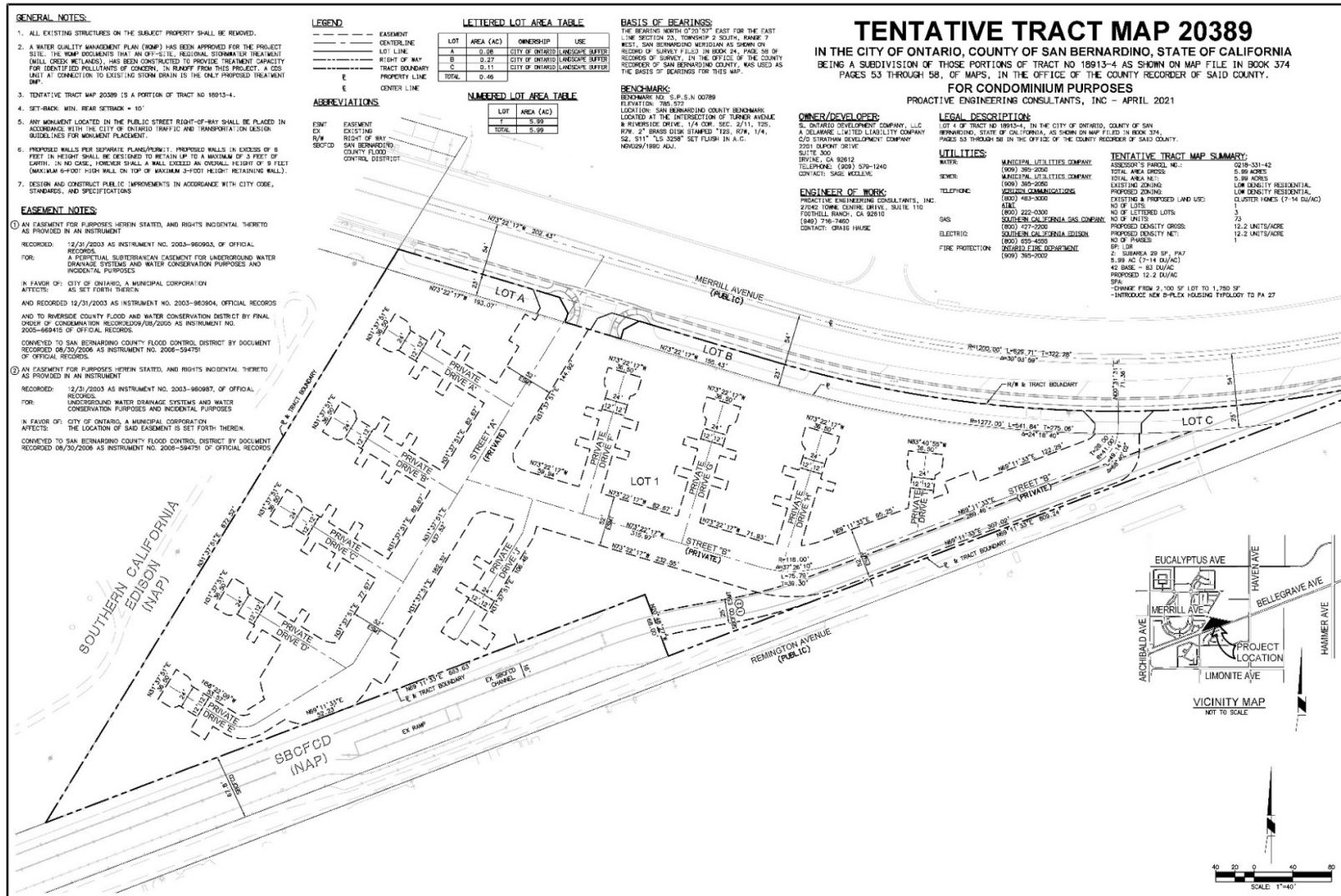


Exhibit C—CONCEPTUAL SITE PLAN



Site Summary:

Product :	Liberty Deluxe
Total Homes :	73
Site Area :	5.99 Acres
(measured to neighborhood edge)	
Density :	12.2 Homes/Acre
Parking Required : Small Lot & Condo	
2 Garage Spaces/ Unit	
73 x 2 =	146 Spaces
0.25 Guest Space/Unit	
73 x 0.25 =	18.25 Spaces
Total Spaces Required =	164.25 Spaces
Parking Provided :	
2 Garage Spaces /Unit	= 146 Spaces
Uncovered Spaces	= 65 Spaces
	211 (2.89:1)

Exhibit D—SUBAREA 29 SPECIFIC PLAN LAND USE SUMMARY

TABLE I—SPECIFIC PLAN LAND USE SUMMARY

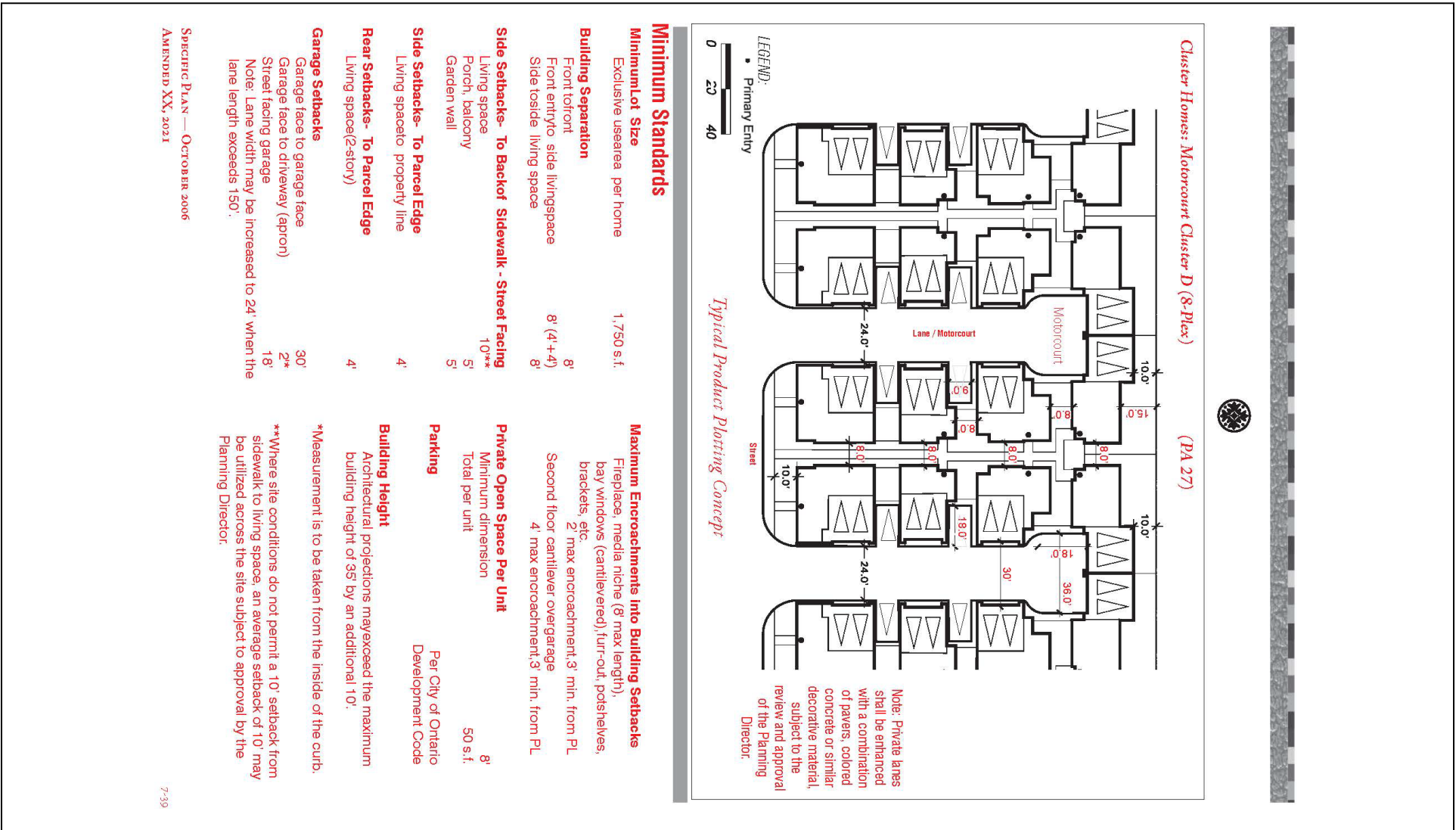
Planning Area	Land Use	Minimum Lot Size (S.F.)	Planned Dwelling Units ^{2,4,5}	Net Acre ¹	Planned Net Density (Dw/Ac) ^{3,5}	Gross Acres ²	Planned Gross Density (Dw/Ac) ^{3,5}
1*	Conventional Small Lot	3,500	432	83.1	5.2	89.8	4.8
2*	Commercial	N/A	0	12.1	0.0	14.5	0.0
3*	Conventional Medium Lot	4,500	186	34.5	5.4	40.2	4.6
4**	Conventional Medium Lot	4,250	88	10.1	8.7	17.8	4.9
5**	Conventional Small Lot	3,825	68	7.2	9.4	13.7	5.0
6**	Conventional Medium Lot	5,000	67	13.0	5.2	17.0	4.0
7**	Conventional Large Lot	6,300	65	15.3	4.2	18.3	3.6
8**	Conventional Medium Lot	4,250	46	9.1	5.1	9.6	4.8
9**	Lane Loaded	3,150	69	9.9	7.0	11.9	5.8
10**	Lane Loaded	3,600	57	6.6	8.7	7.8	7.3
11**	Neighborhood Park 2	N/A	0	5.7	0.0	6.2	0.0
12**	Conventional Small Lot	3,825	53	9.5	5.6	9.5	5.6
13**	Cluster Homes	2,100 ⁶	75	7.8	9.6	7.8	9.6
14**	Neighborhood Park 1	N/A	0	6.3	0.0	7.7	0.0
15**	Recreation Center	N/A	0	2.7	0.0	3.1	0.0
16**	Conventional Small Lot	3,015	41	5.9	7.0	6.1	6.8
17**	Conventional Small Lot	3,015	56	5.3	10.6	8.4	6.7
18**	School	N/A	0	10.0	N/A	11.2	N/A
19**	Lane Loaded	3,150	61	7.8	7.9	9.0	6.8
20**	Conventional Medium Lot	4,250	67	11.8	5.7	13.3	5.0
21**	Conventional Medium Lot	5,000	48	10.1	4.8	11.5	4.2
22**	Conventional Large Lot	6,300	79	19.7	4.0	21.3	3.7
23**	Conventional Small Lot	3,825	82	12.9	6.3	14.4	5.7
24**	Conventional Small Lot	3,400	75	8.1	9.3	12.8	5.8
25**	Cluster Homes	2,100 ⁶	102	8.6	11.8	12.9	7.9
26**	Cluster Homes	2,100 ⁶	102	8.7	11.7	13.2	7.7
27**	Cluster Homes	1,750 ⁶ 2,100 ⁶	73 47	7.6	9.7 6.2	7.6	9.6 6.2
28*	Conventional Medium Lot	4,050	121	23.0	5.3	25.8	4.7
29***	Lane Loaded or Conventional Medium Lot	3,150 or 4,000	108	21.4	5.0	27.2	4.0
30*	Conventional Large Lot	5,040	110	21.9	5.0	28.3	3.9
31*	Conventional Medium Lot	4,050	87	16.0	5.4	23.1	3.8
Flood Control Channel	Flood Control Channel	N/A	0	7.2	0.0	7.2	0.0
Pump Station	Pump Station	N/A	0	0.2	0.0	0.4	0.0
SCE Corridor	Park Place SCE Easement	N/A	0	11.2	0.0	11.2	0.0
Sub Area 29 Total			2,418 2,242	449.9	5.4 5.2	539.7	4.3 4.4

- A) Minimum square footage identified is for exclusive use area on a per home basis, recorded lot size may differ.
- 1) Gross Acres calculated to centerlines of Master Planned streets minus SCE easements.
- 2) Net Acres excludes street rights-of-way and SCE easements.
- 3) Actual total units and gross/net density and acreage will be dependent on final lotting.
- 4) Target unit count based on submitted Tentative "B" Maps.
- 5) A density transfer of 15.0% may occur between Planning Areas.

- * Indicates Planning Areas as controlled/owned by Richland Communities, Inc.
- ** Indicates "Park Place" Planning Areas as controlled/owned by St. Ontario Development Company
- *** Indicates Planning Areas as controlled/owned by Brookfield Homes

Specific Plan — OCTOBER 2006
 AMENDED XX, 2021

Exhibit E—MOTORCOURT CLUSTER D (8-PLEX)



Minimum Standards

Minimum Lot Size

Exclusive use area, per home 1,750 s.f.

Building Separation

Front to front 8'
 Front entry to side living space 8' (4' + 4')
 Side to side living space 8'

Side Setbacks- To Back of Sidewalk - Street Facing

Living space 10'***
 Porch, balcony 5'
 Garden wall 5'

Side Setbacks- To Parcel Edge

Living space to property line 4'

Rear Setbacks- To Parcel Edge

Living space (2-story) 4'

Garage Setbacks

Garage face to garage face 30'
 Garage face to driveway (apron) 2**
 Street facing garage 18'
 Note: Lane width may be increased to 24' when the lane length exceeds 150'.

Maximum Encroachments into Building Setbacks

Fireplace, media niche (8' max length), bay windows (cantilevered), jurr-out, potshelves, brackets, etc.

Second floor cantilever overgarage

4' max encroachment, 3' min. from PL

Private Open Space Per Unit

Minimum dimension 8'
 Total per unit 50 s.f.

Parking

Per City of Ontario Development Code

Building Height

Architectural projections may exceed the maximum building height of 35' by an additional 10'.

**Measurement is to be taken from the inside of the curb.

***Where site conditions do not permit a 10' setback from sidewalk to living space, an average setback of 10' may be utilized across the site subject to approval by the Planning Director.

Specific Plan — OCTOBER 2006
 AMENDED XX, 2021

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING THE CITY COUNCIL APPROVE THE USE OF AN ADDENDUM TO THE SUBAREA 29 SPECIFIC PLAN ENVIRONMENTAL IMPACT REPORT, PURSUANT TO THE REQUIREMENTS OF THE CALIFORNIA ENVIRONMENTAL QUALITY ACT, AS AMENDED, FOR FILE NO. PSP03-003

WHEREAS, SL ONTARIO DEVELOPMENT COMPANY, LLC (hereinafter referred to as "Applicant") has filed an Application for the approval of an Amendment to the Subarea 29 Specific Plan (File No. PSPA20-006) to increase the overall density within Planning Area 27 (Cluster Homes – 7-14 du/ac) from 4.8 to 4.9 dwelling units per gross acre and establish a new residential product type (Motorcourt Cluster D – 8-Plex) in conjunction with a Tentative Tract Map (File No. PMTT20-012 / TTM 20389) to subdivide 5.99 acres of land into one numbered lot and three lettered lots for condominium purposes, bounded by Merrill Avenue to the north, Southern California Edison easement to the west, and the San Bernardino County Flood Control District and the City of Eastvale to the south, in the City of Ontario, California (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the property to the north of the Project site is within the Planning Area 28 (Conventional Medium Lot) land use designation of the Subarea 29 Specific Plan and a residential subdivision in currently under construction. The properties to the east are located in the City of Eastvale within R-1 land use district and developed with residential land uses. The properties to the south are located in the City of Eastvale within the PRD (Planned Residential Development) land use district and developed with residential land uses. The property to the west is located within the Subarea 29 Specific Plan and has an SCE Corridor land use designation that is developed with SCE 180-foot-tall transmission towers (115kV) and power lines; and

WHEREAS, the Subarea 29 Specific Plan Environmental Impact Report (State Clearinghouse No. 2004011009) was certified on October 17, 2006 (hereinafter referred to as "Certified EIR"), in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario has prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as "EIR Addendum") in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as "CEQA"); and

WHEREAS, the EIR Addendum concluded that implementation of the Project could result in a number of significant effects on the environment that were previously

analyzed in the Certified EIR, and that the Certified EIR identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, pursuant to State CEQA Guidelines Section 15164(a), a lead agency shall prepare an addendum to a previously certified EIR if some changes or additions are necessary to a project, but the preparation of a subsequent or supplemental EIR is not required; and

WHEREAS, the City determined that none of the conditions requiring preparation of a subsequent or supplemental EIR would occur from the Project, and that preparation of an Addendum to the Certified EIR was appropriate; and

WHEREAS, the City of Ontario is the lead agency on the Project, and the Planning Commission is the recommending authority for the requested approval to construct and otherwise undertake the Project; and

WHEREAS, the Planning Commission has reviewed and considered the EIR Addendum for the Project, has concluded that none of the conditions requiring preparation of a subsequent of supplemental EIR have occurred, and intends to take actions on the Project in compliance with CEQA and state and local guidelines implementing CEQA; and

WHEREAS, the EIR Addendum for the Project are on file in the Planning Department, located at 303 East B Street, Ontario, CA 91764, are available for inspection by any interested person at that location and are, by this reference, incorporated into this Resolution as if fully set forth herein; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the recommending authority for the Project, The Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The environmental impacts of this project were reviewed in conjunction with an Addendum to the Subarea 29 Specific Plan Environmental Impact Report (State Clearinghouse No. 2004011009) was certified on October 17, 2006 in conjunction with File No. PSP03-003.

(2) The EIR Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines.

(3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts.

(4) All previously adopted mitigation measures shall be a condition of project approval, as they are applicable to the Project, and are incorporated herein by this reference.

(5) The EIR Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and

(6) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts.

SECTION 2: *Additional Environmental Review Not Required.* Based on the Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Environmental Impact Report is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and.

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

(d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: *Planning Commission Action.* Based upon the findings and conclusions set forth in Sections 1 and 2, above, the Planning Commission hereby recommends the City Council finds that based upon the entire record of proceedings before it, and all information received, that there is no substantial evidence that the Project will constitute substantial changes to the Certified EIR, and does hereby approve the EIR Addendum, attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 4: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 5: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 6: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of May 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 25, 2021, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**Addendum to the Subarea 29 Specific Plan
Environmental Impact Report**

(Addendum to follow this page)



California Environmental Quality Act Environmental Checklist Form

Project Title/File No(s): Park Place/Planning Area 27 – PSPA20-006 and PMTT20-012 (TTM 20389)

Lead Agency: City of Ontario, 303 East "B" Street, Ontario, California 91764, (909) 395-2036

Contact Person: Jeanie Irene Aguilo, Associate Planner

Project Sponsor: SL Ontario Development Company, LLC, 1156 N. Mountain Avenue, Upland, CA 91786

Project Location and Setting: The 5.99-net-acre¹ Project site includes Planning Area 27 of the Subarea 29 Specific Plan and is located in southwestern San Bernardino County, within the City of Ontario. The City of Ontario is located approximately 40 miles from downtown Los Angeles, 20 miles from downtown San Bernardino, and 30 miles from Orange County (refer to Figure 1, Regional Location Map, and Figure 2, Vicinity Map). As illustrated on Figure 3, the Project site is located south of Merrill Avenue, approximately 1,000 feet of Celebration Avenue).

The Project site is vacant but was previously graded; the Southern California Edison (SCE) corridor extends along the western portion of the Project site (refer to Figure 3, Aerial Photograph). The areas surrounding the site to the west, east and south are developed with residential uses. The area to the north of the Project site is currently under construction.

General Plan Designation: Low Density Residential (2.1 – 5 du/ac) and within the Chino Airport Overlay area.

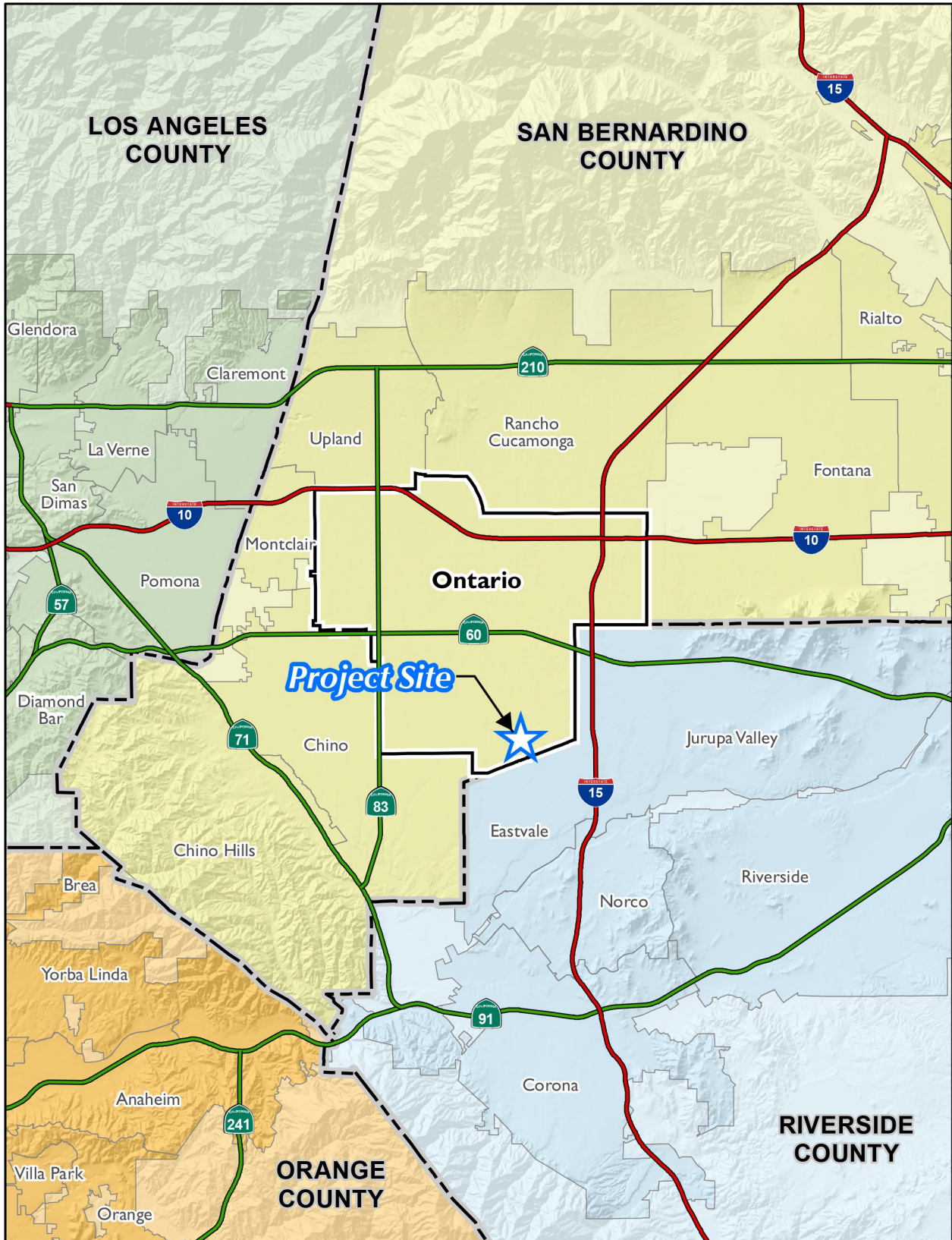
Zoning: SP – Subarea 29 Specific Plan

Description of Project: The proposed Project involves an amendment to the Subarea 29 Specific Plan (File No. PSPA20-006) to: (1) increase the number of allowed units within Planning Area 27 (Cluster Homes – 7-14 du/ac) from 47 dwelling units to 73 dwelling units (an increase of 26 dwelling units), which would increase the total number of allowed units in the Subarea 29 Specific Plan area from 2,392 dwelling units to 2,418 dwelling units; and (2) provide an additional housing typology (Motorhome Cluster D – 8-Plex). The modified Specific Plan Land Use Summary table and additional housing typology are provided in Attachment A of this document). With the increase in units, the overall density in the Specific Plan area would increase from 4.8 dwelling units per gross acre to 4.9 dwelling units per gross acre, consistent with the Low Density Residential (2.1 – 5 du/ac) land use designation in The Ontario Plan (TOP). Additionally, the proposed Project involves a Tentative Tract Map (TTM 20389) (File No. PMTT20-012) to subdivide 5.99 acres of land into one numbered lot and three lettered lots for condominium purposes (refer to proposed TTM 20389 included in Attachment B of this document).

The proposed Specific Plan Amendment and TTM 20389 would allow for the development of 73 cluster homes, which are single-family detached residential units with vehicular access from lanes (private alleys or motorcourts) via interior streets, with direct access garages. A conceptual site plan for the proposed development within Planning Area 27 is provided on Figure 4. As shown, access would be provided from two locations along Merrill Avenue. The proposed development would be implemented in compliance with Development and Subdivision Regulations contained in the Subarea 29 Specific Plan, and Ontario Development Code Chapters 5.0 and 6.0 (e.g., setbacks and separations, landscaping, parking and circulation).

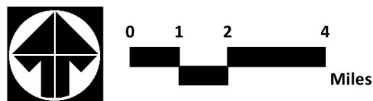
Background: In October 2006, the City of Ontario approved the Subarea 29 Specific Plan (File No. PSPA20-006). The Subarea 29 Specific Plan has been subsequently amended through April 2015. The 2015 amendment included the addition of 99 dwelling units, allowing up to 2,392 units to be developed in the Specific Plan area. The Subarea 29 Specific Plan establishes the land use designations, infrastructure and services, development standards, and design guidelines for the approximately 540-gross-acre Subarea 29 Specific Plan area located east of the Cucamonga Creek Channel, south of Eucalyptus Avenue, north of Bellegrave Avenue and west of Haven Avenue. The southern boundary of the Specific Plan area is also

¹ The Project site is 7.6 gross acres, inclusive of the Southern California Edison (SCE) easement and public right-of-way; Assessor Parcel Number (APN) 0218-331-42.

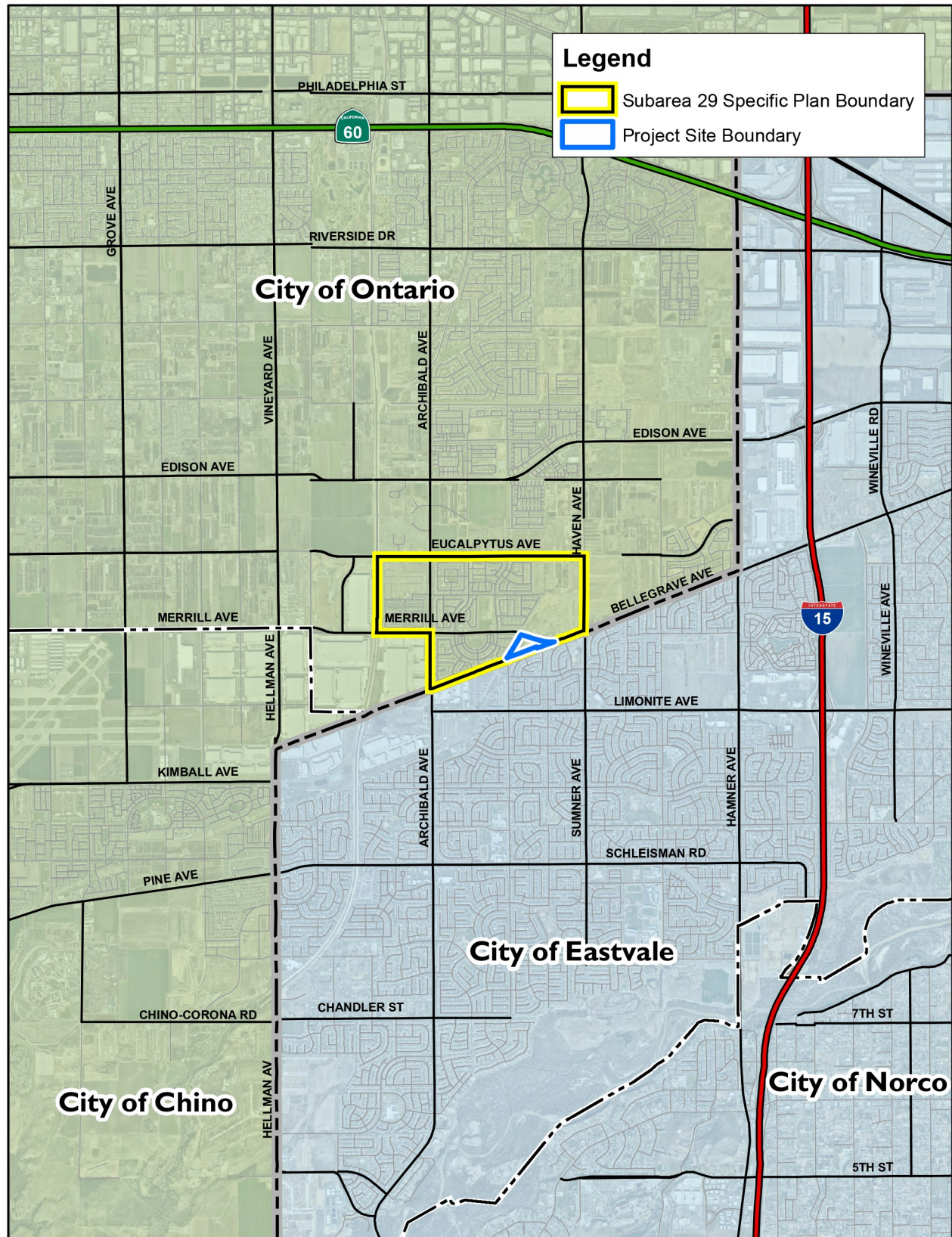


Source(s): ESRI, RCTLMA (2021), SCAG (2021), SB County (2020)

Figure 1

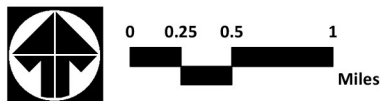


Regional Location Map



Source(s): ESRI, Nearmap Imagery (2021), RCTLMA (2021), SCAG (2021), SB County (2020)

Figure 2



Vicinity Map

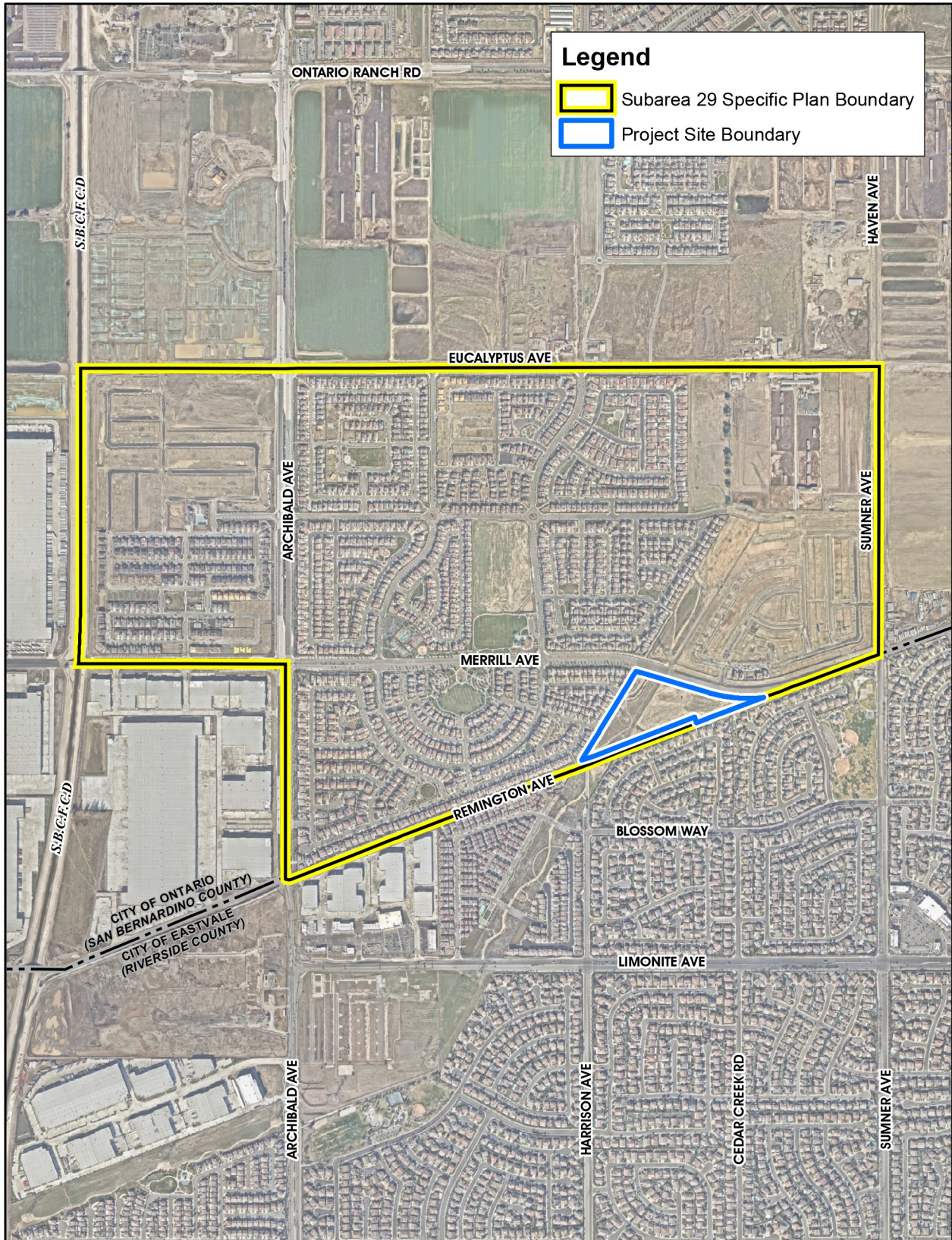
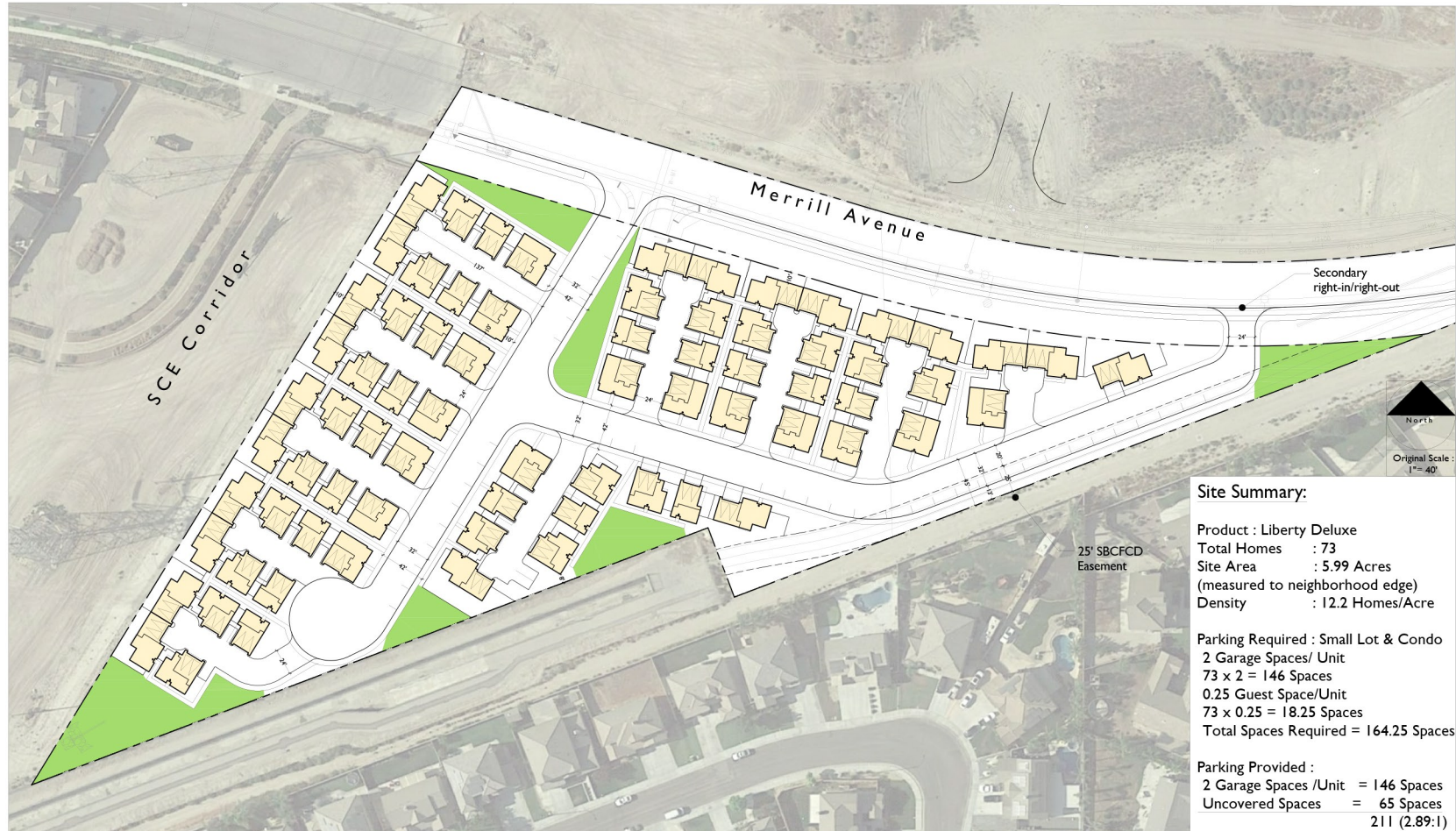


Figure 3



Aerial Photograph



Source(s): Bassenian | Lagoni (02-25-2021)

Figure 4



Not to Scale

Conceptual Site Plan

the jurisdictional boundary between the cities of Ontario and Eastvale, and the counties of San Bernardino and Riverside.

The *Subarea 29 (Hettinga) Specific Plan Final Environmental Impact Report* (Subarea 29 Specific Plan Final EIR) (State Clearinghouse [SCH] No. 2004011009) was prepared for the Subarea 29 Specific Plan and was certified by the City Council in October 2006. The Subarea 29 Specific Plan Final EIR analysis concluded that implementation of the Subarea 29 Specific Plan would result in significant and unavoidable impacts related to loss of Farmland (project and cumulative), air quality impacts (project and cumulative), cumulative surface water quality impacts (due to impaired receiving waters), cumulative traffic-related noise, operational cumulative traffic impacts, and cumulative solid waste generation. A Statement of Overriding Considerations was adopted by the City for these impacts. Mitigation measures were adopted to reduce impacts to less than significant impacts to the extent feasible, and those mitigation measures will continue to apply to development in the Subarea 29 Specific Plan area, including the proposed Project. An Addendum to the Subarea 29 Specific Plan Final EIR was prepared for the 2015 Specific Plan Amendment.

Prior to adoption of TOP, the New Model Colony (NMC) General Plan Amendment established the land uses within the entire NMC area and designated the Subarea 29 Specific Plan area as Low Density Residential (2.1 – 5 du/ac), Neighborhood Commercial, Open Space – Parkland, Open Space – Non Recreation, and Public School. In 2010, TOP was adopted and designated the Project site Low Density Residential (2.1 – 5 du/ac). The associated *The Ontario Plan Final Environmental Impact Report* (SCH No. 2008101140) (TOP Final EIR) was certified in January 2010, and is incorporated by reference in this Addendum. The TOP Final EIR analyzes the environmental impacts that would result from implementation of the TOP, focusing on changes to land use associated with the buildout of the Land Use Plan in the Policy Plan and impacts resultant of population and employment growth in the City. The TOP and TOP Final EIR anticipated 2,700 residential unit for the Subarea 29 Specific Plan area. The significant unavoidable adverse impacts that were identified in the TOP Final EIR include: agriculture resources, air quality, cultural resources, greenhouse gas emissions, noise and transportation/traffic.

CEQA REQUIREMENTS FOR AN ADDENDUM:

Section 15164(a) of the CEQA Guidelines outlines when an Addendum to an EIR is required, and states: “The lead agency or responsible agency shall prepare an addendum to a previously certified EIR if some changes or additions are necessary but none of the conditions described in Section 15162 calling for preparation of a subsequent EIR have occurred.”

Section 15162 of the CEQA Guidelines states: “When an EIR has been certified or a negative declaration adopted for a project, no subsequent EIR shall be prepared for that project unless the lead agency determines, on the basis of substantial evidence in light of the whole record, one or more of the following:

- A. When an EIR has been certified or a negative declaration adopted for a project, no subsequent EIR shall be prepared for that project unless the lead agency determines, on the basis of substantial evidence in the light of the whole record, one or more of the following:
 - (1) Substantial changes are proposed in the project which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;
 - (2) Substantial changes occur with respect to the circumstances under which the project is undertaken which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or
 - (3) New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the previous EIR was certified as complete or the negative declaration was adopted, shows any of the following:
 - (a) The project will have one or more significant effects not discussed in the previous EIR or negative declaration;
 - (b) Significant effects previously examined will be substantially more severe than shown in the previous EIR;

- (c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible, and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or
- (d) Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative.”

Thus, if the proposed Project does not result in any of the circumstances listed in section 15162 (i.e., no new or substantially greater significant impacts), the City may adopt an addendum to the Subarea 29 Specific Plan Final EIR.

Section 16164(e) of the CEQA Guidelines require that a brief explanation be provided to support the findings that no subsequent EIR is needed for further discretionary approval. These findings are described below, and are based on the analysis presented in this document:

1. *Required Finding: Substantial changes are not proposed for the project that will require major revisions of the previous EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects.* Substantial changes are not proposed with the Project and the proposed Project will not require revisions to the Subarea 29 Specific Plan Final EIR or TOP Final EIR. The Specific Plan is divided into 31 Planning Areas and the Subarea 29 Specific Plan Final EIR evaluated the impacts associated with the development capacity of 2,293 dwelling units; a subsequent Specific Plan Amendment increased the allowed number of units to 2,392 dwelling units, which was evaluated in the 2015 Addendum. The proposed Project involves a Specific Plan Amendment to increase the residential unit count in Planning Area 27 of the Subarea 29 Specific Plan by 26 dwelling units (from 47 dwelling units to 73 dwelling units). This would increase the total number of allowed residential units in the Specific Plan area by 1%, from 2,392 dwelling units to 2,418 dwelling units, an insubstantial increase. The proposed Specific Plan Amendment also involves the introduction of an additional housing typology (Motorhome Cluster D – 8-Plex). A TTM is also proposed (TTM 20389) to accommodate the proposed development in Planning Area 27. On January 26, 2010, the City of Ontario adopted TOP Policy Plan (General Plan). The SPA proposes a maximum of 2,418 residential units within the Subarea 29 Specific Plan which is less than the 2,700 residential unit development capacity established by the Policy Plan (General Plan) for the Specific Plan area. Additionally, the overall density of the Specific Plan area with the proposed Project would be 4.9 dwelling units per acre, which is consistent with the Policy Plan (General Plan) that allows up to 5 dwelling units per acre within the Low Density Residential (2.1 – 5 du/ac) land use designation. Additionally, the City’s water, recycled water, and sewer infrastructure would have sufficient capacity to serve the additional units with the proposed Specific Plan Amendment. There are no new significant impacts or a substantial increase in the severity of previously identified significant impacts due to the proposed Specific Plan Amendment. Therefore, no proposed changes or revisions to the EIR are required.
2. *Required Finding: Substantial changes have not occurred with respect to the circumstances under which the project is undertaken, which would require major revisions of the previous EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects.* As shown on the aerial photograph provided in Figure 3, the Project site (Planning Area 27) was previously graded and the areas surrounding the Project site are developed or under construction. Grading activities and other site disturbance were conducted in accordance with the mitigation requirements outlined in the Subarea 29 Specific Plan Final EIR, including measures required for the protection of biological, cultural, and paleontological resources. No sensitive biological resources, cultural resources, or paleontological resources existing at the Project site. No proposed changes or revisions to the EIR are required.

3. *Required Finding. No new information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the previous EIR was certified as complete, has been provided that would indicate: the proposed project would result in one or more significant effects not discussed in the previous EIR; significant effects previously examined would be substantially more severe than shown in the previous EIR; mitigation measures or alternatives previously found not to be feasible would in fact be feasible, and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or, mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternatives.* The Subarea 29 Specific Plan Final EIR did not address Global Climate Change impacts as required by Assembly Bill 32, passed in August of 2006. However, the impact of buildout of TOP on the environment due to the emission of greenhouse gases (GHGs) were analyzed in TOP Final EIR. According to TOP Final EIR, this impact would be significant and unavoidable. This EIR was certified by the City on January 27, 2010, at which time a statement of overriding considerations was also adopted for TOP's significant and unavoidable impacts, including that concerning the emission of greenhouse gases. Pursuant to Public Resources Code section 21083.3, this impact need not be analyzed further, because (1) the proposed buildout of the Subarea 29 Specific Plan would result in an impact that was previously analyzed in TOP Final EIR, which was certified by the City; (2) the proposed Project would not result in any GHG impacts that were not addressed in TOP Final EIR; (3) the proposed Project would only increase the number of residential units by 26 Low Density residential dwelling units and the total number of units in the Specific Plan area (2,481 units) is less than evaluated in TOP Final EIR. As part of the City's certification of TOP Final EIR and its adoption of TOP, the City adopted mitigation measures with regard to the significant and unavoidable impacts relating to GHG emissions. These mitigation measures are outlined in Section 6 of this Initial Study.

The Subarea 29 Specific Plan Final EIR evaluated the Riverside County Airport Land Use Compatibility Plan – Chino Airport, but the final report was not adopted prior to approval of the Subarea 29 Specific Plan Final EIR. Additionally, the Ontario International Airport (ONT) Land Use Compatibility Plan (ALUCP) was adopted by the City of Ontario in 2011. As determined by the analysis presented in this Addendum, the proposed Project would not result in any new impacts related to the Chino Airport that were not addressed in TOP Final EIR. As part of the City's certification of TOP Final EIR and its adoption of TOP, the City adopted mitigation measures with regard to the significant and unavoidable impacts relating to the Chino Airport. These mitigation measures are outlined in Section 6 of this Initial Study. There are no significant impacts associated with the ONT.

Lastly, Appendix G of the CEQA Guidelines was modified as part of the CEQA Guidelines updates that were approved in December 2018. New checklist topics related to Energy, Tribal Cultural Resources and Wildfire were added and some checklist questions for other environmental topics were revised. The Initial Study presented in this Addendum uses the updated checklist in Appendix G of the CEQA Guidelines. As identified through the analysis presented in this Addendum, there would be no new significant impacts resulting from the proposed Project related to the changes Appendix G of the CEQA Guidelines.

Conclusion:

Accordingly, and based on the findings and information contained in the previously certified the Subarea 29 Specific Plan Final EIR, the analysis above, the attached Initial Study, and the CEQA statute and CEQA Guidelines, including sections 15164 and 15162, the proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum, and the TOP Final EIR, as appropriate. No changes or additions to the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum or TOP Final EIR analyses are necessary, nor is there a need for any additional mitigation measures.

The attached Initial Study provides an analysis of the proposed Project and verification that the proposed Project will not cause environmental impacts such that any of the circumstances identified in CEQA Guidelines section 15162 are present.

Surrounding Land Uses:

	<u>Existing Land Use</u>	<u>General Plan Designation</u>	<u>Zoning Designation</u>	<u>Specific Plan Land Use</u>
City of Ontario				
Site	Vacant and SCE Corridor	Low Density Residential (2.1 – 5 du/ac) and Open Space – Non-Recreation	SP – Subarea 29 Specific Plan	Cluster Homes (7-14 du/acre)
North	Vacant	Low Density Residential (2.1 – 5 du/ac)	SP – Subarea 29 Specific Plan	Conventional Medium Lot (4-6 du/acre)
West:	Residential	Low Density Residential (2.1 – 5 du/ac)	SP – Subarea 29 Specific Plan Specific Plan	Cluster Homes (7-14 du/acre) and Conventional Small Lot (5-9 du/acre)
City of Eastvale				
South	Residential	Medium Density Residential	R-1 One-Family Dwellings	NA

du/acre – dwelling units per acre; NA – Not Applicable

Other public agencies whose approval is required (e.g., permits, financing approval or participation agreement): None.

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

- | | |
|---|---|
| <input type="checkbox"/> Aesthetics | <input type="checkbox"/> Agriculture and Forestry Resources |
| <input type="checkbox"/> Air Quality | <input type="checkbox"/> Biological Resources |
| <input type="checkbox"/> Cultural Resources | <input type="checkbox"/> Energy |
| <input type="checkbox"/> Geology / Soils | <input type="checkbox"/> Greenhouse Gas Emissions |
| <input type="checkbox"/> Hazards & Hazardous Materials | <input type="checkbox"/> Hydrology / Water Quality |
| <input type="checkbox"/> Land Use / Planning | <input type="checkbox"/> Mineral Resources |
| <input type="checkbox"/> Noise | <input type="checkbox"/> Population / Housing |
| <input type="checkbox"/> Public Services | <input type="checkbox"/> Recreation |
| <input type="checkbox"/> Transportation | <input type="checkbox"/> Tribal Cultural Resources |
| <input type="checkbox"/> Utilities / Service Systems | <input type="checkbox"/> Wildfire |
| <input type="checkbox"/> Mandatory Findings of Significance | |

DETERMINATION (To be completed by the Lead Agency):

On the basis of this initial evaluation:

- I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
- I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
- I find that the proposed project MAY have a "potentially significant" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed Project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier certified Subarea 29 Specific Plan Final EIR and associated 2015 Addendum pursuant to applicable standards, and (b) The certified TOP Final EIR and (c) have been avoided or mitigated pursuant to the earlier certified environmental documents, including revisions or mitigation measures that are imposed upon the proposed project, the analysis from the certified Subarea 29 Specific Plan Final EIR and associated 2015 Addendum, and the certified TOP EIR was used as a basis for this Addendum, and nothing further is required.

Signature Jeanie Irene Aguilo

Jeanie Irene Aguilo, Associate Planner
Printed Name

May 3, 2021
Date

City of Ontario Planning Department
For

EVALUATION OF ENVIRONMENTAL IMPACTS:

- 1) A brief explanation is required for all answers except "No Impact" answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g., the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general standards (e.g., the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).
- 2) All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
- 3) Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. "Potentially Significant Impact" is appropriate if there is substantial evidence that an effect is significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.
- 4) "Negative Declaration: Less Than Significant With Mitigation Incorporated" applies where the incorporation of mitigation measures has reduced an effect from "Potentially Significant Impact" to a "Less than Significant Impact." The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from the "Earlier Analyses" Section may be cross-referenced).
- 5) Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration. Section 15063(c)(3)(D). In this case, a brief discussion should identify the following:
 - a) Earlier Analyses Used. Identify and state where they are available for review.
 - b) Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
 - c) Mitigation Measures. For effects that are "Less than Significant with Mitigation Measures Incorporated," describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.
- 6) Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g., general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.
- 7) Supporting Information Sources. A source list should be attached, and other sources used or individuals contacted should be cited in the discussion.
- 8) This is only a suggested form, and lead agencies are free to use different formats; however, lead agencies should normally address the questions from this checklist that are relevant to a project's environmental effects in whatever format is selected.
- 9) The explanation of each issue should identify:
 - a) The significance criteria or threshold, if any, used to evaluate each question; and
 - b) The mitigation measure identified, if any, to reduce the impact to less than significance.

<i>Issues</i>	<i>Potentially Significant Impact</i>	<i>Less Than Significant With Mitigation</i>	<i>Less Than Significant Impact</i>	<i>Impacts Analyzed in Previous EIR</i>
1. AESTHETICS. Would the project:				
a. Have a substantial adverse effect on a scenic vista?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. AGRICULTURE AND FOREST RESOURCES. In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest protocols adopted by the California Air Resources Board. Would the project:				
a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in the loss of forest land or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<i>Issues</i>	<i>Potentially Significant Impact</i>	<i>Less Than Significant With Mitigation</i>	<i>Less Than Significant Impact</i>	<i>Impacts Analyzed in Previous EIR</i>
e. Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3. AIR QUALITY. Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:				
a. Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in other emissions (such as those leading to odors adversely affecting a substantial number of people?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4. BIOLOGICAL RESOURCES. Would the project:				
a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<i>Issues</i>	<i>Potentially Significant Impact</i>	<i>Less Than Significant With Mitigation</i>	<i>Less Than Significant Impact</i>	<i>Impacts Analyzed in Previous EIR</i>
5. CULTURAL RESOURCES. Would the project:				
a. Cause a substantial adverse change in the significance of a historical resource as defined in Section 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Disturb any human remains, including those interred outside of dedicated cemeteries?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
6. ENERGY. Would the project:				
a. Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
7. GEOLOGY AND SOILS. Would the project:				
a. Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury, or death involving:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii) Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii) Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv) Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial direct or indirect risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<i>Issues</i>	<i>Potentially Significant Impact</i>	<i>Less Than Significant With Mitigation</i>	<i>Less Than Significant Impact</i>	<i>Impacts Analyzed in Previous EIR</i>
8. GREENHOUSE GAS EMISSIONS. Would the project:				
a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emission of greenhouse gases?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
9. HAZARDS AND HAZARDOUS MATERIALS. Would the project:				
a. Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard or excessive noise for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g. Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
10. HYDROLOGY AND WATER QUALITY. Would the project:				
a. Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or groundwater quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<i>Issues</i>	<i>Potentially Significant Impact</i>	<i>Less Than Significant With Mitigation</i>	<i>Less Than Significant Impact</i>	<i>Impacts Analyzed in Previous EIR</i>
c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i) result in substantial erosion or siltation on- or off-site;	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii) substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or offsite;	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii) create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff; or	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv) impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
11. LAND USE AND PLANNING. Would the project:				
a. Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
12. MINERAL RESOURCES. Would the project:				
a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
13. NOISE. Would the project result in:				
a. Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<i>Issues</i>	<i>Potentially Significant Impact</i>	<i>Less Than Significant With Mitigation</i>	<i>Less Than Significant Impact</i>	<i>Impacts Analyzed in Previous EIR</i>
14. POPULATION AND HOUSING. Would the project:				
a. Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of road or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
15. PUBLIC SERVICES. Would the project:				
a. Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:				
i) Fire protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii) Police protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii) Schools?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv) Parks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
v) Other public facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
16. RECREATION. Would the project:				
a. Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
17. TRANSPORTATION/TRAFFIC. Would the project:				
a. Conflict with a program, plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict or be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<i>Issues</i>	<i>Potentially Significant Impact</i>	<i>Less Than Significant With Mitigation</i>	<i>Less Than Significant Impact</i>	<i>Impacts Analyzed in Previous EIR</i>
18. TRIBAL CULTURAL RESOURCES. Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:				
a. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code section 5024.1? In applying the criteria set forth in subdivision (c) of Public Resources Code section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
19. UTILITIES AND SERVICE SYSTEMS. Would the project:				
a. Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Generate solid waste in excess of State or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
20. WILDFIRE. If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:				
a. Substantially impair an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<i>Issues</i>	<i>Potentially Significant Impact</i>	<i>Less Than Significant With Mitigation</i>	<i>Less Than Significant Impact</i>	<i>Impacts Analyzed in Previous EIR</i>
b. Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
21. MANDATORY FINDINGS OF SIGNIFICANCE.				
a. Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current project, and the effects of probable future projects.)	1. <input type="checkbox"/>	2. <input type="checkbox"/>	3. <input type="checkbox"/>	4. <input checked="" type="checkbox"/>
c. Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?	5. <input type="checkbox"/>	6. <input type="checkbox"/>	7. <input type="checkbox"/>	8. <input checked="" type="checkbox"/>

EXPLANATION OF ISSUES

The Subarea 29 Specific Plan Final EIR, certified in October 2006, was prepared as a Program EIR in accordance with CEQA, the CEQA Guidelines, and the City's Rules for the Implementation of CEQA. As required, the EIR considered the direct physical changes and reasonably foreseeable indirect physical changes in the environment that would be caused by implementation of the Subarea 29 Specific Plan. The Final EIR focused on impacts from the proposed land uses associated with buildout of the Specific Plan Land Use Plan, and impacts from the resultant population and employment growth from the Specific Plan.

Once a Program EIR has been prepared, subsequent activities within the program must be evaluated to determine whether an additional CEQA document needs to be prepared. However, if the Program EIR addresses the program's effects as specifically and comprehensively as possible, many subsequent activities could be found to be within the Program EIR scope and additional environmental documents may not be required (CEQA Guidelines Section 15168[c]). When a Program EIR is relied on for a subsequent activity, the lead agency must incorporate feasible mitigation measures and alternatives developed in the Program EIR into the subsequent activities (Guidelines Section 15168[c][3]). If a later activity would have effects that were not examined in the Program EIR, a new Initial Study would need to be prepared leading to either an EIR or a Negative Declaration.

Here, an Initial Study has been prepared to determine if the proposed Project is within the scope of the Subarea 29 Specific Plan Final EIR such that additional environmental review is not required. As discussed below, the City has concluded that no additional environmental review is required, such that this Initial Study can serve as an Addendum to the Subarea 29 Specific Plan Final EIR pursuant to Section 15164 of the CEQA Guidelines. Substantial changes are not proposed to the Subarea 29 Specific Plan Project and will not require revisions to the Subarea 29 Specific Plan Final EIR. The Specific Plan is divided into 31 Planning Areas and the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum evaluated the impacts associated with the development capacity of 2,392 units. As previously addressed, the currently proposed Project would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 to 73 (an increase of 26 units), would introduce a new housing typology, and includes proposed TTM 20389. Applicable mitigation measures from the Subarea 29 Specific Plan Final EIR are incorporated by reference in each impact area discussion and are listed at the conclusion of this Addendum under the "Earlier Analysis" section.

On January 26, 2010, the City of Ontario adopted TOP Policy Plan (General Plan). With the proposed Specific Plan Amendment, the total number of dwelling units allowed to be developed in the Subarea 29 Specific Plan area would increase from 2,392 dwelling units to 2,418 dwelling units, which is less than the 2,700 residential unit development capacity established by the Policy Plan (General Plan) for the Subarea 29 Specific Plan area. Additionally, the overall density of the Specific Plan area of 4.9 dwelling units per acre is consistent with the Policy Plan (General Plan) that allows up to 5 dwelling units per acre within the Low Density Residential (2.1 – 5 du/ac) land use designation. Finally, the City's water, recycled water, and sewer infrastructure would have sufficient capacity to serve the additional development allowed by the proposed Project. Therefore, no proposed changes or revisions to the Subarea 29 Final EIR are required. The proposed Project would not result in any additional impacts beyond those previously analyzed in the Subarea 29 Specific Plan Final EIR or the TOP Final EIR.

1. **AESTHETICS.** Would the project:

a. **Have a substantial adverse effect on a scenic vista?**

Discussion of Effects: The Subarea 29 Specific Plan Final EIR concluded that no substantial effect on a scenic vista would result from implementation of the proposed development. The Policy Plan (General Plan) does not identify scenic vistas within the City. However, the Policy Plan (Policy CD1-5) of TOP requires all major north-south streets be designed and constructed to feature views of the San Gabriel Mountains, which are part of the City's visual identity and a key to geographic orientation. North-south streets should be clear of visual clutter, including billboards and be enhanced appropriately by framing corridors with trees.

The Project site is located south of Merrill Avenue and is not located along any major north-south streets. Therefore, no adverse impacts related to scenic vistas would result from the proposed Project.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

b. Substantially damage scenic resources, including, but not limited to, tress, rock outcroppings and historic buildings within a state scenic highway?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR concluded that no impacts to state scenic highways would result from implementation of the proposed development. The City of Ontario is served by three freeways: I-10, I-15, and SR-60. I-10 and SR-60 traverse the northern and central portion of the City, respectively, in an east–west direction. I-15 traverses the northeastern portion of the City in a north–south direction. These segments of I-10, I-15, and SR-60 have not been officially designated as scenic highways by the California Department of Transportation. There are no historic buildings or any scenic resources identified on or in the vicinity of the Project site. Therefore, no scenic resources within a state scenic highway would be impacted by the proposed Project.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

c. In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR concluded that the visual character of the Specific Plan area would be changed dramatically but would not be degraded with the introduction of a well-planned and landscaped new residential community, resulting in a less than significant impact. The Project site has been graded and remains vacant. The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology (Motorcourt Cluster D – 8-plex) at the Project site. The proposed residential units would be consistent with the design standards of the Subarea 29 Specific Plan and the policies of the Community Design Element of the Policy Plan (General Plan) and zoning designations on the property, as well as with the existing and future development in the surrounding area. Therefore, the proposed Project would not degrade the visual quality of the area through development of the site with single-family homes. Therefore, no adverse impacts related to the degradation of the existing visual character or quality would result from the proposed Project.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

d. Create a new source of substantial light or glare that would adversely affect day or nighttime views in the area?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR concluded that potential impacts associated with light and glare would be less than significant levels with adhere to the City's standard practices and procedures, including requirements to ensure that light does not spill onto

adjacent properties. The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology (Motorcourt Cluster D – 8-plex) at the Project site. New lighting beyond that anticipated in the previous environmental analysis would not be introduced to the site with the development of the proposed Project. Pursuant to the requirements of the City's Development Code, on-site lighting would be shielded, diffused or indirect, to avoid glare to pedestrians or motorists. In addition, lighting fixtures would be selected and located to confine the area of illumination to within the Project site and minimize light spillage.

Site lighting plans would be subject to review by the Planning Department and Police Department prior to issuance of building permits (pursuant to the City's Building Security Ordinance). Therefore, no adverse impacts are anticipated.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

2. AGRICULTURE AND FOREST RESOURCES. In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest protocols adopted by the California Air Resources Board. Would the project:

a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR concluded that implementation of development allowed by the Specific Plan would result in the conversion of Prime Farmland to nonagricultural uses and considered the impact significant and unavoidable. According to the 2016 California Department of Conservation's (CDC) California Important Farmland Finder (the latest information available), the western portion of the Project site (including the SCE corridor) is classified as Prime Farmland and the eastern portion of the Project site is classified Other Land (CDC, 2016). The Project site has been graded and is currently vacant; therefore, the conversion of Prime Farmland to non-agricultural uses anticipated in the Subarea 29 Specific Plan Final EIR has already occurred. The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. There would be no change to the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. The Subarea 29 Specific Plan Final EIR identified approximately 50% of the 540-acre Specific Plan Area designated Prime Farmland. The impact would remain as a significant and unavoidable impact.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

b. Conflict with existing zoning for agricultural use, or a Williamson Act contract?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR concluded that impacts related to

conflict with existing Williamson Act contracts would be significant and unavoidable. However, based on review of Figure III-1-1, Williamson Active Contracts Locations Map, there are no areas within the Specific Plan area that remain under a Williamson Act contract. Further, the Project site is zoned "Subarea 29 Specific Plan". The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. There would be no change to the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR and no changes to the impact conclusions presented in the Final EIR.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

- c. Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?**

Discussion of Effects: The Subarea 29 Specific Plan Final EIR did not address impacts associated with lands zone for forestland or timberland. The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. The Project site is zoned "Subarea 29 Specific Plan". There is currently no land in the City of Ontario that qualifies as forest land as defined in Public Resources Code section 12220(g). Neither TOP nor the City's Zoning Code provide designations for forest land, timberland, or timberland production. There would be no change to the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. The proposed Project would not result in any conflict with zoning for forest land, timberland, or timberland production.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

- d. Result in the loss of forest land or conversion of forest land to non-forest use?**

Discussion of Effects: The Subarea 29 Specific Plan Final EIR did not address impacts to forestland or timberland. The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. The Project site has been graded and is currently vacant; there is no forest land at or near the Project site. There would be no change to the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. The proposed Project would not result in the loss of forest land or the conversion of forest land to non-forest uses. No impacts would result.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

- e. Involve other changes in the existing environment, which, due to their location or nature, could result in the conversion of Farmland to non-agricultural use or conversion of forest land to non-forest use?**

Discussion of Effects: As previously discussed, the Subarea 29 Specific Plan Final EIR concluded that implementation of development in the Specific Plan area would convert Farmland, and specifically Prime Farmland, to non-agricultural use. The conversion of farmland to urban uses was determined to be a potentially significant impact that is unavoidable. The Project site has been previously graded and although the western portion of the site continues to be classified as Prime Farmland, the conversion of Farmland to non-agricultural use has already occurred. Similarly, the areas surrounding the Project site have been developed or are under construction. There is no forest land at or near the Project site so no conversion of forest land to non-forest use would occur.

There would be no change to the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR and the conversion of Farmland to non-agricultural use would remain as a significant and unavoidable impact. There is no additional mitigation available that could potentially reduce this impact. The impact will remain as a significant unavoidable impact.

It should also be noted that in order to minimize conflicts between urban and agricultural land uses, MM Ag 2 from the Subarea 29 Specific Plan Final EIR requires that all residential units in the Subarea 29 Specific Plan be provided with a deed disclosure, or similar notice, approved by the City Attorney, regarding the proximity and nature, including odors, of neighboring agricultural uses. Mitigation Measure (MM) Ag 2 remains applicable to proposed development in Planning Area 27 and is presented under the discussion of "Earlier Analysis" at the end of this document.

Mitigation Required: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. Applicable mitigation measures included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

3. **AIR QUALITY.** Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:

a. **Conflict with or obstruct implementation of the applicable air quality plan?**

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the implementation of the Subarea 29 Specific Plan would not impair implementation of the Air Quality Management Plan (AQMP) and would result in less-than-significant impacts. Since certification of the Subarea 29 Specific Plan Final EIR, the South Coast Air Quality Management District (SCAQMD) has updated the AQMP. The current AQMP for CEQA analysis purposes is the 2016 AQMP, which is a regional and multi-agency effort (SCAQMD, California Air Resources Board [CARB], Southern California Association of Governments [SCAG], and United States Environmental Protection Agency [USEPA]). The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. City and county general plans were used to develop the growth and pollutant emissions forecasts in the 2016 AQMP. With the proposed Specific Plan Amendment, the total number of units allowed in the Subarea 29 Specific Plan area would increase from 2,392 dwelling units to 2,418 units, which is less than the 2,700 residential unit development capacity established by TOP Policy Plan (General Plan) for the Subarea 29 Specific Plan area. Therefore, consistent with the conclusions of the Subarea 29 Specific Plan Final EIR, the proposed Project would not conflict with the AQMP's growth assumptions or the AQMP, resulting in a less than significant impact.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum, and TOP Final EIR. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

b. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that during construction and operation, implementation of development allowed by the Subarea 29 Specific Plan would result in significant and unavoidable project-level and cumulative impacts associated with emissions of air pollutants for which the region (South Coast Air Basin [SCAB]) is in non-attainment. The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site.

With respect to construction emissions, the types of construction activities and construction equipment that would be used for construction in Planning Area 27 would be the same as that evaluated in the Subarea 29 Specific Plan Final EIR. However, as shown on the aerial photograph provided on Figure 3, the majority of approved land uses in the Subarea 29 Specific Plan area have been constructed or are currently under construction. Additionally, the mass grading for the 5.99-acre Project site, which uses larger equipment (and generates higher air quality emissions) has been completed; finish grading activities would be conducted as part of the proposed Project. Therefore, the construction activities associated with the proposed Project would be less than what was anticipated for a peak construction day as analyzed in the Subarea 29 Specific Plan Final EIR. It should also be noted that federal and State requirements for cleaner diesel engines would further reduce construction emissions compared to estimates in the Subarea 29 Specific Plan Final EIR. Additionally, MM Air 1 through MM Air 3, which identify requirements to reduce construction emissions, would be implemented during construction of the proposed Project. These mitigation measures are presented under the discussion of “Earlier Analysis” at the end of this document. Therefore, daily construction emissions resulting from the proposed Project would not exceed those presented in the Subarea 29 Specific Plan Final EIR and likely would be less.

Mobile emissions are the primary factor associated with operational emissions. Based on the *Park Place Specific Plan (Planning Area 27) Trip Generation Assessment* (Trip Generation Assessment) prepared by Urban Crossroads (November 2020), the increase in 26 units within Planning Area 27 associated with proposed Project would generate 168 additional daily trips compared to that anticipated for Planning Area 27 in the Subarea 29 Specific Plan Final EIR (Urban Crossroads, 2020). However, the total number of units and associated vehicular trip generation would be less than anticipated in TOP and evaluated in TOP Final EIR for the Subarea 29 Specific Plan area (2,700 dwelling units are anticipated in the TOP, compared to 2,418 dwelling units with the proposed Specific Plan Amendment). Additionally, based on the SCAQMD 1993 CEQA Handbook, it is estimated that a potentially significant air quality impact would occur with the development of approximately 166 units; therefore, the development of an additional 26 dwelling units alone would not represent a significant air quality impact and would not represent a substantial increase in emissions beyond those already approved in the Subarea 29 Specific Plan Final EIR and 2015 Addendum.

Therefore, although the proposed Project would increase the number of units and associated operational air pollutant emissions associated with development in the Subarea 29 Specific Plan area, and specifically Planning Area 27, including emissions of criteria pollutants for which the Project region is non-attainment under an applicable federal or state ambient air quality standard, the proposed Project would not result in any new or substantially more severe impacts beyond that previously analyzed in the Subarea 29 Specific Plan Final EIR and TOP Final EIR. Additionally, consistent with MM Air 4 from the Subarea 29 Specific Plan Final EIR, a bus stop is planned at Haven Avenue, just north of Merrill Avenue. This bus stop will be approximately 0.25 mile from the Project site (within walking distance), and would facilitate use of transit to reduce vehicular trips and associated air pollutant emissions. MM Air 4 is presented under the discussion of “Earlier Analysis” at the end of this document.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum, and TOP Final EIR. No

changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measures included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

c. Expose sensitive receptors to substantial pollutant concentrations?

Discussion of Effects: Sensitive receptors are defined as populations that are more susceptible to the effects of pollution than the population at large. The SCAQMD identifies the following as sensitive receptors: long-term health care facilities, rehabilitation centers, convalescent centers, retirement homes, residences, schools, playgrounds, child care centers, and athletic facilities. The Subarea 29 Specific Plan Final EIR analysis concluded that the implementation of the Specific Plan would result in increased local traffic volumes, but would not expose sensitive receptors to substantial localized carbon monoxide (CO) concentrations. Additionally, the Final EIR concluded that emissions during project construction would exceed the SCAQMD localized significance thresholds (LSTs) for nitrogen oxides (NOx) and particulates (PM10), resulting in a potentially significant and unavoidable impact to sensitive receptors.

The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. There would not be a change or increase in the type and amount of construction activities, type of uses proposed (residential), or proximity to sensitive receptors. Therefore, the proposed minimal increase of 26 residential dwelling units within Planning Area 27 would not result in a substantial increase in the number of sensitive receptors being exposed to pollutant concentrations, or the amount of pollutant concentrations resulting from implementation of development in the Subarea 29 Specific Plan area.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

d. Result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that potential odor impacts resulting from implementation of the Specific Plan would be less than significant. The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. The proposed additional residential units, as well as those permitted within the Low-Density Residential zoning district, do not create objectionable odors. Therefore, no adverse impacts are anticipated.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

4. BIOLOGICAL RESOURCES. Would the project:

a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded there may be a probability of burrowing owl (*Athene cunicularia hypogea*) colonization within the Subarea 29 Specific Plan area considering the presence of foraging habitat and previous records of presence. To ensure that no direct loss of individuals occurs, mitigation is required prior to initiation of on-site

grading activities for each development phase. With mitigation impacts to burrowing owl were determined to be less than significant. Specifically, MM Bio 1 requires a pre-construction survey for resident burrowing owls be conducted by a qualified biologist 30 days prior to construction activities. If ground-disturbing activities are delayed or suspended for more than 30 days after the preconstruction survey, the site should be resurveyed for owls. The Project site was previously graded and the required pre-construction surveys were conducted. However, the site remains vacant and pre-construction surveys would be conducted again prior to construction for the proposed Project. MM Bio 1 is presented under the discussion of "Earlier Analysis" at the end of this document. The Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR and impacts to burrowing owl would remain less than significant.

The Subarea 29 Specific Plan EIR analysis also identified that potential habitat for the federally listed Delhi sands Flower Loving Fly (DSFLF) (*Rhaphiomidas terminatus abdominalis*). Planning Areas 28 A & B (including Bellegrave Avenue in Planning Area 28), 30 A & B, 31, and 32 were included in the general biological assessment for the area and were determined to contain the soil series Delhi fines and may contain suitable habitat for the DSFLF. Based on the results of focused surveys, the Final EIR concluded that Planning Area 27 (the current Project site) does not support the DSFLF. Further, the Project site has been graded and is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. Therefore, the proposed Project would not impact the DSFLF.

In August 2014, ECORP Consulting updated the Biological Reconnaissance for the Subarea 29 Specific Plan Area and concluded that due to the high level of disturbance, the land use activity, and lack of suitable habitat that all other sensitive species identified during the database search are either presumed to be absent or have a very low potential to occur. The Project site has been graded and is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. Additionally, the Project Applicant has paid the required City of Ontario open space mitigation fee (MM Bio 2) to address potential cumulative impacts associated with loss of habitat. The proposed Project would have a less than significant impact on candidate, sensitive, or special status species.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measure included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the man-made ponds within the Specific Plan area did not support riparian habitat, that the habitat value was low due to the lack of species and structural diversity, and that habitat for sensitive biological resources is not present. The Project site has been graded and is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. The Project site does not support riparian habitat or other sensitive natural communities and no impact to such resources would occur with implementation of the proposed Project.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

- c. Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?**

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the implementation of the Specific Plan (through construction and operation) would not have direct and indirect effects upon the hydrology and aquatic habitat quality of state or federally protected wetlands and “Others Waters” of the United States as defined Section 404 of the *Clean Water Act*. The Project site has been graded and is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. There are no protected wetlands on-site, as defined by Section 404 of the Clean Water Act and no impact to protected wetlands would occur with implementation of the proposed Project.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

- d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?**

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the implementation of the Specific Plan would not interfere with migratory movement and considered the impact less than significant because habitat fragmentation already occurred due to agricultural practices, development, and road construction. No wildlife corridors were identified within the Specific Plan area, including the Project site. The Project site has been graded and is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. The Project site is surrounding be existing development and is not within or near any native wildlife nursery sites. Additionally, construction activities would be conducted in compliance with the Migratory Bird Treaty Act (MBTA) and CDFW requirements (refer to MM Bio 4, which is presented under the discussion of “Earlier Analysis” at the end of this document). Therefore, the proposed Project would not interfere with the movement of any species, with migratory wildlife corridors, or impede the use of a native wildlife nursery site, and impacts would be less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measure included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

- e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?**

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that there are no specific local policies or ordinances established to protect biological resources that would relate to the Project site and impacts relative to this issue would be less than significant. The City of Ontario does not have any municipal ordinances for the protection of trees on private property; however, Municipal Code Sections 10-1.25 and 10-2.05 prohibit the damaging or destruction of trees on City property, except under conditions specified in the Municipal Code. There are existing trees planted in the public right-of-way along Merrill Avenue north of the Project site; however, these trees would be retained, and the proposed Project would not conflict with any policies or ordinances protecting biological resources.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions

to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

f. Conflict with the provisions of an adopted Habitat Conservation Plan (HCP), Natural Community Conservation Plan (NCCP), or other approved local, regional, or state habitat conservation plan?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the Specific Plan area is not part of an adopted HCP, NCCP or other approved habitat conservation plan and that no significant impacts would result. The Project site has been graded and is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. Therefore, the Project site is not within an adopted HCP, NCCP or another approved habitat conservation plan. However, it is within the Ontario Recover Unit for the DSFLF; refer to the discussion above related to potential impacts to this species. As identified, impacts would be less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

5. CULTURAL RESOURCES. Would the project:

a. Cause a substantial adverse change in the significance of a historical resource as defined in Section 15064.5?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the implementation of the Specific Plan would not result in the destruction of historical resources and considered the impact less than significant. The Project site has been graded, is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR, and is currently vacant. City records do not reflect the presence of a historic resource as defined in CEQA Guideline section 15064.5 at, or in the vicinity of the Project site and no impacts to historical resources would result from implementation of the proposed Project.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

b. Cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded there is a low potential for adverse environmental impacts to unique archaeological resources and potential impacts were determined to be less than significant. However, the Final EIR included mitigation outlining actions to take in the unlikely event unknown resources were discovered during grading (refer to MM Cultural 1). The Project site has been graded, and is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. No archaeological resources were encountered during previous grading activities and no impacts to archaeological resources would occur during remaining construction activities, which would occur in previously disturbed soils. However, in the unlikely event archaeological resources are encountered the requirements outlined in MM Cultural 1, which is presented under the discussion of "Earlier Analysis" at the end of this document, would be followed and impacts would remain less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measure included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

c. Disturb any human remains, including those interred outside of formal cemeteries?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded there is low potential for adverse environmental impacts to human remains, including those interred outside of a formal cemetery, and impacts were determined to be less than significant. However, the Final EIR included mitigation outlining actions to take in the unlikely event human remains were discovered during grading (refer to MM Cultural 2). The Project site has been graded, and is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. No human remains were discovered during previous grading activities and it is not anticipated that human remains would be encountered during remaining construction activities. However, in the unlikely event human remains are encountered the requirements outlined in MM Cultural 2, which is presented under the discussion of “Earlier Analysis” at the end of this document, would be followed and impacts would remain less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measure included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

6. ENERGY. Would the project:

Although Energy was added in December 2018 as a topic in the Environmental Checklist included in Appendix G of the CEQA Guidelines, analysis of a project’s potential to result in a significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources is not a new requirement. This issue is also addressed in Appendix F of the CEQA Guidelines. Energy consumption was addressed in the Subarea 29 Specific Plan Final EIR (Section III.12, Utilities/Service Systems).

a. Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that development in the Specific Plan area would have less than significant impacts related to energy consumption (natural gas and electric). It was identified that energy consumption can be reduced through design considerations that are more sustainable than conventional construction; MM Util 6 requires that the City and developer include sustainable systems for use of energy within the project design.

The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. The proposed Project would not change the type of land uses anticipated for Planning Area 27 (residential) and would not change the anticipated construction and operational characteristics of the proposed development. Therefore, the associated energy demand for these activities would be the same as discussed below.

Construction-related energy demand includes energy and fuel used by construction equipment, construction worker vehicles, and construction vendor / hauling vehicles, coupled with construction energy efficiency / conservation measures. The construction equipment, use of electricity, and fuel for the proposed Project would be typical for the type of construction proposed because there are no aspects of the proposed construction process that are unusual or energy-intensive, and construction equipment would conform to applicable CARB emissions standards, which promote equipment fuel efficiencies. It should also be noted that fuel efficiencies are improving for on- and off-road vehicle engines due to more stringent government requirements. Construction energy consumption would represent a “single-event” demand and would not require ongoing or permanent commitment of energy resources. Thus, construction energy consumption would not be considered inefficient, wasteful, or otherwise unnecessary.

With respect to operations, Title 24 energy standards have become more stringent since 2006. These regulations are regularly updated. The 2019 version of Title 24 was adopted by the CEC and became effective on January 1, 2020. The 2019 Title are applicable to building permit applications submitted on or after January 1, 2020. Further, the California Green Building Standards (CALGreen) Code, which contains mandatory and voluntary requirements for new residential and nonresidential buildings, became effective in August 2009 following certification of the Subarea 29 Specific Plan Final EIR. The proposed residential buildings would be constructed to achieve the building energy standards set forth in the Title 24 requirements in effect at the time of building permit issuance. Therefore, there would be additional reductions in energy consumption pursuant to the new and updated codes compared to those anticipated in the Subarea 29 Specific Plan Final EIR. Further, the types of trips and vehicle mix generated by the proposed Project would be consistent with other residential development of similar scale and configuration, including other development in the Subarea 29 Specific Plan area. The proposed Project does not propose uses or operations that would inherently result in excessive and wasteful vehicle trips and vehicle miles traveled, nor associated excess and wasteful vehicle energy consumption.

Therefore, the proposed Project would not result in wasteful, inefficient, or unnecessary consumption of energy resources during construction or operation and the impact would remain less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

b. Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?

Discussion of Effects: The potential for development within the Subarea 29 Specific Plan area to conflict with or obstruct a state or local plan for renewable energy or energy efficiency was not specifically addressed in the Final EIR or 2015 Addendum. However, federal and state agencies regulated energy use and consumption through various means and programs when the 2006 EIR was prepared and continue to do so. On the state level, the California Public Utilities Commission (CPUC) and the California Energy Commission (CEC) are two agencies with authority over different aspects of energy. In addition to Title 24 energy standards and the CALGreen Code addressed above, relevant state energy-related laws and plans are summarized below.

- **Integrated Energy Policy Report.** Senate Bill (SB) 1389 (Bowen, Chapter 568, Statutes of 2002) requires the CEC to prepare a biennial integrated energy policy report that assesses major energy trends and issues facing California's electricity, natural gas, and transportation fuel sectors and provides policy recommendations to conserve resources; protect the environment; ensure reliable, secure, and diverse energy supplies; enhance the state's economy; and protect public health and safety. The 2019 IEPR was adopted January 31, 2020, and continues to work towards improving electricity, natural gas, and transportation fuel energy use in California. The 2019 IEPR focuses on a variety of topics such as including the environmental performance of the electricity generation system, landscape-scale planning, the response to the gas leak at the Aliso Canyon natural gas storage facility, transportation fuel supply reliability issues, updates on Southern California electricity reliability, methane leakage, climate adaptation activities for the energy sector, climate and sea level rise scenarios, and the California Energy Demand Forecast. The 2019 IEPR is a State Policy report and is not applied to individual development projects such as the proposed Project or other development in the Subarea 29 Specific Plan area. However, the proposed Project would not involve any uses or activities that would conflict with or otherwise hinder or obstruct implementation of the goals presented in the 2019 IEPR.
- **State of California Energy Plan.** The CEC is responsible for preparing the State of California Energy Plan (State Energy Plan), which identifies emerging trends related to energy supply, demand, conservation, public health and safety, and the maintenance of a healthy economy. The State Energy Plan calls for the state to assist in the transformation

of the transportation system to improve air quality, reduce congestion, and increase the efficient use of fuel supplies with the least environmental and energy costs. To further this policy, the State Energy Plan identifies a number of strategies, including assistance to public agencies and fleet operators and encouragement of urban designs that reduce vehicle miles traveled and accommodate pedestrian and bicycle access. The proposed Project takes advantage of existing infrastructure systems, and promotes land use compatibilities through the development of residential uses on a site designated for such uses in the TOP and the Subarea 29 Specific Plan. Further, Merrill Avenue extends along the northern boundary of the Project site and will provide Class II bike lanes, a pedestrian walkway, and a multi-purpose trail to promote non-vehicular travel. The Subarea 29 Specific Plan and the proposed Project would support urban design and planning processes identified under the State Energy Plan, and would not otherwise interfere with or obstruct implementation of the State Energy Plan.

- **State of California Renewables Portfolio Standard (SB 1078, SB 107, and SBX1-2).** Established in 2002 under SB 1078, and accelerated in 2006 under SB 107 and again in 2011 under SBX1-2, California's Renewables Portfolio Standard (RPS) Program requires retail sellers of electric services to increase procurement from eligible renewable energy resources. The RPS applies to all electricity retailers in the State including publicly owned utilities, investor-owned utilities, electricity service providers, and community choice aggregators. All of these entities must adopt the RPS goals of 20% of retail sales from renewables by the end of 2013, 25% by the end of 2016, and 33% by the end of 2020. As with the rest of the Subarea 29 Specific Plan area, the proposed Project would receive electricity from SCE. SCE is required by law to comply with RPS Goals. Therefore, the proposed Project would not interfere with nor obstruct implementation of the RPS.

The proposed Project would also comply with the requirements of the City's Climate Action Plan (CAP), which is addressed under the Greenhouse Gas Emissions section of this document. Compliance with the CAP would be achieved through the implementation of an array of project design features that would conserve or reduce energy use, (i.e., improved insulation, water use reduction, use of recycled water, solar energy, etc.).

Therefore, the proposed Project would not conflict with or obstruct a state or local plan for renewable energy or energy efficiency, and this impact would be less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

7. GEOLOGY & SOILS. Would the project:

- a. Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury, or death involving:**
 - i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.**

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the Specific Plan area, including the Project site, is located outside the Fault Rupture Hazard Zone (formerly Alquist-Priolo Zone) and there would no impacts related to rupture of a known earthquake fault. The Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. Therefore, no impacts associated with rupture of a known earthquake fault would result.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No

changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

ii) Strong seismic ground shaking?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the implementation of the Specific Plan could expose people or structures to seismic hazards; however, with implementation of mitigation this impact would be less than significant. The Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR and the underlying geologic conditions at the Project site remain the same. As identified in the Final EIR, all construction would be conducted in compliance with the Uniform Building Code (UBC), the Ontario Municipal Code, TOP and all other ordinances adopted by the City related to construction and safety. Further, as required, the previous grading activities at the Project site (Planning Area 27) were subject to observation and testing. It was concluded that rough grading was performed in general accordance with the recommendations presented in the geotechnical reports and field, and Planning Area 27 is acceptable from a geotechnical standpoint (Leighton and Associates, 2019). Further, MM Geo 4 from the Subarea 29 Specific Plan Final EIR requires that a project-specific geotechnical investigation be prepared prior to issuance of building permits and the recommendations be incorporated during project design and grading. MM Geo 4, which is presented under the discussion of “Earlier Analysis” at the end of this document, is applicable to the proposed Project. This impact would be less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measure included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

iii) Seismic-related ground failure, including liquefaction?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that due to the consistency of the underlying soils and the deep groundwater levels, the potential for seismic-induced liquefaction was less than significant. The Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR and the potential for seismic-induced liquefaction remains the same (less significant). Further, as identified above, based on observations and testing during grading, Planning Area 27 is acceptable from a geotechnical standpoint (Leighton and Associates, 2019). Therefore, this impact would be less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

iv) Landslides?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the topography of the Specific Plan area is virtually flat, and the potential for landslides is considered not significant. The Project site has been graded and the terrain remains level. No impacts related to seismically induced landslides would result.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

b. Result in substantial soil erosion or the loss of topsoil?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the implementation of the Specific Plan could alter site topography, which could affect the rate or extent of erosion; however, with implementation of mitigation measures this impact would be less than significant. The Project site has been graded and remains vacant with exposed soil. Compliance with applicable regulations, including implementation of erosion control and dust reduction measures required by the Stormwater Pollution Prevention Plan (SWPPP), review of grading plans by the City Engineer, and obtaining required permits (refer to MM Geo 1, which is presented under the discussion of “Earlier Analysis” at the end of this document), ensure that erosion impacts are less than significant, and. Under the developed condition, and with adherence to the requirements of the National Pollutant Discharge Elimination System (NPDES) requirements and the Environmental Resource Element of the Policy Plan (General Plan) strategies, and compliance with the UBC and Ontario Municipal code requirements the potential for erosion would be reduced to a less than significant level.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measures included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the implementation of the Specific Plan would locate structures on soils that are considered potentially unstable and prone to settlement and corrosion; however, with implementation of mitigation measures this impact would be less than significant. The Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR and the underlying geologic conditions at the Project site remain the same. As required, the previous grading activities at the Project site (Planning Area 27) were subject to observation and testing. It was concluded that rough grading was performed in general accordance with the recommendations presented in the geotechnical reports and field, and Planning Area 27 is acceptable from a geotechnical standpoint (Leighton and Associates, 2019). Further, implementation of TOP strategies, adherence to requirements outlined in the Uniform Building Code and Ontario Municipal code, and implementation of MM Geo 2 and MM Geo 4, which require further geotechnical evaluation of on-site soils prior to development, would ensure that impacts remain less than significant. MM Geo 2 and MM Geo 4 are presented under the discussion of “Earlier Analysis” at the end of this document.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measures included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

d. Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the implementation of the Specific Plan would locate structures on soils characterized by their sandy texture and inability to hold moisture. Therefore, the potential for expansive soils was determined to be less than significant. The Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR and the underlying geologic conditions at the Project site remain the same. Further, as required, the previous grading activities at the Project site (Planning Area 27) were subject to observation and testing. It was concluded that rough grading was performed in general accordance with the recommendations presented in the geotechnical reports

and field, and Planning Area 27 is acceptable from a geotechnical standpoint (Leighton and Associates, 2019). This impact would be less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

e. Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the implementation of the Specific Plan would involve the removal of existing septic tanks, and a complete sewer system would be installed that does not require the use of septic tanks. The Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR, and the proposed residential uses would be connected to sewer lines that have been installed to serve the Specific Plan area. There would be no use of septic systems or alternative wastewater treatment systems and no impact would result.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

f. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that earth-disturbing activities associated with the implementation of the Specific Plan could potentially disturb or damage undocumented paleontological resources; however, with implementation of MM Cultural 3, this impact would be less than significant. The Project site has been graded, and is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. No paleontological resources were encountered during previous grading activities, which were conducted in compliance with the requirements outlined in the *Paleontological Resources Monitoring and Treatment Plan* established for the Subarea 28 Specific Plan. No impacts to paleontological resources are expected to occur during remaining construction activities, which would occur in previously disturbed soils. However, in the unlikely event paleontological resources are encountered, applicable requirements outlined in MM Cultural 3 would be followed and impacts would remain less than significant. MM Cultural 3 is presented under the discussion of "Earlier Analysis" at the end of this document.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measure included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

8. GREENHOUSE GAS EMISSIONS.

The State of California enacted Assembly Bill (AB) 32, the California Global Warming Solutions Act of 2006, after certification of the Subarea 29 Specific Plan Final EIR, and, as a result, increased attention has been paid to the impact of GHG emissions. The California Natural Resources Agency (CNRA) in 2010, adopted amendments to the CEQA Guidelines in a new Section 15064.4 entitled "Determining the Significance of Impacts from Greenhouse Gas Emissions", which require evaluation of GHG emissions. Therefore, GHG emissions were not specifically identified as such in the Subarea 29 Specific Plan Final EIR analyses. However, as described in the following paragraphs, courts have ruled

that there is no requirement to address GHG emissions in an Addendum to an EIR that was completed prior to the adopted CEQA amendments. “Information on the effect of greenhouse gas emissions on climate change” does not constitute “new information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the IS / MND was adopted.” (See *Citizens for Responsible Equitable Environmental Development v. City of San Diego* [2011] 196 Cal. App. 4th 515, 531–532 [rejecting claim that such information triggered the need for a supplemental EIR, and explaining that such information was known “long before the City approved the 1994 EIR” at issue]). Further, the impact of buildout of TOP on the environment due to the emission of GHG emissions was analyzed in the TOP Final EIR. As previously discussed, the proposed buildout of the Subarea 29 Specific Plan was previously analyzed in TOP Final EIR, which was certified by the City.

Would the project:

a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?

Discussion of Effects: Limiting GHG emissions to combat climate change has been a governmental goal since the late 1970s. As explained by the United States Supreme Court in *Massachusetts v. EPA* (2007) 549 U.S. 497: “In the late 1970s, the Federal Government began devoting serious attention to the possibility that carbon dioxide emissions associated with human activity could provoke climate change. In 1978, Congress enacted the National Climate Program Act, 92 Stat. 601, which required the President to establish a program to “assist the Nation and the world to understand and respond to natural and man-induced climate processes and their implications”. In 1987, Congress enacted the Global Climate Protection Act for the purpose of “establish[ing] a national climate program that will assist the Nation and the world to understand and respond to natural and man-induced climate processes and their implications” (15 *United States Code* [USC] 2902). The act required the establishment of various programs to further climate change research (15 USC 2904[d]).

In 1988, the United Nations created the Intergovernmental Panel on Climate Change (IPCC) to provide scientific information regarding climate change to policymakers. In 1992, 154 nations, including the United States, entered into the United Nations Framework Convention on Climate Change (UNFCCC), a nonbinding agreement under which industrialized countries pledged to work to reduce GHG emissions. Five years later, in 1997, the parties to the UNFCCC adopted the Kyoto Protocol, which set binding GHG reduction targets for 37 industrialized countries and the European Community, with the objective of reducing their collective emissions by 5% below 1990 levels during the “commitment period” of 2008–2012.

As noted by the court in *Citizens for Responsible Equitable Environmental Development v. City of San Diego* (supra, 196 Cal. App. 4th 515), by 1990, the potential impacts of GHG emissions were already the subject of litigation, with the “Natural Resources Defense Council (NRDC) argu[ing that an] “increase in fossil fuel combustion ... will ... lead to a global increase in temperatures, causing a rise in sea level and a decrease in snow cover that would damage the shoreline, forests, and agriculture of California.” (Id. at 531, quoting *City of Los Angeles v. National Highway Traffic Safety Administration* [D.C. Cir. 1990] 286 U.S. App.D.C. 78.)

Thus, by the 1990s, California’s local governmental agencies were well aware of the importance of monitoring and limiting GHG emissions when approving projects. Since GHG impacts were known at the time that the Subarea 29 Specific Plan Final EIR was conducted, information regarding the proposed Project’s potential to impact climate change does not constitute “new information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time . . . the IS / MND was adopted”. Accordingly, the inclusion of GHG impacts as a requirement of CEQA analysis does not trigger the need for any further environmental review. (See *Citizens for Responsible Equitable Environmental Development v. City of San Diego* [supra, 196 Cal. App. 4th at 531–532]). Notwithstanding this conclusion, the discussion below addresses the analysis of the proposed Project included in TOP Final EIR.

The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. With the

proposed Specific Plan Amendment, the total number of units allowed in the Subarea 29 Specific Plan area would increase from 2,392 dwelling units to 2,418 units, which is less than the 2,700 residential unit development capacity established by TOP Policy Plan (General Plan) for the Subarea 29 Specific Plan area. Therefore, the Project site was previously analyzed by the Certified TOP EIR as a residential use that may have an impact on the environment at buildout of The Ontario Plan due to the emission of GHGs. According to TOP Final EIR, this impact would be significant and unavoidable (Re-circulated Portions of the Ontario Plan Draft Environmental Impact Report, p. 2-118.) TOP Final EIR was certified by the City on January 27, 2010, at which time a statement of overriding considerations was also adopted for The Ontario Plan's significant and unavoidable impacts, including that concerning the emission of GHGs.

Implementation of the proposed Project would not create significantly greater impacts than were identified in the Certified TOP EIR. The proposed Project includes a sample GHG Reduction Measures Screening Threshold Table, which provides guidance in measuring the reduction of GHG emissions attributable to certain design and construction measures incorporated into development projects. The analysis, methodology, and significance determination (thresholds) are based upon the City's Climate Action Plan (CAP), which includes GHG emission inventories (2008 and 2020 forecasts), a year 2020 emission reduction target, the goals and policies to reach the target, together with the Addendum prepared for the CAP. The Screening Table assigns points for each option incorporated into a project as mitigation or a project design feature (collectively referred to as "feature"). The point values correspond to the minimum emissions reduction expected from each feature. The menu of features allows maximum flexibility and options for how development projects can implement the GHG reduction measures. The point levels are based upon improvements compared to 2008 emission levels of efficiency. Projects that garner at least 100 points will be consistent with the reduction quantities anticipated in the City's CAP. As such, those projects that garner a total of 100 points or greater would not require quantification of project specific GHG emissions. Consistent with CEQA Guidelines, such projects would be determined to have a less than significant individual and cumulative impact for GHG emissions. Future development on the Project site will be required to meet or exceed the minimum 100 points; therefore, quantification of Project-specific GHG emissions is not required.

Additionally, pursuant to Public Resources Code Section 21083.3, this impact need not be analyzed further, because (1) the proposed Project would result in an impact that was previously analyzed in the Certified TOP Final EIR; (2) the proposed Project would not result in any greenhouse gas impacts that were not addressed in the Certified EIR; (3) the proposed Project is consistent with The Ontario Plan. The proposed impacts of the Project were already analyzed in the Certified TOP Final EIR and the Project would be built to current energy efficient standards. Potential impacts of project implementation would be less than significant with mitigation already required under the Certified TOP Final EIR and, CAP Screening Tables, and current energy efficiency standards. No changes or additions to the Certified TOP Final EIR analyses are necessary.

Mitigation: No new mitigation measures required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP Final EIR. No changes or additions to Certified TOP Final EIR analyses are necessary. The mitigation measures adopted as part of Certified TOP Final EIR adequately address any potential significant impacts and there is no need for any additional mitigation measures. The City has reviewed the emission reduction measures and concepts in TOP Final EIR, and has determined that the following actions apply and shall be undertaken by the applicant in connection with the proposed Project: energy efficient design, efficient irrigation systems, and compliance with Title 24 of the California Code of Regulations.

b. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?

Discussion of Effects: The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. With the proposed Specific Plan Amendment, the total number of units allowed in the Subarea 29 Specific Plan area would increase from 2,392 dwelling units to 2,418 units, which

is less than the 2,700 residential unit development capacity established by TOP Policy Plan (General Plan) for the Subarea 29 Specific Plan area.

The proposed Project is consistent with TOP Goal ER 4 of improving air quality by, among other things, implementation of Policy ER4-3, regarding the reduction of GHG emissions in accordance with regional, state and federal regulations. As discussed above, the proposed Project is also consistent with the City's CAP. Further, the proposed Project is consistent with the policies outlined in Section 5.6.4 of TOP Final EIR, which aims to reduce the City's contribution of greenhouse gas emissions at build-out by 15%, because the proposed Project is upholding the applicable City's adopted mitigation measures from the TOP Final EIR. Therefore, the proposed Project does not conflict with an applicable plan, policy or regulation adopted for the purpose of reducing GHG emissions. This impact is less than significant.

Mitigation Required: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in TOP Final EIR. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

9. HAZARDS & HAZARDOUS MATERIALS. Would the project:

a. Create a significant hazard to the public or the environment through the routine transport, use or disposal of hazardous materials?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR concluded that implementation of the proposed residential and retail uses would not generate hazardous materials other than those typically associated with household products, and there would be no transport of non-construction related hazardous materials to or from the Specific Plan area. The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. There would be no new uses proposed and the Proposed Project would not involve the transport, use or disposal of hazardous materials during either construction or operation. Therefore, no adverse impacts are anticipated and this impact would be less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR concluded that if known and unknown hazardous materials/situations within the Specific Plan area are not mitigated, current and future residents could be exposed to hazards or hazardous materials (e.g., asbestos and lead from building materials and paints in older structures, pesticides from past agricultural uses, or petroleum products used or leaked on the site), resulting in a potentially significant impact. However, this impact would be less than significant with implementation of mitigation measures. The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. Further, the Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR, the Project site has already been graded, and mitigation measures outlined in the Subarea 29 Specific Plan Final EIR for pre-demolition and grading activities have been completed. There are no new uses proposed and development the proposed residential uses would not expose the public or the environment to a significant hazard through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment. In the unlikely event of an accident, implementation of the strategies included in TOP would decrease the

potential for health and safety risks from hazardous materials, Further, MM Haz 4 from the Subarea 29 Specific Plan Final EIR, which is presented under the discussion of “Earlier Analysis” at the end of this document, identifies actions to take if material that is believed to be hazardous waste is discovered during construction, ensuring the impact remains less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measure included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances or waste within one-quarter mile of an existing or proposed school?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the Specific Plan area was more than one-mile from the Phoenix High School (Corona-Norco Unified School District [CNUSD]) and Colony High School (Chaffey Joint Union High School District [CJUHS]), but within one-quarter mile of the proposed elementary school within the Specific Plan area. Therefore, while the proposed uses would not involve hazardous emissions or handling of acutely hazardous materials, future students could be exposed to possible safety hazards associated with hazardous emissions or hazardous material handling in proximity to a school resulting in a potentially significant impact. However, this impact was considered less than significant with implementation of mitigation measures.

The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. Further, the Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR, the Project site has already been graded, and mitigation measures outlined in the Subarea 29 Specific Plan Final EIR for pre-demolition and grading activities have been completed. The Project site is 0.2-mile from the proposed elementary school; however, there are no new uses proposed and development the proposed residential uses would not involve hazardous emissions or handling of acutely hazardous materials. This impact would be less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that Specific Plan area was not included on a list of hazardous materials compiled pursuant to Government Code Section 65962.5 (Cortese List), and none of the sites identified in the Phase I Environmental Site Assessment conducted during preparation of the Final EIR represent an environmental concern for proposed uses. The Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR, and the Project site has already been graded. Further, based on review of the current California Department of Toxic Substances Control (DTSC) current Cortese List, the Project site is not listed on the hazardous materials site compiled pursuant to Government Code Section 65962.5 (DTSC, 2021). Therefore, no impact would occur.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

- e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard or excessive noise for people residing or working in the project area?**

Discussion of Effects: The Subarea 29 Specific Plan Final EIR concluded that the Specific Plan area is within the Airport Influence Area (AIA) for the Chino Airport resulting in a potentially significant impact. However, because planned land uses would be consistent with those allowed in the applicable airport safety zones, building heights would not exceed applicable height requirements related to airport safety (refer to MM Haz 8), and buyers would be notified of proximity to the airport (refer to MM Haz 9), proposed development in the Specific Plan area would not result in a safety hazard or excessive noise related to proximity to the Chino Airport. MM Haz 8 and MM Haz 9 are presented under the discussion of “Earlier Analysis” at the end of this document.

The Subarea 29 Specific Plan area is located within the Chino ALUCP Compatibility Zone D (refer to Exhibit C – Airport Influence Areas) that requires: (1) 10% of the area be set aside as Open Land for the purpose of serving as emergency landing areas; (2) residential density be either higher than 5.0 dwelling units per acre or have an average parcel size of less than 0.2 acres (8,712 SF); (3) limits the maximum building height to 70 feet; and (4) recording of an Overflight Notification on the Property Deed and Title and provision of a Real Estate Transaction Disclosure. The impacts of the Chino Airport were analyzed in the TOP Final EIR, which was certified by the City on January 27, 2010, at which time mitigation measures were adopted for TOP, including those concerning the impacts related to the Chino Airport. Consistent with all development in the City, development at the Project site would be required to be consistent with the Chino ALUCP. Impacts related to the Chino Airport would be less than significant.

The Subarea 29 Specific Plan area is also located within the Airport Influence Area of the Ontario International Airport (ONT) (refer to Exhibit C) and is subject to the ONT Airport Land Use Compatibility Plan (ALUCP), which was adopted by the Ontario City Council on in April 2011 (Ontario, 2011), after certification of the TOP Final EIR. Policy Map 2-2: Safety Zones of the ONT ALUCP identifies the geographic locations of Safety Zones; the proposed Project is located outside the established Safety Zones and would not result in safety hazards for people residing at the Project. The proposed Project was also evaluated for hazards to aircraft in flight utilizing by Policy Map 2-4: Airspace Obstruction Zones of the ONT ALUCP, which identifies height restrictions of proposed structures or buildings. While portions of the Subarea 29 Specific Plan area are subject to height restrictions (greater than 200 feet); the Project site (Planning Area 27) is not. However, as further discussed in the Noise section of this Addendum, in compliance with ONT ALUCP’s Overflight Policy O2, a Real Estate Transaction Disclosure is required for all development at the Project site.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum, and TOP Final EIR. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measures included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

- f. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?**

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded proposed that the Specific Plan, and all tracts within it, would be designed to meet Fire Department emergency access requirements and would not interfere in any way with emergency evacuation or response plans. The City's Safety Element, as contained within TOP, includes policies and procedures to be administered in the event of a disaster. TOP seeks interdepartmental and inter-jurisdictional coordination and collaboration to be prepared for, respond to, and recover from every day and disaster emergencies. As required, the proposed Project would comply with the requirements of the Ontario Fire Department and all City requirements for fire and other emergency access, resulting in a less than significant impact.

Mitigation: None required. The proposed Project would not result in any new, substantially more

severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

g. Expose people or structures to a significant risk of loss, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that wildland fires do not pose a threat to the Specific Plan area. The Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan EIR, and not located in or near wildlands. Therefore, no impact would occur.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

10. HYDROLOGY & WATER QUALITY. Would the project:

a. Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or groundwater quality?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the implementation of the Specific Plan could violate quality standards and/or waste discharge requirements; however, with implementation of mitigation measures and adherence to statewide NPDES General Industrial Activities Stormwater Permit, the San Bernardino County Area-Wide Urban Runoff Permit (MS4 permit) and the City of Ontario's Municipal Code (Section 6, Title 6), potential impacts to surface water quality would be less than significant. Cumulative impact to surface water quality were determined to be significant and unavoidable due to impaired receiving waters. The Final EIR analysis also concluded that groundwater quality would be improved within the Chino II Groundwater Sub-basin because the agricultural uses that cause high levels of nitrates in the drinking water supply would be eliminated with implementation of the Subarea 29 Specific Plan.

The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. Further, the Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. The proposed Project would not involve any construction activities or new uses, and would be required to comply with applicable water quality regulations (refer to Final EIR MM, Hydro 1, MM Hydro 2, and MM Hydro 6, which are presented under the discussion of "Earlier Analysis" at the end of this document). Further, all Priority Land Use (PLU) areas within the Specific Plan Area shall comply with the statewide Trash Provisions adopted by the State Water Resources Control Board (SWRCB) and trash requirements in the most current San Bernardino County Area-Wide MS4 Permit. Drainage from the PLU shall be designed with conveyance tributary to a certified full trash capture device approved by the SWRCB. Therefore, impacts would be less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measures included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

b. Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that implementation of the Specific Plan would further the ground water management objectives for the Chino Basin by limiting recharge into the southern portion of the basin, since the development of the Chino Basin management program anticipated the cumulative impacts of urbanization of the Chino Basin and consequent conversion of agricultural land use (e.g., diminished agricultural ground water extraction and projected need to increase ground water pumping by desalters), no significant individual or cumulative negative impacts to aquifer volume or the ground water table were expected to occur. Nevertheless, MM Hydro 5 is included in the Subarea 29 Specific Plan Final EIR and requires application of measures to conserve water and enhance ground water recharge. The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. Further, the Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR, would not involve any construction activities or new uses beyond that anticipated in the Subarea 29 Specific Plan Final EIR, and would be required to implement Final EIR MM Hydro 5, which is presented under the discussion of “Earlier Analysis” at the end of this document. Therefore, impacts would be less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measures included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:

- i) result in substantial erosion or siltation on- or off-site;**
- ii) substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or offsite;**
- iii) create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff; or**
- iv) impede or redirect flood flows?**

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis identified that no streams or streambeds are present within the Specific Plan area. The Final EIR concluded that implementation of the Specific Plan could alter the drainage patterns of the site and in a manner that could create substantial flooding, erosion, or siltation on or off-site. However, with implementation of the on-site storm drain system, and required Subarea 29 Specific Plan Final EIR mitigation measures, which include requirement requirements to adhere to applicant regulations, as discussed above, impacts would be less than significant.

The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. Further, the Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR, would not involve any construction activities or new uses beyond that anticipated in the Subarea 29 Specific Plan Final EIR. Therefore, with implementation of an on-site storm drain system, which would connect to existing storm drain facilities constructed in accordance with the master drainage plan, as required by the Specific Plan, the amount and rate of runoff from the Project site would be the same as that anticipated in the Subarea 29 Specific Plan under post-

development conditions and impacts related to potential flooding and storm drain capacity would be less than significant.

As required by MM Hydro 1 and MM Hydro 2 from the Subarea 29 Specific Plan Final EIR, the proposed Project would be constructed in compliance with applicable regulations to minimize water quality impacts during construction and potential development, including from erosion, and impacts related to erosion and polluted runoff would be less than significant. MM Hydro 1 and MM Hydro 2 are presented under the discussion of “Earlier Analysis” at the end of this document

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measures included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

d. In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR concluded that no structures within the Specific Plan area would be placed within a 100-year flood, and the Specific Plan area is not in proximity to a large body of water or the ocean, so the threat of an earthquake-induced seiche or tsunami would not occur. Figure 5.9-2, Flood Hazard Areas, of TOP Final EIR indicates that the Specific Plan area is within the San Antonio Creek dam failure inundation area; however, TOP Final EIR concludes that because the likelihood of catastrophic failure of the San Antonio Dam is very low and the City is prepared in the event of such failure, impacts are considered less than significant. The Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR and TOP Final EIR and would also have no impacts or less than related to the risk of pollutant release from inundation of the Project site from seiche, tsunami or dam failure. Based on review of the Federal Emergency Management Agency (FEMA) National Flood Hazard Layer (NFHL) Viewer, the Project site is not located within a 100-year flood hazard area; the Project site is located within FEMA Zone X, a minimal flood hazard area (FEMA, 2021). Therefore, the Project would also have a less than significant impact related to the risk of pollutant release from flood inundation.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum, and TOP Final EIR. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

e. Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?

Discussion of Effects: As identified in the Subarea 29 Specific Plan Final EIR, the Specific Plan area is subject the requirements of the Water Quality Management Plan of the Santa Ana River Basin (Basin Plan), and overlies the Chino Groundwater Basin. As previously discussed, the Subarea 29 Specific Plan Final EIR analysis concluded that with adherence to applicable water quality regulations, as required by mitigation measures in the Subarea 29 Specific Plan Final EIR, water quality and groundwater impacts would be less than significant, and the Project would not conflict with the Basin Plan.

The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. Further, the Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. The proposed Project would not involve any construction activities or new uses, and would be required to comply with applicable water quality regulations (refer to Final EIR MM Hydro 1, MM Hydro 2, and MM Hydro 6 presented under the discussion of “Earlier Analysis” at the end of this document). Therefore, the Project would comply with the Basin Plan.

On September 16, 2014, subsequent to certification of the Subarea 29 Specific Plan Final EIR and TOP Final EIR, Governor Jerry Brown signed into law the Sustainable Groundwater Management Act (SGMA). The 2014 SGMA requires local public agencies and Groundwater Sustainability Agencies (GSAs) in “high-” and “medium”-priority basins to develop and implement Groundwater Sustainability Plans (GSPs) or Alternatives to GSPs. GSPs are detailed road maps for how groundwater basins will reach long-term sustainability. The California Department of Water Resources (DWR) currently categorizes the Chino Groundwater Basins as “very low” priority. Therefore, the Chino Groundwater Basins is not subject to the requirements of the SGMA (DWR, 2021). Accordingly, the proposed Project would no conflict with or obstruct implementation of a sustainable groundwater management plan.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum, and TOP Final EIR. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measures included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

11. LAND USE & PLANNING. Would the project:

a. Physically divide an established community?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the Specific Plan area was not located within a “community” and all major circulation routes would be maintained through the Specific Plan area; therefore, the proposed development would not physically divide and established community, and no impact would result. The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. The Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR, and is surrounded by existing residential development implemented in compliance with the Subarea 29 Specific Plan, TOP, and Eastvale General Plan. The proposed Project would involve development of residential uses consistent with the Subarea 29 Specific Plan and TOP, and would not divide an established community. No impact would result.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

b. Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR concluded that proposed development would comply within the Specific Plan area with the land use designations and the land use policies in the General Plan Amendment for the NMC, and would comply with the requirements established in the Subarea 29 Specific Plan, resulting in a less than significant impact related to land use policies.

The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. However, the proposed Project would not require a change in the Specific Plan land use designation (Cluster Homes – 7-14 du/ac) or TOP land use designation (Low Density Residential [2.1 – 5 du/ac]). Further, the proposed Project would be designed to be consistent with the development regulations of the Subarea 29 Specific Plan. The proposed increase in residential units at the Project site, which was previously graded and is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR and TOP Final EIR, would not conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect, and

this impact would be less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum, and TOP Final EIR. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

12. MINERAL RESOURCES. Would the project:

a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR concluded that the Specific Plan area does not contain any known mineral resource and is not located within an area that has been classified or designated as a mineral resource area by the State Board of Mining and Geology, there are no known mines on or near the Specific Plan area, and no impacts to known mineral resources would result. The Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR and does not contain any mineral resources. Therefore, no impacts would result.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

b. Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR concluded that the Specific Plan area is not located within an area of locally important mineral resource recovery delineated in any plans. The Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR and is not located within an area that has been classified or designated as a mineral resource recovery site in the TOP, Subarea 29 Specific Plan or other land use plan. Therefore, no impacts would result.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

13. NOISE. Would the project result in:

a. Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that implementation of the Specific Plan would result in project-level noise impacts during construction and operation; however, these impacts would be reduced to less than significant levels with implementation of mitigation measures. The Subarea 29 Specific Plan Final EIR analysis concluded that cumulative traffic-related noise impacts were determined to be significant and unavoidable. The TOP Final EIR concluded that build-out of the uses anticipated by TOP, which includes development in the Subarea 29 Specific Plan area, would result in significant and unavoidable traffic-related noise impacts and noise and vibration impacts to sensitive receptors during construction.

With respect to construction emissions, the types of construction activities and construction equipment that would be used for construction in Planning Area 27 would be the same as that evaluated in the Subarea 29 Specific Plan Final EIR. Additionally, the Project site is entirely within

the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. The mass grading for the Project site, which uses larger equipment and generates higher noise levels than other construction activities) has been completed; finish grading activities and building construction would be conducted as part of the proposed Project. Construction activities would be subject to the mitigation measures outlined in the Subarea 29 Specific Plan Final EIR, which require that construction activities comply with the City of Ontario noise ordinance that restricts the days and hours of construction activities (MM Noi 1), and that construction staging area not be located within 150 feet of existing sensitive receptors and construction equipment be fitted with properly operating and maintained mufflers (MM Noi 2). Construction-related noise impacts would be less than significant. MM Noi 1 and MM Noi 2 are presented under the discussion of "Earlier Analysis" at the end of this document.

With respect to operations, the Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. However, the overall number of dwelling units in the Specific Plan area with the proposed Specific Plan Amendment (2,418 dwelling units), would not exceed the number of units anticipated in TOP for the Specific Plan area (2,700 units). The noise sources associated with the proposed residential uses would be the same as that anticipated in the Subarea 29 Specific Plan Final EIR and TOP Final EIR at the Project site and with adherence to the City's Noise Ordinance would be less than significant. As required by MM Noi 7, architectural plans would be submitted to the City for an acoustical plan check prior to the issuance of building permits to assure that interior noise level requirements are met. MM Noi 7 is presented under the discussion of "Earlier Analysis" at the end of this document.

The increase in 26 dwelling units at the Project site would result in a net increase of approximately 168 trips (increase from 450 daily trips to 618 daily trips) compared to the number of trips anticipated in the traffic analysis and associated noise analysis in the Subarea 29 Specific Plan Final EIR, and would be within the trip generation anticipated in the traffic-related noise analysis in the TOP Final EIR. The circulation system for the Specific Plan area would not change with the proposed Project and the trip distribution would be the same; the additional daily vehicular trips generated by the proposed Project would be distributed along various roadways. A doubling of traffic volumes is required to increase average traffic noise levels by 3 dBA, a change which is barely discernable to human hearing. The increase of 168 daily trips would not double the traffic generated by residential uses at the Project site and would not double the daily trips on any roadway segment. Therefore, the proposed increase in units associated with proposed Project would not represent a significant noise impact or substantial increase in traffic-related noise impacts compared to that evaluated in the Subarea 29 Specific Plan Final EIR and TOP Final EIR. Further, soundwalls required by MM Noi 3 through MM Noi 6 from the Subarea 29 Specific Plan Final EIR to address traffic-related noise impacts have been or will be implemented by the Project Applicant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum, or TOP Final EIR. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measures included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

b. Generation of excessive groundborne vibration or groundborne noise levels?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that proposed uses in the Specific Plan area would not generate excessive groundborne vibrations or groundborne noise levels during normal operations. The Final EIR also concluded that during construction, groundborne vibrations may be generated infrequently by use of heavy construction equipment. However, this type of vibration would be temporary and infrequent. Therefore, this impact is considered less than significant and no mitigation measures are required. The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. The operations

associated with residential uses at the Project site would be the same as evaluated in the Subarea 29 Specific Plan Final EIR and would not generate excessive groundborne vibration or noise. Further, the Project site was previously graded and is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. The construction activities associated with the proposed Project would be the same as those evaluated in the Subarea 29 Specific Plan Final EIR and would also be considered less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum, or TOP Final EIR. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

- c. For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?**

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the Specific Plan area is located outside the 65 dBA CNEL contour line of ONT and the Chino Airport airports and people residing and working in the Specific Plan area would not be exposed to excessive noise levels from airport operations.

The Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. Based on review of Policy Map 2-3: Noise Impact Zones, of the ONT ALUCP, the Project site (Planning Area 27) is not located within a noise impact zone for the ONT; therefore, the proposed Project would not expose people residing at the Project site to excessive noise levels from airport operations and this impact would be less than significant. However, based on review of Policy Map 2-5: Overflight Notification Zones, and as presented in the Land Use Element (Exhibit LU-01 Official Land Use Plan) of TOP Policy Plan (General Plan), the entire City of Ontario, including the Project site, is within the area subject to Real Estate Transaction Disclosure policies.

The Project site is also located within the Chino ALUCP Compatibility Zone D that requires residential developments to record an Overflight Notification on the Property Deed and Title and provide a Real Estate Transaction Disclosure. The Project site underlies the Chino Airport traffic pattern, but is outside of the area that would be subject to average exterior noise levels of 55 CNEL under the ultimate airport development conditions. Therefore, no special noise attenuation measures are required for future residential development. Since the Project site is located outside of the noise impact zone, the proposed Project would not expose people residing at the Project site to excessive noise levels from airport operations and impacts would be less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum, or TOP Final EIR. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

However, in compliance with ONT ALUCP's Overflight Policy O2, a Real Estate Transaction Disclosure is required for all development at the Project site. State Law (Business and Professions Code Section 11010) provides the following disclosure language:

NOTICE OF AIRPORT IN VICINITY: This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example, noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.

The Project site is located within the Chino ALUCP Compatibility Zone D that requires residential developments to record an Overflight Notification on the Property Deed and Title and provide a

Real Estate Transaction Disclosure.

14. POPULATION & HOUSING. Would the project:

- a. Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of road or other infrastructure)?**

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that growth associated with implementation of the Subarea 29 Specific Plan would be consistent with regional growth forecasts and regional jobs/housing balance projections, and impacts would be less than significant.

The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. However, the overall number of dwelling units in the Specific Plan area with the proposed Specific Plan Amendment (2,418 dwelling units), would not exceed the number of units anticipated in TOP for the Specific Plan area (2,700 units). The California Department of Finance (DOF) estimates that in January 2020, the City of Ontario had an estimated population of 182,871 residents, with approximately 3.69 persons per household (DOF, 2020a). Therefore, the increase in 26 units resulting from the proposed Project would represent an increase in population of approximately 100 residents, a negligible increase (0.05%) in the existing City population. Additionally, this represents growth that was planned and anticipated in the TOP, and would not represent substantial unplanned growth in the area. Further, the Project would not include any new roadways or infrastructure that would indirectly increase population growth in the area. This impact would be less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum, or TOP Final EIR. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

- b. Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?**

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the implementation of the Specific Plan would result in replacement of existing dairies, agriculture fields, fields and nursery with residential uses. As a result, less than significant impacts related to the displacement of housing and population would occur. The Project site was previously graded and is currently vacant. The proposed Project would not result in the displacement of people or housing and no impact would occur.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

15. PUBLIC SERVICES. Would the project:

- a. Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:**

- i) Fire protection?**

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the implementation of the Specific Plan would add residential uses to the area and would increase

demands upon fire protection; however, with implementation of mitigation measures and adherence to applicable regulations, impacts would be reduced to a less than significant level. The Ontario Fire Department currently provides fire and Emergency Medical Services to the Specific Plan area, including the Project site from Fire Station No. 6 located northeast of the Project site, at 2931 E. Philadelphia Street. A fire station is under construction approximately one-mile northwest of the Project site on the west side of Archibald Avenue within the Parkside Specific Plan area; it is anticipated that construction of this first station will be complete at the end of this year (2021), prior to occupation of residential uses at the Project site. The proposed Project would increase the number of residential units to be constructed at the Project site and would have an associated increase in the population within the Specific Plan area. Therefore, the proposed Project would increase the demand for fire protection services. However, the payment of Development Impact Fees from Subarea 29 will help fund construction of this station (refer to MM Serv 8 presented under the discussion of “Earlier Analysis” at the end of this document). When completed, response time from Station No. 9 will be within the current Fire Department Emergency Response Guideline and the proposed Project would not result in the need for new or physically altered fire protection facilities that would cause a physical environmental impact. Further, the proposed Project would implement mitigation measures to reduce fire hazards (MM Serv 1 through MM Serve 6), which include adherence to regulations for access, building materials, fire flow, etc. Impacts related to fire protection services would be less than significant. MM Serv 1 through MM Serve 6 are presented under the discussion of “Earlier Analysis” at the end of this document.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measures included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

ii) Police protection?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the implementation of the Specific Plan would add residential uses to the area and would increase demands upon police protection; however, the impacts would be less than significant. Police services will be provided by the Ontario Police Department. Since police services are based upon per capita service levels, the proposed Project, which would increase the residential population at the Project site, would require an incremental increase in police services to maintain required service levels. The City’s development review process and building permit plan check processes include review by the City’s Police Department to ensure incorporation of defensible space concepts in site design and construction. Property taxes and City fees support the general fund to help offset the cost of additional personnel. MM Serv 8 presented under the discussion of “Earlier Analysis” at the end of this document requires the payment of applicable fees for police services. Since response time for police service is not based on proximity to the station and since the new main station is close to the project site, no adverse physical impacts associated with the need for, or provision of, new or physically altered police facilities would result from the project. Therefore, impacts to police protection are considered less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measure included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

iii) Schools?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that with adherence to regulations requiring payment of school fees, impacts to school services would be less than significant. The proposed Project would increase the number of dwelling units at the Project site; however, as with all development in the City, the payment of required school fees would reduce this impact to a less than significant level (refer to MM Serv 9 presented under the discussion of “Earlier Analysis” at the end of this document).

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measures included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

iv) Parks?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that with the provision of parkland or payment of in-lieu park fees, impacts to related to park services would be less than significant. The Quimby Act requires local jurisdictions with parks responsibilities to provide parks and recreation opportunities through the receipt of fees or the acceptance of facilities/land. Each tract within the Specific Plan area could either provide adequate local park facilities or pay fees to the City in lieu thereof or some combination of both approaches for a total of 24 acres within the Specific Plan (refer to MM Serv 10 presented under the discussion of “Earlier Analysis” at the end of this document). With adherence to park requirements for the additional residential units associated with the proposed Project, impacts related to park services would be less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measure included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

v) Other public facilities?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that impacts to other existing public facilities (i.e., libraries) would be less than significant with payment of the City’s library development impact fee. The increased demand for library services associated with the increase in population resulting from the proposed Project would be addressed through payment of the required development impact fee (refer to MM Serv 8 presented under the discussion of “Earlier Analysis” at the end of this document). Because libraries need enough people within a geographic area to warrant their construction, the fees are considered adequate mitigation and the construction of new library facilities is not required. Impacts to library services would be less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measure included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

16. RECREATION. Would the project:

- a. Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?**

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that if parks within the Specific Plan are built out based on the population-based service criteria, as required, potential impacts related to increase use of existing parks would be less than significant. The Subarea 29 Specific Plan includes a network of paseos, parks and bicycle trails. Notably, at buildout the Specific Plan includes two neighborhood parks, a recreational area, mini-parks, and a 10-acre elementary school site. The nearest regional park to the Project site is the Santa Ana River Wildlife Area and the Prado Regional Park to the south. Due to the proximity of the Project site to these large recreational areas, they may get some use by the Project residents, but these regional facilities are designed to serve this region. Regional parks would be built out over time to serve the region. Existing local park facilities in the area could experience accelerated deterioration due to the additional residents resulting from the proposed Project. However, as concluded in the Subarea 29 Specific Plan Final EIR, with the timely implementation of parks based on the population-based service criteria, such potential impacts would be reduced to a less than significant level.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

- b. Include recreational facilities or require the construction or expansion of recreational facilities that have an adverse physical effect on the environment?**

Discussion of Effects: The Subarea 29 Specific Plan Final EIR included evaluation of the physical environmental impacts resulting from implementation of recreational facilities and mitigation measured were identified to reduce these impacts as feasible. The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. There are no new park or recreational facilities proposed as part of the Project. Therefore, no physical environmental impacts associated with construction of park and recreational facilities would occur beyond that already addressed in the Subarea 29 Specific Plan Final EIR.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

17. TRANSPORTATION/TRAFFIC. Would the project:

- a. Conflict with a program, plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?**

Discussion of Effects: As discussed under Threshold b, below, pursuant to Senate Bill (SB) 743, the requirement for analyzing congestion impacts for CEQA purposes was eliminated in December 2018. Therefore, this analysis focuses on the consistency of the Project with programs, plans, ordinances, or policies addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities. Notwithstanding, it should be noted that an assessment of the trip generation from the proposed Project was conducted by Urban Crossroads. Based on the Trip Generation Assessment, the increase in 26 units within Planning Area 27 associated with proposed Project would generate 168 additional daily trips, with 12 additional AM peak hour trips and 15 additional PM peak hour trips. Although the proposed Project results in a net increase in trips, the net increase is anticipated to be less than 50 peak hour trips. Therefore, the increase in proposed Project trips is nominal and is not anticipated to change the analysis findings and recommendations previously identified in the *Traffic Impact Study Report (Revised) Sub-Area 29 Specific Plan* (August 5, 2005)

included in the Subarea 29 Specific Plan Final EIR (Urban Crossroads, 2020). Further, the total number of units and associated vehicular trip generation would be less than anticipated in TOP and evaluated in TOP Final EIR for the Subarea 29 Specific Plan area (2,700 dwelling units are anticipated in the TOP, compared to 2,418 dwelling units with the proposed Specific Plan Amendment). Further, mitigation measures included in the Subarea 29 Specific Plan Final EIR involving the completion of intersection improvements to address operational deficiencies at intersections have already been implemented. The remaining mitigation is related to the required payment of developer impact fees, which would also be paid for the proposed Project (refer to MM Trans 7 presented under the discussion of “Earlier Analysis” at the end of this document).

The Subarea 29 Specific Plan Final EIR analysis concluded that the implementation of the Specific Plan would not conflict with a plan, ordinance, or policy addressing circulation system, taking into account all modes of transportation including transit, roadway, and non-vehicular modes of transportation. The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. The proposed Project does not involve any changes to the Specific Plan related to the circulation system. Merrill Avenue, which forms the northern boundary of the Project site has been constructed to its full width as required by the City and the Subarea 29 Specific Plan, and required bicycle and pedestrian facilities have been provided. Further, a bus stop is planned along Haven Avenue just north of Merrill Avenue and is within walking distance to the Project site, which would facilitate use of transit. Therefore, the proposed Project would not conflict with a program, plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

b. Conflict with or be inconsistent with CEQA Guidelines Section 15064.3, subdivision (b)?

Discussion of Effects: SB 743, approved in 2013 and codified in Public Resources Code Section 21099, changes the way transportation impacts are determined according to CEQA. The Office of Planning and Research (OPR) recommended the use of VMT as the replacement for automobile delay-based LOS for the purposes of determining a significant transportation impact under CEQA. On December 28, 2018, the State approved updates to the CEQA Guidelines, which entailed changes to the thresholds of significance for the evaluation of impacts to transportation. Updates to the CEQA Guidelines included the addition of CEQA Guidelines Section 15064.3, of which Subdivision b establishes criteria for evaluating a project’s transportation impacts based on project type and using automobile VMT as the metric. Beginning July 1, 2020, the provisions of CEQA Guidelines Section 15064.3 apply statewide. As identified in Section 15064.3(b)(4) of the CEQA Guidelines, a lead agency has the discretion to choose the most appropriate methodology to evaluate a project’s VMT. The City of Ontario adopted its VMT thresholds of significance on June 16, 2020.

The TOP Final EIR traffic analysis did not employ a VMT-based threshold of significance for evaluating traffic impacts, but nonetheless acknowledged that TOP Land Use Plan would exceed SCAG’s VMT projections for the City. The GHG section of the Recirculated TOP Draft EIR discusses CARB and SCAG strategies to reduce community VMT. In response to the sole VMT comment on the TOP Final EIR from SCAG, the City noted that while buildout of the City under TOP would increase VMT, implementation of TOP would nonetheless “improve the SANDBAG region’s job/housing balance,” thus “reduce VMT by shortening commute distances” and that development pursuant to TOP would be required to implement measures to further “reduce vehicle trips and VMT.” (See, e.g., TOP Final EIR, pp. 2-97, 33-12; Recirculated TOP Draft EIR, pp. 2-18, 2-43, 2-84, 2-110.) The TOP Final EIR Air Quality and GHG Analysis likewise necessarily calculated the VMT generated by buildout of the City under TOP in order to determine development-related air pollutant emissions. (See, e.g., Recirculated TOP Draft EIR, pp. 2-1, 2-5,

2-20, 2-21 [explaining traffic data was used to calculate emissions and VMT calculations]; see also Appendix D, [using VMT to calculate project emissions].) Buildout of the City under TOP anticipated development of the Subarea 29 Specific Plan Area with 2,700 units, which exceeds the 2,418 dwelling units currently proposed. Therefore, the VMT generated by Subarea 29 was anticipated in TOP Final EIR.

Information regarding VMT impacts was available at the time the Certified EIR was prepared. The new VMT requirements implemented under the CEQA Guidelines do not relate to a different type of impact, but merely a different way of analyzing transportation impacts. Moreover, as discussed above, the TOP Final EIR both discussed VMT, including the regional goal of reducing per capita VMT and project features intended to reduce VMT. Accordingly, information regarding potential impacts related to VMT not only could have been included in the TOP Final EIR, but to a large extent it already was included.

The adoption of VMT as a new metric for the measurement of transportation impacts under CEQA does not require the preparation of a subsequent or supplemental EIR, because VMT associated with the Subarea 29 Specific Plan does not constitute new significant information requiring additional environmental analysis. An addendum is not required to consider new impact areas added to CEQA after the underlying EIR was certified. See *Concerned Dublin Citizens v. City of Dublin* (2013) 214 Cal.App.4th 1301, 1320 (adoption of new guidelines for GHG evaluation was not significant new information requiring further CEQA review because GHG emissions were known information and could have been addressed in the original EIR); *Fort Mojave Indian Tribe v. Dept. of Health Servs.* (1995) 38 Cal.App.4th 1574, 1605 (new critical habitat regulation was not significant new information because impacts to the species had already been addressed in original EIR.). As with the adoption of new GHG guidelines discussed above, the adoption of VMT as a metric for analyzing transportation impacts (and corresponding GHG impacts) pursuant to SB 743 is not new information, as VMT-related impacts were knowable and known when the Subarea 29 Specific Plan Final EIR was adopted. Since VMT impacts were known at the time that the Subarea 29 Specific Plan Final EIR was conducted, information regarding the proposed Project's potential VMT-related impacts does not constitute "new information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time . . . the IS / MND was adopted". Accordingly, the inclusion of VMT impacts as a requirement of CEQA analysis does not trigger the need for any further environmental review. (See *Citizens for Responsible Equitable Environmental Development v. City of San Diego* [supra, 196 Cal. App. 4th at 531–532]).

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

c. Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that with the development of residential units, the means of automobile conveyance with relation to design features could be a potential problem. However, with the implementation of traffic mitigation measures, impacts related to design-feature hazards would be less than significant. The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. The proposed Project does not involve any changes to the Subarea 29 Specific Plan related to the circulation system. Access to the Project site would be provided from Merrill Avenue as anticipated in the Specific Plan, and internal roadways, access driveways, and sight distance would comply with the City design requirements, and roadway standards outlined in the Specific Plan (refer to MM Trans 1 through MM Trans 3 presented under the discussion of "Earlier Analysis" at the end of this document). This impact would be less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measures included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

d. Result in inadequate emergency access?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR concluded that implementation of the Specific Plan would improve emergency access by completing improved road segments in the Specific Plan area, and development would adhere to City of Ontario standard conditions of approval, and permits related to emergency access. The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. The proposed Project does not involve any changes to the Subarea 29 Specific Plan related to the circulation system or emergency access. Access to the Project site would be provided from Merrill Avenue as anticipated in the Specific Plan, and would comply with applicable requirements for emergency access. This impact would be less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

18. Tribal Cultural Resources. The provisions of Assembly Bill (AB) 52 are not applicable to the proposed Project. AB 52 applies "...only to a project that has a notice of preparation or a notice of negative declaration or mitigated negative declaration filed on or after July 1, 2015." AB 52, which became effective on July 1, 2015, established a consultation process with California Native American tribes, and established Tribal Cultural Resources as a new class of resources to be considered in the determination of project impacts and mitigation under CEQA. AB 52 requires lead agencies to provide notice to tribes that are traditionally and culturally affiliated with the geographic area of a proposed project, if they have requested such notice in writing. The project notification is required prior to the lead agency's release of a Notice of Preparation (NOP) of an EIR or notice of intent to adopt an MND or ND, and is not required for Addendums. However, the analysis of impacts to cultural resources, including prehistoric archaeological sites, resulting from implementation of the Subarea 29 Specific Plan is provided in the Subarea 29 Specific Plan Final EIR (Section III.4, Cultural Resources), as summarized above in Section 5 of this Addendum. The Subarea 29 Specific Plan Final EIR found that implementation of the Specific Plan would result in less than significant impact to archaeological resources.

Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:

a. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k)?

Discussion of Effects: As discussed in the Cultural Resources section of this Addendum, the Subarea 29 Specific Plan Final EIR analysis concluded that the implementation of the Specific Plan would not result in the destruction of historical resources and considered the impact less than significant. The Project site has been graded, is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR, and is currently vacant. City records do not reflect the presence of a historic resource as defined in CEQA Guideline section 15064.5 at, or in the vicinity of the Project site and no impacts to tribal cultural resources listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k) would result from implementation of the proposed

Project.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

- b. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code section 5024.1? In applying the criteria set forth in subdivision (c) of Public Resources Code section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe?**

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded there is a low potential for adverse environmental impacts to archaeological resources and potential impacts were determined to be less than significant. However, the Final EIR included mitigation outlining actions to take in the unlikely event unknown resources were discovered during grading. The Project site has been graded, and is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. No tribal cultural resources were encountered during previous grading activities and no impacts to tribal cultural resources would occur during remaining construction activities, which would occur in previously disturbed soils.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

19. UTILITIES AND SERVICE SYSTEMS. Would the project:

- a. Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?**

Discussion of Effects: The Subarea 29 Specific Plan identifies the required infrastructure systems necessary to serve the proposed development within the Specific Plan area, and the Subarea 29 Specific Plan Final EIR addressed the utility demands associated with proposed development and potential environmental impacts associated with installation of the required infrastructure. Utility and service system impacts were determined to be less than significant with mitigation. The Final EIR also identified that the Subarea 29 Specific Plan would be one of many projects developed within the NMC, which is only a portion of the Inland Empire Utility Agency's (IEUA) Southern Service Area. The cumulative effects of the IEUA Wastewater Master Plan were evaluated under CEQA in the IEUA Wastewater, Recycled Water and Organics Management Master Plan Program EIR, dated July 3, 2002 (SCH No. 2002011116); this EIR also concluded that impacts would be less than significant with mitigation.

The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. The proposed Project does not involve any changes that would require new or expanded utility infrastructure beyond that already anticipated in the Subarea 29 Specific Plan and evaluated in the associated Final EIR, and on-site utility infrastructure that would serve the Project. Notably, a hydraulic analysis was prepared to address the proposed changes to development at the Project site and to analyze the resulting impact to downstream sewers (including Eastern Trunk Sewer capacity). The *OSS2021-0004 - Subarea 29 Specific Plan Amendment for Park Place Planning Area 27 Sewer Study* (Sewer Study) concluded that there were no capacity deficiencies in the sewer system (AKM Consulting Engineers, 2021). With respect to storm drain facilities, as discussed in the Hydrology and Water Quality section of this Addendum, the increase in residential units would not substantially alter the post-development drainage characteristics of the Project site; therefore, the existing storm

drain system has sufficient capacity to accommodate runoff from the Project site and on-site storm drains would connect to the existing drainage system. Further, MM Util 1 requires that utility infrastructure be constructed to funded to the satisfaction of the City, and MM Util 4 requires that coordination with utilities agencies be conducted to ensure existing utility lines are protected during construction. MM Util 1 and MM Util 4 are presented under the discussion of “Earlier Analysis” at the end of this document. Impacts related to the installation of utility infrastructure necessary to serve the proposed Project would be less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measures included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

b. Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the implementation of the Specific Plan would generate an additional demand for water; however, there will be sufficient water supply exists to meet the City’s existing and planned future uses. The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. Further, the total number of units would be less than anticipated in TOP (2,700 dwelling units are anticipated in the TOP, compared to 2,418 dwelling units with the proposed Specific Plan Amendment). Buildout of TOP is also anticipated in the *2015 Urban Water Management Plan (2015 UWMP)* prepared by the Ontario Municipal Utilities Company in July 2016 and amended through 2018 (Ontario Municipal Utilities Company, 2018). Pursuant to SB 610 (codified in the California Water Code beginning at Section 10910), the proposed Project does not require preparation of a Project-specific Water Supply Assessment (WSA); a WSA is required for residential projects that meet certain criteria relative to size (i.e., 500 dwelling units or more). Further, MM Util 5 and MM Util 6 presented under the discussion of “Earlier Analysis” at the end of this document require design considerations to reduce water consumption. Impacts to water supplies are considered less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures. The applicable mitigation measures included in the Subarea 29 Specific Plan Final EIR will continue to apply to Project activities.

c. Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that the implementation of the Specific Plan would be one of many projects developed within the NMC which is only a portion of IEUA’s Southern Service Area. As previously discussed, the cumulative effects of the IEUA Wastewater Master Plan were evaluated in the IEUA Wastewater, Recycled Water and Organics Management Master Plan Program EIR and found to be less than significant. The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. The total number of units would be less than anticipated in TOP and evaluated in TOP Final EIR (2,700 dwelling units are anticipated in the TOP, compared to 2,418 dwelling units with the proposed Specific Plan Amendment).

The Specific Plan area, including the Project site, is served by the City of Ontario sewer system, which has waste treated by the IEUA at the Regional Water Recycling Plant No. 1 (RP)-1 or RP-5.

RP-1 and RP-5 are not at capacity and the proposed Project would not cause these facilities to exceed capacity. Notably, RP-1 is located in the City of Ontario and has undergone several expansions to increase the design hydraulic domestic sewage (wastewater) treatment capacity to 44 million gallons per day (mgd). The plant serves areas of Chino, Fontana, Montclair, Ontario, Rancho Cucamonga, Upland, and solids removed from RP-4, located in Rancho Cucamonga. RP-1 treats an average influent wastewater flow of approximately 28 mgd (IEUA, 2021). The additional wastewater generated by an increase of 26 dwelling units, which are anticipated in TOP, would not exceed the capacity of RP-1 or RP-5 and this impact would be less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum, and TOP Final EIR. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

d. Generate solid waste in excess of State or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that implementation of the Specific Plan site would not substantially contribute to the exceedance of the permitted landfill capacity at project-level; however, cumulative impacts were determined to be significant and unavoidable. TOP Final EIR determined that impact related to solid waste generation would be less than significant.

The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. The total number of units would be less than anticipated in TOP and evaluated in TOP Final EIR (2,700 dwelling units are anticipated in the TOP, compared to 2,418 dwelling units with the proposed Specific Plan Amendment). Further, considering the proposed Project's future residents' participation in the source reduction and household hazardous waste programs offered by the City, and which are more stringent than when the Subarea 29 Specific Plan EIR was prepared, the solid waste stream generated by the additional residential uses may be reduced over time. Currently, the City of Ontario contracts with a waste disposal company that transports trash to a landfill with sufficient capacity to handle the City's solid waste disposal needs. Less than significant impacts to the existing landfills are expected.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum, and TOP Final EIR. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

e. Comply with federal, state, and local statutes and regulations related to solid waste?

Discussion of Effects: The Subarea 29 Specific Plan Final EIR analysis concluded that construction and operation of proposed uses in the Specific Plan area would comply with regulations associated with solid waste and impacts would be less than significant. As with all development in the City and the Specific Plan area, the proposed additional uses within Planning Area 27 would also be required to comply with applicable regulations related to solid waste management, disposal, recycling, etc., and this impact would be less than significant.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

20. WILDFIRE. If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:

- a. **Substantially impair an adopted emergency response plan or emergency evacuation plan?**
- b. **Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?**
- c. **Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?**
- d. **Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?**

Discussion of Effects: Wildfire was added as a new topic in the environmental checklist form in Appendix G of the State Guidelines as part of the CEQA Guidelines updates that were approved in December 2018. However, as discussed in the Hazards and Hazardous Materials section of this Addendum, wildland fires were discussed in the Hazards/Hazardous Materials section of the Subarea 29 Specific Plan Final EIR (Section III.6) and it was determined the Specific Plan area is not subject to wildfires.

The State Responsibility Area (SRA) is the land where the State of California is financially responsible for the prevention and suppression of wildfires. The SRA does not include lands within city boundaries or in federal ownership; therefore, the Project site is not within an SRA. According to the California Department of Forestry and Fire Protection (CalFire), the City of Ontario, including the Project site is not located within a very high fire hazard severity zone (VHFHSZ) (CalFire, 2008). As such, no impacts related to wildfires would occur.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

21. MANDATORY FINDINGS OF SIGNIFICANCE

Does the project have the potential to substantially degrade the quality of the environment, substantially reduce the habitat or a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?

Discussion of Effects: The Project site was previously graded, and the proposed Project is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR and no natural or sensitive habitats exist at the Project site. The proposed Project does not have the potential to degrade the quality of the environment, reduce the fish and wildlife habitat, threaten plant, fish or wildlife species, or eliminate historical, archeological, or cultural resources. As previously discussed in the Biological Resources section of this document, based on focused surveys conducted for the Specific Plan area it was concluded that Planning Area 27 (the current Project site) does not support the DSFLF. Further, although the site was previously graded and the required pre-construction surveys were conducted, these surveys would be conducted again prior to construction for the proposed Project. Impacts to burrowing owl would remain less than significant. Due to the high level of disturbance, the land use activity, and lack of suitable habitat, other sensitive species are either presumed to be absent or have a very low potential to occur within the Specific Plan area, including the Project site. Further, there are no historic or prehistoric resources located at the Project site and no such resources were discovered during the previous grading activities.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions

to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

- a. **Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current project, and the effects of probable future projects.)**

Discussion of Effects: The potential cumulative impacts identified in the Subarea 29 Specific Plan Final EIR include: loss of Farmland, air quality impacts, surface water quality (due to impaired receiving waters), traffic-related noise, operational traffic impacts, and solid waste generation. Substantial changes are not proposed with the Project and the proposed Project would not require revisions to the Subarea 29 Specific Plan Final EIR. The Specific Plan is divided into 31 Planning Areas that were assigned a maximum development capacity. The Subarea 29 Specific Plan Final EIR and associated 2015 Addendum evaluated the impacts associated with the development capacity of 2,392 single family units. Further, the TOP and TOP Final EIR evaluated up to 2,700 units within the Subarea 29 Specific Plan area. The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. There would be no change in the type of land allowed (residential), and the Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. Additionally, the City's water, recycled water, and sewer infrastructure would have sufficient capacity to serve the proposed Project. Therefore, the Project's contribution to significant cumulative impacts would not be cumulatively considerable.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

- b. **Does the project have environmental effects that will cause substantial adverse effects on human beings, either directly or indirectly?**

Discussion of Effects: Substantial changes are not proposed with the proposed Project and the proposed Project would not require revisions to the Subarea 29 Specific Plan Final EIR. The Subarea 29 Specific Plan Final EIR and associated 2015 Addendum evaluated the impacts associated with the development capacity of 2,392 single family units. Further, the TOP and TOP Final EIR evaluated up to 2,700 units within the Subarea 29 Specific Plan area. The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. There would be no change in the type of land allowed (residential), and the Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. Therefore, the proposed Project would not result in any adverse effects to human beings directly or indirectly that were not considered in the previous environmental documents.

Mitigation: None required. The proposed Project would not result in any new, substantially more severe, or substantially different impacts, other than those previously considered and addressed in the Subarea 29 Specific Plan Final EIR and associated 2015 Addendum. No changes or additions to the previous environmental documents are necessary, nor is there a need for any additional mitigation measures.

EARLIER ANALYSIS

(Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, one or more effects have been adequately analyzed in an earlier EIR or Negative Declaration. Section 15063(c)(3)(D)):

- 1) **Earlier analyzes used.** Identify earlier analyzes used and state where they are available for review.
 - a) The Ontario Plan Final EIR
 - b) The Ontario Plan
 - c) The Subarea 29 Specific Plan Final EIR
 - d) The Subarea 29 Specific Plan EIR Mitigation Monitoring and Reporting Program
 - e) The Subarea 29 Specific Plan

All documents listed above are on file with the City of Ontario Planning Department, 303 East "B" Street, Ontario, California 91764, (909) 395-2036. Other references used to prepare this Addendum are listed below.

- 2) **Impacts adequately addressed.** Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards.

Most of the checklist items were analyzed in the Subarea 29 Specific Plan Final EIR. The Subarea 29 Specific Plan Final EIR and associated 2015 Addendum evaluated the impacts associated with the development capacity of 2,392 single family units. Further, the TOP and TOP Final EIR evaluated up to 2,700 units within the Subarea 29 Specific Plan area. The proposed Project includes a Specific Plan Amendment that would increase the allowed number of dwelling units in Planning Area 27 of the Subarea 29 Specific Plan from 47 units to 73 units (an increase of 26 units), and would introduce a new housing typology at the Project site. There would be no change in the type of land allowed (residential), and the Project site is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR. Additionally, the City's water, recycled water, and sewer infrastructure would have sufficient capacity to serve the proposed Specific Plan Amendment. Therefore, the proposed Project would not introduce any impacts beyond those previously analyzed in the Subarea 29 Specific Plan Final EIR or TOP Final EIR.

MITIGATION MEASURES *(For effects that are "Less than Significant with Mitigation Incorporated," describe the mitigation measures, which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project):*

As the project does not have any adverse environmental impacts beyond those identified in the previous environmental document, no mitigation beyond that previously imposed is required. The Subarea 29 Specific Plan Final EIR mitigation measures applicable to the proposed Project area listed below.

Agricultural Resources

MM Ag 2: In order to minimize conflicts between urban and agricultural land uses, all residential units in the Subarea 29 Specific Plan shall be provided with a deed disclosure, or similar notice, approved by the City Attorney, regarding the proximity and nature, including odors, of neighboring agricultural uses.

Air Quality

MM Air 1: During construction, mobile construction equipment will be properly maintained at an offsite location, which includes proper tuning and timing of engines. Equipment maintenance records and equipment design specification data sheets shall be kept on-site during construction.

MM Air 2: During construction of the proposed improvements, all contractors will be advised not to idle construction equipment on site for more than ten minutes.

MM Air 3: Configure construction parking to minimize traffic interference.

MM Air 4: Local transit agencies shall be contacted to determine bus routing in the project area that can accommodate bus stops at the project access points and the project shall provide bus passenger benches and shelters at these project access points.

Biological Resources

MM Bio 1: There may be a probability of owl colonization within the project site considering the presence of foraging habitat and previous records of presence. To ensure that no direct loss of individuals occurs, mitigation shall be completed prior to initiation of on-site grading activities for each development phase. A preconstruction survey for resident burrowing owls will be conducted by a qualified biologist. The survey will be conducted 30 days prior to construction activities. If ground-disturbing activities are delayed or suspended for more than 30 days after the preconstruction survey, the site should be resurveyed for owls.

If owls are determined to be present within the construction footprint, they will be captured and relocated. If non-breeding owls must be moved away from the disturbance area, passive relocation techniques will be used. The pre-construction survey and any relocation activity will be conducted in accordance with the CDFG Report on Burrowing Owl Mitigation, 1995. According to CDFG guidelines, mitigation actions will be conducted from September 1 to January 31, which is prior to the nesting season. However, burrowing owl nesting activity is variable, and as such the time frame will be adjusted accordingly. Should eggs or fledglings be discovered in any owl burrow, the burrow cannot be disturbed (pursuant to CDFG guidelines) until the young have hatched and fledged (matured to a stage that they can leave the nest on their own). Occupied burrows will not be disturbed during the nesting season (February 1 through August 31) unless a qualified biologist approved by the Department of Fish and Game verifies through non-invasive methods that either: a) the adult birds have not begun egg-laying and incubation; or b) the juveniles from the occupied burrows are foraging independently and are capable of independent survival. If a biologist is unable to verify one of the above conditions, then no disturbance shall occur within 300 feet of the burrowing owls nest during the breeding season to avoid abandonment of the young.

Passive relocation can be used to exclude owls from their burrows (outside the breeding season or once the young are able to leave the nest and fly) by installing one-way doors in burrow entrances. These one-way doors allow the owl to exit the burrow, but not enter it. These doors should be left in place 48 hours to ensure owls have left the burrow. Artificial burrows should be provided nearby. The project area should be monitored daily for one week to confirm owl use of burrows before excavating burrows in the impact area. Burrows should be excavated using hand tools and refilled to prevent reoccupation. Sections of flexible pipe should be inserted into the tunnels during excavation to maintain an escape route for any animals inside the burrow.

MM Bio 4: If project construction activities involving heavy equipment and/or windrow tree removal are to occur during the nesting/breeding season (between February 1st and August 31st) of potentially occurring sensitive bird species, a pre-construction field survey shall be conducted by a qualified biologist to determine if active nests of species protected by MBTA or CDFG are present in the construction zone or within a buffer of 500 feet. Preconstruction nesting/breeding surveys shall be conducted in all CDFG jurisdictional areas and within windrow trees. If no active nests are found during the survey, construction activities may proceed. If active nests are located during the pre-construction surveys, no grading, heavy equipment or tree removal activities shall take place within at least 500 feet of an active listed species or raptor nest, 300 feet of other sensitive bird nests (non-listed), and 100 feet of most common songbird nests.

Cultural Resources

MM Cultural 1: Should any cultural and/or archaeological resources be accidentally discovered during construction, construction activities shall be moved to other parts of the project site and a qualified

archaeologist shall be contacted to determine the significance of these resources. If the find is determined to be an historical or unique archaeological resource, as defined in Section 15064.5 of the CEQA Guidelines, avoidance or other appropriate measures shall be implemented.

MM Cultural 2: If human remains are uncovered at any time, all activities in the area of the find shall be halted by the developer or its contractor and the County Coroner shall be notified immediately pursuant to CA Health & Safety Code Section 7050.5 and CA PRC Section 5097.98. If the Coroner determines that the remains are of Native American origin, the Coroner shall proceed as directed in Section 15064.5(e) of the CEQA Guidelines.

Geology and Soils

MM Geo 1: To reduce impacts associated with erosion due to high winds, prior to construction, all tentative tracts and other construction activities will apply for and adhere to the permit given by the City of Ontario and enforced by the Building Official found in Title 6, Chapter 12, sections 6-12.01– 6-12.07. The permit lasts for one (1) year, therefore all construction lasting for a period of more than one calendar year from the date of issue will reapply for the permit and pay applicable fees.

MM Geo 2: To properly assess and address the suitability of onsite soils to be used as fill, a geotechnical evaluation shall be performed by a qualified professional prior to the approval of the Tentative Tract map or site plan for a given phase of development. This evaluation will include an analysis of the organic matter content of soils on the site. If the organic matter content of the soils is greater than 2 percent when mixed with subsurface soils and/or imported fill, then manure will be removed from the site prior to grading operations.

MM Geo 4: Prior to the issuance of building permits, a project-specific geotechnical investigation for the site must be prepared and submitted to the City for approval. All recommendations contained within the geotechnical investigation must be incorporated during project design and construction. Examples of recommendations include, but are not limited to, specific seismic design parameters and subgrade preparation parameters specifying the amount of overexcavation and recompaction of specific soils in buildings pad and pavement areas.

MM Cultural 3: Since grading plans have not yet been prepared to establish how deep excavation is needed, prior to the issuance of grading permits, and as recommended in the Phase I Cultural and Paleontological Resources Assessment for this site, a qualified paleontologist shall be retained to develop a Paleontological Resources Monitoring and Treatment Plan (PRMTP) for approval by the City. Following City approval of the PRMTP, grading and construction activities may proceed in compliance with the provisions of the approved PRMTP. The PRMTP shall include the following measures:

- a. Identification of those locations within the project site where paleontological resources are likely to be uncovered during grading.
- b. A monitoring program specifying the procedures for the monitoring of grading activities by a qualified paleontologist or qualified designee.
- c. If fossil remains large enough to be seen are uncovered by earth-moving activities, a qualified paleontologist or qualified designee shall temporarily divert earthmoving activities around the fossil site until the remains have been evaluated for significance and, if appropriate, have been recovered; and the paleontologist or qualified designee allows earth-moving activities to proceed through the site. If potentially significant resources are encountered, a letter of notification shall be provided in a timely manner to the City, in addition to the report (described below) that is filed at completion of grading.
- d. If a qualified paleontologist or qualified designee is not present when fossil remains are uncovered by earthmoving activities, these activities shall be stopped and a qualified paleontologist or qualified designee shall be called to the site immediately to evaluate the significance of the fossil remains.
- e. At a qualified paleontologist or qualified designee's discretion and to reduce any construction

delay, a construction worker shall assist in removing fossiliferous rock samples to an adjacent location for temporary stockpiling pending eventual transport to a laboratory facility for processing.

- f. A qualified paleontologist or qualified designee shall collect all significant identifiable fossil remains. All fossil sites shall be plotted on a topographic map of the project site.
- g. If the qualified paleontologist or qualified designee determines that insufficient fossil remains have been found after fifty percent of earthmoving activities have been completed, monitoring can be reduced or discontinued.
- h. Any significant fossil remains recovered in the field as a result of monitoring or by processing rock samples shall be prepared, identified, catalogued, curated, and accessioned into the fossil collections of the San Bernardino County Museum, or another museum repository complying with the Society of Vertebrate Paleontology standard guidelines. Accompanying specimen and site data, notes, maps, and photographs also shall be archived at the repository.
- i. Within 6 months following completion of the above tasks, a qualified paleontologist or qualified designee shall prepare a final report summarizing the results of the mitigation program and presenting an inventory and describing the scientific significance of any fossil remains accessioned into the museum repository. The report shall be submitted to the City Planning Department and the museum repository. The report shall comply with the Society of Vertebrate Paleontology standard guidelines for assessing and mitigating impacts on paleontological resources.

Hazardous Materials

MM Haz 4: If, while performing any excavation as part of project construction, material that is believed to be hazardous waste is discovered, as defined in Section 25117 of the California Health & Safety Code, the developer shall contact the City of Ontario Fire Department and the County of San Bernardino Fire Department Hazardous Materials Division. Excavation shall be stopped until the material has been tested and the presence of hazardous waste has been confirmed. If no hazardous waste is present, excavation may continue. If hazardous waste is determined to be present, the California Department of Toxic Substances Control shall be contacted and the material shall be removed and disposed of pursuant to applicable provisions of California law.

MM Haz 8: To mitigate for any potential impacts related to proximity to the Chino Airport, all development with the Specific Plan will comply with the building height constraints identified in the GPA for the NMC (1998).

MM Haz 9: To disclose to the buyer or lessee of subdivided lands within the Subarea 29 project of the proximity of this site to the Chino Airport as required by AB 2776, the City shall disclose, and ensure that the developer makes disclosures, as required by law, to all future buyers.

Hydrology and Water Quality

MM Hydro 1: In order to ensure that construction activities associated with the Subarea 29 Specific Plan will not cause a violation of any water quality standard or waste discharge requirements and to assure no substantial degradation of water quality occurs, and to implement the intent of mitigation measures included in the Final Environmental Impact Report for the NMC, developments within the project area shall comply with all applicable provisions of the State's General Permit for Construction Activities (Order No. 99-08-DWQ, or most recent version) during all phases of construction. A copy of evidence of the receipt of a Waste Discharge Identification Number from the State Regional Water Quality Control Board shall be filed with the City Engineer along with a copy of the Storm Water Pollution Prevention Plan (SWPPP) maps and BMPs. The City Engineer shall review and approve the provisions of the SWPPP prior to implementation of any SWPPP provision or starting any construction activity.

MM Hydro 2: In order to ensure that development within the Specific Plan will not cause or contribute to violations of any water quality standard or waste discharge requirements, and to assure no substantial degradation of water quality occurs, the project will complete a Water Quality Management

Plan (WQMP) pursuant to the MS4 permit (Order No. 2002-0012) adopted by the City of Ontario. The project shall incorporate Site Design BMPs and Source Control BMPs, and potentially Treatment Control BMPs. The following tables (Table III-7-F and G) provide guidelines and BMPs that shall be incorporated as appropriate into project design (on construction drawings) and/or project specifications and implemented in the field to reduce the expected pollutants from various types of development. Prior to acceptance of the WQMP, the City shall assure that maintenance responsibilities of BMPs approved for the project are identified and enforceable. Table III-7-G correlates each BMP to the pollutants of concern which it removes/reduces and/or meets the design objectives for the BMP.

MM Hydro 5: In order to conserve water and to mitigate for any potential unforeseen adverse impacts to a reduction in ground water recharge, the following measure has been recommended by the Chino Basin Water Conservation District. Landscaping within individual development projects will retain and percolate both applied irrigation water and storm water in vegetated areas of parking lots and other areas, where appropriate; “depressed” planted areas bordered by shrubbery screens will be implemented rather than “mounded” grass and shrubbery planted screens.

MM Hydro 6: In order to reduce pollutants in post construction run-off and to implement mitigation measures included in the GPA for the NMC FEIR, the individual project owners and operators (e.g., homeowner associations, retail center owners, school district, parks department, etc.) shall ensure that all pest control, herbicide, insecticide and other similar substances used as part of maintenance of project features are handled, stored, applied and disposed of by those conducting facility maintenance in a manner consistent with all applicable federal, state and local regulations. According to Title 6, Chapter 6, Section 6 of the City’s code, the City Engineer shall monitor and enforce this provision.

Noise

MM Noi 1: The construction activities of the proposed project shall comply with the City of Ontario noise ordinance that prohibits construction activities on Sundays, federal holidays, and other days between the hours of 7:00 p.m. and 7:00 a.m.

MM Noi 2: Construction staging areas shall not be located within 150 feet of existing sensitive receptors and construction equipment shall be fitted with properly operating and maintained mufflers.

MM Noi 7: Architectural plans shall be submitted to the City of Ontario for an acoustical plan check prior to the issuance of building permits to assure that second story windows are upgraded for sound reduction and proper ventilation systems are incorporated in order to meet the interior noise level requirement.

Public Services

MM Serv 1: Wood-shingled and shake-shingled roofs are prohibited.

MM Serv 2: Fire hydrant locations and water main sizes shall meet standards established by the City Fire Department and reviewed and implemented by the Engineering Department.

MM Serv 3: To reduce fire hazards, adequate fire flow pressure shall be provided for residential and non-residential projects in accordance with currently adopted City standards.

MM Serv 4: To reduce fire hazards, adequate water supply shall be provided as approved by the Fire Department prior to the framing stages of construction.

MM Serv 5: Houses located on cul-de-sacs longer than 300 feet shall be constructed with residential fire sprinklers.

MM Serv 6: Access roadways designed in accordance with Fire Department standards to within 150’ of all structures, shall be provided prior to the framing stages of construction. This access is to be maintained in an unobstructed manner throughout construction.

MM Serv 8: The developer shall pay library, police, and fire service development impact fees.

MM Serv 9: The developer shall pay school fees or otherwise, in lieu of fees, meet project obligations to schools, as required by Mountain View and Chaffey Joint Union High School Districts.

MM Serv 10: Park development impact fees, Quimby fees, and/or developed parkland shall be provided to the City commensurate with the requirements of the General Plan equivalent to 24 acres.

Transportation

MM Trans 1: Construction of full width of internal roadways not specified in the Design Considerations of the project such that they shall comply with City of Ontario standards.

MM Trans 2: Sight distance at the project entrance roadways should be reviewed with respect to standard City of Ontario sight distance standards at the time of preparation of final grading, landscape and street improvement plans.

MM Trans 3: Signing/striping should be implemented in conjunction with detailed construction plans for the project site.

MM Trans 7: The project shall participate in the cost of offsite improvements through the payment of "fair-share" development impact fees. These fees should be collected and utilized as needed by the City of Ontario to maintain acceptable levels of service.

Utilities and Service Systems

MM Util 1: All water and sewer pipelines within and adjacent to the project boundaries shall be constructed and/or funded for construction on a fair share basis based on the NMC Infrastructure Master Plans and to the satisfaction of the City.

MM Util 4: Prior to obtaining grading permit(s), the project proponent shall coordinate with the applicable natural gas, electrical, and telephone utility providers for the project site to ensure that all existing underground and overhead lines are not damaged during project construction.

MM Util 5: To reduce the quantity of energy used and to conserve water resources, the project developer and City of Ontario should work to include sustainable systems for use of water and energy within the project design.

MM Util 6: The project applicant shall plan and construct a dual pipe system to supply reclaimed water when available in the future (GP Policy 5.1.4).

REFERENCES

Cited As	Reference
(AKM Consulting Engineers, 2021)	AKM Consulting Engineers. 2021 (February 16). <i>OSS2021-0004 - Subarea 29 Specific Plan Amendment for Park Place Planning Area 27 Sewer Study</i>
(CalFire, 2008)	California Department of Forestry and Fire Protection (CalFire). 2008 (November 13). <i>Very High Fire Hazard Severity Zones in Local Responsibility Area, SW San Bernardino County</i> . Web. Accessed: March 1, 2021. Available: https://osfm.fire.ca.gov/media/6783/fhszl_map62.pdf
(CDC, 2016)	California Department of Conservation (CDC). 2016. <i>California Important Farmland Finder</i> . Web. Accessed: February 26, 2021. Available: https://maps.conservation.ca.gov/DLRP/CIFF/
(DOF, 2020a)	California Department of Finance (DOF). 2020 (May). <i>Table E-5 Population and Housing Estimates</i> . Web. Accessed March 1, 2021. Available: http://www.dof.ca.gov/Forecasting/Demographics/Estimates/E-5/
(DTSC, 2021)	Department of Toxic Substances Control (DTSC), 2021. <i>EnviroStor Sites and Facilities Database</i> . Web. Accessed: February 28, 2021. Available: http://www.envirostor.dtsc.ca.gov/?surl=imk7w
(DWR, 2021)	Department of Water Resources (DWR), 2021. <i>SGMA Basin Prioritization Dashboard</i> . Web. Accessed: February 28, 2021. Available: https://gis.water.ca.gov/app/bp-dashboard/final/
(FEMA, 2021)	Federal Emergency Management Agency (FEMA). 2021. <i>National Flood Hazard Layer (NFHL) Viewer</i> . Web. Accessed: February 28, 2021. Available: https://hazards-fema.maps.arcgis.com/apps/webappviewer/index.html?id=8b0adb51996444d4879338b5529aa9cd
(IEUA, 2021)	Inland Empire Utilities Agency (IEUA). 2021. <i>Facilities, Regional Water Recycling Plant No. 1</i> . Web. Accessed March 1, 2021. Available: https://www.ieua.org/facilities/regional-water-recycling-plant-no-1/
(Leighton and Associates, 2019)	Leighton and Associates, Inc. 2019 (August 28). <i>Geotechnical Report of Observation and Testing During Rough Grading of Tract 18913-5, PA-27, Part of the Park Place Residential Development, Northeast of Merrill Avenue and Archibald Avenue, City of Ontario, California</i> .
(Ontario, 2011)	City of Ontario (Ontario). 2011. <i>Ontario International Airport Land Use Compatibility Plan</i> . Web. Accessed February 26, 2021. Available: http://www.ontarioplan.org/alucp-for-ontario-international-airport/
(Ontario Municipal Utilities Company, 2018)	Ontario Municipal Utility Company. As updated through 2018 (September 14). <i>2015 Urban Water Management Plan</i> . Web. Accessed: March 1, 2021. Available: http://www.ontarioplan.org/alucp-for-ontario-international-airport/
(Urban Crossroads, 2020)	Urban Crossroads. 2020 (November 17). <i>Park Place Specific Plan (Planning Area 27) Trip Generation Assessment</i> .

Exhibit A – Subarea 29 Specific Plan Land Use Map and Summary

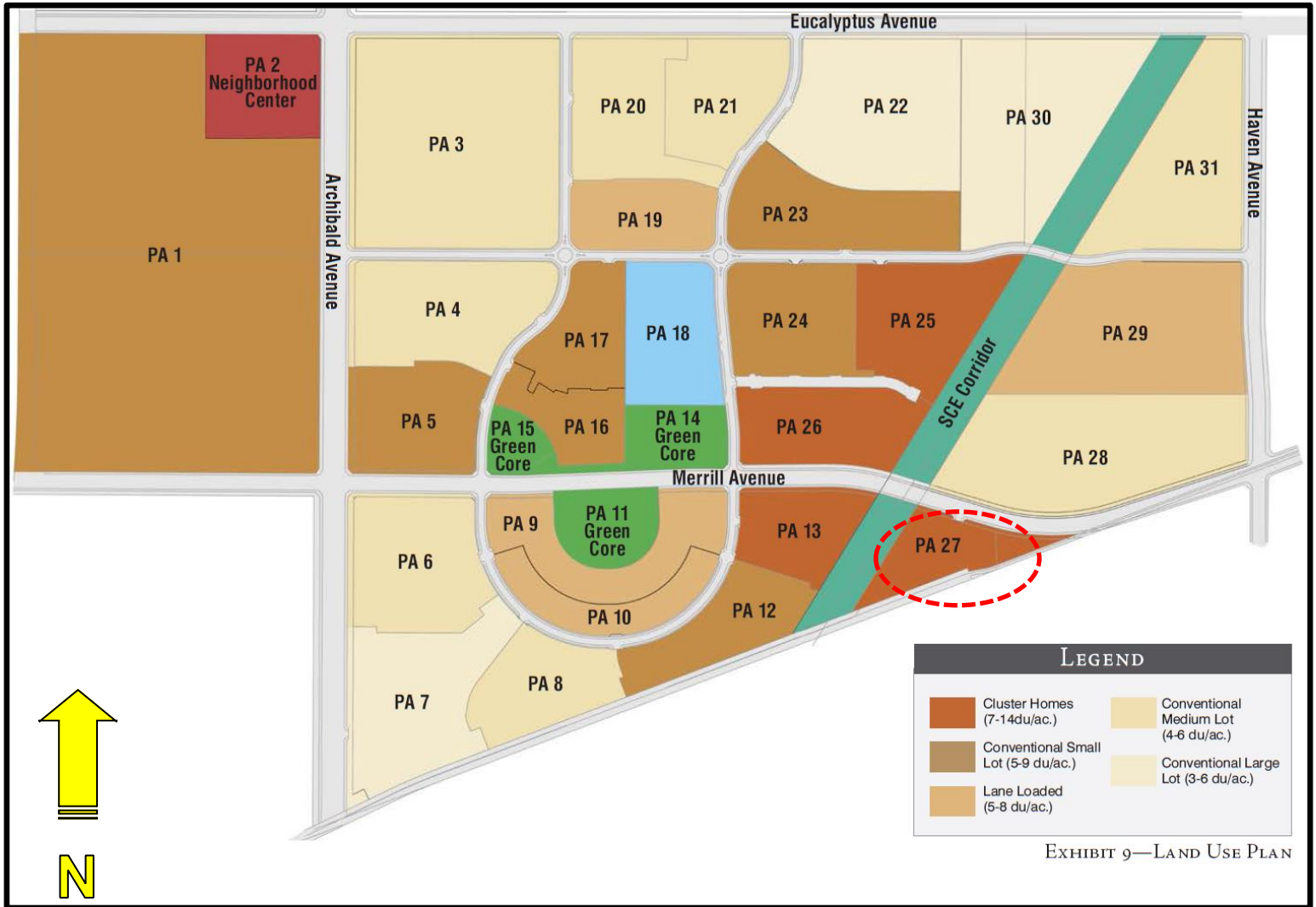


Exhibit A – Cont'd

Table 3—Specific Plan Land Use Summary

Planning Area	Land Use	Minimum Lot Size (S.F)	Planned Dwelling Units ^{3,4,5}	Net Acre ¹	Planned Net Density (Du/Ac.) ^{3,5}	Gross Acres ²	Planned Gross Density (Du/Ac.) ^{3,5}
1*	Conventional Small Lot	3,500	432	83.1	5.2	89.8	4.8
2*	Commercial	N / A	0	12.1	0.0	14.5	0.0
3*	Conventional Medium Lot	4,500	186	34.5	5.4	40.2	4.6
4**	Conventional Medium Lot	4,250	88	10.1	8.7	17.8	4.9
5**	Conventional Small Lot	3,825	68	7.2	9.4	13.7	5.0
6**	Conventional Medium Lot	5,000	67	13.0	5.2	17.0	4.0
7**	Conventional Large Lot	6,300	65	15.3	4.2	18.3	3.6
8**	Conventional Medium Lot	4,250	46	9.1	5.1	9.6	4.8
9**	Lane Loaded	3,150	69	9.9	7.0	11.9	5.8
10**	Lane Loaded	3,600	57	6.6	8.7	7.8	7.3
11**	Neighborhood Park 2	N / A	0	5.7	0.0	6.2	0.0
12**	Conventional Small Lot	3,825	53	9.5	5.6	9.5	5.6
13**	Cluster Homes	2,100 ^A	75	7.8	9.6	7.8	9.6
14**	Neighborhood Park 1	N / A	0	6.3	0.0	7.7	0.0
15**	Recreation Center	N / A	0	2.7	0.0	3.1	0.0
16**	Conventional Small Lot	3,015	41	5.9	7.0	6.1	6.8
17**	Conventional Small Lot	3,015	56	5.3	10.6	8.4	6.7
18**	School	N / A	0	10.0	N / A	11.2	N / A
19**	Lane Loaded	3,150	61	7.8	7.9	9.0	6.8
20**	Conventional Medium Lot	4,250	67	11.8	5.7	13.3	5.0
21**	Conventional Medium Lot	5,000	48	10.1	4.8	11.5	4.2
22**	Conventional Large Lot	6,300	79	19.7	4.0	21.3	3.7
23**	Conventional Small Lot	3,825	82	12.9	6.3	14.4	5.7
24**	Conventional Small Lot	3,400	75	8.1	9.3	12.8	5.8
25**	Cluster Homes	2,100 ^A	102	8.6	11.8	12.9	7.9
26**	Cluster Homes	2,100 ^A	102	8.7	11.7	13.2	7.7
27**	Cluster Homes	1,750 ^A	73	7.6	9.7	7.6	9.6
28*	Conventional Medium Lot	4,050	121	23.0	5.3	25.8	4.7
29***	Lane Loaded or Conventional Medium Lot	3,150 or 4,000	108	21.4	5.0	27.2	4.0
30*	Conventional Large Lot	5,040	110	21.9	5.0	28.3	3.9
31*	Conventional Medium Lot	4,050	87	16.0	5.4	23.1	3.8
Flood Control Channel	Flood Control Channel	N / A	0	7.2	0.0	7.2	0.0
Pump Station	Pump Station	N / A	0	0.2	0.0	0.4	0.0
SCE Corridor	Park Place SCE Easement	N / A	0	11.2	0.0	11.2	0.0
Sub Area 29 Total			2,418	449.9	5.4	539.7	4.5

A) Minimum square footage identified is for exclusive use area on a per home basis, recorded lot size may differ.

- 1) Gross Acres calculated to centerlines of Master Planned streets minus SCE easements.
- 2) Net Acres excludes street rights-of-way and SCE easements.
- 3) Actual total units and gross/net density and acreage will be dependent on final lotting.
- 4) Target unit count based on submitted Tentative "B" Maps
- 5) A density transfer of 15.0% may occur between Planning Areas.

* Indicates Planning Areas as controlled/owned by Richland Communities, Inc.

** Indicates "Park Place" Planning Areas as controlled/owned by SL Ontario Development Company

*** Indicates Planning Areas as controlled/owned by Brookfield Homes

Exhibit A – Cont’d

Table 3a—Subarea 29 Specific Plan Residential Allocations Summary (by Ownership)

<i>Description</i>	<i>Park Place</i>	<i>Richland</i>	<i>Brookfield Homes</i>	<i>Totals</i>
Total Gross Acres	290.7 ac	221.7 ac	27.2ac	539.7 ac
SCE Easement	- (11.2 ac)	- (9.2 ac)	- (3.8 ac)	- (24.3 ac)
Elementary School (*)(**)	- (10.0 ac)	-0-	-0-	- (10.0 ac)
Neighborhood Center(**)	-0-	- (11.2 ac)	-0-	- (11.2 ac)
Residential Gross Acreage	269.5 ac	201.3 ac	23.4 ac	494.2 ac
Percent of Total Residential Acreage	54.5%	40.7%	4.7%	100%
Total Residential Lots	1,374 du	936 du	108 du	2,418 du
Planned Residential Gross Density (du/gross ac.)	5.1 du/gross ac.	4.6 du/gross ac.	4.6 du/grossac.	4.9 du/gross ac.
Residential Gross Density permitted by TOP (5.0 du/gross ac.)	1,347.4 du	1,006.5 du	117.0 du	2,470.9 du

Exhibit B – Proposed Tentative Tract Map 20389

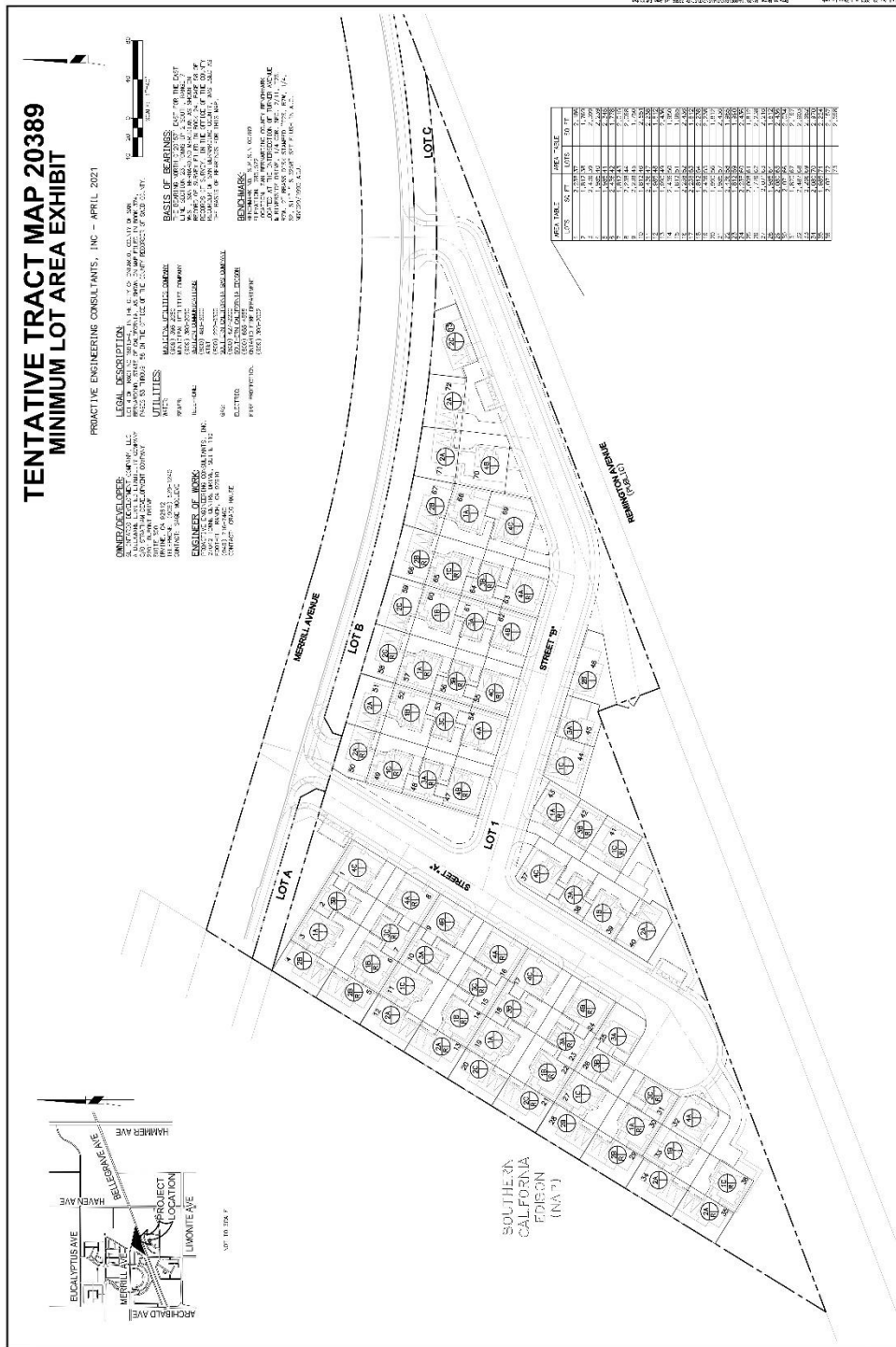
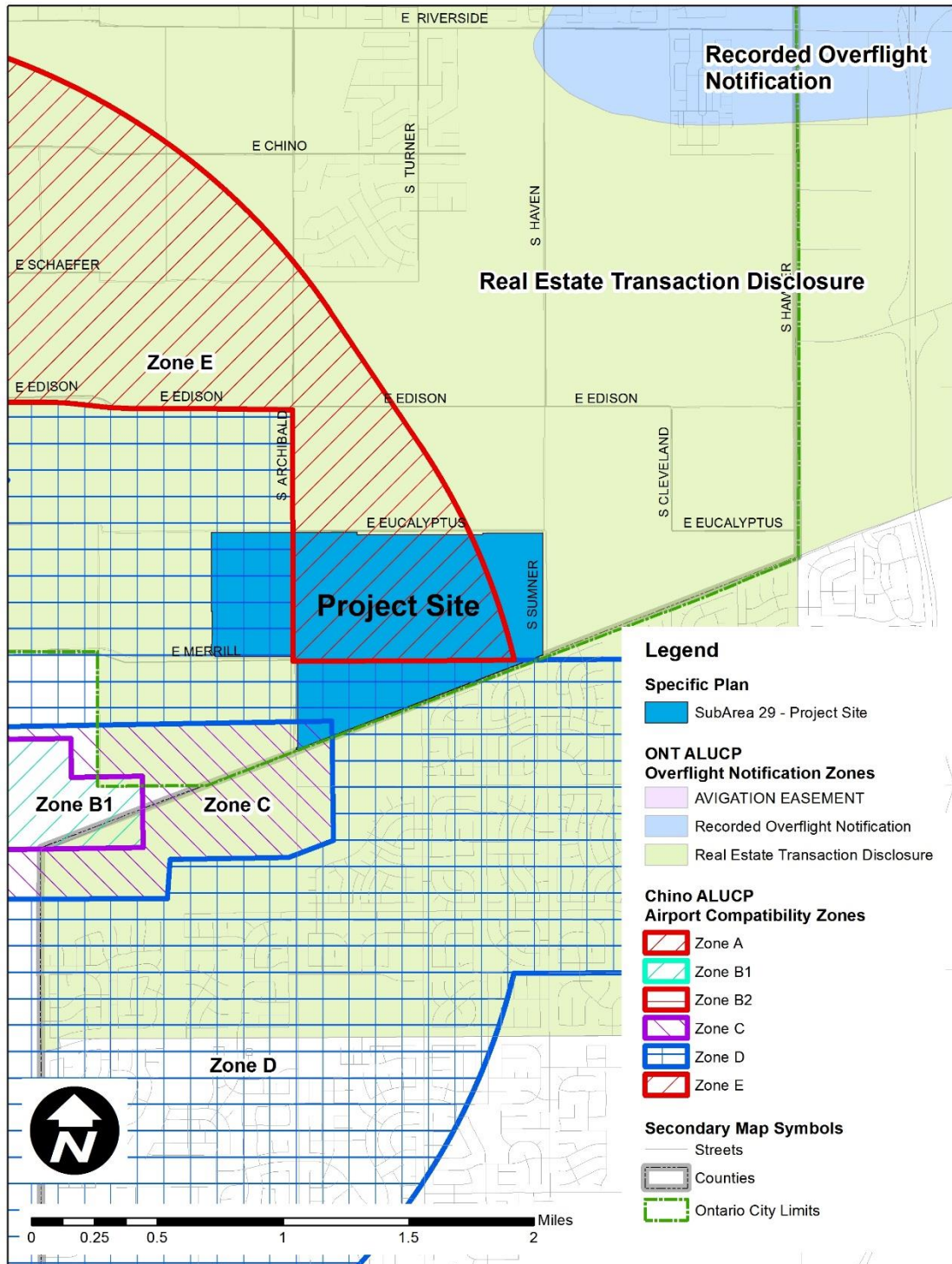


Exhibit C - Airport Influence Areas



RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING THE CITY COUNCIL APPROVE FILE NO. PSPA20-006, AN AMENDMENT TO THE SUBAREA 29 SPECIFIC PLAN TO INCREASE THE OVERALL DENSITY FROM 4.8 TO 4.9 DWELLING UNITS PER GROSS ACRE AND ESTABLISH A NEW RESIDENTIAL PRODUCT TYPE (MOTORCOURT CLUSTER D – 8-PLEX) WITHIN PLANNING AREA 27. THE PROJECT IS BOUNDED BY MERRILL AVENUE TO THE NORTH, SOUTHERN CALIFORNIA EDISON EASEMENT TO THE WEST, AND THE SAN BERNARDINO COUNTY FLOOD CONTROL DISTRICT AND THE CITY OF EASTVALE TO THE SOUTH AND MAKING FINDINGS IN SUPPORT THEREOF—APN: 0218-331-42.

WHEREAS, SL ONTARIO DEVELOPMENT COMPANY, LLC (hereinafter referred to as "Applicant") has filed an Application for the approval of a Specific Plan Amendment, File No. PSPA20-006, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 5.99 acres of land generally bounded by Merrill Avenue to the north, Southern California Edison easement to the west, and the San Bernardino County Flood Control District and the City of Eastvale to the south, and is presently vacant; and

WHEREAS, the property to the north of the Project site is within the Planning Area 28 (Conventional Medium Lot) land use designation of the Subarea 29 Specific Plan and a residential subdivision is currently under construction. The properties to the east are located in the City of Eastvale within R-1 land use district and developed with residential land uses. The properties to the south are located in the City of Eastvale within the PRD (Planned Residential Development) land use district and developed with residential land uses. The property to the west is located within the Subarea 29 Specific Plan and has an SCE Corridor land use designation that is developed with SCE 180-foot-tall transmission towers (115kV) and power lines; and

WHEREAS, the Specific Plan Amendment will increase the density of the Subarea 29 Specific Plan from 4.8 to 4.9 dwelling units per gross acre; and

WHEREAS, the Specific Plan Amendment will increase the planned gross density from 6.2 to 9.7 dwelling units per gross acre to allow an increase from 47 dwelling units to a maximum of 73 dwelling units within Planning Area 27 (Cluster Homes – 7-14 du/ac); and

WHEREAS, the Specific Plan Amendment will establish a new residential product type (Motorcourt Cluster D – 8-Plex) within Planning Area 27 (Cluster Homes – 7-14 du/ac); and

WHEREAS, all changes and additions to the Specific Plan (exhibits, tables, development standards and design guidelines) are contained within the revised Specific Plan document; and

WHEREAS, the Applicant submitted a Tentative Tract Map (File No. PMTT20-012/TTM 20389) to subdivide 5.99 acres of land into one numbered lot and three lettered lots for condominium purposes in conjunction with the Subarea 29 Specific Plan amendment; and

WHEREAS, the Subarea 29 Specific Plan Environmental Impact Report (State Clearinghouse No. 2004011009) was certified on October 17, 2006 (hereinafter referred to as “Certified EIR”), in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as “EIR Addendum”) in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as “CEQA”); and

WHEREAS, the environmental impacts of this project were thoroughly analyzed in the EIR Addendum, which concluded that implementation of the Project could result in a number of significant effects on the environment that were previously analyzed in the Certified EIR, and that the Certified EIR identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an EIR Addendum has been prepared to determine possible environmental impacts; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and make recommendation to the City Council on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity. The project site is also located within the Airport Influence area of Chino Airport and is consistent with policies and criteria set forth within the 2011 California Airport Land Use Planning Handbook published by the California Department of Transportation, Division of Aeronautics; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 17, 2021, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Addendum and the Project, and concluded said hearing on that date, voting to issue Decision No. DAB21-020, recommending that the Planning Commission recommend the City Council approve the Addendum; and

WHEREAS, as the first action on the Project, on May 25, 2021, the Planning Commission issued a Resolution recommending the City Council adopt the EIR Addendum, finding that the proposed Project introduces no new significant environmental impacts and applying all previously adopted mitigation measures to the Project, which were incorporated by reference; and

WHEREAS, on May 25, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: *Environmental Determination and Findings.* As the recommending body for the Project, the Planning Commission has reviewed and considered the information contained in the Addendum, the initial study, and the administrative record for the Project, including all written and oral evidence provided during the comment period. Based upon the facts and information contained in the Addendum, the initial study, and the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The environmental impacts of this project were reviewed in conjunction with an Addendum to the Subarea 29 Specific Plan Environmental Impact Report (State Clearinghouse No. 2004011009) was certified on October 17, 2006 in conjunction with File No. PSP03-003; and

(2) The EIR Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts; and

(4) All previously adopted mitigation measures shall be a condition of project approval, as they are applicable to the Project, and are incorporated herein by this reference; and

(5) The EIR Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and

(6) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts.

SECTION 2: *Subsequent or Supplemental Environmental Review Not Required.* Based on the EIR Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant

environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and.

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The Project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: *Housing Element Compliance.* Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the recommending authority for the Project, the Planning Commission finds that based upon the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and with approval of the related Specific Plan Amendment (File No. PSPA20-006) the proposed project will be consistent with the number of dwelling units and density (7-14 DU/AC) within Planning Area 27, as specified in the Available Land Inventory.

SECTION 4: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted

Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as "ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the recommending authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). The project site is also located within the Airport Influence area of Chino Airport and is consistent with policies and criteria set forth within the 2011 California Airport Land Use Planning Handbook published by the California Department of Transportation, Division of Aeronautics. As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP and the 2011 California Airport Land Use Planning Handbook.

SECTION 5: Concluding Facts and Reasons. Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 and 4, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed Specific Plan, or amendment thereto, is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Subarea 29 Specific Plan amendment will provide consistency between the Policy Plan Land Use Plan and the Subarea 29 Specific Plan Planning Area 27 land use designation. The proposed amendment will accommodate the construction of future residential land uses within the Specific Plan which is consistent with goals, policies, plans and City Council priorities of The Ontario Plan.

(2) ***The proposed Specific Plan, or amendment thereto, would not be detrimental to the public interest, health, safety, convenience, or general welfare of the City.*** The proposed amendment to the Subarea 29 Specific Plan will provide consistency between the Policy Plan Land Use Plan and the Planning Area 27 land use designation. The proposed Specific Plan Amendment will not be detrimental to the public interest, health, safety, convenience, or general welfare of the City. The land use change will provide residential land uses within the Subarea 29 Specific Plan, which is consistent with the type and intensity of development specified in The Ontario Plan and evaluated by the Subarea 29 Specific Plan Environmental Impact Report.

(3) ***In the case of an application affecting specific property, the proposed Specific Plan, or amendment thereto, will not adversely affect the harmonious relationship with adjacent properties and land uses.*** The proposed amendment will accommodate the construction of future residential land uses. The Specific Plan currently allows for a Motorcourt Cluster A (6- or 8- Plex), Paseo Cluster Lots, Motorcourt Cluster B (6-Plex), Motorcourt Cluster C (5-Plex), and Green Courts. The new Motorcourt Cluster D (8-Plex) product will allow for a cluster of up to 8 single-family homes, with smaller and more efficient building footprints.

(4) ***In the case of an application affecting specific property, the subject site is physically suitable, including, but not limited to, parcel size, shape, access, and availability of utilities, for the request and anticipated development.*** The subject site is physically suitable to accommodate the future residential land uses. The Subarea 29 Specific Plan amendment includes development standards to facilitate the proposed land use, which will be developed with adequate lot sizes, access, and utilities to serve the project site.

SECTION 6: Planning Commission Action. Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby RECOMMENDS THE CITY COUNCIL APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," the Subarea 29 Specific Plan Document "Attachment B," and incorporated herein by this reference.

SECTION 7: Indemnification. The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 8: Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 9: Certification to Adoption. The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of May 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 25, 2021, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**File No. PSPA20-006
Departmental Conditions of Approval**

(Departmental conditions of approval to follow this page)

ATTACHMENT B:

**File No. PSPA20-006
Subarea 29 Specific Plan Document**

(Subarea 29 Specific Plan document to follow this page)



SUBAREA 29

SPECIFIC PLAN ADOPTED OCTOBER 17, 2006

AMENDED MAY 1, 2007

AMENDED MARCH 19, 2008

AMENDED AUGUST 28, 2013

AMENDED APRIL 21, 2015

AMENDED XXX



SPECIFIC PLAN AMENDMENT

SUBAREA 29 SPECIFIC PLAN AT NEW MODEL COLONY

PSP 03-003; PSPA 07-003; PSPA 07-007; PSPA 13-002; PSPA 14-002

Approved by City of Ontario City Council on 10/17/2006

Approved by City of Ontario City Council on 05/01/2007

Approved by City of Ontario City Council on 03/19/2008

Approved by Zoning Administrator on 08/28/2013

Approved by City of Ontario City Council on 04/21/2015

Approved by City of Ontario City Council on 00/00/2021

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1. Specific Plan Overview





1.0 Specific Plan Overview

The Subarea 29 Specific Plan is comprised of approximately 539.7 gross acres. The project site is bounded by Eucalyptus Avenue to the north, Riverside County Flood Control Channel to the south, Haven Avenue to the east, Cucamonga Creek Channel to the west as illustrated in *Exhibit 1, Regional Location and Exhibit 2, Vicinity Map*.

The net acreage of the Specific Plan area is 449.9 net acres. This excludes street right-of-ways, Southern California Edison (SCE), and County Line easements.

The regional context and local setting of the Specific Plan area are illustrated in the *Regional Location Map, Exhibit 1, and Vicinity Map, Exhibit 2*.

The project site is consistent with and implements the gross Low Density Residential allocation (5.0 dwelling units permitted per gross acre) and broad “community design” policy requirements for the New Model Colony, including a general neo-traditional community design. These include:

- Creation of a “sense of place;”
- Diversity in housing product and architecture;
- Pedestrian connectivity;
- Smaller “neighborhoods;” and
- Elimination of the “subdivision” feel.

The New Model Colony General Plan Amendment adopted by the City of Ontario designated Planning Subarea 29 for development of residential, Neighborhood Center commercial, three (3) elementary, and one (1) middle school land uses. Residential land uses are designated within the Low Density Residential category, with a maximum permitted density of 5.0 dwelling units (du) per gross acre. A General Plan Amendment was adopted concurrently with the Specific Plan in 2006, modifying the General Plan Land Use Policy Map and Circulation Map to 1) reassign the location of one elementary school, 2) eliminate one elementary school, 3) relocate the middle school designation southeast of the intersection of Eucalyptus Ave. and Haven Ave., 4) “straighten” the Haven Ave. alignment between Merrill Ave. and Eucalyptus Ave., and 5) eliminate Neighborhood Center in the northeast portion of Subarea 29.

On January 26th, 2010, the City of Ontario adopted The Ontario Plan (TOP) which serves as the City’s new business plan and includes a long term Vision and a principle based Policy Plan (General Plan). The City’s Policy Plan, which acts as the City’s General Plan, designates (Policy Plan Exhibit LU-1 - Land Use Plan) the project site for development of Low Density Residential (2.1 -5.0 dwelling units per acre), Neighborhood Commercial, Public Schools, and Open Space - Parkland.

Consistent with the adopted General Plan allowing for 5.0 du/gross acre, Subarea 29 is permitted to include a total of 2,470 residential units based on a gross residential acreage of 494.2 acre; see Table 3-A for calculations. The Subarea 29 Specific Plan (2006) originally allocated a total of 1,937 dwelling units. The Specific Plan Amendment (2015) reallocated a variety of residential home types in Subarea 29 resulting in a total of 2,392 dwelling units, or 4.8 du/gross residential acre, 4.4 du/gross acre overall. Adjusting the total unit count implements the land use designation of the General Plan while maintaining the neighborhood crafting and architectural design standards of the original Specific Plan. This Specific Plan Amendment increases the total number of allocated dwelling units to 2,418 or 4.9 du/gross residential acre, 4.5 du/gross acre overall.

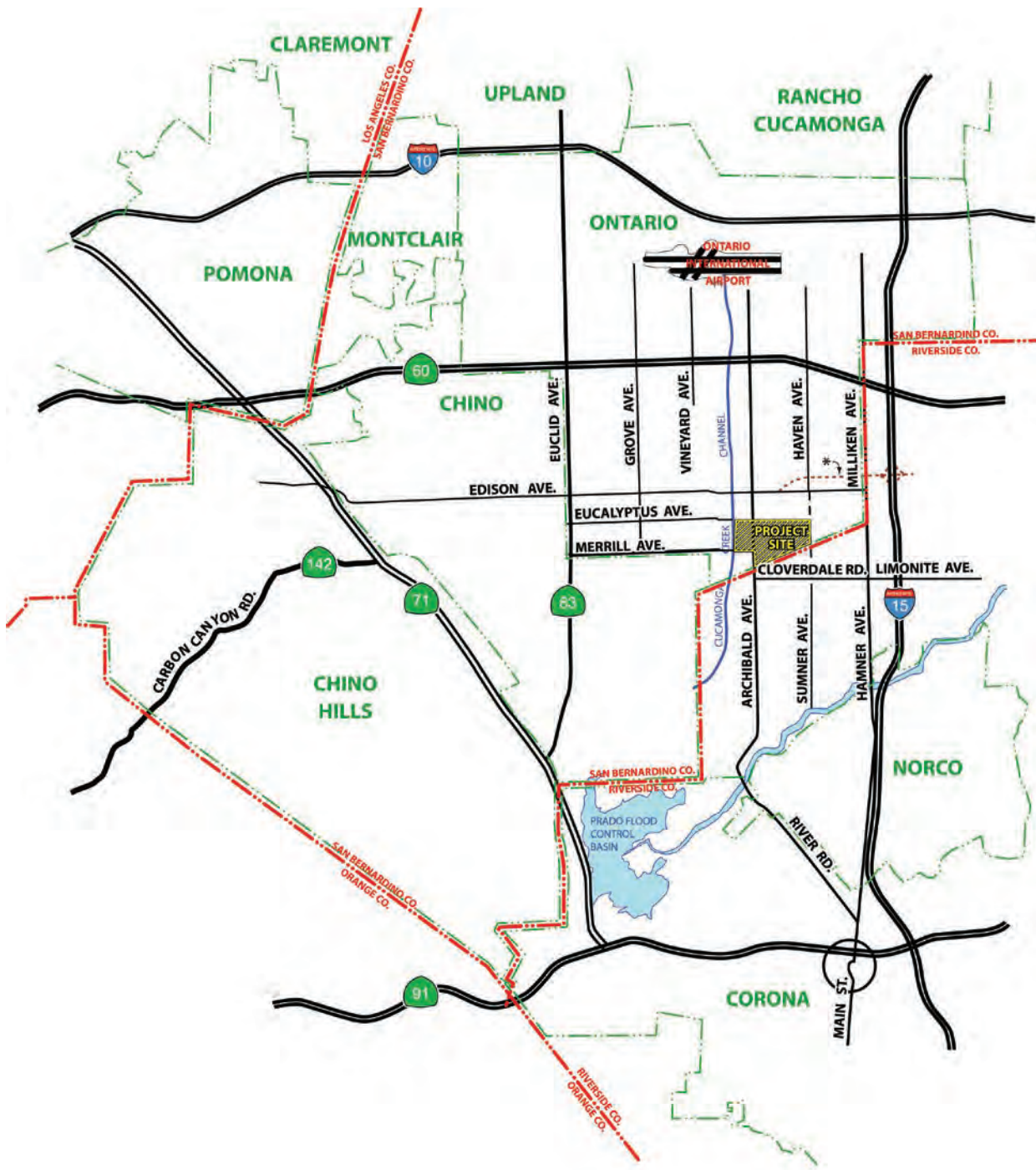
Overall, Subarea, 29 is planned for a total of thirty-one (31) planning areas. A total of twenty-six (26) residential neighborhoods are planned within Park Place, with varying lot sizes, product design, and architectural character. *Refer to Exhibit 3, Land Use Plan*. Single-family detached home types range from Cluster Homes to 6,300 square foot lots. A maximum total of 1,374 dwelling units are proposed for Park Place. Richland Communities, Inc. controls a total of five (5) residential neighborhoods with varying product, and lot sizes ranging from 3,500 square feet to 5,040 square feet. A maximum total of 936 dwelling units are planned for the five (5) planning areas controlled by Richland Communities, Inc. Brookfield Homes owns Planning Area 29. This planning area is planned for 108 lots featuring either lane or conventionally loaded designs. Combined these three land holders may develop 2,418 potential dwelling units within Subarea 29 Specific Plan area. *Refer to Table 3a on page 4-4*.

Non-residential uses proposed in the Specific Plan include a commercial parcel (Planning Area 2) consisting of 14.5 gross acres on the southwest corner of Eucalyptus Ave. and Archibald Ave. An elementary school (Planning Area 18) made up of 11.2 gross acres is included at the center of Subarea 29, directly adjacent to a neighborhood park (Planning Area 14) consisting of 7.7 gross acres.

Parks and the private Recreation Center form the “heart” of the Specific Plan and entire planned community. These include a private 3.1-acre private Recreation Center (Planning Area 15) and two (2) neighborhood parks (Planning Area 11 and 14) north and south of Merrill Ave. The two (2) neighborhood parks amount to 7.7 gross acres (north) and 6.2 gross acres (south) of Merrill Ave. Subarea 29 is planned as a walkable community.

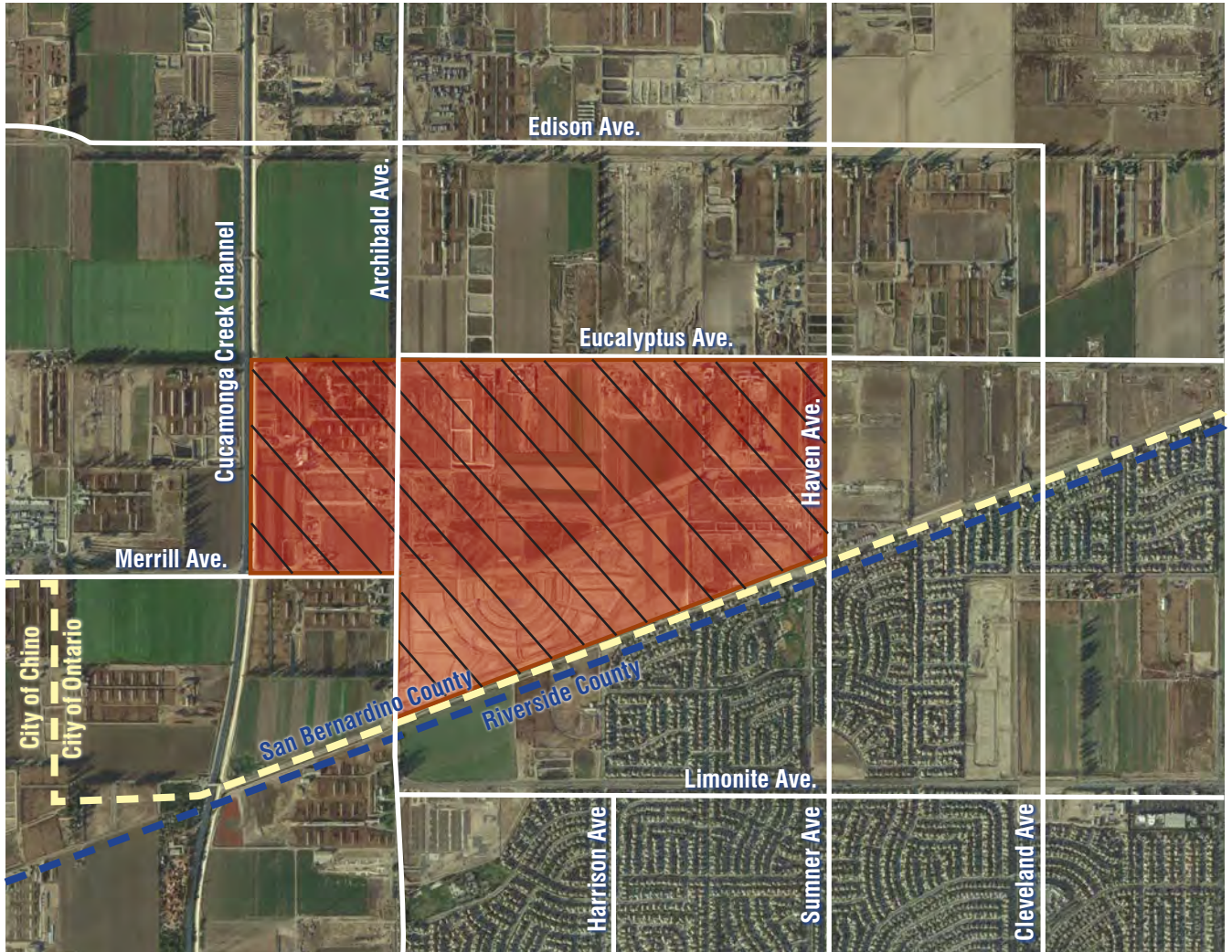
A system of pedestrian trails link all residential neighborhoods with the planned school, parks, and recreation amenities. Additionally, connections to the SCE Corridor trail and County Line Channel trail are also planned.

The internal street system includes a modified grid and landscaped parkways in all local residential streets. *Refer to Exhibit 3, Land Use Plan*.



*Cantu - Galleano Ranch Road Realignment

EXHIBIT I—REGIONAL LOCATION



LEGEND	
	Specific Plan Area
	County Boundaries
	City Boundaries

EXHIBIT 2—VICINITY MAP

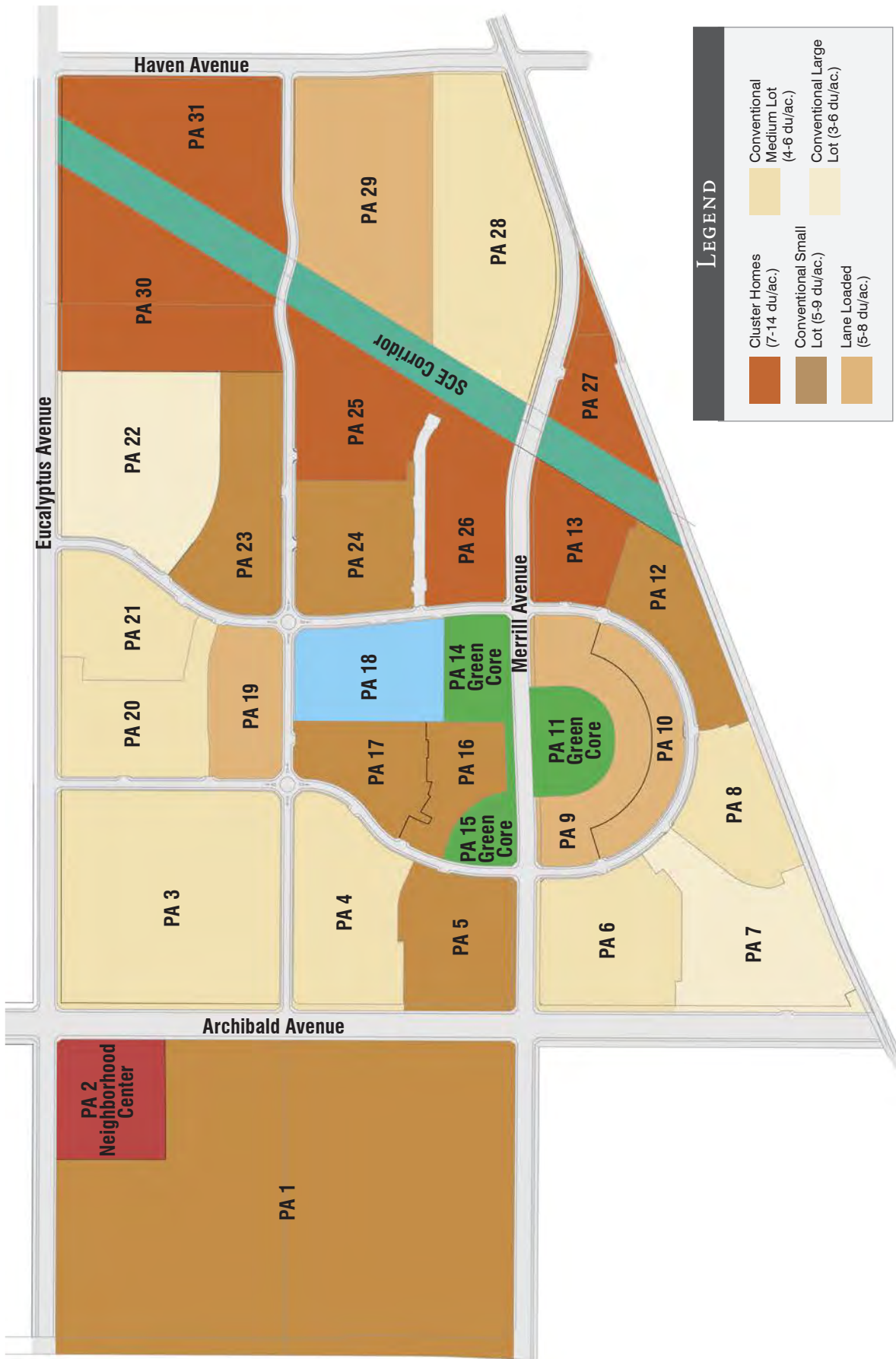


EXHIBIT 3—LAND USE PLAN

TABLE I—SPECIFIC PLAN LAND USE SUMMARY

Planning Area	Land Use	Minimum Lot Size (S.F)	Planned Dwelling Units ^{3,4,5}	Net Acre ¹	Planned Net Density (Du/Ac.) ^{3,5}	Gross Acres ²	Planned Gross Density (Du/Ac.) ^{3,5}
1*	Conventional Small Lot	3,500	432	83.1	5.2	89.8	4.8
2*	Commercial	N / A	0	12.1	0.0	14.5	0.0
3*	Conventional Medium Lot	4,500	186	34.5	5.4	40.2	4.6
4**	Conventional Medium Lot	4,250	88	10.1	8.7	17.8	4.9
5**	Conventional Small Lot	3,825	68	7.2	9.4	13.7	5.0
6**	Conventional Medium Lot	5,000	67	13.0	5.2	17.0	4.0
7**	Conventional Large Lot	6,300	65	15.3	4.2	18.3	3.6
8**	Conventional Medium Lot	4,250	46	9.1	5.1	9.6	4.8
9**	Lane Loaded	3,150	69	9.9	7.0	11.9	5.8
10**	Lane Loaded	3,600	57	6.6	8.7	7.8	7.3
11**	Neighborhood Park 2	N / A	0	5.7	0.0	6.2	0.0
12**	Conventional Small Lot	3,825	53	9.5	5.6	9.5	5.6
13**	Cluster Homes	2,100 ^A	75	7.8	9.6	7.8	9.6
14**	Neighborhood Park 1	N / A	0	6.3	0.0	7.7	0.0
15**	Recreation Center	N / A	0	2.7	0.0	3.1	0.0
16**	Conventional Small Lot	3,015	41	5.9	7.0	6.1	6.8
17**	Conventional Small Lot	3,015	56	5.3	10.6	8.4	6.7
18**	School	N / A	0	10.0	N / A	11.2	N / A
19**	Lane Loaded	3,150	61	7.8	7.9	9.0	6.8
20**	Conventional Medium Lot	4,250	67	11.8	5.7	13.3	5.0
21**	Conventional Medium Lot	5,000	48	10.1	4.8	11.5	4.2
22**	Conventional Large Lot	6,300	79	19.7	4.0	21.3	3.7
23**	Conventional Small Lot	3,825	82	12.9	6.3	14.4	5.7
24**	Conventional Small Lot	3,400	75	8.1	9.3	12.8	5.8
25**	Cluster Homes	2,100 ^A	102	8.6	11.8	12.9	7.9
26**	Cluster Homes	2,100 ^A	102	8.7	11.7	13.2	7.7
27**	Cluster Homes	1,750 ^A 2,100	73 47	7.6	9.7 6.2	7.6	9.6 6.2
28*	Conventional Medium Lot	4,050	121	23.0	5.3	25.8	4.7
29***	Lane Loaded or Conventional Medium Lot	3,150 or 4,000	108	21.4	5.0	27.2	4.0
30*	Conventional Large Lot	5,040	110	21.9	5.0	28.3	3.9
31*	Conventional Medium Lot	4,050	87	16.0	5.4	23.1	3.8
Flood Control Channel	Flood Control Channel	N / A	0	7.2	0.0	7.2	0.0
Pump Station	Pump Station	N / A	0	0.2	0.0	0.4	0.0
SCE Corridor	Park Place SCE Easement	N / A	0	11.2	0.0	11.2	0.0
Sub Area 29 Total			2,418 2,392	449.9	5.4 5.3	539.7	4.5 4.4

- A) Minimum square footage identified is for exclusive use area on a per home basis, recorded lot size may differ.
- 1) Gross Acres calculated to centerlines of Master Planned streets minus SCE easements.
- 2) Net Acres excludes street rights-of-way and SCE easements.
- 3) Actual total units and gross/net density and acreage will be dependent on final lotting.
- 4) Target unit count based on submitted Tentative "B" Maps
- 5) A density transfer of 15.0% may occur between Planning Areas.

- *) Indicates Planning Areas as controlled/owned by Richland Communities, Inc.
- ***) Indicates "Park Place" Planning Areas as controlled/owned by SL Ontario Development Company
- ***) Indicates Planning Areas as controlled/owned by Brookfield Homes



1.1 PURPOSE FOR AMENDMENT

This Specific Plan Amendment (2021) modifies PA-27 to permit a greater number of units. This unit increase would change the overall Subarea 29 density from 4.8 dwelling units per gross acre to 4.9 dwelling units per gross acre. This Amendment also permits an additional cluster housing typology.

1.2 GOVERNING DOCUMENTS

Development of the Subarea 29 Specific Plan will be regulated by the following documents:

- The Ontario Plan (TOP) Policy Plan (General Plan) which establishes policies governing land use, circulation, housing, conservation and open space, noise, safety, and public facilities within the Specific Plan area;
- The Specific Plan, to include a Land Use Plan, Infrastructure Plan, Design Guidelines, and Development Regulations;
- The City of Ontario Development Code, as applicable to the project site, where the Specific Plan is silent on development standards and regulations;
- The City of Ontario Subdivision Ordinance, regulating the subdivision of land within the project site area;
- The Airport Land Use Compatibility Plans of Chino Airport and Ontario International Airport;
- The approved Subarea 29 Environmental Impact Report; and
- Covenants, Conditions, and Restrictions (CC&R's) to be established by the developer of Park Place, and other developers in Subarea 29 as a means of ensuring and enforcing quality design and development of the master planned community.

Specific Plan Components

The Subarea 29 Specific Plan is organized into the following sections in addition to Section 1, Specific Plan Overview.

2.0 Introduction

The Introduction provides:

- The project site setting;
- A general description of the project site proposal;
- The goals and policies of the Specific Plan;
- The entitlements to accompany the Specific Plan; and
- The relationship of the Specific Plan to The Ontario Plan (TOP) Policy Plan (General Plan) and the City of Ontario Development Code.

3.0 Existing Conditions

This section describes the setting for the project site, outlining the existing physical conditions on and around the site at the time of the original approval.

4.0 Land Use

The Land Use section describes the various residential planning areas and residential types, commercial planning area, school planning area, and the parks and trails within the planned community.

5.0 Infrastructure and Public Improvements

This section provides information on proposed circulation improvements, the planned backbone water, sewer, and storm drain systems, the grading concept for the project site, and a discussion of public utilities and services to serve the Specific Plan area.



6.0 Development Regulations

The Development Regulations identify permitted uses and standards regulating the development of various residential product types and commercial uses. The relationship of the Specific Plan development regulations to the City of Ontario Development Ordinance is also provided. The policies and procedures for the City's review and approval of future development proposals within the Specific Plan are included in this section. This section provides the methods and procedures for interpreting and amending the Specific Plan, as necessary in the future.

7.0 Design Guidelines

The Specific Plan Design Guidelines are intended to direct the site planning, landscaping, and architectural quality of the development. Streetscapes, entries, edge treatments, walls and fencing, lighting, signage, and architectural design are addressed in the Design Guidelines.

8.0 Commercial Design Guidelines

This section will direct site planning, architectural design, and landscaping for the commercial parcel.

9.0 General Plan Consistency

This section includes the City of Ontario General Plan consistency matrix describing the relationship of the Specific Plan to each policy of The Ontario Plan (TOP) Policy Plan (General Plan).



2. Introduction





2.0 Introduction

The Specific Plan is comprised of 539.7 gross acres designated as Planning Subarea 29 of the New Model Colony, within the City of Ontario. The Specific Plan area illustrated in Exhibit 2, Vicinity Map, is bounded by Eucalyptus Ave. on the north, Haven Ave. on the east, County Line Channel on the south, and Cucamonga Channel and Archibald Ave. on the west. The Specific Plan is a comprehensive plan proposed by SL Ontario Development Company, Richland Communities, Inc, and Brookfield Homes, for the development of residential neighborhoods designed to reflect the small town character of older established communities.

The Specific Plan also includes a commercial site, parks, and school uses. The Specific Plan establishes Development Standards and Design Guidelines for a commercial land use area within Planning Subarea 29.

It should be noted that use of the term “Park Place” throughout this document applies only to the SL Ontario Development Company ownership.

2.1 PURPOSE AND OBJECTIVES

2.1.1 PURPOSE

The Subarea 29 Specific Plan serves to implement the City’s Policy Plan (General Plan) for the project site and provides zoning regulations for development of the project site by establishing permitted land use, development standards, infrastructure requirements, and implementation requirements for development. A comprehensive set of design guidelines and development regulations are included to guide and regulate site planning, landscape, and architectural character within the community ensuring that excellence in community design is achieved during project development. The Subarea 29 Specific Plan establishes the procedures and requirements to approve new development within the project site.

The Specific Plan is designed to address the following guiding planning principles:

- ♦ Diversity and choice of single-family housing types and opportunities to address a variety of lifestyles, home sizes, and economic segments of the marketplace;
- ♦ Connectivity among land uses within the Specific Plan area and to surrounding public facilities and the existing Ontario community;
- ♦ Use of traditional development patterns as found in older established neighborhoods in Southern California;

- ♦ Recreational amenities within walking distance of all residential neighborhoods;
- ♦ Bicycle and pedestrian accessibility and mobility to encourage alternative modes of travel;
- ♦ Diversity in architectural design; and
- ♦ Sustainable development practices addressing energy efficiency.

2.1.2 Objectives

The following objectives are established for the Subarea 29 Specific Plan.

Residential Areas

- ♦ Residential neighborhoods designed at a “human scale” and oriented to pedestrian activity;
- ♦ Connectivity provided among residential neighborhoods and recreational areas through a network of pedestrian sidewalks and on-street bicycle trails;
- ♦ Connectivity provided between residential neighborhoods and the adjacent commercial land use, as well as to the central elementary school, by means of pedestrian and bicycle trail linkages;
- ♦ A variety of housing types incorporated into the land use plan addressing lifestyle considerations of singles, families, and empty nesters;
- ♦ Residential neighborhoods designed around a central park “green core,” “framing” green spaces, promoting active and passive recreational activity and casual social interaction among neighbors; and
- ♦ Residential neighborhoods with diverse architectural styles and traditional design elements reflecting the characteristics of older established Ontario neighborhoods.

Commercial Area

- ♦ Neighborhood commercial uses to meet the needs of the residential community within the project site as well as the larger surrounding market area;
- ♦ Provision for trails connecting the residential community with the commercial center and adjacent uses;
- ♦ The development of plaza areas and other amenities within the commercial center providing space for social interaction; and
- ♦ Orientation of commercial buildings to the street wherever possible to create an urban edge and sense of arrival.

Streets and Pedestrian/Bicycle Mobility

- ♦ Streets and lanes (private alleys) designed in a modified grid pattern, reminiscent of traditional neighborhood streets;
- ♦ Streets with landscaped parkways and pedestrian walkways separated from the street, to create a pleasant and safe pedestrian environment, promoting friendly interaction among neighbors;
- ♦ Several traffic calming techniques within the project site street design including enhanced parkway landscaping, median landscaping, and tapered street intersections and lane entrances to influence a driver's peripheral vision and encourage motorists to drive more slowly; and
- ♦ Inside turning radii at corners reduced to slow traffic at corners.

Recreation / Trails

- ♦ New recreational opportunities for residents through the development of two (2) neighborhood parks centered within the community "green core" to include play fields, tot lots, and picnic areas. Additionally, a private Recreation Center including a recreation building with swimming pool for Park Place residents (3.13 acres), community tennis courts, and other areas for passive recreation. The two (2) neighborhood parks amount to 7.7 acres (north) and 6.2 acres (south) of Merrill Ave.;
- ♦ A portion of the City's Multipurpose Trails and Bikeway Corridor Master Plan provided through development of a multipurpose trail within the SCE property connecting County Line Channel and Merrill Ave. with Eucalyptus Ave.; and
- ♦ A system of on-street multipurpose trails and bicycle lanes integrated into the project site providing bicycle access from the residential community to surrounding areas.

Sustainable Development

- ♦ The opportunity for development of residential units designed with living areas on the second floor and home office areas on the first floor;
- ♦ Native plant materials and non-invasive ornamental landscape materials will be incorporated into the landscaping plan for portions of the SCE property developed for trail purposes;
- ♦ Residential development will be connected to the commercial land use area located within the site through pedestrian and bicycle trails promoting walking and bicycling as an alternative means of travel to the automobile to and from these areas;
- ♦ Residential development will be equipped with the latest technology for internet access allowing residents to shop and work on-line, thereby reducing vehicle trips to employment centers and shopping;

- ♦ Canopy trees will be utilized in public areas such as parkways, medians, and the public parks to provide shade to counteract the heat island effect;
- ♦ Roadway widths will be reduced as much as possible to minimize heat generating asphalt surfaces; and
- ♦ Recycled water will be utilized for the central public parks, neighborhood edges, and other common landscape areas.

2.2 SPECIFIC PLAN PROPOSAL

2.2.1 Project Summary

The Specific Plan Land Use Plan is described below and in the Specific Plan Statistical Summary, Tables 1-1b, pages 4-3 through 4-4.

Residential Uses

The Subarea 29 Specific Plan will provide for development of a variety of residential housing types, oriented toward park and open space amenities and designed to promote walkability and interaction among residents. Residential development within 494.2 acres of the Specific Plan area (per Table 1a, Specific Plan Land Use Summary) will contain up to 2,392 dwelling units, providing a varied mix of single-family detached housing types, as described below.

Residential Detached

Five (5) types of single-family detached residential development products are planned for the site; Planning Areas may be identified with more than one type of planned development product to allow for flexibility within the maximum number of permitted dwelling units. These include:

Cluster Homes (PA 13, 25, 26 & 27)

Cluster Homes are single-family detached residential development with vehicular access from lanes (private alleys or motorcourts) via interior streets, with direct access garages. These residential areas will be developed at an approximate net density of 7-14 dwelling units per acre with a exclusive use area size of at least ~~2,100~~ 1,750 square feet.

Conventional Small Lot

(PA 1, 5, 12, 16, 17, 23 & 24)

Conventional single-family detached residential development will consist of cottage style residential homes designed with vehicular access provided from local interior streets. Residential areas will be developed at an approximate net density of 5-9 dwelling units per acre with a minimum lot size of at least 3,015 square feet.



Lane Loaded (PA 9, 10, 19 & 29)

Detached single-family residential development consists of lane-served residential units designed with an orientation to the street or green spaces, by locating garages to the rear of residential units. Residential areas will be developed at an approximate net density of 5-8 dwelling units per acre with a minimum lot size of at least 3,100 square feet.

Conventional Medium Lot (PA 3, 4, 6, 8, 20, 21, 28, 29 & 31)

Conventional single-family detached units with orientation to local streets and direct access garages, in varying orientations. Residential neighborhoods will be developed at an approximate net density of 4-6 dwelling units per acre, with a minimum lot size of 4,000 square feet.

Conventional Large Lot (PA 7, 22 & 30)

These are the largest single-family detached lots in the Specific Plan, conventionally accessed from local streets. Net densities average approximately 3-6 dwelling units per acre, with a minimum lot size of 5,000 square feet.

- ♦ The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan;
- ♦ Standards and criteria by which improvements will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable; and
- ♦ A program of implementation measures including regulations, programs, public works projects and the financing measures necessary to carry out paragraphs 1, 2, and 3 above. The Specific Plan shall include a statement of the relationship of the Specific Plan to the General Plan.

The Subarea 29 Specific Plan meets the requirements of the State of California Government Code.

2.3 AUTHORITY AND REQUIREMENTS

2.3.1 Authority

State of California Government Code, Title 7, Division 1, Chapter 3, Article 8, Section 65450-57 grants authority to cities to adopt Specific Plans for purposes of implementing the goals and policies of their General Plans. The Government Code specifies that specific plans may be adopted either by resolution or by ordinance and that the Specific Plan is required to be consistent with the General Plan. The City of Ontario adopts specific plans by ordinance, thereby establishing the zoning regulations for development of the project site. The requirements of the Specific Plan shall take precedence over the City of Ontario Development Code. In instances where the Specific Plan is silent, the City of Ontario Development Code shall prevail.

2.3.2 Requirements of the Specific Plan

California Government Code Section 65451 sets forth the minimum requirements and review procedures for Specific Plans as follows:

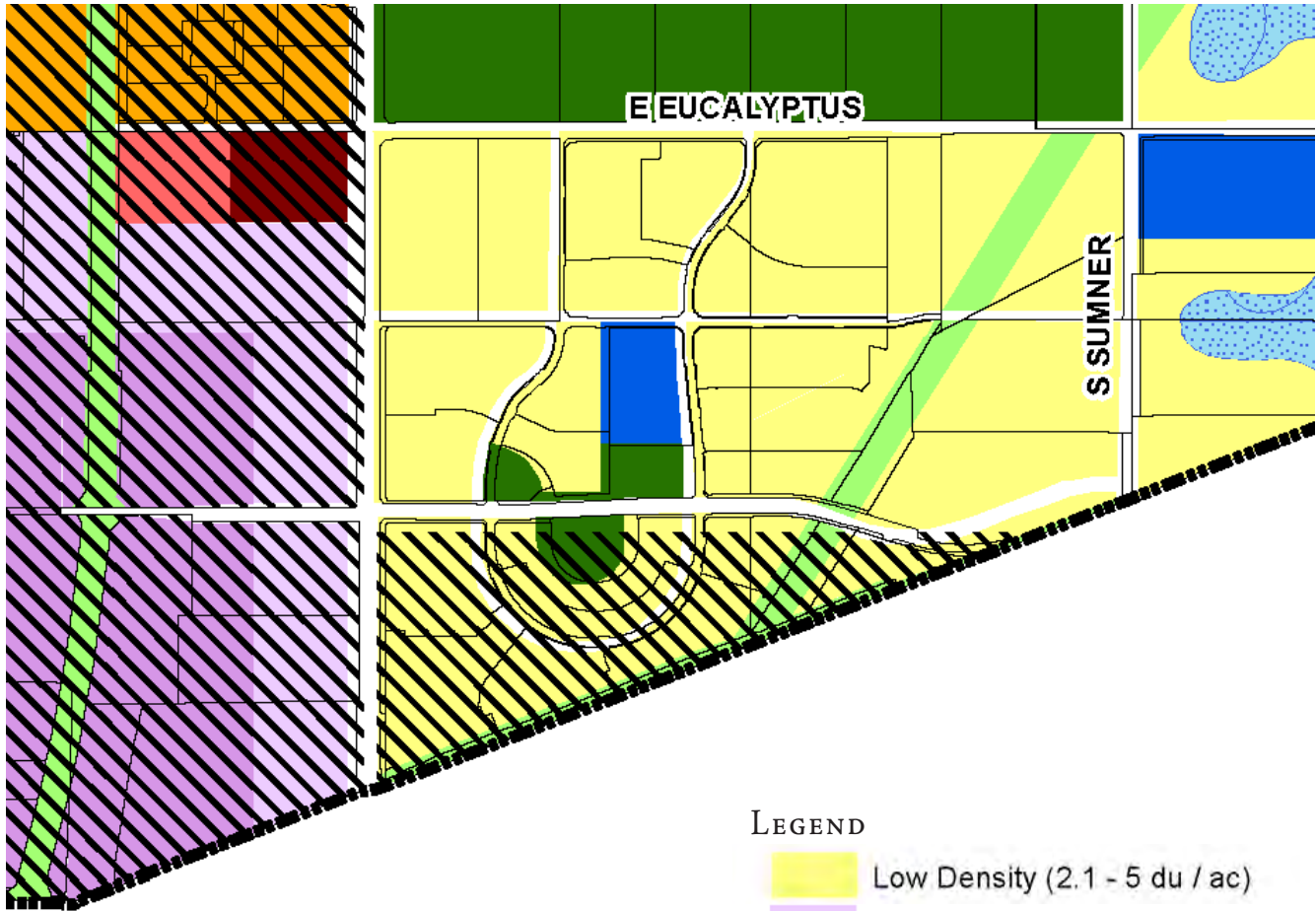
A Specific Plan shall include a text and a diagram or diagrams, which specify all of the following in detail:

- ♦ The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan;

2.3.3 Development Agreement Required

Unless done in a coordinated manner and with adequate fiscal planning, development projects within the New Model Colony are likely to present a challenge in their implementation because of the lack of existing public facilities, including, streets, sewerage, transportation, drinking water, school, and utility facilities. California law has established a mechanism for ensuring the adequate provision of such facilities, while at the same time providing assurances to applicants that, upon approval of the project, the applicants can proceed with their projects. Approval of this Specific Plan without a development agreement may result in a waste of resources, escalate the cost of housing to the consumer, and discourage investment in and commitment to comprehensive planning, as envisioned by the City, which seeks to make maximum efficient utilization of resources at the least economic cost to the public.

Therefore, a statutory development agreement, authorized pursuant to California Government Code sections 65864 et seq., shall be required as part of the approval of this Specific Plan. For the above-mentioned reasons, the development agreement for this Specific Plan shall include, among other things, methods for financing acquisition and construction of infrastructure, acquisition and development of adequate levels of park land, and schools, as well as the provision of adequate housing opportunities for various segments of the community consistent with the regional housing needs assessment. SL Ontario Development Company, Richland Communities, Inc., and Brookfield Homes may each enter into separate development agreements with the City.



LEGEND

- Low Density (2.1 - 5 du / ac)
- Business Park (0.6 FAR)
- Industrial (0.55 FAR)
- Open Space - Non Recreation
- Open Space - Parkland
- Public School
- Chino Airport Overlay



EXHIBIT 4—POLICY PLAN LAND USE PLAN (EXHIBIT LU_01)

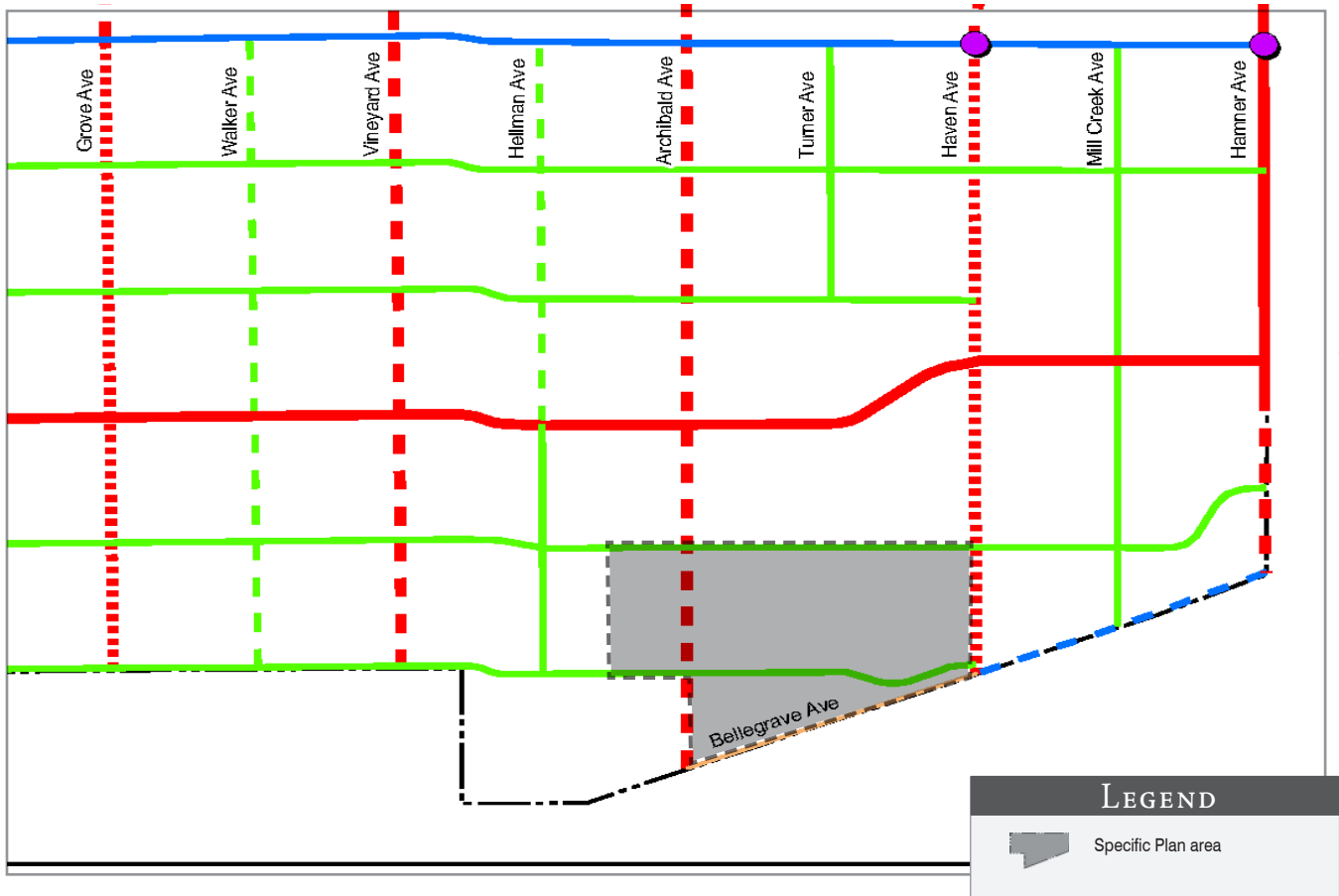


EXHIBIT 5—POLICY PLAN FUNCTIONAL ROADWAY CLASSIFICATION PLAN



2.4 DEVELOPMENT APPROVAL COMPONENTS

The components of the development approval process for the Specific Plan are as follows:

General Plan (Policy Plan)

The Ontario Plan (TOP) establishes the direction and vision for the City of Ontario providing a single guidance system that will shape the Ontario community for the future. TOP provides for policies to accommodate change over a 30 year period commencing in 2010, the beginning of the planning period. TOP consists of a six part Component Framework: 1) Vision, 2) Governance Manual, 3) Policy Plan, 4) City Council Priorities, 5) Implementation, and 6) Tracking and Feedback.

Specific Plan

The Policy Plan (General Plan) requires the approval of a Specific Plan for development of the project site to ensure that sufficient land area is included to achieve unified districts and neighborhoods. The City of Ontario has zoned the project site as AG-Specific Plan. The zoning designation of AG-Specific Plan requires that a Specific Plan be approved to guide development of the project site and to implement the goals and policies of the Policy Plan. The Specific Plan provides the zoning regulations to govern development of the project site. The requirements of the Specific Plan shall take precedence over the City of Ontario Development Code. In instances where the Specific Plan is silent, the City of Ontario Development Code shall prevail.

Subdivision Maps

A tentative tract map(s) will be reviewed by the City of Ontario for the residential portion of the Specific Plan indicating the approximate location of lot lines, streets, and proposed grading. Following approval by the City of the tentative tract map(s), a final map(s) will be prepared. Parcel maps will be approved by the City of Ontario for development proposed within the commercial portion of the Specific Plan.

2.5 CEQA COMPLIANCE

A project site Environmental Impact Report (EIR) prepared by the City of Ontario for the Specific Plan, in accordance with the California Environmental Quality Act (CEQA), addressed impacts associated with the Specific Plan and subdivision map. The EIR recommended mitigation measures to reduce impacts of the project site to a less than significant level. The EIR was prepared as the basis for the environmental review for all subsequent discretionary and ministerial actions.

2.6 RELATIONSHIP TO GENERAL PLAN AND ZONING

The City's Policy Plan, which acts as the City's General Plan, designates the project site for development as follows:

- Low Density Residential (2.1 -5.0 dwelling units per acre)
- Neighborhood Commercial
- Public Schools
- Open Space - Parkland
- Open Space - Non-Recreation

The Policy Plan (General Plan) establishes a development capacity of 2,228 dwelling units and approximately

189,486 square feet of commercial use and 369,171 of commercial office use for the project site. The Policy Plan Residential Capacity is further broken down as follows:

TABLE 2—POLICY PLAN RESIDENTIAL CAPACITY

Gross Acres	TOP Land Use Designation	Maximum Permitted Dwelling Units
495.2	Low Density Residential	2,228
Total: 495.2		2,228*

*Densities do not reflect maximum allowable densities by the Policy Plan and are based on TOP EIR densities (average) of 4.5 du/ac for Low Density.

California Government Code (Title 7, Division 1, Chapter 3, Article 8, Section 65450-65457) permits the adoption and administration of specific plans as an implementation tool for elements contained in the local general plan. Specific plans must demonstrate consistency in regulations, guidelines, and programs with the goals and policies set forth in the general plan. The Subarea 29 Specific Plan has been prepared in conformance with the goals and policies of the City of Ontario Policy Plan (General Plan). The policy analysis is located in the Appendix of this document, describes the manner in which the Subarea 29 Specific Plan complies with the Policy Plan policies applicable to the project.

2.7 DEVELOPMENT PLAN REVIEW

Following the approval of the Specific Plan, all development proposals for individual residential neighborhoods or product areas within the Specific Plan will be subject to the Development Plan Review process pursuant to Article 8: Development Plan Review of the City of Ontario Development Code.

2.8 AIRPORT LAND USE COMPATIBILITY PLANNING CONSISTENCY

All development proposals or Specific Plan Amendments are required to be consistent with the Airport Land Use Compatibility Plans of Chino Airport and Ontario International Airport.



3. Existing Conditions





3.0 Existing Conditions

3.1 PROPERTY OWNERSHIP

The 539.7 gross acre Subarea 29 Specific Plan consists of twenty-two (22) parcels with eight (8) separate ownerships. Three (3) separate developers, SL Ontario Development Company, Richland Communities, Inc., and Brookfield Homes are involved in the entitlement process to develop portions of the study area at this time. *Exhibit 6, Existing Property Ownership & Williamson Act Contract Status* illustrates this information.

3.2 WILLIAMSON ACT STATUS

The Williamson Act program is designed as a mechanism for the preservation of agricultural and open space lands in the State of California. Within the Subarea 29 Specific Plan, only one Williamson Act contract is active. *Exhibit 6, Existing Property Ownerships & Williamson Act Contract Status* illustrates the location and status of the Williamson Act contract within the Subarea 29 Specific Plan.

3.3 AIRPORT LAND USE COMPATIBILITY PLAN CONSISTENCY

The Project Site is located within the Airport Influence Areas of Chino Airport and Ontario International Airport (ONT) as depicted in Exhibit 10, Airport Influence Areas.

3.4 EXISTING SITE CONDITIONS AND IMPROVEMENTS

Existing on-site land uses include dairy farming, agriculture, and the SCE transmission corridor. The 539.7 gross acre Specific Plan area has historically been used for general agricultural purposes. Agricultural activities remain within the Specific Plan area and include operations such as dairy farming and row crop production. The site contains farm buildings supporting the existing agricultural activities.

3.5 SURROUNDING LAND USES

Uses surrounding the Specific Plan area include residential, dairy farming, agriculture, vacant open space, and roadways. Refer to *Exhibit 8, Aerial Photo*, which documents existing and surrounding land uses.

3.6 TOPOGRAPHY

The project site is relatively flat with gently sloping topography of approximately 2%, sloping in a northeasterly to southwesterly direction. *Exhibit 18, Conceptual Grading Plan*, illustrates existing and proposed topographic contours on the site. There are no significant landforms or rock outcroppings within the project site.

3.7 HYDROLOGY

Most of the project site is presently in agricultural use, and only a limited portion is now covered with impervious surfaces. Normal rainfall in the area is, therefore, able to percolate through on-site soils and does not result in high volumes of surface runoff, typically associated with urban areas. During periods of heavy rainfall, when ground surfaces are saturated, surface runoff is collected in the existing storm drains, culverts, and retention basins located within the surrounding area.

Groundwater within and surrounding the project site as a whole, contains high concentrations of salt, attributable to historic agricultural activities such as dairy farming. The high organic content of on-site soils has contributed incrementally to the degradation of surface and groundwater quality. Removal of the organic materials which constitute by-products of those dairy operations and compliance with National Pollution Discharge Elimination System (NPDES) and other storm water permit requirements will be required as part of project site development.

3.8 EXISTING CIRCULATION/ACCESS

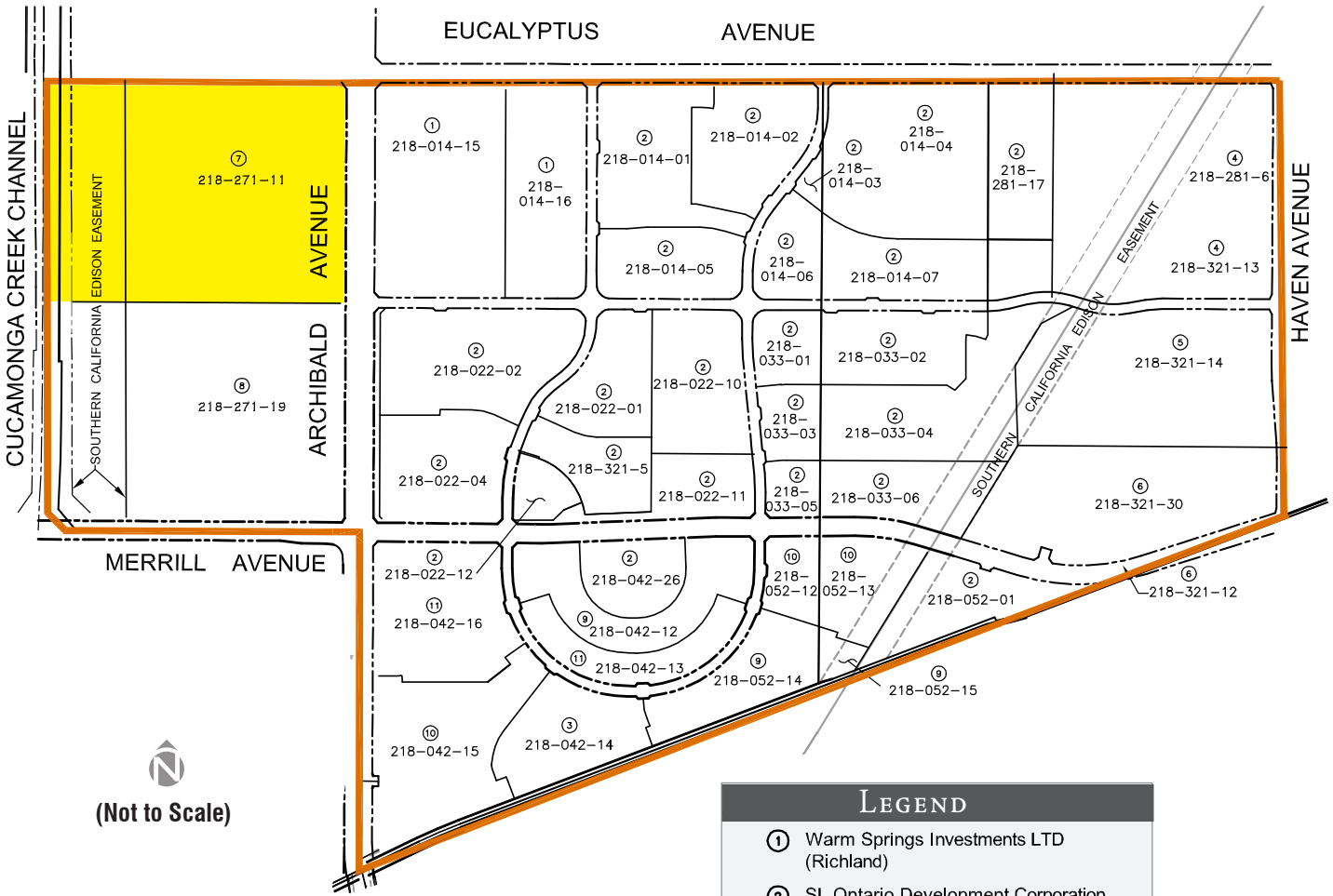
3.8.1 Regional Circulation

The Pomona Freeway, State Route (SR-60), is located approximately one (1) mile to the north of the New Model Colony General Plan area. In this area, SR-60 has full diamond-type interchanges with Euclid Ave., Grove Ave., Vineyard Ave., Archibald Ave., Haven Ave., and Milliken Ave. SR-60 carries approximately 160,000 vehicles per day near the City of Ontario sphere of influence area, also referred to as the New Model Colony (NMC) General Plan area.

The Ontario Freeway, Interstate 15 (I-15), is located approximately one-half mile to the east of the New Model Colony General Plan area, and carries approximately 90,000 vehicles per day in the vicinity of the NMC General Plan area. The closest arterial interchange to the NMC General Plan area is located at Limonite Road in Riverside County, approximately 3-¼ miles south of the SR-60 interchange with I-15.

State Highway SR-83 (Euclid Ave.) extends along the western boundary of the NMC General Plan area. Another major freeway in the area is SR-71 (Chino Valley Freeway), located between SR-91 and SR-60, and is approximately 2-3 miles southwest of the NMC General Plan area. This freeway is a major commuter route in southwestern San Bernardino County, connecting the SR-91 freeway with SR-60 and I-10 freeways.

Regional access to the project site is provided from Interstate 15 (I-15) located approximately 1.5 miles east of the project site and from SR-60, located approximately 2.2 miles north of the project site. Refer to *Exhibit 1, Regional Location*, page 1-2.




(Not to Scale)



LEGEND	
①	Warm Springs Investments LTD (Richland)
②	SL Ontario Development Corporation
③	Ryland Homes of California
④	Van Dam Gysbert & Patricia Trust (Richland)
⑤	Brookfield Homes
⑥	Richland Roseville LTD (Richland)
⑦	Ketchoyian Rose TR (Richland)
⑧	Te Velde Ralph And Carol Jean (CM) (Richland)
⑨	KB Home
⑩	Woodside Homes
⑪	Tri Pointe Homes
	Sub Area 29 Specific Plan Boundary
	Active Contract (till 2016)

EXHIBIT 6—EXISTING PROPERTY OWNERSHIPS AND WILLIAMSON ACT CONTRACT STATUS

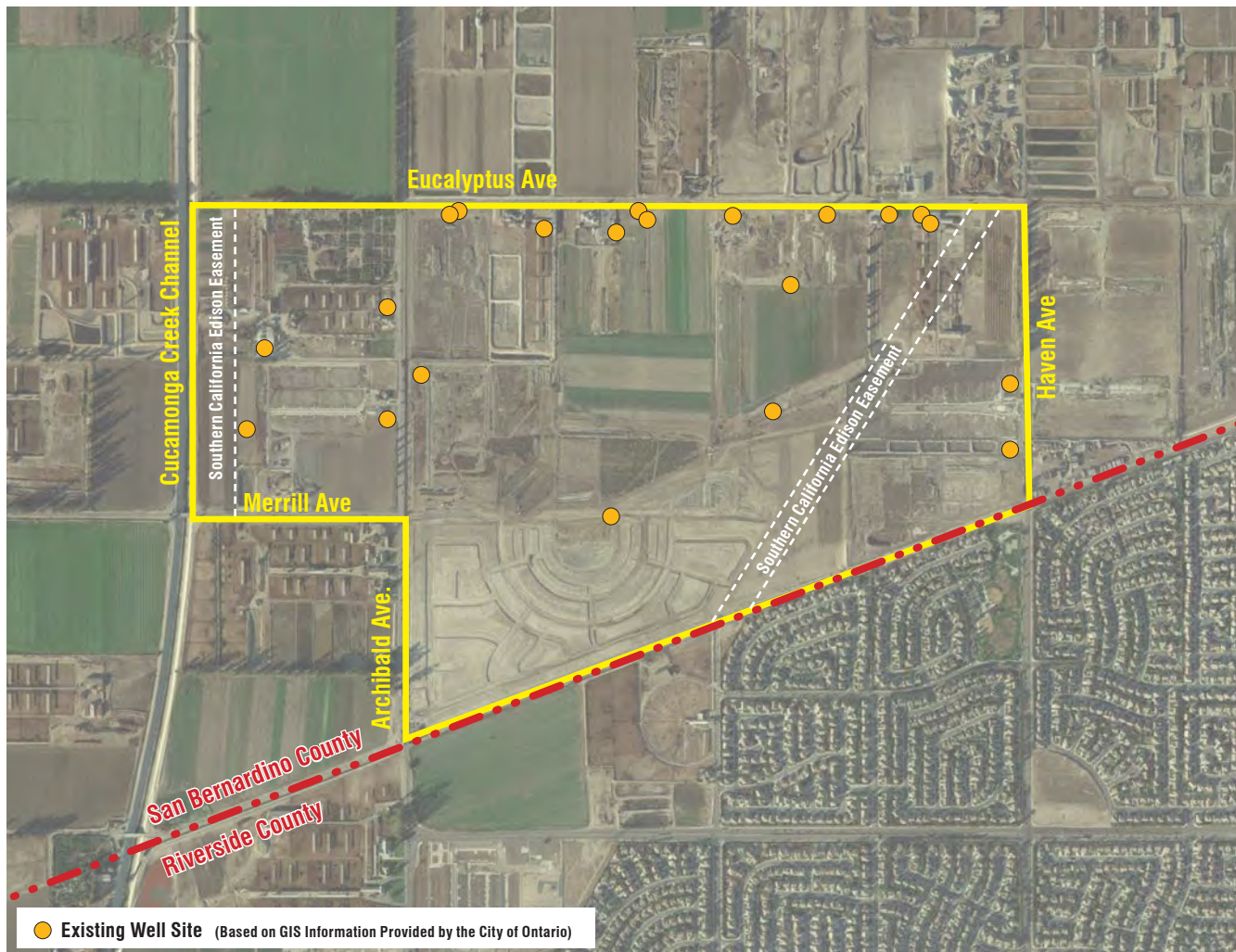


EXHIBIT 7—AERIAL PHOTO



3.8.2 Local Circulation

No local streets exist at present to serve the project site. Archibald Ave. provides arterial access. It has an existing intersection with Eucalyptus Ave. west of the project site.

Archibald Ave. is improved with approximately 36 (thirty-six) feet of paved area from the southerly boundary of the site to Eucalyptus Ave. A five-foot wide shoulder is located adjacent to Archibald Ave. on both sides of the street. Eucalyptus Ave. is improved with approximately 26.5 feet of paved area between Archibald Ave. and the Cucamonga Channel. Haven Ave. is improved with a paved area varying in width from fourteen (14) feet to seventeen (17) feet. Refer to *Exhibit 8, Policy Plan Functional Roadway Classification Plan*, which illustrates the General Plan Circulation network for the New Model Colony.

3.9 EXISTING INFRASTRUCTURE/UTILITIES

3.9.1 Water Supply Facilities

The closest water mains to the project site are located over 1.5 miles north of the Specific Plan area in Riverside Drive. This water main has not been sized to serve significant development within the New Model Colony. A 16-inch water main, owned and operated by the Jurupa Community Services District, is located within Riverside County along the east side of Milliken Avenue, east of the site. As part of the development of the project site, the construction of new water systems shall be consistent with the City's approved Water Master Plan.

On-site residential and agricultural uses are served by private wells, as illustrated on *Exhibit 7, Aerial Photo*. Please note that the well sites depicted on this exhibit may not be all inclusive, and existing wells will be identified at the Tentative Map stage. Upon development of the well site areas, these wells, or any other wells found on the property, will be abandoned per California Department of Water Resources Health Guidelines and the City of Ontario Guidelines. In compliance with the Chino Basin Water Master's *Well Procedure for Developers*, a well use/destruction plan and schedule for all existing private/agricultural wells shall be submitted to the City of Ontario for approval prior to the issuance of permits for any construction activity. If a private well is actively used for water supply, the Developer shall submit a plan to abandon such well and connect users to the City's water system (residential to the domestic water system and agricultural to the recycled water system) when available. Wells shall be destroyed/abandoned per the California Water Resource Guidelines and require permitting from County Health Department. A copy of such permit shall be provided to the Engineering and Public Works Agency prior to issuance of grading and/or building permits. If the Developer proposes temporary use of an existing agricultural well for

purposes other than agriculture, such as grading, dust control, etc., the developer shall make a formal request to the City of Ontario for such use prior to issuance of permits for any construction activity. Upon approval, the Developer shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by the agreement.

The City of Ontario does not have recycled water facilities on or within the area of the Specific Plan. Construction of new recycled water facilities will be required of the developer as part of development of the project. Construction of these recycled water facilities will be consistent with the City's approved Recycled Water Master Plan.

3.9.2 Sewer

The City of Ontario does not have sewer facilities within the vicinity of the project site. Wastewater disposal within the Specific Plan area is currently provided through septic tanks and subsurface disposal fields. Prior to grading operations, existing septic tanks and subsurface disposal fields will need to be abandoned in accordance with Department of Health Services requirements. The City of Ontario's Sewer Master Plan provides for significant new facilities to serve the project site.

3.9.3 Stormwater/Drainage Facilities

There are no existing storm drain facilities located within the vicinity of the site. The existing storm drainage system surrounding the site generally consists of open earthen swales adjacent to area roadways. Storm water run-off from the site is generally by sheet flow. As part of the development of the project site, construction of new storm drain facilities will be required consistent with the City's approved Storm Drain Master Plan.

3.9.4 Solid Waste Disposal

There is no solid waste disposal in the vicinity of the project site.

3.9.5 Natural Gas Lines

Natural gas service in the vicinity of the site is provided by the Southern California Gas Company.

3.9.6 Electrical Facilities

Electrical service in the vicinity of the site is provided by Southern California Edison.



3.9.7 Communications Systems

Time Warner provides telephone service within the project area. Currently, telephone service is provided to the residences, dairies and farms in the area.

3.10 GEOLOGY AND SOILS

The City of Ontario General Plan identified the project site as containing Delhi series soils, as mapped by the United States Department of Agriculture, Soil Conservation Service in 1971 and 1980. Delhi series soils have been used for agriculture, primarily for grapes and citrus, since the 1800's.

3.11 Seismicity

The General Plan identified numerous earthquake faults within a 50-mile radius of the project site. Major mapped faults include, but are not limited to, the Chino, Whittier and North Elsinore, and Cucamonga Faults. For the "maximum probable earthquake" (MPE), defined as the 100-year event normally considered in the design of non-critical structures, the values range from about 0.13 to 0.20 g (i.e., the unit force of gravity). In the design of certain critical or important facilities such as hospitals and dams, the "maximum credible earthquake" (MCE) should yield an estimated peak horizontal acceleration in the range of 0.33 to 0.52 g.

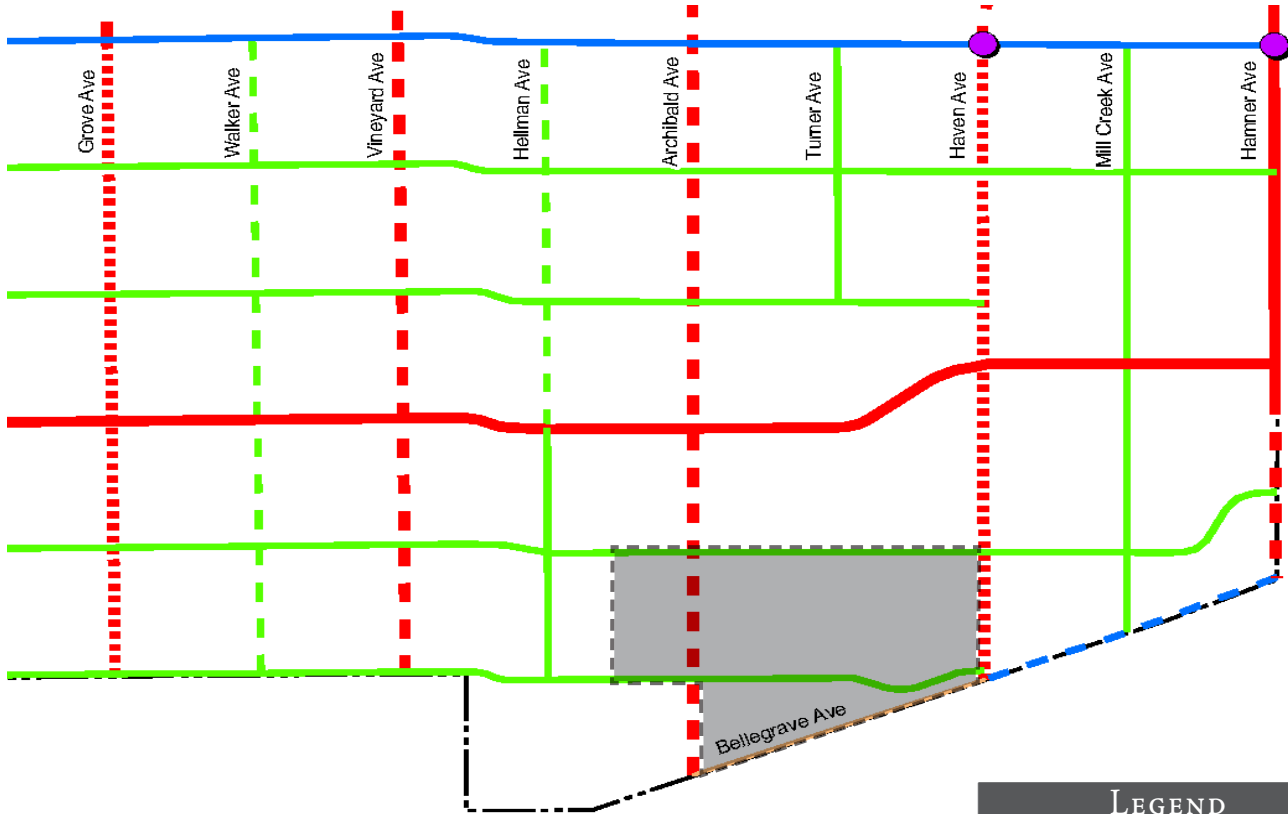
A zone of concentrated, relatively low-magnitude seismicity extends to the southwest from the San Jacinto fault zone (Rialto-Colton branch) along an inferred fault near Fontana. Where the inferred fault (Fontana trend) stops, this zone of micro-seismicity continues in a southwesterly to westerly direction terminating in the New Model Colony area. It is expected that the MPE for this fault structure could produce horizontal accelerations in the range of 0.3 to 0.5 g. More distant faults are capable of larger earthquakes with a higher probability of occurrence. The San Andreas fault is expected to generate a MCE event every 150 to 200 years, yielding a peak horizontal ground acceleration of approximately 0.21 to 0.26 g. The Uniform Building Code (UBC), establishes procedures designed to ensure that all development occurs in a safe manner relative to those known hazards.

3.12 Vegetation

The project site has been extensively used for agricultural operations, including both dairy use and the cultivation of row crops. Those areas not in active agricultural production are occupied by rural residential housing, farm buildings, and other ancillary facilities. The natural vegetation and soils conditions that once occurred throughout the site have been significantly altered through human uses.

3.13 Biological Resources

The Program EIR prepared for the City's NMC General Plan Amendment states that the Ontario NMC General Plan area has been greatly altered from natural conditions, under the influence of intensive agriculture and dairy industry.



LEGEND

Specific Plan area

8 Lanes

6 Lanes

4 Lanes

Minor Arterial

6 Lanes

4 Lanes

Collector Street

4 Lanes

2 Lanes



EXHIBIT 8—POLICY PLAN FUNCTIONAL ROADWAY CLASSIFICATION PLAN



4. Land Use





4.0 Land Use

4.1 INTRODUCTION

The 539.7 gross acre Specific Plan is envisioned to provide a neighborhood lifestyle featuring a variety of residential housing types designed within easy walking distance to the school, recreational amenities, and commercial retail uses. Connectivity within the Specific Plan is provided through a system of pedestrian and bicycle trails linking residential neighborhoods to one another, to parks, to school, and to the commercial land use area. A component of the trail system will be provided through the improvement of a portion of the SCE owned property (SCE Corridor) within the Specific Plan. These improvements represent a part of the City's Master Planned multi-purpose trail system planned for the New Model Colony.

Residential development is designed to address a variety of income levels and lifestyles, such as singles, families, executives, and "empty nesters." Housing types will include single-family detached homes ranging from cluster homes and lane loaded homes with significant architectural street and "green" frontage and garages facing courts or lanes, to conventional single-family detached cottage homes with lot sizes from approximately 3,015 square feet, and scaling up to larger single-family detached homes on lot sizes varying between 5,000 and 6,300 square feet.

A commercial land use is provided for in the Specific Plan, in Planning Area 2. The Specific Plan allows for the development of approximately 87,000 square feet of neighborhood commercial, retail, and service uses in Planning Area 2 conveniently located adjacent to Archibald Ave. and Eucalyptus Ave. on approximately 12.1 net acres.

The Land Use diagram shown in *Exhibit 10, Land Use Plan* illustrates the overall land use pattern within Subarea 29. Table 1 documents statistical totals for the entire Subarea, Table 1a provides residential allocations for Subarea 29 by ownership, and Table 1b provides the Specific Plan Land Use Summary. *Refer to pages 4-3 through 4-5, Tables 1-1b.*

4.2 RESIDENTIAL USE

Residential land uses within the Subarea 29 Specific Plan comprise approximately 494.2 gross acres. The Specific Plan will permit the development of up to 2,418 single-family detached residential dwelling units. Residential land use areas are contained within 26 distinctive neighborhoods, linked by a network of sidewalks and on-street bicycle paths connecting all the neighborhoods to the central neighborhood parks, the elementary school, and a private Community Recreation Center (Planning Area 15).

4.2.1 Variety of Housing Types

A total of five (5) housing types or product lines are included in the Specific Plan; these single-family detached housing types proposed for Park Place include:

- ♦ Cluster Homes: minimum 1,750-square foot exclusive use area per home;
- ♦ Conventional Small Lot Homes: minimum 3,015-square foot lots;
- ♦ Lane Loaded Homes: minimum 3,100-square foot lots;
- ♦ Conventional Medium Lot Homes: minimum 4,000-square foot lots; and
- ♦ Conventional Large Lot Homes: minimum 5,000-square foot lots.

4.2.2 Traditional Neighborhood Design

The community plan for Park Place offers a strong neighborhood character and identity for residents and visitors through a traditional approach to street design, architecture, and landscape design elements to reflect a similar character to that of older traditional Southern California neighborhoods, including Ontario. The design features described below "activate" the streetscene and promote friendly interaction between neighbors.

- ♦ A traditional grid and geometric street design in residential neighborhoods, with sidewalks separated by landscaped parkways, which provide visual interest, slower traffic, lower traffic volumes, and enhance a pedestrian orientation for neighborhoods. Sidewalks separated by a landscaped parkway promote pedestrian mobility, beautify the streetscene, and lend a pedestrian scale.
- ♦ The primary entry street into the residential area (Merrill Ave.) shall include on-street parking for park and recreation uses to the north and south, and a "linear" landscape treatment on both sides of the street. Parkway adjacent to both sides of the street are planned to be heavily landscaped. This street leads directly to the central neighborhood parks (Planning Areas 11 and 14) and the private Recreation Center (Planning Area 15), the focal point designed to reinforce the sense of arrival and establish a sense of community.
- ♦ The architecture of residences within Park Place will be designed to focus on human scale details which will enhance the pedestrian friendly character of the community. These features may include the use of front porches, railings, enhanced entries, a mix of materials and textures, and authentic detailing on elements such as windows and doors, columns, balconies, and lighting.

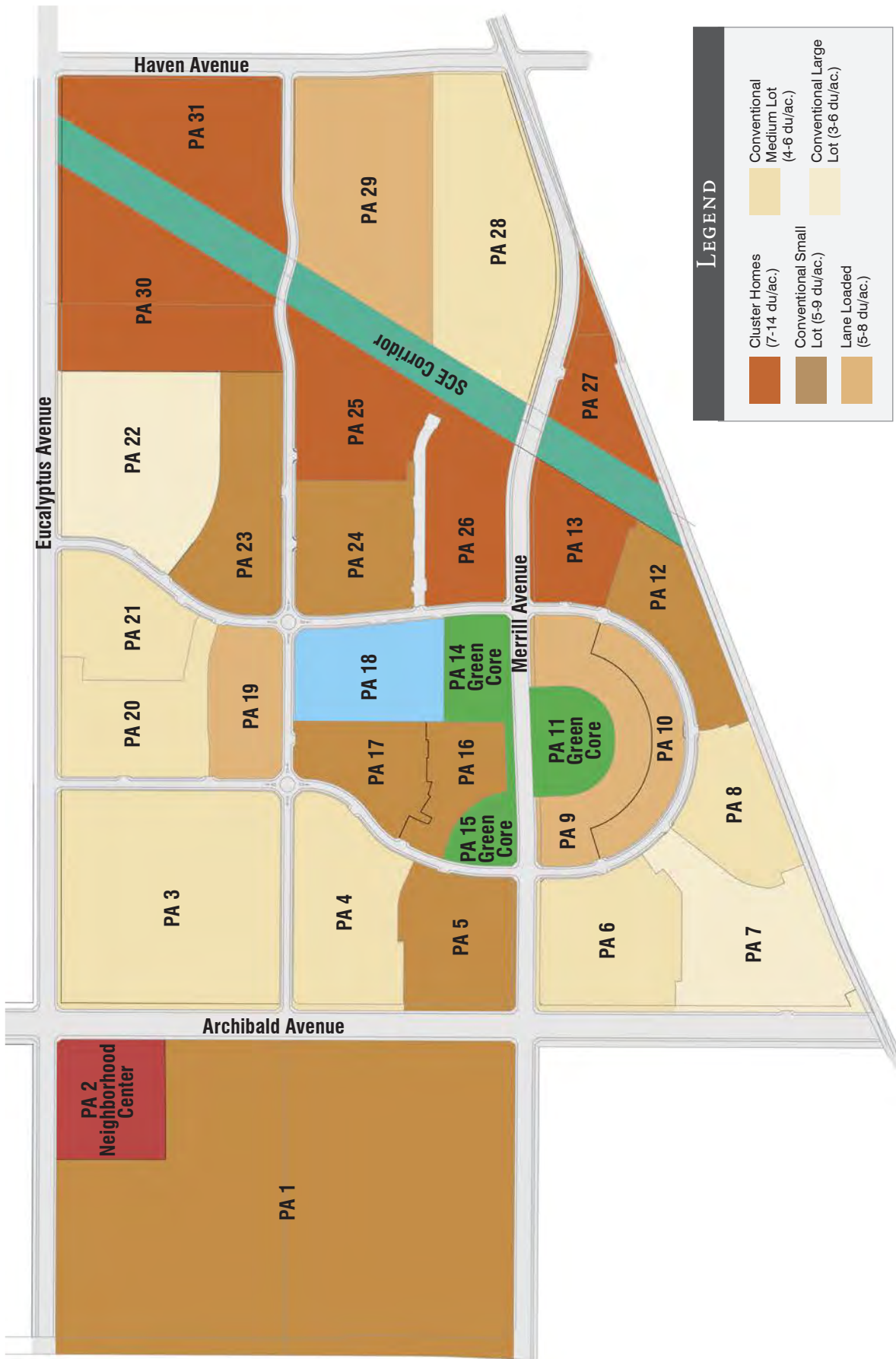


EXHIBIT 9—LAND USE PLAN

TABLE 3—SPECIFIC PLAN LAND USE SUMMARY

Planning Area	Land Use	Minimum Lot Size (S.F)	Planned Dwelling Units ^{3,4,5}	Net Acre ¹	Planned Net Density (Du/Ac.) ^{3,5}	Gross Acres ²	Planned Gross Density (Du/Ac.) ^{3,5}
1*	Conventional Small Lot	3,500	432	83.1	5.2	89.8	4.8
2*	Commercial	N / A	0	12.1	0.0	14.5	0.0
3*	Conventional Medium Lot	4,500	186	34.5	5.4	40.2	4.6
4**	Conventional Medium Lot	4,250	88	10.1	8.7	17.8	4.9
5**	Conventional Small Lot	3,825	68	7.2	9.4	13.7	5.0
6**	Conventional Medium Lot	5,000	67	13.0	5.2	17.0	4.0
7**	Conventional Large Lot	6,300	65	15.3	4.2	18.3	3.6
8**	Conventional Medium Lot	4,250	46	9.1	5.1	9.6	4.8
9**	Lane Loaded	3,150	69	9.9	7.0	11.9	5.8
10**	Lane Loaded	3,600	57	6.6	8.7	7.8	7.3
11**	Neighborhood Park 2	N / A	0	5.7	0.0	6.2	0.0
12**	Conventional Small Lot	3,825	53	9.5	5.6	9.5	5.6
13**	Cluster Homes	2,100 ^A	75	7.8	9.6	7.8	9.6
14**	Neighborhood Park 1	N / A	0	6.3	0.0	7.7	0.0
15**	Recreation Center	N / A	0	2.7	0.0	3.1	0.0
16**	Conventional Small Lot	3,015	41	5.9	7.0	6.1	6.8
17**	Conventional Small Lot	3,015	56	5.3	10.6	8.4	6.7
18**	School	N / A	0	10.0	N / A	11.2	N / A
19**	Lane Loaded	3,150	61	7.8	7.9	9.0	6.8
20**	Conventional Medium Lot	4,250	67	11.8	5.7	13.3	5.0
21**	Conventional Medium Lot	5,000	48	10.1	4.8	11.5	4.2
22**	Conventional Large Lot	6,300	79	19.7	4.0	21.3	3.7
23**	Conventional Small Lot	3,825	82	12.9	6.3	14.4	5.7
24**	Conventional Small Lot	3,400	75	8.1	9.3	12.8	5.8
25**	Cluster Homes	2,100 ^A	102	8.6	11.8	12.9	7.9
26**	Cluster Homes	2,100 ^A	102	8.7	11.7	13.2	7.7
27**	Cluster Homes	1,750 ^A 2,100	7347	7.6	9.7 6.2	7.6	9.6 6.2
28*	Conventional Medium Lot	4,050	121	23.0	5.3	25.8	4.7
29***	Lane Loaded or Conventional Medium Lot	3,150 or 4,000	108	21.4	5.0	27.2	4.0
30*	Conventional Large Lot	5,040	110	21.9	5.0	28.3	3.9
31*	Conventional Medium Lot	4,050	87	16.0	5.4	23.1	3.8
Flood Control Channel	Flood Control Channel	N / A	0	7.2	0.0	7.2	0.0
Pump Station	Pump Station	N / A	0	0.2	0.0	0.4	0.0
SCE Corridor	Park Place SCE Easement	N / A	0	11.2	0.0	11.2	0.0
Sub Area 29 Total			2,4182,392	449.9	5.4 5.3	539.7	4.5 4.4

- A) Minimum square footage identified is for exclusive use area on a per home basis, recorded lot size may differ.
- 1) Gross Acres calculated to centerlines of Master Planned streets minus SCE easements.
- 2) Net Acres excludes street rights-of-way and SCE easements.
- 3) Actual total units and gross/net density and acreage will be dependent on final lotting.
- 4) Target unit count based on submitted Tentative "B" Maps
- 5) A density transfer of 15.0% may occur between Planning Areas.

- *) Indicates Planning Areas as controlled/owned by Richland Communities, Inc.
- ***) Indicates "Park Place" Planning Areas as controlled/owned by SL Ontario Development Company
- ****) Indicates Planning Areas as controlled/owned by Brookfield Homes



TABLE 3A—SUBAREA 29 SPECIFIC PLAN RESIDENTIAL ALLOCATIONS SUMMARY (BY OWNERSHIP)

<i>Description</i>	<i>Park Place</i>	<i>Richland</i>	<i>Brookfield Homes</i>	<i>Totals</i>
Total Gross Acres	290.7 ac	221.7 ac	27.2 ac	539.7 ac
SCE Easement	-(11.2 ac)	-(9.2 ac)	-(3.8 ac)	-(24.3 ac)
Elementary School (*)(**)	-(10.0 ac)	-0-	-0-	-(10.0 ac)
Neighborhood Center(**)	-0-	-(11.2 ac)	-0-	-(11.2 ac)
Residential Gross Acreage	269.5 ac	201.3 ac	23.4 ac	494.2 ac
Percent of Total Residential Acreage	54.5%	40.7%	4.7%	100%
Total Residential Lots	1,374 du	936 du	108 du	2,418 du
Planned Residential Gross Density (du/gross ac.)	5.1 du/gross ac.	4.6 du/gross ac.	4.6 du/gross ac.	4.9 du/gross ac.
Residential Gross Density permitted by TOP (5.0 du/gross ac.)	1,347.4 du	1,006.5 du	117.0 du	2,470.9 du

TABLE 3B—SUBAREA 29 SPECIFIC PLAN TOTAL UNITS RECORD

<i>Description</i>	<i>Dwelling Units</i>
Original Subarea 29 (2006)	1,937 du, 3.6 du/gross ac.
March 2008 Amendment	2,293 du, 4.2 du/gross ac.
Subarea 29 Amendment (2015)	2,392 du, 4.8 du/gross ac.
Subarea 29 Amendment (2021)	2,418 du, 4.9 du/gross ac.

(*) Only one (1) Elementary School, No Residential Allocation for Deleting,
 (**) Elementary School and Neighborhood Center are “Useable” (excluding Street, Right-of-Way, and Additional Neighborhood Edge)



- ♦ Innovative garage designs will be utilized in order to de-emphasize the visual impact of garage doors on the streetscene. Design techniques may include varied garage setback requirements, split-garages, turn-in garages, garages located on rear lanes (private alleys), garages located on motorcourts, or other similar techniques that de-emphasize the view of garage doors from the street.
- ♦ Lane Loaded Homes permitted in Planning Areas 9, 10, 19, and 29 will front directly on green spaces and local streets, framing these spaces with architecture.

4.3 PARKS, OPEN SPACE AND RECREATIONAL FACILITIES

The Park Place portion of the Specific Plan will have a central, “Green Core” that consists primarily of open space for recreation and passive use, linked to the elementary school. Exterior walkways and paseos lead to this centralized area. Several opportunities exist for community interaction and recreation within these parklands. The “Green Core” is comprised of several facilities and amenities to meet the needs of the community. The following areas make up the “Green Core”: two (2) neighborhood parks, the linear park system, a private Community Recreation Center, and the paseo trail system, which are all linked to the elementary school (Planning Area 18).

4.3.1 Parks

An integral part of the development of the residential program within the development boundary of the Specific Plan is park and recreational areas, providing active and passive recreational opportunities for residents of the project site. A total of 12.0 net acres of usable neighborhood park is included in Park Place. The Quimby Act for the City of Ontario, provides for the dedication of three (3) acres of parkland per 1,000 residents. However, pursuant to the GPA Goal 12.0, Objective 12.2, Policy 12.1.3, each specific plan is required to define a park acquisition and improvement financing plan to ensure the achievement of a park standard of five (5) acres of parkland per 1,000 residents through a combination of Neighborhood and pocket parks. Pursuant to the foregoing policy, the total parkland requirement for the project is 45.5 gross acres. However, at the City of Ontario election, it was determined that the developers of Subarea 29 may pay park area fees in lieu of park dedication. Neighborhood parks shall be dedicated to the City and pocket parks shall be transferred to a homeowners association. Refer to Chapter 7.6, Landscape Design, included in the overall Residential Design Guidelines for the project, for park plans for Park Place, including the overall core plan and detailed site plans of the two (2) neighborhood parks and the private Community Recreation Center.

4.3.2 Trails

Bicycle trails are an integral element in creating accessibility and mobility within the Specific Plan. An off-street Class I bicycle trail system will be provided within the right-of-way of Haven Ave. (west side of street) adjacent to the project site as part of the ultimate development of the Specific Plan area. An off-street Class I bicycle trail system will also be provided within the right-of-way of Archibald Ave. (west side of street) adjacent to the development area between Merrill Ave. and Eucalyptus Ave. Adequate right-of-way will be provided for on-street bicycle routes with the improvement of Archibald Ave. adjacent to the development area south of Eucalyptus Ave., Eucalyptus Ave. adjacent to the development area, and Merrill Ave. serving the central portion of the community. The bicycle trail system will link residential neighborhoods to the planned parks and the elementary school site located within the project site, as well as providing connectivity to future development areas within the Specific Plan area and to the proposed school site located adjacent to the project site at the southeast corner of Haven Ave. and Eucalyptus Ave.

Refer to *Exhibit 11, Master Circulation Plan*, for the location of Class 1 bike paths.

4.4 SCE CORRIDOR TRAIL

A link within the City’s Master Plan of Trails proposed for SCE easements and corridors includes a portion of the trail which will be developed in the Specific Plan, extending a multi-purpose bicycle trail from Eucalyptus Ave. south to County Line Channel Trail. Access to the SCE Corridor Trail will be provided at key points within residential areas to provide pedestrian and bicycle accessibility. A conceptual plan for the SCE Corridor Trail is illustrated in *Exhibits 13, Pedestrian and Bicycle Circulation Plan, and Exhibit 29, SCE Easement Paseo System*, in Chapter 7, Design Guidelines.

4.5 COMMERCIAL / Neighborhood Center

The Specific Plan includes approximately 12.1-net acres (11.2 usable acres) designated for development of commercial uses as a neighborhood center in Planning Area 2. Approximately 87,000 square feet of neighborhood center commercial land uses are planned adjacent to Archibald and Eucalyptus Avenues. Commercial development at this location is conveniently located to serve Subarea 29. Pedestrian and bicycle connectivity between residential land use areas within the Specific Plan area and the future commercial center will be provided through an extensive network of trails. Commercial uses which could be developed within this land use district include local servicing commercial uses, such as small restaurants, small retail shops, and personal services.



4.6 SCHOOLS

The Specific Plan is located within the Mountain View School District, which serves the school needs for grades K-8 and the Chaffey Joint Union School District, which serves the school needs for grades 9-12.

One elementary school (Planning Area 18) is included within Park Place and Subarea 29 Specific Plan. A 10.0 net acre (10-acre usable) K-5 elementary school is located in the heart of Park Place, adjacent to the northern neighborhood park, north of Merrill Ave. Joint use school/park facilities are envisioned, serving the K-5 school needs generated by the project. A middle school of approximately 20 acres is conceptually planned east of the Specific Plan area, east of Haven Ave. and south of Eucalyptus Ave. The developers of Park Place and other developers in Subarea 29 will be required to pay school fees or construct school facilities, as required by the State of California.

Final approval of the elementary school site rests with the State of California Department of Education. It shall be the responsibility of the school district to seek such approval and to comply with all applicable regulations and procedures. Should the Mountain View School District find it necessary to build the elementary school or another public school in a location other than Planning Area 18, the placement of a school at another location within Subarea 29 Specific Plan shall be considered consistent with the provisions of Specific Plan.

In the event that the Mountain View School District elects to construct the elementary school at an alternate location outside the Subarea 29 Specific Plan, the designated school site (Planning Area 18) will be developed with residential uses. Should this site be developed with residential uses, in lieu of the school, it will be developed as Low Density Residential within a density of 4.6 dwelling units per acre and with a development target of 46 dwelling units; and the total number of dwelling units within the specific plan will be increased to 2,438 dwelling units with the approval of a Specific Plan Amendment (SPA).

In order to develop Planning Area 18 with residential uses in lieu of the elementary school, the developer shall be required to obtain written confirmation from the Mountain View School District that it will not utilize the planning area for development of an elementary or other school. The written confirmation will be submitted to the City of Ontario Planning Department concurrently with any development application for Planning Area 18, which would require an amendment to the Specific Plan.

4.7 DAIRY FARMING

The Specific Plan Area contains existing dairies and feed lots. A minimum 100-foot separation shall be required between a new residential, commercial, or industrial development, or structures used for public assembly and an existing animal feed trough, corral/pen, or an existing dairy/feed lot including manure stockpiles and related wastewater detention basins. The 100-foot separation requirement may be satisfied by an off-site easement acceptable to the Planning Director with adjacent properties, submitted with the initial final map and recorded prior to or concurrent with the final map.

4.8 AIRPORT LAND USE COMPATIBILITY PLANNING (ALUCP)

4.8.1 ONTARIO AIRPORT (ONT) REQUIREMENTS

The ALUCP for ONT was adopted in April of 2011 and the project site is located outside noise and safety impact zones but is located within the Real Estate Transaction Disclosure zone due to overflight and does not place development restrictions on the property but does require disclosure to new property owners. The Business and Professions Code Section 11010 and Civil Code Sections 1102.6, 1103.4, and 1353 require that, as part of many residential real estate transactions, information be disclosed regarding whether the property is situated within an Airport Influence Area (AIA). The Business and Professions Code applies the disclosure requirement to the sale or lease of newly subdivided lands and condominium conversions and to the sale of certain existing residential property.

The Civil Code applies the disclosure requirement to existing residential property transfers only when certain natural conditions (earthquake, fire, or flood hazards) warrant disclosure. State Law provides the following disclosure language:

NOTICE OF AIRPORT IN VICINITY: This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.



EXHIBIT 10—AIRPORT INFLUENCE AREAS



4.8.2 CHINO AIRPORT REQUIREMENTS

The most recent ALUCP for Chino Airport was completed by the County of Riverside Airport Land Use Commission in 2008 and is being used by the City as a guide for development around Chino Airport. The project site is partially located within Zones C, D and E.

- ✦ The portions located within Zone C are considered existing land uses.
- ✦ Residential land uses within Chino Airport Compatibility Zone D are required to have a Recorded Overflight Notification appearing on the Property Deed and Title incorporating the following language:

(NOTICE OF AIRPORT IN VICINITY: This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.)

- ✦ Zone D requires that 10% of the area be set aside as Open Land. Open Land must have a minimum dimensions of (75 feet by 300 feet). Open Land area must be free most structures and other major obstacles such as walls, large trees or poles (greater than 4 inches in diameter, measured 4 feet above the ground), and overhead wires. Small trees and shrubs that exceed 4 feet in height and/or thickness of 4 inches may be allowed along the edge of Open Land areas where the area abuts a wall or similar feature, provided that they are planted within 4 feet of the wall. Roads and automobile parking lots are acceptable as Open Land areas if they meet the above criteria.
- ✦ Zone D requires residential densities to be at or above 5.0 dwelling units per acre or have an average parcel size of less than 0.2 gross acres (8,712 SF).
- ✦ Zone D limits building/structure heights to 70 feet. Any permanent or temporary object greater than 70 feet in height are required to file a FAA Form 7460-1 and receive a determination of No Hazard from the FAA prior to project approval. During construction or grading of the site construction equipment such as cranes that exceed 70 feet will also be required to file a FAA Form 7460-1 and receive a No Hazard determination.
- ✦ Zone E and does not place any land use restrictions on the property but due to flight patterns the height of any structure cannot exceed 100 feet without FAA approval. If a structure exceeds 100 feet in height or has the potential to be a hazard to flight due to visual glare or electronic interference the project applicant is required to notify the FAA by electronically filing a 7460-1 Form.



5. Infrastructure and Services





5.0 Infrastructure and Services

The infrastructure, utilities, and public services to be provided to support development of the Subarea 29 Specific Plan are discussed in this section.

5.1 CIRCULATION

The circulation plan for the Specific Plan area will provide efficient movement of vehicular traffic throughout the project as well as an environment for pedestrian circulation and bicycle traffic, reducing the reliance on the automobile as a means of travel. The plan reinforces the concept of traditional neighborhood design with a grid street pattern and traffic calming measures. The *Master Circulation Plan, Exhibit 11* illustrates the roadway system planned for the Specific Plan area, as well as new backbone street traffic signals.

The project site will be served by a system of new arterial roadways to be improved per the requirements of the City Council Approved 2010 The Ontario Plan (TOP), General Plan, the 2012 Master Plan of Streets and Highways, and through proposed new local streets to be constructed as part of project development. Improvements will be constructed to Archibald Ave., Haven Ave., and Eucalyptus Ave. adjacent to the site and elsewhere to accommodate traffic increases as specified in the traffic impact analysis and as required by the City. Proposed Merrill Ave. will be fully developed through the site to provide access to and from the project.

Within the development area, local collector roadways and local interior streets will provide for efficient movement of vehicles and pedestrians through the project. The developer will be required to submit a parking analysis to the City of Ontario to ensure the proposal is in conformance with the City's Parking Code for residential developments.

5.1.1 Streets

The developer(s) of the Subarea 29 Specific Plan area will be responsible for all offsite improvements for the entire project frontage or as otherwise indicated. Phasing of the improvements will be implemented as required by the City Engineer and pursuant to the mitigation measures identified in the EIR and/or the Conditions of Approval established on the approved tentative maps for the project. Additionally, bus turnouts within the Specific Plan area may be required to be constructed to the satisfaction of the City of Ontario and Omnitrans.

The minimum design speeds to be used for centerline curve radii, superelevation, corner sight distance, vertical and horizontal alignment, and sight distance, etc. are listed:

Eucalyptus Ave.	45 m.p.h.
Merrill Ave.	45 m.p.h.
Haven Avenue	45 m.p.h.
Archibald Ave.	50 m.p.h.

The traffic study prepared for the Subarea 29 Specific Plan will verify the need for additional rights-of-way at critical intersections in order to accommodate additional left and right turn lanes.

On-street parking is prohibited on all master planned roadways, except that section of Merrill Ave. along the neighborhood parks.

Any walls and/or fencing exceeding 3 feet in height proposed within the 10-foot street (interior street) side yard setback area shall be setback a minimum of 5-feet from side yard property line. The 5-foot setback area shall be landscaped and maintained by the home owner and/or HOA.

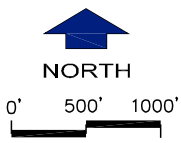
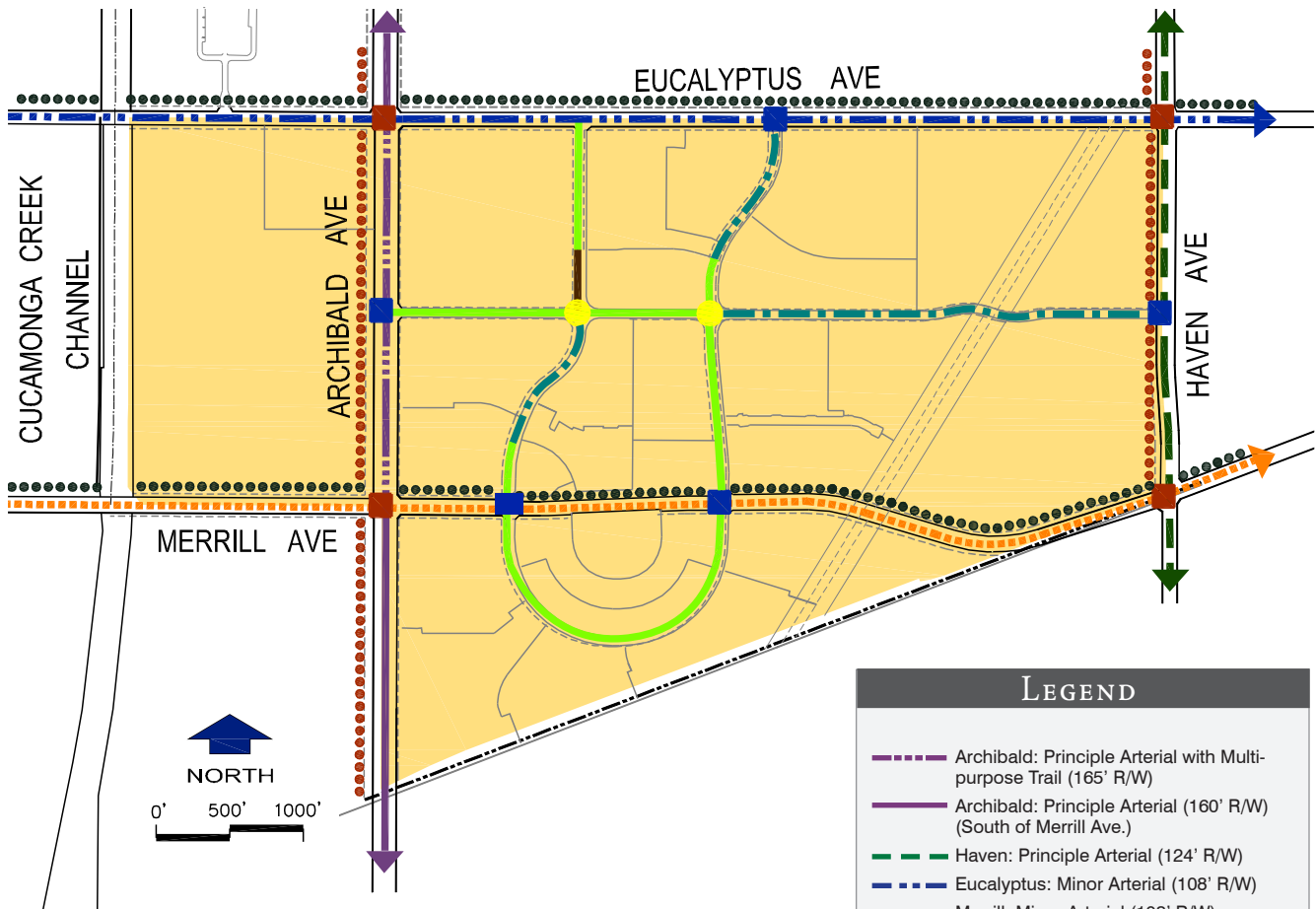
Archibald Ave.

Per the City's 2012 Master Plan of Streets and Highways, Archibald Ave. is designated as a Principal Arterial. The Street Sections for Archibald Ave. are per the approved Archibald Ave. Street Improvement Plans. Per this plan set, Archibald Ave. has a 165-foot right-of-way (north of Merrill Ave. to Edison Ave) and a 160-foot right-of-way (south of Merrill Ave. to the County Line). Archibald Ave. will provide north/south access along the western portion of the Specific Plan. Per The Ontario Plan (TOP) Figure M-3: Multipurpose Trails and Bikeway Corridor Plan, Archibald Ave. is to include an off-street multipurpose trail. Access to this street via driveways from residential units and on-street parking will be prohibited. Improvements to Archibald Ave. are illustrated on *Exhibit 12, Arterial Street Sections, Archibald Ave. and Archibald Ave. (East Side)*.

Merrill Ave. (Minor Arterial Street)

Merrill Ave. is designated as a Minor Arterial street with a 108-foot right-of-way and will provide east/west access through the Specific Plan. Access to this street via driveways from residential units and on-street parking will be prohibited (on-street parking along the Neighborhood Parks will be allowed). Improvements to Merrill Ave. are illustrated on *Exhibit 12, Arterial Street Sections Merrill Ave. and Merrill Ave. (North Side)*.

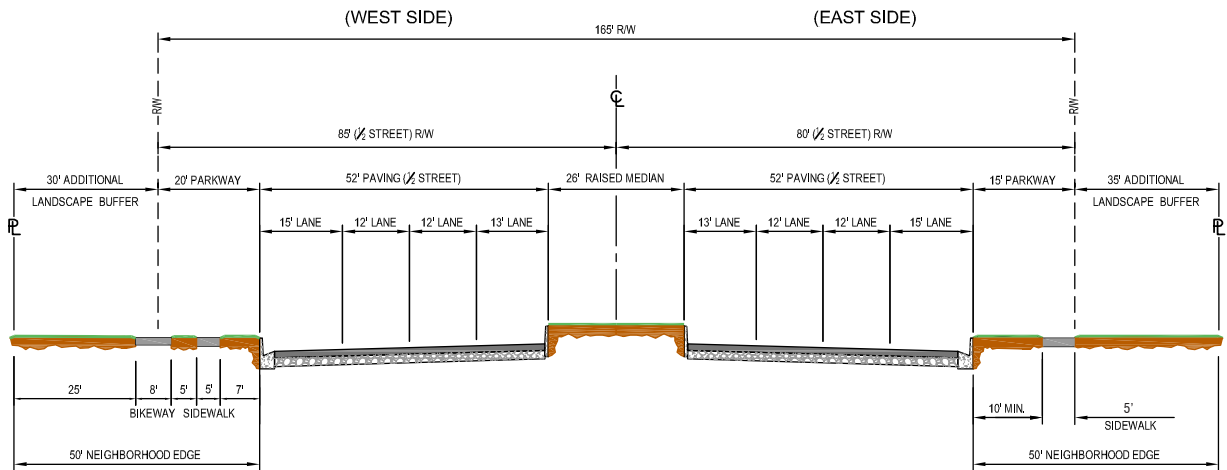
Per TOP Figure M-3: Multipurpose Trails and Bikeway Corridor Plan, Merrill Ave. is to include a class II (on-street striped and signed bike lanes) bicycle system and an off-street multipurpose trail.



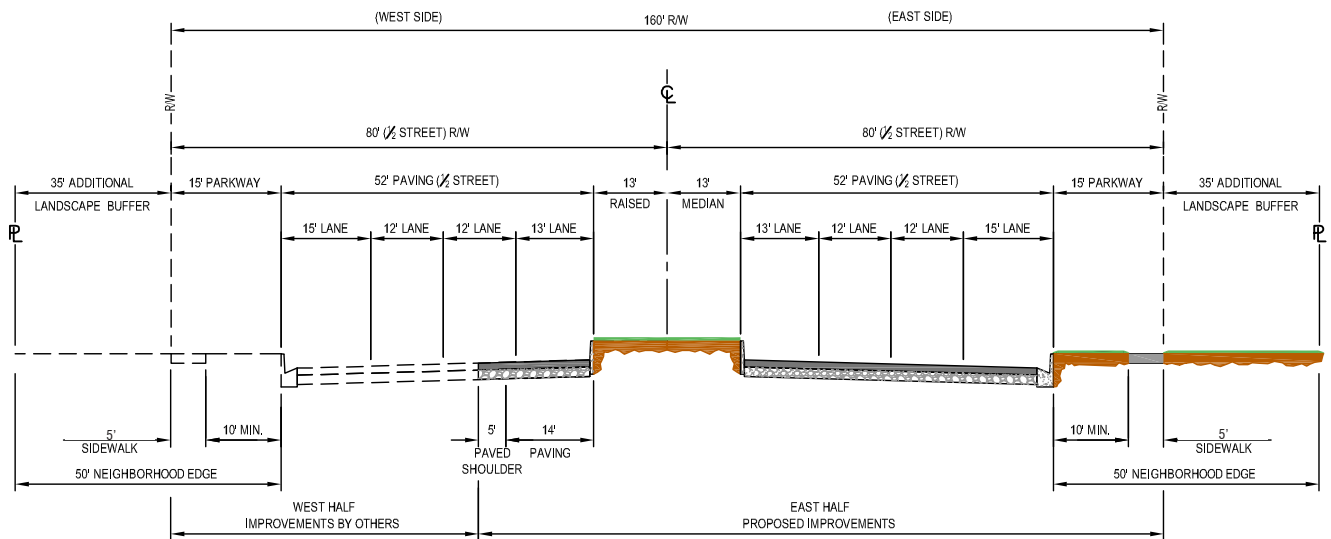
LEGEND	
	Archibald: Principle Arterial with Multi-purpose Trail (165' R/W)
	Archibald: Principle Arterial (160' R/W) (South of Merrill Ave.)
	Haven: Principle Arterial (124' R/W)
	Eucalyptus: Minor Arterial (108' R/W)
	Merrill: Minor Arterial (108' R/W)
	Primary Local Street (60' R/W With 14' Total Additional Neighborhood Edge)
	Primary Local Street (60' R/W With 7' Additional Neighborhood Edge)
	Primary Local Street (60' R/W)
	Multipurpose Trail (Within Parkway)
	Class 2 and Multipurpose Trail
	Master Planned Traffic Signal
	Proposed Traffic Signal
	Conceptual Roundabout Location
	Specific Plan area

EXHIBIT II— MASTER CIRCULATION PLAN

SPECIFIC PLAN — OCTOBER 2006
 AMENDED APRIL 21, 2015

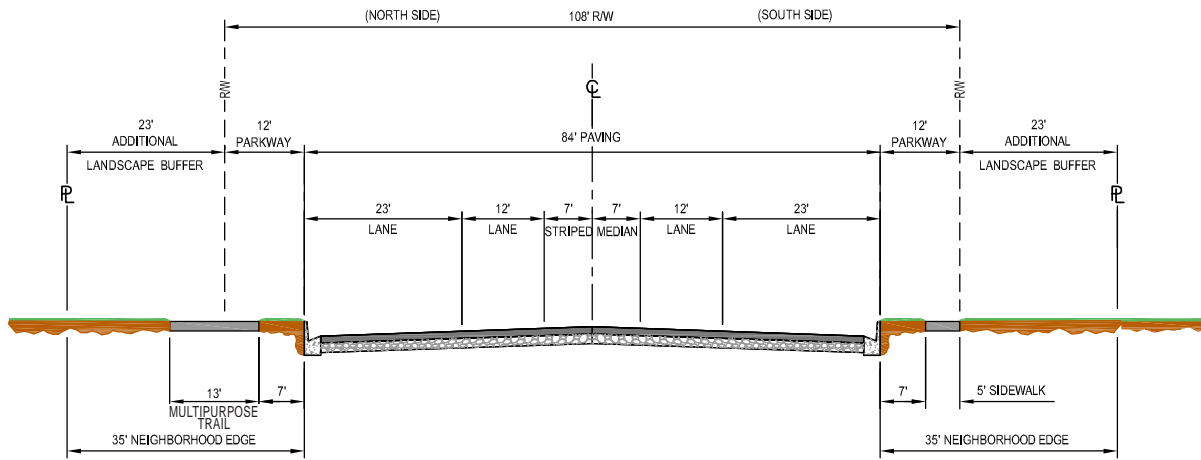


ARCHIBALD AVE.
 Divided Principal Arterial With Bikeway
 South of Eucalyptus Avenue to Merrill
 (On-Street Parking is Prohibited)
 N.T.S.



ARCHIBALD AVE. (EAST SIDE)
 Principal Arterial
 South of Merrill
 (On-Street Parking is Prohibited)
 N.T.S.

EXHIBIT 12— ARTERIAL STREET SECTIONS

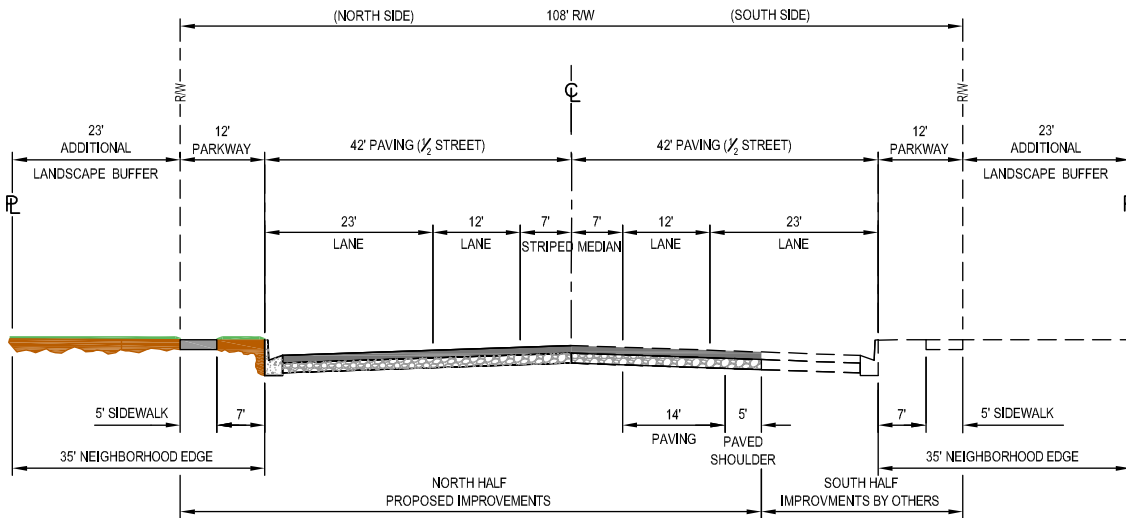


MERRILL AVE.

Collector

(On-Street Parking is Permitted along the Neighborhood Parks)

N.T.S.



MERRILL AVE. (NORTH SIDE)

Collector

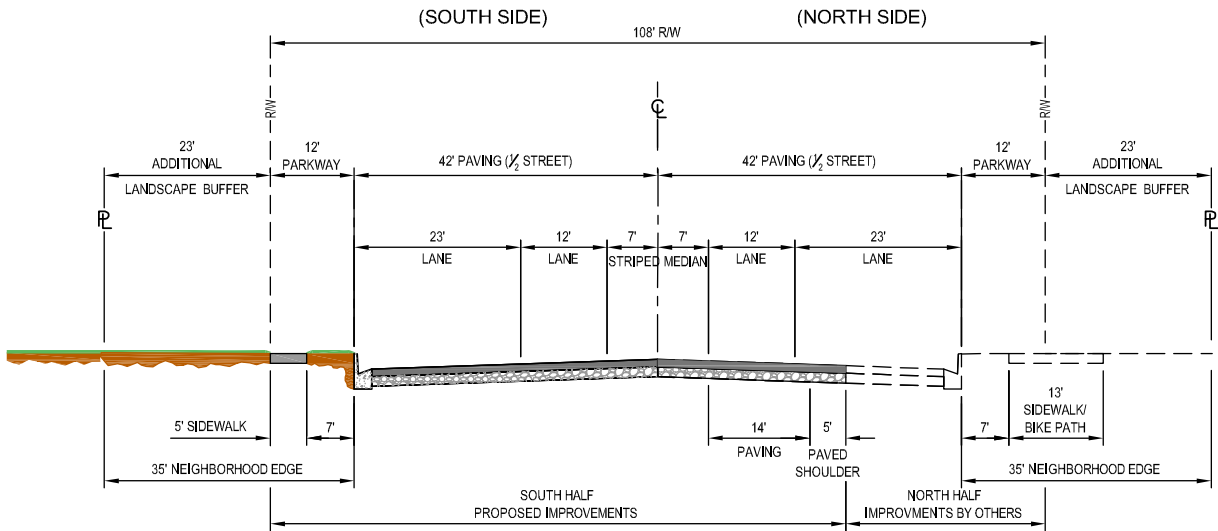
West of Archibald Avenue to the Cucamonga Creek Channel

(On-Street Parking is Prohibited)

N.T.S.

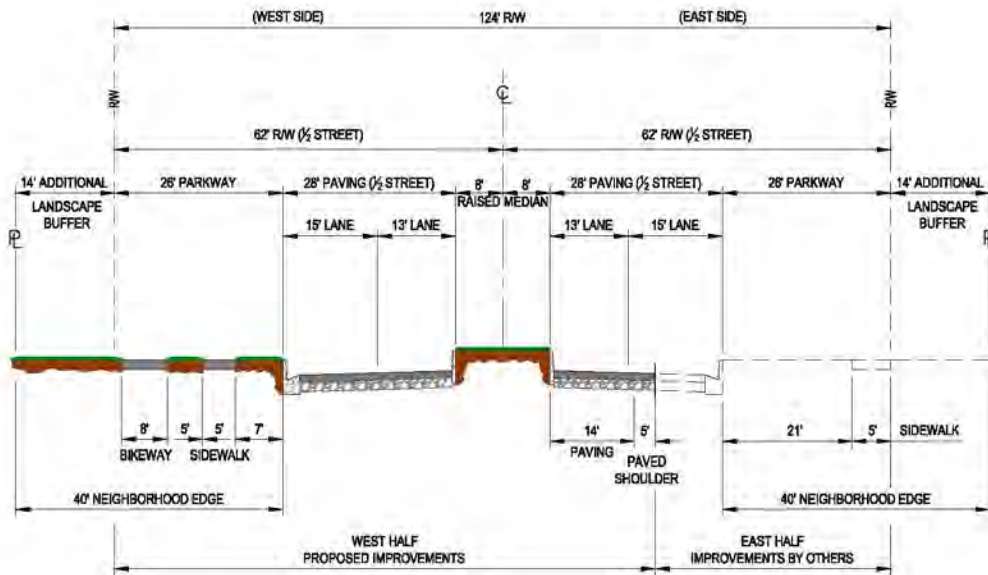
Note: "Multipurpose Trail" Indicates a Combined Bikeway / Sidewalk Use

EXHIBIT 12— ARTERIAL STREET SECTIONS



EUCALYPTUS AVE. (SOUTH SIDE)

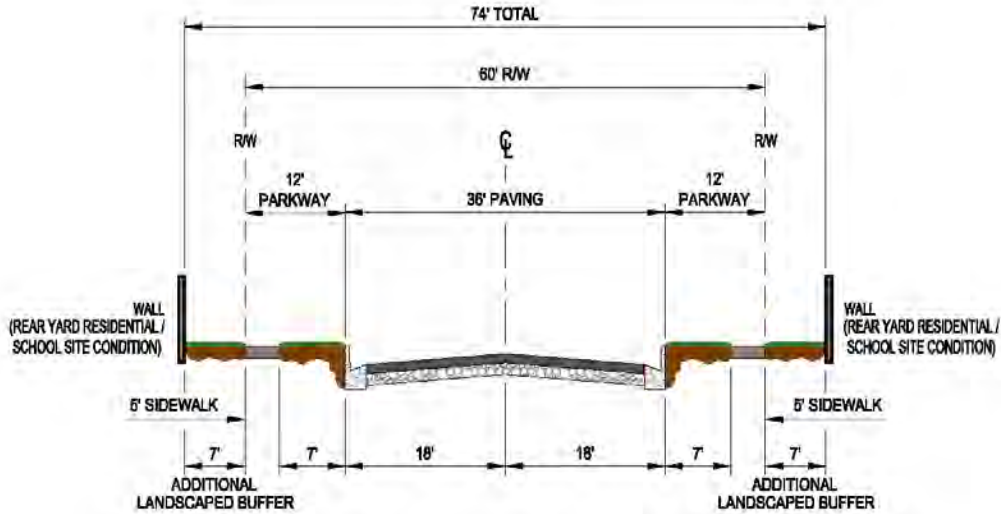
Collector
(On-Street Parking is Prohibited)
N.T.S.



HAVEN AVE. (WEST SIDE)

Principal Arterial
(On-Street Parking is Prohibited)
N.T.S.

EXHIBIT 12— ARTERIAL STREET SECTIONS

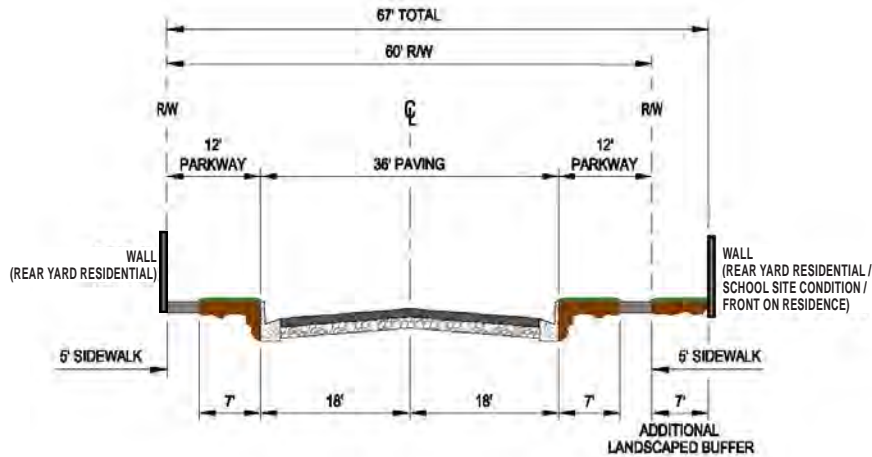


LOCAL STREET

(60' R/W with 14' Total Additional Landscaped Buffer)

On-Street Parking is Allowed

N.T.S.



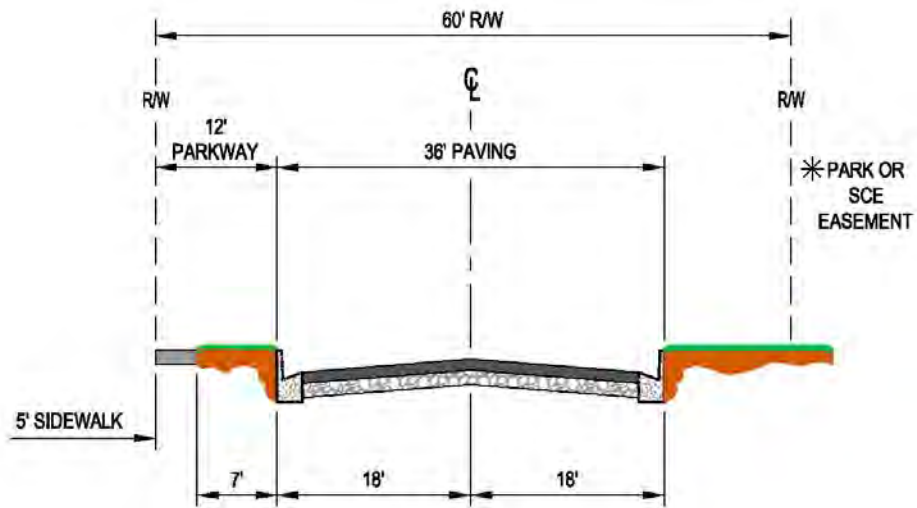
LOCAL STREET

(60' R/W with 7' Additional Landscaped Buffer)

On-Street Parking is Allowed

N.T.S.

EXHIBIT I2A— LOCAL STREET SECTIONS



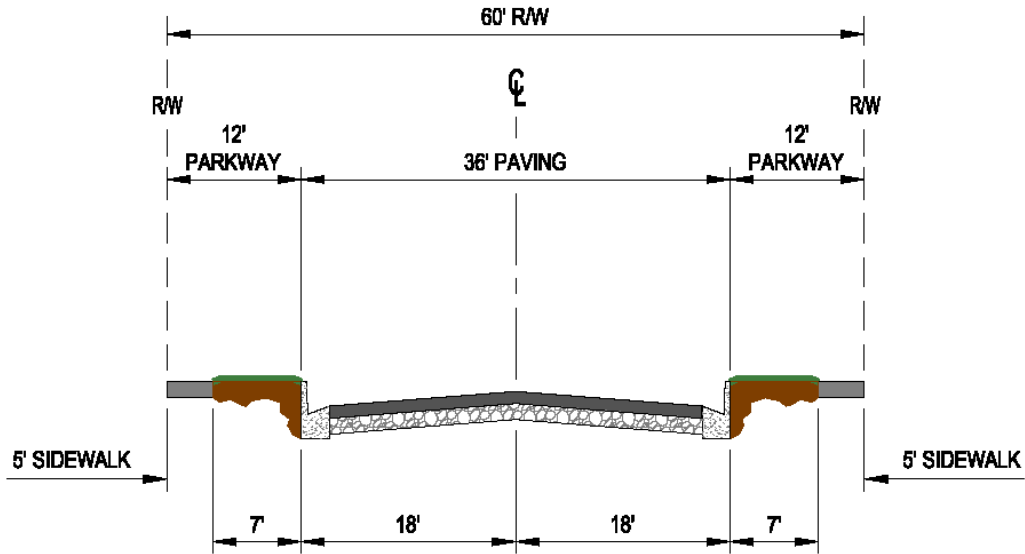
LOCAL STREET & CUL-DE-SAC STREET
ALONG PARK/SCE EASEMENT

(60' R/W)

On-Street Parking is Allowed

N.T.S.

*5' sidewalk/paseo to be placed within the park/SCE Easement
(Location and widths to be determined by the city)



LOCAL STREET & CUL-DE-SAC STREET

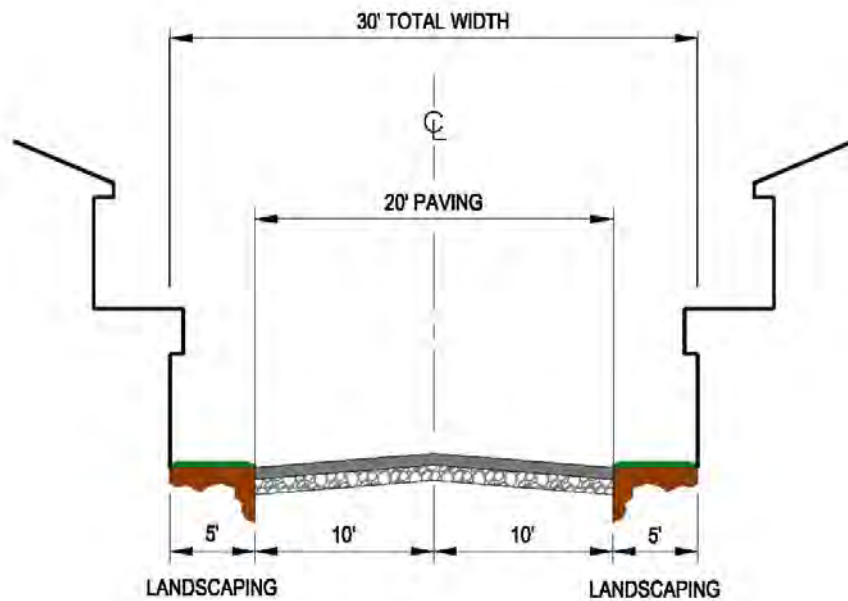
(60' R/W)

On-Street Parking is Allowed

N.T.S.

EXHIBIT I2A— LOCAL STREET SECTIONS

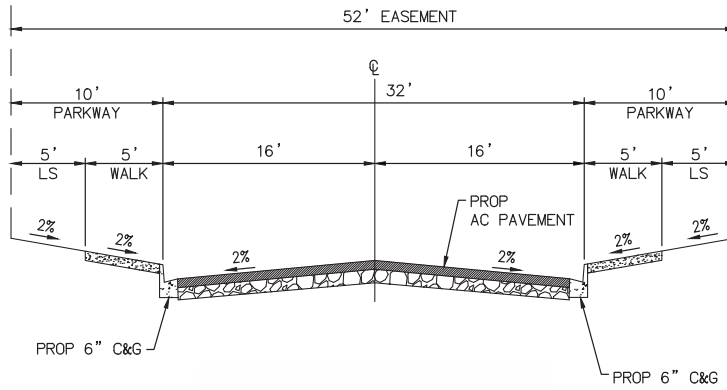
SPECIFIC PLAN — OCTOBER 2006
AMENDED APRIL 21, 2015



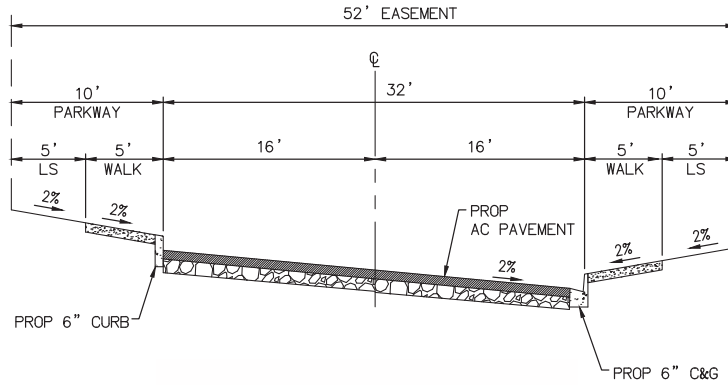
LANES (PRIVATE ALLEYS)
20' Paved Section/30' Width to Garage
N.T.S.

NOTE: Where emergency access lanes are to be used within the residential development, a 24-foot paved section with 3-foot Minimum.* Landscaping will be utilized (30-feet total)

*Measurement taken from inside of curb



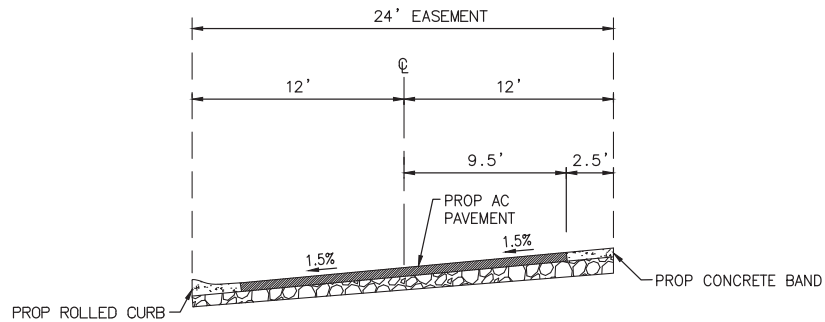
PRIVATE STREET A(PA-27 ONLY)
(52' Easement)
On-Street Parking is Allowed on One Side of Street
N.T.S.



PRIVATE STREET B (PA-27 ONLY)

(52' Easement)

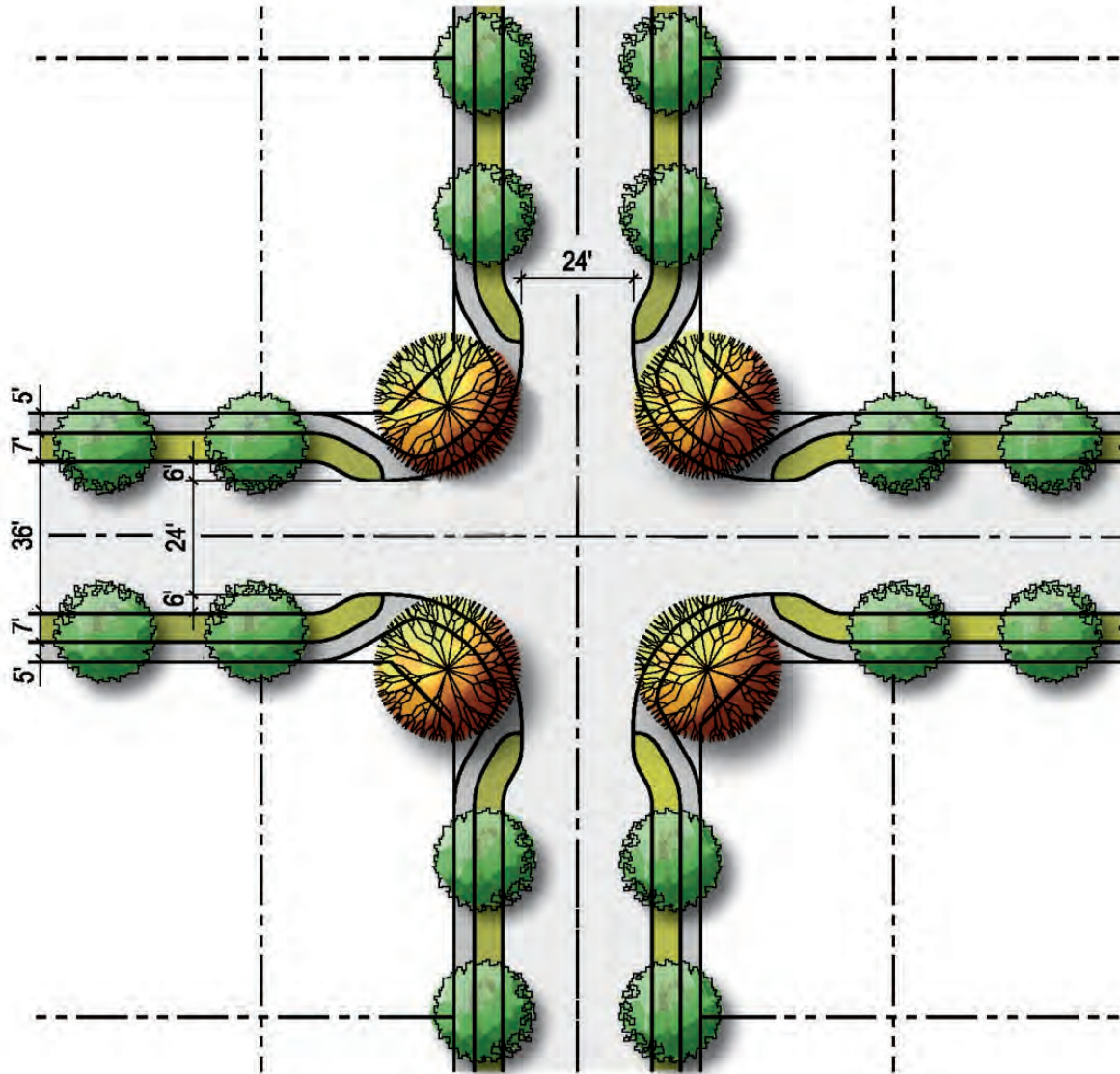
**On-Street Parking is Allowed on One Side of Street
N.T.S.**



PRIVATE ALLEY (PA-27 ONLY)

(24' Easement)

N.T.S.

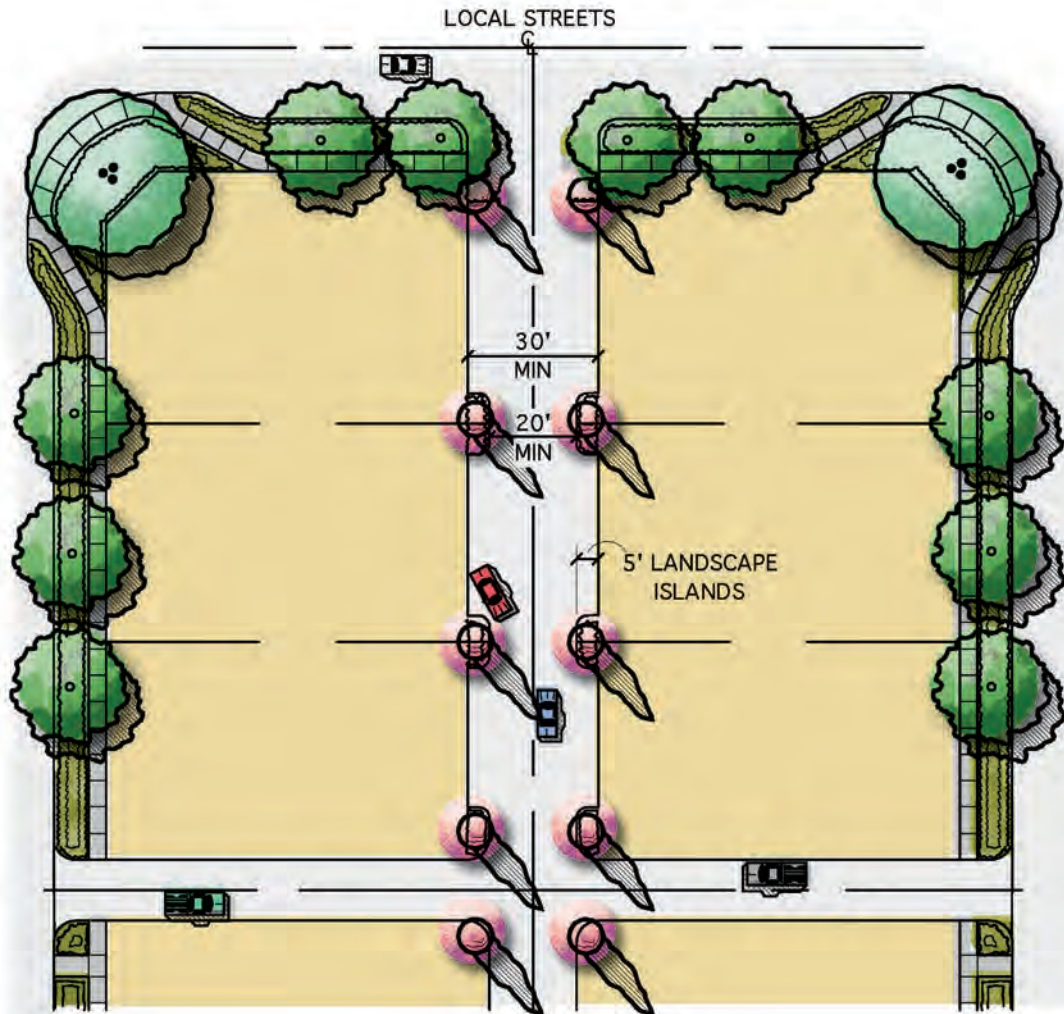


N.T.S.

Note: Refer to the City of Ontario Standard Drawing for Local Street Chokers.

EXHIBIT I2C—NEIGHBORHOOD STREET CHOKERS

SPECIFIC PLAN — OCTOBER 2006
AMENDED APRIL 21, 2015



N.T.S.

EXHIBIT I2D— LANE (PRIVATE ALLEY)



Eucalyptus Ave. (Minor Arterial Street)

The Minor Arterial roadway, Eucalyptus Ave., abuts the project site on the north. Eucalyptus Ave. will be improved as a half-street along the project's northern boundary (from the Cucamonga Creek Channel to Haven Ave.). Improvements to be constructed to Eucalyptus Ave. will include the south half-street and parkway improvements of the 108-foot right-of-way, as well as the additional neighborhood edge landscaped buffer. These improvements include 42 feet of pavement, including two (2) lanes for eastbound traffic, a 12-foot landscaped parkway, which includes a 5-foot sidewalk separated from the street by a 7-foot landscaped buffer; as well as 23 feet of additional landscaped buffer (35-foot total neighborhood edge), provided from the street right-of-way extending to the property line. Additionally, north side improvements to Eucalyptus Ave. include a 7-foot painted median, a 14-foot paved lane and 5 feet of graded shoulder. *Exhibit 12, Eucalyptus Ave. (South Side)*, illustrates the half-street improvements for Eucalyptus Ave. The Project Level EIR will determine the need for constructing additional lanes for westbound traffic.

Per TOP Figure M-3: Multipurpose Trails and Bikeway Corridor Plan, Eucalyptus Ave. is to include a class II (on-street striped and signed bike lanes) bicycle system and an off-street multipurpose trail.

Haven Ave.

Haven Ave. is designated as a Principle Arterial with 124-foot right-of-way. Haven Ave and will provide north/south access along the eastern boundary of the Specific Plan. Access to this street via driveways from residential units and on-street parking will be prohibited. Improvements to Haven Ave. are illustrated on *Exhibit 12, Arterial Street Sections, Haven Ave. (West Side)*.

Refer to *Exhibit 13, Pedestrian and Bicycle Circulation Plan*, which illustrates all pedestrian and multipurpose trails in Park Place.

Primary Local Streets

Primary Local Streets include a 60-foot right-of-way and provide for circulation between the varying Planning Areas within the Specific Plan. Refer to *Exhibit 12a, Local Street Sections, Primary Local/Interior Street and Cul-De-Sac Street*, which illustrates these improvements. Varying additional landscaped buffers are provided along the Primary Street where specific conditions apply, as detailed below. Development conditions, where walls are proposed on either side of the roadway will include seven (7) feet of additional landscaped buffers between the rights-of-way and the proposed walls. Refer to *Exhibit 12a, Local Street Sections, Primary Local Street (60' R/W with 14' Total Additional Landscaped Buffer)*, which illustrates this condition. Development conditions where a wall is proposed on one (1) side of the roadway will

include seven (7) feet of additional landscaped buffers between the right-of-way and the proposed wall. *Exhibit 12a, Local Street Sections: Primary Local Street (60' R/W with 7' Total Additional Landscaped Buffer)* illustrates this condition.

Local Streets

Local Streets include a 60-foot right-of-way and provide for circulation within the residential planning areas of the project site. Exhibit 12a, Local Street Sections: Primary Local/Interior Street & Cul-De-Sac Street, illustrates these improvements. Where any local street is adjacent to a park, paseo area, or paseo link, the street parkway will be included as park/paseo area/acreage. *Exhibit 12a, Local Street Sections, Primary Local/Interior Street & Cul-De-Sac Street* along Park/SCE Easement, illustrates these improvements.

Lanes

Private lanes are to be utilized within the residential development. These lanes will be private, but will offer a public utility easement along the paved section. Where private lanes are utilized for non-emergency access a 20-foot minimum paved section with 5-foot minimum (measurement taken from inside of curb) landscaping on each side will be used as illustrated on *Exhibit 12b, Lane (Private Alley) Sections*. Where emergency access lanes or public utility easements are proposed within the residential development, a 24-foot minimum paved section with 3-foot minimum landscaping (measurement taken from inside of curb) on each side will be utilized.

Motorcourts

Motorcourts shall be scaled and developed consistent with the development standards of the typical product plotting diagram of the applicable residential product (Motorcourt Cluster A, B, C); see Section 7.5.2 for home type criteria.

5.1.2 Traffic Calming

The Specific Plan area provides for traffic calming within residential neighborhoods to slow traffic and reduce traffic noise on streets contributing to livable neighborhoods in which to walk, bike, and drive.

Traffic calming within the Specific Plan is designed to address the following:

- ♦ Reduction in traffic speeds;
- ♦ Reduction in traffic related noise;
- ♦ Reduction in "cut-through" traffic within residential neighborhoods;
- ♦ A secure pedestrian friendly circulation system to encourage walking;



- ♦ Allowance for non-restricted access for emergency services vehicles such as police, fire, and ambulances; and
- ♦ Reduction in radii of streets.

Factors affecting traffic speeds are those that influence the driver's perception of the roadway such as:

- ♦ Type of adjacent development and the distance of development from the roadway;
- ♦ Frequency of access points onto the roadway;
- ♦ Roadway alignment and curvature;
- ♦ Type and massing of landscaping adjacent to the roadway;
- ♦ Frequency of traffic control devices along the roadway;
- ♦ Width of travel lanes; and
- ♦ Roundabout locations.

The following traffic calming techniques may be implemented in the design of the roadways within the Specific Plan.

Local Neighborhood Street Design

Neighborhood streets within the Specific Plan are designed in a grid with landscaping on either side within parkways to add interest in the street, encouraging drivers to slow their travel speed and observe their surroundings. Traffic speeds on local residential streets may be reduced by incorporating tapered intersections for local streets as illustrated in *Exhibit 12c, Neighborhood Street Chokers*. Tapers may not be appropriate at all intersections.

Lanes (Private Alleys)

The lanes (private alleys) within the Specific Plan will incorporate tapers at the entrance to lanes to slow traffic in these areas and to provide a visual element to discourage drivers from using lanes as a through street. Speeds for traffic entering the lanes and cut-through traffic can be reduced by incorporating tapers at the entrance to these lanes as illustrated on *Exhibit 12d, Lane (Private Alley) Tapers*.

Landscaping

Landscaping adjacent to streets within the Specific Plan area will combine the use of shade trees, shrubs, and ground cover adjacent to sidewalks to create a more intimate streetscape, encouraging drivers to reduce driving speeds. The landscape concept for the Specific Plan is designed to contribute a sense that the street system is a pedestrian "protected area" to promote slower traffic speeds.

5.1.3 Pedestrian Circulation

Off-street pedestrian circulation will be available throughout the Specific Plan by means of the interconnected paved sidewalk system within the roadway right-of-way separated from vehicular travel lanes by a landscaped parkway. All pedestrian circulation improvements within the SCE corridor and County Line Channel require approval from the appropriate public entity prior to the construction of any improvements.

5.1.4 Bicycle Trails

Bicycle trails are an integral element creating accessibility and mobility within the Specific Plan. A multi-purpose pedestrian and bicycle trail will be provided within the SCE Corridor between Eucalyptus Ave. and County Line Channel. This multi-purpose trail will provide a link within the City's Master Planned trail system proposed for SCE easements and corridors throughout the City. All bicycle trail improvements within the SCE corridor and County Line Channel require approval from the appropriate public entity prior to the construction of any improvements. Refer to *Exhibit 13, Pedestrian and Bicycle Circulation Plan*.

5.2 WATER MASTER PLANS

All pipe improvements, sizing and alignment shall follow the most currently approved version of the Master Plan (currently 2012), in case there are discrepancies between pipe improvements, sizing and/or alignment as shown on the utility exhibits versus the Master Plans.

5.2.1 Domestic and Recycled Water Master Plan

Domestic water will be provided by the City of Ontario. The New Model Colony Water Master Plan Phase 1 (Phases 1a-1e) identifies new water facilities to include three reservoirs, four groundwater wells, and potable and recycled transmission water lines. Construction of the on-site and off-site Master Plan water service improvements shall be the responsibility of the developer(s) and is required prior to issuance of occupancy permits for the Specific Plan. One of the waterlines included in the Phase 1 improvements is a 24-inch water main in Eucalyptus Ave., which borders the Specific Plan on the north. Master planned domestic and recycled water main lines serving surrounding and within the Specific Plan, as identified in the Water Master Plan, shall be constructed prior to issuance of first occupancy.

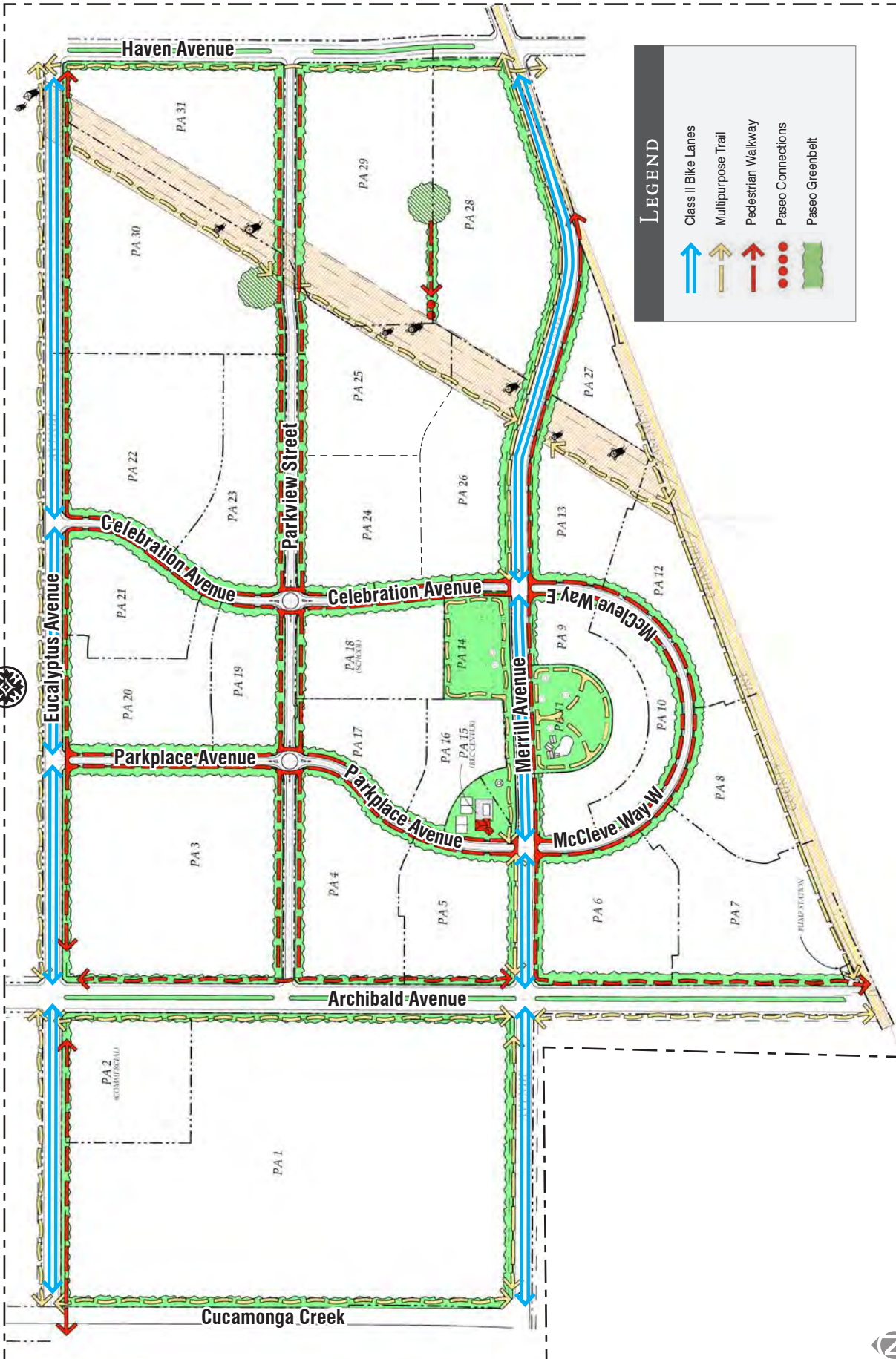
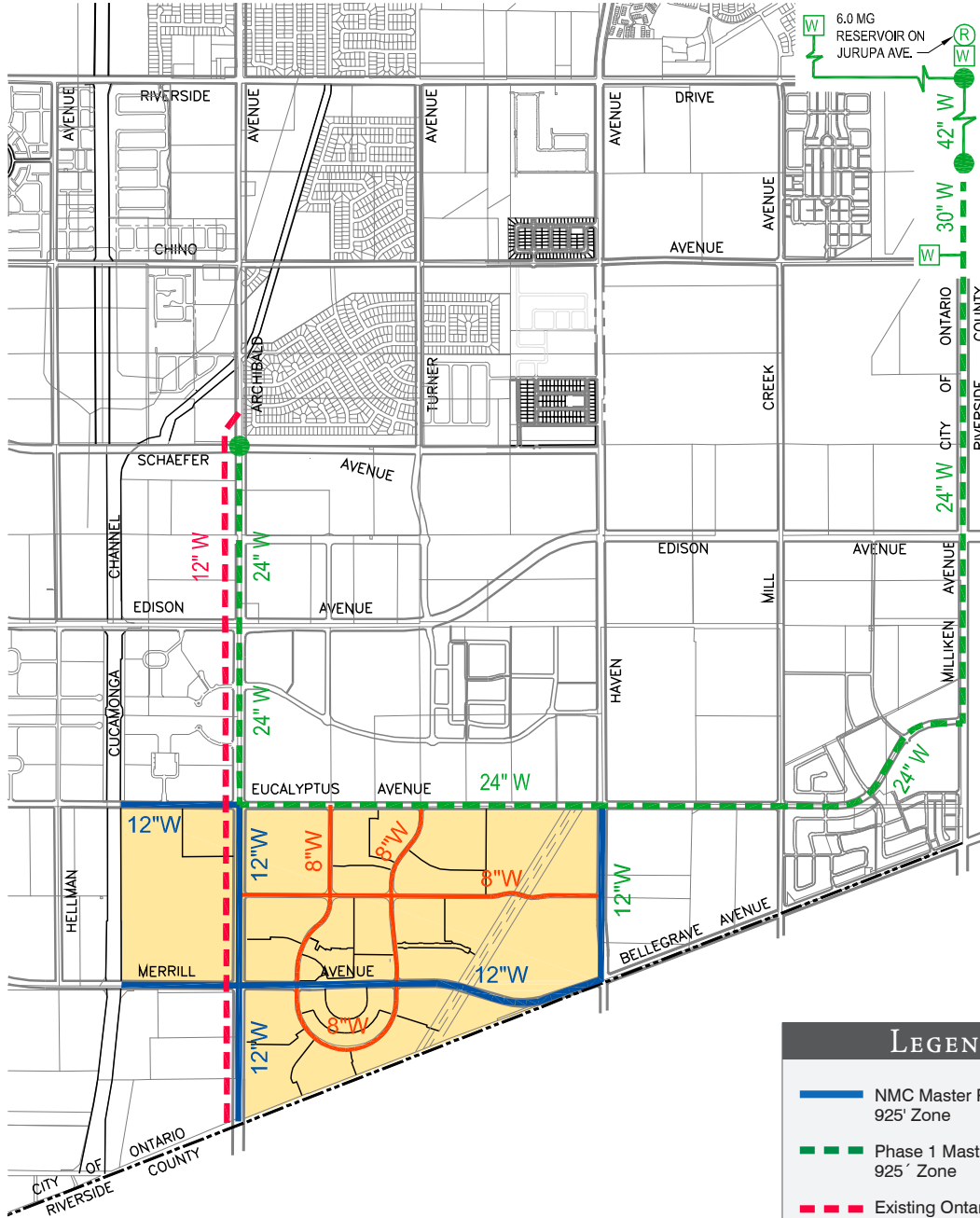


EXHIBIT 13 — PEDESTRIAN AND BICYCLE CIRCULATION PLAN



LEGEND	
	NMC Master Plan Water 925' Zone
	Phase 1 Master Plan Water 925' Zone
	Existing Ontario / CDA Interconnection
	Conceptual On-Site Water
	Specific Plan Boundary



EXHIBIT 14— CONCEPTUAL DOMESTIC WATER MASTER PLAN



5.2.2 Domestic Water

The project site lies within the 925 Pressure Zone. Master Plan water mains to be constructed as part of the development of the Specific Plan will include a 12-inch water main in Merrill Ave., Haven Ave., and Archibald Ave. Within the project site, a network of minimum 8-inch water lines will be installed. The proposed on-site public water system sizing is subject to the recommendations and approval of the required hydraulic analysis.

The Master Plan for domestic water for the Specific Plan is illustrated on Exhibit 14, Conceptual Domestic Water Improvement Plan.

5.2.3 Recycled Water System

New recycled water lines in conformance with the City of Ontario's Recycled Water Master Plan will be constructed as part of the development of the Specific Plan. The source of recycled water is an Inland Empire Utilities Agency (IEUA) Facility located east of the Cucamonga Creek Channel and north of Riverside Drive. The project site lies within the 930 Pressure Zone. The master plan improvements, adjacent to the project site, will include a 16-inch recycled water line in Eucalyptus Ave., a 12-inch recycled water line in Archibald Ave., and in Merrill Ave. (west of Archibald Ave.), and 8-inch recycled water lines in Merrill Ave. (east of Archibald Ave.) and in Haven Ave. The planned recycled water line along Riverside Drive will be constructed generally as follows: Easterly along Riverside Drive from RP-1, southerly along Haven to Chino Avenue, including the pressure reducing station at Haven & Chino. This pipeline shall be fully operational prior to occupancy. Within the project site, a network of minimum 8-inch recycled water lines will be installed. The proposed on-site public water system sizing is subject to the recommendations and approval of the required hydraulic analysis. The Subarea 29 Specific Plan shall comply with City Ordinance 2689 and make use of recycled water for all approved uses, including but not limited to irrigation of parks, schools, street landscaping, recreational trails, HOA maintained on-site common areas, and commercial/industrial landscaping. An Engineering Report approved by the City and the Department of Health Services is required prior to the use of recycled water.

There may be an interim period where recycled water supply does not have adequate pressure depending on the timing of development and construction of a regional booster station. In this case, the recycled water system will connect to the potable system initially, until such point that recycled water service is available.

The Recycled Water Master Plan for recycled water for the Specific Plan is illustrated on *Exhibit 15, Conceptual Recycled Water Master Plan*.

5.3 SEWER MASTER PLAN

All pipe, improvements, sizing, and alignment shall follow the most current approved version of the Sewer Master Plan (currently 2012), in case there are discrepancies between pipe improvements, sizing, and/or alignments as show on the utility exhibits versus the Master Plan. Sewer service for the Specific Plan area will be provided by the City of Ontario. The City's Sewer Master Plan identifies ultimate sewer facilities from the project site to the Eastern Trunk Sewer. Master Planned facilities within the project area include a 24-inch sewer main extending along Merrill Ave. from the easterly project boundary to the Eastern Trunk Sewer. Completion of these Master Plan improvements is required to provide ultimate sewer service to the project area. Within the project site, a network of minimum 8-inch sewer lines will be installed. The proposed on-site public sewer system sizing is subject to the recommendations and approval of the required sewer analysis.

The Sewer Master Plan for the Specific Plan is illustrated on *Exhibit 16, Conceptual Sewer Master Plan*.

5.4 DRAINAGE

All pipe, improvements, sizing, and alignment shall follow the most current approved version of the Storm Drain Master Plan (currently 2012), in case there are discrepancies between pipe improvements, sizing, and/or alignments as show on the utility exhibits versus the Master Plan. The City's Storm Drain Master Plan identifies two new storm drain systems to serve the site and properties north of the site. The storm drain systems, namely the Turner Storm Drain and the Archibald Storm Drain, will connect to the County Line Channel. Completion of these Master Plan improvements will provide permanent storm drain service to the Specific Plan area.

That portion of the Master Planned line, which lies within the Specific Plan, will be constructed as a part of the development of the Specific Plan. The Archibald Ave. storm drain includes a 90-inch storm drain in Archibald Ave., which transitions to a 96-inch storm drain and connects to the County Line Channel. The Celebration Ave. storm drain includes a 84-inch storm drain in Turner Ave., which transitions to an 90-inch storm drain, then transitions to a 96-inch storm drain and connects to the County Line Channel.

On-site storm drains will be constructed to convey the on-site flows to the proposed Master Planned lines. Size and location of proposed SD may change based on final design. On-site storm flows from the Specific Plan west of Archibald Ave. will be collected at an outlet into the Cucamonga Creek Channel.

The Storm Drain Master Plan for the Specific Plan is illustrated in *Exhibit 17, Conceptual Storm Drain Master Plan*. Size and location of proposed storm drain may change based on final design.



5.4.1 NPDES Compliance

The grading and drainage of the Specific Plan area shall be designed to detain, filter, and treat surface runoff, in a manner and combination which is practical, to comply with the most recent requirements of the San Bernardino County NPDES Storm Water Program's Quality Management (WQMP) for significant new development projects. The objective of the WQMP for this project is to minimize the detrimental effects of urbanization on the beneficial uses of receiving waters, including effects caused by increased pollutants and changes in hydrology. These effects may be minimized through the implementation of site designs that reduce runoff and pollutant transport by minimizing impervious surfaces and maximizing on-site infiltration, Source Control Best Management Practices (BMP's) and/or either on-site Structural Treatment Control BMP's, or participation in regional or watershed-based Treatment Control BMP's.

Prior to the issuance of a grading or construction permit, a Storm Water Pollution Prevention Plan (SWPPP) will also be prepared. The SWPPP will be prepared to comply with the California State Water Resources Control Board's (State Water Board) current, "General Permit to Discharge Storm Water Associated With Construction Activity" and the current Area Wide Urban Storm Water Runoff (Regional NPDES Permit). The SWPPP will identify and detail all appropriate Best Management Practices (BMP's) to be implemented or installed during construction of the project.

In addition to the preparation of a SWPPP for construction-related activities, and as part of the approval of any grading plans within the Specific Plan Area, the applicant will be required to submit a Water Quality Management Plan (WQMP) on the regional model form provided by the City. The WQMP shall identify and detail all Site Design BMP's, Source Control BMP's and Treatment Control BMP's to be implemented or installed at this site in order to reduce storm water pollutants and site runoff.

The Mill Creek Wetlands, a regional runoff treatment facility for the sub-watershed area that this project lies within has been constructed. This regional treatment facility is part of an overall solution for storm water treatment.

5.5 SCHOOLS

The Specific Plan area is located within the Mountain View School District which serves the school needs for grades K-8 and the Chaffey Joint Union School District, which serves the school needs for grades 9-12.

Development of the Subarea 29 Specific Plan area will generate an estimated student population as follows (based on student generation numbers supplied by the City of Ontario, "School Generation for NMC Subareas," July 2, 2004):

Grades K-5 - Generation Factor 0.38/du - $0.38 \times 2,392 = 909$

Grades 6-8 - Generation Factor 0.22/du - $0.22 \times 2,315 = 527$

Grades 9-12 - Generation Factor 0.20/du - $0.20 \times 2,315 = 479$

One 10-acre elementary school is included within the Subarea 29 Specific Plan (Planning Area 18). The school is located north of Merrill Ave. in the heart of Park Place, adjacent to the two (2) neighborhood parks. Joint use school/park facilities are envisioned, serving the K-5 school needs generated by the project.

A 20-acre middle school site is conceptually located east of Subarea 29, east of Haven Ave. and south of Eucalyptus Ave. (not within the Specific Plan area).

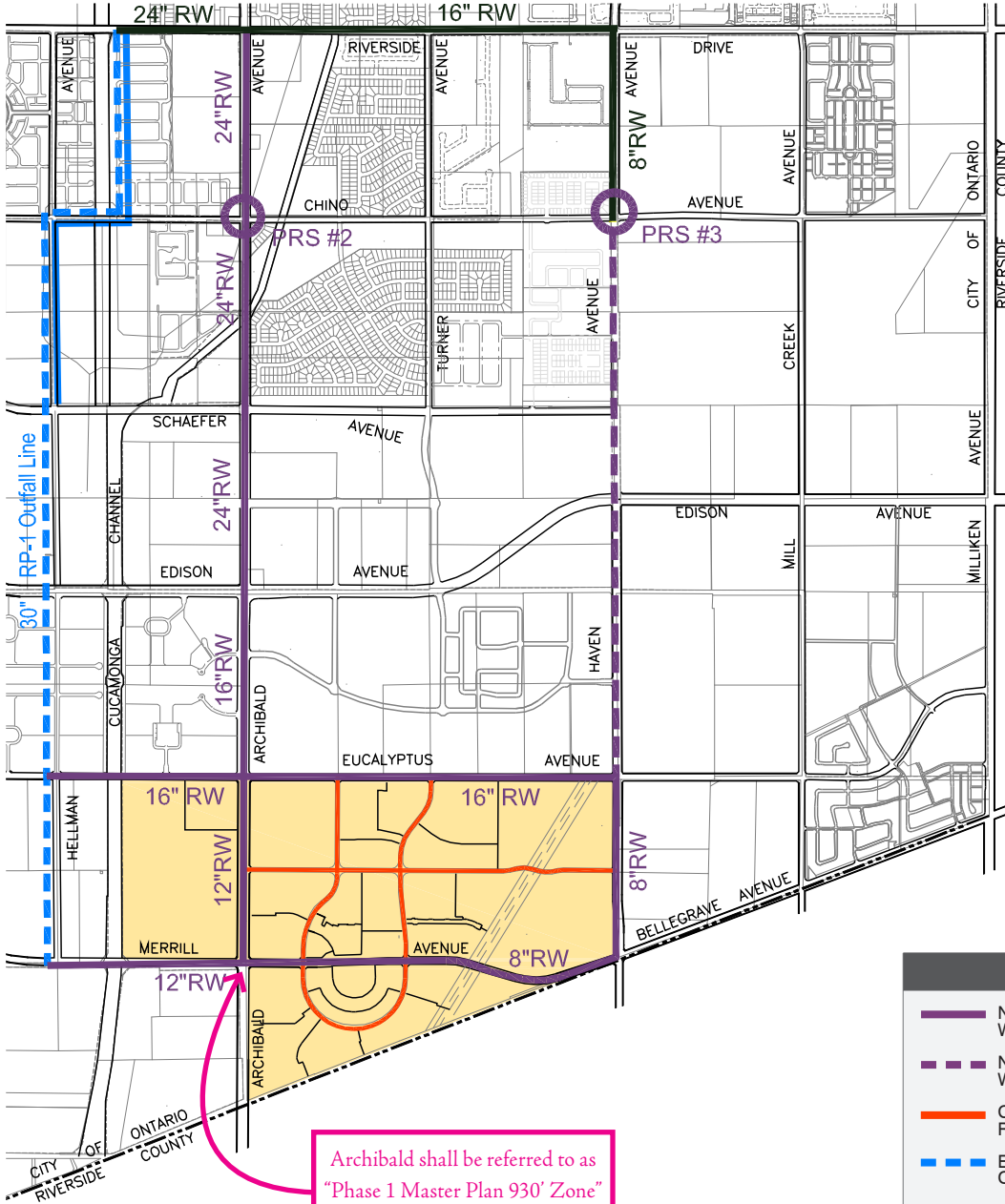
The developers of the Specific Plan will be required to pay school fees or construct school facilities as required by the State of California.

5.6 PUBLIC UTILITIES

Public utilities including telephone, gas, and electrical service will be available to the site and will be extended into the planned community as part of the development of the Specific Plan. All existing overhead utilities shall be under-grounded, including all on-site utilities and those within the public right-of-way along streets fronting the project. All new facilities will be under-grounded by developer and according to the City of Ontario adopted ordinance.

5.6.1 Telecommunications

Proposed Fiber Optics (conduits and fiber) will be placed underground within a duct and structure system to be installed by the Developer. The fiber optic conduits along the backbone streets shall be installed in a joint trench by each Developer as the last lane improvements are completed. In-tract fiber optic conduits shall be installed by the Developer in joint trenches with City Street Light conduits where possible. Maintenance of the installed system will be the responsibility of the City/Special District. Development of the Project requires the installation by the Developer of all fiber optic conduits and pullboxes as well as payment of Development Impact Fees to cover the installation of the fiber optic cable.



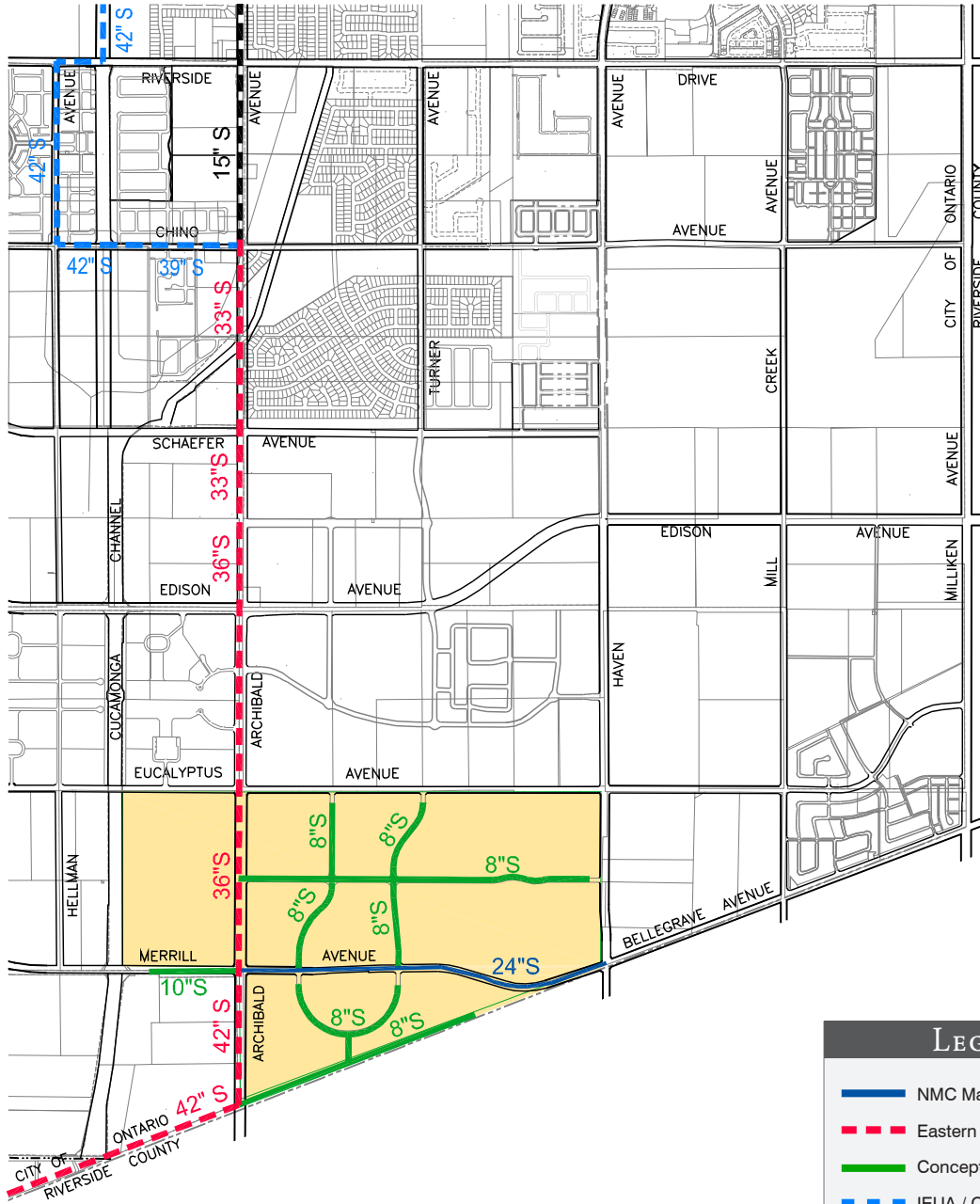
Archibald shall be referred to as
 "Phase 1 Master Plan 930' Zone"
 Including PRS#2

LEGEND	
	NMC Master Plan Recycled Water 930' Zone
	NMC Master Plan Recycled Water 930' Zone by Others
	Conceptual On-Site Recycled Water
	Existing IEUA 30" RP-1 Outfall Line
	Ultimate IEUA Outfall
	NMC Master Plan Recycled Water 1050' Zone
	Pressure Reducing Station 1050' PZ to 930' PZ
	Specific Plan Boundary



EXHIBIT 15— CONCEPTUAL RECYCLED WATER MASTER PLAN

SPECIFIC PLAN — OCTOBER 2006
 AMENDED APRIL 21, 2015



LEGEND	
	NMC Master Plan Sewer Main
	Eastern Trunk Sewer
	Conceptual On-Site Sewer
	IEUA / City of Ontario RP-1 Bypass
	City of Ontario Sewer
	Specific Plan Boundary



EXHIBIT I6— CONCEPTUAL SEWER MASTER PLAN

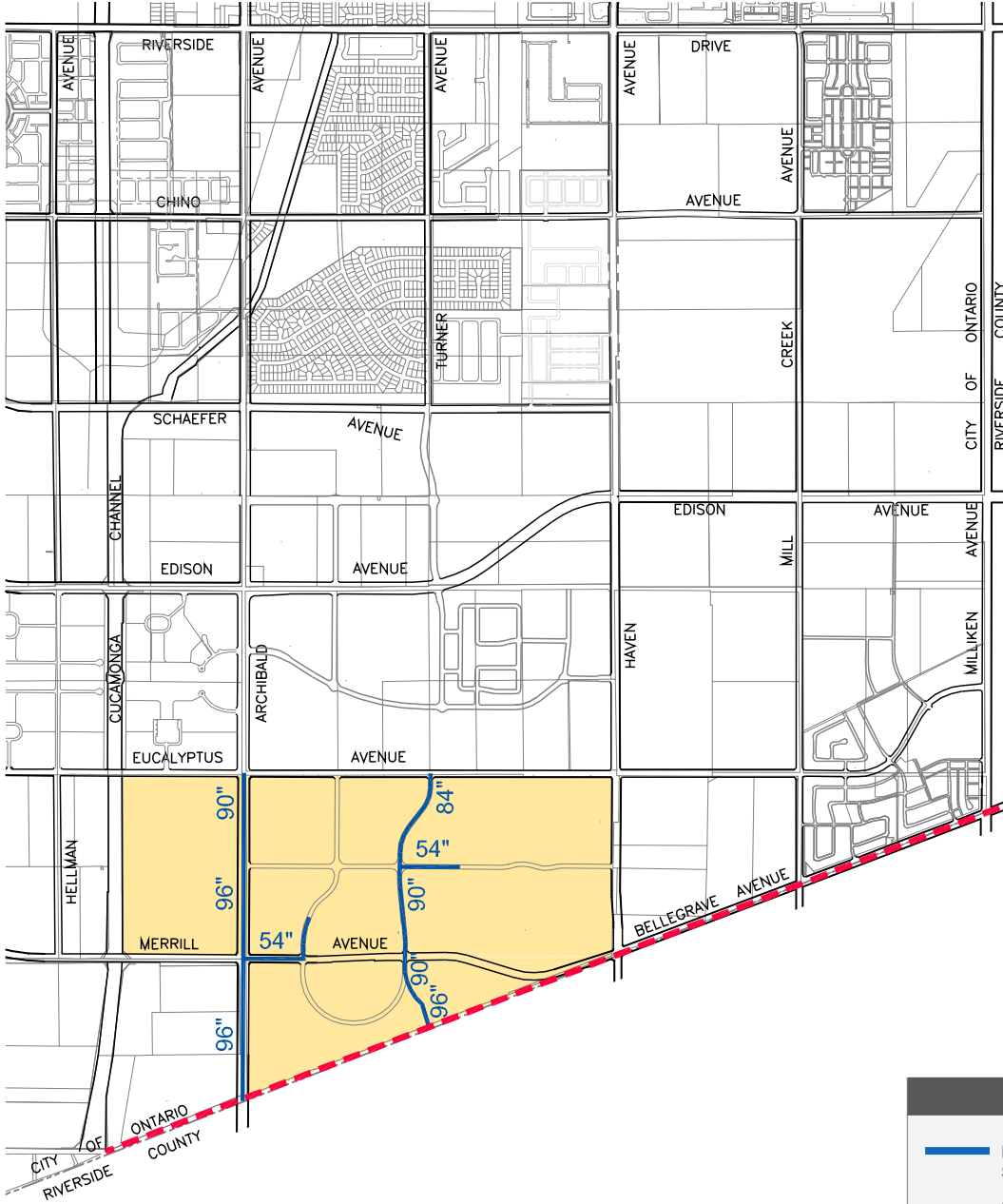
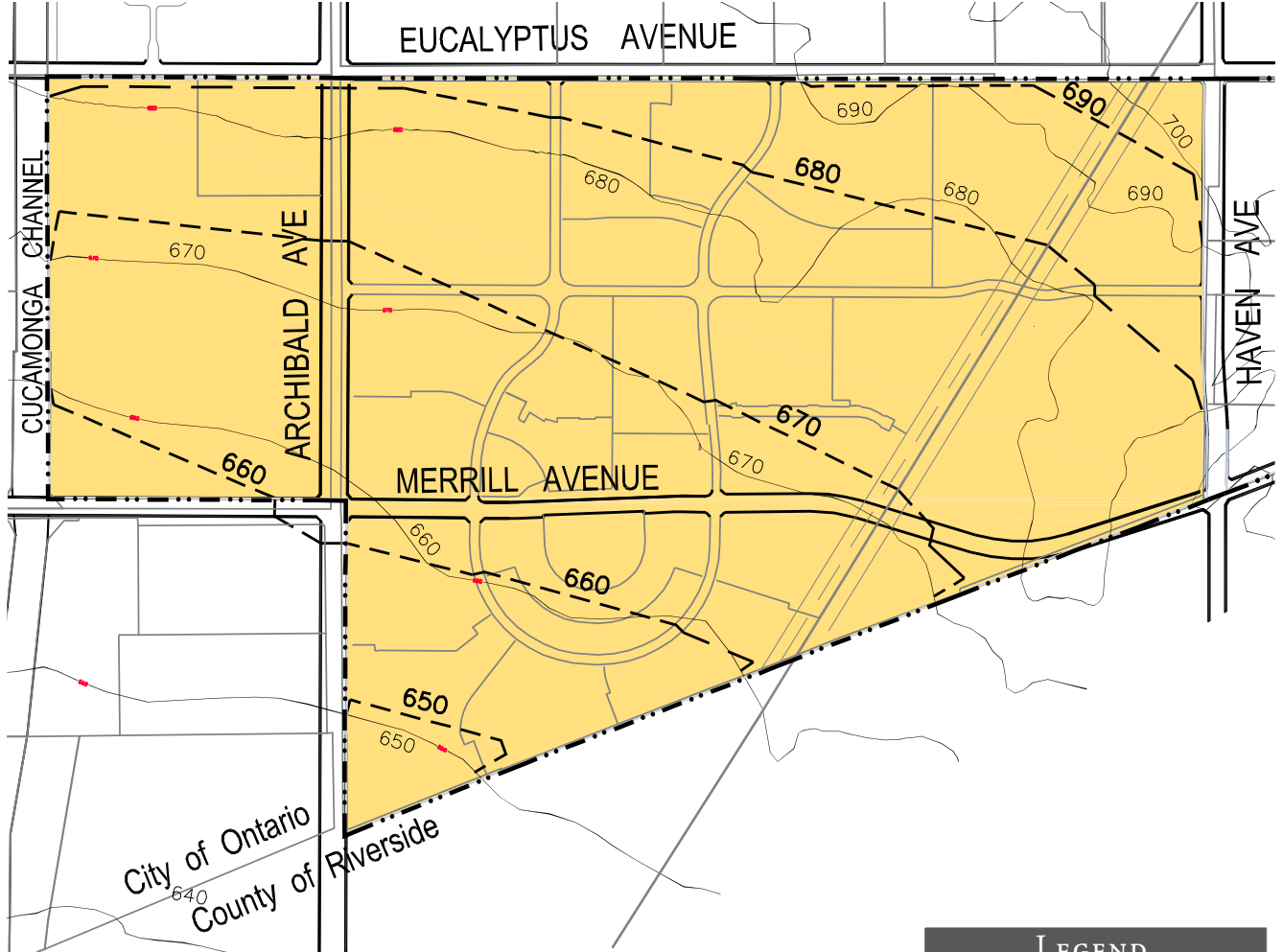


EXHIBIT 17—CONCEPTUAL STORM DRAIN MASTER PLAN

SPECIFIC PLAN — OCTOBER 2006
 AMENDED APRIL 21, 2015



LEGEND	
	Specific Plan Boundary
	650 Existing Contours
	660 Proposed Contours



EXHIBIT 18—CONCEPTUAL GRADING PLAN



5.6.2 Natural Gas

The Southern California Gas Company will provide natural gas to the site.

5.6.3 Electricity

Southern California Edison will provide electricity to the site from existing facilities in the vicinity of the site. Proposed on-site facilities will be located underground.

5.6.4 Solid Waste

- ♦ Commercial - Developer shall comply with Municipal Code Sec. 6-3.314 Commercial Storage Standards, and Sec. 6-3.601 Business Recycling Plan.
- ♦ Apartment - For apartments using commercial bin service developer shall comply with Municipal Code Sec. 6-3.314 Commercial Storage Standards and Sec. 6-3.601 Business Recycling Plan.
- ♦ Residential - For curbside automated container service developer shall comply with Municipal Code Sec. 6-3.308.9(a) and (d), Residential Receptacles, Placement.
- ♦ Recycling Requirements - Developer shall comply with Municipal Code Article 6. Recycling Requirements for Specified Business Activity, Sec. 6-3.601 Business Recycling Plan, and Sec. 6-3.602 Construction and Demolition Recycling Plan.
- ♦ Site Improvement Plans shall follow the City of Ontario refuse collections standards.
- ♦ Standard two bin enclosures (for solid waste) may be used to service the residents. Modification to the standard two bin enclosure may be allowed to accommodate architectural/design layout for the cluster products.
- ♦ CC&Rs/HOA shall define areas where tenants place their waste cans for pickup by service vehicles. Said policy shall be reviewed/ approved by the Solid Waste Department.

5.7 GRADING CONCEPT

The existing ground has a gentle, uniform slope to the south, at approximately 1.0 to 2.0% and will not require a large grading operation, nor will it result in noticeable cut or fill slope banks. The grading operation will generally consist of the removal of any manure left over from dairy operations, clearing and grubbing, demolition of existing structures, and movement of surface soils to construct residential building pads and streets.

The grading and drainage of the Subarea 29 Specific Plan shall be designed to retain, infiltrate, and treat surface runoff, in a manner and combination which is practical, to reach NPDES compliance. Landscaped strips and landscaped setback areas can be swaled and depressed to retain and infiltrate irrigation water and runoff from smaller storm events. Pedestrian paseos, open space, and paseo connectors are examples of the landscaped strips that work well for this purpose in residential areas. If slopes are required on the property, the top of the slopes shall be set at the property lines. In areas adjacent to a ridgeline or in moderate slope areas, dwelling units and structures should be sited to use the natural ridgeline as a backdrop for structures; use landscape plant materials as a backdrop; and use structure to maximize concealment of cut slope. If retaining walls are constructed, they shall be kept to a minimum height and step with the contours of the property. Where retaining walls or fences face roadways, they shall be built, when feasible, of decorative materials consistent with the wall theme of the neighborhood. Exposed walls and fences facing roadways shall be no greater than six feet (6) in height, except as necessary for acoustical purposes to satisfy the intent of the noise ordinance.

5.7.1 Preliminary Earthwork Calculations

Based on the Conceptual Grading Plan, the estimated total amount of cut/fill for the project would be approximately 1,200,000 cubic yards.

5.7.2 Preliminary Grading/Drainage Concept

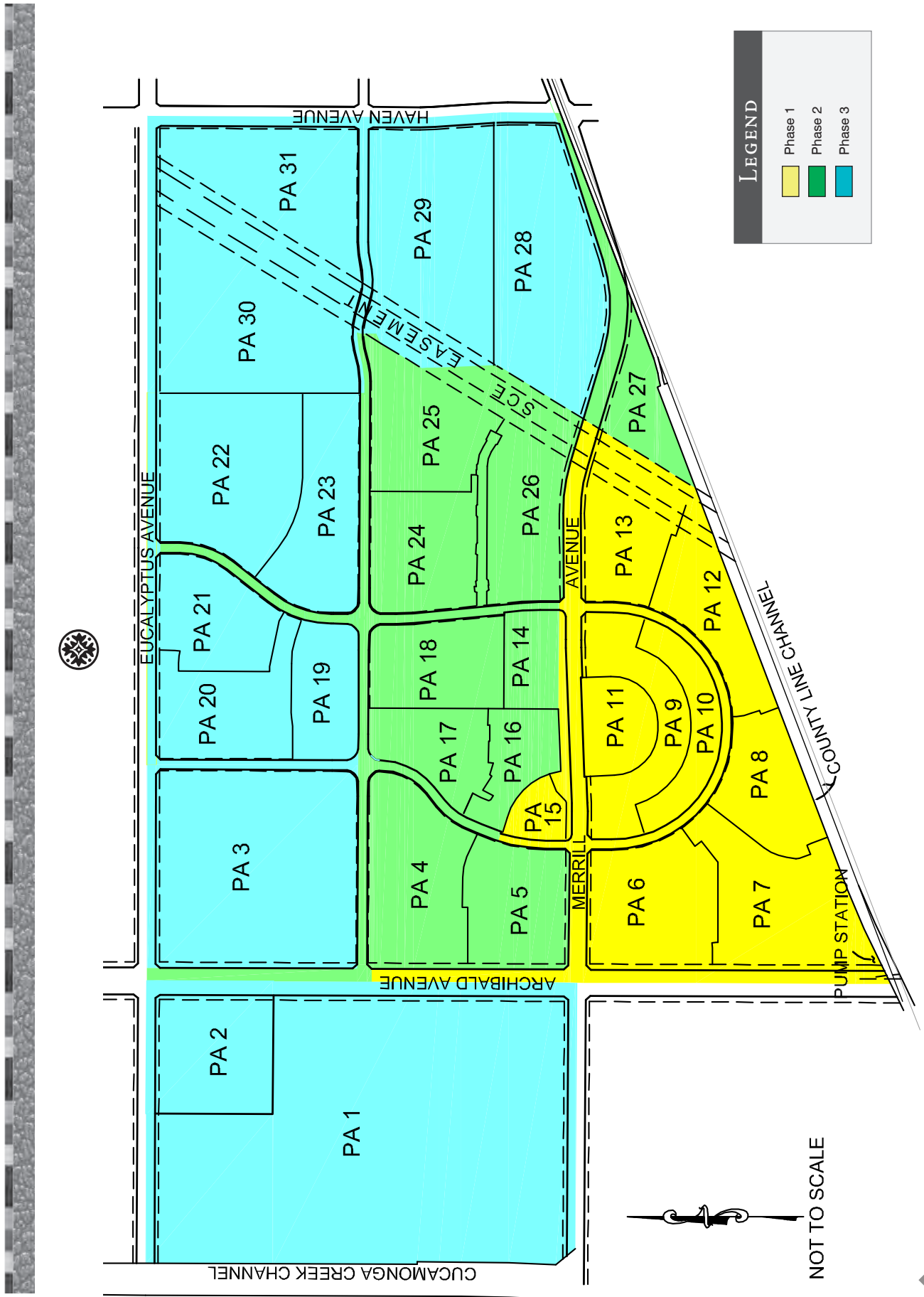
Grading plans for each tract in the Specific Plan area will be reviewed and approved by the City of Ontario Building, Planning, and Engineering Departments prior to the issuance of grading permits. All grading plans and activities will conform to the City grading ordinance and dust control and erosion control requirements.

A Conceptual Grading Plan for the project site is illustrated in Exhibit 18.

5.8 PHASING

Development of the Specific Plan area will be phased to meet the following objectives:

- ♦ Orderly build-out of the community based upon market and economic conditions;
- ♦ Provisions for adequate infrastructure and public facilities as determined and deemed necessary by the City Engineer; and
- ♦ Protection of public health, safety and welfare.





Development phasing of the project site will be determined by the landowner and/or developer based upon real estate market conditions. Phasing will occur as appropriate levels of infrastructure, community facilities, and open space dedications are provided. Phasing sequencing is subject to change over time to respond to these various factors and individual phases may overlap or develop concurrently. Phased infrastructure improvements, as required and approved by the City Engineer to support each phase, will be installed by the developer. Refer to *Exhibit 19, Conceptual Phasing Plan*. This plan describes three general phases of development, starting in the southern portion of Park Place and extending north over time.

Infrastructure required to serve the commercial development area will be installed by the developer(s) of these areas. The timing for installation of infrastructure and utilities and the provision of public services for the commercial development within the Specific Plan will be determined as part of the City's approval of parcel maps. Facilities will be constructed and services made available in a timely manner as development progresses.

Backbone infrastructure to serve all areas of the Specific Plan shall be installed by the developer(s) in accordance with the City's adopted Master Plan for the areas or any approved amendments to it. Infill service mains will be installed/constructed in phases as development proceeds and conditioned by the City Engineer's office to support individual phases of development. This may require installation of off-site infrastructure improvements beyond a given phase boundary.

Flexibility to adjust final product selection in Phases 2 and 3 shall be provided to the master developer, based on evolving market conditions. In particular, parcels designated for lane loaded product may be re-designated to conventionally loaded product in the future, at the time Phases 2 and 3 are underway. Other single-family detached product lines may also be adjusted over time, while not sacrificing the overall integrity of the proposed land plan and with the approval of the City of Ontario. Should a new product be introduced, a Specific Plan Amendment will be required.

A "Model Home Permit" is required and will be approved through the "Temporary Use Permit" process. Flexibility to adjust final product

Land Use, Infrastructure and Services

Water, recycled water and sewer utilities may be designated as "public utilities" if located within public or private streets. All public utilities within private streets shall be designed and constructed per City standards and contained within acceptable easements. The CC&RS shall contain language that requires all proposed work by the HOA within said easements to be plan checked and inspected by the City, including applicable fees. Generally, utilities will not be accepted as public within lanes, parking areas or driveways. Utilities within commercial and industrial parking lots and loading areas shall be designated as private. The extent to which said utilities will be accepted as public utilities shall be determined, at the full discretion of the City, during final design plan review.



6. Development Regulations





6.0 Development Regulations

6.1 INTRODUCTION

The provisions contained herein will regulate design and development within the Subarea 29 Specific Plan. The regulations contained herein provide for the development of all land use categories with development regulations established for residential uses, commercial uses, parks and recreation, and landscaping.

6.2 DEFINITION OF TERMS

The meaning and construction of words, phrases, titles, and terms shall be the same as provided in the City of Ontario Development Code Article 2, "Definitions," unless otherwise specifically provided herein.

6.3 APPLICABILITY

The development regulations contained herein provide specific standards for land use development within the Subarea 29 Specific Plan. Regulations address residential, commercial, and parks and recreation land uses. General landscaping regulations are also provided. Application of the following regulations is intended to encourage the most appropriate use of the land, ensure the highest quality of development, and protect the public health, safety, and general welfare. The adoption of the Subarea 29 Specific Plan by ordinance, supersedes otherwise applicable City of Ontario zoning regulations unless stated herein to the contrary. Whenever the provisions and development standards contained herein conflict with those contained in the City of Ontario Development Code, the provisions of the Subarea 29 Specific Plan shall take precedence. Where the Specific Plan is silent, City codes shall apply. These regulations shall reinforce specific site planning, architectural design, and landscape design guidelines contained in *Chapter 7, Residential Design Guidelines*, and *Chapter 8, Commercial Design Guidelines*, of the Specific Plan.

6.4 ADMINISTRATION

The Specific Plan is adopted by ordinance and will serve as the implementation tool for the General Plan as well as the zoning for the site. The Specific Plan Development Regulations address general provisions, permitted uses, and development standards for the project.

6.5 METHODS AND INTERPRETATION

Development within the Specific Plan shall be implemented through the approval by the City of Ontario of parcel maps, tract maps, and development permits. The administration process described herein provides the mechanisms for review and approval of development projects within the project, consistent with the Specific Plan objectives.

6.6 GENERAL SITE DEVELOPMENT CRITERIA

The following general site development criteria shall apply to all land development proposed in the Specific Plan.

- ♦ Gross Acres-Except as otherwise indicated, gross acres for all development areas are measured to the centerline of streets;
- ♦ Grading-Development within the site shall utilize grading techniques as approved by the City of Ontario. Grading concepts shall respond to the grading design included in this Specific Plan which guide the development of land use toward the goal of providing for a livable community with streets and entries designed for walking and resident interaction;
- ♦ Building Modification-Additions shall match the architectural style of the primary unit and shall be constructed of the same materials and colors as the primary unit;
- ♦ Utilities-All new and existing public utility distribution lines of 66 kV or less shall be subsurface throughout the planned community;
- ♦ Technology-All homes and businesses shall accommodate the most modern technology for computer internet access, phone, fax, and television. Broadband fiber optics cable will be installed on all peripheral streets per the approved Broadband Master Plan;
- ♦ Density-The Specific Plan Land Use Plan allocates a total number of units to each residential neighborhood as indicated in the "Land Use Summaries" Tables 1 and 1b, included in Section 4, Land Use, of the Specific Plan. Variations in the number and type of dwelling units within each residential neighborhood may occur at the time of final design of the neighborhood, depending upon the residential product identified for development. Increases in allocation of residential units up to a maximum of fifteen percent (15%) are permitted among the residential neighborhoods within the Specific Plan, provided the total number of units established for the Specific Plan area is not exceeded; and



- ♦ Maximum Number of Dwelling Units-The maximum number of dwelling units permitted within the Subarea 29 Specific Plan shall be 2,392 dwelling units, as set forth in the *Specific Plan Land Use Summaries*. Consistent with Table 1a, the maximum number of residential dwelling units permitted within Park Place shall be 1,348 dwelling units. The maximum number of residential dwelling units permitted within Richland Communities, Inc. controlled properties shall be 936 dwelling units. The maximum number of residential dwelling units permitted within Brookfield Homes controlled property shall be 108 dwelling units.
- ♦ The measurement of the width of the lots on curvilinear streets shall be taken from the front and rear setback lines. The front setback lines shall be displayed by dotted lines or by plotting the footprint of the dwelling unit.
- ♦ Buildings or parts of buildings, in particular roof overhangs, shall not encroach into the 3-foot building setback from the property line.

6.7 IMPLEMENTATION

- ♦ Development proposals within the project site shall be subject to the implementation procedures established herein. The provisions and development standards contained herein supercede those contained in the City of Ontario Development Code. However, where the development standards are silent, the City's Development Code shall apply;
- ♦ Severability - If any portion of these regulations is declared to be invalid or ineffective in whole or in part, such decision shall not affect the validity of the remaining portions thereof. The legislative body hereby declares that they would have enacted these regulations and each portion thereof irrespective of the fact that any one or more portions be declared invalid or ineffective;
- ♦ Unless otherwise provided, any ambiguity concerning the content or application of the Specific Plan shall be resolved by the Planning Director or his/her designee, hereinafter referred to as Director, in a manner consistent with the goals, policies, purpose, and intent established in this Specific Plan; and
- ♦ Any major deviation from the design guidelines within the Specific Plan shall require a Specific Plan Amendment. The Director shall determine whether a proposed change constitutes a major deviation.

6.8 DEVELOPMENT PERMIT

All development projects within the Specific Plan shall be subject to the Development Plan Review Process as established in Article 8: Development Plan Review, City of Ontario Development Code. Adoption of the Specific Plan by the City includes adoption of the design guidelines contained within the Specific Plan. Where the Specific Plan design guidelines are silent, the applicant's design guidelines contained within the City's Development Code shall apply. The design guidelines are intended to be flexible in nature while establishing basic evaluation criteria for the review by the City of developer projects during design review.

Pursuant to these provisions, the Development Permit process constitutes a design review of project architecture, site plans, and landscape plans. All development project applications shall include a landscape and irrigation plan describing plant materials and their growth habits, plant size and spacing, methods of irrigation and landscaping maintenance, site plans, architectural elevations, floor plans, grading plans, and other requirements as specified by the City. Development permits will be approved with conditions of approval.

6.9 SUBDIVISION MAPS

Approval of the Tentative Tract Map(s) and Parcel Maps will create legal lots for development. All Tentative Tract Maps and Parcel Maps will be reviewed and approved pursuant to applicable provisions of the City of Ontario Subdivision Ordinance and consistent with the applicable provisions contained within this Specific Plan.

6.10 SPECIFIC PLAN MODIFICATIONS AND AMENDMENTS

6.10.1 Minor Modifications

The following constitute minor modifications to the Specific Plan, not requiring a Specific Plan Amendment, and are subject to review and approval by the Zoning Administrator. The Zoning Administrator shall have the discretion to refer any such request for modification to the Planning Commission or the City Council.

- ♦ Change in utility and/or public service provider;
- ♦ An increase of more than fifteen percent (15%) to the number of units within an individual neighborhood, provided the total number of units for the entire Specific Plan areas does not exceed that established in the approved Specific Plan;



- ♦ Adjustment of a neighborhood boundary, provided the total acreage of the affected area does not increase or decrease by more than fifteen percent (15%) of the total acreage stated in the approved Specific Plan;
- ♦ Minor changes to the design guidelines, which are intended to be conceptual in nature only, and are intended to be flexible in implementation; and
- ♦ Other modifications of a similar nature to those listed above, which are deemed minor by the Director, which are in keeping with the purpose and intent of the approved Specific Plan and which are in conformance with the General Plan.

6.10.2 Specific Plan Amendments

Amendments to the Specific Plan may be requested by the applicant or by the City at any time pursuant to Section 65433(a) of the Government Code. Amendments shall be processed pursuant to the provisions of the Government Code for Specific Plan Amendments. In the event the proposed amendment requires supplemental environmental analysis pursuant to the California Environmental Quality Act (CEQA), the applicant(s) will be responsible for preparing the necessary CEQA documentation.

6.11 APPEALS

Appeals from any determination of the Zoning Administrator shall be made to the Planning Commission. The applicant(s) or any other entity shall have the right to appeal the decision of the Planning Commission on any determination. Appeals shall be submitted by filing an application on forms provided by the City of Ontario within ten (10) days following the final date of action for which an appeal is made. Appeals shall be processed consistent with the provisions of Article 5, "Appeals" of the City of Ontario Development Code.

6.12 PROJECT FINANCING

Construction and maintenance of public improvements and facilities to serve the Specific Plan will be financed through a combination of financing mechanisms including the use of public and private funds. In order for the project to be fiscally self-sufficient the following options can be considered for implementation:

Maintenance

A lighting and landscape district can be implemented in order to defray the costs associated with street maintenance and street light costs.

Facilities and Services

A variety of other financing options can be considered by the City of Ontario for financing facilities and services to be provided within the Specific Plan to include:

- ♦ Establishment of a Mello-Roos Community Facilities District, which enables governmental entities to finance public infrastructure on a tax-exempt basis;
- ♦ Special assessment districts;
- ♦ Benefit assessment financing for improvements such as storm drains and street lighting; and
- ♦ Integrated financing districts such as landowner assessments that are contingent upon development.

City Council approval is required for the establishment of all special district financing mechanisms. Establishment of a district would enable the City to levy a special tax and to issue bonds for the financing of improvements and/or services. Among the facilities that potentially could be financed are flood control, drainage, utilities, street improvements, and other infrastructure and related facilities. The district provisions for those services would cover all phases, including planning, design, acquisition, construction, operations, and maintenance. These assessments would only affect the Subarea 29 Specific Plan area, and the debt service incurred by these assessments would be paid by landowners or homeowners within the Specific Plan area.

6.13 MAINTENANCE RESPONSIBILITIES

The public and private improvements constructed within the Specific Plan area will be maintained through a combination of public and private entities as described below and in *Table 4, Maintenance Responsibilities*. During the course of maintenance of public utilities (including storm drain) within private and public streets, the City will pave the streets and restore landscaping per City standards. Restoration of any enhancements above and beyond City standards, including but not limited to architectural, hardscaping, and landscaping enhancements shall be the responsibility of the HOA or other entity maintaining those enhancements. This applies to all areas where public utilities are located including but not limited to public and private streets, gated communities, alleys, etc.

6.13.1 Public Maintenance

Water, recycled water, and sewer utilities may be designated as "public utilities" if located within public or private streets. All public utilities within private streets shall be designed and constructed per City



TABLE 4—MAINTENANCE RESPONSIBILITIES

	City CFD	Private Homeowners Association (HOA)	Private (Homeowner, Commercial/Property Owners)	Utility Entity
Master Plan Roadways (Archibald Avenue, Merrill Avenue, Eucalyptus Avenue, Haven Avenue)	X			
Interior Project Streets (curb to curb Primary Entry Street, Secondary Entry Streets, Neighborhood Streets)	X			
Parkways and Neighborhood Edges of Master Plan Roadways and Entry Streets (curb to perimeter walls including landscape, sidewalks, trees)	X			
Parkways of Local Streets (landscaping, sidewalks, trees) ⁽¹⁾		X		
Interior Tract Graffiti Removal		X		
Private Recreation Areas		X		
Monument Signs (within Neighborhood edges on Master Plan Roadways)	X			
Monument Signs within tract entry		X		
Traffic Signals/Street Lights	X			
Traffic Control Signs (Public)	X			
Traffic Control Signs (Private)		X		
Public Roadways	X			
Lanes (Private Alleys)		X		
Driveways and Parking Areas Serving Commercial			X	
Community Trail (SCE Corridor Trail)	X			
Off-site and on-site water, sewer, and storm drain improvements (excluding laterals) ⁽²⁾	X			
Public Parks	X			
Pocket Parks		X		
Paseos		X		
Front Yard/Corner Streetside Landscaping Areas			X	
Private Interior Yard Walls			X	
Landscaped Common Areas		X		
Private Exclusive Use Landscaped Areas			X	
Community Theme Wall and Entry Monuments on Master Planned Roadways (outside face for graffiti removal and paint)	X			
Community Theme Wall and Entry Monuments on Master Planned Roadways: Surface (interior) opposite streetside (structural integrity and face repairs)		X		
Lane (Private Alley) Landscaping and Lighting		X		
Electricity				X
Natural Gas				X
Communication Systems	X			X
Police	X			
Fire	X			
NPDES Facilities (On-site)/W.Q.M.P. ⁽³⁾	X			
Interim Detention Basin		X		
NPDES Facilities (On-site) in Public Right-of-Way		X		
NPDES Facilities on Arterial Streets (Off-site)	X			

1. Including restoration work following public street repairs.
 2. Only those facilities in the public right-of-way and/or easements.



standards and contained within acceptable easements. The CC&RS shall contain language that requires all proposed work by the HOA within said easements to be plan checked and inspected by the City, including applicable fees. Generally, utilities will not be accepted as public within alleys, parking areas or driveways. Utilities within commercial and industrial parking lots and loading areas shall be designated as private. The extent to which said utilities would be accepted as public utilities shall be determined, at the full discretion of the City, during final design plan review. Refer to *Table 4, Maintenance Responsibilities*.

6.13.2 Homeowner Association

Refer to *Table 4, Maintenance Responsibilities*.

6.14 Residential Development Standards

6.14.1 Residential Detached

General

This category includes the development of all single-family detached dwelling units. The purpose of the residential standards for single-family detached housing is to establish the minimum criteria for the development of these product types on individual lots within the neighborhoods specified within the Subarea 29 Specific Plan. Further residential design guidelines are in Chapter 7.0.

Permitted Uses

- ♦ Public or private parks, recreational buildings, newsstands, coffee concessions, and sales of grocery items, greenbelts, or open space.
- ♦ Accessory uses to include the following:
 - ♦ Garages (minimum 2-car);
 - ♦ Granny Flats (i.e. Second Dwelling Units, in accordance with the City's Zoning Code;
 - ♦ Home occupations;
 - ♦ Swimming pools, spas, sports courts, and other similar outdoor recreational amenities;
 - ♦ Patios and patio covers;
 - ♦ Storage, garden structures, cabanas, and greenhouses;
 - ♦ Monument signage;
 - ♦ Temporary uses such as model home and subdivision sales trailers; temporary construction parking, offices, and facilities; real estate signs, signage indicating future development and directional signage with the approval of a Temporary Use Permit; and
 - ♦ Second story additions to existing single story dwelling units.

Conditionally Permitted Uses

- ♦ Places of worship including but not limited to churches and synagogues; and
- ♦ Child care facilities/day care facilities.

6.15 NEIGHBORHOOD SERVING COMMERCIAL LAND USE DEVELOPMENT STANDARDS

General

This section sets forth the development regulations for development of commercial land uses within the Specific Plan Planning Area 2.

Permitted Uses

- ♦ Bakery (Retail);
- ♦ Barber Shop and Beauty/Nail Salon;
- ♦ Book Stores;
- ♦ Camera and Photographic Supply Stores;
- ♦ Clothing and Accessory Stores;
- ♦ Convenience Market;
- ♦ Dry Cleaner;
- ♦ Florist;
- ♦ Jewelry and watches/clocks;
- ♦ Laundry – Self-Service;
- ♦ Locksmith/Key Shop;
- ♦ Police Storefront/Sub-Station;
- ♦ Satellite Dishes/Ham Radio Antennas (private use);
- ♦ Shoe Repair;
- ♦ Specialty Food Stores;
- ♦ Tailor; and
- ♦ Travel Agency



TABLE 5—NEIGHBORHOOD SERVICE COMMERCIAL SITE DEVELOPMENT STANDARDS

<i>Planning Area 2</i>	
Minimum Parcel Area	N/A
Minimum Lot Size	Shall be large enough to accommodate the proposed use and meet all Development Standards as specified within the Specific Plan. Minimum site area shall be 7 acres; however, the site area may be less when developed as part of an integrated commercial center.
Maximum Development Floor Area Ratio	0.35
<i>Minimum Building Setbacks</i> ⁽¹⁾⁽²⁾	
♦ From Public Street Property Lines	20'
♦ From Private Street Property Lines	15'
♦ From Interior Property Lines	0'
♦ From Haven Avenue Right-of-Way	14'
♦ From Archibald Avenue Right-of-Way	19'
♦ From Eucalyptus Avenue Right-of-Way	
♦ From Private or Local Street	15'
♦ From Interior Property Line	0'
♦ From Interior Property Line Adjacent to Residential District	25'
<i>Parking & Drive Aisle Setbacks</i>	
♦ From Public Arterial Streets	25'
♦ From Private Streets & Drive Aisles to Bldgs.	10'
♦ From Interior Property Line	5'
♦ From Rear Property Line	5'
♦ From Haven Avenue Right-of-Way	14'
♦ From Archibald Avenue Right-of-Way	19'
♦ From Merrill Avenue Right-of-Way	N/A
♦ From Private or Local Street	10'
♦ From Interior Property Line	5'
♦ From Interior Property Line Adjacent to Residential District	5'
Minimum Landscape Coverage	15%
<i>Maximum Building Height</i>	
♦ Main Structure	35'



TABLE 5—NEIGHBORHOOD SERVICE COMMERCIAL SITE DEVELOPMENT STANDARDS CONTINUED

Architectural Projections and Focal Elements such as Towers, Cupolas, and other ⁽³⁾	55'
Porte-Cocheres ⁽⁴⁾	1 story
Other Development Regulations	Per Article 16, City of Ontario Dev. Code.
• Walls, Fences & Hedges	Per Article 16, City of Ontario Dev. Code. When a non-residential use adjoins a residential district, a 6-foot high masonry wall shall be required at the interior side or rear property lines.
• Parking	Per Article 30, City of Ontario Dev. Code.
• Screening	All loading areas shall be screened from adjacent public streets, residential, and open space uses through the use of landscaping, earthen berms, decorative walls, or fencing. All storage, including cartons, containers, materials, or trash shall be shielded from view within a building or area enclosed by a solid fence or wall not less than 6' in height
	All roof-mounted equipment, including but not limited to, mechanical equipment, satellite dishes, tanks, ducts, towers, and all equipment appurtenant thereto, shall be screened on all sides from public view from the street adjoining properties, and neighboring residential units, by a parapet wall, decorative enclosure, or other architectural element. Equipment screening shall appear as an integral part of the building architecture.
• Lighting	All interior and exterior lighting shall be directed away from residential and open space uses.

1. All setbacks areas shall be landscaped.
2. All setbacks are measured to habitable area not architectural appurtenance or projection. An architectural projection is defined as an element that articulates the building elevation such as eaves, window and door pop-out surrounds, bay windows, pot shelves, chimneys, enhanced window sills, shutter detail, window trim, and balconies, and other similar elements. Such elements may project a max. of 3' into setback areas. Building setbacks shall be determined by the hierarchy of streets (major arterial and local streets) and measured from PL for local streets and RW for major arterials.
3. Architectural element only, not to be used for signage, subject to Planning Director approval. Within commercial areas, towers and vertical projections are limited in size to 10% of the total building square footage.
4. Porte-Cocheres shall be open on three sides.



Conditionally Permitted Uses

- ✦ Alcoholic Beverage Sales;
- ✦ Commercial Day Care Facility (including Adult Day Care);
- ✦ Banks, Credit Unions and Other Depository Institutions;
- ✦ Church;
- ✦ Money Transmitting;
- ✦ Other Financial Services;
- ✦ Police/Fire Station;
- ✦ Recreation Center; and
- ✦ Senior Citizen Center

Determination of Use

Uses not specifically listed may be deemed permitted subject to a “determination of Use” as provided for in Section 9-1.1310 of the City of Ontario Development Code.

Design Guidelines

Prior to the approval of the first site development permit and/or parcel map within the Community Commercial land use area, a detailed set of design guidelines for the area shall be submitted by the developer and approved by the City. These guidelines shall be compatible with the residential architectural styles in this Specific Plan and discretion of the City.

6.16 LANDSCAPE STANDARDS

6.16.1 General Provisions

- ✦ All landscape plans, streetscape plans and graphic designs which create neighborhood identity or enhance entry monuments shall conform to the design guidelines and regulations as set forth herein and shall be subject to review and approval by the City of Ontario;
- ✦ The landscape/streetscape improvements for the Specific Plan shall establish a landscape theme reminiscent of the regional landscape character of the surrounding area;
- ✦ Installation of landscaping within the front yards of single-family detached housing products will be provided by the home builder. Front yard landscaping shall be installed prior to Certificate of Occupancy;
- ✦ Installation of landscaping in residential areas of Park Place may be provided by the home builder, based on future landscape plans submitted to the City during site plan review; and

- ✦ Installation of landscaping within the commercial area of the Specific Plan will be provided by the developer/builder.
- ✦ All loading areas shall be screened from adjacent public streets, residential, and open space use through the use of landscaping, earthen berms, and/or decorative walls or fencing. All storage including cartons, containers, materials, or trash shall be shielded from view within a building or area enclosed by a solid fence or wall not less than six feet in height.

6.16.2 Landscape Standards

- ✦ Landscaping within the Specific Plan shall be provided in accordance with the Design Guidelines for Streetscapes and Entries utilizing plant materials specified on the Plant Palette included in *Chapter 7, Residential Design Guidelines, and Chapter 8, Commercial Design Guidelines*, established for the Specific Plan;
- ✦ Boundary landscaping will be required adjacent to residential, commercial, park, and school areas. Landscaping shall generally be placed along the entire property line;
- ✦ Landscaping and irrigation systems within the public rights-of-way of the Specific Plan area shall be installed by the developer;
- ✦ Freestanding perimeter walls and view fencing shall be provided within and at the perimeter of the project as specified in the *Wall and Fence Master Plan, Chapter 7, Residential Design Guidelines*. Such walls and fences will be constructed concurrently with the construction of improvements required for development of the neighborhoods of the Specific Plan;
- ✦ Walls and Fencing—Perimeter walls shall be constructed in locations and of a design consistent with the Residential Design Guidelines and the Wall and Fence Master Plan, and Wall Details, provided in Chapter 7, Residential Design Guidelines, and shall not exceed six feet (6) in height from finished grade. Perimeter walls may exceed six feet (6) in height if required for sound attenuation. Perimeter walls shall be constructed of either decorative masonry (both sides) or other permanent, durable, low maintenance material subject to approval by the City;



- ♦ Residential—Side and rear yard walls and fencing shall not exceed six (6) feet in height from the highest adjacent finished grade. Walls and fencing within the residential front yard setback area shall not exceed three feet (3) in height. Walls shall be decorative masonry construction (both sides) or other permanent low maintenance materials. Front yard fences may be constructed of attractive, durable materials, including, but not limited to, wrought iron with pilasters, textured concrete block, formed concrete with reveals, or other materials approved by the Planning Director. View fencing may be of a decorative wrought iron, glass panels, or other durable material subject to approval by the City;
- ♦ All perimeter wall and fence materials throughout the Specific Plan will be of uniform manufacture with colors specified for the overall design theme;
- ♦ All interior walls (both sides) that are visible from the public street shall be constructed of attractive, durable materials, including, but not limited to, wrought iron with pilasters, textured concrete block, formed concrete with reveals, or other materials approved by the Planning Director. Colors shall be consistent with the colors specified for the overall design theme and shall be decoratively capped. Decorative all-weather vinyl fencing is not permitted in the Specific Plan area;
- ♦ The developer will provide site inspection of all construction and installation of open space areas in accordance with City of Ontario requirements;
- ♦ Parking lot landscaping within commercial areas shall be required in accordance with Section 9-1.3040 of the City's Development Code; and
- ♦ Non-toxic and non-invasive vegetation shall be utilized adjacent to all public open space areas.
- ♦ All landscaping features shall comply with City of Ontario requirements for sight distance.

6.17 SIGNAGE

A Master Sign Program shall be submitted by the developer(s) of each land use component for the Specific Plan and approved by the City of Ontario pursuant to Article 31 of the City's Development Code to address residential project entries, residential neighborhood identification signs, commercial center identification, tenant signage, and "way-finding" signs within the Specific Plan area. All other signs shall be subject to the approval of a sign permit pursuant to the City's Development Code. All traffic-related signs shall conform to City of Ontario Traffic and Transportation Design Guidelines and California Manual of Uniform Traffic Control Devices (MUTCD), latest editions.

6.17.1 Master Sign Program Contents

All sign programs shall address, at a minimum, the following:

- ♦ Permitted signs;
- ♦ Prohibited signs;
- ♦ The hierarchy of signage; and
- ♦ Definition of types of signs.
- ♦ Locations and dimensions for monument signs, neighborhood identification signs, and public facilities signs;
- ♦ Locations and dimensions of directional signage;
- ♦ Provision for size, location, and duration of display of temporary signs;
- ♦ Permitted sign types, styles, construction materials, colors, and lettering styles;
- ♦ Requirements for a sign permit application;
- ♦ Procedures for obtaining approval of a sign permit; and
- ♦ Procedures for amendments to the sign program.

6.18 PRIVATE RECREATIONAL FACILITIES

This category includes the development of all private recreation facilities and is subject to the Development Permit process contained herein.



7. Residential Design Guidelines





7.0 Residential Design Guidelines

7.1 INTRODUCTION

Subarea 29 is a new residential master-planned community located within the New Model Colony in Ontario, California. The historical character of this area is typically rural and agricultural in nature. However, being located in the path of the advancing railroad, the later automobile Route 66, and eventually Ontario International Airport, Ontario has also typically been associated with transit and forward-moving progress.

The proposed community design integrates the historic character of the past with a plan which provides a variety of housing choices that serve the needs and lifestyles of today.

Upon entering the community, residents and visitors arrive at a central community core consisting of a school and community park. Connections to both community and regional trails are accessed from this core area. With a focus on promoting a pedestrian friendly, healthy lifestyle this core area is within approximately ¼ mile walk from all neighborhoods in the community.

Community Overview

Community design elements include:

- ♦ A central village core consisting of a school, parks, and private recreation center;
- ♦ A wide variety of home sizes and plans to fulfill the needs of different life stages;
- ♦ Tree-lined neighborhood streets with curb separated sidewalks and generous plantings in parks and open spaces;
- ♦ Neighborhood parks and a private recreation center to support physical and social activities within the community;
- ♦ A network of sidewalks and trails that link all neighborhoods to schools, parks, open spaces, a regional trail system and nearby retail; and
- ♦ Nearby connections to local, regional and global transit.

The community character is inspired by:

- ♦ The charm and tradition of Ontario's historic downtown and the existing neighborhood's eclectic mix of architectural styles;
- ♦ The simple, yet stately landscape along Euclid Ave. and the region's agricultural heritage; and
- ♦ Environments that encourage wellness by providing opportunities for pedestrian and outdoor physical activity.

Community retail goods and services are conveniently located in a planning area at the Northwest corner of the Specific Plan at the intersection of Eucalyptus Ave. and Archibald Ave.

The Specific Plan community entries along Archibald Ave., Haven Ave., and Merrill Ave. will be identified with community character monuments and entry statements. Signage, lighting, and monuments within Park Place will reflect an identifiable community character.

Upon arrival at the primary Merrill Ave. entry, from Archibald Ave., residents and visitors are greeted by the central "village core;" a private community recreation center and two (2) neighborhood parks frame the lane loaded lot homes on the south side and an elementary school and lane loaded homes on the north side. These elements are all easily accessed from all neighborhoods by way of pedestrian, bicycle, and vehicular routes.

Farther into the community, Merrill Ave. crosses through a green space over the SCE Easement before connecting with Haven Ave. to the east. Circulation throughout the community is organized by a network of interior streets with curb separated sidewalks framed by street trees and varied architectural styles that reflect the chosen community character.

7.1.1 Guiding Principles

The following guiding principles have been created to help develop architecture that reflects quality in design, simplicity in form and plan and contributes charm and appeal to the neighborhoods of Subarea 29 as a whole.

- ♦ The home, not the garage, is the primary focus of the front elevation. A variety of garage placements emphasizes the pedestrian environment as opposed to the automobile;
- ♦ A variety of compatible architectural styles are provided to ensure a degree of individuality throughout neighborhoods;
- ♦ Appropriate massing and roof forms play a major role in defining architecture styles, roof framing related to each plan versus individual elevations;
- ♦ Architectural elements, details, and appropriate color palettes are used to reinforce individual architectural styles;
- ♦ Rear elevations visible to the public viewshed should provide articulated walls, windows, and varied roof profiles, all defining level of quality in detail and interest in neighborhood architecture; and
- ♦ Selected architectural styles are related to those historically represented in Ontario. These are: Craftsman, Cottage, American Colonial, Farmhouse, American Traditional, Spanish Colonial, and Andalusian.



7.2 ARCHITECTURAL STYLES

7.2.1 Introduction

The style information on the following pages provides the builder and design consultants with the tools to create functional and attractive architectural designs without using gimmicks or sacrificing the integrity of the style.

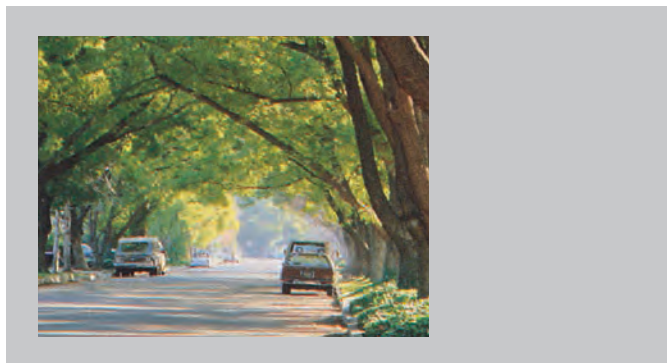
Authenticity

The design criteria in these guidelines is offered to encourage a high quality of architecture and reasonable level of authenticity of styles through the use of appropriate elements. Although detail elements may be used to further convey the character of a style, the overall massing and appropriate roof forms should be used to establish a recognizable style. Proper scale and proportion of architectural elements and appropriate choice of details are all factors in achieving authenticity.

7.2.2 Architectural Philosophy

Looking back to Ontario's older, established neighborhoods for inspiration, the design intent for Subarea 29 is to re-capture the charm and essence of these original home styles and express them in the simple, honest manner that they originated. These homes reflected function and tradition, simplicity in massing, plan and roof forms, and authentic, regional styles.

Homes will provide the kind of function, quality, value, and appeal that were built into those original homes. Neighborhood parks, sidewalks, parkways, and street trees will add to the traditional feeling in these neighborhoods, linking them with the overall community.



Design Character

The design character of the neighborhoods will be one of hometown appeal characterized by simple, yet charming homes, curb separated sidewalks, and parkways lined with street trees. Homes and street scenes will reflect an overall feeling of quality and tradition. The architectural palette of styles has a regional historic background and context in keeping with the character of Ontario.

7.2.3 Architectural Styles

Architectural Style/Plan Mix

An important goal in this community is to develop varied and interesting streetscenes. In order to achieve this, the following architectural style and plan requirements shall be met for all Lane Loaded and Conventional Small, Medium, and Large Lot residential development products:

In each neighborhood that consists of 0-80 homes, provide:

- A minimum of three (3) plans;
- A minimum of three (3) elevations per plan using a minimum of two (2) significantly different styles per plan; and
- A minimum of four (4) different color schemes per number of plans.

In each neighborhood that consists of 81-100 homes, provide:

- A minimum of four (4) plans;
- A minimum of three (3) elevations per plan using a minimum of three (3) significantly different styles per plan; and
- A minimum of four (4) different color schemes per number of plans.

Cluster Home Style/Plan Mix

The development of Cluster Homes generally results in the same 1 or 2 plans being repeated along the street front. To support varied neighborhood streetscenes, each Cluster Home residential product neighborhood shall provide:

- A minimum of three (3) significantly different styles per plan. Although the styles shall be significantly different, they shall be coordinated to provide a harmonious and cohesive neighborhood preferably from a family of related architectural styles; and
- A minimum of four (4) different color schemes per number of plans.

Number of plans and elevations shall not be regulated for interior units; the above standards shall apply to any unit plotted along (facing or siding on to) a street more than one (1) time.



Architectural Styles



Farmhouse



Cottage



American Traditional



American Colonial



Craftsman



Spanish Colonial



Andalusian

Specific Plan Style Selection

Builders will select from the following palette of architectural styles:

- ♦ Farmhouse;
- ♦ Cottage;
- ♦ American Colonial;
- ♦ American Traditional;
- ♦ Craftsman;
- ♦ Andalusian; and
- ♦ Spanish Colonial.

The “common features” for each architectural style are displayed on the following pages. These elements will assist in defining the character of each one.

As future phases of the Subarea 29 Specific Plan are developed, architectural styles may be added or deleted provided that they are compatible with the overall style palette and approved by the City of Ontario.



Farmhouse



The American Farmhouse represents a practical and picturesque country house. Its beginnings are traced to both Colonial styles from New England and later the Mid-West. As the American Frontier moved westward, the American Farmhouse style evolved according to availability of materials and technological advancements, such as balloon framing.

Predominant features of the style are large front and/or wrapping porches with a variety of wood columns and railings. Two story massing, dormers, and symmetrical elevations occur most often on the New England Farmhouse variations. The asymmetrical, casual cottage look, with a more decorated appearance, is typical of the Midwest American Farmhouse. Roof ornamentation is a characteristic detail consisting of cupolas, weather vanes, and dovecotes.



FULL TO PARTIAL WIDTH FRONT PORCH



MULTI-PANE BAY WINDOW
HORIZONTAL LAPPED SIDING



SIMPLE WOOD COLUMNS & WOOD RAILINGS

Farmhouse

Element	Common Features
<i>Form</i>	<ul style="list-style-type: none"> † Simple plan form massing and simple roof design
<i>Roof</i>	<ul style="list-style-type: none"> † 6:12 to 12:12 roof pitch, possibly lower pitch at main roof † Front to back main gable roof † 12" overhangs with open eaves † Concrete tiles with smooth flat or shingled appearance
<i>Walls</i>	<ul style="list-style-type: none"> † Horizontal siding may be combined with stucco † All material changes must occur at an inside corner or another logical location
<i>Windows</i>	<ul style="list-style-type: none"> † Vertical multi-paned windows or use of an accent window appropriate to style, such as: bay windows or single hung windows at front elevation † Multi-paned windows or inserts on side and rear elevations in high visibility public view areas † Vinyl wrapped windows
<i>Details</i>	<ul style="list-style-type: none"> † Porches with simple wood or shaped wood columns and wood railings † Minimum 2' x 4' window and door trim - wood on siding, foam on stucco † All surface mounted fixtures on front elevations must complement architectural style (i.e. use of some style appropriate details such as: wood pot shelves, louvered attic vents, cupolas or dormers). † Stucco or siding chimney wrap † Garage door patterns and colors to complement style
<i>Colors</i>	<ul style="list-style-type: none"> † Field: Whites or light tinted colors † Trim: Whites or light shades complementary to field color † Accents: Light or dark shades in contrast with field color

Note: Enhancements are required at corners and second-story elevations seen from public views.



Cottage



Cottage is a picturesque style derived from medieval Norman and Tudor domestic architecture. The resulting English and French inspired “cottage” became extremely popular nationwide after the adoption of stone and brick veneer techniques in the 1920s.

Although the cottage is looked upon as small and not costly, it was recognized as one of the most popular styles in suburban America. The design of the home was reflected in the rural setting that they evolved in. Established neighborhoods in Riverside, Orange, and Santa Ana contained many homes of charm and character that depicted the alluring, yet unpretentious lifestyle of the Cottage home.



VARYING ROOF SLOPES & FORMS



FEATURE WINDOWS & GABLE ACCENTS



STUCCO WITH STONE ACCENTS

Cottage

<i>Element</i>	<i>Common Features</i>
<i>Form</i>	<ul style="list-style-type: none"> † Rectangular plan form massing with some recessed second floor area
<i>Roof</i>	<ul style="list-style-type: none"> † Main roof hip or gable with intersecting gable roofs † Secondary roof with either a curved slope or steeper pitch than main roof † 6:12 to 12:12 roof pitch, possibly lower pitch at main roof † 0" to 12" overhangs - (limit tight overhangs to distinguishing character elements) † Architectural quality asphalt shingles or smooth flat concrete tiles
<i>Walls</i>	<ul style="list-style-type: none"> † Stucco, wood siding, or stone accents
<i>Windows</i>	<ul style="list-style-type: none"> † Vertical multi-paned windows at front elevation † Multi-paned windows or inserts on side and rear elevations in high visibility public view areas † Vinyl wrapped windows † Use of an accent window where appropriate to style, such as: curved or round top accent windows, bay windows or single hung windows at front
<i>Details</i>	<ul style="list-style-type: none"> † Entry accents with natural or manufactured stone † Shutters † Simple wood 2x window and door trim - wood on stone or siding, foam on stucco † All surface mounted fixtures on front elevations must complement architectural style. † Garage door patterns and colors to complement style
<i>Colors</i>	<ul style="list-style-type: none"> † Field: Whites or light tinted colors † Trim: Whites or light shades complementary to field color † Accents: Light or dark shades in contrast with field color

Note: Enhancements are required at corners and second-story elevations seen from public views.



American Traditional



The American Traditional style evolved in the early twentieth century being influenced by American Colonial styles as formal as Georgian Revival and those as simple and functional as Cape Cod, New England Colonial, and Farmhouse.

With respect to East Coast traditions and regional culture, the Colonial Revival vocabulary of white-painted columns, clapboards, and shuttered windows were combined with the less symmetrical, unpretentious massing, and functional forms of the Cape Cod and Farmhouse. Combined one and two-story massing with single story wings, gabled roofs with dormered windows, or occasional saltbox roof forms were favored solutions, later becoming classic variations of the traditional style. These inspirations yielded a truly American inspired architecture for a more relaxed, yet still dignified style that would become the new American dream.



CLOSED PEDIMENT AND SHUTTERED WINDOWS



BRICK SIDING AND VERTICAL MULTI-PANED WINDOWS



VENTED GABLE AND HORIZONTAL SIDING

American Traditional

<i>Element</i>	<i>Common Features</i>
<i>Form</i>	<ul style="list-style-type: none"> † Asymmetrical massing with a vertical and a horizontal break
<i>Roof</i>	<ul style="list-style-type: none"> † 5:12 to 12:12 roof pitch, possibly lower pitch at main roof † 16" to 24" overhangs † Concrete roof tile - flat or shake appearance † Front to back gable or hip roof with intersecting hip or gable roofs
<i>Walls</i>	<ul style="list-style-type: none"> † Horizontal siding - may be combined with stucco † All material changes must occur at an inside corner or another logical location
<i>Windows</i>	<ul style="list-style-type: none"> † Vertical multi-paned windows or use of an accent window where appropriate to architectural style, such as: round top accent or bay windows at front elevations † Multi-paned windows or inserts on side and rear elevations in high visibility public view areas † Vinyl wrapped windows
<i>Details</i>	<ul style="list-style-type: none"> † Simplified cornice trim at gable ends † 2x6 window and door trim - wood on siding, foam on stucco † All surface mounted fixtures on front elevations must complement architectural style (i.e. use some style appropriate details such as: header window wood accent trim, full porches or balconies, square columns with trim, window pot shelves, brick veneer chimney wrap) † Garage door patterns and colors to complement style
<i>Colors</i>	<ul style="list-style-type: none"> † Field: Whites, off-white, dark, or light colors † Trim: White or contrasting with field color † Accents: White, light, or dark colors in contrast or harmony with field color

Note: Enhancements are required at corners and second-story elevations seen from public views.



American Colonial



This classic American style descended directly from the first homes built in the New England colonies in the 17th century. Their beginnings were as small and unpretentious as the one story saltbox, favoring the cultures and traditions of the settlements.

As living functions became more defined and prosperity increased, so did the need for additional space. Second stories with overhangs, dormers, and gabled roof forms became favored solutions, later evolving into classic elements of this traditional American style. Wood shutters and an enhanced entry element and/or trim are the finishing details for an otherwise simple and functional form.



VERTICAL MULTI-PANED WINDOWS WITH SHUTTERS



ACCENT WINDOW AND ENHANCED MOLDING DETAILS



ARTICULATED PORTICO ENTRY

American Colonial

<i>Element</i>	<i>Common Features</i>
<i>Form</i>	<ul style="list-style-type: none"> † Simple plan form and massing; or shed or pitched roof dormers
<i>Roof</i>	<ul style="list-style-type: none"> † 5:12 to 12:12 roof pitch, possibly lower pitch at main roof † 12" to 18" overhang † Simple front to back gable roof † Architectural quality asphalt shingles or shingle texture flat concrete tiles
<i>Walls</i>	<ul style="list-style-type: none"> † Horizontal siding or brick may be combined with stucco. All material changes must occur at an inside corner or other logical location
<i>Windows</i>	<ul style="list-style-type: none"> † Vertical multi-paned windows or use of an accent window where appropriate to architectural style such as: bay windows, dormer window in roof at front elevations † Multi-paned windows or inserts on side and rear elevations in high visibility public view areas † Vinyl wrapped windows
<i>Details</i>	<ul style="list-style-type: none"> † Neoclassic columns or posts at entry † Simplified cornice trim at gable ends † 2 x 6 window and door trim - wood on siding, foam on stucco † Enhanced entry element such as portico or covered porch † All surface mounted fixtures on front elevations must complement architectural style (i.e. use of some style appropriate details such as: knee braces at dormer, wood porch columns and rails, shutters, enhanced window trims, and louvered attic vents) † Garage door patterns and colors to complement style
<i>Colors</i>	<ul style="list-style-type: none"> † Field: Pastels, whites, light earth tones, off-whites, or dark earth tones † Trim: Light or dark in contrast to field color † Accents: Whites, light tones, or medium to dark tones in contrast to field, grayed or dark greens, blues, reds, or black

Note: Enhancements are required at corners and second-story elevations seen from public views.



Craftsman



The Craftsman style was inspired by the English Arts and Crafts Movement of the late 19th century. Of the utmost importance was that all exterior and interior elements received both tasteful and “artful” attention. The movement influenced numerous California architects such as Green and Green, and Bernard Maybeck.

The resulting Craftsman style responded with extensive built-in elements and by treating details such as windows or ceilings as if they were furniture. The style was further characterized by the rustic texture of building materials, broad overhangs with exposed rafter tails at the eaves, and trellises over the porches. The overall affect was the creation of a natural, warm and livable home. In Southern California, the Craftsman style spun out of bungalows that were the production home of the time.



FRONT PORCH WITH BATTERED COLUMNS



GABLED ROOF WITH DEEP OVERHANG



VERTICAL MULTI-PANED WINDOWS

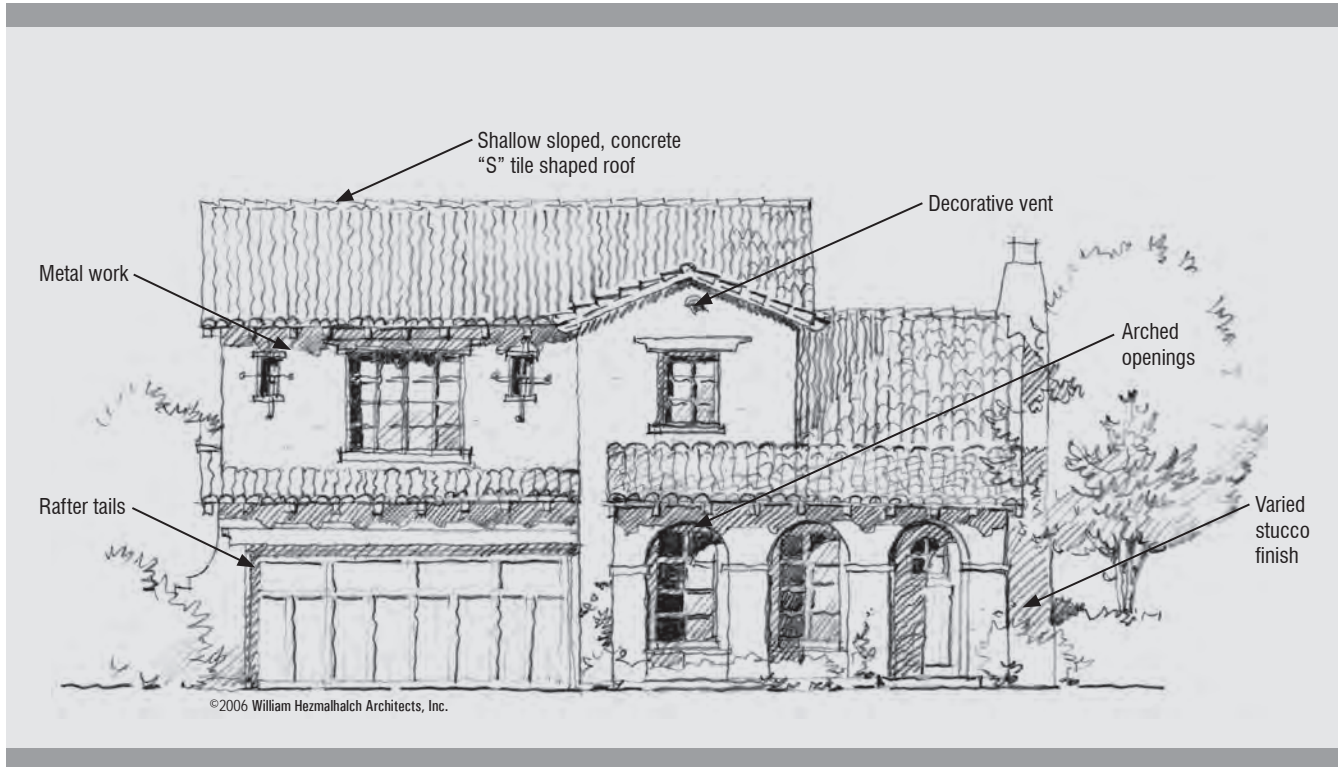
Craftsman

<i>Element</i>	<i>Common Features</i>
<i>Form</i>	<ul style="list-style-type: none"> † Simple 2 story boxed massing with vertical and horizontal breaks
<i>Roof</i>	<ul style="list-style-type: none"> † 4:12 to 5:12 roof pitch, possibly lower pitch at main roof † 16" to 24" overhangs † Flat concrete tile - shingle appearance † Basic gabled roof - side to side with cross gables
<i>Walls</i>	<ul style="list-style-type: none"> † Horizontal siding or stone base accents may be combined with stucco. All material changes must occur on inside corner or other logical location
<i>Windows</i>	<ul style="list-style-type: none"> † Vertical multi-paned windows or use of accent window where appropriate to architectural style such as: single hung windows at front elevation, feature ribbon windows (3 or more) at front elevation † Multi-paned windows or inserts on side and rear elevations in high visibility public view areas † Vinyl wrapped windows † Use windows individually or in groups
<i>Details</i>	<ul style="list-style-type: none"> † Entry or full porches with heavy square columns or posts on stone or brick piers † Shaped wood header trim at windows and doors - wood on siding, foam on stucco † Simple or triangulated knee brace † All surface mounted fixtures on front elevations must complement architectural style † Garage door patterns and colors to complement style
<i>Colors</i>	<ul style="list-style-type: none"> † Field: Light to dark earth tones † Trim: Light to dark earth tones in harmony or contrasting with field color † Accents: Light to dark earth tones in contrast with field color

Note: Enhancements are required at corners and second-story elevations seen from public views.



Spanish Colonial



Spanish Colonial, also known as Spanish Eclectic is an adaptation of Mission Revival enriched with additional Latin American details and elements. The style attained widespread popularity after its use in the Panama-California Exposition of 1915.

The simple courtyards of the Spanish Colonial heritage with hanging pots, a flowering garden, and sprawling shade trees are hardly surpassed as foreground design elements. Further architectural distinction was established through the use of tile roofs, stucco walls, heavily textured wooden doors and highlighted ornamental ironwork.

Key features of this style were adapted to the Southern California locale. The plans were informally organized around a courtyard with the front elevation very simply articulated and detailed. The charm of this style lies in the directness, adaptability, and contrast of materials and textures.



FEATURE WINDOW



TILE ROOF, GABLE VENT AND WROUGHT IRON ACCENTS



STUCCO WALLS WITH ARCHED OPENINGS

Spanish Colonial

Element	Common Features
<i>Form</i>	<ul style="list-style-type: none"> † 2 story massing with strong one-story element
<i>Roof</i>	<ul style="list-style-type: none"> † 4:12 to 5:12 roof pitch, possibly lower pitch at main roof † 12" to 18" overhang † Simple hip or gable roof with one intersecting gable roof † Barrel or 'S' shape concrete tiles
<i>Walls</i>	<ul style="list-style-type: none"> † Stucco
<i>Windows</i>	<ul style="list-style-type: none"> † Vertical multi-paned windows or use of an accent window where appropriate to architectural style such as: feature recessed arched window, accent beveled glass recessed window, single or grouped round top windows at front elevations † Multi-paned windows or inserts on side and rear elevations in high visibility public view areas † Vinyl wrapped windows † Simple 2x window and door trim - wood or stucco over foam
<i>Details</i>	<ul style="list-style-type: none"> † Stucco over foam window and door trim † Arched stucco column porches † All surface mounted fixtures on front elevations must complement architectural style (i.e. some style appropriate details such as: wrought iron balconies and accent details and shaped rafter tails at feature areas † Garage door patterns and colors to complement style
<i>Colors</i>	<ul style="list-style-type: none"> † Field: Whites or off-whites † Trim: Dark browns † Accents: Deep jewel tones of green, blue, red (on shutters, door, balcony trim)

Note: Enhancements are required at corners and second-story elevations seen from public views.



Andalusian



The Andalusian style is an adaptation of the farmhouses and cottages found in the rural areas throughout Andalusia, in the South of Spain. Enriched with details and elements derived from Moroccan, Moorish, and classical Roman architecture this style represents a simple, yet elegant blend of cultures.

The informality of rural life originally inspired a simple, rectangular form that evolved organically over time to meet the spatial needs of the owner. Subsequent massing was typically similar in form, but changed in dimension with new functions and arrangement of spaces. The resultant irregularity in massing and appearance adds to the charm and romance of this southern European style. Its informal rustic character is highlighted with sophisticated details, natural, indigenous materials, warm, sun-bleached colors, and rich textures.

Architectural distinction is established through the use of details such as softly sculpted or curved wall and chimney articulation, colorful mosaics and tile work, rustic stone accents, balconies, perforated wall patterns and vents, cantilevered windows, classical elements, arches, and/or wrought iron grillwork accents.



FEATURE WINDOWS AND SIMPLE MASSING



VERTICAL WINDOWS WITH WROUGHT IRON DETAIL



WELL ARTICULATED ENTRY

Andalusian

<i>Element</i>	<i>Common Features</i>
<i>Form</i>	+ 2 story massing with one story elements at courtyards and/or entries
<i>Roof</i>	+ 4:12 to 5:12 roof pitch, possibly lower pitch at main roof + 12" to 18" overhang + Simple hip or gable roof + Barrel or 'S' shape concrete or clay tiles
<i>Walls</i>	+ Stucco finish
<i>Windows</i>	+ Vertical multi-paned windows or use of an accent window where appropriate to architectural style such as: feature recessed arched window, accent beveled glass recessed window, single or grouped round top windows at front elevations + Multi-paned windows or inserts on side and rear elevations in high visibility public view areas + Vinyl wrapped windows + Simple 2x window and door trim - wood or stucco over foam
<i>Details</i>	+ Arcades supported by classic columns at courtyards, balconies, and porches + Enhanced articulation at entrances and feature windows + Classical and/or Moorish details + Wood or wrought iron railings at balconies or porches + Garage door patterns and colors to complement style
<i>Colors</i>	+ White or beige tones on field color + Dark brown or jewel toned trim

Note: Enhancements are required at corners and second-story elevations seen from public views.



7.3 MASSING

7.3.1 Introduction

Creating street scenes that function well and have visual interest is a primary community objective. The following basic elements and criteria are intended to develop variations in appearance and a sense of individuality for each home. Neighborhoods that have nearly identical homes and streets without variation in product placement and form will not be approved in design review. This section lists architectural massing and plotting techniques that will help to create a quality environment.

Objectives:

- ♦ Minimize visual impact of garages;
- ♦ Give attention to composition of building mass;
- ♦ Incorporate single-story elements in two-story buildings;
- ♦ Vary setbacks at porches, living and garage areas;
- ♦ Minimize two-story dominance on streetscene, sidewalks and open spaces;
- ♦ Design with sensitivity to corner lot conditions;
- ♦ Use appropriate transition of scale; and
- ♦ Use four-sided elevation design.

7.3.2 General Elements

The general elements of building massing include:

- ♦ Front articulation;
- ♦ Roof form;
- ♦ Garage placement;
- ♦ Balconies and projections;
- ♦ Rear articulation;
- ♦ Variable setbacks; and
- ♦ Corner lots/critical edge conditions.

Neighborhood Massing and Plotting Concepts

To provide more interesting neighborhood streetscenes, varied massing on all elevations is encouraged.

Where possible when plotting, refrain from strict compliance to minimum garage setbacks to avoid contributing to repetitious street scenes. Typically, plans are to be reversed and plotted so that garages and entries are adjacent to each other to create an undulating setback. Occasionally, this pattern should be broken to avoid monotony. Additionally, no more than two (2) of the same plan/elevations shall be plotted next to each other or directly across the street from one another.

Simple Home Design

In an attempt to design aesthetically pleasing architecture that is economically feasible and functions well the concept of “The Simple House” is to be implemented. This suggests that starting with simple structural forms and building masses and appropriate roof forms can lead to the development of an “authentic” style while maintaining acceptable building costs. In order to achieve this the following basic criteria have been established:

- Design “simple,” attractive homes using simple plan forms and style-appropriate massing and roof pitches;
- Use style-appropriate elements and details to enhance good architecture;
- Provide a variety of single-story heights and profiles while stepping back second story massing where appropriate;



SINGLE STORY ELEMENTS



- Acknowledge both street front facades on corner homes or buildings by ‘wrapping’ usable spaces, architectural elements & materials around to face the side street frontage;
- Give added attention to side and rear articulation on all building facades;
- Use covered and shaded spaces such as porches, landscaped courtyards or structural landscape elements to provide a sense of entry and respond to local climate conditions; and
- Respond to overall community character with appropriate choices of style, color and materials.

Refer to sections 7.2, 7.3, and 7.4 of this document for specific guidelines concerning styles, massing, elements and details of simple residential architectural design.

Single Story Elements

Single-story elements are encouraged on all homes to help establish neighborhood pedestrian scale and add variety to the street scene. A minimum of 33% of homes in each neighborhood shall have at least one (1) significant single-story element on an exposed front or side elevation.

Significant single-story elements include:

- Front or wraparound porch (minimum 7’ deep and 10’ wide);
- Roofed porte cochere;
- Single-story living space in conjunction with a second story recess of at least 5’; and
- Pop-out gable element - enclosed or open.

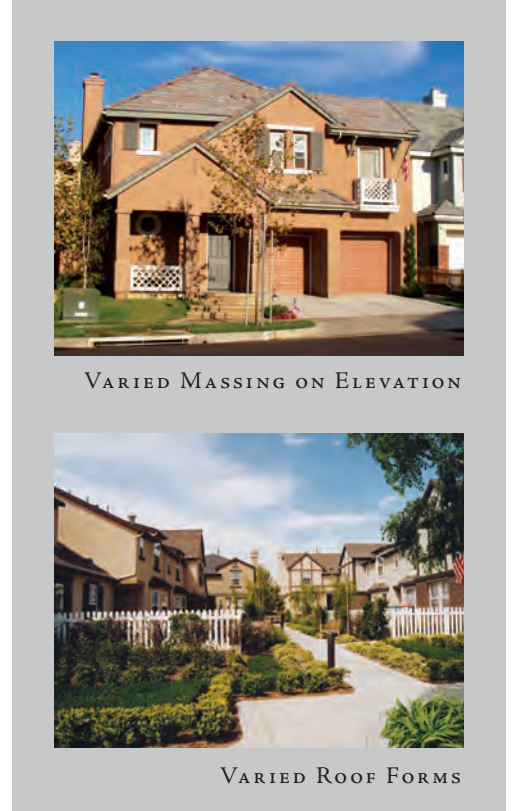
In neighborhoods with required lot sizes of less than 5,000 square feet, this requirement may be satisfied with the addition of a one-story porch or other elements listed above.

In neighborhoods with required lot sizes of 5,000 square feet or greater, this requirement shall be satisfied with an enclosed single-story living space in conjunction with a second story recess of at least five (5) feet.

Where forward garages are used, single story massing is encouraged.

7.3.3 Front Articulation

The appearance of the street scene and collection of homes within it is typically the first impression of any neighborhood. Most homes are seen primarily from the front and represent the transition from public to private space as well as the character of those who dwell within .



With this in mind it is important that the front of the home, and any sides exposed to the street receive thoughtful design and articulation to bring character and quality to the individual home and to the neighborhood as a whole. The following guidelines will help to achieve that level of articulation:

- Design neighborhood plan packages with forms and front elevations in mind to work together in controlling the impact of buildings on the street scene;
- Provide variety in form and massing to reflect interior uses and achieve attractive and interesting elevations;
- Design placement of windows to work with interior uses and provide “eyes on the street;”
- Consider the entry as a focal point when designing the front elevation; and
- Provide an appropriate level of detail and trim at entry, doors and windows to enhance good design.



7.3.4 Roof Forms

Rows of homes seen from a distance or along arterial roads are perceived by their contrast against the skyline or background. The dominant impact is the shape of the building and roof line. Articulate the building mass and roof lines to express a variety of conditions and minimize the visual impact of repetitious flat planes, similar building silhouettes and similar ridge heights. This can be achieved by designing a discernibly different roof for each home plan. Individual roof plans may be simple but between different plans should exhibit variety by using front to rear, side-to-side, gables, hipped roofs, and/or by the introduction of one-story elements.

7.3.5 Garage Placement & Treatment

Create attractive and comfortable street scenes and pedestrian spaces by de-emphasizing garages, implementing “architecture forward,” and encouraging “corner homesites.”

A. Street Facing Garages

The impact of repetitive, street-front garages can be reduced by using the following techniques:

- ✦ Vary garage placement mix within neighborhood plotting plan;
- ✦ Vary garage door appearance;
- ✦ Vary garage placement; and
- ✦ Provide optional treatments that occur forward of the garage to buffer the view impact of garages and garage doors.

Vary garage placement with-in neighborhood plotting plan

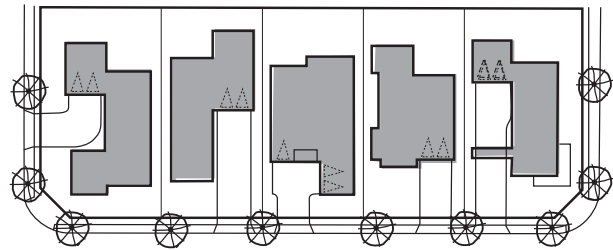
Vary placement whenever possible by alternating front setbacks. Alternate plans with different garage locations when plotting adjacent homes.

Vary garage door appearance

Vary garage door pattern, windows and/or color as appropriate to individual architectural styles. Where possible, use two (2) single doors instead of a large one. Additional treatments are encouraged to buffer direct view of the garage door.

Vary garage placement from plan to plan

The following are examples of garage placements in each Conventional Small, Medium, and Large Lot neighborhood (Cluster Home and Lane Loaded neighborhoods are exempt). The intent is to limit the impact of street facing garages and vary garage placement (i.e. forward, shallow, deep recessed) within each neighborhood.



SIDE-ON CORNER PLAN DEEP RECESSED FORWARD SIDE-ON SHALLOW RECESSED DEEP RECESSED CORNER PLAN

VARIED GARAGE PLACEMENT—STREET FACING



VARIED GARAGE DOOR APPEARANCE



1. Shallow-Recessed Garage

All front facing, shallow-recessed garages shall be located a minimum of five (5) feet behind the front elevation/living space and a minimum of twenty (20) feet from the property line. Homes with shallow recessed garages shall be limited to 25 percent of the total number of units in each builder's plan package; 50 percent of units may be permitted with shallow-recessed garages if front porches are provided on those plans. Porches shall have a minimum depth between 5 to 7 feet (Subject to Planning Department review and approval).

2. Mid-Recessed Garage

Mid-recessed garages are located ten (10) feet or more behind the front elevation, living space, or front porch feature.

3. Deep-Recessed Garage

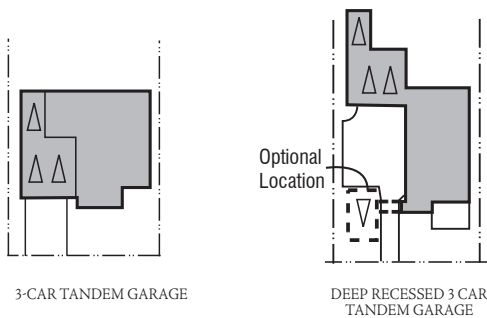
Deep-recessed garages are located twenty (20) feet or more behind the front elevation, living space, or front porch feature. These may be attached or detached.

4. Corner Lot with Side-street Entry Garage

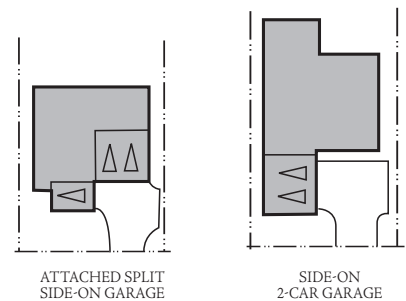
This garage placement allows the option of entering from the side street, thereby eliminating the garage and driveway from the front face of the house.

5. Garage Forward

This garage placement includes front-facing or swing-in access. It applies to any garage located forward of the home's front facade. Extra attention and treatments should be applied when using this garage location.



VARIED TANDEM GARAGE EXAMPLES



VARIED SWING-IN GARAGE EXAMPLES



**SPLIT 3-CAR SWING-IN EXAMPLE
(55' WIDE MINIMUM)**



DETACHED, DEEP RECESSED EXAMPLES

FORWARD SIDE-ON GARAGE

Side-on garages greatly reduce the impact of garage door faces on the streetscape. The resultant street-facing garage wall shall be articulated with the same level of detail as the front facade of the home including windows, trim, etc.

- Side-on garages are prohibited on lots less than 55' wide; and
- All garage doors on side-on plans shall be recessed a minimum of twelve (12) inches behind the garage wall plane.

Forward-Front facing Garage

This garage placement is limited and must be used in combination with others to lessen the impact on the street scene. The minimum garage setbacks (as stated in Section 7.5, Home Types, of this document) shall be maintained.

- All garage doors located forward of the front living space shall be recessed a minimum of twelve (12) inches behind the garage wall plane.

Garage Treatments

Garage Placement

TANDEM GARAGE OR SPLIT GARAGES

When a three (3)-car garage is planned, the impact on the streetscape can be reduced by constructing the additional car bay in tandem behind the standard two (2)-car garage door. Designing split or separated garages is another option to reduce the impact of adjacent garage doors.

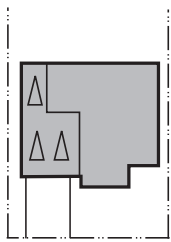
3-Car Front-Facing Garage Requirements

Three (3)-car garages with all doors facing front are limited to:

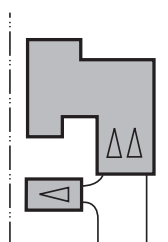
- One plan per neighborhood plan package ;
- Lots that are 55' wide or greater; and
- All other standard garage requirements of the Specific Plan

When a 3-car front-facing garage is used, in addition to standard garage requirements and a minimum seven (7) feet deep by ten (10) feet wide porch, it shall provide at least one (1) of the following additional front-facing plan requirements:

- Offset a single garage door at least two (2) feet from a double door; and



3-CAR TANDEM GARAGE



DETACHED SPLIT
SIDE-ON GARAGE

VARIED TANDEM GARAGE EXAMPLES



- Provide three (3) single garage doors each separated by at least one (1) foot. Three (3)-car garages may be located forward of the house or recessed. If they are located forward of the living space they are subject to all requirements of garage forward plans.
- Provide an 18- to 24-inch box-out or architectural projection forward of the garage to create a plane break.



Detached Garages

Detached garages may be deep-recessed or corner-facing with a side street entry. Detached garages are located toward the rear of the lot behind the primary structure. They are enhanced by applying the same details found on the home. When detached garages are front-facing, long driveways should be improved by adding texture, pattern or a 'ribbon' driveway.

Optional Garage Treatments

The following optional treatments are encouraged to reduce or buffer the direct view of garages and garage doors from the sidewalk or street.

Porte Cochere

A recessed garage plan with a porte cochere creates an additional, partially covered, parking space and serves an occasional outdoor private space while buffering the view of the garage.

Garage Door Screening Elements

The following screening elements may be utilized to reduce direct impact of the garage:

- An attached overhead trellis installed beneath garage roof fascia and/or above garage door header trim.
- Driveway may be spanned with a gated element or overhead trellis.



THREE CAR TANDEM GARAGES



OFFSET GARAGE DOORS



THREE SINGLE GARAGE DOORS



PORTE COCHERE EXAMPLE



B. Garage Placement and Treatments

Lane Facing / Motorcourt Garages

Although lanes and motorcourts are thought of as merely a transitional vehicular space, they also provide garage and sometimes direct residence access, trash collection and other daily utility uses. They may be experienced by homeowners as often as the front entries to these homes.

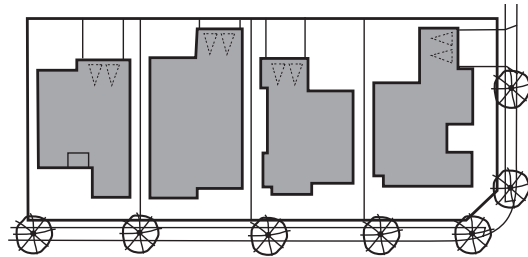
Therefore, when designing lanes and motorcourts, both functional and aesthetic issues are to be considered. It is the intent to provide a pleasant experience for daily users by incorporating both in the elements of lane design.

Lanes and motorcourts with home and garage elevations that have similar or identical facades, setbacks, roof lines, or color schemes are not permitted.

In order to avoid this, the following requirements must be met:

- ♦ Avoid a multi-storied, “canyon-like” effect within the lane by providing varied massing, vertical and horizontal offsets of primary architectural elements and roof lines. Where possible, vary ground floor building setbacks to provide undulation in the line of garage doors. All drive apron and building separation setback requirements shall still be maintained, (refer to Section 7.5, Home Types for specific setback requirements);
- ♦ Articulate lane-facing and motorcourt building elevations with style-appropriate window trim, elements and details;
- ♦ Vary garage door appearance by using door patterns, colors, and windows appropriate to individual architectural styles;
- ♦ Design and locate rear privacy walls and pedestrian-gate entries to provide ease of access to the rear yard, allow sufficient dimension for landscape requirements and screen trash can storage from lane and motorcourt view; and
- ♦ Provide landscape planters (as feasible) within lanes to soften the appearance of the building elevations.

LANE (PRIVATE ALLEY) LOADED ACCESS



**VARIED GARAGE PLACEMENT—LANE
(PRIVATE ALLEY) FACING**

C. Off-Street Parking

If providing off-street parking, ensure the dimensions of the space are 9 feet by 18 feet for uncovered spaces. A 2-foot landscape area may be used to reduce the stall length minimum to 16 feet. Carports must have 10 feet by 20 feet clear at minimum.

7.3.6 Balconies and Projections

The inclusion of balconies and projections are encouraged for both aesthetic and practical purposes. They are useful in breaking up large wall planes, offsetting floors, providing shade, creating visual interest, and adding human scale to a building. Balconies also provide the practical advantage of extending living areas to the outdoors and providing elevated spaces and views.

- ♦ Balconies may be covered or open. They may be partially recessed into the mass of the building or serve as a projecting element; and
- ♦ Design balconies as integral elements of the building with details, eaves, supports, and railings to be consistent with the architectural style and other elements of the building’s design.

Other architectural projections may include elements such as cantilevered massing, secondary roof changes, niches in plan and bay windows.

7.3.7 Elevation Enhancements

The second story portion of elevations facing a public street should include a variety of window treatments, roof projections, etc. to provide an appearance similar in quality to front elevations. A variety of hipped and gabled roof forms should be used in each neighborhood. One-story elements also help to provide this variety.





7.3.8 Varied Massing

Provide varied massing and articulated elevations consistent with the selected style. Provide alternative garage locations, from shallow to deep, and alternate plan configurations within each neighborhood, for conventionally loaded product.

Floor Plan Form

Keep architectural styles in mind when designing the floor plan. For example, styles such as Cottage or Farmhouse that reflect asymmetrical massing and steeper roof pitches may be applied to a similar floor plan and elevation while both maintain an authentic appearance.

7.3.9 Corner Lots/Critical Edge Conditions

Neighborhood quality will be enhanced by adding a home plan designed specifically for the corner, or by enhancing an interior lot plan for use on the corner with additional architectural elements and/or details as found on the front elevation.

All corner homes (Conventional, Lane Loaded, and Cluster Home product) shall wrap materials (where applicable to the style/front elevation) and continue articulation around to the corner side facade. All material changes shall occur on an inside corner such as a porch, fireplace, niche or bay window, OR coincide with an architectural element that conceals the material change, OR terminate a logical stopping point such as a fence, column, or window.

Corner Plans

At least two (2) plans per Conventional and Lane Loaded residential product neighborhood should be designed for optional use on corner lots. Cluster Home product should be designed appropriately to address the front and corner side exposure.

- One (1) of these plans shall include a significant architectural element that encompasses an interior room or living space that wraps around or occupies the corner side yard.

A second plan (primarily for conventional product) should include an element that helps to articulate the side facade such as:

- A wraparound porch;
- pop-out side gable;
- significant wall plane offset;
- appropriately detailed second story pop-out.

These elements will also provide logical stopping points/less conspicuous inside corners for wrapping materials. Corner plans should also consider use of the following elements:

- A corner-related front entry door placement; and
- A garage placement that allows entry from the side street or lane.

Community Recreation and Common Facilities

Private, common recreation facilities may include a pool, spa, clubhouse, barbeque areas or other facilities as appropriate to the community. Common facilities act as key character elements. Therefore, the following should be considered when designing for such facilities.

- Clubhouse and other common buildings should exhibit a high level of quality and attention to detail on all sides of the building;
- All architectural and community elements, such as street furnishings, benches, and lighting standards should be consistent with the selected overall architectural character for the community; and
- Colors, massing, roof pitch, and materials of surrounding residential buildings should also be considered in conjunction with the design of the common facility.



WRAPPING MATERIALS



WRAPAROUND PORCH ON CORNER PLAN



7.4 MATERIALS AND DETAILS

7.4.1 Introduction

Although the character and style of homes should be established primarily through the use of massing, roof forms and key architectural elements, the addition of appropriate secondary character elements, detailing and accent materials provides an effective means of further reinforcing and enhancing character and style interpretations.

Key elements are those primary architectural features of the home that clearly discern one style from another. Secondary elements and details are identified as those that help to reinforce or enhance an already identifiable style. A list of appropriate elements, colors and materials for each individual style is found in the requirements matrices in the "Architectural Styles" section of this document.

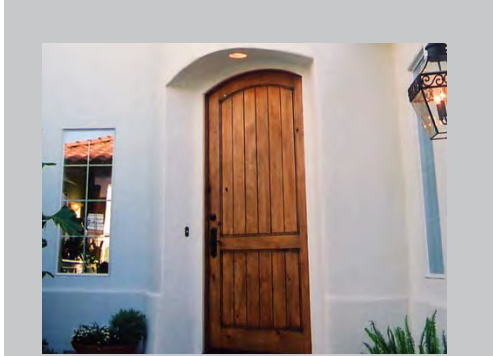
The Key or Primary elements are:

- ✦ Doors and Entries;
- ✦ Windows;
- ✦ Rakes and Eaves;
- ✦ Exterior Lighting Fixtures;
- ✦ Roof Materials;
- ✦ Colors;
- ✦ Accent Materials; and
- ✦ Wall Finishes

7.4.2 Doors and Entries

Building entries represent an important point of interface between public and private spaces typically creating the initial impression for the user and a focal point for the viewer. Front doors and associated entry spaces should thereby be treated with a sense of importance and detailed so as to emphasize their location and purpose. In designing the door and entry space of homes the following guidelines shall be observed:

- ✦ Design of door and entry spaces shall be consistent with the architectural style of the home;
- ✦ Doors shall be protected by a deep recess, porch or other covered element;
- ✦ Door and entry spaces shall reflect a level of detail appropriate to their function and architectural style;
- ✦ Surface mounted lights shall not be permitted in garage door soffits; and
- ✦ Wall mounted lighting fixtures shall be selected according to the individual style of the home or building.



DOORS



WINDOWS



7.4.3 Windows

The following guidelines shall be observed for windows:

- † Proportions and alignment shall be appropriate to individual style;
- † No highly reflective glazing shall be used;
- † Windows may be vinyl-wrapped or wood;
- † Inset windows shall be a minimum of two (2) inches in depth;
- † Recessed windows are encouraged to be a minimum of six (6) inches in depth;
- † Window placement shall be designed to enhance privacy between neighbors;
- † Full window trim is encouraged on street facing elevations;
- † Shutters shall be sized to match window width and exhibit style-appropriate hardware; and
- † Style-appropriate grates, shutters and tile surrounds are encouraged.

7.4.4 Rakes and Eaves

As with other architectural elements, roof edge treatments vary with the character of each style. Rakes and eaves shall reflect that of each individual style and contribute to the overall look and feel of the community.

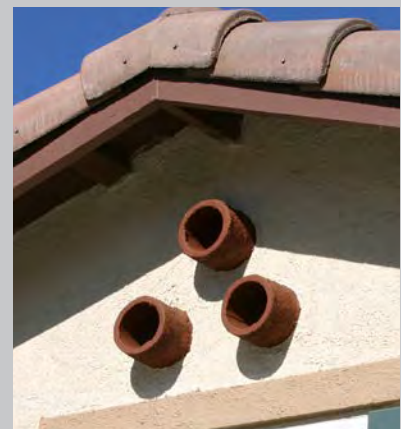
- † Elements and proportions of rakes and eaves shall be appropriate to the individual style;
- † Where appropriate to individual style, larger eave overhangs are encouraged to provide opportunities for shading and relief; and
- † Rafter tails, when exposed, shall be a minimum of four (4) inches and painted or stained.

7.4.5 Exterior Lighting and Fixtures

In the neighborhoods, streets and lanes of the Specific Plan emphasis shall be placed on lighting for safety, security and aesthetics. Street poles and wall fixtures shall be used to provide sufficient light for traversal and visibility, thereby providing the community with a comfortable level of safety and security at night. Every attempt should be made to reduce overflowing light on adjacent homes and to increase the amount of light in less visible areas.

Opportunities to use lighting for directional and aesthetic reasons should also be considered. Where fixtures are not an important focal point, conceal light sources and concentrate on achieving the maximum effects of good lighting design. Where fixtures are part of architectural detailing they should reflect the selected style.

- † Lighting used on walls and walkways shall focus light down and provide appropriate down-casting hardware to minimize glare; and
- † Ambient light shall be down-cast to reduce the impact on the neighborhood.



EXTERIOR TREATMENTS



7.4.6 Roof Materials

Roof materials, colors and treatments should correspond to the individual character or style of the home or building and be compatible with the overall look of the neighborhood. Style appropriate materials and colors are found in each appropriate matrix in the “Architectural Styles” section of this document.

- ♦ A variety of roof plans, forms and ridge heights shall be provided using hips, gables and intersecting elements to avoid a repetitious skyline;
- ♦ High quality composition, concrete or clay tiles may be used in conjunction with the style of home; and
- ♦ Skylights are prohibited on sloped roofs facing public streets.

7.4.7 Colors

A varied palette of regional and style-appropriate colors and materials have been selected for the homes in the Specific Plan. This inherently rich and diverse palette will provide the variety required for visual interest while providing a sense of unity within the neighborhoods. Style-appropriate color selections will enhance the architectural character and impart individuality to each home.

- ♦ Color shall contribute to distinguishing the overall architectural character of the dwelling. Refer to “Architectural Styles” matrices for individual color palettes;
- ♦ Hue variation in adjacent homes shall be provided to create diversity within the neighborhood;
- ♦ Diversity of color is encouraged; and
- ♦ Roof tile colors will be selected according to architectural styles.

To further this goal of diversity, the following criteria shall be met:

- ♦ Elevation/color scheme shall consider the composition of colors, appropriately combining colors for an aesthetic and varied streetscape. Consider field color, trim color, accent colors, and accent materials colors.
- ♦ Individual color schemes must be appropriate to the architectural styles with a harmonious selection of accent materials, roof profiles and colors;
- ♦ No adjacent or facing home shall have the same color scheme; and
- ♦ No more than two (2) of the same plan/elevation shall be plotted adjacent to one another.



ROOF MATERIALS



ACCENT MATERIALS



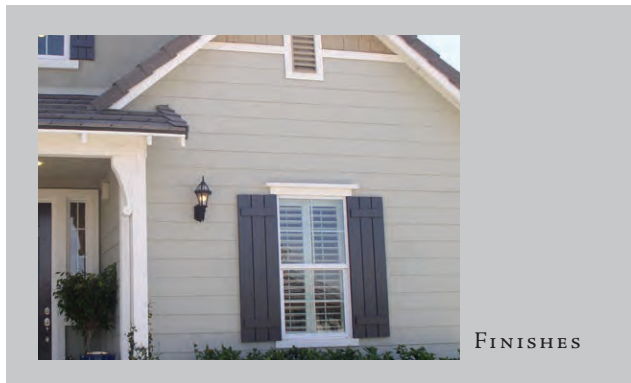
7.4.8 Accent Materials

Accent materials should be used to enhance and reinforce the architectural style and composition of individual homes and provide variety in the street scene. Selective use of appropriate materials, color and placement can provide maximum impact while imparting a sense of unique character to each home.

- ♦ Culmination of accent materials shall terminate at inside corners or coincide with an edge or architectural element to conceal changes in material. Where views are limited or edges concealed by an architectural element, accent materials may terminate at privacy wall conditions;
- ♦ Natural stone, approved manufactured or cultured stone, painted or natural brick, precast concrete, ceramic tile, slump block, and horizontal or vertical wood siding (or approved manufactured siding, i.e. cementitious board) are encouraged;
- ♦ Accent materials shall be selected to complement the overall color and style of the home or building; and
- ♦ Architectural trim applied to all elevations is encouraged to be consistent with front elevation and the architectural style.

7.4.9 Wall Finishes

Style-appropriate wall finishes should visually enhance individual homes. The use and placement of extreme contrasts in color and/or materials should be carefully considered in order to maintain a compatible overall look throughout the neighborhood. Footings shall be exposed no higher than 6" above finished grade.



Permitted Finishes:

- ♦ Stucco: light to medium sand finish ("Spanish Lace" finish is prohibited);
- ♦ Exposed masonry walls (brick, slump block, etc.);
- ♦ Stone, brick, adobe, brick veneers (accent materials);
- ♦ Horizontal plank siding; and
- ♦ Board and batten siding.

7.4.10 Trash Container Pads in Lanes

Builders are required to build one (1) 42-inch by 42-inch brick pane pad for each lot immediately adjacent to the lane for the purpose of temporarily placing trash containers for pick-up by the Sanitation Department on scheduled days. The design of the enclosures shall comply with NPDES requirements.

7.5 HOME TYPES

7.5.1 Introduction

A primary design objective for the Specific Plan is to provide a variety of home sizes and plans to suit the needs of different life stages and market segments.

The following section provides conceptual plotting examples for a variety of single-family detached homes, arrangements, and lot size ranges. The plotting concepts are provided only to illustrate a typical layout of each building type. These concepts are not intended to be mandated layouts.

7.5.2 Home Type Criteria

The following are design standards and minimum setbacks by typical residential product and building type.



Cluster Homes Residential Product: Motorcourt Cluster A (6- or 8-Plex)

(PA 13, 25, 26 & 27)*



CHARACTERISTICS

- ♦ Potential net density of 7.5 to 9 du/ac.
- ♦ Single-family detached dwellings clustered around motor court
- ♦ Automobile access via common motor court driveway
- ♦ Primary entries and walks face motor court or street
- ♦ Private side and rear yards
- ♦ Parking for residents provided in garages
- ♦ Guest parking provided on local streets, designated parking areas, or as approved by the City of Ontario
- ♦ Reciprocal use easements extend private space

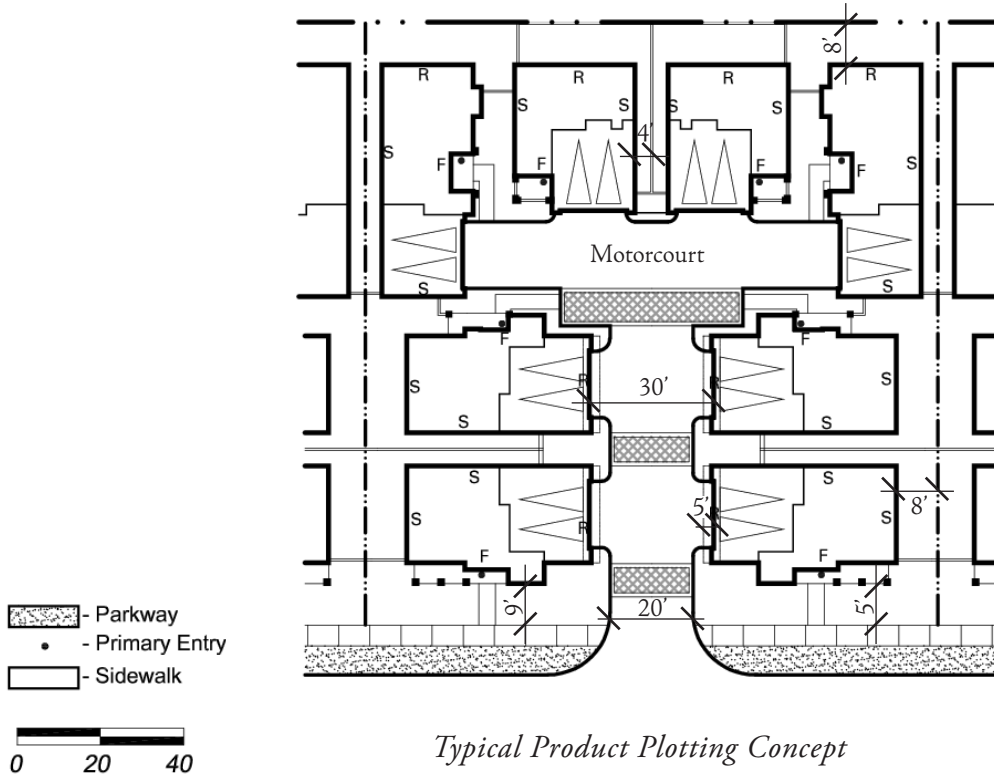
Notes:

1. Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
 2. Refer to other areas of **Chapter 7 – Residential Design Guidelines**, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
 3. Refer to other areas of 7.6 Landscape Design, for specific criteria regarding all landscape design and implementation.
 4. Typical Product Plotting Concepts are provided for representative purposes only - not intended to be mandated layout.
- * Proposed representative product types in each planning area are subject to final development submittal and are subject to change.



Cluster Homes: Motorcourt Cluster A (6- or 8- Plex)

(PA 13, 25, 26 & 27)



Note: Private lanes shall be enhanced with a combination of pavers, colored concrete or similar decorative material, subject to the review and approval of the Planning Director.

Typical Product Plotting Concept

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Minimum Standards

Minimum Lot Size

Exclusive use area per home 2,100 s.f.

Building Separation

Front to side living space N/A
 Front entry to side living space 8' (4' + 4')
 Side to side living space 8' (4' + 4')

Side Setbacks - To Back of Sidewalk - Street Facing

Living space 9'
 Porch, balcony 5'
 Garden wall N/A

Side Setbacks - To Parcel Edge

Living space to property line 4'

Rear Setbacks - To Parcel Edge

Living space (2-story) 8'

Garage Setbacks

Garage face to garage face 30'
 Garage face to driveway (apron) 5'*
 Note: Lane width may be increased to 24' when the lane length exceeds 150'.

Maximum Encroachments into Building Setbacks

Fireplace, media niche (8' max length), 2'
 bay windows (cantilevered),
 potshelves, brackets, etc.
 Second floor cantilever over garage 2'

Private Open Space Per Unit

Minimum dimension 6'
 Total per unit 100 s.f.

Parking

Per City of Ontario Development Code

Building Height

Architectural projections may exceed the maximum building height of 35' by an additional 10'.

*Measurement is to be taken from the inside of the curb.



Cluster Homes Residential Product: Paseo Cluster Lots (PA 25 & 27)*



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Not to Scale

Characteristics

- ✦ Potential net density of 7.5 to 9 du/ac.
- ✦ Single-family dwellings with front orientation (primary entries and walks) along green court, paseo, or street
- ✦ Automobile access via lane
- ✦ Private side and rear yards/patios
- ✦ Parking for residents provided in garages
- ✦ Guest parking provided on local streets or in designated parking areas
- ✦ Reciprocal use easements may be used to increase private yards and meet building standards

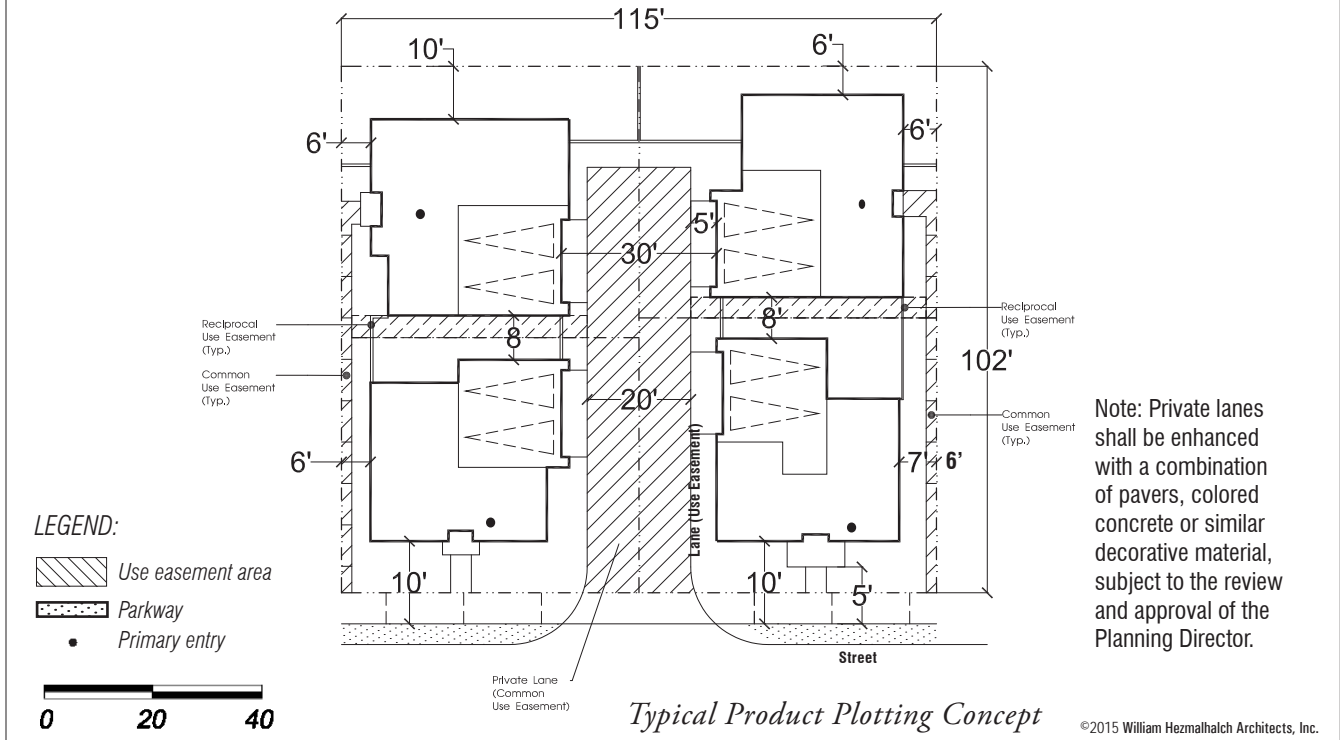
Notes:

1. Actual lot sizes and densities may vary based on final plan.
 2. Refer to other areas of Section 7 - Residential Design Guidelines for more specific criteria regarding building design, plotting, styles, materials, details, and implementation.
 3. Refer to Section 7.6 Landscape Design, for specific criteria regarding landscape design and implementation.
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Cluster Homes: Paseo Cluster Lots

(PA 25 & 27)



Minimum Standards

Minimum Lot Size

Exclusive use area per home 2,100 s.f.

Cluster Dimensions*

Lot width standard 115'
 Lot width on corner Lot 120'
 Lot depth on standard lot 102'
 Maximum building coverage 45%

*Minimum lot dimensions on curvilinear streets shall be measured at front and rear building setbacks.

Building Separation

Side to side living space 8' (4'+4')
 Paseo separation 12' (6'+6')

Front Setbacks - To Back of Sidewalk - Street Facing

Living space 10'
 Porch, balcony 10'
 Garage facing street --
 Garage facing lane 10'

Building Height

Architectural projections may exceed the maximum building height of 35 feet by an additional 10 feet.

Side Setbacks - To Back of Sidewalk - Street Facing

Living space 10'
 Porch, balcony 5'
 Corner facing garage 18'

Interior Setbacks to Property Line or Lane

Living space to PL (ground floor) 6'
 Living space to lane (ground floor) 5'
 Garage to PL or lane 5'

Encroachments into Setbacks

Fireplace, media niche (8' max length), bay windows, potshelves, brackets, roof overhangs etc. 2' max encroachment, 3' min. from PL

Second story living space over garage, offset or cantilever from face of garage (or as approved by the City of Ontario) 4' max encroachment, 3' min. from PL

Private Open Space Per Unit

Minimum dimension 10'
 Total per unit 100 s.f.

Parking

Per City of Ontario Development Code

Cluster Homes Residential Product:
Motorcourt Cluster B (PA 26 & 27)*



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Not to Scale

Characteristics

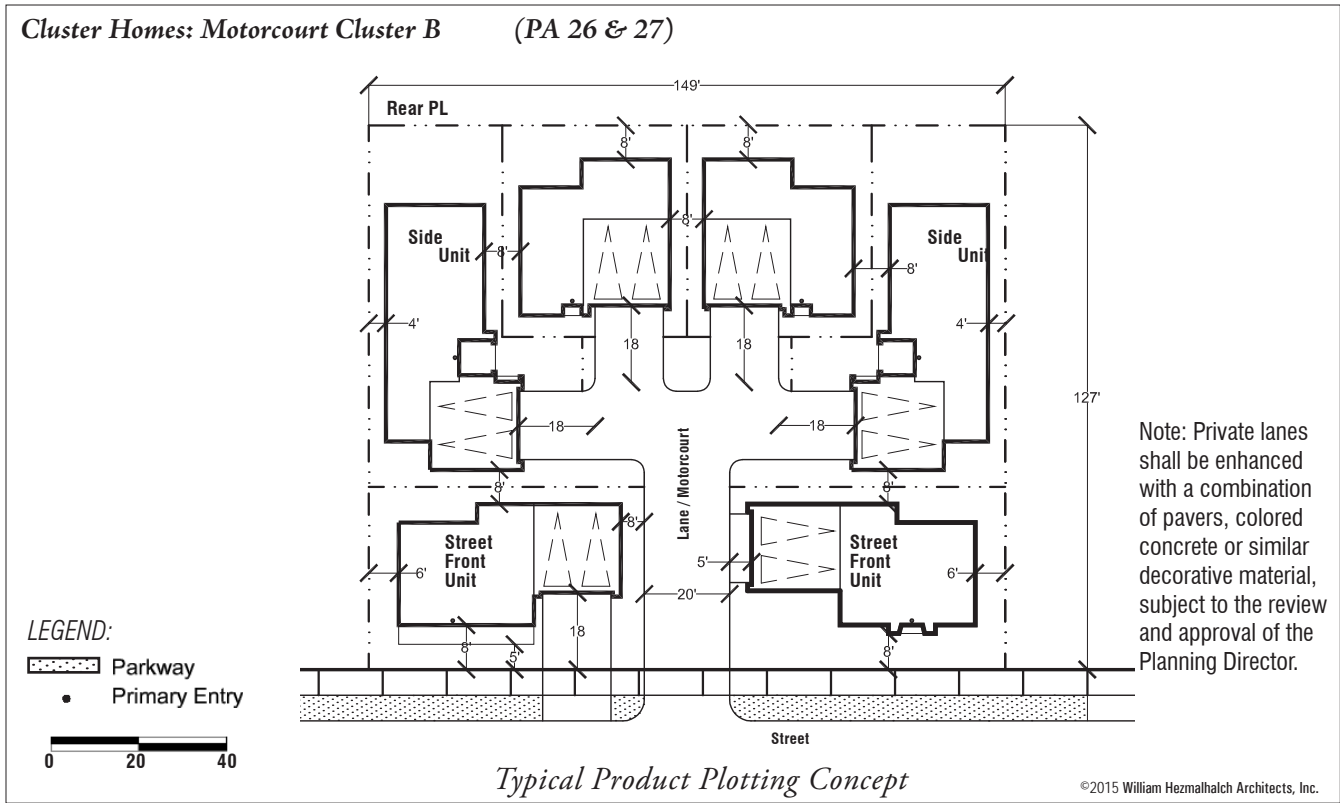
- ✦ Potential net density of 8.5 to 10 du/ac.
- ✦ Single-family dwellings with front orientation along paseos, motorcourt, or street
- ✦ Automobile access via lane or motorcourt
- ✦ Primary entries and walks face paseos or motorcourt
- ✦ Private yards/patios
- ✦ Parking for residents provided in garages
- ✦ Guest parking provided on local streets or in designated parking areas
- ✦ Reciprocal use easements may be used to increase private yards and meet building standards

Notes:

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 3. Refer to Section 7.6 Landscape Design, for specific criteria regarding landscape design and implementation.
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Cluster Homes: Motorcourt Cluster B (PA 26 & 27)



Typical Product Plotting Concept

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Minimum Standards

Minimum Lot Size

Exclusive use area per home 2,100 s.f.

Cluster Dimensions

Lot width on front 149'
 Lot width on corner 154'
 Lot depth on standard lot 127'
 Maximum building coverage 45%
 Minimum front lot width on cul-de-sac,
 standard knuckle or modified knuckle street 90'
 (partial motorcourt)

Building Separation

Side to side living space 8' (4' + 4')

Front Setbacks - To Back of Sidewalk - Street Facing

Living space 8'
 Porch, balcony 5'
 Garage facing street 18'
 Garage facing lane/motorcourt 10'

Building Height

Architectural projections may exceed the maximum building height of 35 feet by an additional 10 feet.

Side Setbacks - To Back of Sidewalk - Street Facing

Living space 8'
 Porch, balcony 5'
 Corner facing garage 18'

Interior Setbacks to Property Line, or Motorcourt

Living space to PL (street front units) 6'
 Living space to PL (side units) 4'
 Living space to rear PL 8'
 Living space to motorcourt (ground floor) 5'
 Garage to PL or lane 5'

Encroachments into Setbacks

Fireplace, media niche (8' max length), bay windows, potshelves, brackets, roof overhangs etc. 2' max encroachment, 3' min. from PL
 Second story living space over garage, offset or cantilever from face of garage (or as approved by the City of Ontario) 4' max encroachment, 3' min. from PL

Private Open Space Per Unit

Minimum dimension 10'
 Total per unit 70 s.f.

Parking

Per City of Ontario Development Code



**Cluster Homes Residential Product:
Motorcourt Cluster C (5-Plex) (PA 13, 25, 26, & 27)***



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Not to Scale

CHARACTERISTICS

- ♦ Potential net density of 8 to 14 du/ac.
- ♦ Single-family detached dwellings clustered around motor court
- ♦ Automobile access via driveway or common motor court driveway
- ♦ Primary entries and walks face motor court or street
- ♦ Private side and rear yards
- ♦ Parking for residents provided in garages
- ♦ Guest parking provided on local streets, designated parking areas, or as approved by the City of Ontario
- ♦ Reciprocal use easement may be used to create private space between units

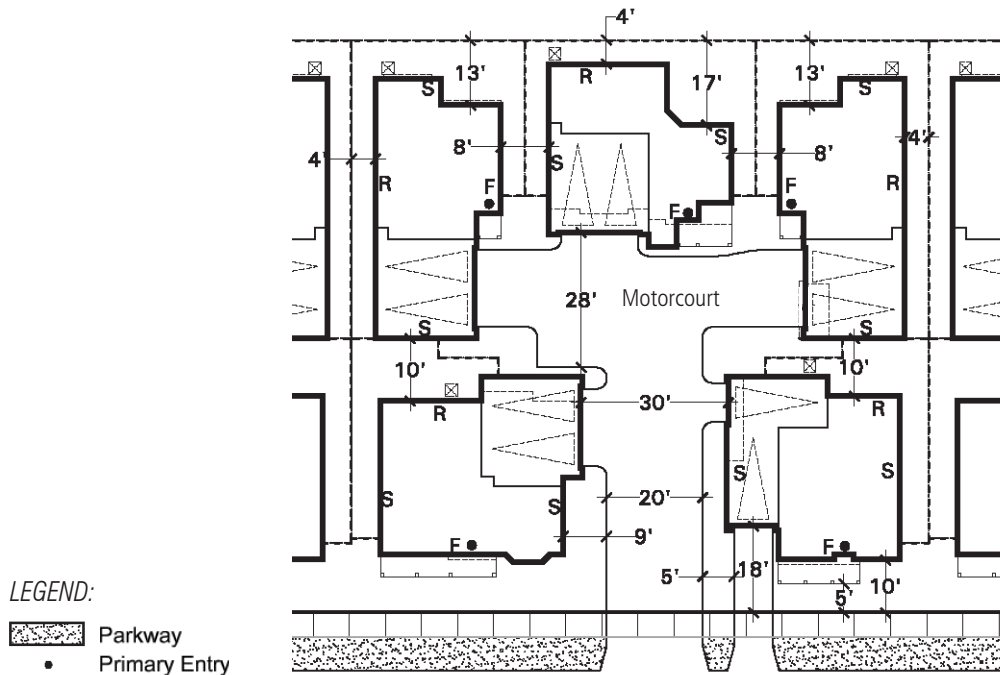
Notes:

1. Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
 2. Refer to other areas of Section 7 – Residential Design Guidelines, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
 3. Refer to Section 7.6 Landscape Design, for specific criteria regarding landscape design and implementation.
 4. Typical Product Plotting Concepts are provided for representative purposes only - not intended to be mandated layout.
- * Proposed representative product types in each planning area are subject to final development submittal and are subject to change.



Cluster Homes: Motorcourt Cluster C (5-Plex)

(PA 13, 25, 26, & 27)



Note: Private lanes shall be enhanced with a combination of pavers, colored concrete or similar decorative material, subject to the review and approval of the Planning Director.

Typical Product Plotting Concept

Minimum Standards

Minimum Lot Size

Exclusive use area per home 2,100 s.f.

Building Separation

Front to side living space N/A
 Front entry to side living space 8' (4' + 4')
 Side to side living space N/A

Side Setbacks - To Back of Sidewalk - Street Facing

Living space 10'
 Porch, balcony 5'
 Garden wall N/A

Side Setbacks - To Parcel Edge

Living space to property line 4'

Rear Setbacks - To Parcel Edge

Living space (2-story) 4'

Garage Setbacks

Garage face to garage face 30'
 Garage face to driveway (apron) 2'*
 Street facing garage 18'
 Note: Lane width may be increased to 24' when the lane length exceeds 150'.

Maximum Encroachments into Building Setbacks

Fireplace, media niche (8' max length), bay windows (cantilevered), potshelves, brackets, etc. 2' max encroachment, 3' min. from PL
 Second floor cantilever over garage 4' max encroachment, 3' min. from PL

Private Open Space Per Unit

Minimum dimension 10'
 Total per unit 100 s.f.

Parking

Per City of Ontario Development Code

Building Height

Architectural projections may exceed the maximum building height of 35' by an additional 10'.

*Measurement is to be taken from the inside of the curb.



Cluster Homes Residential Product:
Motorcourt Cluster D (8-Plex) (PA 27)*



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Not to Scale

CHARACTERISTICS

- ♦ Potential net density of 8 to 14 du/ac.
- ♦ Single-family detached dwellings clustered around motor court
- ♦ Automobile access via driveway or common motor court driveway
- ♦ Primary entries and walks face motor court or street
- ♦ Private side and rear yards
- ♦ Parking for residents provided in garages
- ♦ Guest parking provided on local streets, designated parking areas, or as approved by the City of Ontario
- ♦ Reciprocal use easement may be used to create private space between units

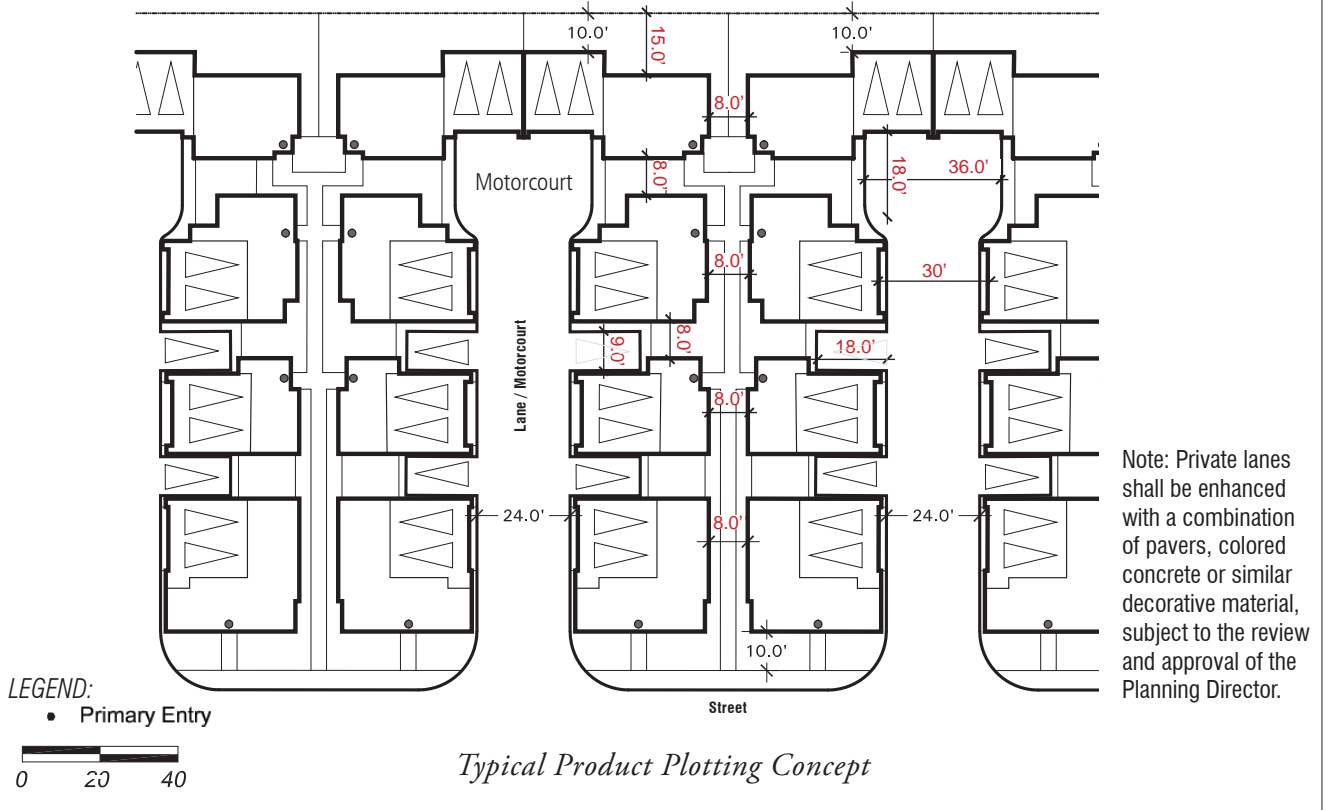
Notes:

1. Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
 2. Refer to other areas of Section 7 – Residential Design Guidelines, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
 3. Refer to Section 7.6 Landscape Design, for specific criteria regarding landscape design and implementation.
 4. Typical Product Plotting Concepts are provided for representative purposes only - not intended to be mandated layout.
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Cluster Homes: Motorcourt Cluster D (8-Plex)

(PA 27)



Typical Product Plotting Concept

Minimum Standards

Minimum Lot Size

Exclusive use area per home 1,750 s.f.

Building Separation

Front to front 8'
 Front entry to side living space 8' (4'+4')
 Side to side living space 8'

Side Setbacks - To Back of Sidewalk - Street Facing

Living space 10'**
 Porch, balcony 5'
 Garden wall 5'

Side Setbacks - To Parcel Edge

Living space to property line 4'

Rear Setbacks - To Parcel Edge

Living space (2-story) 4'

Garage Setbacks

Garage face to garage face 30'
 Garage face to driveway (apron) 2'*
 Street facing garage 18'
 Note: Lane width may be increased to 24' when the lane length exceeds 150'.

Maximum Encroachments into Building Setbacks

Fireplace, media niche (8' max length), bay windows (cantilevered), furr-out, potshelves, brackets, etc. 2' max encroachment, 3' min. from PL
 Second floor cantilever over garage 4' max encroachment, 3' min. from PL

Private Open Space Per Unit

Minimum dimension 8'
 Total per unit 50 s.f.

Parking

Per City of Ontario Development Code

Building Height

Architectural projections may exceed the maximum building height of 35' by an additional 10'.

*Measurement is to be taken from the inside of the curb.

**Where site conditions do not permit a 10' setback from sidewalk to living space, an average setback of 10' may be utilized across the site subject to approval by the Planning Director.



Cluster Homes Residential Product:



Green Court (PA 13, 25, 26 & 27)*

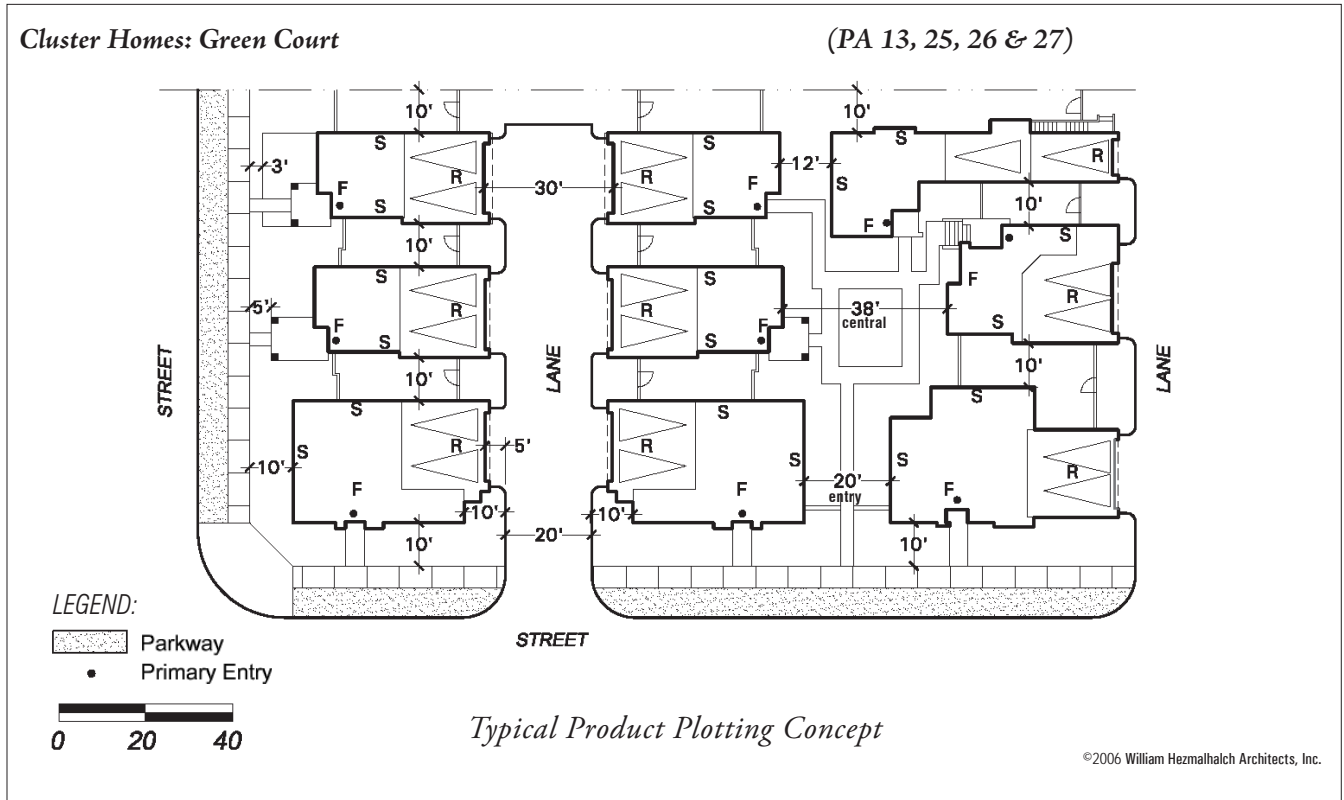
Characteristics

- † Potential net density of 7 to 10 du/ac.
- † Single-family detached dwellings surround green court or face street or paseo
- † Automobile access via lanes
- † Primary entries and walks face green court, street or paseo
- † Private side yards/patios
- † Reciprocal use easements extend private spaces on sides of buildings
- † Parking for residents provided in garages
- † Guest parking provided on local streets or in designated

parking areas

Notes:

1. Actual lot sizes and densities may vary based on final plan.
 2. Refer to other areas of Section 7 - Residential Design Guidelines for more specific criteria regarding building design, plotting, styles, materials, details, and implementation.
 3. Refer to Section 7.6 Landscape Design, for specific criteria regarding landscape design and implementation.
 4. Typical Product Plotting Concepts are provided for representative purposes only and are not intended to be mandated layouts.
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subject to final development submittal and are subject to change.

Minimum Standards

Minimum Lot Size

Exclusive use area per home 2,100 s.f.

Building Separation

Front to front living space - central 38'
 Front to front living space - entry 20'
 Front to side living space 12'
 Side to side living space 10'
 Garage face to garage face 30'
 Rear to rear living space 26'

Front/Side Setbacks - To Back of Sidewalk - Street Facing

Living space 10'
 Porch, balcony 5'
 Garden wall 3'

Side Setbacks - To parcel edge

Living space, porch, garden wall, balcony 10'

Building Height

Architectural projections may exceed the maximum building height of 35' feet by an additional 10 feet.

Rear Setbacks - Lane

Living space (ground and second floor) 10'
 Garage face to lane (drive apron) 3'-5'*
 Living space over garage - minimum offset 2'
 or max. cantilever from face of garage 2' max

Encroachments into Setbacks

Fireplace, media niche (8' max length), bay windows, potshelves, brackets, roof overhangs, etc. 3' min. from PL
 Second floor cantilever over garage 4' max encroachment, 3' min. from PL

Private Open Space Per Unit

Minimum dimension 7'
 Total per unit 70 s.f.

Parking

Per City of Ontario Development Code

Maximum Building Coverage

60%

*Measurement is to be taken from the inside of the curb.

Conventional Small Lot Residential Product:
*Cottage Homes (PA 1, 5, 12, 16, 17, 23 & 24)**

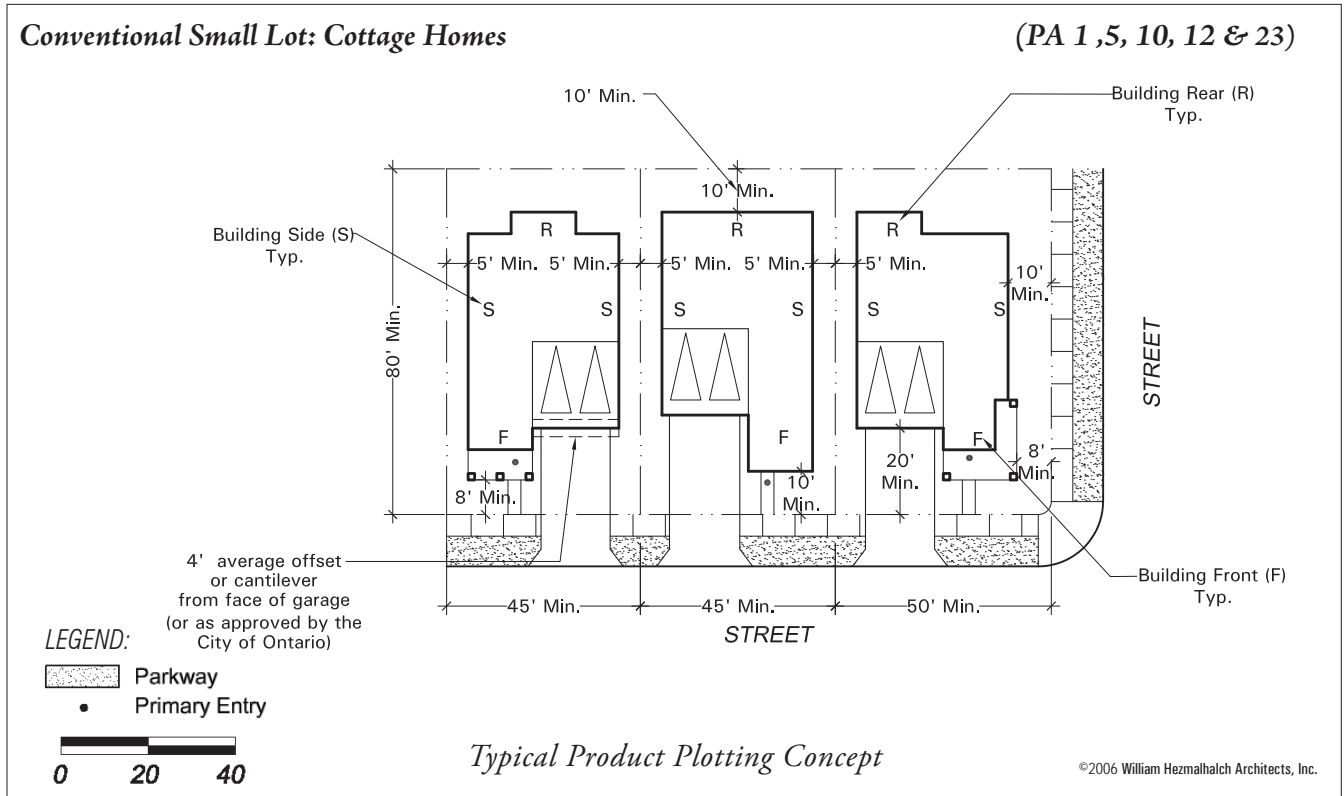


Characteristics

- ♦ Potential net density of 5 to 9 du/ac.
- ♦ Single-family dwellings with street front orientation
- ♦ Automobile access via street
- ♦ Primary entries and walks face street
- ♦ Private rear and side yards
- ♦ Parking for residents provided in garages
- ♦ Guest parking provided on local streets or in designated parking areas

Notes:

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 3. Refer to Section 7.6 Landscape Design, for specific criteria regarding landscape design and implementation.
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Minimum Standards

Lot Dimensions

Minimum lot size	3,600 sq. ft.
Lot width	45'
Lot width on corner	50'
Lot depth on standard lot	80'
Maximum building coverage	50%
Minimum front lot width on cul-de-sac, standard knuckle or modified knuckle street	35'

Minimum lot dimensions on curvilinear streets shall be measured at front and rear building setbacks.

Building Separation

Side to side living space	10' (5'+5')
---------------------------	-------------

Front Setbacks - To Back of Sidewalk - Street Facing

Living space	10'
Porch, balcony	8'
Garage	20'

Building Height

Architectural projections may exceed the maximum building height of 35 feet by an additional 10 feet.

Side Setbacks - To Back of Sidewalk - Street Facing

Living space	10'
Porch, balcony	8'
Side-on garage*	15'

Rear Setbacks to Property Line

Living space (ground floor)	10' minimum
Planning Area 10 - Living space (ground floor)	12' minimum

Encroachments into Setbacks

Fireplace, media niche (8' max length), bay windows, potshelves, brackets, roof overhangs etc.	2' max encroachment, 3' min. from PL
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Second floor cantilever over garage (or as approved by the City of Ontario)	4' max encroachment, 3' min. from PL
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Private Open Space Per Unit

Minimum dimension	7'
Total per unit	70 s.f.

Parking

Per City of Ontario Development Code

*Side-on garages are prohibited on lots less than 60' wide.



Conventional Small Lot Residential Product:

*Paired Homes (PA 5, 16, 17, 23 & 24)**



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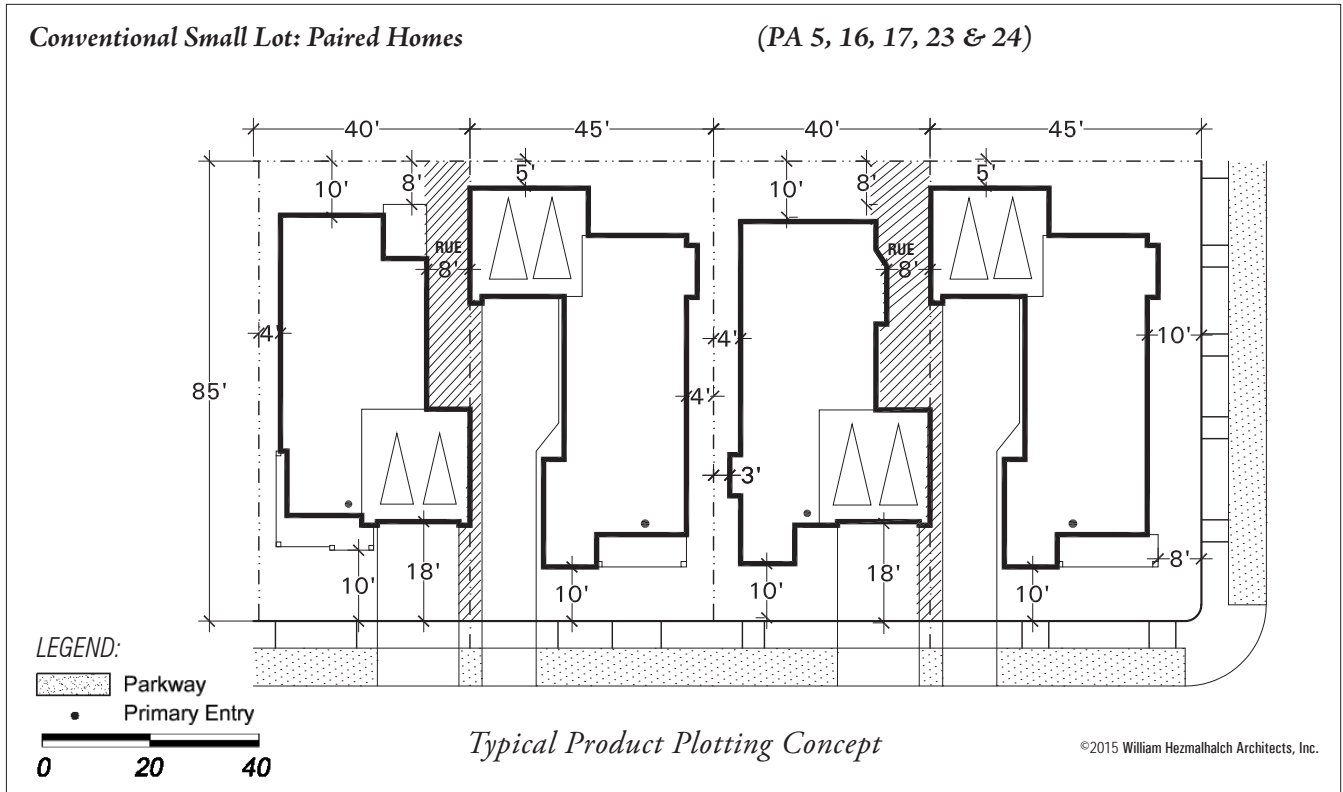
Not to Scale

Characteristics

- ✦ Potential net density of 5 to 7 du/ac.
- ✦ Single-family dwellings with street front orientation and reciprocal use easements
- ✦ Automobile access via street
- ✦ Primary entries and walks face street
- ✦ Private side and rear yards/patios
- ✦ Parking for residents provided in garages
- ✦ Guest parking provided on local streets or in designated parking areas

Notes:

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 3. Refer to Section 7.6 Landscape Design, for specific criteria regarding landscape design and implementation.
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Minimum Standards

Lot Dimensions

Minimum lot size	3,400 sq. ft.
Lot width on front	40'
Lot width on corner	45'
Lot depth on standard lot	85'
Lot depth on corner lot	85'
Maximum building coverage	45%
Minimum front lot width on cul-de-sac, standard knuckle or modified knuckle street	30'

Minimum lot dimensions on curvilinear streets shall be measured at front and rear building setbacks.

Building Separation

Side to side living space	8' (4'+4')
Reciprocal Use Easement (RUE) Side	8' (0'+8')
Building separation to interior (RUE) PL	0'

Front Setbacks - To Back of Sidewalk - Street Facing

Living space	10'
Porch, balcony	10'
Garage	18'
Side-on garage*	10'

Building Height

Architectural projections may exceed the maximum building height of 35 feet by an additional 10 feet.

Side Setbacks - To Back of Sidewalk - Street Facing

Living space	10'
Porch, balcony	8'
Side-on garage*	18'
Corner facing garage	18'

Rear Setbacks to Property Line

Living space (ground floor)	10'
Recessed garage	5'

Encroachments into Setbacks

Fireplace, media niche (8' max length), bay windows, potshelves, brackets, roof overhangs etc.	2' max encroachment, 3' min. from PL
--	--------------------------------------

Second floor cantilever over garage (or as approved by the City of Ontario)	4' max encroachment, 3' min. from PL
---	--------------------------------------

Private Open Space Per Unit

Minimum dimension	7'
Total per unit	70 s.f.

Parking

Per City of Ontario Development Code

*Side-on garages are prohibited on lots less than 60' wide



Conventional Small Lot Residential Product: *Park Homes (PA 16 & 17)**



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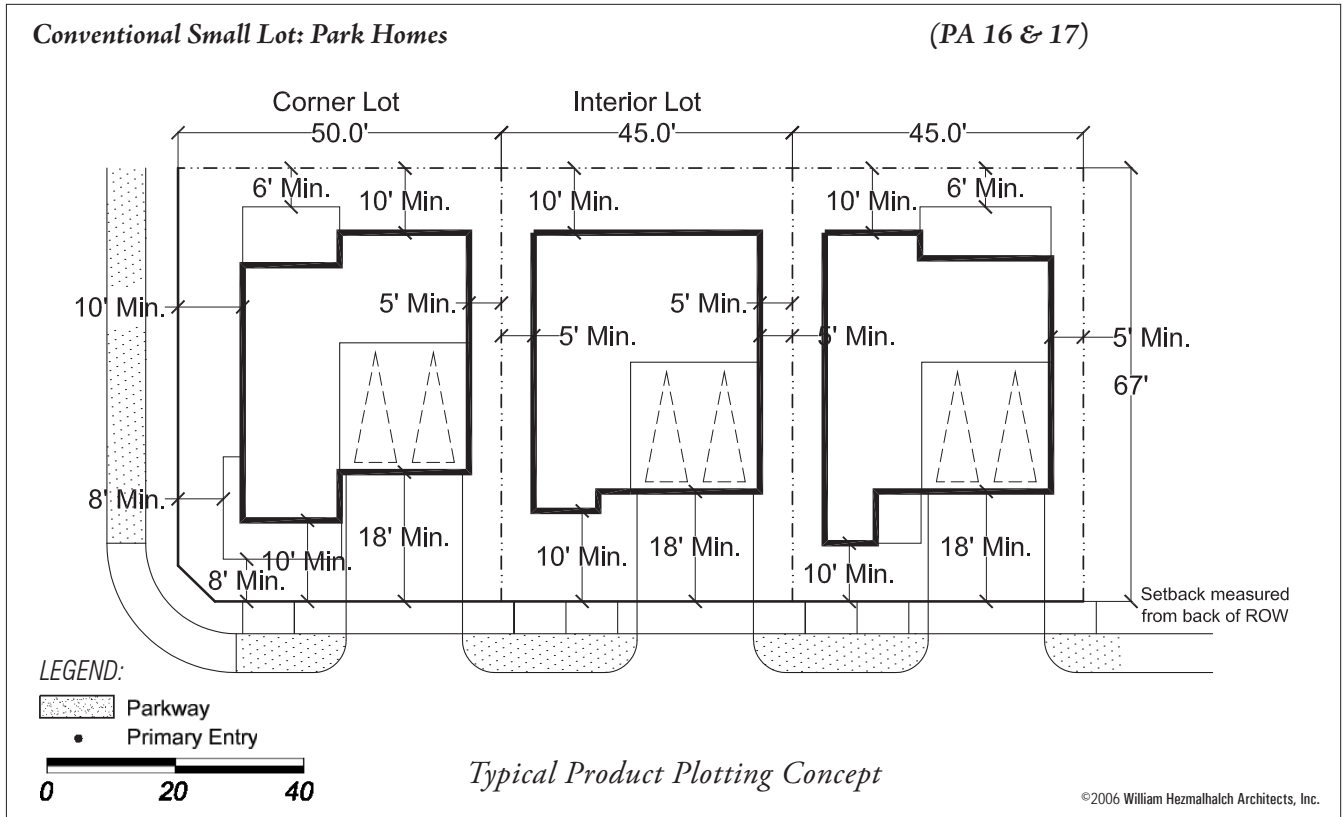
Not to Scale

Characteristics

- ✦ Potential net density of 7 to 9 du/ac.
- ✦ Single-family dwellings with street front orientation
- ✦ Automobile access via street
- ✦ Primary entries and walks face street
- ✦ Private rear yards/patios
- ✦ Parking for residents provided in garages
- ✦ Guest parking provided on local streets or in designated parking areas

Notes:

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 3. Refer to Section 7.6 Landscape Design, for specific criteria regarding landscape design and implementation.
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Minimum Standards

Lot Dimensions

Minimum lot size	3,015 sq. ft.
Lot width on front	45'
Lot width on corner	50'
Lot depth on standard lot	67'
Maximum building coverage	45%
Minimum front lot width on cul-de-sac, standard knuckle or modified knuckle street	30'

Minimum lot dimensions on curvilinear streets shall be measured at front and rear building setbacks.

Building Separation

Side to side living space	10' (5'+5')
---------------------------	-------------

Front Setbacks - To Back of Sidewalk - Street Facing

Living space	10'
Porch, balcony	8'
Garage	18'
Side-on garage*	10'

Building Height

Architectural projections may exceed the maximum building height of 35 feet by an additional 10 feet.

Side Setbacks - To Back of Sidewalk - Street Facing

Living space	10'
Porch, balcony	10'
Side-on garage*	18'
Corner facing garage	18'

Rear Setbacks to Property Line

Living space (ground floor)	10'
Recessed garage	10'
Patio cover, balcony, outdoor room	6'

Encroachments into Setbacks

Fireplace, media niche (8' max length), bay windows, potshelves, brackets, roof overhangs etc. 2' max encroachment, 3' min. from PL

Second floor cantilever over garage (or as approved by the City of Ontario) 4' max encroachment, 3' min. from PL

Private Open Space Per Unit

Minimum dimension	7'
Total per unit	70 s.f.

Parking

Per City of Ontario Development Code

Lane Loaded Residential Product:

Lane Homes (PA 9, 10, 19 & 29)*



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Not to Scale

Characteristics

- ♦ Potential net density of 5 to 8 du/ac.
- ♦ Single-family dwellings with street front orientation
- ♦ Automobile access via lanes
- ♦ Primary entries and walks face street
- ♦ Private side yards
- ♦ Reciprocal use easements extend private yard space
- ♦ Parking for residents provided in garages
- ♦ Guest parking provided on local streets or in designated parking areas

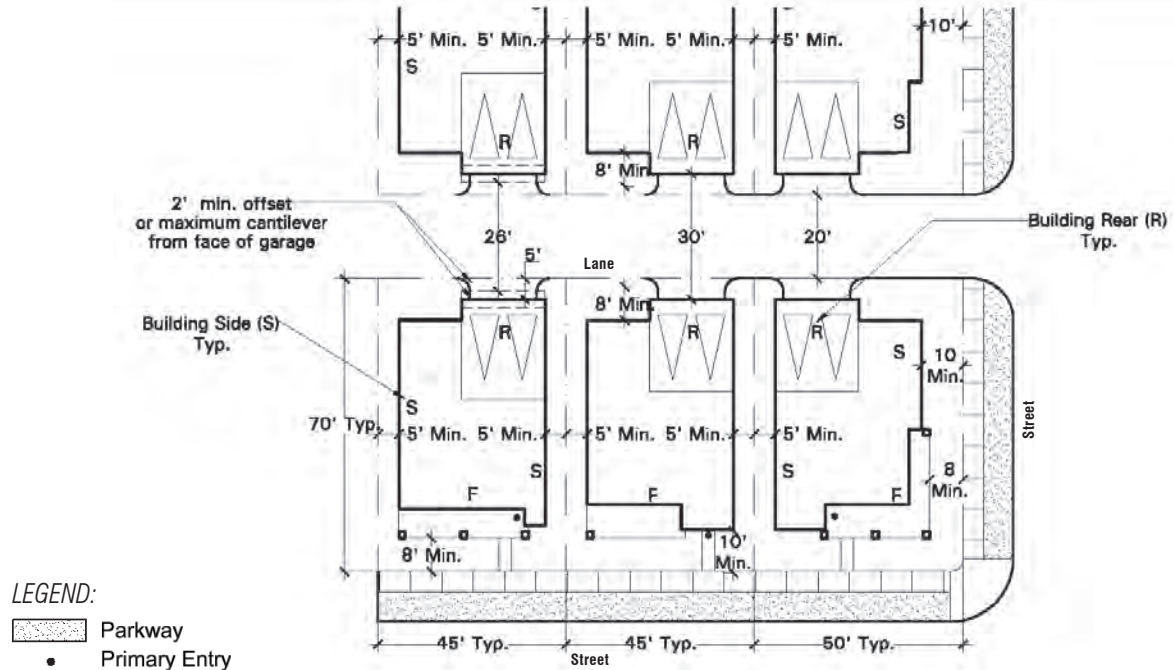
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Lane Loaded: Lane Homes

(PA 9, 10, 19 & 29)



LEGEND:

- Parkway
- Primary Entry

Typical Product Plotting Concept

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Minimum Standards

Lot Dimensions

Minimum lot size	3,150 sq. ft.
Lot width on front	45'
Lot width on corner	50'
Lot depth on standard lot	70'
Maximum building coverage	50%

Building Separation

Side to side living space	10' (5'+5')
Rear to rear living space	26'
Garage face to garage face	30'

Front Setbacks - to Back of Sidewalk

Living space	10'
Garden wall	3'
Porch, balcony	8'

Side Setbacks

Living space to property line or paseo easement 5'

Building Height

Architectural projections may exceed the maximum building height of 35 feet by an additional 10 feet.

Side Setbacks - at Corners - to Back of Sidewalk

Living space	10'
Porch or balcony	8'
Garden wall	3'

Rear Setbacks - to Lane

Living space (ground floor)	8'
Garage face (drive apron)	3'-5'*
Living space over garage - min. offset or max. cantilever from face of garage	2'

Encroachments into Setbacks

Fireplace, media niche (8' max length), bay windows, potshelves, brackets, roof overhangs etc. 2' max encroachment, 3' min. from PL

Second floor cantilever over garage (or as approved by the City of Ontario) 4' max encroachment, 3' min. from PL

Private Open Space Per Unit

Minimum dimension	7'
Total per unit	70 s.f.

Parking

Per City of Ontario Development Code

*Measurement is to be taken from the inside of the curb.

Conventional Medium Lot Residential Product: Village Homes (PA 3, 4, 6, 8, 20, 21, 28, 29 & 31)*

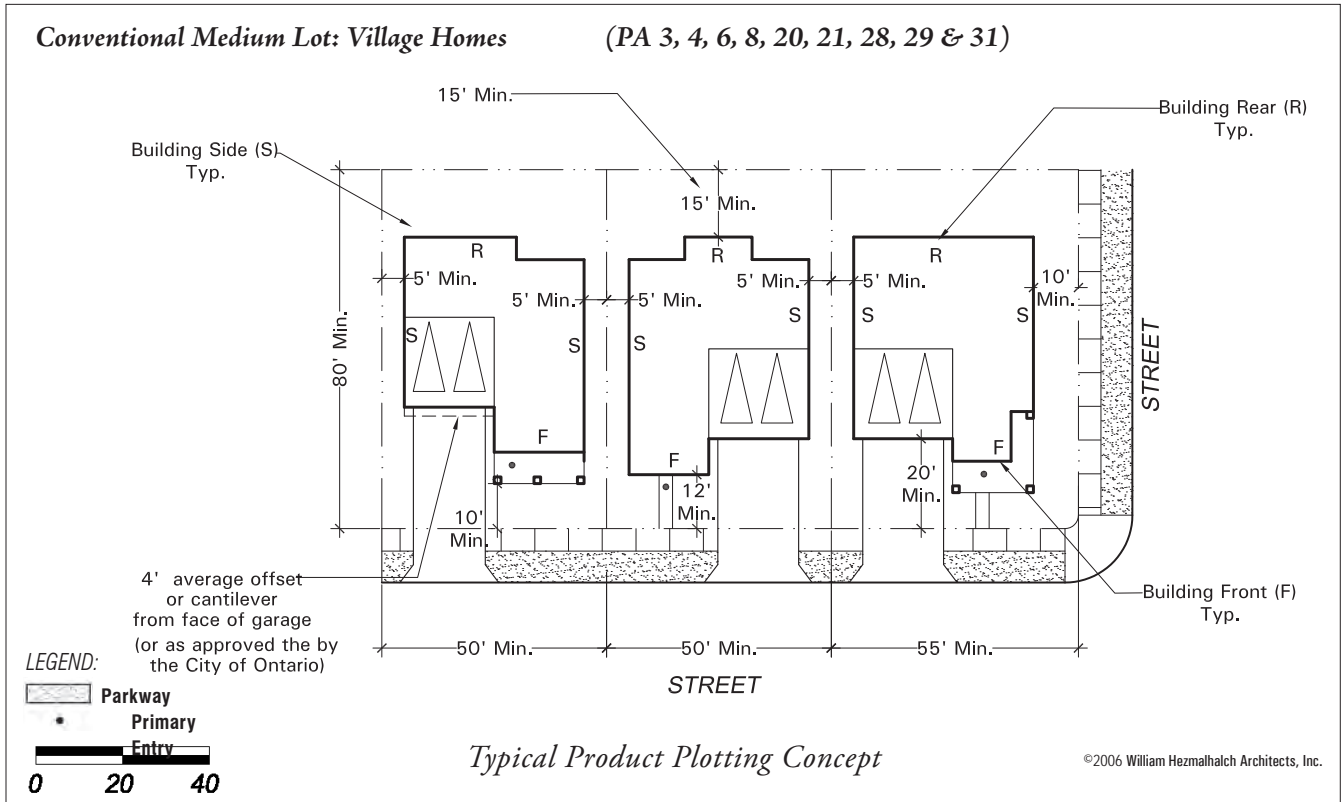


Characteristics

- ✦ Potential net density of 4 to 6 du/ac.
- ✦ Single-family dwellings with street front orientation
- ✦ Automobile access via street
- ✦ Primary entries and walks face street
- ✦ Private rear and side yards
- ✦ Parking for residents provided in garages
- ✦ Guest parking provided on local streets or in designated parking areas

Notes:

1. Actual lot sizes and densities may vary based on final plan.
 2. Refer to other areas of Section 7 - Residential Design Guidelines for more specific criteria regarding building design, plotting, styles, materials, details, and implementation.
 3. Refer to Section 7.6 Landscape Design, for specific criteria regarding landscape design and implementation.
 4. Typical Product Plotting Concepts are provided for representative purposes only and are not intended to be mandated layouts.
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Minimum Standards

Lot Dimensions

Minimum lot size	4,000 sq. ft.
Lot width on front property line	50'*
Lot width on corner	55'
Lot depth on standard lot	80'
Maximum building coverage	50%
Minimum front lot width on cul-de-sac, standard knuckle or modified knuckle street	35'

Minimum lot dimensions on curvilinear streets shall be measured at front and rear building setbacks.

Building Separation

Side to side living space	10' (5'+5')
---------------------------	-------------

Front Setbacks - To Back of Sidewalk - Street Facing

Living space	12'
Porch, balcony	10'
Garage	20'
Side-on garage**	15'

Building Height

Architectural projections may exceed the maximum building height of 35 feet by an additional 10 feet.

Side Setbacks - To Back of Sidewalk - Street Facing

Living space	10'
Porch, balcony	10'
Side-on garage**	15'
Corner facing garage	20'

Rear Setbacks to Property Line

Living space (ground floor) minimum	15'
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Encroachments into Setbacks

Fireplace, media niche (8' max length), bay windows, potshelves, brackets, roof overhangs etc.	2' max encroachment, 3' min. from PL
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Second floor cantilever over garage (or as approved by the City of Ontario)	4' max encroachment, 3' min. from PL
---	--------------------------------------

Private Open Space Per Unit

Minimum dimension	7'
Total per unit	70 s.f.

Parking

Per City of Ontario Development Code

Conventional Large Lot Residential Product:

Enclave Homes (PA 7, 22 & 30)*



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Not to Scale

Characteristics

- ♦ Potential net density of 3 to 6 du/ac.
- ♦ Single-family dwellings with street front orientation
- ♦ Automobile access via street
- ♦ Primary entries and walks face street
- ♦ Private rear and side yards/patios
- ♦ Parking for residents provided in garages
- ♦ Guest parking provided on local streets or in designated parking areas

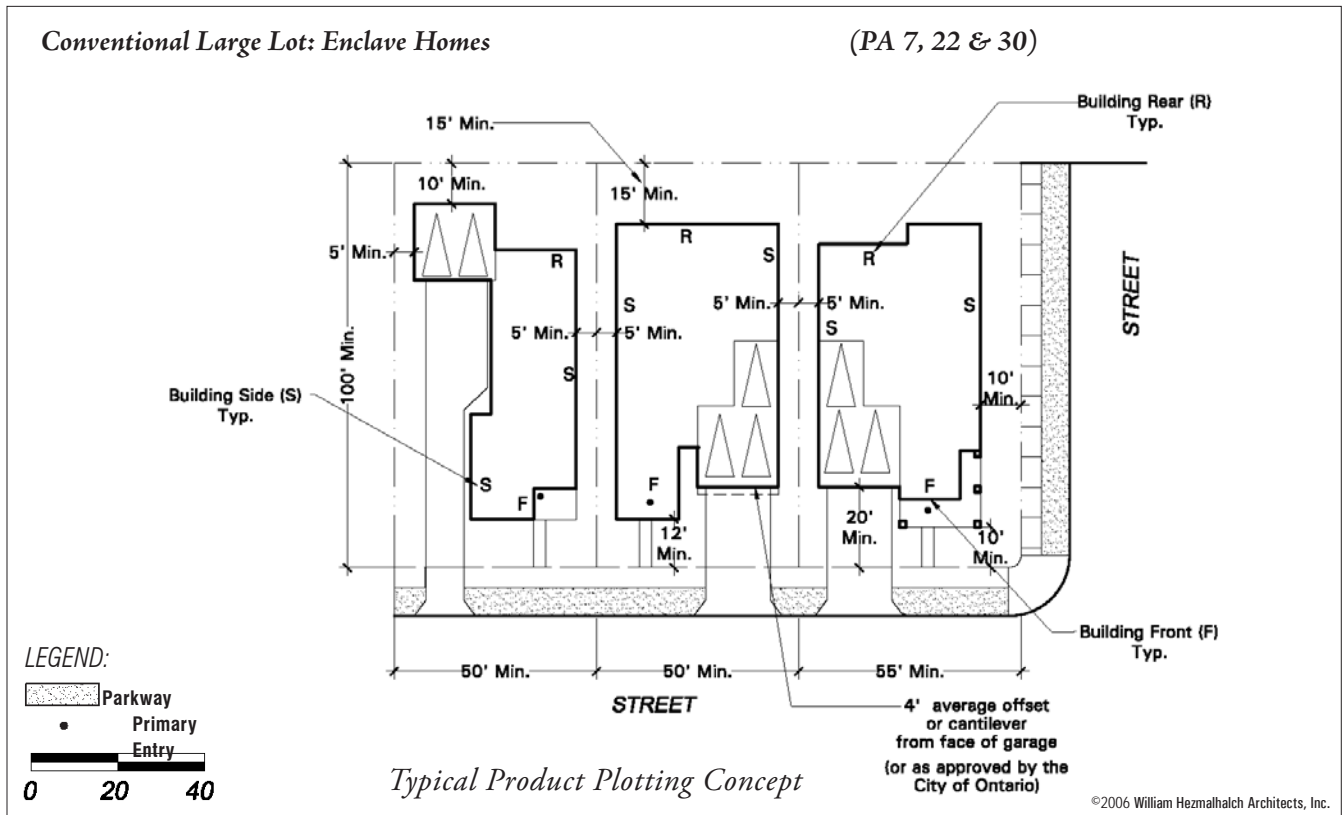
Notes:

1. Actual lot sizes and densities may vary based on final plan.
 2. Refer to other areas of Section 7 - Residential Design Guidelines for more specific criteria regarding building design, plotting, styles, materials, details, and implementation.
 3. Refer to Section 7.6 Landscape Design, for specific criteria regarding landscape design and implementation.
 4. Typical Product Plotting Concepts are provided for representative purposes only and are not intended to be mandated layouts.
- * Proposed representative product types in each planning area are subject to final development submittal and are subject to change.



Conventional Large Lot: Enclave Homes

(PA 7, 22 & 30)



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Minimum Standards

Lot Dimensions

Minimum lot size	5,000 sq. ft.
Lot width on front	50'
Lot width on corner	55'
Lot depth on standard lot	100'
Maximum building coverage	45%
Minimum front lot width on cul-de-sac, standard knuckle or modified knuckle street	35'

Minimum lot dimensions on curvilinear streets shall be measured at front and rear building setbacks.

Building Separation

Side to side living space	10' (5'+5')
---------------------------	-------------

Front Setbacks - To Back of Sidewalk - Street Facing

Living space	12'
Porch, balcony	10'
Garage	20'
Side-on garage*	10'

Building Height

Architectural projections may exceed the maximum building height of 35 feet by an additional 10 feet.

Side Setbacks - To Back of Sidewalk - Street Facing

Living space	10'
Porch, balcony	10'
Side-on garage*	15'
Corner facing garage	20'

Rear Setbacks to Property Line

Living space (ground floor)	15' minimum
Recessed garage	10'

Encroachments into Setbacks

Fireplace, media niche (8' max length), bay windows, potshelves, brackets, roof overhangs etc.	2' max encroachment, 3' min. from PL
--	--------------------------------------

Second floor cantilever over garage (or as approved by the City of Ontario)	4' max encroachment, 3' min. from PL
---	--------------------------------------

Private Open Space Per Unit

Minimum dimension	7'
Total per unit	70 s.f.

Parking

Per City of Ontario Development Code

*Side-on garages are prohibited on lots less than 60' wide



7.6 LANDSCAPE

7.6.1 Introduction

Careful consideration has been given to the design of the community landscape architectural character for the Specific Plan. The following design guidelines are organized to help define the basic landscape design principles for the Specific Plan. Observing these guidelines will help to assure the “design vision” and integrity of this planned community.

All landscape plans, streetscape plans, and graphic designs with regard to community identity, neighborhood identity, or entry monumentation shall conform to the guideline as set forth herein, and shall be subject to review and approval by the City of Ontario.

7.6.2 Overall Streetscapes and Entries

PERIMETER STREETScape DESIGN

Streetscape design guidelines establish a hierarchy for the landscape development along the surrounding roadways, as well as establishing a framework for consistency of design. Three (3) major master planned arterial roadways surround the Specific Plan area: Archibald Ave. to the west, Eucalyptus Ave. to the north, and Haven Ave. to the east. Landscape easements associated with these roadways have been defined, as noted in The Ontario Plan (TOP) Policy Plan (General Plan).

Landscape development surrounding the Specific Plan area will help to set the character, while maintaining consistency with the City of Ontario’s pedestrian pathway system illustrated within the Mobility Element Exhibit M-3: Multipurpose Trails and Bikeway Corridor Plan of The Ontario Plan (TOP) Policy Plan (General Plan).

Archibald Ave.

Archibald Ave. streetscape (North of Merrill Ave.) shall include the following:

- ♦ A landscaped parkway 7-foot wide parkway on the west side and 10-foot wide parkway on the east side with a double row of street trees per the New Model Colony Streetscape Master Plan along both sides of the street;
- ♦ A split sidewalk (5-foot wide)/bikeway (8-foot wide) along the west side of the street. Multipurpose trail shall be separated from the concrete sidewalk and shall be constructed of stabilized decomposed granite;
- ♦ A landscaped median (26-foot wide) with a single row of street trees per the New Model Colony Streetscape Master Plan;
- ♦ A landscape easement of fifty (50) feet measured from face of curb to perimeter wall on both sides of the street;
- ♦ Background trees (minimum of 24-inch box) and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant grouping while combining interesting foliage textures and color; and

- ♦ Monumentation as shown in the *Conceptual Landscape Master Plan, Exhibit 20*.
- ♦ Refer to the *Streetscape/Paseo Sections, Exhibit 21 and Archibald Ave., Exhibit 22A* for illustration.

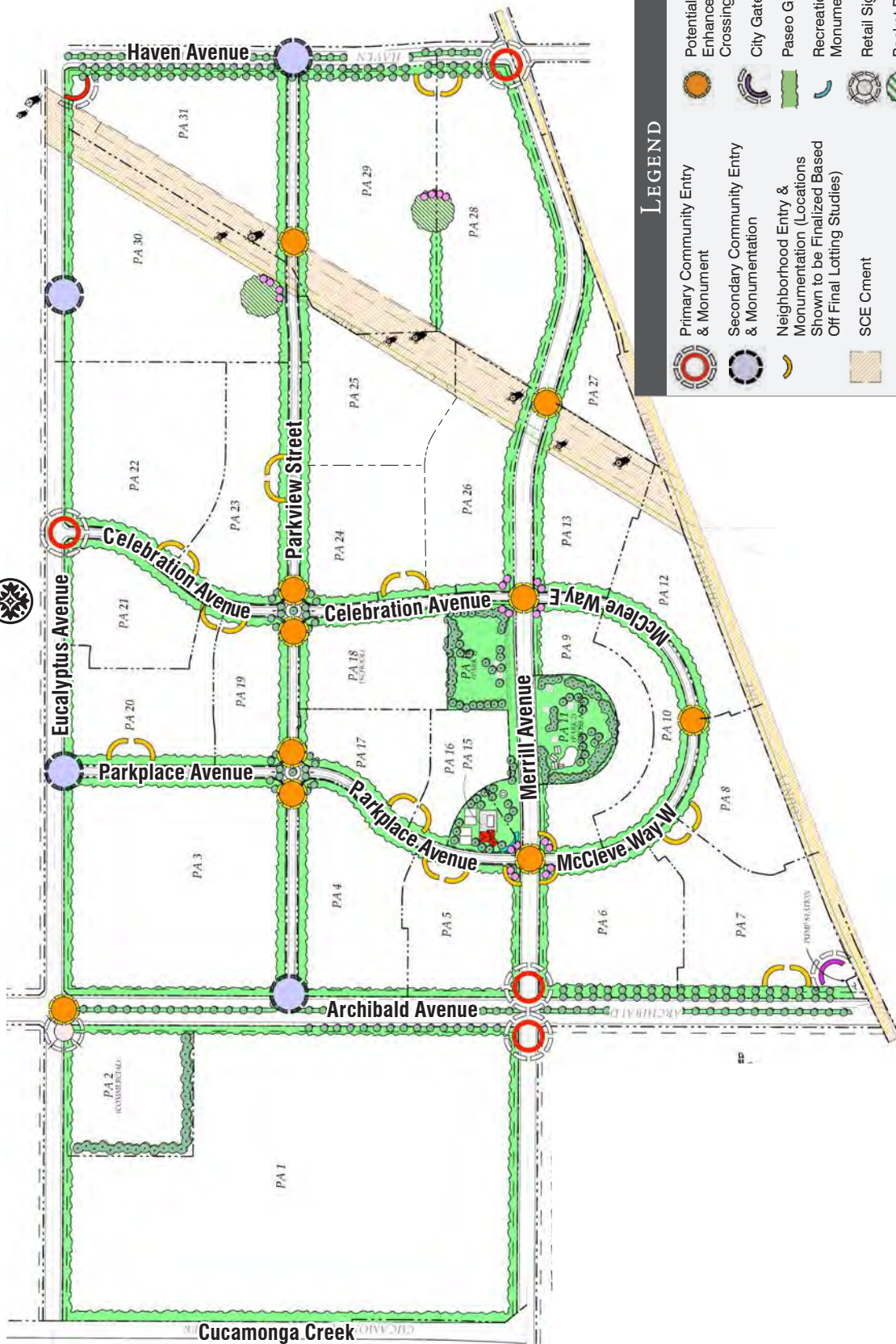
Archibald Ave. streetscape (South of Merrill Ave.) shall include the following:

- ♦ A landscaped parkway 7-foot wide parkway on the west side and 10-foot wide parkway on the east side with a double row of street trees per the New Model Colony Streetscape Master Plan along both sides of the street;
- ♦ A sidewalk (5-foot wide) along both sides of the street. Multipurpose trail shall be separated from the concrete sidewalk and shall be constructed of stabilized decomposed granite;
- ♦ A landscaped median (26-foot wide) with a single row of street trees per the New Model Colony Streetscape Master Plan;
- ♦ A landscape easement of 50-foot measured from face of curb to perimeter wall on both sides of the street;
- ♦ Background trees (minimum of 24-inch box) and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant groupings while combining interesting foliage textures and color; and
- ♦ Monumentation as shown in the *Conceptual Landscape Master Plan, Exhibit 20*.
- ♦ Refer to the *Streetscape/Paseo Sections, Exhibit 21 and Archibald Ave., Exhibit 22* for illustration.

Eucalyptus Ave.

Eucalyptus Ave. streetscape shall include the following:

- ♦ A landscaped parkway (7-foot wide min.) with a single row of street trees per the City of Ontario Street Tree Master Plan along both sides of the street;
- ♦ A lineal sidewalk (5-foot wide) on the south side of the street, with a (13-foot wide) sidewalk/bike path on the north side;
- ♦ A landscape easement of 35 feet measured from face of curb to perimeter wall on both sides of the street;
- ♦ Background trees (minimum of 24-inch box) and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant groupings while combining interesting foliage textures and color; and
- ♦ Monumentation as shown in the *Conceptual Landscape Master Plan, Exhibit 20*.
- ♦ Refer to the *Streetscape/Paseo Sections, Exhibit 21 and Eucalyptus Ave., Exhibit 25* for illustration.



LEGEND

	Primary Community Entry & Monument		Potential Location for Enhanced Pedestrian Crossings
	Secondary Community Entry & Monumentation		City Gateway Monument
	Neighborhood Entry & Monumentation (Locations Shown to be Finalized Based Off Final Lotting Studies)		Paseo Greenbelt
	SCE Cment		Recreation Center/Park Monumentation
			Retail Signage
			Pocket Park

Note: Enhanced pedestrian crossings (pavers, stamped, scored, colored concrete/paving, and type of landscaping installed in and/or adjacent to pedestrian crossings) will be provided at various locations within the Specific Plan as approved by the City of Ontario.



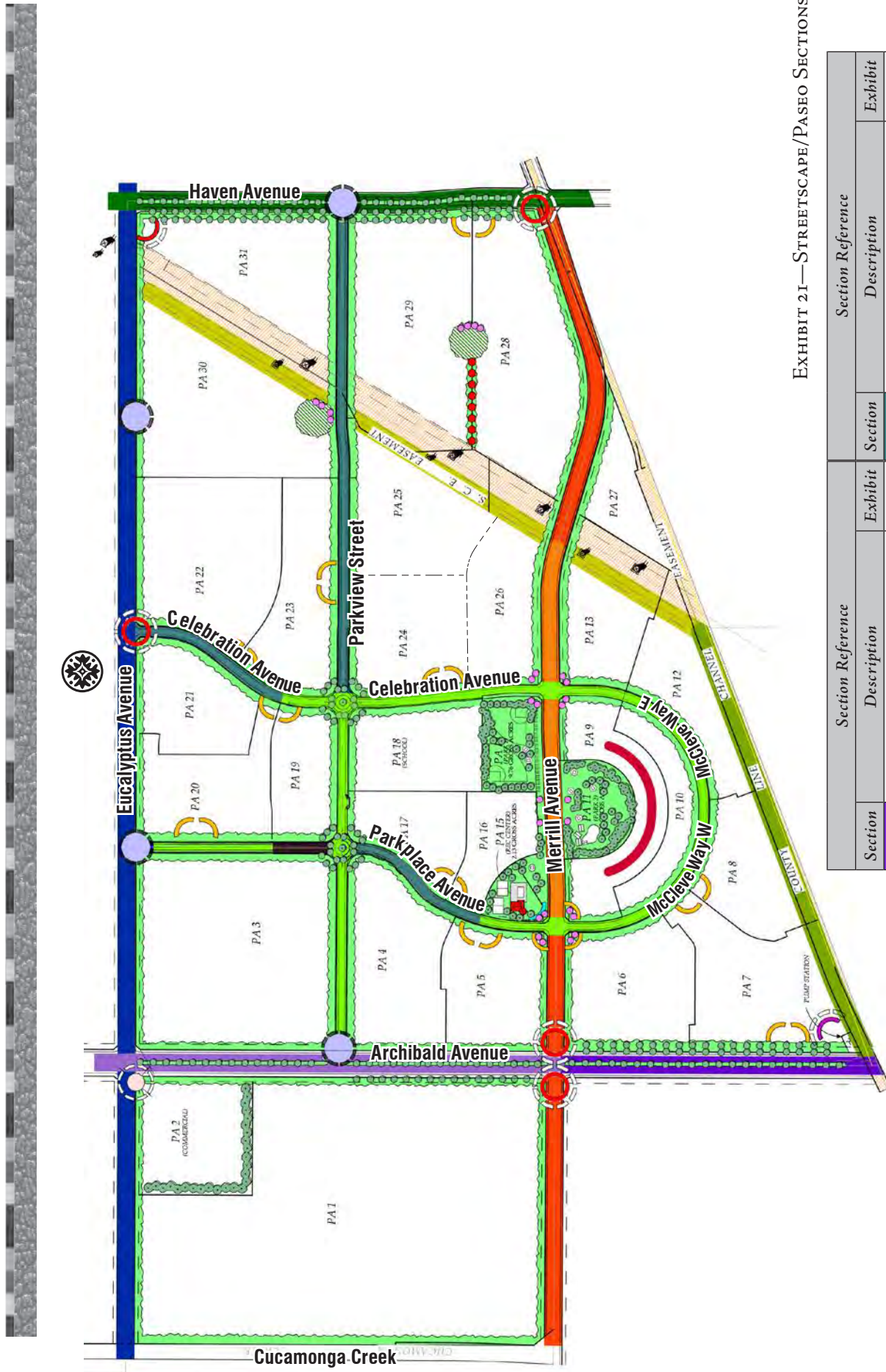
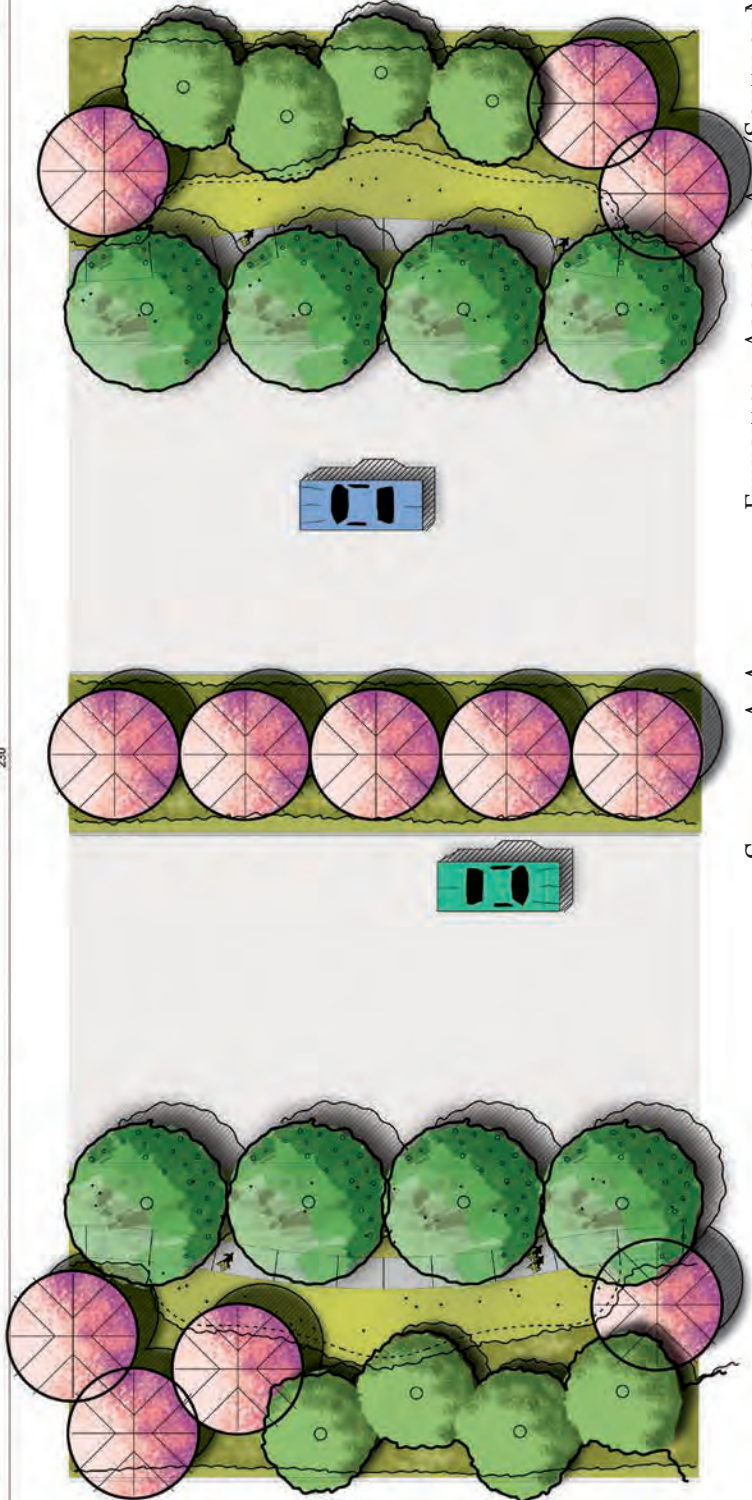
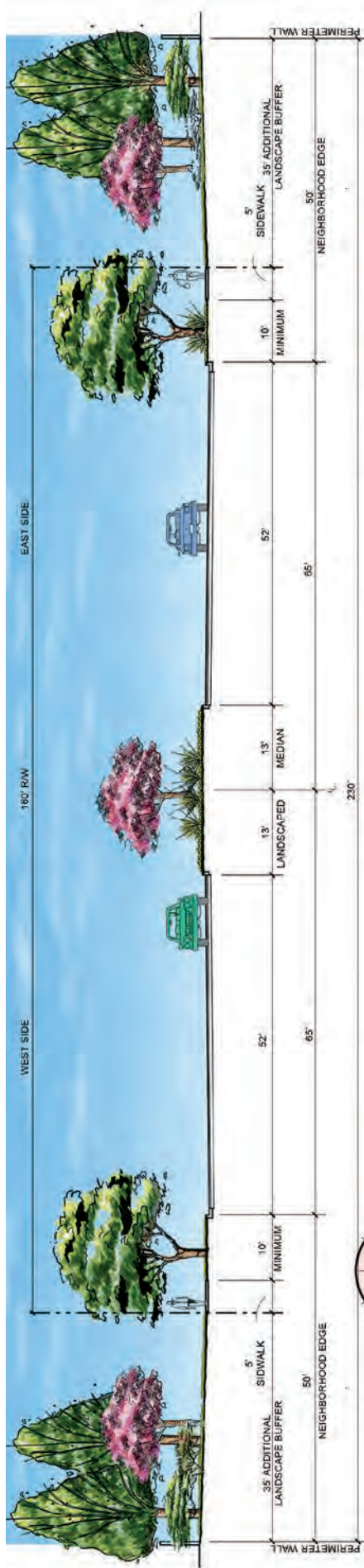


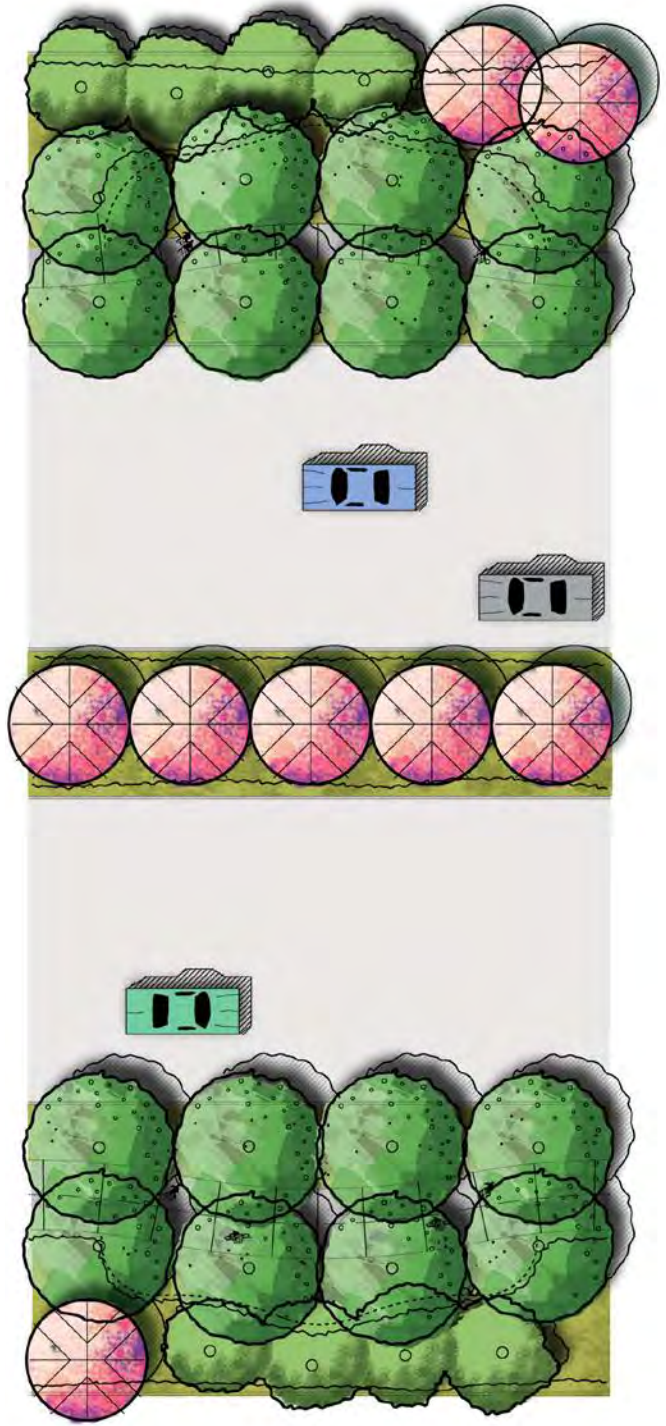
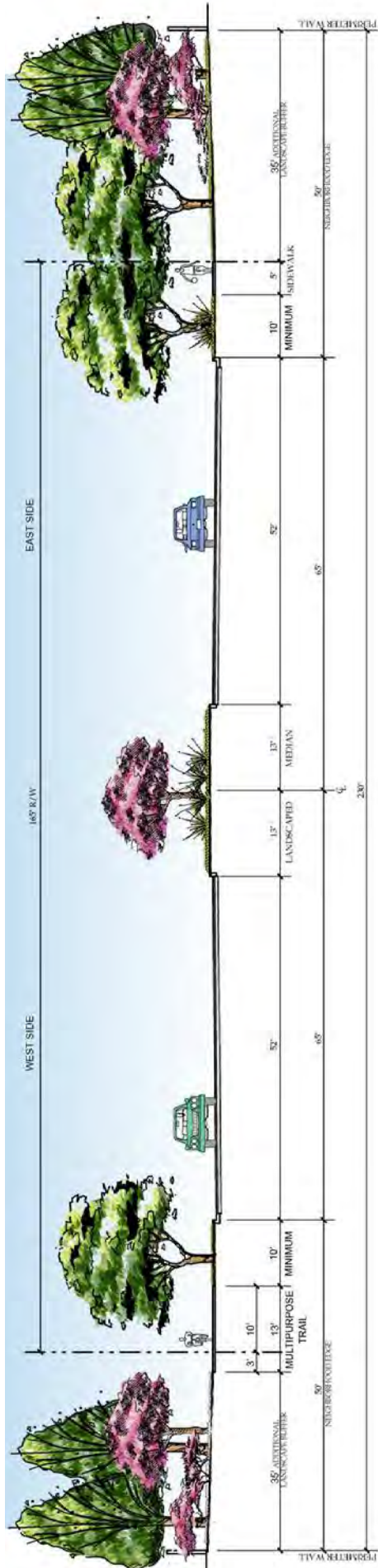
EXHIBIT 2I—STREETSCAPE/PASEO SECTIONS

Section Reference		Section Reference	
Section	Description	Exhibit	Description
A-A	Archibald Ave. (South of Merrill Ave.)	22	Primary Local Streets (60'R/W with 14' Total Landscape Buffer)
B-B	Archibald Ave. (North of Merrill Ave.)	22A	Primary Local Streets (60'R/W with 7' Total Landscape Buffer)
C-C	Merrill Ave. (From Archibald to Rec. Center & from SCE Easement to Haven Ave.)	23	Primary Local Streets (60'R/W)
D-D	Merrill Ave. (From Rec Center to SCE Easement)	23A	Lanes (Private Alleys)
E-E	Haven Ave.	24	Internal Paseo Easement
F-F	Eucalyptus Ave.	25	SCE Easement Pasco System
			County Line Channel Pasco System



SECTION A-A

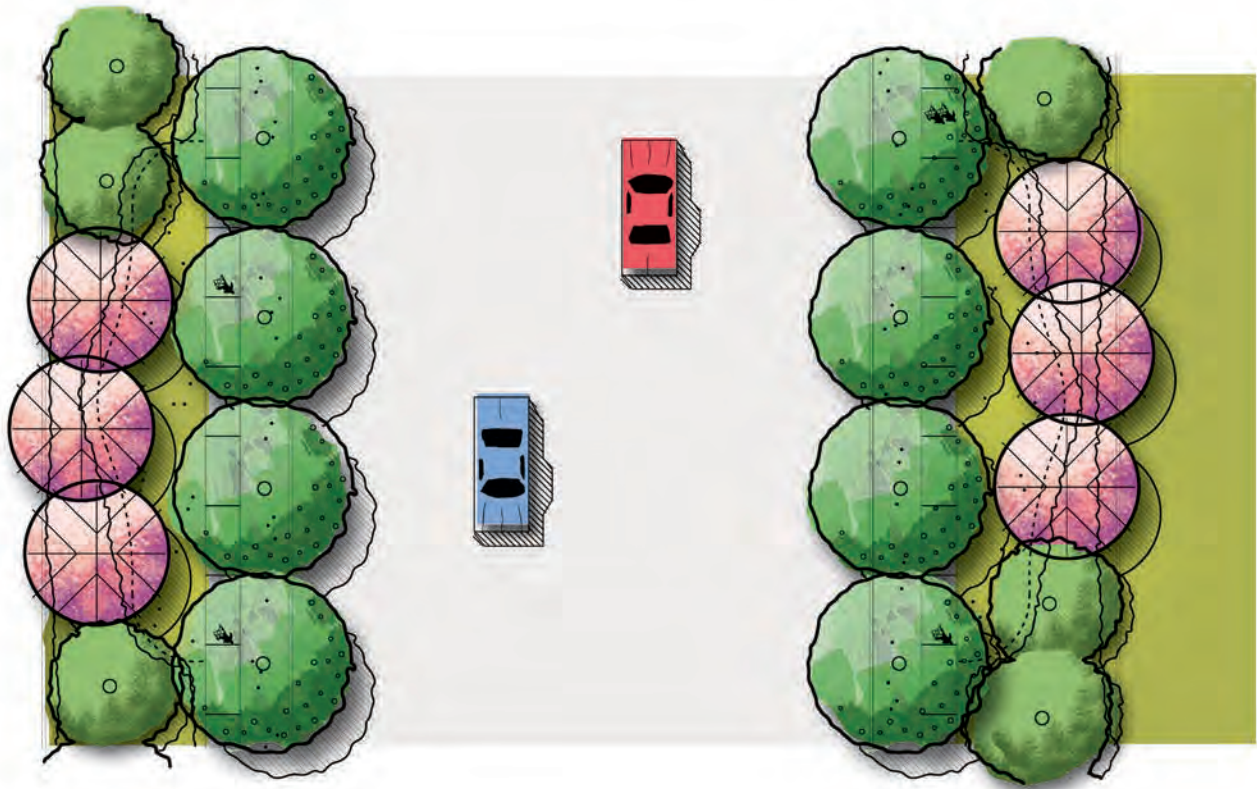
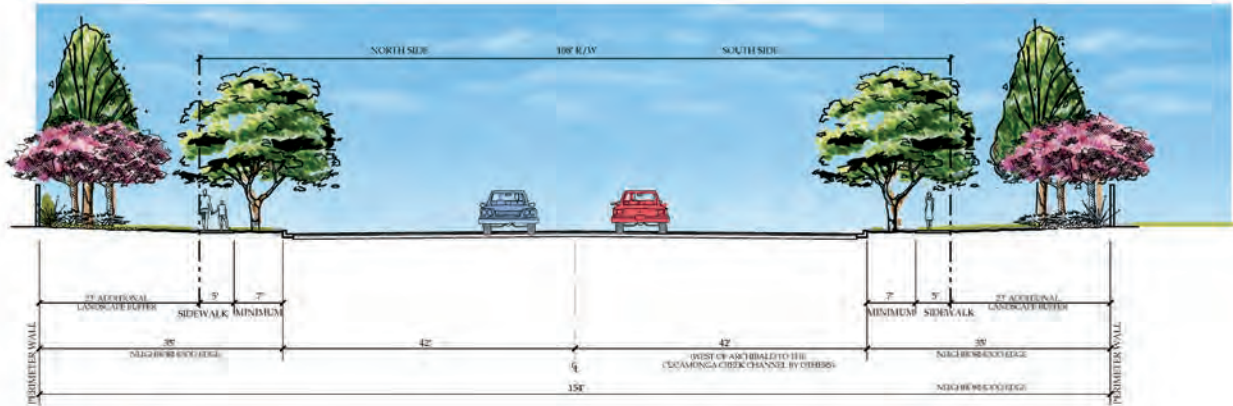
EXHIBIT 22—ARCHIBALD AVE. (SOUTH OF MERRILL AVE.)



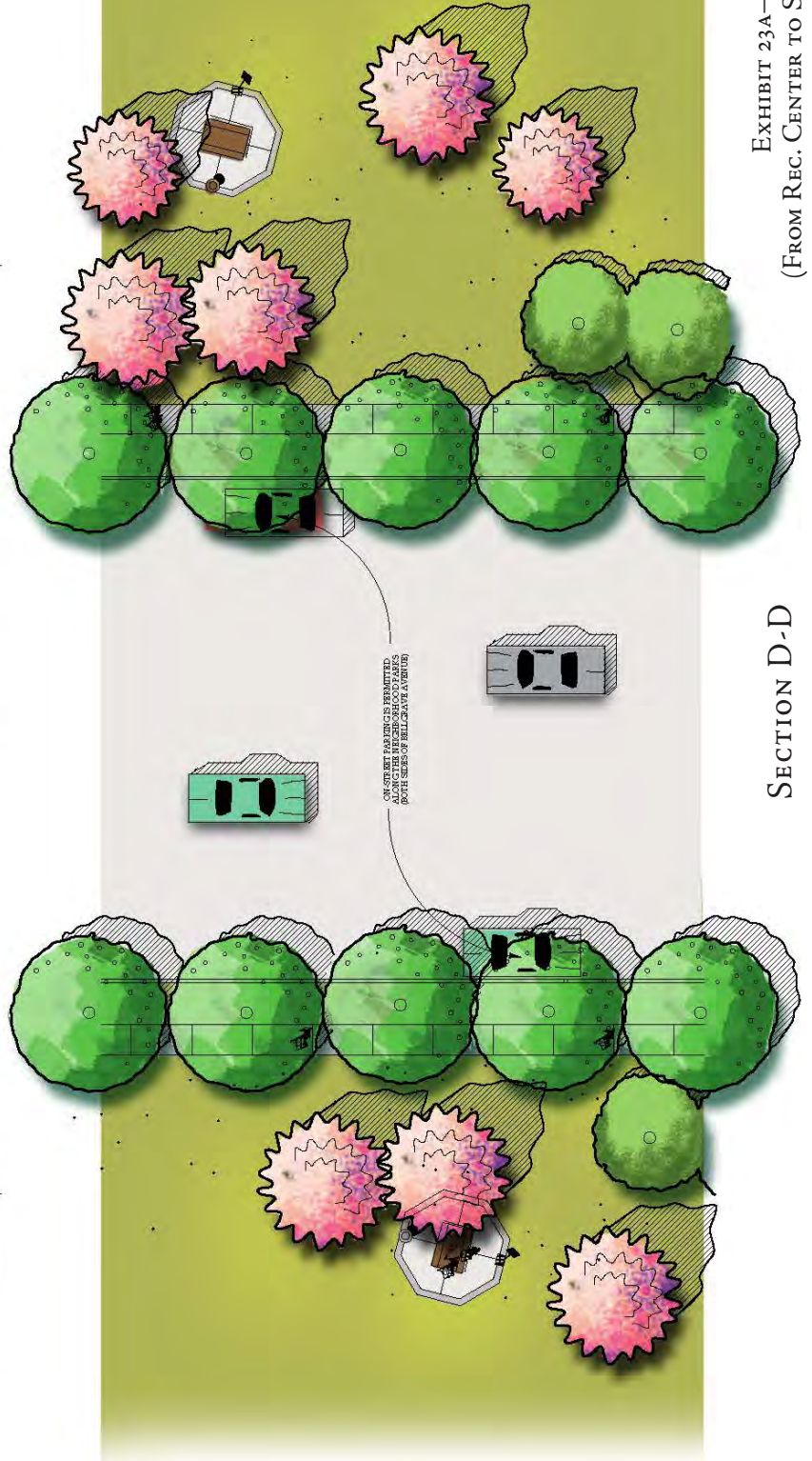
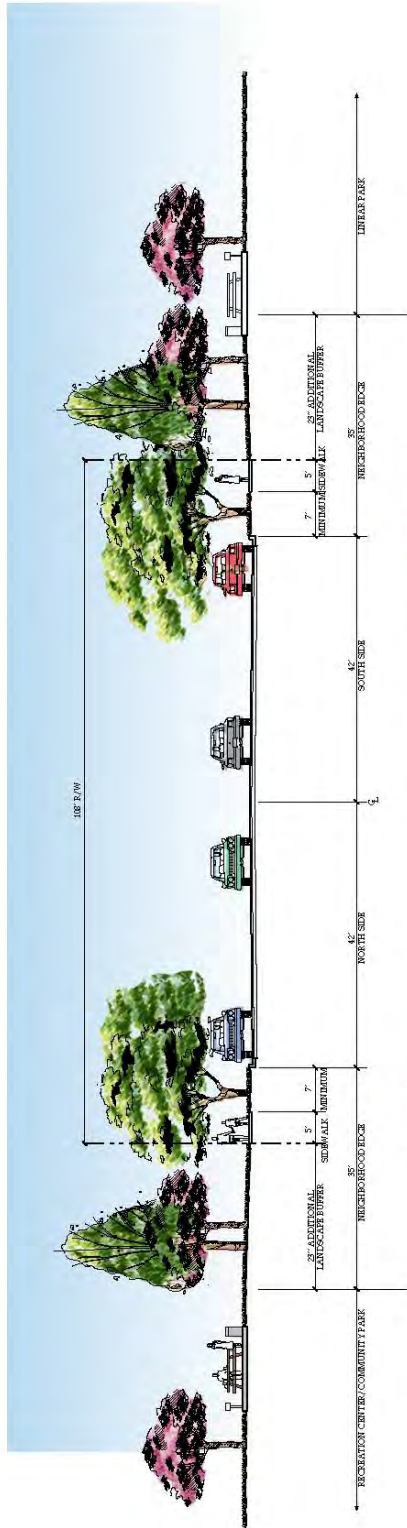
SECTION B-B

EXHIBIT 22A—ARCHIBALD AVE. (NORTH OF MERRILL AVE.)





SECTION C-C



SECTION D-D

EXHIBIT 23A—MERRILL AVE.
(FROM REC. CENTER TO SCE EASEMENT)



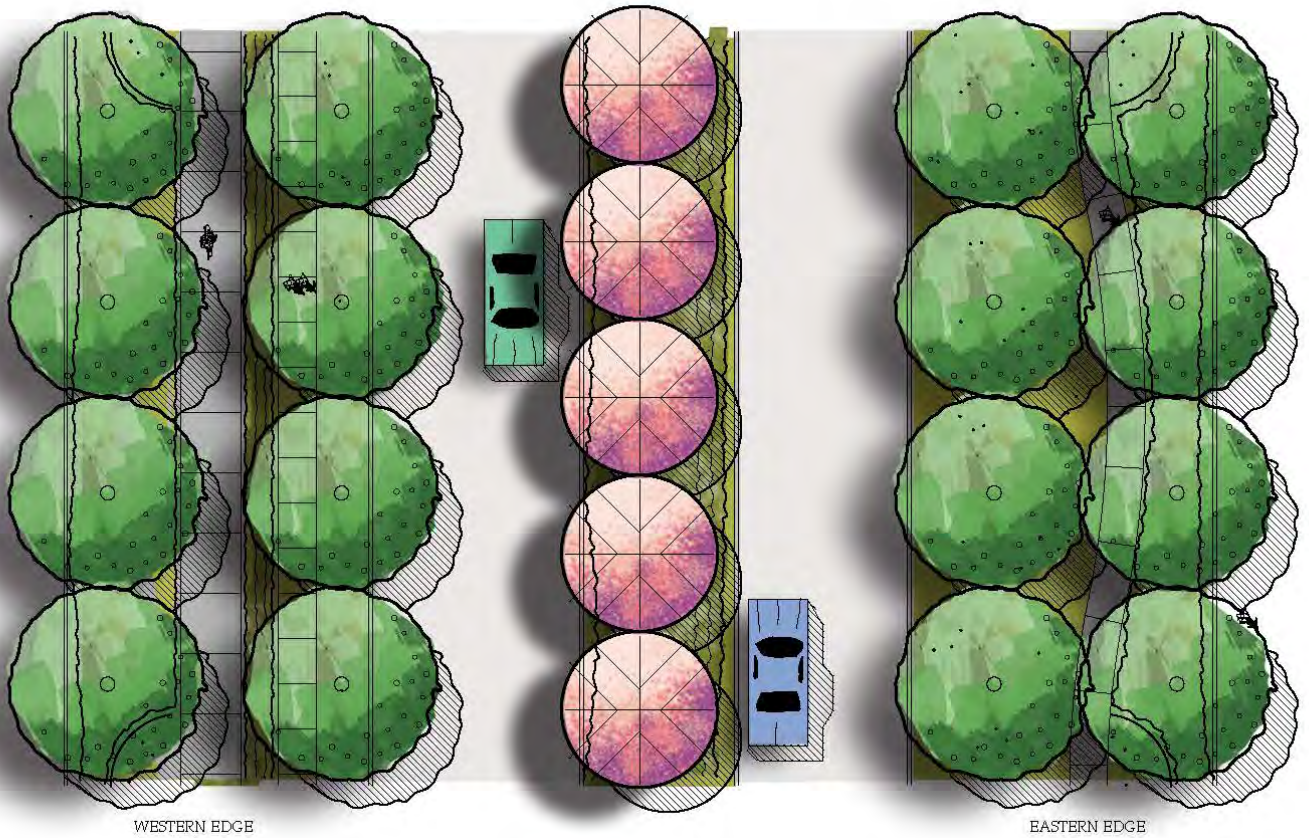
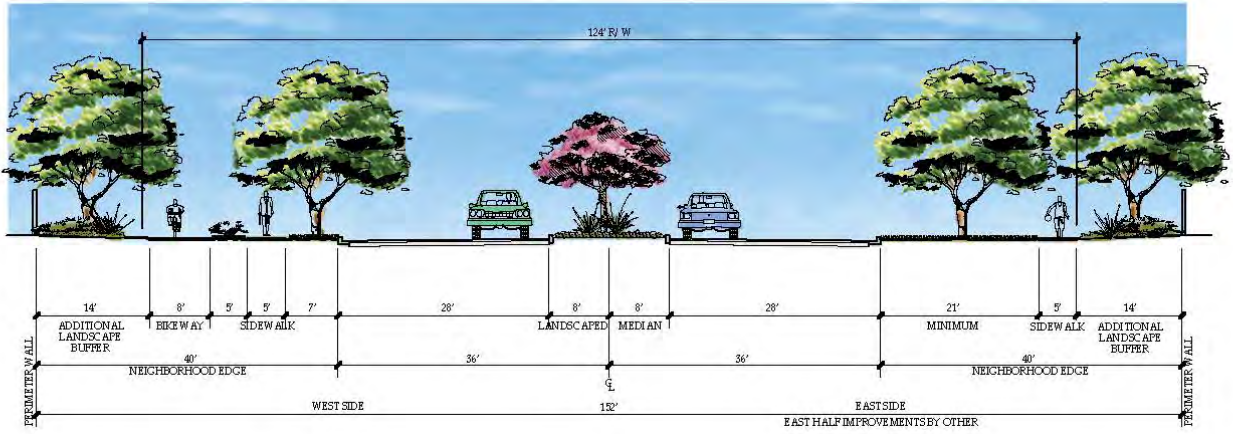
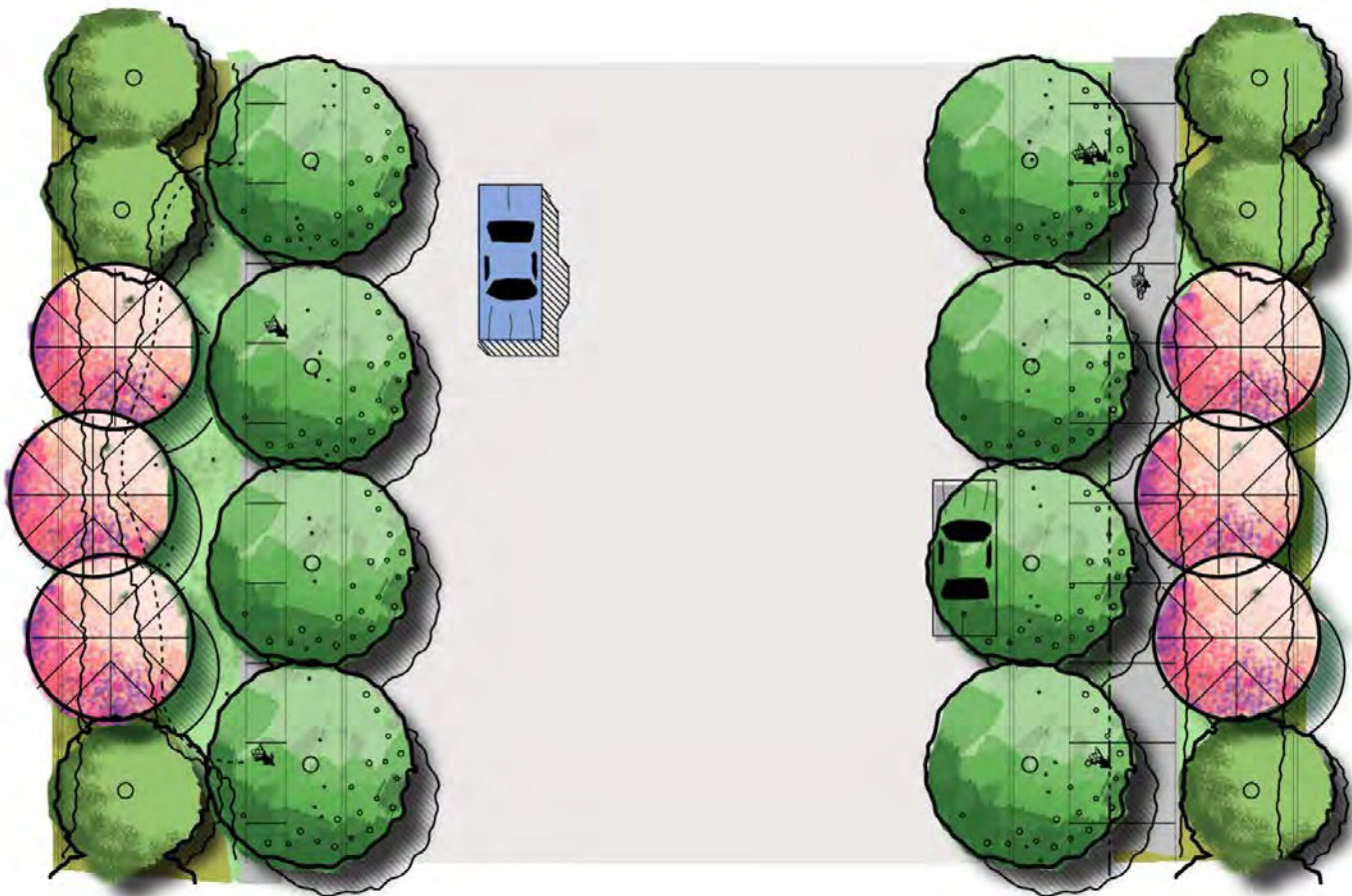
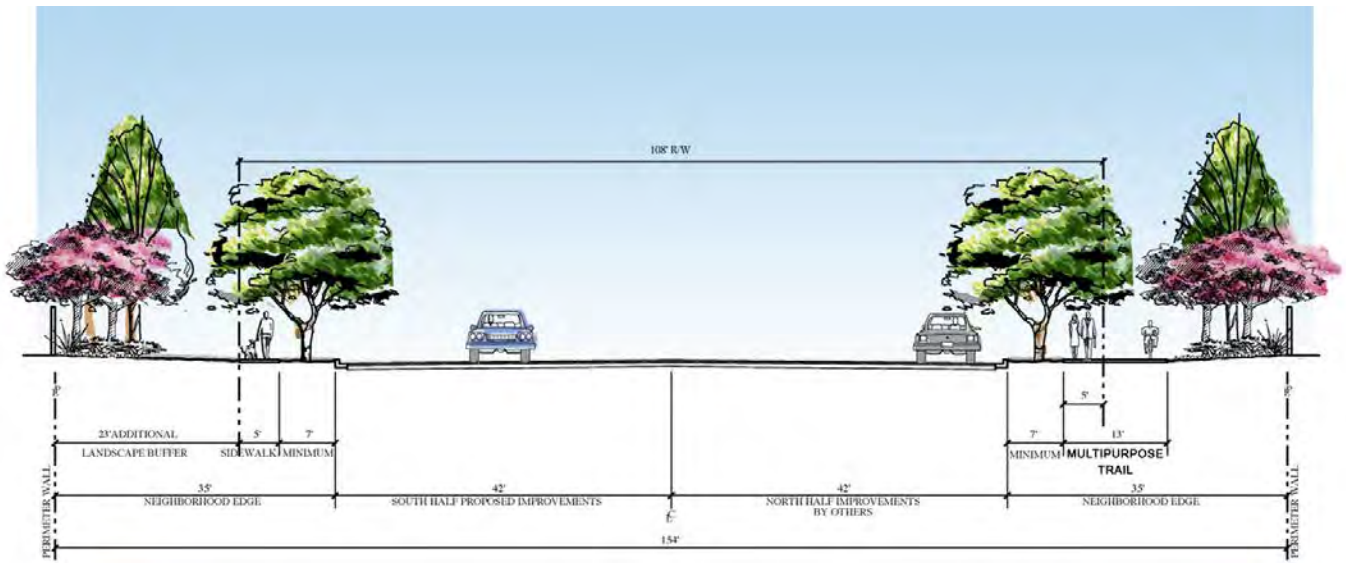


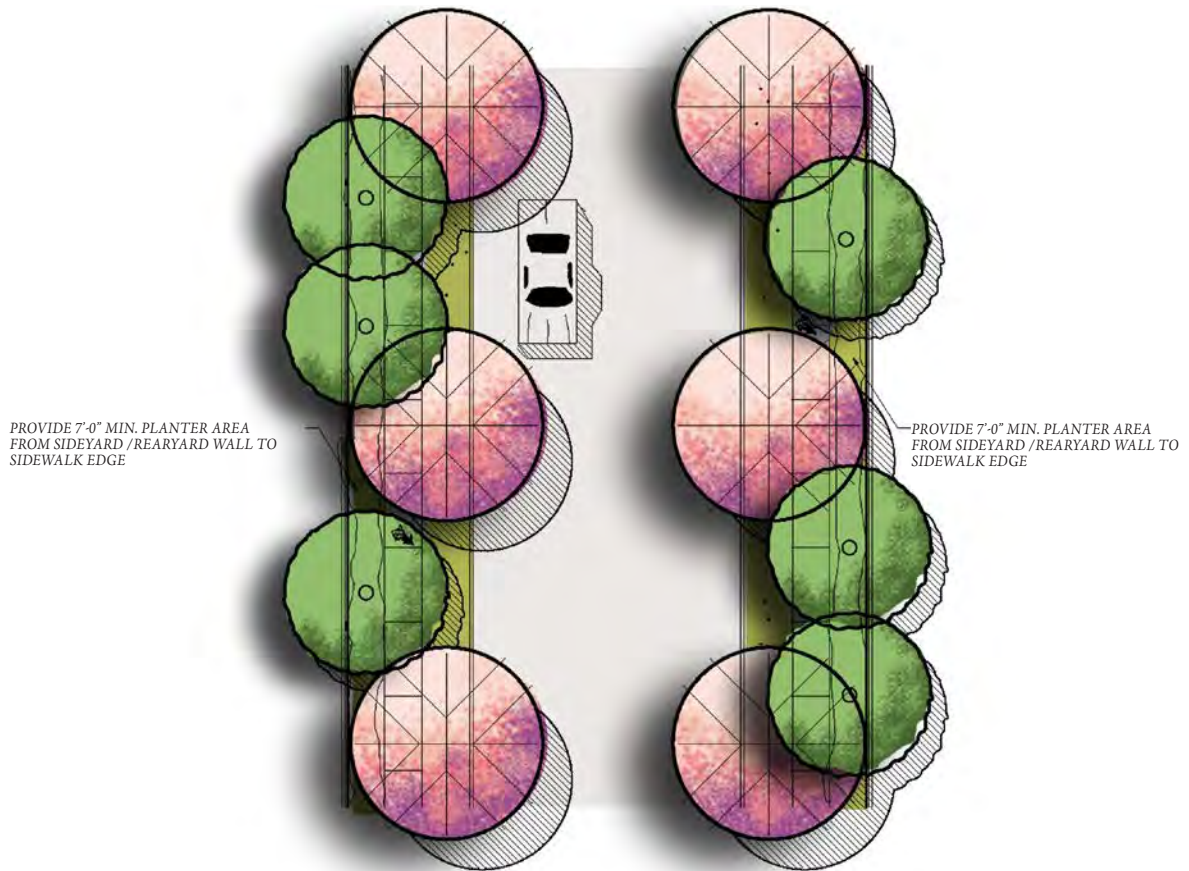
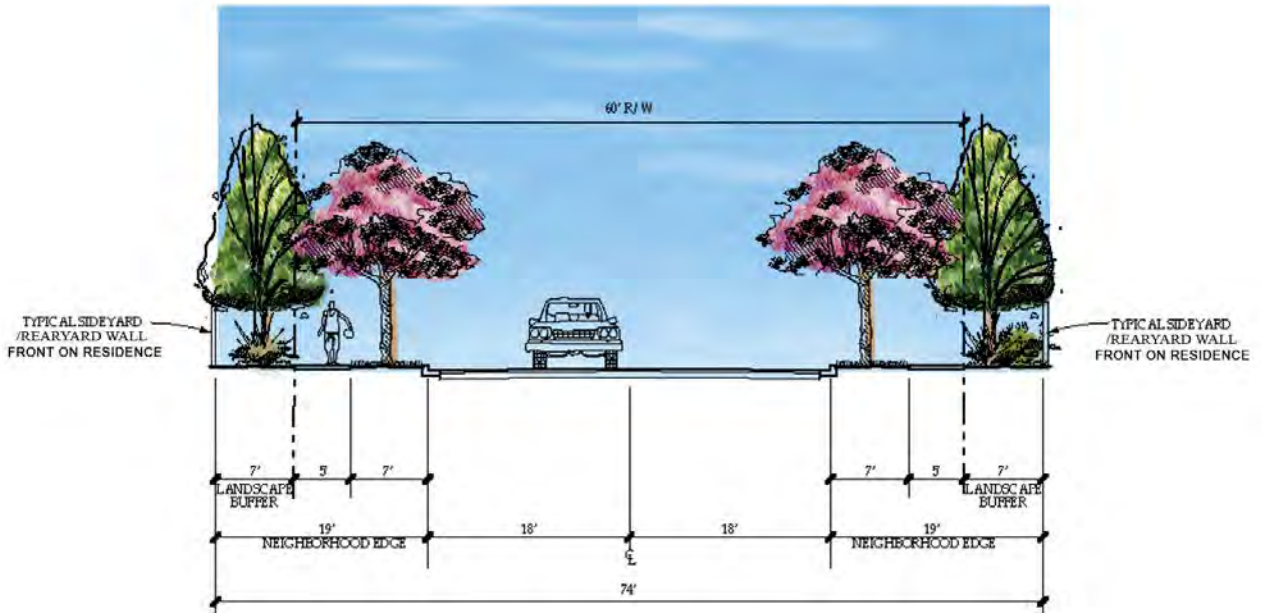
EXHIBIT 24—HAVEN AVE.



SECTION F-F

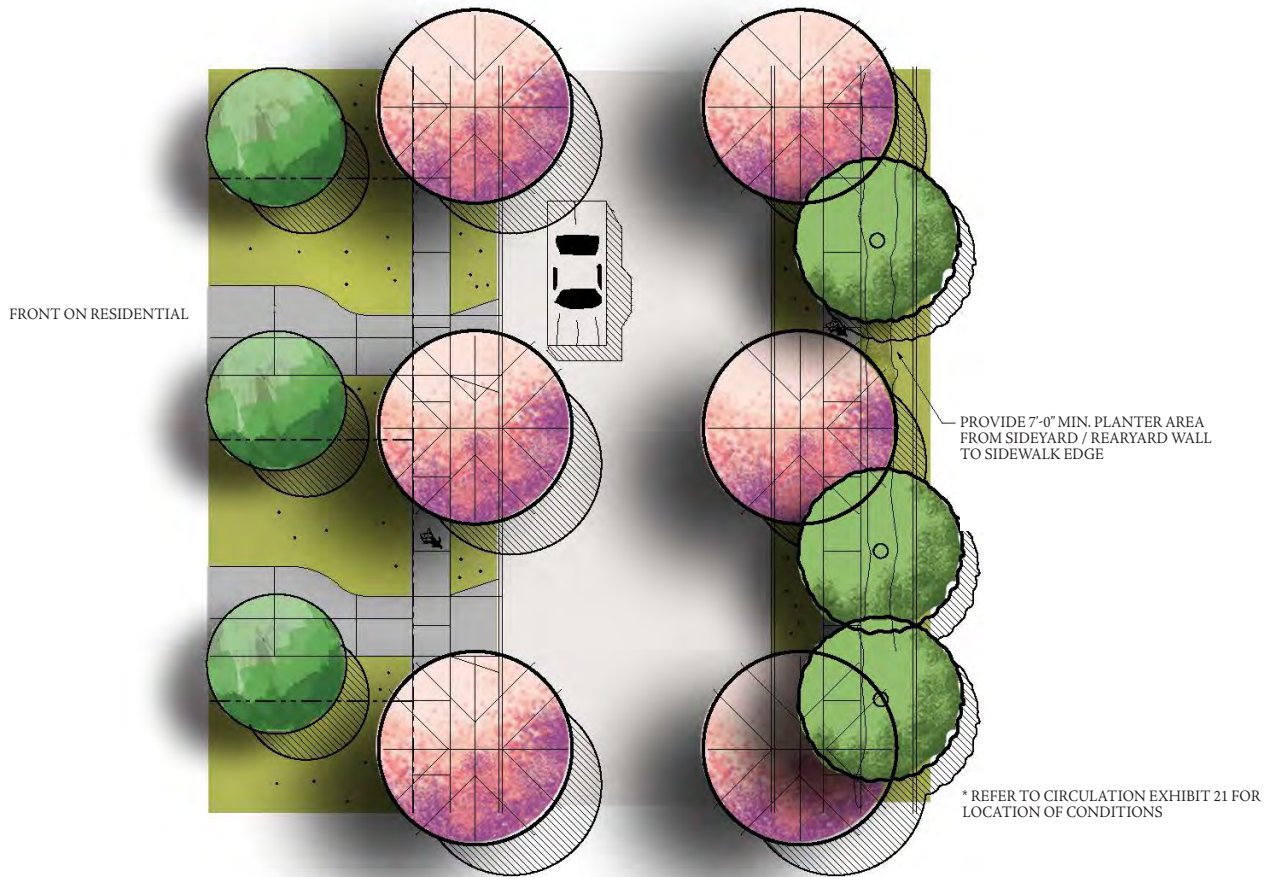
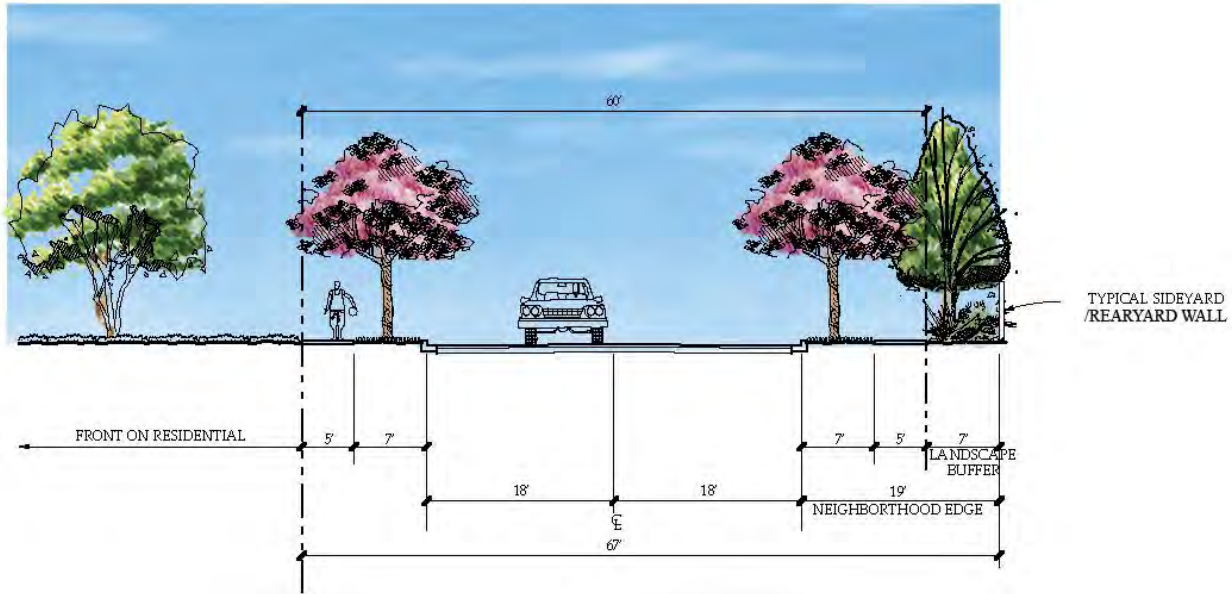
EXHIBIT 25—EUCALYPTUS AVE.

SPECIFIC PLAN — OCTOBER 2006
 AMENDED APRIL 21, 2015



SECTION G-G

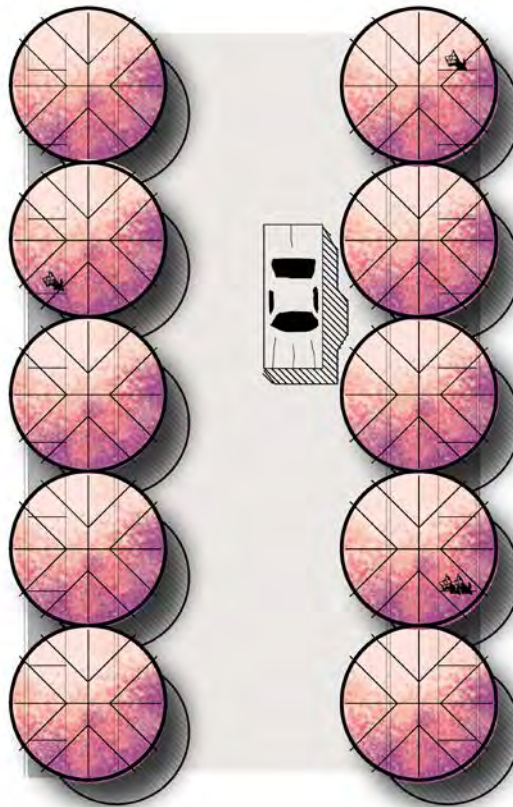
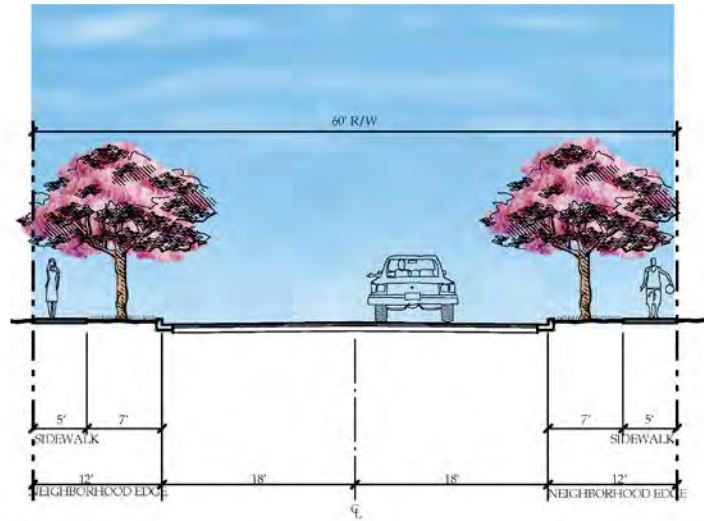
EXHIBIT 26—PRIMARY LOCAL STREET (60' R/W WITH 14' TOTAL LANDSCAPE BUFFER)



SECTION H-H

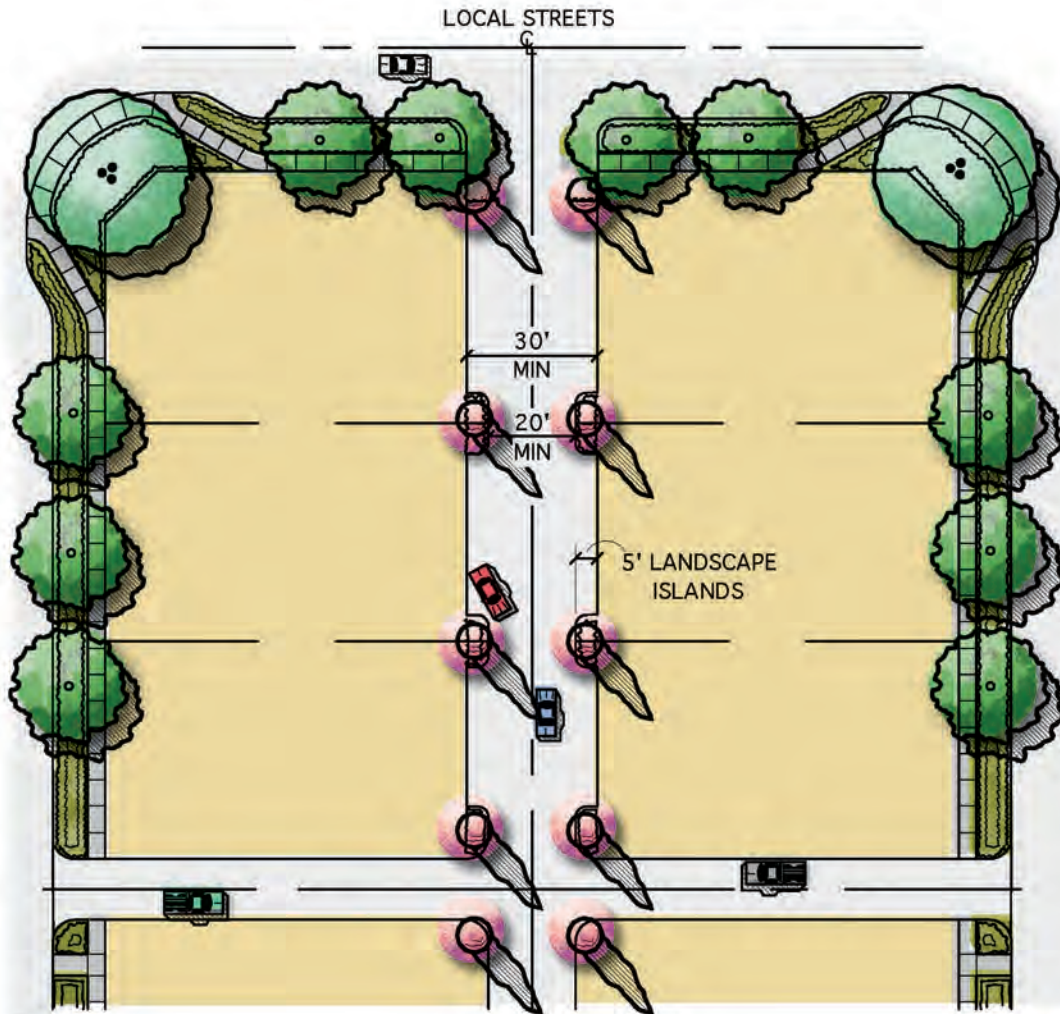
EXHIBIT 26A—LOCAL AND CUL-DE-SAC STREET
(60' R/W WITH 12' PARKWAY AND 7' LANDSCAPE BUFFER)

SPECIFIC PLAN — OCTOBER 2006
AMENDED APRIL 21, 2015



SECTION I-I

EXHIBIT 26B—PRIMARY LOCAL STREET (60' R/W)



SECTION J-J



EXHIBIT 27—LANES (PRIVATE ALLEYS)

SPECIFIC PLAN — OCTOBER 2006
AMENDED APRIL 21, 2015

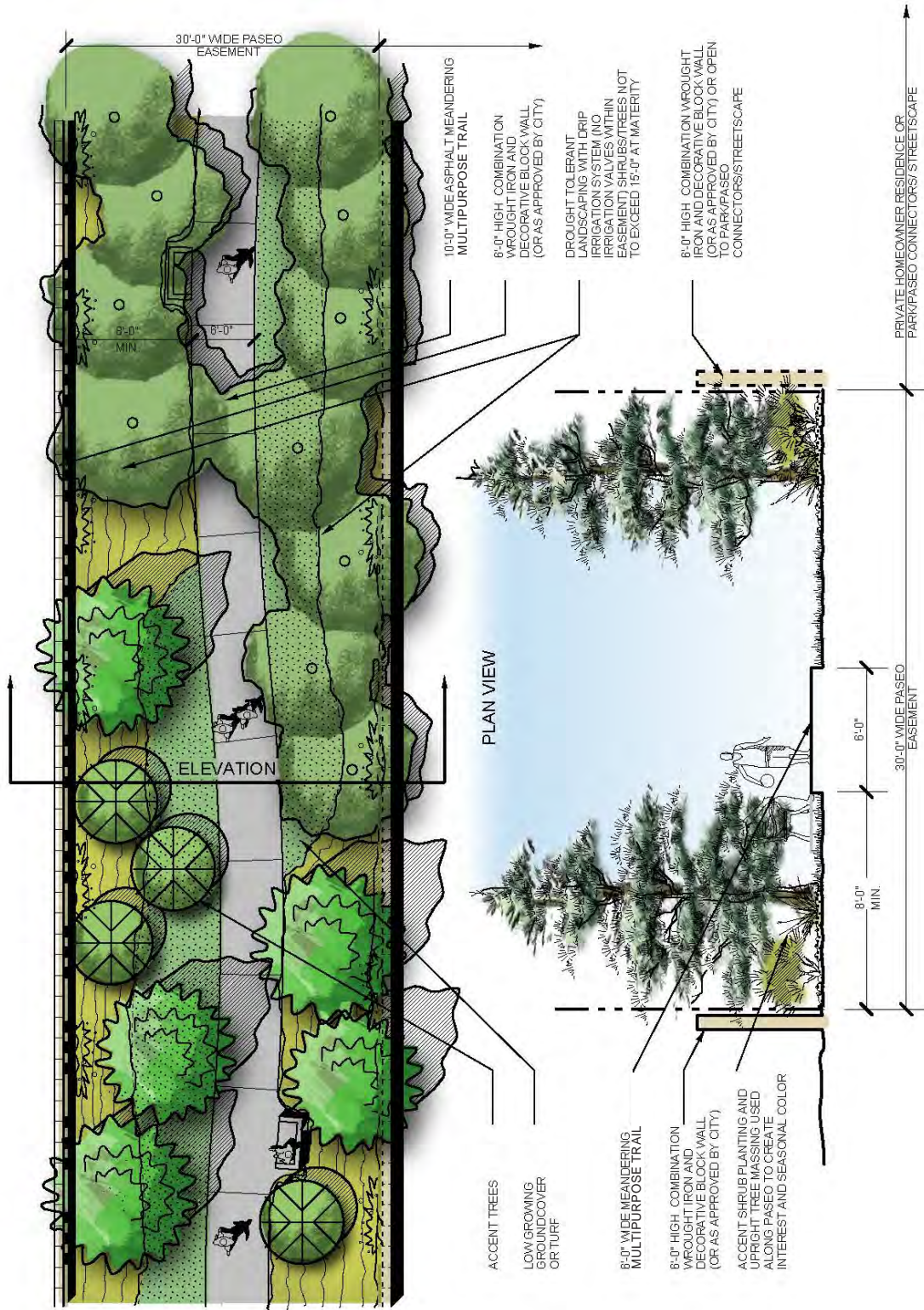


EXHIBIT 28—INTERNAL PASEO EASEMENT

SECTION K-K



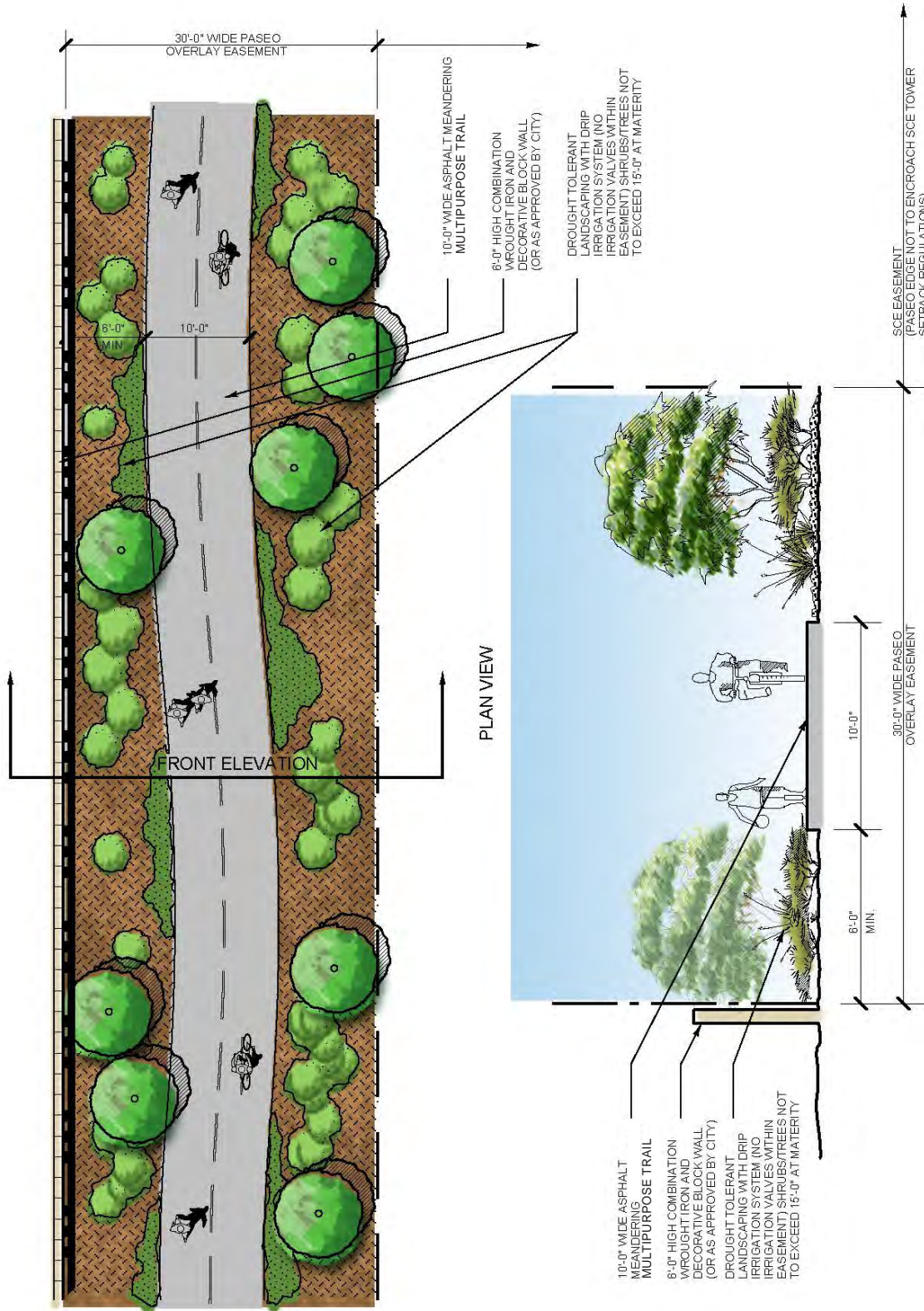
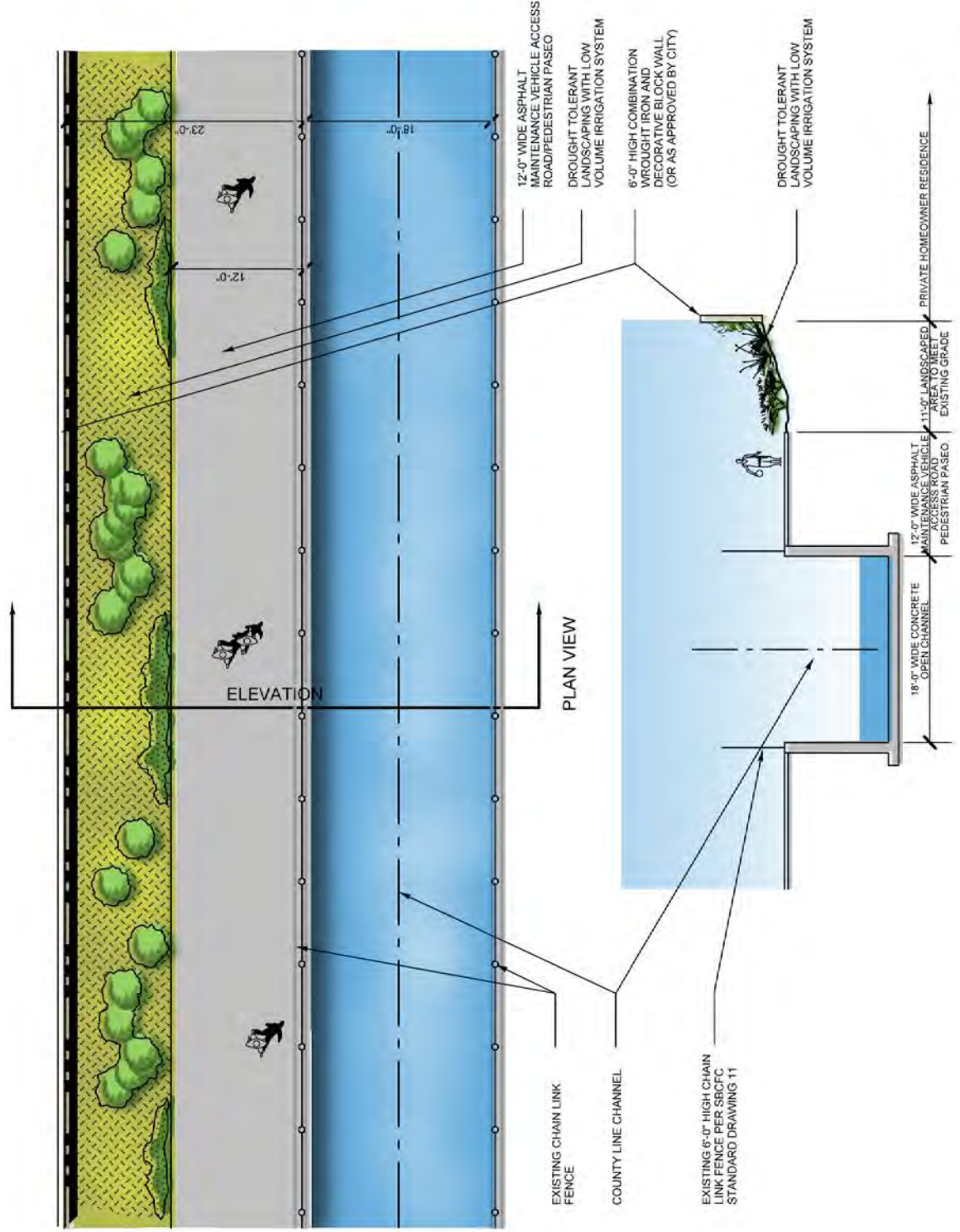


EXHIBIT 29—SCE EASEMENT PASEO SYSTEM



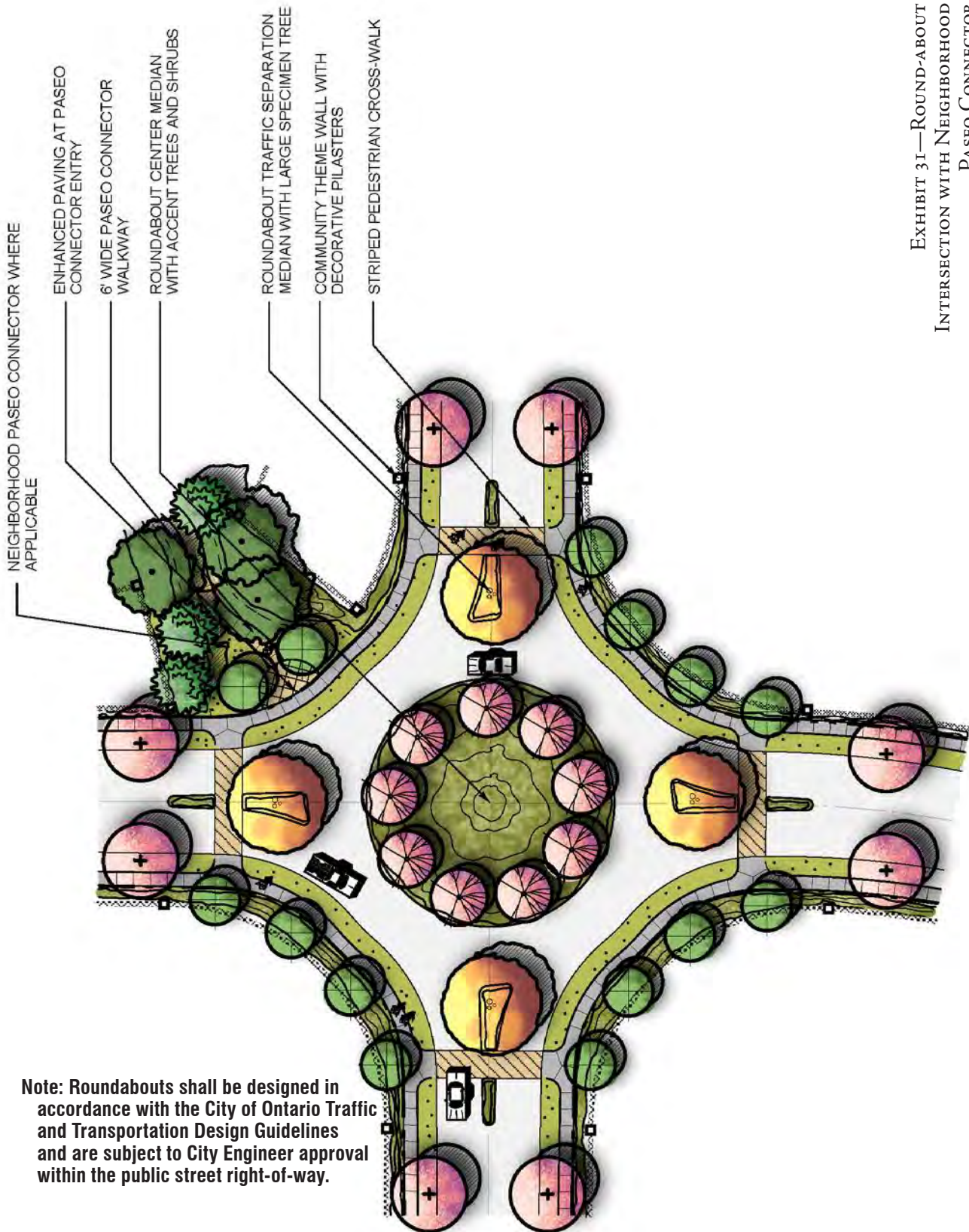


EXHIBIT 31—ROUND-ABOUT
INTERSECTION WITH NEIGHBORHOOD
PASEO CONNECTOR



Haven Ave.

Haven Ave. streetscape shall include the following:

- ♦ A landscaped parkway (16-foot wide min.-west side, 21-foot wide min.-east side) with a double row of street trees per the City of Ontario Street Tree Master Plan along both sides of the street;
- ♦ A split sidewalk (5-foot wide)/bikeway (8-foot wide) on the west side of Haven Ave. and a lineal sidewalk (5-foot wide) on the east side of Haven Ave.;
- ♦ A landscape easement of forty (40) feet measured from face of curb to perimeter wall on both sides of the street;
- ♦ Background trees (minimum 24-inch box) and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant groupings while combining interesting foliage textures and color;
- ♦ Monumentation as shown in the *Conceptual Landscape Master Plan, Exhibit 20*; and
- ♦ Refer to the *Streetscape/Paseo Sections, Exhibit 21 and Haven Ave., Exhibit 24* for illustration.

7.6.3 Interior Streetscape Design

Streetscape design within the interior of the Specific Plan area shall be consistent in character with the perimeter streetscapes and should help to promote pedestrian circulation into the “Community Green Core.”

Where interior streetscapes interface with community parks and open space, special consideration should be taken to integrate pedestrian circulation into these areas via a streetside pedestrian paseo system that links city sidewalk to active walking trails and open space uses. Refer to the Interior Local Street Paseo Edge for illustration.

Where interior streetscapes include a turn-about median, special considerations should be taken to integrate pedestrian circulation through enhanced landscaping and traffic calming. Refer to *Exhibit 31, Round-about Intersection with Neighborhood Paseo Connector* for illustration.

Two major interior roadways bring residents into the “Community Green Core”, Merrill Ave., extending east/west, and Turner Ave. that enters off Eucalyptus Ave., extending north/south.

Merrill Ave.

Merrill Ave. streetscape shall include the following:

- ♦ A landscaped parkway (7-foot wide min.) with a single row of street trees per the City of Ontario Street Tree Master Plan along both sides of the street;
- ♦ A lineal sidewalk (5-foot wide) along both sides of the street;
- ♦ A landscape easement of 35 feet taken from face of curb to perimeter wall on both sides of the street;
- ♦ Background trees (minimum of 24-inch box) and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant groupings while combining interesting foliage textures and color;
- ♦ Monumentation as shown in the *Conceptual Landscape Master Plan, Exhibit 19*; and
- ♦ Refer to the *Streetscape/Paseo Sections, Exhibit 21 and Merrill Ave. (From Archibald to Rec. Center & From SCE Easement to Haven Ave.), Exhibit 23* for illustration.

Merrill Ave. (between Community Recreation Center and the SCE Easement) streetscape shall include the following:

- ♦ A landscaped parkway (7-foot wide min.) and an additional landscaped parkway projection (7-foot wide by approximately 25-foot long) to accommodate a single row of street trees per the City of Ontario Street Tree Master Plan along both sides of the street;
- ♦ A lineal sidewalk (5-foot wide) along both sides of the street that makes additional connections to park and paseo walkways;
- ♦ A landscape easement of thirty-five (35) feet measured from face of curb to perimeter wall on both sides of the street;
- ♦ Background landscaping that harmonizes with the landscape character set in the “Community Green Core”;
- ♦ Monumentation as shown in the *Conceptual Landscape Master Plan, Exhibit 20*; and
- ♦ Refer to the *Streetscape/Paseo Sections, Exhibit 21 and Merrill Ave., Exhibits 23 and 23A* for illustration.



7.6.4 Lanes

Lanes will occur in single-family detached lane-loaded planning areas 9, 16, 17, and 19. In these product areas, garage access will be provided along lanes. Lanes consist of a twenty (20) feet wide asphalt travel way with five (5) feet of landscaping provided in planter pockets on both sides of the lane. Refer to *Exhibit 32, Conceptual Lane (Private Alley) Section*, which illustrates the conceptual location of planter pockets in a lane condition. Where emergency access is required, residential lanes will have a twenty-four (24)-foot paved section with three (3) feet minimum landscaping on each side. Measurement will be taken from inside of curb.

A key design goal of these guidelines is to design lanes which are “scaled” to the driver and do not appear as “tunnels” of garages. Planter pockets may occur at common property lines and at corners of lanes, to include trees and shrubs, to break up the massing of buildings and create a more inviting street scene.

7.6.5 Entries and Monumentation

Monumentation occurs throughout the Specific Plan area and is designed to establish a hierarchy for entering each area of the community. Along the perimeter edges there are several entry points into the community. At key entries a landscape and monumentation program will be utilized to help identify the community as well as convey a “welcoming” feeling for both vehicular and pedestrian traffic. Three monument treatments are used to set this hierarchy: the Primary Community Entry and Monumentation, the Secondary Community Entry and Monumentation, and the Neighborhood Entry and Monumentation.

7.6.6 Primary and Secondary Community Entry, and Neighborhood Entry Monumentation

A. Community Entry and Monumentation

The Primary Community Entry and Monumentation shall include the following:

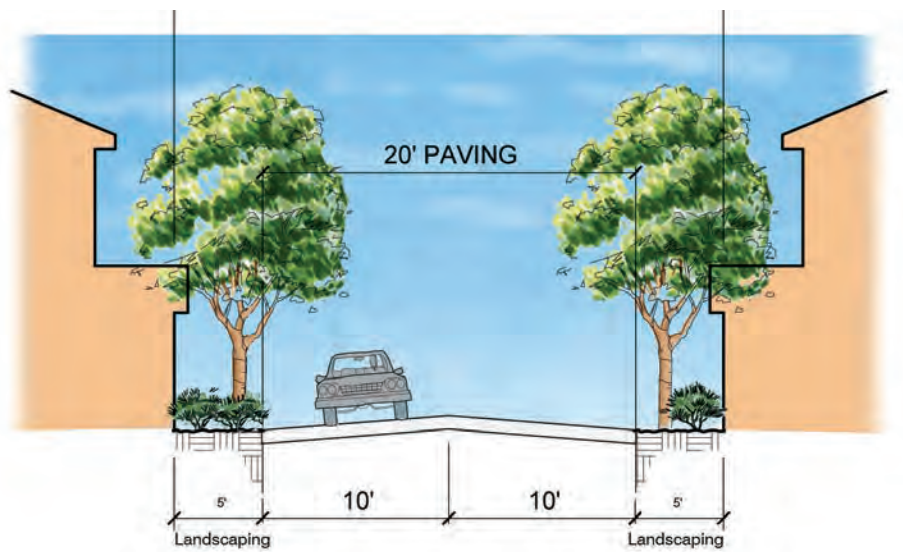
- ♦ Decorative, freestanding curved monument walls at each corner with anchoring large entry pilaster;
- ♦ Identification field for potential sign lettering placement;
- ♦ Decorative, architectural concrete caps, trim, and bases to help delineate each veneered material used;
- ♦ Short curved wall in front of main freestanding wall to create layering effect of monumentation;
- ♦ Use of veneer materials;
- ♦ Use of large specimen trees (minimum of 48-inch box) to flank each side of entry drive at site entry;
- ♦ Use of seasonal perennial shrubs to allow for flowering interest throughout the year;

- ♦ Enhanced pedestrian paving at street crossing and at monument location;
- ♦ Accent trees (minimum of 48-inch box) and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant groupings while combining interesting foliage textures and color;
- ♦ Accent lighting of landscape/monumentation; and
- ♦ Refer to the *Conceptual Landscape Plan, Exhibit 20* and the *Primary Community Entry and Monument, Exhibit 33* for detailed conceptual illustration.

B. Secondary Community Entry and Monumentation

The Secondary Community Entry and Monumentation shall include the following:

- ♦ Decorative, freestanding curved walls at each corner with anchoring entry pilaster. The large entry plaster should embody the same character as that of the Primary Community Entry Monument;
- ♦ Identification field for potential sign lettering placement on enhanced perimeter corner cut wall;
- ♦ Decorative, architectural concrete caps, trim, and bases to help delineate each veneered material used;
- ♦ Enhancement of corner cut wall and use of accent pilasters to anchor each side;
- ♦ Use of veneer materials instead;
- ♦ Use of large specimen native trees (minimum 48-inch box) to flank each side of entry drive at site entry;
- ♦ Use of seasonal perennial shrubs to allow for flowering interest throughout the year;
- ♦ Enhanced pedestrian paving at street crossing and at monument location;
- ♦ Accent trees (minimum 48-inch box) and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant groupings while combining interesting foliage textures and color;
- ♦ Accent lighting of landscape/monumentation;
- ♦ All monumentation shall be designed in accordance with the City of Ontario Traffic and Transportation Design Guidelines and are subject to City Engineer approval within the public street right-of-way; and
- ♦ Refer to the *Conceptual Landscape Plan, Exhibit 20*, and the *Secondary Community Entry and Monument, Exhibit 34* for detailed conceptual illustration.



LANES (PRIVATE ALLEYS)

20' Paved Section / 30' Width, Garage to Garage
N.T.S.

Note: where emergency access lanes are to be used within the residential development, a 24-foot paved section with 3-foot minimum landscaped area will be utilized (30-feet total)

* Measurement taken from inside of curb

EXHIBIT 32—CONCEPTUAL LANE (PRIVATE ALLEY) SECTION



C. *Neighborhood Entry and Monumentation*

Neighborhood entries and monumentation should occur on interior corner entries within the Specific Plan area. These entries should be used to help continue the landscape character theme to the “core” of the community. Each neighborhood built within the project will have the opportunity to identify their individual project character while providing the basic design features of the other monuments.

THE NEIGHBORHOOD ENTRY AND MONUMENTATION SHALL INCLUDE THE FOLLOWING:

- ♦ Freestanding, large, decorative entry pilaster set within the landscaped parkway. This pilaster should embody the same character as that of the Primary Community Entry Monument but is slightly reduced in scale to create a pedestrian gateway” into each neighborhood;
- ♦ Identification field for potential sign lettering placement on enhanced perimeter corner cut wall;
- ♦ Architectural decorative concrete caps, trim, and bases to help delineate each veneered material used;
- ♦ Enhancement of corner cut wall and use of accent pilasters to anchor each side;
- ♦ Use of veneer materials;
- ♦ Use of seasonal perennial shrubs to allow for flowering interest throughout the year;
- ♦ Enhanced pedestrian paving at street crossing and at monument location;

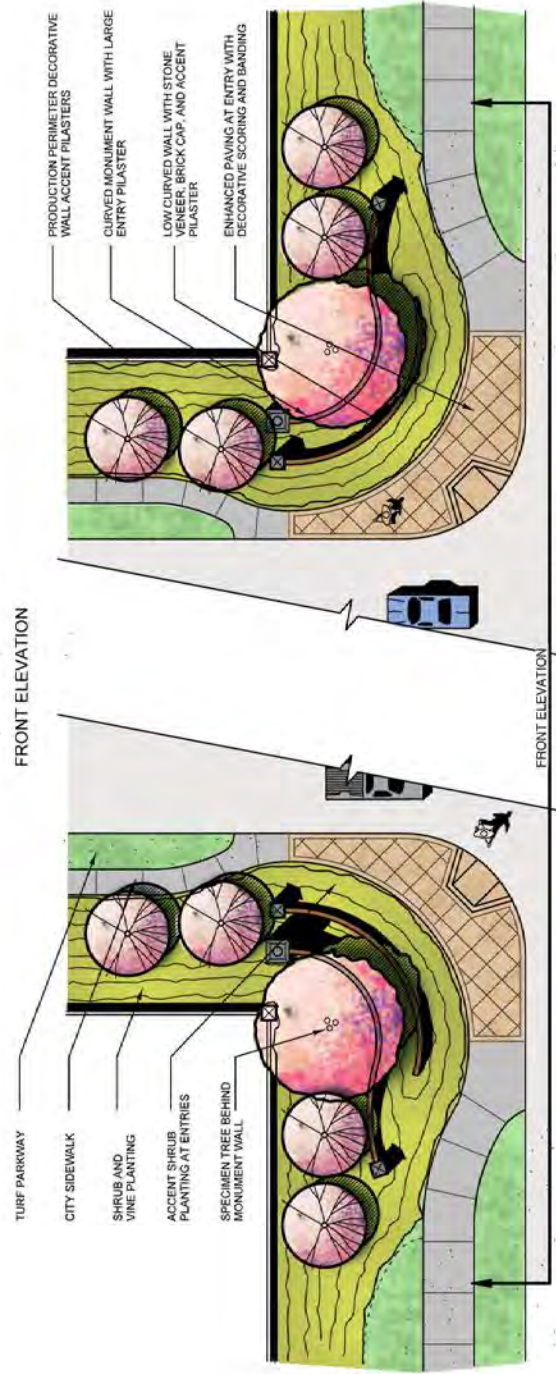
- ♦ Accent trees (minimum 48-inch box) and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant groupings while combining interesting foliage textures and color;
- ♦ Accent lighting of landscape/monumentation; and
- ♦ Refer to the *Conceptual Landscape Master Plan, Exhibit 20*, and *Neighborhood Entry and Monument, Exhibit 35* for detailed conceptual illustrations.

City of Ontario “Gateway” Monument

At the corner of Archibald Ave. and the southern corner of the Specific Plan planning area, a city “Gateway” monument will be located. Special consideration should be made to integrate the landscaping within the easement on Archibald Ave. into the monument. The City of Ontario is currently developing the “Gateway” monument program for the New Model Colony. Additional coordination with the City of Ontario will be needed prior to developing this area.



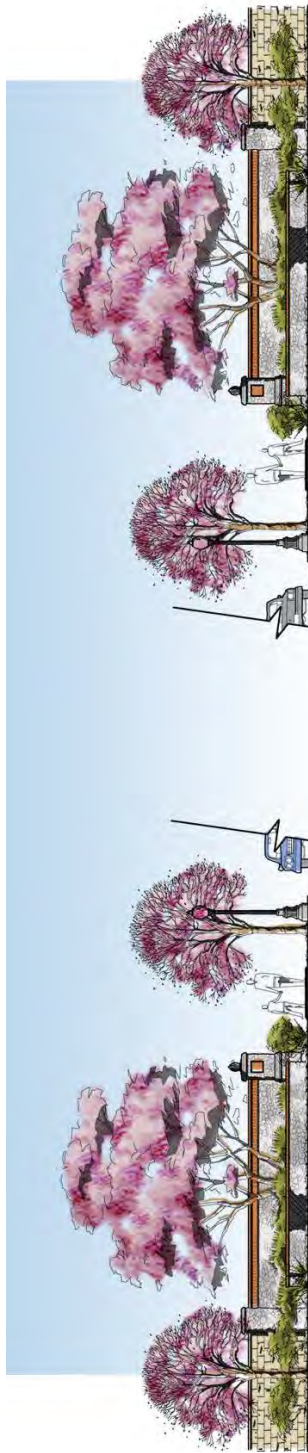
FRONT ELEVATION



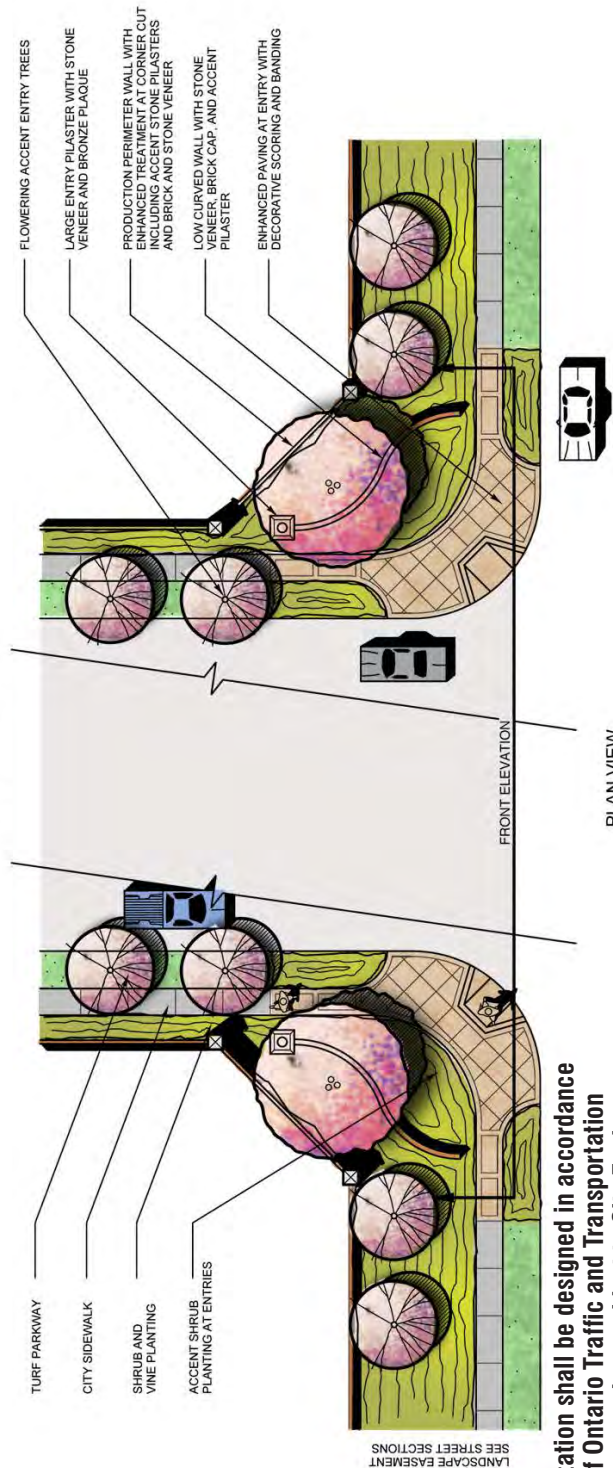
FRONT ELEVATION

PLAN VIEW

Note: Monumentation shall be designed in accordance with the City of Ontario Traffic and Transportation Design Guidelines and are subject to City Engineer approval within the public street right-of-way;



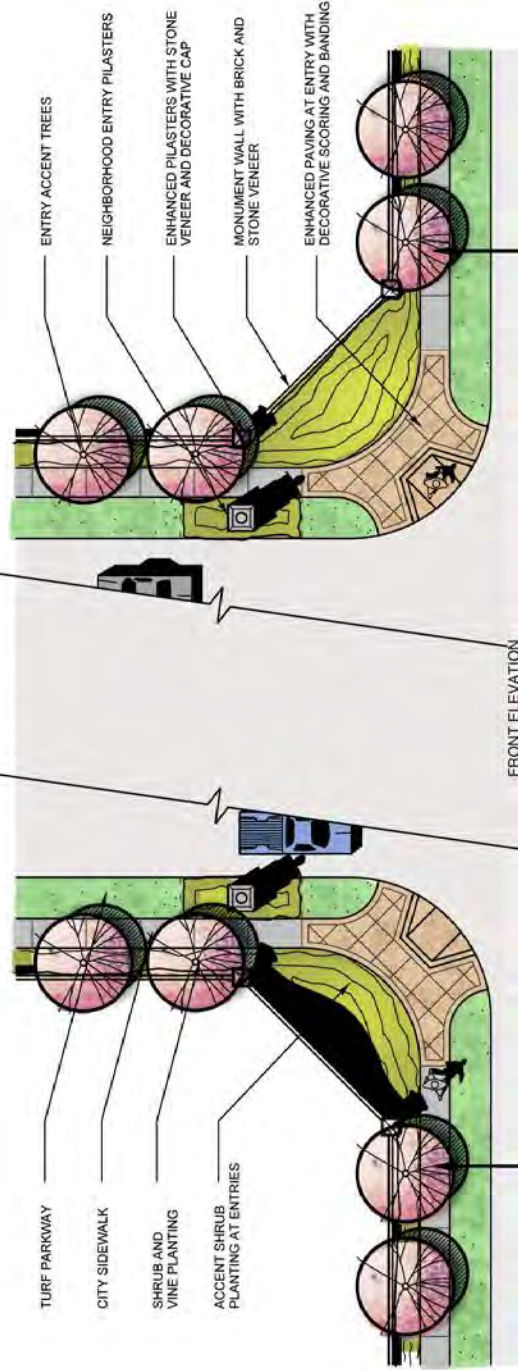
FRONT ELEVATION



Note: Monumentation shall be designed in accordance with the City of Ontario Traffic and Transportation Design Guidelines and are subject to City Engineer approval within the public street right-of-way;



FRONT ELEVATION



FRONT ELEVATION

PLAN VIEW

Note: Monumentation shall be designed in accordance with the City of Ontario Traffic and Transportation Design Guidelines and are subject to City Engineer approval within the public street right-of-way;



7.6.7 Neighborhood Parks and Open Space

The Specific Plan area will have a central “Community Green Core” that consists primarily of open space for recreation and passive use. Exterior walkways and paseos lead to this centralized area. Several opportunities exist for community interaction and recreation within these parklands. The “Community Green Core” is comprised of several facilities and amenities to meet the needs of the community.

The following areas make up the “Community Green Core:” the two (2) Park Place Neighborhood Parks, the Linear Park System, the Community Recreation Center, and the Paseo Trail System.

A. The Public Neighborhood Parks

Public Neighborhood Parks will be designed and constructed per City guidelines and approvals. The Neighborhood Parks consists of the following:

- ♦ Parking - Parking shall be adequate to accommodate daily use of the park, and should be screened from public view using a combination of berming and landscaping. In addition, the parking configuration should take into consideration possible shared use with the proposed neighboring elementary school to the North;
- ♦ On-street parallel parking is provided along the corridor from the Private Community Recreation Center to the SCE Easement, along Merrill Ave.;
- ♦ Children’s Tot Lot Play Area - A tot lot will be located within close proximity to the parking lot and private Park Place Community Recreation Center. Play structures and equipment should be staged to allow for separated use based on age. The tot lot should also follow ADA (Americans With Disabilities Act) guidelines and provide access based off of equipment selected. Seating areas shall be located near the tot lot to provide areas for parental supervision;
- ♦ Open Turf Area/Barbeque Picnic Facilities - The two (2) Park Place Neighborhood Parks should provide an open turf area and barbeque picnic facilities scattered throughout a looped concrete walkway system. Barbeque facilities (minimum of 50%) should be located adjacent to the walkway system for ADA accessibility; the remaining percentage set in open turf areas. Each barbeque picnic facility shall provide a picnic table, freestanding barbeque, hot coal container, and trash receptacle. These barbeque facilities can be placed on concrete or any other ADA acceptable surfacing. The design of the two (2) Park Place Neighborhood Parks open space should take into account pedestrian circulation and the linkage to the two adjacent roads as well as the surrounding community;
- ♦ Sports Fields for unorganized play - The open space turf area should be arranged to accommodate two baseball/softball fields and a soccer field overlay;
- ♦ Tennis Courts - An area for tennis courts should be implemented into the site with special consideration to solar orientation. The tennis court facility will be fenced with standard tennis court fencing and will allow public access. Hours of operation will be enforced in an effort to avoid evening disturbance to adjacent neighborhoods;
- ♦ Pedestrian Plaza - A centralized pedestrian plaza should be located near the children’s tot lot as a entry walkway system from the city sidewalk. This space should include bench seating and decorative pots, as well as shade for protection from the sun. Enhanced paving should be utilized within this area for decorative treatment, and should match the material used in the enhanced pedestrian crossings;
- ♦ Refer to the *Park Blow-ups, Exhibits 37-39*, for detailed conceptual illustrations;
- ♦ Landscaping - Landscaping within the park system shall harmonize with the surrounding streetscapes. Large specimen trees should be used within the open turf areas to help provide shade and screening of unwanted views. Accent trees should also be used at pedestrian entries and around the tot lot for color and seasonal interest. All trees shall be a minimum of 24-inch box. Specimen trees surrounding play areas shall be a minimum of 36-inch box;
- ♦ Landform Berming - Landform berming should occur throughout the park system to provide interest and opportunities for screening;
- ♦ Paseo Walkway System - A paseo walkway system should be designed to provide connections to adjacent neighborhoods as well as linking the private Park Place Community Recreation Center and Park together; and
- ♦ Lighting - No sports lighting should be implemented in the park. However, safety lighting will be provided in the parks and paseos within the Subarea 29 Specific Plan. A photometric plan will be required to be submitted with the landscape plans for these areas.



7.6.8 The Park Place Private Recreation Center

The Private Recreation Center consists of the following:

- ♦ Private Parking - Parking shall be adequate to accommodate daily use of the Private Recreation Center, and should be screened from public view using a combination of berming and landscaping;
- ♦ Recreation Building - The Park Place Recreation Building should adhere to the architectural design guidelines and should be designed to accommodate the community needs;
- ♦ Private Pool and Spa Facility - The Park Place residents will have access to a pool sized adequately to accommodate competitive swimming as well as serve as a summer leisure recreational amenity for adults and children of the community;
- ♦ The pool decking area should be sized to allow for lounge chairs and tables. A spa area separate of the pool will also be located within the Park Place Community Pool and Spa Facility. A group barbeque facility and wood shade structure should be located within the pool deck area. The pool and spa facility should be fenced and gated for resident entry only. Hours of operation will be enforced in an effort to avoid evening disturbance to adjacent neighborhoods;
- ♦ Landform Berming - Landform berming should occur throughout the linear park system to provide interest and opportunities for screening; and
- ♦ Paseo Walkway System - A paseo walkway system should be designed to provide connections to adjacent neighborhoods as well as linking the Private Park Place Community Recreation Center and Park together with the linear park system.
- ♦ Tennis Courts will be available through the Private Recreation Center, and therefore will be private.

7.6.9 Paseo Trail System

Pedestrian circulation is highly encouraged within the Specific Plan Community. Landscape easements are provided along major roadways and are encouraged within the neighborhood communities.

- ♦ Paseo Trail System - A paseo walkway system should be designed to provide connections to adjacent neighborhoods as well as linking the Private Park Place Community Recreation Center and Park together with the linear park system;
- ♦ Landscaping - Landscaping along the paseo trail system shall harmonize with the surrounding streetscapes. All trees shall be a minimum of 24-inch box. Specimen trees shall be a minimum of 36-inch box;

- ♦ Meandering sidewalks are encouraged;
- ♦ Enhanced paving should be provided at paseo connections where pedestrian circulation crosses roadways;
- ♦ Paseos should provide strong connections to the “Community Village Core;”
- ♦ Utilization of SCE Easement Corridor for multipurpose trails should be provided. Refer to the detailed conceptual illustrations. Final approval of paseo configuration and design to by SCE. Refer to *Exhibit 29, SCE Easement Paseo System*; and
- ♦ Utilization of the County Line Channel Flood Control Easement. Refer to the detailed conceptual illustrations, *Exhibit 30, County Line Channel Paseo System*.
- ♦ Connection of neighborhood pocket parks to paseo trail system. Refer to the detailed conceptual illustrations. At strategic places within neighborhoods, pocket parks will be implemented to serve each neighborhood. These pocket parks are intended to provide minimal amenities, and should be linked throughout each neighborhood link via the paseo trail system. Pocket parks should be sized between 1/4–2 acre in size. The paseo pocket parks will contain the following amenities:
- ♦ Paseo walkway (meandering encouraged);
- ♦ Landscaping - Landscaping shall harmonize with the surrounding streetscapes; and
- ♦ Lighting - Pole mounted fixtures spaced at appropriate intervals for safety and security.

The Paseo Pocket Parks might contain the following additional amenities:

- ♦ Open turf area/barbeque picnic facilities;
- ♦ Basketball courts; and
- ♦ Tot lots

7.6.10 Community Walls and Fencing

Walls are a major component in achieving an overall community theme within the Specific Plan. A strong cohesive appearance is achieved through the use of “community walls” and general overall wall guidelines.

All walls that adjoin community street scenes and are along the boundary edges of the paseo and to the opposite side of the corridor shall be deemed “community walls.” All wall and fencing designs and layout shall be approved by the City of Ontario prior to construction.

Refer to the *Conceptual Wall and Fence Plan, Exhibit 40*.

Brick walls or screening walls shall have climbing plants or anti-graffiti elements to prevent vandalism.

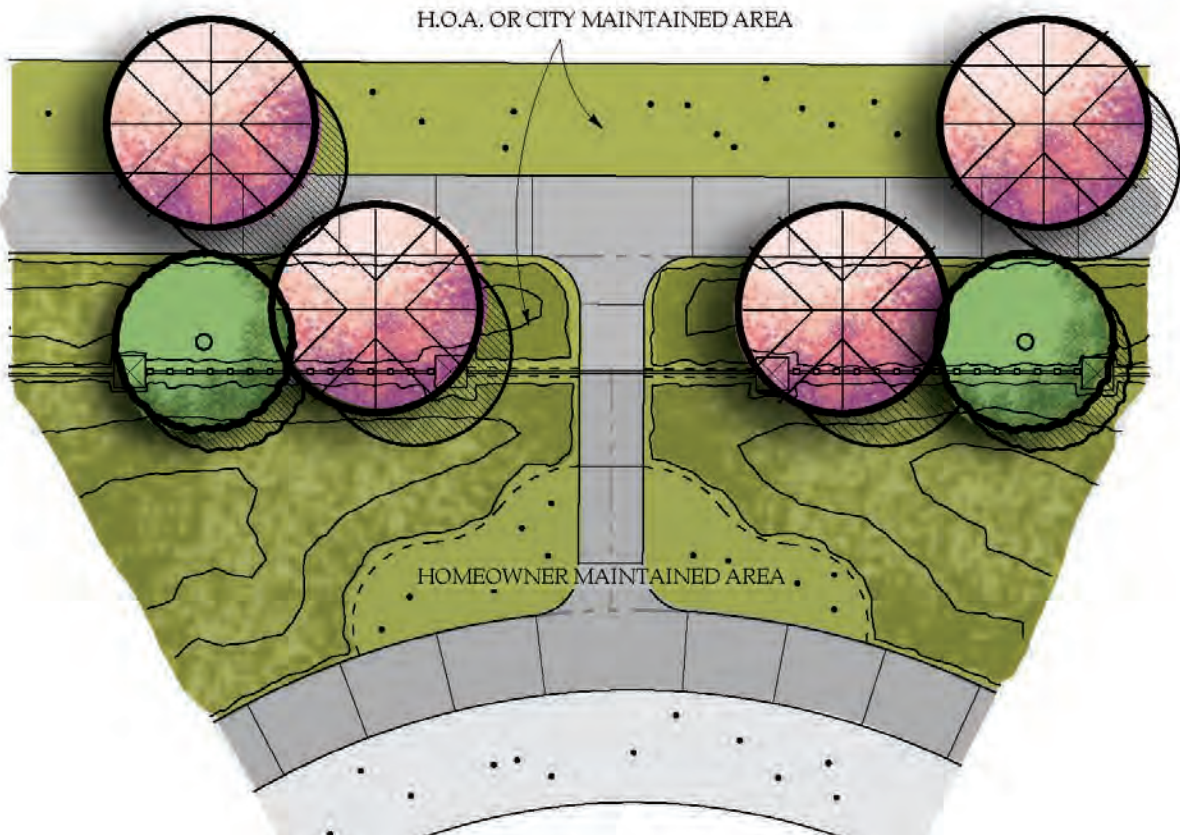
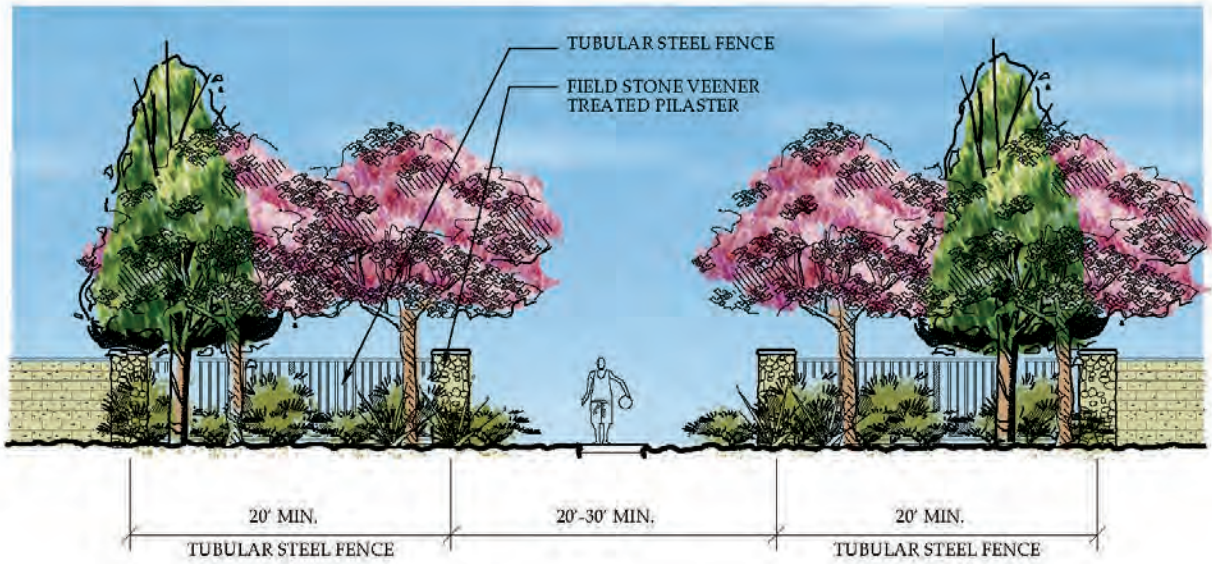
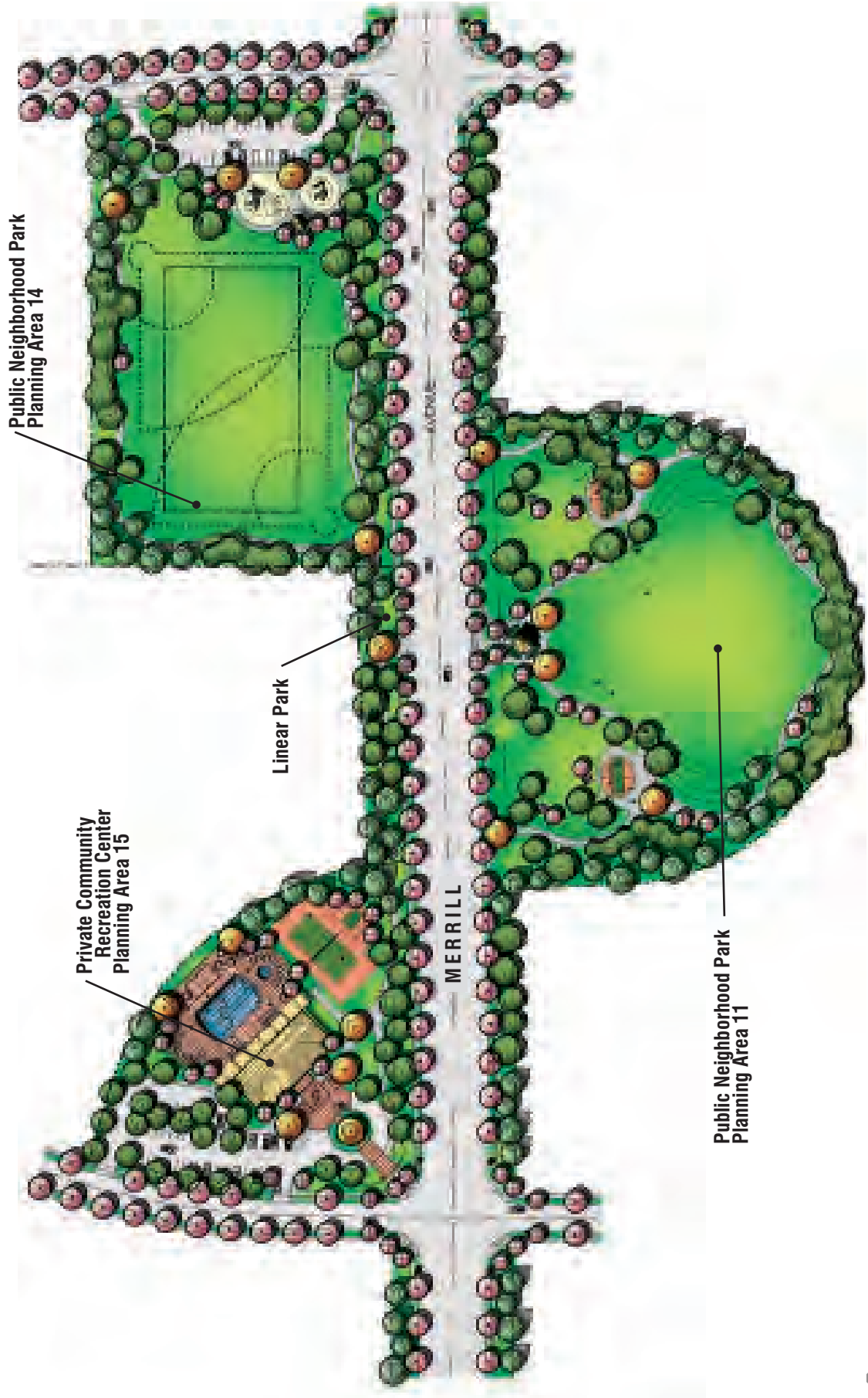


EXHIBIT 36—COMMUNITY PORTAL ENTRY

SPECIFIC PLAN — OCTOBER 2006
AMENDED APRIL 21, 2015



Public Neighborhood Park
Planning Area 14

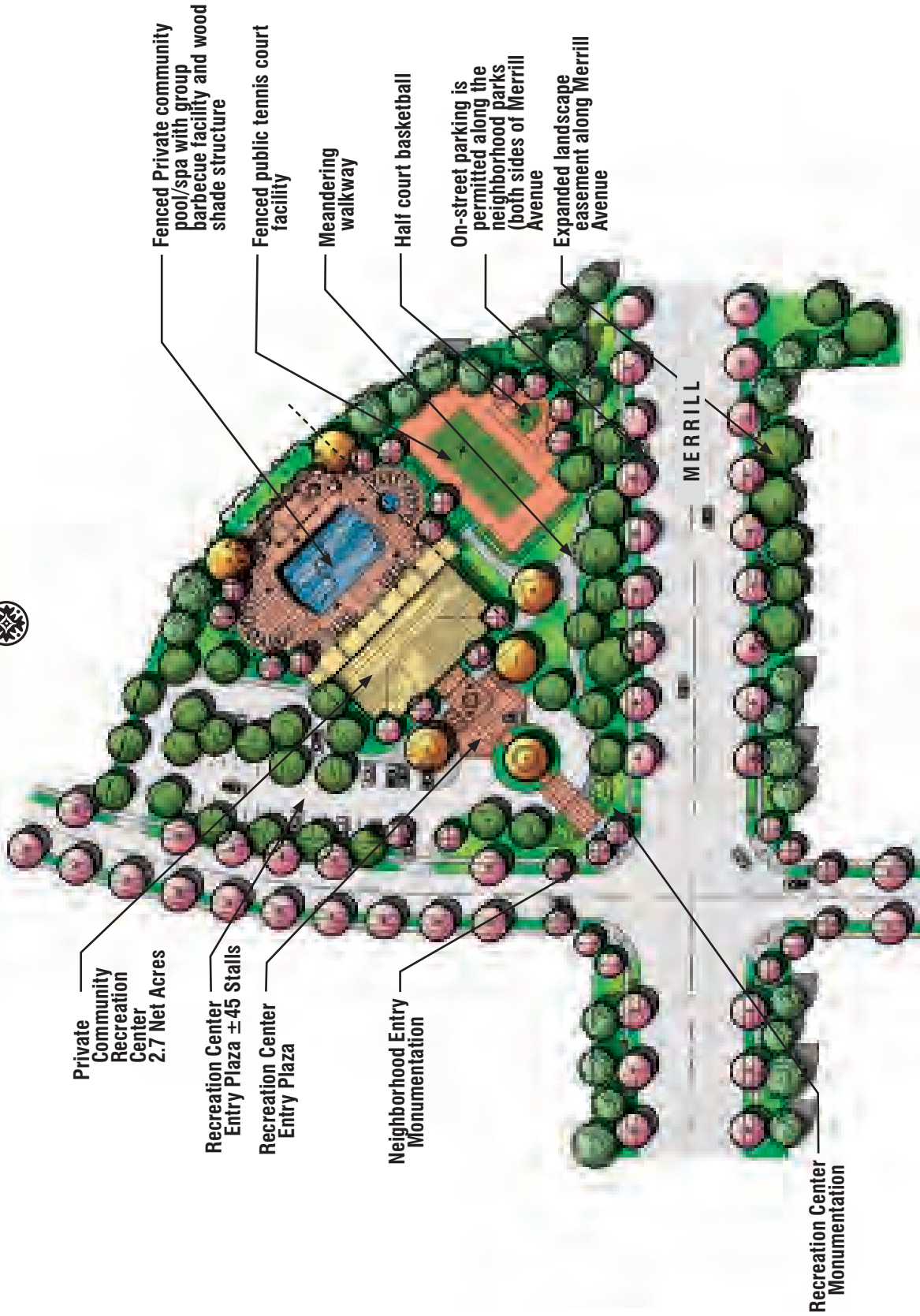
Linear Park

Private Community
Recreation Center
Planning Area 15

MERRILL

Public Neighborhood Park
Planning Area 11

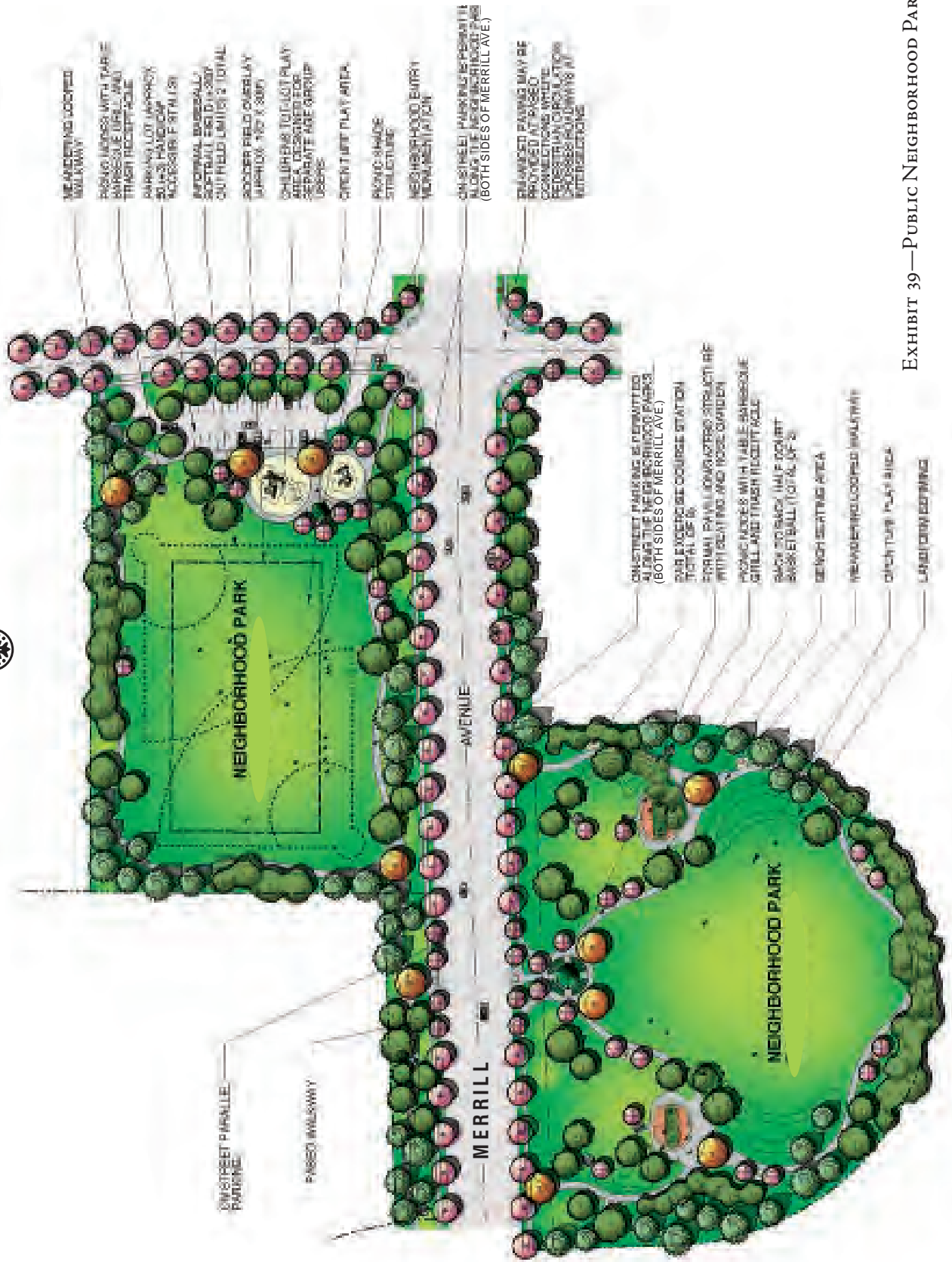




Note: Private Recreation Center for Park
Place residents use. Tennis Court
and Basketball area for public use.



EXHIBIT 38—COMMUNITY RECREATION CENTER
PUBLIC TENNIS COURTS & BASKETBALL COURT



ME ANDERING LOCATED
WALKWAY

PICNIC TABLES WITH TABLE
WHEELS (TOTAL 10) AND
TRASH RECEPTACLE

PARKING LOT (APPROXIMATELY
50 SPACES)

INDOOR BASKETBALL
COURT (TOTAL 10) WITH
OUTFIELD LIMITED TOTAL

SOCCER FIELD OVERLAY
(APPROX. 100' X 300')

CHILDREN TOGGLED PLAY
AREA, DESIGNATED FOR
SEPARATE AGE GROUP
USAGES

CRENSHAW PLAY AREA

PICNIC SHADE
STRUCTURE

NEIGHBORHOOD ENTRY
MONUMENTAL SIGN

ON-STREET PARKING IS PERMITTED
ALONG THE NEIGHBORHOOD PARKS
(BOTH SIDES OF MERRILL AVE.)

UNIMPROVED PARKING MAY BE
PROVIDED AT RAISED
CONCRETE, INTERLOCKING
PAVING OR CONCRETE
PAVING WITH A
CONCRETE CURB

CHESTNUT PARKLE
PARKING

PINEDO ALLEYWAY

MERRILL AVENUE

ON-STREET PARKING IS PERMITTED
ALONG THE NEIGHBORHOOD PARKS
(BOTH SIDES OF MERRILL AVE.)

ON-STREET PARKING IS PERMITTED
ALONG THE NEIGHBORHOOD PARKS
(BOTH SIDES OF MERRILL AVE.)

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(BOTH SIDES OF MERRILL AVE.)





Community Walls

Community walls shall be decorative in nature. Decorative accent pilasters spaced at equal intervals are highly encouraged.

Solid Walls and Fencing

Where privacy or protection of common area views dictates, walls shall be of decorative block. Reverse frontage walls and any wall return that is visible from the street shall be constructed of split-face block or precision block that is plastered or stuccoed, and of a color that blends with the Specific Plan color palette. Decorative caps and the use of pilasters to help enhance the perimeter appeal of the walls are encouraged. Walls visible from the community streets may not be made of exposed or painted precision block or slumped block. Variation shall be provided in the perimeter wall to include pilasters, material changes, and/or other decorative features which offers relief along the proposed perimeter walls or as approved by the City of Ontario Planning Department. All interior walls (both sides) shall be constructed of decorative block whether they are in public view or not. Colors shall be consistent with the colors specific for the overall design theme and shall be decoratively shaped. Combo walls shall be utilized where residential properties abut trails, paseos and/or parks or as approved by the City of Ontario. The overall height established by the City of Ontario for walls and fences shall not be exceeded; however, the height of the decorative block and/or wrought iron utilized to construct the combo walls may vary depending on the proposed location of the combo walls. Wood and vinyl fencing are prohibited within the Specific Plan. The final wall design and location shall be approved by the Planning Department during the tentative map or development review process. Refer to *Exhibit 40A, Wall and Fence Details*.

Tubular Steel Fencing

If applicable, tubular steel fences should be located in the rear yards of those properties abutting large slope areas. In addition, view fencing shall be used at the Private Recreation Center pool area, and any other open space areas where visibility is needed and physical access is controlled. Areas where tubular steel fencing occurs will be subject to review by the City of Ontario.

7.6.11 Outdoor Lighting

Lighting standards within the Specific Plan community shall be consistent in style, color, and materials in order to maintain uniformity throughout. Lighting should be subtle, providing a soft wash of light over illuminated objects such as monumentation. Hierarchy shall be established by using a variety of lighting fixtures and illumination levels based off of lighting design intent. Lighting styles shall tie into architectural styles and provide sufficient illumination for the safety and well being of the community.

Accent lighting of landscape and monumentation shall be incorporated into the following areas: Metal halide lighting to project true colors and clear visibility, and reflective street signs shall be used on all proposed streets. Lighting shall have anti-vandalism fixtures.

Each residential unit shall have illuminated addresses at the front door and the garage door, if the garage faces a lane.

Entry Monuments

Avoid intensely bright or "hot" lighting of monuments; rather, each should be lit to provide a soft wash of light across the monument signage. Specimen trees should be uplighted with several fixtures into the canopy to avoid creating dark sides of the trees.

Recreation Center

Uplighting of specimen trees is very important and providing adequate lighting within the vehicular turn-around areas. Recreation center monumentation/kiosk should also be illuminated.

Community Park/Linear Park/Paseo Lighting

Lighting of the walkways, tot lot, and parking area within the Parks and Paseo Trail System should be considered for safety and security. Utilization of a traditional Globe/Acorn Post mounted light fixture should be considered for both the park open space areas as well as interior street lighting fixtures. Keeping with the character of traditional materials, this will help to create a better sense of scale to the pedestrian.

7.6.12 Mailboxes

The developers shall work with the United States Post Office to determine the best possible location for mailboxes within the residential subdivisions. The developer will determine the specifications for the mailbox units.

7.7 LANDSCAPE IMPLEMENTATION

7.7.1 Improvement Responsibility Landscape Maintenance

Landscape Maintenance

- ♦ All public landscapes, including slopes, street trees, lighting, and irrigation systems, shall be maintained by the City of Ontario Landscape Maintenance District;
- ♦ Front yard landscapes shall be installed by the developer and maintained by the homeowner in a healthy condition at all times; and

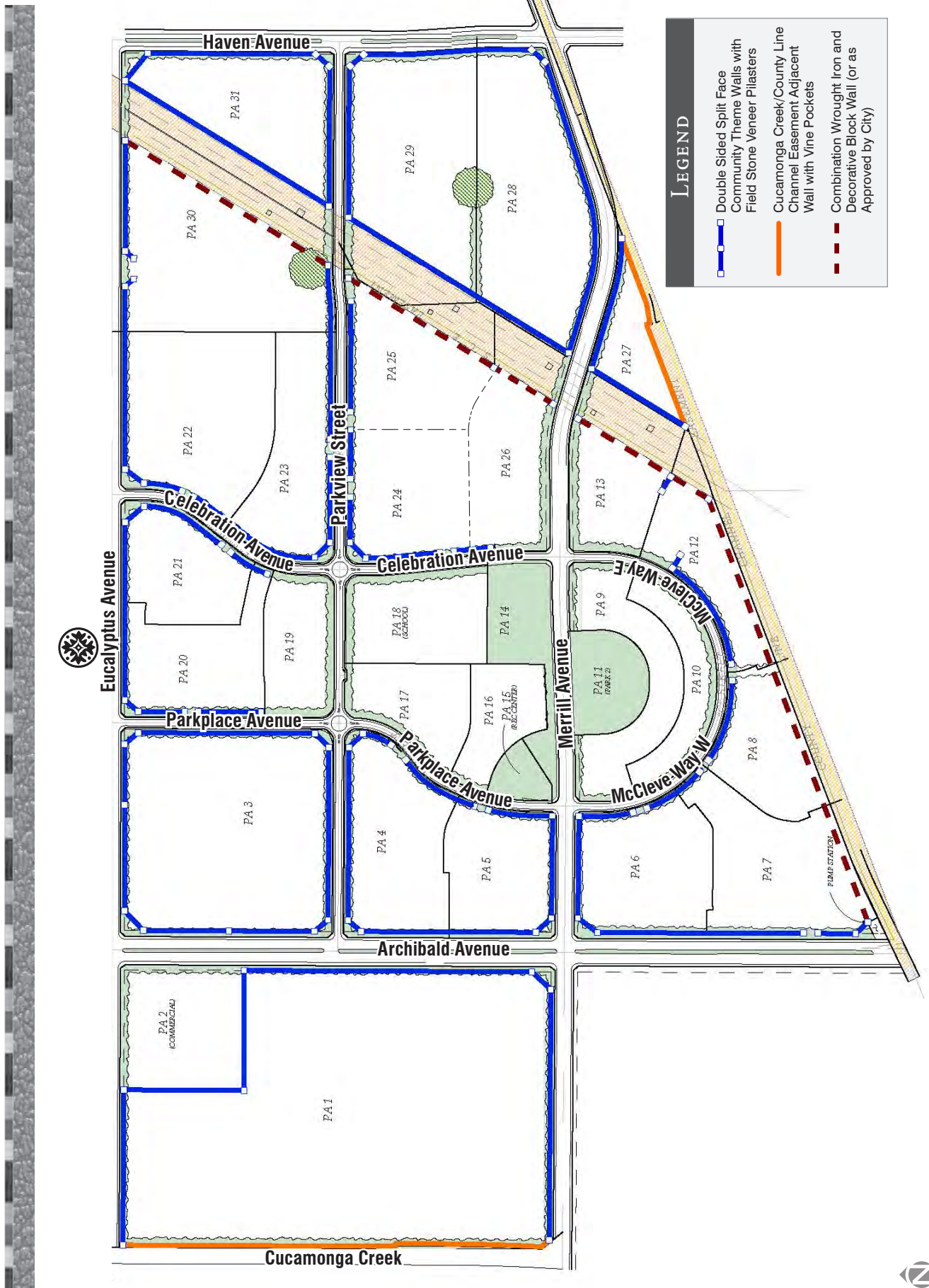
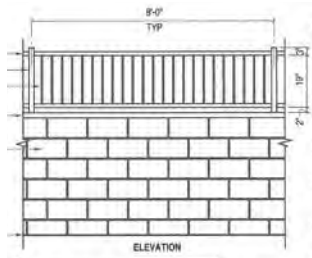
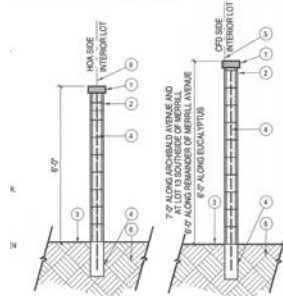


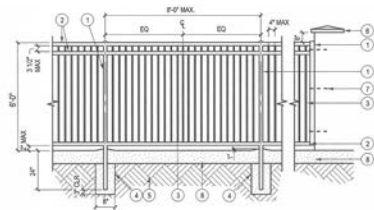
EXHIBIT 40—CONCEPTUAL WALL AND FENCE PLAN



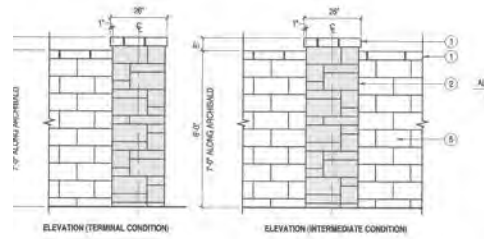
Combo Wall/Fence



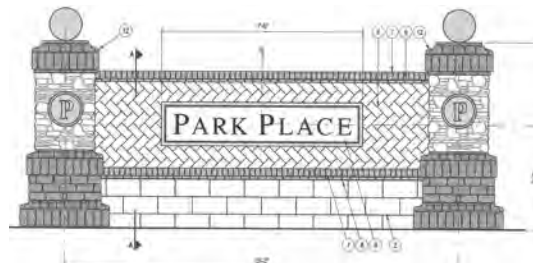
Perimeter Split Face Block Wall



Tubular Steel Fence



Pattern Pilaster



Neighborhood Monument

Note: Use of double sided material when visible from the street

EXHIBIT 40A—WALL AND FENCE DETAILS

**SPECIFIC PLAN — OCTOBER 2006
AMENDED APRIL 21, 2015**



- ♦ All manufactured and cut/fill slopes which exceed three (3) feet in height shall be planted with an effective mixture of groundcover, shrubs, and trees by the Developer. Such slopes shall also be irrigated as necessary to ensure germination and establishment in conformance with the fuel modification guidelines described later in this document.

Tree Size	Percentage on-site
15 Gallon	60%
24" Box	25%
36" Box	10%
48" Box	5%

7.7.2 Homebuilder Landscape Requirements

Public Landscapes (HOA and City maintained areas)

- ♦ Landscape plantings in public areas should reflect a commitment to both developing a “sense of place” and maintaining harmony with the New Model Colony;
- ♦ A landscape architect licensed in California shall be retained to prepare planting and irrigation plans for all public areas. Arrangement of plants should incorporate the concepts of mass planting; plants should be placed to allow them to grow to their natural sizes and forms, and sheared hedges should be kept to a minimum; and
- ♦ All pathway areas shall be designed to be barrier free and meet ADA guidelines.

The plant matrix at the end of this section offers a suggested plant palette for the Specific Plan; while it is by no means all-inclusive, plantings in public areas should draw primarily from this palette for visual community continuity.

Front Yard Landscapes

Plantings in front yards may vary substantially from the Specific Plan Plant Matrix, but should retain some of the character and style of the public plantings. No more than 50 percent of the total square footage of any front yard shall be lawn; the balance shall be composed of shrubs and groundcovers, with an emphasis on drought tolerant plant species. Each front yard shall be planted with one 24-gallon tree (minimum). Installation of landscaping within the front yards of single family detached housing products will be provided by the home builder. Front yard landscaping shall be installed prior to Certificate of Occupancy.

7.7.3 Soil Testing

Soil samples shall be taken from several locations after the completion of rough grading operations, and a reputable soil-testing laboratory shall perform an agronomic soils test. The test shall assess soil fertility needs for water-wise California native and Mediterranean plant types. No planting shall take place until the soil has been properly prepared based on the recommendations of the soils testing laboratory.

7.7.4 Slope Landscaping - HOA

All manufactured and cut/fill slopes which exceed three (3) feet in height shall be planted with an effective mixture of groundcover, shrubs, and trees. These slopes shall be installed before the Certificates of Occupancy. Such slopes shall also be irrigated as necessary to ensure germination and establishment in conformance with the fuel modification guidelines.

7.7.5 Private Interior Slopes: Residential Interior

- ♦ Private interior slopes may be more ornamental in character than exterior slopes. They may have a somewhat broader range of plant materials than exterior slopes, but should still be chosen primarily from the plant palette and are subject to the same fuel modification restrictions.
- ♦ All manufactured and cut/fill slopes which exceed three (3) feet in height shall be planted by the Developer with an effective mixture of groundcover, shrubs, and trees. Such slopes shall also be irrigated as necessary to ensure germination and establishment in conformance with the fuel modification guidelines described later in this document. All slopes shall be landscaped in accordance with the City of Ontario’s standards.

7.7.6 Streetscape Landscaping

STREETSCAPE DEVELOPMENT STANDARDS

- ♦ Turf grass shall not exceed 30 percent of streetscape planting and shall be located adjacent to the sidewalk or curb line;
- ♦ All new plantings within the planned community shall draw substantially from the Park Place Plant Matrix included in this document;
- ♦ All streetscape landscaping within the Specific Plan planned community will be implemented by the Developer in accordance with this Specific Plan;



- ♦ A balance of evergreen and deciduous trees shall be considered during the planning process for the selection of street trees and understory trees between them (if any). Street trees shall be sized per City of Ontario’s standards;
- ♦ The Developer shall install all primary and secondary improvements concurrently with the construction of the roadway on which they front. Neighborhood intersections shall be constructed as each neighborhood street is built;
- ♦ The Developer shall provide site inspection of all construction and installation of entries and intersections in accordance with the City of Ontario requirements;
- ♦ Self-attached vines are to be added against block walls;
- ♦ Along community walls, the use of 5-gallon plant material as hedgegrow is preferred; and
- ♦ Closely spaced plant materials (to horticultural practices) is expected on this project.

7.7.7 Irrigation Design

Irrigation for both public and private landscapes should be designed to be as water-efficient as possible. All irrigation systems shall have automatic controllers designed to properly water plant materials given the site’s soil conditions, and irrigation systems for all public landscapes shall have automatic rain shut-off devices. Low volume irrigation is encouraged. Spray systems shall have low-gallons-per-minute, matched-precipitation heads. All City maintained landscapes (LMD areas) are to be controlled with a central controlled system as approved by the City of Ontario,

Parks and Maintenance Department, and trees plantings are to consist of root barriers and a pop up stream bubbler system on a separate valve. Irrigation design shall meet the requirements of the Landscape Development Standards.

ADDRESSES

Each single family home will be required to have the address numbers painted on the curb with black numerals on a white reflective paint background. Each homeowner will be responsible for the maintenance of these numbers.

7.8 Design Guideline Implementation

7.8.1 Design Review Submittal Requirements

The respective developers. SL Ontario Development Corporation, Richland Communities, Inc, and Brookfield Homes shall review all plans prior to submittal to the City for Plan Check for their projects. This design review process will include these steps: Conceptual review, Refined review and Construction document review prior to plans being submitted for city processing. The Design Review process will ensure the vision of Subarea 29 will be implemented.

7.9 PLANT MATRIX

Per City of Ontario Street Tree Master Plan.

Vines

Botanical Name	Common Name	Eucalyptus Avenue	Merrill Avenue	Archibald Avenue	Turner Avenue	Haven Avenue	Primary Community Entries	Secondary Community Entries	Neighborhood Entries	Community Park Open Space	Linear Park	Community Recreation Center	Paseo Trail System	Commercial Area	Alleys
<i>Bigonia c. 'Tangerine Beauty'</i>	Cross Vine														
<i>Bougainvillea hybrids</i>	Bougainvillea	■	■	■	■	■	■	■	■	■	■	■	■	■	■
<i>Clytostoma callestegioides</i>	Purple Trumpet Vine														
<i>Distictis buccinatoria</i>	Blood-red Trumpet Vine	■	■	■	■	■	■	■	■	■	■	■	■	■	■
<i>Jasminum polyanthum</i>	Pink Jasmine														
<i>Macfadyena unguis-cati</i>	Cat's Claw Vine	■	■	■	■	■	■	■	■	■	■	■	■	■	■
<i>Parthenocissus tripcuspidata</i>	Boston Ivy	•	•	•	•	•	•	•	•	•	•	•	•	•	•



Shrubs

Botanical Name	Common Name	Eucalyptus Avenue	Merrill Avenue	Archibald Avenue	Turner Avenue	Haven Avenue	Primary Community Entries	Secondary Community Entries	Neighborhood Entries	Community Park Open Space	Linear Park	Community Recreation Center	Paseo Trail System	Commercial Area	Alleys	SCE Corridor Easement	Bellgrave Channel Paseo
<i>Agapanthus species</i>	Lily of the Nile																
<i>Anigozanthos species</i>	Kangaroo Paws																
<i>Arbutus unedo 'Compacta'</i>	Compact Strawberry Tree																
<i>Arctostaphylos d. 'Howard McMinn'</i>	McMinn Manzanita																
<i>Asparagus densiflorus 'Myers'</i>	Myers Asparagus																
<i>Bergenia cordifolia</i>	Bergenia																
<i>Buxus japonica</i>	Japanese Boxwood																
<i>Calliandra haematocephala</i>	Pink Powder Puff																
<i>Callistemon 'Little John'</i>	Dwarf Callistemon																
<i>Camellia species</i>	Camellia																
<i>Campanula poschcharskyana</i>	Serbian Bellflower																
<i>Carex tumulicola</i>	Berkeley Sedge																
<i>Cercis occidentalis</i>	Western Redbud																
<i>Correa pulchella 'Mission Bells'</i>	Australian Fuchsia																
<i>Cotoneaster lacteus</i>	Cotoneaster																
<i>Deschampsia cespitosa</i>	Hair Grass																
<i>Dietes iridioides</i>	Fortnight Lily																
<i>Dodonaea microzyga</i>	Brilliant Hop-Bush																
<i>Echium fastuosum</i>	Pride of Madiera																
<i>Festuca mairei</i>	Atlas Fescue																
<i>Geranium incanum</i>	Carpet Geranium																
<i>Geranium 'Johnson's Blue'</i>	Johnson's Blue Geranium																
<i>Grevillea lanigera</i>	Wooly Grevillea																
<i>Grevillea species</i>	Grevillea																
<i>Helictotrichon sempervirens</i>	Blue Oat Grass																
<i>Hemerocallis hybrids (evergreen)</i>	Daylily - mixed colors																
<i>Heuchera sanguinea</i>	Coral Bells																
<i>Kniphofia uvaria</i>	Red Hot Poker																
<i>Leptospermum scoparium</i>	New Zealand Tea Tree																
<i>Ligustrum japonicum 'Texanum'</i>	Japanese Privet																
<i>Mahonia aquifolium</i>	Oregon Grape																
<i>Muhlenbergia rigens</i>	Deer Grass																
<i>Myoporum parvifolium</i>	Trailing Myoporum																
<i>Myrtus communis 'Compacta'</i>	Dwarf Myrtle																
<i>Nandina domestica</i>	Heavenly Bamboo																
<i>Nephrolepis cordifolia</i>	Southern Sword Fern																
<i>Nessella tenuissima</i>	Mexican Feather Grass																
<i>Penstemon species</i>	Beard Tongue																
<i>Phormium hybrids</i>	Hybrid New Zealand Flax																
<i>Pittosporum species</i>	Pittosporum																
<i>Rhapiolepis indica species</i>	Indian Hawthorn																
<i>Rosa species (shrub and groundcover)</i>	Rose																
<i>Rosmarinus officianalis and hybrids</i>	Rosemary																
<i>Salvia greggii and cultivars</i>	Autumn Sage																
<i>Sedum 'Autumn Joy'</i>	Autumn Joy Stonecrop																
<i>Teucrium x lucidrys</i>	Bush Germander																
<i>Viburnum tinus 'Spring Boquet'</i>	Spring Boquet Laurustinus																



Trees

Botanical Name	Common Name	Eucalyptus Avenue (Per NMC Streetscape Master Plan)	Merrill Avenue (Per NMC Streetscape Master Plan)	Archibald Avenue (Per NMC Streetscape Master Plan)	Turner Avenue (Per NMC Streetscape Master Plan)	Haven Avenue (Per NMC Streetscape Master Plan)	Primary Community Entries	Secondary Community Entries	Neighborhood Entries	Community Park Open Space	Linear Park	Community Recreation Center	Paseo Trail System	Commercial Area	Alleys	SCE Corridor Easement	Belgrave Channel Paseo
<i>Arbutus unedo</i>	Strawberry Tree																
<i>Brachychiton populneus</i>	Bottle Tree								•	•	•						
<i>Cedrus deodara</i>	Deodar Cedar								•								
<i>Chionanthus retusus</i>	Chinese Fringe Tree								•	•	•						
<i>Cupressus sempervirens</i>	Italian Cypress											•					
<i>Dodonaea viscosa</i>	Hopseed Bush															•	•
<i>Eriobotrya deflexa</i>	Bronze Loquat											•				•	•
<i>Juniperus chinensis 'Torulosa'</i>	Hollywood Juniper											•				•	
<i>Lagerstroemia indica hybrids</i>	Crape Myrtle								•	•	•	•					
<i>Magnolia grandiflora 'St. Mary'</i>	NCN										•	•					
<i>Magnolia soulangeana</i>	Saucer Magnolia								•	•	•	•				•	
<i>Melaleuca linariifolia</i>	Falxleaf Paperbark								•			•		•			
<i>Melaleuca nesophila</i>	Pink Melaleuca												•			•	•
<i>Melaleuca quinquinervia</i>	Cajeput Tree								•								
<i>Pinus canarensis</i>	Canary Island Pine								•	•	•	•		•			
<i>Platanus acerifolia</i>	London Plane Tree								•	•	•						
<i>Platanus racemosa</i>	Western Sycamore								•		•						
<i>Podocarpus gracilior</i>	Yew Pine											•					
<i>Populus nigra 'Italica'</i>	Lombardy Poplar								•	•	•	•					
<i>Pyrus calleryana 'Aristocrat'</i>	Ornamental Pear						•	•	•								
<i>Quercus agrifolia</i>	Coast Live Oak						•	•	•								
<i>Quercus ilex</i>	Holly Oak																
<i>Rhus lancea</i>	African Sumac											•					
<i>Schinus molle</i>	California Pepper						•	•	•					•			
<i>Tristania laurina</i>	Water Gum												•	•			



8. Commercial Design Guidelines





8.0 Commercial Design Guidelines

8.1 INTRODUCTION

The Subarea 29 Specific Plan area will include a commercial area in the northwest corner of the project, at the intersection of Archibald Ave. and Merrill Ave. This non-residential use will allow the residents of the Specific Plan area with shopping and employment conveniently located nearby. Residents will have the ability to access services by foot or ride their bicycles to work via pedestrian trails connecting the residential uses to the commercial center and the local community.

Throughout the commercial development, landscaping, site planning, and architectural design should be considered to create friendly and welcoming places to shop and work. These non-residential uses will continue to vision established in the residential community, including integrating pedestrian-friendly design.

The Commercial Design Guidelines will help guide the development of the area to enhance the pedestrian experience and innovative design that contributes to the character of the Specific Plan area. These guidelines are not intended to be strictly enforced rules, but rather as a tool in aiding the design process. Additionally, these guidelines have been generalized for the Specific Plan. Potential developers will be required to submit their own supplemental design guidelines for the Commercial development area to the City of Ontario for approval.

The Commercial Section is organized into the following components:

- † Site Planning;
- † Building Design;
- † Architectural Details; and
- † Open Space and Landscaping.

8.1.1 Guiding Principles

Guiding Principles for commercial design for the Specific Plan include:

Buildings should contribute to the qualitative nature of the overall neighborhood.

- † Proper scale and proportions in massing and details should be used;
- † Pedestrian open spaces should be implemented;
- † Massing and articulation should be varied;
- † Buildings should be designed at a human-scale; and
- † Parking lots should be de-emphasized as much as possible.

Buildings should have an enduring quality

- † Design should draw inspiration from the rich and diverse Southern California architectural heritage;
- † Materials and colors typical to southern California should be used;
- † Details and ornamentation should be authentic, as much as possible; and
- † Design should create an inviting place to shop and work. Pedestrian trails from the residential neighborhoods should not lead to the rear of the Commercial Center.

8.1.2 Review Process

The Commercial Design Guidelines are to be used as a general tool to guide the development of the northwest portion of the Specific Plan area. Final designs will be subject to the City of Ontario's approval. Additionally, more detailed Design Guidelines for each developer may be required by the City prior to final approval. Refer to the City of Ontario Code for further information regarding the review process.

8.2 COMMERCIAL DESIGN GUIDELINES

8.2.1 Introduction

Commercial development within the Specific Plan will continue the pedestrian-friendly character established in the residential community. By reducing the impact of the automobile on the appearance of the commercial development, appropriate site planning can de-emphasize the vehicle, creating a safe outdoor environment without ignoring the necessary needs of vehicular traffic. Through well executed planning, architecture, and landscape design, the commercial uses within the Specific Plan will complement and, to some extent, integrate the residential community rather than turn its back completely to the community residents.

8.2.2 Site Planning

Orientation

Buildings should be oriented for best visibility from the street. Windows and entries should face the street, avoiding blank walls which could dominate public views. Stores with entries not visible from the street may be oriented towards pedestrian open space.

All primary entries should face onto a street or a connecting walkway.



Driveways and Sidewalks

The site should be provided with a limited number of driveways to reduce the traffic impact along Merrill Ave. and Archibald Ave. Pedestrian crossing distances should be minimized at driveways. To increase safety for pedestrians, a raised median as a pedestrian island should be provided between travel lanes, especially those entries with more than one lane in any direction. Driveways should be paved with asphalt, concrete or an enhanced paving material such as brick.

Sidewalks should be located along natural travel paths for pedestrians. Sidewalks should be a minimum of six (6) feet wide along pedestrian pathways and a minimum of eight (8) feet width along more heavily traveled commercial areas.

Street Frontage and Buildings

Streetside buildings, or those with no parking separating them from the street, should use windows and entry features to soften the building's appearance to the street. These buildings should not be more than 100-foot long without a pedestrian plaza or walkway breaking the building mass and connecting the parking lot with the street. On corners, buildings should utilize massing elements to anchor the corner.

Streetside Setbacks and Buildings

The sides of buildings along street edges should be landscaped within the setback to soften the building's appearance to the street as well as have additional windows or design elements. Refer to the City of Ontario Development Code for specific setback requirements.

Street Frontage and Parking Lots

Parking lots should generally be placed away from streets. No street frontages should consist of a continuous parking lot, but should be broken up with buildings, landscaping, plazas, and other pedestrian features. Any parking along street edges should be set back with a landscaped buffer to minimize the dominant feeling of the automobile along the street. Continuous parking stalls should be interrupted by landscaped islands, no more than ten (10) stalls apart.

Service and Storage Areas

Loading docks, garages and storage areas are to be located behind or to the side of buildings siding onto secondary access and not primary streets. These features must be screened with walls and landscaping as much as possible from public view of streets, residences, pedestrians walkways and connecting trails. Loading docks and storage areas should not conflict with pedestrian walkways.

Refuse Containers, Utility and Mechanical Equipment

Refuse containers and equipment should be easily accessed by service vehicles but screened from view of the streets, parking lots, and connecting

walkways through roof forms, walls and/or landscaping. Screening details should incorporate elements that are compatible with the architectural style of the building. Proper landscaping including trellises also may help to screen these elements. Equipment and enclosures shall not be located near pedestrian walkways.

8.2.3 Building Design

Massing

With larger buildings, appropriate massing becomes more important to maintain the human-scale of the development and create a visually pleasing environment. Bay windows, stepped buildings, height changes and setback variations between stores helps to break up large buildings as well as indicate entries and store locations to customers. Tower elements or monumental features are encouraged at focal points, such as corners, plazas, major entrances, or where walkways meet streets. All elevations of a tower element or monumental feature shall be completely constructed. In no instance shall a feature of this nature be left unfinished and exposed to public view.

Roof Forms

Hipped and gable roofs are encouraged. Roof pitches should be compatible with the architectural style of the building. Flat roofs should vary in height and be accompanied by cornice, trim or other accent features.

Entry Design

Building entries should be prominent and easily identified. Various elements can be used to enhance the entry features including massing variation, materials and color change, change in roof form, and awnings. For smaller retail, as part of a cluster or strip, arcades, awnings, and simple signage may be acceptable as entries.

Arcades, Trellises, and Awnings

Outdoor arcades along store fronts are encouraged over pedestrian walkways. Arcades may be used to connect separate buildings, or providing a more pleasing experience for pedestrians. Trellises or awnings may also be used to create a covered walkway to protect pedestrians from the sun and rain.

8.2.4 Architectural Details

Architectural Styles

A consistent architectural style should be used throughout the commercial development to create a sense of continuity between the buildings. Related elements, such as trellises, planters, light-standards, windows, doors, etc. should also adopt detailing that is compatible with



the selected architectural style. The selected style of the commercial development does not necessarily need to be of any one of those available for the residential community but should be compatible to them.

Architectural styles should derive from Southern California and Ontario's history. Modern and "high-tech" styles are not acceptable style choices. Acceptable styles generally respond to the region's climate including such features as shade windows, outdoor circulation, and outdoor courtyards or plazas, recessed window frames, awnings, arcades, loggias, and trellises.

Building Wall Treatment

No wall should have a blank, uninterrupted length exceeding 20 feet without including one of the following:

- ♦ Change in texture;
- ♦ Change in material;
- ♦ Change in plane;
- ♦ Lattice; and
- ♦ A tree or equivalent element.

Facades that are visible from adjacent streets or walkways should display even greater visual interest by using architectural elements that break up the massing of large buildings, such as windows, arcades, awnings, porticos, and other architectural features.

Base and Top Treatments

Base and top treatments help to balance the "weight" of the building visually. Bases should appear to "ground" the building, while tops create a defined edge to the roofline. Possible treatment techniques are as follows:

- ♦ Base;
- ♦ Thicker walls;
- ♦ Natural materials;
- ♦ Different colored materials;
- ♦ Enriched landscaping with a mature height of at least 18";
- ♦ Special materials, such as ceramic tile, granite and marble, are encouraged on the base of buildings that face streets or connecting walkways, especially adjacent to major entries;
- ♦ Tops;
- ♦ Cornice treatments;
- ♦ Roof overhangs with brackets;

- ♦ Stepped parapets;
- ♦ Textured materials; and
- ♦ Different colored materials.

Exterior Materials

Recommended materials include stucco, exterior plaster, brick, wood siding, tile, or stone. Exterior materials that appear pre-fabricated are not recommended. Selected materials and detailing should have an enduring appearance.

Roof Materials

Roofing materials that are generally acceptable include metal standing seam, concrete tile, ceramic tile and slate or slate-like materials.

Colors

Color selection should be consistent with the selected architecture style.

8.2.5 Open Space and Landscaping

Site Accessories

Site accessories, such as recycling bins, bike racks, litter cans, planters, benches and transit shelters, should use materials and have an architectural character consistent with the overall project. These accessories should be graffiti-resistant with materials that are easy to clean and paint.

Connecting Walkways

Walkways throughout the Specific Plan's commercial development should connect the various buildings to each other and to the sidewalks. Use of trellises, landscaping, sitting areas and the site planning of buildings adjacent to one another will encourage pedestrian activity within the commercial area. A typical walkway should be a minimum of six (6) feet wide. In large parking lots, pedestrian walkways should be provided within the median of at least two parking aisles for every five (5) aisles of 20 stalls or more.

Pedestrian walkways that may also be used for emergency access must allow minimum clearance of 30' wide and 14' vertical clearance.

Off-site Connections

Off-site connections should be provided from the commercial area to the Specific Plan residential development. Access to the residential community will be available by adjacent sidewalks. Pedestrian trails should not lead to the rear of a Commercial Center.



Plazas

Plazas are encouraged as much as possible to emphasize a pedestrian-friendly environment by creating locations within the commercial development that allow for people to gather. Plazas create a more inviting feeling to pedestrians, allowing them to feel safe from the vehicular dominated parking lot. These outdoor places may include water features, landscaping, and other elements to create a comfortable place to sit. Plazas are encouraged where high levels of pedestrian activity are expected, such as adjacent to major entrances and nearby uses that allow for more interactivity with the plaza such as delis, cafes, restaurants, bakeries and other food services.

Landscaping

Landscaping helps to soften the feel of the buildings and parking lot while enhancing the visual quality of the site. The following landscaping techniques may be used:

Shrubs and tree canopies shall not be higher than three and lower than seven feet respectively to allow police officers the ability to patrol the project site without visual obstructions. Plants near low-lying windows shall have thorns as a deterrent for suspects hiding and removing glass panes as a point of entry.

- ♦ Provide special landscaping treatment at all project entries and building entries;
- ♦ Provide shade/canopy trees within parking areas;
- ♦ Use plants to define outdoor spaces such as edges, outdoor plazas, or movement paths between parking and building entrances;
- ♦ Chosen plantings should provide a continuity of form across the entire project.
- ♦ Where a parking lot contains 10 or more parking spaces and is visible from a street, not less than 5 percent of the total area, excluding any perimeter landscaping, shall be landscaped. Such landscaping shall be concentrated in any one location. A minimum of 50 percent of the plant material shall be canopy or shade trees. Contact the Community Services Department for specific requirements;
- ♦ All rows of parking spaces shall be provided with landscape islands at each row terminus, minimum five (5) feet in width, exclusive of curbs, to protect parked vehicles, ensure visibility, confine moving traffic to drive aisles and driveways, and provide within each row of parking spaces, landscaped islands, minimum five (5) feet in width exclusive of curbs, located so as to prevent more than ten vehicles from being parked side-by-side in an abutting configuration.

- ♦ Landscaped areas shall be delineated with a concrete curb, minimum 6-inches high by 6-inches wide; except where a landscape area is parallel and adjacent to a parking stall, the curb shall be a minimum 6-inches high by 12-inches wide, to provide an area for persons to step when entering or exiting a motor vehicle.

Outdoor Storage Areas

Outdoor storage areas should be incorporated into the design of a project to avoid visual impacts on the site.

Outdoor storage areas should be located away from the street, behind or to the side of buildings. Walls and landscaping should be used to screen stored materials.

Transit Shelters

Transit shelters should be incorporated into the design of the commercial project and will be designed according to guidelines provided by the transit agency of Ontario. The commercial developer should explore with the transit agency and the City, possibilities for a structure that is integrated architecturally with the project through its color, materials and architectural style.

Pedestrian walkways should be provided for easy accessibility by pedestrians moving to and from the transit stop to the commercial development.

Lighting

Lighting standards under 15 feet should provide adequate illumination throughout the site during the nighttime hours. Lighting fixtures should be designed to be compatible with the architectural styles selected for the project. At a minimum, all light standards should have an attractive base and top; overhead "cobrahead" standards are not permitted. Buildings and landscaping can be illuminated indirectly to create a strong positive image. Along pedestrian walks, lighting should provide appropriate illumination at a human scale without too much glare. Lighting within larger parking lots should provide safe visibility. Service area lighting should be positioned as to not be seen from public view.



9. General Plan Consistency





9.0 General Plan Consistency

California Government Code (Title 7, Division 1, Chapter 3, Article 8, Section 65450-65457) permits the adoption and administration of specific plans as an implementation tool for elements contained in the local General Plan. Specific plans must demonstrate consistency in regulations, guidelines, and programs with the goals and policies set forth in the General Plan.

The Subarea 29 Specific Plan (SP) has been prepared in conformance with the goals and policies of The Ontario Plan (TOP) Policy Plan (General Plan). The policy analysis in this Chapter describes the manner in which the SP complies with TOP Goals and Policies applicable to the project.

<i>Land Use Element</i>	
POLICY PLAN GOALS AND POLICIES	SP CONSISTENCY
GOAL LU1: A COMMUNITY THAT HAS A SPECTRUM OF HOUSING TYPES AND PRICE RANGES THAT MATCH THE JOBS IN THE CITY AND THAT MAKE IT POSSIBLE FOR PEOPLE TO LIVE AND WORK IN ONTARIO AND MAINTAIN A QUALITY OF LIFE.	
LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identify, maximizes available and planned infrastructure, and foster the development of transit.	The SP area is within the New Model Colony as identified in The Ontario Plan. The SP will adhere to current approved City of Ontario Infrastructure Master Plans.
LU1-2 Sustainable Community Strategy. We integrate state, regional and local Sustainable Community/ Smart Growth principles into the development and entitlement process.	Sustainable Community/Smart Growth principles are incorporated into Land Use Plan. Pedestrian sidewalks and multipurpose trails provide connectivity.
LU1-3 Adequate Capacity. We require adequate infrastructure and services for all development.	The SP establishes an infrastructure and public facilities plan to ensure that adequate roadways and public utilities including sewer, water and drainage facilities along with parks and other public facilities are provided to serve the SP area.
LU1-4 Mobility. We require development and urban design, where appropriate, that reduces reliance on the automobile and capitalizes on multi-modal transportation opportunities. (Link to Mobility Element Policy M3-3)	Multipurpose trails are provided within the SP area. These trails connect with the trails located on the City's Multipurpose Trails and Bikeway Corridor Plan and provides points of connection to parks, residential neighborhoods, schools and the neighborhood commercial area.
LU1-6 Complete Community. We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Link to Complete Community Section of Community Economics Element)	The SP plans for a complete community with a mix of residential, commercial, and parks where residents can live, work, shop and recreate.
GOAL LU2: COMPATIBILITY BETWEEN A WIDE RANGE OF USES.	
LU2-6 Infrastructure Compatibility. We require infrastructure to be aesthetically pleasing and in context with the community character.	The SP provides for an aesthetically pleasing streetscape concept.
LU2-9 Methane Gas Sites. We require sensitive land uses and new uses on former dairy farms or other methane-producing sites be designed to minimize health risks.	The project will comply with appropriate mitigation measures identified in the project EIR for soil remediation and proper venting to address the potential existence of methane gases within the project. Per development requirements, several feet of soil that is potentially methane producing will be removed from the site to minimize risks.
GOAL LU5: INTEGRATED AIRPORT FACILITIES THAT MINIMIZE NEGATIVE IMPACTS TO THE COMMUNITY AND MAXIMIZE ECONOMIC BENEFITS.	
LU5-7 ALUCP Consistency and Land Use Regulations. We comply with state law that requires general plans, specific plans and all new development to be consistent with the policies and criteria set forth within an Airport Land Use Compatibility Plan for any public use airport.	The SP is located within the Airport Influence Areas of Chino Airport and Ontario International Airport. Section 4.8 requires all development to be consistent with the Airport Land Use Compatibility Plans for both airports.

<i>Housing Element</i>	
GOAL H2: DIVERSITY OF TYPES OF QUALITY HOUSING THAT ARE AFFORDABLE TO A RANGE OF HOUSEHOLD INCOME LEVELS, ACCOMMODATE CHANGING DEMOGRAPHICS, AND SUPPORT AND REINFORCE THE ECONOMIC SUSTAINABILITY OF ONTARIO.	
H2-4 New Model Colony. We support a premier lifestyle community in the New Model Colony distinguished by diverse housing, highest design quality, and cohesive and highly amenitized neighborhoods.	The SP features single family detached housing types in a variety of plotting configurations distinguished by the highest design quality and cohesive and highly amenitized neighborhoods.
H2-5 Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices and other best practices.	The SP has its own design guidelines to achieve architectural excellence.
GOAL H4: INCREASED OPPORTUNITIES FOR LOW AND MODERATE INCOME HOUSEHOLDS AND FAMILIES TO AFFORD AND MAINTAIN QUALITY OWNERSHIP AND RENTAL HOUSING OPPORTUNITIES, INCLUDING MOVE-UP OPPORTUNITIES.	
H4-4: Mixed-income Housing. We encourage the integration of affordable housing in the New Model Colony, Ontario Airport Metro Center Area, and existing neighborhoods.	The SP provides for a wide range of home ownership opportunities for many different lifestyles.
<i>Parks and Recreation Element</i>	
GOAL PR1: A SYSTEM OF SAFE AND ACCESSIBLE PARKS THAT MEETS THE NEEDS OF THE COMMUNITY.	
PR1-1 Access to Parks. We strive to provide a park and/or recreational facility within walking distances (1/4 mile) of every residence.	The SP provides for parks and multipurpose trails that allow all homes to be within walking distance.
PR1-5 Acreage Standard. We strive to provide 5 acres of parkland (public and private) per 1,000 residents.	The SP provides for park land, park improvements and/or park fees or a combination thereof in order to meet and comply with City park standards.
PR1-6 Private Parks. We expect development to provide a minimum of 2 acres of developed private park space per 1,000 residents.	The SP will provide a minimum of 2 acres of developed private park space per 1,000 residents.
PR1-9 Phased Development. We require parks be built in new communities before a significant proportion of residents move in.	Development within the SP is required to be reviewed and approved pursuant to the City's Subdivision Ordinance which requires the approval of tentative and final subdivision maps for the project. Conditions of approval associated with the City's approval of tentative subdivision maps will provide for the timing of construction of parks as part of the development.
PR1-11 Environmental Function of Parks. We require new parks to meet environmental management objectives.	The SP requires all new development to be reviewed and approved pursuant to the provisions of the City's Subdivision Ordinance and Development Plan Review process which provides for review by the City's Planning Department which may require the development to incorporate environmental management objectives into the design of parks.
PR1-12 Trails. We promote connections between parks and local trails including those managed by other public agencies.	Multipurpose trails are provided within the SP area. These trails connect with the trails located on the City's Multipurpose Trails and Bikeway Corridor Plan and provides points of connection to parks, residential neighborhoods, schools and the neighborhood commercial area.
PR1-14 Multi-family Residential Developments. We require that new multi-family residential developments of five or more units provide recreational facilities or open space, in addition to paying adopted impact fees.	The SP requires that all condominium and multi-family developments within the project provide private recreational areas and/or pocket parks for residents of the development in addition to the payment by the developer of adopted impact fees.



<i>Environmental Resources Element</i>	
GOAL ER1: A RELIABLE AND COST EFFECTIVE SYSTEM THAT PERMITS THE CITY TO MANAGE ITS DIVERSE WATER RESOURCES AND NEEDS.	
ER1-3 Conservation. We require conservation strategies that reduce water usage.	All SP development will meet CALgreen low flow fixture requirements. All irrigation systems will be designed per AB 325 guidelines and/or the City of Ontario's Water Conservation Ordinance.
ER1-5 Groundwater Management. We protect groundwater quality by incorporating strategies that prevent pollution, require remediation where necessary, capture and treat urban run-off, and recharge the aquifer.	The SP development will comply with all applicable regulations in regards to protecting groundwater quality.
ER1-6 Urban Run-off Quantity. We encourage the use of low impact development strategies to intercept run-off, slow the discharge rate, increase infiltration and ultimately reduce discharge volumes to traditional storm drain systems.	SP development shall be designed to detain, filter and treat surface runoff, in a manner and combination which is practical, to comply with the most recent requirements of the San Bernardino County NPDES Stormwater Program's Water Quality Management (WQMP) for significant new development projects. Low impact development strategies will be implemented as part of source control best management practices.
ER1-7 Urban Run-off Quality. We require the control and management of urban run-off, consistent with Regional Water Quality Control Board regulations.	The project is obligated to control urban run-off, consistent with Regional Water Quality Control Board regulations as part of the regional NPDES Permit.
ER1-8 Wastewater Management. We require the management of wastewater discharge and collection consistent with waste discharge requirements adopted by the Regional Water Quality Control Board.	The project is obligated to comply with wastewater discharge and collection requirements adopted by the Regional Water Quality Control Board.
GOAL ER3: COST-EFFECTIVE AND RELIABLE ENERGY SYSTEM SUSTAINED THROUGH A COMBINATION OF LOW IMPACT BUILDING, SITE AND NEIGHBORHOOD ENERGY CONSERVATION AND DIVERSE SOURCES OF ENERGY GENERATION THAT COLLECTIVELY HELPS TO MINIMIZE THE REGION'S CARBON FOOTPRINT.	
ER3-1 Conservation Strategy. We require conservation as the first strategy to be employed to meet applicable energy-saving standards.	All development within the SP will meet California's stringent CALgreen and Energy Codes.
ER3-2 Green Development- Communities. We require the use of best practices identified in green community rating systems to guide the planning and development of all new communities.	The SP contains a broad mix of land uses, density, trails and proximity to planned transit as identified in the best practices of several rating systems.
R3-3 Building and Site Design. We require new construction to incorporate energy efficient building and site design strategies, which could include appropriate solar orientation, maximum use of natural daylight, passive solar and natural ventilation.	All development within the SP will meet California's stringent CALgreen and Energy Codes.
GOAL ER4: IMPROVED INDOOR AND OUTDOOR AIR QUALITY AND REDUCED LOCALLY GENERATED POLLUTANT EMISSIONS.	
ER4-4 Indoor Air Quality. We will comply with State Green Building Codes relative to indoor air quality.	SP development will be required to comply with all State laws in regards to indoor air quality.
GOAL ER5: PROTECTED HIGH VALUE HABITAT AND FARMING AND MINERAL RESOURCE EXTRACTION ACTIVITIES THAT ARE COMPATIBLE WITH ADJACENT DEVELOPMENT.	
ER5-2 Entitlement and Permitting Process. We comply with state and federal regulations regarding protected species.	The project will comply with all mitigation measures identified in the project EIR with regard to biological resources.

ER5-3 Right to Farm. We support the right of existing farms to continue their operations within the New Model Colony.	N/A, this refers to a City function.
ER5-4 Transition of Farms. We protect both existing farms and sensitive uses around them as agricultural areas transition to urban uses.	N/A, this refers to a City function.
<i>Community Economics Element</i>	
GOAL CE1: A COMPLETE COMMUNITY THAT PROVIDES FOR ALL INCOMES AND STAGES OF LIFE.	
CE1-7 Retail Goods and Services. We seek to ensure a mix of retail businesses that provide the full continuum of goods and services for the community.	The SP contains an approximately 12.1-acre neighborhood commercial site to provide goods and services to the community as well as surrounding areas.
CE1-12 Circulation. We continuously plan and improve public transit and non-vehicular circulation for the mobility of all, including those with limited or no access to private automobiles. (Link to Mobility Public Transit)	The SP multipurpose trails throughout the community. A bus turnout is provided on Merrill Avenue.
GOAL CE2: A CITY OF DISTINCTIVE NEIGHBORHOODS, DISTRICTS, AND CORRIDORS, WHERE PEOPLE CHOOSE TO BE.	
CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community. (Link to Community Design Element)	The SP sets forth criteria for creating high-quality places.
CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.	The SP provides a framework for high quality architecture and urban design.
<i>Safety Element</i>	
GOAL S1: MINIMIZED RISK OF INJURY, LOSS OF LIFE, PROPERTY DAMAGE AND ECONOMIC AND SOCIAL DISRUPTION CAUSED BY EARTHQUAKE-INDUCED AND OTHER GEOLOGIC HAZARDS.	
S1-1: Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.	All habitable buildings within the SP will be required to meet the California Building Code.
GOAL S3: REDUCED RISK OF DEATH, INJURY, PROPERTY DAMAGE AND ECONOMIC LOSS DUE TO FIRES, ACCIDENTS AND NORMAL EVERYDAY OCCURRENCES THROUGH PROMPT AND CAPABLE EMERGENCY RESPONSE.	
S3-8: Fire Prevention through Environmental Design. We require new development to incorporate fire prevention consideration in the design of streetscapes, sites, open space and buildings. (Link to Community Design Element)	All development within the SP incorporates fire prevention considerations in the design of streetscapes, sites, open spaces and buildings and will be verified for compliance during the review process.
GOAL S5: REDUCED RISK OF INJURY, PROPERTY DAMAGE AND ECONOMIC LOSS RESULTING FROM WINDSTORMS AND WIND- RELATED HAZARDS.	
S5-2 Dust Control Measures. We require the implementation of Best Management Practices for dust control at all excavation and grading project.	Construction within the Specific Plan will comply with all mitigation measures identified in the project EIR with regard to dust control.

GOAL S6: REDUCED POTENTIAL FOR HAZARDOUS MATERIALS EXPOSURE AND CONTAMINATION.	
S6-9 Remediation of Methane. We require development to assess and mitigate the presence of methane, per regulatory standards and guidelines.	The project will comply with all mitigation measures identified as part of the venting to address the potential existence of methane gases within the project.
GOAL S7: NEIGHBORHOODS AND COMMERCIAL AND INDUSTRIAL DISTRICTS THAT ARE KEPT SAFE THROUGH A MULTI-FACETED APPROACH OF PREVENTION, SUPPRESSION, COMMUNITY INVOLVEMENT AND A SYSTEM OF CONTINUOUS MONITORING.	
S7-4 Crime Prevention through Environmental Design (CPTED). We require new development to incorporate CPTED in the design of streetscapes, sites, open spaces and buildings.	The Specific Plan requires all new development to be reviewed and approved pursuant to the provisions of the City's Subdivision Ordinance and Development Plan Review process which provides for review by the City's Police Department which may require the development to incorporate CPTED in the design of streetscapes, sites, open spaces and buildings.
<i>Mobility Element</i>	
GOAL M1: A SYSTEM OF ROADWAYS THAT MEETS THE MOBILITY NEEDS OF A DYNAMIC AND PROSPEROUS ONTARIO.	
<p>M1-1: Roadway Design and Maintenance. We require our roadways to:</p> <ul style="list-style-type: none"> * Comply with federal, state and local design and safety standards. * Meet the needs of multiple transportation modes and users. * Handle the capacity envisioned in the Functional Roadway Classification Plan. * Maintain a peak hour Level of Service (LOS) E or better at all intersections. * Be compatible with the streetscape and surrounding land uses. * Be maintained in accordance with best practices and our Right-of-Way Management Plan. 	The SP is consistent with the requirements of the City's Functional Roadway Classification Plan. The City will maintain arterial streets and curb-to-curb internal streets through the creation of an Community Facilities District. Parkway improvements within the ROW along interior streets and alleys shall be maintained by an HOA.
M1-2: Mitigation of Impacts. We require development to mitigate its traffic impacts.	Development within the SP will implement all traffic mitigation measures, standard conditions and project design features identified in the project EIR.
GOAL M2: A SYSTEM OF TRAILS AND CORRIDORS THAT FACILITATE AND ENCOURAGE BICYCLING AND WALKING.	
M2-3: Pedestrian Walkways. We require walkways that promote safe and convenient travel between residential areas, businesses, schools, parks, recreation areas, and other key destination points. (Link to Community Design CD3-1)	Pedestrian walkways, as shown in the roadway plans and sections, exist throughout the SP area promoting safe and convenient travel between destination points.
GOAL M3: A PUBLIC TRANSIT SYSTEM THAT IS A VIABLE ALTERNATIVE TO AUTOMOBILE TRAVEL AND MEETS THE BASIC TRANSPORTATION NEEDS OF THE TRANSIT DEPENDENT.	
M3-2 Transit Facilities at New Development. We require new development to provide transit facilities, such as bus shelters, transit bays and turnouts, as necessary.	A bus turnout in Merril Avenue is being developed.



<i>Community Design Element</i>	
GOAL CD1: A DYNAMIC, PROGRESSIVE CITY CONTAINING DISTINCT NEIGHBORHOODS AND COMMERCIAL DISTRICTS THAT FOSTER A POSITIVE SENSE OF IDENTITY AND BELONGING AMONG RESIDENTS, VISITORS, AND BUSINESSES.	
CD1-2 Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.	The Design Guidelines set forth a clear and cohesive design theme by providing design criteria for site planning, architectural styles, landscaping, and walls and fences.
CD1-5 View Corridors. We require all major north- south streets be designed and redeveloped to feature views of the San Gabriel Mountains, which are part of the City’s visual identity and a key to geographic orientation. Such views should be free of visual clutter, including billboards and may be enhanced by framing with trees.	The major north-south streets are designed to feature views of the San Gabriel Mountains and comply with City standards for these streets.
GOAL CD2: A HIGH LEVEL OF DESIGN QUALITY RESULTING IN PUBLIC SPACES, STREETSCAPES, AND DEVELOPMENTS THAT ARE ATTRACTIVE, SAFE, FUNCTIONAL AND DISTRICT.	
CD2-1: Quality Architecture. We encourage all development projects to convey visual interest and character through: <ul style="list-style-type: none"> * building volume, massing, and height to provide appropriate scale and proportion; * a true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and * exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style. 	The Design Guidelines set forth a clear and cohesive design theme by providing design criteria for architecture, landscaping, and walls and fences.
CD2-2 Neighborhood Design. We create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as: <ul style="list-style-type: none"> * a pattern of smaller, walkable blocks that promote access, activity and safety; * variable setbacks and parcel sizes to accommodate a diversity of housing types; * traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows; * floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the “outdoor living room”), as appropriate; and * landscaped parkways, with sidewalks separated from the curb. 	The Design Guidelines provide for distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places.
CD2-5: Streetscapes. We design new and, when necessary, retrofit existing streets to improve walkability, bicycling and transit integration, strengthen connectivity, and enhance community identity through improvements to the public right of way such as sidewalks, street trees, parkways, curbs, street lighting and street furniture.	The Circulation Plan is designed to accommodate pedestrians, bicycles and autos equally.

<p>CD2-6: Connectivity. We promote development of local street patterns and pedestrian networks that create and unify neighborhoods, rather than divide them, and create cohesive and continuous corridors, rather than independent “islands” through the following means: local street patterns that provide access between subdivisions and within neighborhoods and discourage through traffic; a local street system that is logical and understandable for the user. A grid system is preferred to avoid circuitous and confusing travel paths between internal neighborhood areas and adjacent arterials; and neighborhoods, centers, public schools, and parks that are linked by pedestrian greenways open space networks. These may also be used to establish clear boundaries between distinct neighborhoods and/or centers.</p>	<p>The Circulation Plan is designed for connectivity that unifies neighborhoods.</p>
<p>CD 2-7: Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.</p>	<p>All buildings will meet or exceed California’s stringent CALgreen and California Energy Codes reducing energy demand.</p>
<p>CD2-8: Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.</p>	<p>The SP encourages “eyes on the street” for neighborhood safety and security.</p>
<p>CD2-9: Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.</p>	<p>The SP landscape enhances the aesthetics of structures, created and defines public and private spaces and provides shade.</p>
<p>CD2-11: Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.</p>	<p>The SP contains guidelines for neighborhood entries and monumentation.</p>
<p>CD2-12: Site and Building Signage. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.</p>	<p>The SP contains guidelines for neighborhood entries and monumentation.</p>
<p>GOAL CD3: VIBRANT URBAN ENVIRONMENTS THAT ARE ORGANIZED AROUND INTENSE BUILDINGS, PEDESTRIAN AND TRANSIT AREAS, PUBLIC PLAZAS, AND LINKAGES BETWEEN AND WITHIN DEVELOPMENTS THAT ARE CONVENIENTLY LOCATED, VISUALLY APPEALING, AND SAFE DURING ALL HOURS.</p>	
<p>CD3-1 Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics. (Link to Bicycle and Pedestrians Section of the Mobility Element and Policies M2-3 and M2-4)</p>	<p>The SP is designed for bicycle and pedestrian accessibility throughout the community through a network of off-street bike and pedestrian trails.</p>

CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.	Landscaping and paving are used to maintain a visual connectivity between streets, sidewalks, and walkways for pedestrians.
CD3-3 Building Entrances. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces. (Link to Safety Element Policy S7-4)	The SP guidelines for doors and entries promote the interface between public and private spaces.
CD3-5 Paving. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.	Sidewalks and road surfaces contribute to the appearance of streets and public spaces.
GOAL CD5: A SUSTAINED LEVEL OF MAINTENANCE AND IMPROVEMENT OF PROPERTIES, BUILDINGS AND INFRASTRUCTURE THAT PROTECTS THE PROPERTY VALUES AND ENCOURAGES PUBLIC AND PRIVATE INVESTMENTS.	
CD5-1 Maintenance of Buildings and Property. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.	A master Home Owner Association (HOA) will be established to maintain common areas and other areas as shown in the Maintenance Responsibility Matrix.
CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.	The HOA will maintain infrastructure elements as shown in the Maintenance Responsibility Matrix and in accordance with City rules and regulations.
<i>Community Economics Element</i>	
GOAL CE1: A COMPLETE COMMUNITY THAT PROVIDES FOR ALL INCOMES AND STAGES OF LIFE.	
CE1-6 Diversity of Housing. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.	The SP provides a wide array of residential product types, including single family detached, single family attached to provide housing opportunities for a variety of stages of life and price points to foster a socially balanced community.
GOAL CE2: A CITY OF DISTINCTIVE NEIGHBORHOODS, DISTRICTS, AND CORRIDORS, WHERE PEOPLE CHOOSE TO BE.	
CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.	The SP provides for unique, high-quality places that add value to the community.
CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.	The SP requires all new development to be reviewed and approved pursuant to the provisions of the City's Subdivision Ordinance and Development Plan Review process.
CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.	Maintenance will be ensured by the HOA.
CE2-6 Public Maintenance. We require the establishment and operation of maintenance districts or other vehicles to fund the long-term operation and maintenance of the public realm whether on private land, in rights-of-way, or on publicly-owned property.	A Community Facilities District (CFD) may be established in conjunction with the Master Homeowners Association to maintain the public realm at the Avenue.



Appendix





Appendix

APPENDIX A—LEGAL DESCRIPTION

Subarea 29

Section 23, Township 2 South, Range 7 West, San Bernardino Meridian, in the County of San Bernardino, State of CALIFORNIA, according to official plat thereof.

Together with the northeast quarter of Section 22, Township 2 South, Range 7 West, San Bernardino meridian, in the County of San Bernardino, state of CALIFORNIA, according to official plat thereof.

Excepting therefrom that portion lying westerly of the easterly line of that portion conveyed to the San Bernardino County Flood Control District by deed recorded September 9, 1977 in book 9259, page 335 of official records.

Also Excepting therefrom that portion lying westerly of the easterly line of that portion conveyed to the San Bernardino County Flood Control District by deed recorded November 18, 1977 in book 9308, page 683 of official records.

APPENDIX B—GLOSSARY OF SPECIFIC PLAN TERMS

A. Glossary of Specific Plan terms

Accessory Structures —“Accessory structures” shall mean a detached subordinate structure located on the same lot, which is devoted exclusively to a use customarily incidental and subordinate to the principal structure.

Building Height – “Building” Height shall mean the vertical distance measured from the average level of the highest and lowest point of the building site covered by the building, to the highest point of the structure.

Casita—An additional bedroom, home office, or use area located on the same lot as a single-family detached residence, which does not require subdivision. A Casita may contain its own bathroom facilities but not a kitchen. It also may not, by deed restriction, be rented or sold separately from the appurtenant residence.

A Casita shall meet main building setbacks, standard height limits, overall lot coverage, floor area ratios, and other applicable requirements.

Conditional Use – “Conditional Use” shall mean a use permitted on a particular site and within a zoning district only upon finding that such use in a specified location will comply with all the conditions and standards for the location or operation of such use.

Front Yard – “Front Yard” shall mean a yard extending across the full width of the lot, having at no point a depth of less than the minimum required, horizontal distance between the front lot line, or its tangent, and the closest permissible locations of the main building. A line at right angle to the front lot line or its tangent shall measure said distance. Front yard setbacks shall be measured from back of walk or R/W, if no walk.

Home Occupations – “Home Occupations” shall mean an occupation customarily conducted as a secondary use entirely within a dwelling by the occupant of the dwelling in connection with which there is not display, no stock in trade, or commodity sold on the premises, and no persons employed: and which is conducted in such a manner that the outward appearance of the premise gives no indication of other than residential use, and which is not detrimental to the residential character of the neighborhood by virtue of traffic flow, noise, odor, or other adverse conditions.

Lot Coverage – “Lot Coverage” shall mean that portion of the area of a lot, expressed as a percentage, occupied by all buildings or structures which are roofed or otherwise covered (exclusive of eaves or overhangs) and which extend more than three (3) feet above grade level).

Lot Size – “Lot Size” shall mean the total area exclusive of public streets within the boundary lines of the lot.

Lot Width – “Lot Width” shall mean the horizontal distance between the side lot line measured at right angles to the line comprising the depth of the lot at a point midway between the front and rear lot lines.

Minimum Floor Area – “Minimum Floor Area” shall mean the minimum total horizontal area expressed in square feet, of all the floors of a building included within the surrounding walls, including shafts and enclosed exterior stairwells.

Rear Yard – “Rear Yard” shall mean a yard extending across the full width of the lot, having at no point a depth of less than the minimum required horizontal distance as measured from the part of the main building nearest the rear lot line towards the rear lot line, and such measurement shall be along a line representing the shortest distance between said part of the main building and the rear lot line.

Side Yard – “Side Yard” shall mean a yard between the main building and the side lot lines extending from the required front yard, or the front lot line where not front yard is required, to the rear yard, the width of which side yard shall be measured horizontally from, and at right angles to, the nearest point of a side lot line toward the nearest part of a main building.



Temporary Uses – “Temporary Uses” shall mean residential or commercial construction activities, including necessary construction offices and materials and equipment storage, subdivision sales and leasing offices, sales information centers, sales pavilions, model home complexes, and signage located within the subdivision.

APPENDIX C—SUMMARY OF KEY SPECIFIC PLAN BACKGROUND INFORMATION

Technical appendices

- ♦ Traffic Study and/or TIA (Traffic Impact Analysis);
- ♦ Hydrology/Drainage Study and drainage master plan for the Specific Plan area;
- ♦ Sewer Master Plan for the Specific Plan area;
- ♦ Water Master Plan for the Specific Plan area; and
- ♦ Geotechnical/Soils study (if applicable) for the Specific Plan area.



City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

Planning Department Land Development Division Conditions of Approval

Meeting Date: May 25, 2021

File No: PSPA20-006

Related Files: PMTT20-012

Project Description: An Amendment to the Subarea 29 Specific Plan to increase the overall density within Planning Area 27 (Cluster Homes – 7-14 du/ac) from 4.8 to 4.9 dwelling units per gross acre and establish a new residential product type (Motorcourt Cluster D – 8-Plex). Planning Area 27 is bounded by Merrill Avenue to the north, Southern California Edison easement to the west, and the San Bernardino County Flood Control District and the City of Eastvale to the south (APN: 0218-331-42); **submitted by SL Ontario Development Company, LLC.**

Prepared By: Jeanie Irene Aguilo, Associate Planner
Phone: 909.395.2418 (direct)
Email: jaguilo@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Specific Plan/Specific Plan Amendment. The following shall be submitted to the Planning Department within 30 days following City Council approval of the Specific Plan/Specific Plan Amendment:

- (a) Fifteen copies of the final Specific Plan document;
- (b) One complete, unbound copy of the final Specific Plan document;
- (c) One CD containing a complete Microsoft Word copy of the final Specific Plan document, including all required revisions;
- (d) Five CDs, each containing a complete PDF copy of the final Specific Plan document, including all required revisions; and
- (e) One CD containing a complete electronic website version of the final Specific Plan document, including all required revisions.

2.2 Environmental Review.

(a) The environmental impacts of this project were reviewed in conjunction with an **Addendum to The Ontario Plan Environmental Impact Report**, certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001 (City Council Resolution No. 2010-006). This application introduces no new significant environmental impacts. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures are a condition of project approval, and are incorporated herein by this reference. All previously adopted mitigation measures shall be a condition of project approval, as they are applicable, and are incorporated herein by this reference.

2.3 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.4 Tribal Consultation Conditions.

(a) Prior to the commencement of any ground disturbing activity at the project site, the project applicant shall retain a Native American Monitor approved by the Gabrieleno Band of Mission Indians-Kizh Nation – the tribe that consulted on this project pursuant to Assembly Bill A52 – SB18 (the “Tribe” or the “Consulting Tribe”). A copy of the executed contract shall be submitted to the City of Ontario Planning and Building Department prior to the issuance of any permit necessary to commence a ground-disturbing activity. The Tribal monitor will only be present on-site during the construction phases that involve ground-disturbing activities. Ground disturbing activities are defined by the Tribe as activities that may include, but are not limited to, pavement removal, potholing or auguring, grubbing, tree removals, boring, grading, excavation, drilling, and trenching, within the project area. The Tribal Monitor will complete daily monitoring logs that will provide descriptions of the day's activities, including construction activities, locations, soil, and any cultural materials identified. The on-site monitoring shall end when all ground-disturbing activities on the Project Site are completed, or when the Tribal Representatives and Tribal Monitor have indicated that all upcoming ground-disturbing activities at the Project Site have little to no potential for impacting Tribal Cultural Resources. Upon discovery of any Tribal Cultural Resources, construction activities shall cease in the immediate vicinity of the find (not less than the surrounding 100 feet) until the find can be assessed. All Tribal Cultural Resources unearthed by project activities shall be evaluated by the qualified archaeologist and Tribal monitor approved by the Consulting Tribe. If the resources are Native American in origin, the Consulting Tribe will retain it/them in the form and/or manner the Tribe deems appropriate, for educational, cultural and/or historic purposes. If human remains and/or grave goods are discovered or recognized at the Project Site, all ground disturbance shall immediately cease, and the county coroner shall be notified per Public Resources Code Section 5097.98, and Health & Safety Code Section 7050.5. Human remains and grave/burial goods shall be treated alike per California Public Resources Code section 5097.98(d)(1) and (2). Work may continue on other parts of the Project Site while evaluation and, if necessary, mitigation takes place (CEQA Guidelines Section 15064.5[f]). If a non-Native American resource is determined by the qualified archaeologist to constitute a “historical resource” or “unique archaeological resource,” time allotment and funding sufficient to allow for implementation of avoidance measures, or appropriate mitigation, must be available. The treatment plan established for the resources shall be in accordance with CEQA Guidelines Section 15064.5(f) for historical resources and PRC Sections 21083.2(b) for unique archaeological resources.

(b) Preservation in place (i.e., avoidance) is the preferred manner of treatment. If preservation in place is not feasible, treatment may include implementation of archaeological data recovery

excavations to remove the resource along with subsequent laboratory processing and analysis. Any historic archaeological material that is not Native American in origin shall be curated at a public, non-profit institution with a research interest in the materials, such as the Natural History Museum of Los Angeles County or the Fowler Museum, if such an institution agrees to accept the material. If no institution accepts the archaeological material, it shall be offered to a local school or historical society in the area for educational purposes.

2.5 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.



CITY OF ONTARIO

MEMORANDUM

TO: Jeanie Irene Aguilo, Associate Planner
Planning Department

FROM: Mike Gerken, Deputy Fire Chief/Fire Marshal
Fire Department

DATE: January 14, 2021

SUBJECT: PSPA20-006 - An Amendment to the Subarea 29 Specific Plan, to increase the number of units within Planning Area 27 (Cluster Homes) from 47 to 73 dwelling units, located south of and adjacent to Merrill Avenue, approximately 1,000 feet east of Celebration Avenue, within the PA 27 of the Subarea 29 Specific Plan (APN: 0218-331-42). Related File: PMTT20-012.

-
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.
-

SITE AND BUILDING FEATURES:

- A. 2019 CBC Type of Construction: Type V-B Wood Frame
- B. Type of Roof Materials: Non-Rated
- C. Ground Floor Area(s): Various
- D. Number of Stories: Two Story
- E. Total Square Footage: Various
- F. 2019 CBC Occupancy Classification(s): R-3, U

CONDITIONS OF APPROVAL:

1.0 GENERAL

- 1.1 The following are the Ontario Fire Department (“Fire Department”) requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards (“Standards.”) It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at www.ontarioca.gov/Fire/Prevention.
- 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

2.0 FIRE DEPARTMENT ACCESS

- 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25’) inside and forty-five feet (45’) outside turning radius per Standard #B-005.
- 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150’) in length shall have an approved turn-around per Standard #B-002.
- 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-four (24) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by fire department and other emergency services.

3.0 WATER SUPPLY

- 3.1 The required fire flow per Fire Department standards, based on the 2019 California Fire Code, Appendix B, is Choose an item. gallons per minute (g.p.m.) for Choose an item. hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.
- 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300’) apart, per Engineering Department specifications.

- ☒ 3.4 The water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

4.0 FIRE PROTECTION SYSTEMS

- ☒ 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard Choose an item.. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.

5.0 BUILDING CONSTRUCTION FEATURES

- ☒ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ☒ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- ☒ 5.3 Single station smoke alarms and carbon monoxide alarms are required to be installed per the California Building Code and the California Fire Code.
- ☒ 5.5 All residential chimneys shall be equipped with an approved spark arrester meeting the requirements of the California Building Code.

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT



Project File No.: PSPA20-006 & PMTT20-012

Address: SEC Merrill Avenue & SCE Easement

APN: 0218-33-42

Existing Land Use: Vacant

Proposed Land Use: Subarea 29 SPA to increase the number of units within PA27 Tentative Tract Map to 5.99 acres into 1 lot for condominium purposes

Site Acreage: 5.99 Proposed Structure Height: N/A

ONT-IAC Project Review: n/a

Airport Influence Area: ONT & Chino

Reviewed By: Lorena Mejia

Contact Info: 909-395-2276

Project Planner: Jeanie Aguilo

Date: 03/29/2021

CD No.: 2021-002

PALU No.: n/a

The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="checkbox"/> High Terrain Zone	<input type="checkbox"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input type="checkbox"/> FAA Notification Surfaces	<input type="checkbox"/> Recorded Overflight Notification
<input type="radio"/> Zone 2	<input type="checkbox"/> 65 - 70 dB CNEL	<input type="checkbox"/> Airspace Obstruction Surfaces	<input checked="" type="checkbox"/> Real Estate Transaction Disclosure
<input type="checkbox"/> Zone 3	<input type="checkbox"/> 60 - 65 dB CNEL	<input type="checkbox"/> Airspace Avigation Easement Area	
<input type="radio"/> Zone 4		Allowable Height: <u>200 FT +</u>	
<input type="radio"/> Zone 5			

The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1
 Zone 2
 Zone 3
 Zone 4
 Zone 5
 Zone 6

Allowable Height: 200 FT +

CONSISTENCY DETERMINATION

This proposed Project is: Exempt from the ALUCP
 Consistent
 Consistent with Conditions
 Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and Chino Airport was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT and Chino Airport.

See Attached Conditions

Airport Planner Signature: _____

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT

CD No.: 2021-002
PALU No.: _____

PROJECT CONDITIONS

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the Airport Land Use Compatibility Plan (ALUCP) for ONT. The applicant is required to meet the Real Estate Transaction Disclosure in accordance with California Codes (Business and Professions Code Section 11010-11024). New residential subdivisions within an Airport Influence Area are required to file an application for a Public Report consisting of a Notice of Intention (NOI) and a completed questionnaire with the Department of Real Estate and include the following language within the NOI:

NOTICE OF AIRPORT IN VICINITY: This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.



CITY OF ONTARIO

MEMORANDUM

TO: Scott Murphy, Community Development Director
Rudy Zeledon, Planning Director (Copy of memo only)
Diane Ayala, Advanced Planning Division (Copy of memo only)
Charity Hernandez, Economic Development
James Caro, Building Official
Khoi Do, City Engineer
Jamie Richardson, Landscape Planning Division
Ahmed Aly, Municipal Utility Company
Gabriel Gutierrez, Police Department
Mike Gerken, Deputy Fire Chief/Fire Marshal
Jay Bautista, T. E., Traffic/Transportation Manager
Lorena Mejia, Airport Planning
Eric Woosley, Engineering/NPDES
Robin Lucero, Code Enforcement (Copy of memo only)
Jimmy Chang, IT Department

FROM: Jeanie Irene Aguilo, Associate Planner

DATE: January 07, 2021

SUBJECT: FILE #: PSPA20-006 Finance Acct#:

The following project has been submitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by .

- Note:**
- Only DAB action is required
 - Both DAB and Planning Commission actions are required
 - Only Planning Commission action is required
 - DAB, Planning Commission and City Council actions are required
 - Only Zoning Administrator action is required

PROJECT DESCRIPTION: An Amendment to the Subarea 29 Specific Plan, to increase the number of units within PA# by # within the (APN: 0218-331-42). Related File: PMTT20-012

- The plan does adequately address the departmental concerns at this time.
 - No comments
 - Report attached (1 copy and email 1 copy)
 - Standard Conditions of Approval apply
- The plan does not adequately address the departmental concerns.
 - The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

Landscape Planning Division
Department _____ Signature _____ Title _____ Date 1/25/21

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PMTT20-012 / TTM 20389, A TENTATIVE TRACT MAP TO SUBDIVIDE 5.99 ACRES OF LAND INTO ONE NUMBERED LOT AND THREE LETTERED LOTS FOR CONDOMINIUM PURPOSES. THE PROJECT IS BOUNDED BY MERRILL AVENUE TO THE NORTH, SOUTHERN CALIFORNIA EDISON EASEMENT TO THE WEST, AND THE SAN BERNARDINO COUNTY FLOOD CONTROL DISTRICT AND THE CITY OF EASTVALE TO THE SOUTH, AND MAKING FINDINGS IN SUPPORT THEREOF—APN: 0218-331-42.

WHEREAS, SL ONTARIO DEVELOPMENT COMPANY, LLC (hereinafter referred to as "Applicant") has filed an Application for the approval of a Tentative Tract Map, File No. PMTT20-012, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 5.99 acres of land generally located bounded by Merrill Avenue to the north, Southern California Edison easement to the west, and the San Bernardino County Flood Control District and the City of Eastvale to the south, and is presently vacant; and

WHEREAS, the property to the north of the Project site is within the Planning Area 28 (Conventional Medium Lot) land use designation of the Subarea 29 Specific Plan and a residential subdivision in currently under construction. The properties to the east are located in the City of Eastvale within R-1 land use district and developed with residential land uses. The properties to the south are located in the City of Eastvale within the PRD (Planned Residential Development) land use district and developed with residential land uses. The property to the west is located within the Subarea 29 Specific Plan and has an SCE Corridor land use designation that is developed with SCE 180-foot-tall transmission towers (115kV) and power lines; and

WHEREAS, on December 15, 2020, the Applicant submitted a Tentative Tract Map application (File No. PMTT20-012/TTM 20389) to subdivide 5.99 acres of land into one numbered lot and three lettered lots for condominium purposes; and

WHEREAS, in conjunction with the Tentative Tract Map, the Applicant submitted an Amendment to the Subarea 29 Specific Plan (File No. PSPA20-006) to increase the density of the Subarea 29 Specific Plan from 4.8 to 4.9 dwelling units per gross acre within and establish a new residential product type (Motorcourt Cluster D – 8-Plex) within Planning Area 27 (Cluster Homes – 7-14 du/ac). The proposed change will increase the number of units within Planning Area 27 (Cluster Homes – 7-14 du/ac) from 47 to 73

dwelling units, for a total increase from the existing 2,392 dwelling units to 2,418 dwelling units within the Subarea 29 Specific Plan; and

WHEREAS, the Tentative Tract Map 20389 (File No. PMTT20-012) will subdivide 5.99 acres of land into one numbered lot for condominium purposes and three lettered lots for common areas, private streets, public utility easements and neighborhood landscape edges. The map will implement the required infrastructure improvements to serve the site. These improvements include the backbone infrastructure and the internal street circulation for residential neighborhoods. The Tentative Tract Map will accommodate the proposed Motorcourt Cluster D – 8-Plex product type and allow up to 73 units. The future development of the site will require review and approval of a Development Plan; and

WHEREAS, the project will have two access points from Merrill Avenue, which runs east-west along the northern frontage. The developer is responsible for the construction of the remaining backbone street improvements and all interior neighborhood streets necessary to serve the project; and

WHEREAS, future development of the site will be required to meet all Development Code and Subarea 29 Specific Plan standards regarding architecture, including style, decorative elements, enhancements, etc.; and

WHEREAS, future development of the site will be required to meet all Development Code and Subarea 29 Specific Plan standards regarding landscaping. The Policy Plan (Policy PR1-6) requires new developments to provide a minimum of 2 acres of private park per 1,000 residents. The proposed project is required to provide a 0.56-acre park to meet the minimum Policy Plan private park requirement. The final landscape plan, including park amenities and planting plan, will be reviewed and approved as part of the Development Plan process; and

WHEREAS, the project will be required to undergo a more extensive Preliminary Water Quality Management Plan (“PWQMP”) review as part of the Development Plan process. The PWQMP establishes project compliance with storm water discharge/water quality requirements and includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development (“LID”) best management practices (“BMPs”), such as retention and infiltration, biotreatment, and evapotranspiration. Additionally, the Project is consistent with the previously approved Development Agreement (File No. PDA16-001) that required all major backbone infrastructure improvements within the Subarea 29 Specific Plan; and

WHEREAS, the Subarea 29 Specific Plan Environmental Impact Report (State Clearinghouse No. 2004011009) was certified on October 17, 2006 (hereinafter referred

to as “Certified EIR”), in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as “EIR Addendum”) in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as “CEQA”); and

WHEREAS, the environmental impacts of this project were thoroughly analyzed in the EIR Addendum, which concluded that implementation of the Project could result in a number of significant effects on the environment that were previously analyzed in the Certified EIR, and that the Certified EIR identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an EIR Addendum has been prepared to determine possible environmental impacts; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as “ALUCP”), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity. The project site is also located within the

Airport Influence area of Chino Airport and is consistent with policies and criteria set forth within the 2011 California Airport Land Use Planning Handbook published by the California Department of Transportation, Division of Aeronautics; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 17, 2021, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Addendum and the Project, and concluded said hearing on that date, voting to issue Decision No. DAB21-020, recommending that the Planning Commission recommend the City Council approve the Addendum and Decision No. DAB21-021, recommending the Planning Commission approve the Tentative Tract Map, File No. PMTT20-012; and

WHEREAS, as the first action on the Project, on May 25, 2021, the Planning Commission issued a Resolution recommending the City Council adopt the EIR Addendum, finding that the proposed Project introduces no new significant environmental impacts and applying all previously adopted mitigation measures to the Project, which were incorporated by reference; and

WHEREAS, on May 25, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the decision-making body for the Project, the Planning Commission has reviewed and considered the information contained in the Addendum, the initial study, and the administrative record for the Project, including all written and oral evidence provided during the comment period. Based upon the facts and information contained in the Addendum, the initial study, and the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The environmental impacts of this project were reviewed in conjunction with an Addendum to the Subarea 29 Specific Plan Environmental Impact Report (State Clearinghouse No. 2004011009) was certified on October 17, 2006 in conjunction with File No. PSP03-003; and

(2) The EIR Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts; and

(4) All previously adopted mitigation measures shall be a condition of project approval, as they are applicable to the Project, and are incorporated herein by this reference; and

(5) The EIR Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and

(6) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts.

SECTION 2: *Subsequent or Supplemental Environmental Review Not Required.* Based on the EIR Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and.

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The Project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: *Housing Element Compliance.* Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based upon the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and with approval of the related Specific Plan Amendment (File No. PSPA20-006) the proposed project will be consistent with the number of dwelling units and density (7-14 DU/AC) within Planning Area 27, as specified in the Available Land Inventory.

SECTION 4: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as “ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making body for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). The project site is also located within the Airport Influence area of Chino Airport and is consistent with policies and criteria set forth within the 2011 California Airport Land Use Planning Handbook published by the California Department of Transportation, Division of Aeronautics. As a

result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 5: Concluding Facts and Reasons. Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 and 4, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed Tentative Tract Map is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and applicable area and specific plans, and planned unit developments.*** The proposed Tentative Tract Map is located within the Low-Density Residential land use district of the Policy Plan Land Use Map, and the Cluster Homes (Planning Area 27) land use district of the Subarea 29 Specific Plan. With approval of the amendment to the Specific Plan (File No. PSPA20-006), the proposed subdivision will be consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the project will contribute to providing “a spectrum of housing types and price ranges that match the jobs in the City, and that make it possible for people to live and work in Ontario and maintain a quality of life” (Goal LU1). Furthermore, the project will promote the City’s policy to “incorporate a variety of land uses and building types that contribute to a complete community where residents at all stages of life, employers, workers, and visitors, have a wide spectrum of choices of where they can live, work, shop, and recreate within Ontario” (Policy LU1-6 *Complete Community*).

(2) ***The design or improvement of the proposed Tentative Tract is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and applicable specific plans and planned unit developments.*** The proposed Tentative Tract Map is located within the Low-Density Residential land use district of the Policy Plan Land Use Map, and the Cluster Homes (Planning Area 27) land use district of the Subarea 29 Specific Plan. With approval of the amendment to the Subarea 29 Specific Plan, the proposed subdivision will be consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the project will contribute to providing “[a] high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct” (Goal CD2). Furthermore, the project will promote the City’s policy to “create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:

- A pattern of smaller, walkable blocks that promote access, activity and safety;

- Variable setbacks and parcel sizes to accommodate a diversity of housing types;
- Traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows;
- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the “outdoor living room”), as appropriate; and
- Landscaped parkways, with sidewalks separated from the curb.” (Policy CD2-2 *Neighborhood Design*).

(3) ***The site is physically suitable for the type of development proposed.***

With approval of the amendment to the Specific Plan, the project site meets the minimum lot area and dimensions of the Planning Area 27 land use district of the Subarea 29 Specific Plan, and is physically suitable for the type of residential development proposed in terms of zoning, land use and development activity proposed, and existing and proposed site conditions.

(4) ***The site is physically suitable for the density/intensity of development proposed.*** The project site is proposed for residential development at a density of approximately 9.7 DUs/acre. With approval of the amendment to the Specific Plan, the project site meets the minimum lot area and dimensions of Planning Area 27 land use district of the Subarea 29 Specific Plan and is physically suitable for this proposed density of development.

(5) ***The design of the subdivision or the proposed improvements thereon, are not likely to cause substantial environmental damage, or substantially and avoidably injure fish or wildlife, or their habitat.*** The Project site was previously graded, and the proposed Project is entirely within the physical impact area evaluated in the Subarea 29 Specific Plan Final EIR and no natural or sensitive habitats exist at the Project site. The proposed Project does not have the potential to degrade the quality of the environment, reduce the fish and wildlife habitat, threaten plant, fish or wildlife species. The Biological Resources section of the Addendum, concluded that Planning Area 27 does not support the Delhi Sands Flower-Loving Fly and due to the high level of disturbance, the land use activity, and lack of suitable habitat, other sensitive species are either presumed to be absent or have a very low potential to occur within the Specific Plan area, including the Project site.

(6) ***The design of the subdivision, or the type of improvements thereon, are not likely to cause serious public health problems.*** The design of the proposed subdivision, and the residential improvements existing or proposed on the project site, are not likely to cause serious public health problems, as the project is not anticipated to involve the transport, use, or disposal of hazardous materials during either construction or project implementation, include the use of hazardous materials or volatile fuels, nor are there any known stationary commercial or industrial land uses within close proximity to

the subject site that use/store hazardous materials to the extent that they would pose a significant hazard to visitors or occupants to the project site.

(7) ***The design of the subdivision, or the type of improvements thereon, will not conflict with easements acquired by the public at large for access through, or use of property within, the proposed subdivision.*** The proposed subdivision has provided for all necessary public easements and dedications for access through, or use of property within, the proposed subdivision. Furthermore, all such public easements and dedications have been designed pursuant to: (a) the requirements of the Policy Plan component of The Ontario Plan and applicable area plans; (b) applicable specific plans or planned unit developments; (c) applicable provisions of the City of Ontario Development Code; (d) applicable master plans and design guidelines of the City; and (e) applicable Standard Drawings of the City.

SECTION 6: Planning Commission Action. Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference. If the City Council does not approve the Amendment to the Subarea 29 Specific Plan (File No. PSPA20-006) this resolution should become null and void and of no further force and effect.

SECTION 7: Indemnification. The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 8: Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 9: Certification to Adoption. The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of May 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 25, 2021, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**File No. PMTT20-012 / TTM 20389
Departmental Conditions of Approval**

(Departmental conditions of approval to follow this page)



City of Ontario
Planning Department
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*Planning Department
Land Development Division
Conditions of Approval*

Meeting Date: May 25, 2021

File No: PMTT20-012

Related Files: PSPA20-006

Project Description: A Tentative Tract Map (TTM 20389) to subdivide 5.99 acres of land into one numbered lot and three lettered lots for condominium purposes, that is bounded by Merrill Avenue to the north, Southern California Edison easement to the west, and the San Bernardino County Flood Control District and the City of Eastvale to the south; (APN: 0218-331-42) **submitted by SL Ontario Development Company, LLC.**

Prepared By: Jeanie Irene Aguilo, Associate Planner
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Email: jaguilo@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits. Tentative Tract Map approval shall become null and void 2 years following the effective date of application approval, unless the final parcel/tract map has been recorded, or a time extension has been approved by the Planning Commission pursuant to Development Code Section 2.02.025 (Time Limits and Extensions). This Permit does not supersede any individual time limits specified herein for performance of specific conditions or improvements.

2.2 Subdivision Map.

(a) The Final Tract Map shall be in conformance with the approved Tentative Tract/Parcel Map on file with the City. Variations from the approved Tentative Tract/Parcel Map may be reviewed and approved by the Planning Department. A substantial variation from the approved Tentative Tract/Parcel Map may require review and approval by the Planning Commission, as determined by the Planning Director.

(b) Tentative Tract Map approval shall be subject to all conditions, requirements and recommendations from all other departments/agencies provided on the attached reports/memorandums.

(c) The subject Tentative Tract Map for condominium purposes shall require the recordation of a condominium plan concurrent with the recordation of the Final Tract/Parcel Map and CC&Rs.

(d) Pursuant to California Government Section 66474.9, the subdivider agrees that it will defend, indemnify, and hold harmless the City of Ontario or its agents, officers and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer of this subdivision, which action is brought within the time period provided for in Government Code Section 66499.37. The City of Ontario shall promptly notify the subdivider of any such claim, action or proceeding and the City of Ontario shall cooperate fully in the defense.

2.3 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.4 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b) Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

(c) Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(d) Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

2.5 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

2.6 Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

2.7 Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

2.8 Covenants, Conditions and Restrictions (“CC&Rs”)/Mutual Access and Maintenance Agreements.

(a) CC&Rs shall be prepared for the Project and shall be recorded prior to the issuance of a building permit.

(b) The CC&Rs shall be in a form and contain provisions satisfactory to the City. The articles of incorporation for the property owners association and the CC&Rs shall be reviewed and approved by the City.

(c) CC&Rs shall ensure reciprocal parking and access between parcels.

(d) CC&Rs shall ensure reciprocal parking and access between parcels, and common maintenance of:

(i) Landscaping and irrigation systems within common areas;

(ii) Landscaping and irrigation systems within parkways adjacent to the project site, including that portion of any public highway right-of-way between the property line or right-of-way boundary line and the curb line and also the area enclosed within the curb lines of a median divider (Ontario Municipal Code Section 7-3.03), pursuant to Ontario Municipal Code Section 5-22-02;

(iii) Shared parking facilities and access drives; and

(iv) Utility and drainage easements.

(e) CC&Rs shall include authorization for the City’s local law enforcement officers to enforce City and State traffic and penal codes within the project area.

(f) The CC&Rs shall grant the City of Ontario the right of enforcement of the CC&R provisions.

(g) A specific methodology/procedure shall be established within the CC&Rs for enforcement of its provisions by the City of Ontario, if adequate maintenance of the development does not occur, such as, but not limited to, provisions that would grant the City the right of access to correct maintenance issues and assess the property owners association for all costs incurred.

2.9 Disclosure Statements.

(a) A copy of the Public Report from the Department of Real Estate, prepared for the subdivision pursuant to Business and Professions Code Section 11000 et seq., shall be provided to each prospective buyer of the residential units and shall include a statement to the effect that:

(i) This tract is subject to noise from the Ontario International Airport and may be more severely impacted in the future.

(ii) Some of the property adjacent to this tract is zoned for agricultural uses and there could be fly, odor, or related problems due to the proximity of animals.

(iii) The area south of Riverside Drive lies within the San Bernardino County Agricultural Preserve. Dairies currently existing in that area are likely to remain for the foreseeable future.

(iv) This tract is part of a Landscape Maintenance District. The homeowner(s) will be assessed through their property taxes for the continuing maintenance of the district.

2.10 Environmental Review.

(a) The environmental impacts of this project were Project were reviewed in conjunction with an Addendum to the Subarea 29 Specific Plan Environmental Impact Report (State Clearinghouse No. 2004011009), certified by the Ontario City Council on April 21, 2015 in conjunction with

File No. PSPA14-002. This application introduces no new significant environmental impacts. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. The previously adopted mitigation measures shall be a condition of project approval, and are incorporated herein by this reference.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

2.11 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.12 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

2.13 Tribal Consultation Conditions.

(a) Prior to the commencement of any ground disturbing activity at the project site, the project applicant shall retain a Native American Monitor approved by the Gabrieleno Band of Mission Indians-Kizh Nation – the tribe that consulted on this project pursuant to Assembly Bill A52 – SB18 (the "Tribe" or the "Consulting Tribe"). A copy of the executed contract shall be submitted to the City of Ontario Planning and Building Department prior to the issuance of any permit necessary to commence a ground-disturbing activity. The Tribal monitor will only be present on-site during the construction phases that involve ground-disturbing activities. Ground disturbing activities are defined by the Tribe as activities that may include, but are not limited to, pavement removal, potholing or auguring, grubbing, tree removals, boring, grading, excavation, drilling, and trenching, within the project area. The Tribal Monitor will complete daily monitoring logs that will provide descriptions of the day's activities, including construction activities, locations, soil, and any cultural materials identified. The on-site monitoring shall end when all ground-disturbing activities on the Project Site are completed, or when the Tribal Representatives and Tribal Monitor have indicated that all upcoming ground-disturbing activities at the Project Site have little to no potential for impacting Tribal Cultural Resources. Upon discovery of any Tribal Cultural Resources, construction activities shall cease in the immediate vicinity of the find (not less than the surrounding 100 feet) until the find can be assessed. All Tribal Cultural Resources unearthed by project activities shall be

evaluated by the qualified archaeologist and Tribal monitor approved by the Consulting Tribe. If the resources are Native American in origin, the Consulting Tribe will retain it/them in the form and/or manner the Tribe deems appropriate, for educational, cultural and/or historic purposes. If human remains and/or grave goods are discovered or recognized at the Project Site, all ground disturbance shall immediately cease, and the county coroner shall be notified per Public Resources Code Section 5097.98, and Health & Safety Code Section 7050.5. Human remains and grave/burial goods shall be treated alike per California Public Resources Code section 5097.98(d)(1) and (2). Work may continue on other parts of the Project Site while evaluation and, if necessary, mitigation takes place (CEQA Guidelines Section 15064.5[f]). If a non-Native American resource is determined by the qualified archaeologist to constitute a “historical resource” or “unique archaeological resource,” time allotment and funding sufficient to allow for implementation of avoidance measures, or appropriate mitigation, must be available. The treatment plan established for the resources shall be in accordance with CEQA Guidelines Section 15064.5(f) for historical resources and PRC Sections 21083.2(b) for unique archaeological resources.

(b) Preservation in place (i.e., avoidance) is the preferred manner of treatment. If preservation in place is not feasible, treatment may include implementation of archaeological data recovery excavations to remove the resource along with subsequent laboratory processing and analysis. Any historic archaeological material that is not Native American in origin shall be curated at a public, non-profit institution with a research interest in the materials, such as the Natural History Museum of Los Angeles County or the Fowler Museum, if such an institution agrees to accept the material. If no institution accepts the archaeological material, it shall be offered to a local school or historical society in the area for educational purposes.

2.14 Additional Requirements.

(a) The approval of File No. PMTT20-012 shall be final and conclusive upon the approval of File No. PSPA20-006 by the City Council.



**ENGINEERING DEPARTMENT
CONDITIONS OF APPROVAL**

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Broadband Operations & Investment and Revenue Resources Department Conditions incorporated)

<input type="checkbox"/> DEVELOPMENT PLAN <input type="checkbox"/> OTHER	<input type="checkbox"/> PARCEL MAP <input checked="" type="checkbox"/> TRACT MAP <input checked="" type="checkbox"/> FOR CONDOMINIUM PURPOSES
PROJECT FILE NO. TTM-20389 RELATED FILE NO(S). PMTT20-012	
<input checked="" type="checkbox"/> ORIGINAL <input type="checkbox"/> REVISED: __/__/__	

CITY PROJECT ENGINEER & PHONE NO: **Jesus Plasencia, 909-395-2128**


CITY PROJECT PLANNER & PHONE NO: **Jeanie Irene Aguilo, 909-395-2418**


DAB MEETING DATE: **May 17, 2021**

PROJECT NAME / DESCRIPTION: **TM-20389, a Tentative Tract Map to subdivide 5.99 acres of land for Condominium Purposes within the Subarea 29 Specific Plan**

LOCATION: **South side of Merrill west of Haven Avenue**

APPLICANT: **SL Ontario Development Company, LLC**

REVIEWED BY:  5/10/21
 Raymond Lee, P.E. **Date**
 Assistant City Engineer

APPROVED BY:  5-11-21
 Khoi Do, P.E. **Date**
 City Engineer



THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.

1. PRIOR TO FINAL MAP APPROVAL, APPLICANT SHALL:		Check When Complete
<input checked="" type="checkbox"/>	1.01 Dedicate to the City of Ontario in fee simple, the right-of-way, described below: <ul style="list-style-type: none"> • Merrill Avenue to the ultimate ½ right-of-way of 54 feet from CL south along tract frontage. • Lots A-C for landscape Neighborhood Edge and public utility purposes 	<input type="checkbox"/>
<input checked="" type="checkbox"/>	1.02 Dedicate to the City of Ontario, the following easement: <ul style="list-style-type: none"> • Blanket Easement on all private streets for public utility purposes including but not limited to potable water, sewer, and fiber optic improvements. 	<input type="checkbox"/>
<input checked="" type="checkbox"/>	1.03 Restrict vehicular access to the site as follows: Only approved access points per the approved tract map.	<input type="checkbox"/>
<input type="checkbox"/>	1.04 Vacate the following street(s) and/or easement:	<input type="checkbox"/>
<input checked="" type="checkbox"/>	1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.	<input type="checkbox"/>
<input checked="" type="checkbox"/>	1.06 Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.	<input type="checkbox"/>
<input checked="" type="checkbox"/>	1.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at http://tceplumecleanup.com/), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.	<input type="checkbox"/>
<input type="checkbox"/>	1.08 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Financial Services Department at (909) 395-2124 regarding this requirement.	<input type="checkbox"/>
	(1) _____	
	(2) _____	
<input checked="" type="checkbox"/>	1.09 Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.	<input type="checkbox"/>



- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ci.ontario.ca.us) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.
- 1.13 New Model Colony (NMC) Developments:
 - 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
 - 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
 - 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions:
 - Streets "A" and "B" shall be private streets and labeled accordingly in the final map.
 - Public and private improvements shall be maintained in accordance with the Maintenance Responsibility matrix in the Subarea 29 Specific Plan.

2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:

**A. GENERAL
 (Permits includes Grading, Building, Demolition and Encroachment)**

- 2.01 Record Tract Map No. 20389 pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 Submit a PDF of the recorded map to the City Engineer's office.
- 2.03 Note that the subject parcel will be a recognized parcel in the City of Ontario upon recordation of the final map.
- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of _____
- 2.05 Apply for a: Certificate of Compliance with a Record of Survey; Lot Line Adjustment
 - Make a Dedication of Easement.



- 2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.**

- 2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.**

- 2.08 Submit a soils/geology report.**

- 2.09 Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:**
 - State of California Department of Transportation (Caltrans)
 - San Bernardino County Road Department (SBCRD)
 - San Bernardino County Flood Control District (SBCFCD): Approval to construct improvements within Channel Easement**
 - Federal Emergency Management Agency (FEMA)
 - Cucamonga Valley Water District (CVWD) for sewer/water service
 - United States Army Corps of Engineers (USACE)
 - California Department of Fish & Game
 - Inland Empire Utilities Agency (IEUA)
 - Other:**
 - **City of Eastvale: Any encroachment permits needed for the project**
 - **SWRCB/DDW – Recycled Water Usage**
 - **Riverside County Flood Control District: Approval to construct improvements within Channel Easement.**

- 2.10 Dedicate to the City of Ontario the right-of-way described below:**

_____ feet on _____

Property line corner 'cut-back' required at the intersection of _____ and _____.

- 2.11 Dedicate to the City of Ontario the following easement(s):** _____

- 2.12 New Model Colony (NMC) Developments:**
 - 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.
 - 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary



use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.

☒ 3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall.

- 2.13 Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at % of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.
- 2.14 The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.
- 2.15 Pay all Development Impact Fees (DIF) to the Building Department. Storm Drain Development Impact Fee shall be paid to the Building Department. Final fee shall be determined based on the approved site plan.
- 2.16 Other conditions:
 - All the required improvements for this tract shall be subject to completion of the required public improvements including public utilities beyond the tract limits as specified in the Subarea 29 Specific Plan, the Development Agreement, and the Conditions of Approval for Tract Map No. 18913.



B. PUBLIC IMPROVEMENTS
 (See attached Exhibit 'A' for plan check submittal requirements.)

2.17 Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):

Improvement	MERRILL AVENUE	PRIVATE STREETS/ALLEYS
Curb and Gutter	<input checked="" type="checkbox"/> New; 54 ft. from C/L <input checked="" type="checkbox"/> Replace damaged <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
AC Pavement	<input checked="" type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Drive Approach	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Sidewalk	<input checked="" type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
ADA Access Ramp	<input checked="" type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Parkway	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
Raised Landscaped Median	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Fire Hydrant	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation



Sewer (see Sec. 2.C)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral
Water (see Sec. 2.D)	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Service	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input type="checkbox"/> New <input checked="" type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Street Light (see Sec. 2.F)	<input checked="" type="checkbox"/> New / Upgrade <input checked="" type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2G)	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Fiber Optics (see Sec. 2K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
Removal of Improvements	_____	_____
Other Improvements	_____	_____

Specific notes for improvements listed in item no. 2.17, above:

- **Proposed utility connections on Merrill Avenue will be subject to the City's street pavement moratorium policy.**
- **Location of public utility systems in private streets/alleys shall be approved during final design review.**

2.18 Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s): _____

2.19 Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number _____



1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.

- 2.20 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide water service sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.
- 2.21 Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately _____, for undergrounding of utilities in accordance with Section 7-7.303.e of the City's Municipal Code.
- 2.22 Other conditions: _____

C. SEWER

- 2.23 **A 24-inch sewer main is available for connection by this project in Merrill Avenue (Ref: Sewer plan: S15294, S3710, S3711)**
- 2.24 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.25 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.
- 2.26 **Other conditions:**
 - **Construct 8" sanitary sewer main in Private Street "A", Private Street "B", and alleys.**

D. WATER

- 2.27 **A 12-inch water main is available for connection by this project in Merrill Avenue (Ref: Water plan: W13524)**
- 2.28 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.29 **Other conditions:**
 - **Construct 8" domestic water main within Private Street "A" and Private Street "B".**
 - **Final utility design shall comply with all Division of Drinking water separation requirements.**

E. RECYCLED WATER

- 2.30 **A 8-inch recycled water main is available for connection by this project in Merrill Avenue. (Ref: Recycled Water: P10278, 11298)**
- 2.31 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.
- 2.32 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.
- 2.33 **Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.**

Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.



- 2.34 Other conditions:**
- **This development shall comply with City Ordinance 2689 and make use of recycled water for all approved uses, including but not limited to landscaping irrigation for HOA maintained/common areas**

F. TRAFFIC / TRANSPORTATION

- 2.35** Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer:
1. On-site and off-site circulation
 2. Traffic level of service (LOS) at 'build-out' and future years
 3. Impact at specific intersections as selected by the City Engineer
- 2.36** New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.
- 2.37 Other conditions:**
- **The Applicant/Developer shall be responsible to design and construct street improvements along property frontages of Merrill Avenue required to accommodate new tract entry streets. These, and all other street improvements required herein, shall include modifications to, but not be limited to, existing concrete curb and gutter, sidewalk, LED street lights, signing and striping, and parkway landscaping.**
 - **The Applicant/Developer shall be responsible to design and construct the necessary striping modifications on Merrill Avenue to accommodate new tract entry streets. Striping improvements shall include the removal existing signing and striping beyond the project frontage to accommodate the new tract entry streets.**
 - **Proposed entry streets onto Merrill Avenue shall both be allowed as full access entries.**
 - **The Applicant/Developer shall be responsible to design and construct the necessary street lighting modifications on Merrill Avenue to accommodate new tract entry streets. Street light improvements shall include the adjustment of existing conduits, pull boxes and streets lights to accommodate the new tract entry streets.**
 - **Property frontage along Merrill Avenue shall be signed "No Stopping Anytime".**
 - **The Applicant/Developer's engineer-of-record shall meet with City Engineering staff prior to start of signing and striping, and street lighting design in preparation for the plan check stage.**

G. DRAINAGE / HYDROLOGY

- 2.38** **24-inch and 30-inch storm drain mains are available to accept flows from this project in Merrill Avenue. (Ref: Storm Drain plan: D11989, D11990)**
- 2.39** Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.
- 2.40** An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100 year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.
- 2.41** Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.



- 2.42 Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100-year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.
- 2.43 Other conditions: _____

H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)

- 2.44 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels.
If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.
Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.
- 2.45 **Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: <http://www.sbcounty.gov/dpw/land/npdes.asp>.**
- 2.46 Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.
- 2.47 Other conditions: _____

J. SPECIAL DISTRICTS

- 2.48 **File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.**
- 2.49 Other conditions: _____

K. FIBER OPTIC

- 2.50 **Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole.**



- 2.51 Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Broadband Operations Department at (909) 395-2000, regarding this requirement.

L. INTEGRATED WASTE

- 2.52 Onsite solid waste shall be designed in accordance with the City's Refuse & Recycling Planning Manual located at:

<https://www.ontarioca.gov/OMUC/IntegratedWaste>

- 2.53 Other conditions: _____

3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:

- 3.01 Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.

- 3.02 Complete all requirements for recycled water usage.

- 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.

- 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.

- 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.

- 3.03 The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.

- 3.04 NMC Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.

- 3.05 Confirm payment of all Development Impact Fees (DIF) to the Building Department.

- 3.06 Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).



4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:

- 4.01 Complete all Conditions of Approval listed under Sections 1-3 above.**
- 4.02 Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.**
- 4.03 The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall include a completed Acceptance and Bond Release Checklist, state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.**
- 4.04 Submit record drawings (PDF) for all public improvements identified within Section 2 of these Conditions of Approval.**



EXHIBIT 'A'
ENGINEERING DEPARTMENT
First Plan Check Submittal Checklist

Project Number: Tract Map No. 20389

The following items are required to be included with the first plan check submittal:

1. **A copy of this check list**
2. **Payment of fee for Plan Checking**
3. **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4. **One (1) copy of project Conditions of Approval**
5. **Include a PDF (electronic submittal) of each required improvement plan at every submittal.**
6. **Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).**
7. **Three (3) sets of Public Street improvement plan with street cross-sections (Merrill Avenue)**
8. **Three (3) sets of Private Street improvement plan with street cross-sections**
9. **Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)**
10. **Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)**
11. **Four (4) sets of Public Sewer improvement plan**
12. **Five (5) sets of Public Storm Drain improvement plan**
13. **Three (3) sets of Public Street Light improvement plan (Merrill Avenue)**
14. **Three (3) sets of Signing and Striping improvement plan (Merrill Avenue)**
15. **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
16. **Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)**
17. **Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.**
18. **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**
19. **One (1) copy of Hydrology/Drainage study**
20. **One (1) copy of Soils/Geology report**



21. **Payment for Final Map/Parcel Map processing fee**
22. **Three (3) copies of Final Map/Parcel Map**
23. **One (1) copy of approved Tentative Map**
24. **One (1) copy of Preliminary Title Report (current within 30 days)**
25. One (1) copy of Traverse Closure Calculations
26. One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.
27. **Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use**
28. **Other:**
 - **CFD Landscape Plan for the Neighborhood Edge**
 - **Final Utilities System Map (as part of the precise grading plan submittal)**
 - **Final Solid Waste Handling Plan (as part of precise grading plan submittal)**

CITY OF ONTARIO
LANDSCAPE PLANNING DIVISION
 303 East "B" Street, Ontario, CA 91764

CONDITIONS OF APPROVAL	
Sign Off	
	03/31/21
Jamie Richardson, Senior Landscape Planner	Date

Reviewer's Name: Jamie Richardson, Senior Landscape Planner	Phone: (909) 395-2615
---	---------------------------------

D.A.B. File No.: PMTT20-012	Related Files: PSPA20-006	Case Planner: Jeanie Irene Aguilo
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Project Name and Location:
 Subarea 29 PA27
 South of Merrill Ave, East of Celebration

Applicant/Representative:
 SL Ontario Development
 1156 N Mountain Ave
 Upland, CA 91786

<input checked="" type="checkbox"/>	A Tentative Tract Map (dated 3/12/21) has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.
<input type="checkbox"/>	A Tentative Tract Map (dated) has not been approved. Corrections noted below are required prior to DAB approval.

CORRECTIONS REQUIRED

- On Grading or Utility Construction Plans:
- Storm water infiltration devices located in parkways or other landscape areas shall be routed to this department to be reviewed and approved prior to permit approval or installation.
 - Note decorative paving for all motor courts including the lots facing the parking rows aisles.
 - Note for compaction to not be greater than 85% at landscape areas; all finished grades 1 1/2" below finished surfaces; landscaped slopes to be max 3:1.
 - Show infiltrating catch basins with two 3/4" dia. holes in bottom set on 12" square of filter fabric wrapped gravel, located 5' or greater from buildings and 24" from sidewalk, add detail.
 - Show or note transformers shall be located in planter areas, and set back 3' from paving for small transformers less than 4' high and 5' setback for large transformer greater than 4' high. Locate on level grade. Coordinate with landscape plans.
 - Show or note backflow devices shall be located in planter areas, and set back min 3' from paving Locate on level grade. Coordinate with landscape plans.
 - Provide a utility clear space 8' wide in parkways 30' apart for street trees. Move water meters, drain lines, light standards to the minimum spacing to allow space for street trees.
 - Show light standards 15' away from required tree locations.
 - Wall footings shall not restrict landscape; max 12" in front of footing with of 12" of cover.
 - Show on plans step outs at parking spaces adjacent to planters; 12" wide monolithic curb, 12" compacted decomposed granite or pavers adjacent to the 6" curb.
 - Wall openings for drainage overflow shall be max 4" wide.
 - Provide a solid surface path from driveway to side yard gate for entry and trash bin access.
 - AC units shall be located in residential side yards, opposite the main back yard access path with gate, or a second gate and solid surface path on the opposite side added for access.
 - Storm water infiltration devices located in landscape areas shall be reviewed and approved by the Landscape Planning Division prior to installation.
 - Add notes for any tree removal to occur outside of typical nesting season (February 1 through August 31) or per the specific plan EIR mitigation Measures.

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT



Project File No.: PSPA20-006 & PMTT20-012

Address: SEC Merrill Avenue & SCE Easement

APN: 0218-33-42

Existing Land Use: Vacant

Proposed Land Use: Subarea 29 SPA to increase the number of units within PA27 Tentative Tract Map to 5.99 acres into 1 lot for condominium purposes

Site Acreage: 5.99 Proposed Structure Height: N/A

ONT-IAC Project Review: n/a

Airport Influence Area: ONT & Chino

Reviewed By: Lorena Mejia

Contact Info: 909-395-2276

Project Planner: Jeanie Aguilo

Date: 03/29/2021

CD No.: 2021-002

PALU No.: n/a

The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="checkbox"/> High Terrain Zone	<input type="checkbox"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input type="checkbox"/> FAA Notification Surfaces	<input type="checkbox"/> Recorded Overflight Notification
<input type="radio"/> Zone 2	<input type="checkbox"/> 65 - 70 dB CNEL	<input type="checkbox"/> Airspace Obstruction Surfaces	<input checked="" type="checkbox"/> Real Estate Transaction Disclosure
<input type="checkbox"/> Zone 3	<input type="checkbox"/> 60 - 65 dB CNEL	<input type="checkbox"/> Airspace Avigation Easement Area	
<input type="radio"/> Zone 4		Allowable Height: <u>200 FT +</u>	
<input type="radio"/> Zone 5			

The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1
 Zone 2
 Zone 3
 Zone 4
 Zone 5
 Zone 6

Allowable Height: 200 FT +

CONSISTENCY DETERMINATION

This proposed Project is: Exempt from the ALUCP
 Consistent
 Consistent with Conditions
 Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and Chino Airport was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT and Chino Airport.

See Attached Conditions

Airport Planner Signature: _____

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT

CD No.: 2021-002
PALU No.: _____

PROJECT CONDITIONS

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the Airport Land Use Compatibility Plan (ALUCP) for ONT. The applicant is required to meet the Real Estate Transaction Disclosure in accordance with California Codes (Business and Professions Code Section 11010-11024). New residential subdivisions within an Airport Influence Area are required to file an application for a Public Report consisting of a Notice of Intention (NOI) and a completed questionnaire with the Department of Real Estate and include the following language within the NOI:

NOTICE OF AIRPORT IN VICINITY: This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.



CITY OF ONTARIO

MEMORANDUM

TO: Jeanie Irene Aguilo, Associate Planner
Planning Department

FROM: Mike Gerken, Deputy Fire Chief/Fire Marshal
Fire Department

DATE: January 14, 2021

SUBJECT: PMTT20-012 - A Tentative Tract Map (TTM 20389) to subdivide 5.99 acres of land into one numbered lot and 3 lettered lots for condominium purposes, located south of and adjacent to Merrill Avenue, approximately 1,000 feet east of Celebration Avenue, within the PA 27 of the Subarea 29 Specific Plan (APN: 0218-331-42). Related File: PSPA20-006.

-
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.
-

SITE AND BUILDING FEATURES:

- A. 2019 CBC Type of Construction: Type V-B wood frame
- B. Type of Roof Materials: Non-Rated
- C. Ground Floor Area(s): Various
- D. Number of Stories: Two Story
- E. Total Square Footage: Various
- F. 2019 CBC Occupancy Classification(s): R-3, U

CONDITIONS OF APPROVAL:

1.0 GENERAL

- ☒ 1.1 The following are the Ontario Fire Department (“Fire Department”) requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards (“Standards.”) It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario website at www.ontarioca.gov/Fire/Prevention.
- ☒ 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

2.0 FIRE DEPARTMENT ACCESS

- ☒ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- ☒ 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25’) inside and forty-five feet (45’) outside turning radius per Standard #B-005.
- ☒ 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150’) in length shall have an approved turn-around per Standard #B-002.
- ☒ 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-four (24) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by fire department and other emergency services.

3.0 WATER SUPPLY

- ☒ 3.1 The required fire flow per Fire Department standards, based on the 2019 California Fire Code, Appendix B, is 1500 gallons per minute (g.p.m.) for 2 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.
- ☒ 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300’) apart, per Engineering Department specifications.
- ☒ 3.4 The public water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

4.0 FIRE PROTECTION SYSTEMS

- ☒ 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 13 D. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.

5.0 BUILDING CONSTRUCTION FEATURES

- ☒ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ☒ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Homes that do not front street shall be provided with an address entry sign at the street. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- ☒ 5.3 Single station smoke alarms and carbon monoxide alarms are required to be installed per the California Building Code and the California Fire Code.
- ☒ 5.5 All residential chimneys shall be equipped with an approved spark arrester meeting the requirements of the California Building Code.

Jeanie Irene T. Aguilo

From: WLee@ontariopolice.org
Sent: Tuesday, February 16, 2021 10:33 AM
To: Jeanie Irene T. Aguilo
Subject: The Police Review workflow task has been given the status of Approved on Planning Application: PMTT20-012

The Police Review workflow task has been given the status of Approved on Planning Application: PMTT20-012

FILE NOS: PDEV18-022 and PCUP18-021

SUBJECT: A Development Plan (File No. PDEV18-022) to construct a 6,870 square-foot industrial building in conjunction with a Conditional Use Permit (File No. PCUP18-021) to establish and operate a towing service and short-term storage of automobiles, vans, light trucks, and tractor trucks on 3.1 acres of land located at 580 East Belmont Street, within the IL (Light Industrial) zoning district (APNs: 1049-491-01, 1049-491-02 and 1049-491-03); **submitted by Four Sisters Enterprises LLC.**


PROPERTY OWNER: Four Sisters Enterprises, LLC

RECOMMENDED ACTION: That the Planning Commission approve File Nos. PCUP18-021 and PDEV18-022, pursuant to the facts and reasons contained in the staff report and attached resolutions, and subject to the conditions of approval contained in the attached departmental reports.

PROJECT SETTING: The Project site, depicted in Figure 1: Project Location Map, below, is comprised of 3.1 acres of land generally bordered by Belmont Street to the north, Monterey Avenue to the east, Phillips Street to the south, and Sultana Avenue to the south. The site is located within the IL (Light Industrial) zoning district and is currently undeveloped. The existing surrounding land uses, zoning, and general plan and specific plan land use designations are summarized in the "Surrounding Zoning & Land Uses" table located in the Technical Appendix of this report.



Figure 1: Project Location Map

Case Planner:	Denny D. Chen
Planning Director Approval:	
Submittal Date:	6/21/2018

Hearing Body	Date	Decision	Action
DAB	5/18/2020	Approval	Recommend
PC	5/25/2021		Final
CC			

The Project site is surrounded by industrial land uses to the north and residential land uses to the east, south and west. The Ontario Plan Policy Plan (General Plan) land use designations surrounding the site are Industrial to the north, Low Density Residential/Industrial Overlay to the east, Low Density Residential to the south, and Low Medium Density Residential to the west, as depicted in Figure 2 (Policy Plan Land Use Map) below.

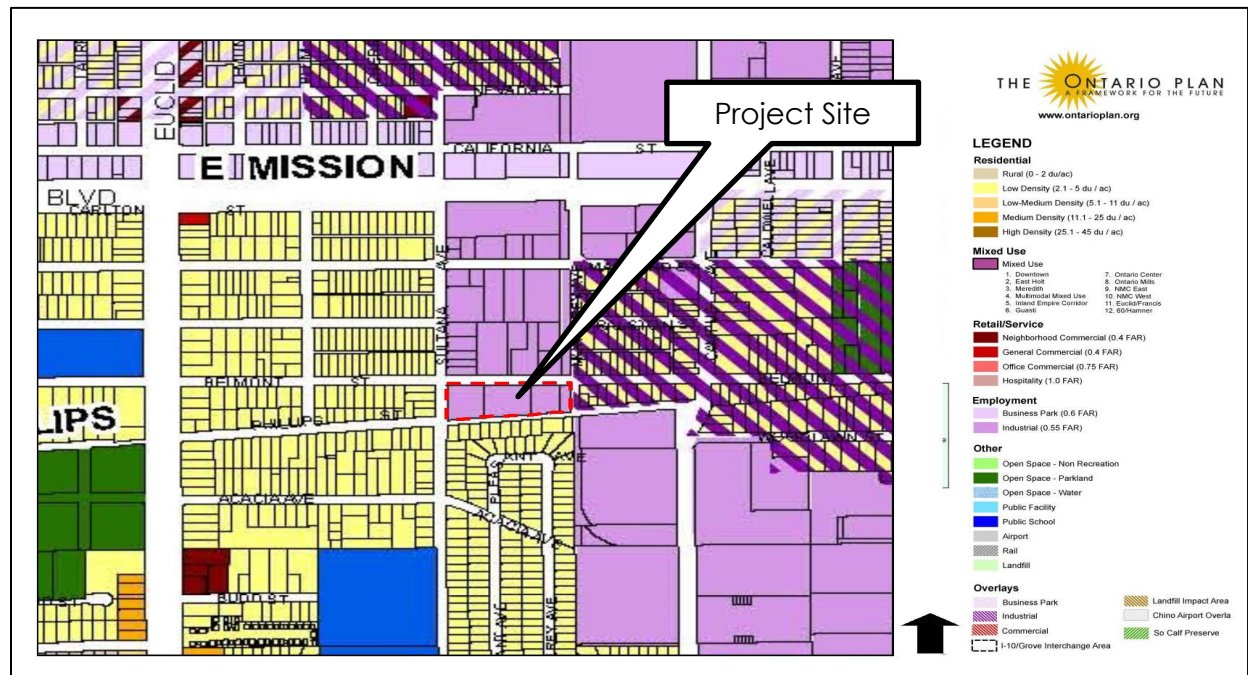


Figure 2: Policy Plan Land Use Map

PROJECT ANALYSIS:

(1) Background — On June 21, 2018, the Applicant submitted a Development Plan application requesting approval to construct a 6,870 square foot industrial building having a floor area ratio (FAR) of 0.05, in conjunction with a Conditional Use Permit (File No. PCUP18-021) to establish and operate a towing service and motor vehicle impound storage yard on the Project site.

On May 18, 2020, the Development Advisory Board reviewed the subject applications and recommended that the Planning Commission approve the proposed Project subject to the departmental conditions of approval.

On May 26, 2020, the Planning Commission approved the proposed Project, subject to conditions of approval. The Project was subsequently appealed to the City Council on June 5, 2020.

On July 7, 2020, the City Council conducted a duly noticed public hearing to consider the appeal. Following testimony from the Appellant, the Project Applicant, and members of the public, the City Council deliberated and voted to refer the Development Plan and Conditional Use Permit applicates back to the Planning Commission to provide the Commission the opportunity to review new Project information presented by the Applicant, regarding the operation of the towing facility. Following the City Council public hearing, the Applicant submitted a modified Project description, including a Health Risk Assessment ("HRA") and updated Noise Assessment addressing proposed revisions in tow facility operations. There are no changes proposed to the previously approved Development Plan; however, for reference, Sections 2 of this staff report discusses the proposed development of the Project site.

On March 30, 2021, the Planning Department hosted a virtual Zoom community meeting for the Pepe's Towing Project. A summary of the meeting is contained within the "Community Outreach" section of this report.

(2) Development Plan (File No. PDEV18-022)

(a) Site Design/Building Layout — The proposed building is located at the southeast corner of the Project site, fronting on to Monterey Avenue (to the east) and Phillips Street (to the south). The building is setback 48 feet from the Belmont Street (north) property line, 10 feet from the east property line (Southern Pacific Transportation Company railroad right-of-way), 10 feet from Phillips Street (south) property line, and approximately 540 feet from Sultana Avenue (west) property line. The building's office and main entrance is situated at the northeast corner of the building, fronting onto Belmont Street. The employee and visitor parking areas are located north of the building, at the northeast corner of the Project site, and are secured by an 8-foot tall decorative tube steel fence.

The motor vehicle impound yard is proposed on the westerly two-thirds of the Project site, covering 79,569 square feet designed to park 324 impounded vehicles. The yard area will be screened from public view by a decorative 8-foot tall wall consisting of split-face masonry block with decorative pilasters and cap, located along the north, west, and south sides of the Project site.

An 800 square foot (50 feet long by 16 feet wide) outdoor truck wash area and an above ground 1,000-gallon fuel tank are also proposed along the south side of the Project site, near the proposed building (see Exhibit C—Site Plan, attached).

(b) Site Access/Circulation — Primary vehicle access is provided by two driveways located along Belmont Street, immediately west of Monterey Avenue. A secondary emergency vehicle access point is provided from Sultana Avenue, mid-block between Belmont and Phillips Streets (see Exhibit B—Site Plan, attached).

(c) Parking — The Project is required to provide a minimum of 16 off-street parking spaces and a total of 20 off-street parking spaces have been provided, exceeding the minimum number of required parking spaces. Parking was calculated as shown in Table 1 – Required Off-Street Parking, below.

Table 1 – Required Off-Street Parking			
Proposed Use	Area	Parking Ratio (per Development Code Table 6.03-1)	Required Parking
Office:	1,056	4 spaces per 1,000 SF of Building GFA exceeding 10% of building GFA (approximately 475 SF).	2
Outdoor Vehicle Storage:	79,569	0.1 space per 1,000 GFA.	8
Warehouse:	5,814	One space per 1,000 SF for first 20,000 GFA and 0.5 per 1,000 SF for building GFA greater than 20,000 SF; plus 1 tractor-trailer parking space per 4 dock-high loading doors; plus, required parking for “general business offices” when those uses exceed 10% of building GFA.	6
Total Building Area:	6,870		16
Total Parking Required:			16
Total Parking Provided:			20

(d) Architecture — The building will be designed in modern style and composed of color coated smooth-faced standing seam metal wall panels that incorporate a contemporary architectural style (see Figure 3: Building Perspective, below, and Exhibit E—Building Elevations and Exhibits F thru F3 – Building Perspectives, attached). The building design incorporates articulation in the building footprint, incorporating a combination of recessed and popped-out wall areas, and articulation in the building parapet/roof line, which serves to accentuate the building's roofline. To accent the building design, the following materials, finishes, and fixtures have been incorporated into the building design:

- Vertical changes in building height that correspond to horizontal changes in the building footprint;
- Smooth-faced, pre-painted seamed metal panels;
- Decorative metal awnings;
- Double-glazed windows with a clear anodized mullion system;
- Decorative split-face block at the base of the building; and
- Decorative cornices on all four sides of the building.



Figure 3 – Building Perspective

(e) Landscaping — The Project proposes 15 percent landscape coverage, which meets the minimum landscape coverage required by the Development Code for corner lots located within the IL (Light Industrial) zoning district. The landscape pallet for the Project incorporates a mixture of 24-inch, 36-inch, and 48-inch box accent and shade trees. Some of the proposed trees include Raywood Ash, Western Redbud, Crape Myrtle, Purple Leaf Plum, Coast Live Oak, along with a variety of low water use grasses, shrubs, and groundcovers.

Additionally, the Project has provided the required 10-foot landscaped setback areas along all Project street frontages, including Belmont Street, Sultana Avenue, Phillips Street, and Monterey Avenue/Southern Pacific Railroad (see Exhibit H—Landscape Plan, attached).

(f) Signage — The Applicant is proposing two monument signs along the perimeter of the Project site. All Project signage shall be reviewed and approved by the Planning Department and shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations).

(g) Utilities (drainage, sewer) — Public utilities (water and sewer) are available to serve the Project. Additionally, the Applicant has submitted a Preliminary Water Quality Management Plan (PWQMP), which establishes the Project's compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizing low impact development (LID) best management practices (BMPs), such as retention and infiltration, biotreatment, and evapotranspiration. The PWQMP proposes a vegetated swale designed to accept runoff from building roofs, parking lots and Project roadways, which lead to an underground stormwater infiltration system. The proposed underground infiltration system is located along the southeast portion of the site and any overflow drainage will be conveyed to the surrounding streets.

(3) Conditional Use Permit (File No. PCUP18-021) — Pursuant to the Ontario Development Code, Conditional Use Permit ("CUP") approval is required to establish an outdoor automobile storage yard (tow yard) within the IL (Light Industrial) zoning district. A CUP is required in order to adequately review the potential impacts the use might have on the surrounding properties and eliminate or mitigate those impacts. As previously mentioned in the "Background" section of this report, on July 7, 2020, the City Council held a public hearing to hear the appeal of the proposed CUP and recommended that the be Project referred back to the Planning Commission to provide the Commission the opportunity to review new Project information presented by the Applicant regarding the operation of the towing and short-term vehicle storage facility. The new information included the clarification in the overall tow operation, hours of operation, and the types of vehicles that would be towed to the facility, which are discussed below.

Pepe's Towing currently operates another towing facility within the City, located at 810 East Main Street. The towing facility was approved with a Conditional Use Permit (File No. PCUP17-016) on February 21, 2018, by the Zoning Administrator. The Applicant is requesting to establish and operate an additional vehicle towing facility and motor vehicle impound storage yard on the Project site. The proposed towing facility and vehicle impound storage yard, consists of a 79,569 square foot outdoor vehicle storage area that will accommodate up to 324 motor vehicles, including towing services and short-term storage of automobiles, vans, light trucks, and tractor trucks.

The towing service operation will also include a 6,870 square foot, two-story industrial building, and an 800 square foot outdoor truck wash area. The industrial building is comprised of an office area, which includes a dispatch office, warehouse/storage area, and a vehicle evidence room. The evidence room will serve as a staging and storage area for nonoperational vehicles that were involved in serious accidents that need to be secured for Police Department inspections (see Exhibit D—Floor Plans, attached).

Vehicles not involved in serious accidents will be kept within the outdoor storage yard area, which is screened by a decorative 8-foot tall masonry block wall. The towing service operation will have three full-time employees (dispatchers), one clerk, and two yardmen.

Normal business office hours are from 8:00 a.m. to 5:00 p.m., Monday thru Friday. After hours customer services may be arranged on an appointment-only basis.

Pepe's Towing provides 24-hour roadside towing services, every day of the week, along with tire changes, fuel delivery, lock-out, and police storage services. Vehicles from Ontario Police Department, Chino Hills Sheriff Department, Rancho Cucamonga California Highway Patrol (CHP), as well as vehicles towed within a 10-mile radius of the Project site, will be towed to the proposed location.

Towed vehicles will have access to the facility on a 24 hour basis, every day of the week. Three tow vehicles will be used by the business, including flatbed and wheel-lift trucks to accommodate automobiles, vans, and light trucks, and a heavy-duty truck to accommodate tractor trucks (see Exhibit G—Tow Vehicles Used by Pepe's Towing, attached).

(4) Community Outreach — The Project site is surrounded by residential properties to the east, south, and west. To ensure community input from the surrounding residents, a Neighborhood Meeting Notice was mailed on March 18, 2021, to all residential property owners and residents extended beyond 500-feet of the Project site, in both English and Spanish. The intent of the notification was to inform the surrounding residents and property owners of the proposed Project and to address any questions and/or concerns they may have regarding the Project.

On March 30, 2021, the Planning Department hosted a virtual Zoom community meeting for the Pepe's Towing Project. The purpose of the community meeting was to present the community with the proposed Project and all new information provided by the Applicant and to gather the community's input and comments. The community meeting was held in both English and Spanish.

In order to help residents that did not have access to a computer, the Dorothy Quesada Community Center was provided as a location to view and participate in the virtual Zoom meeting. Planning Department staff were on hand to provide Spanish translation assistance and ensure that proper social distancing measures were followed. A total of 10 people attended the meeting at the Quesada Community Center and 18 people logged on to the virtual Zoom meeting. Below is a list of issues and concerns raised at the meeting by the surrounding community.

- a) Safety issues for children walking to and from nearby schools.

In response to community comments/concerns, the Project site has been designed to minimize impacts to surrounding residents by orienting Project access to the north, away from surrounding residential land uses. The Project will only be accessed from Belmont Street. No access will be taken from Sultana Avenue (excepting emergency vehicle access) or Phillips Street. The Project has been conditioned to require that all tow trucks entering and exiting the site use Campus Avenue, by way of Belmont Street. Tow trucks

will not be allowed to use Sultana Avenue or Phillips Street to access the Project site. The Project will also be required to fully widen and improve Phillips Street and construct and improve Sultana Avenue, Phillips Street and Belmont Street, to include curbs, gutters, sidewalks, parkway landscaping (street trees, shrubs, and groundcover), and streetlights.

b) Reduction of property values due to the proposed Project.

Staff cannot address the impacts of property values as a result of the proposed use. The project site is zoned for industrial uses and therefore appraisals of nearby residential properties will take into account industrial land use designation of the project site and the potential uses. The land uses within the Project site's vicinity consist of a combination of industrial, residential, and nonconforming residential land uses. The residential properties located within the vicinity of the Project site are presently impacted by aircraft noise from Ontario International Airport. The Project site is zoned Light Industrial, and the surrounding area is located within the 65-70 dB CNEL noise impact zone of Ontario International Airport. The properties to the east of the Project site are located within the Policy Plan's Industrial Overlay Zone, which is an area that is affected by noise and safety impacts from Ontario International Airport. The Industrial Overlay allows residential uses to transition to an industrial land use if an entire block can be assembled in order to remove sensitive land uses within areas of the City most severely impacted by Ontario International Airport. Over the past 30 years, the City has worked with other governmental agencies and has applied for grants to purchase residential properties, relocate households, transition land uses to be compatible with the Airport, and insulate homes to reduce interior noise levels within high noise impact zones.

c) Safety concerns with the proposed 1,000 gallon above ground fuel tank in relation to the recent firework explosion in one of our residential communities.

The Project proposes the installation of a 1,000 gallon above-ground fueling tank. The fuel tank is proposed to be located at southeast area of the site, just to the west of the office/warehouse building, and setback 15 feet from the south property line along Phillips Street and more than 100 feet from the nearest residential home on the south side of Phillips Street. The fuel tank will be screened and protected from Phillips Street by an 8-foot high split-face masonry block wall. The Fire Department reviewed the Project and recommended approval subject to conditions of approval. The installation of the fuel tank will be required to be reviewed, approved, and permitted by the Fire Department, Ontario Building Department, and the San Bernardino County Fire Department Hazardous Materials Division. An exterior emergency pump shut-off switch will be required to be installed. In addition, the fuel tank will be required to obtain all necessary Air Quality Management District (AQMD) permits prior to its operation. However, staff understands the residential safety concerns with the fuel tank location and therefore has placed a condition of approval on project, that will require the fuel tank to be relocated to the northeast area of the site to a more appropriate location across from the existing industrial development.

d) Increased traffic impacts in the surrounding area and safety concerns at the Phillips and Belmont Street railroad crossing.

The subject site is presently vacant and any new development will generate new vehicle trips over what is existing. The Engineering Department's Traffic Division has reviewed the Project for access and circulation and does not anticipate traffic or noise to increase beyond the traffic and noise that currently exists on Sultana Avenue, Belmont Street, Phillips Street, and the surrounding areas.

The existing Pepe's Towing yard, located at 810 East Main Street, averages approximately two tow service calls per day and 8 to 12 calls per day when on a 24-hour rotation. In addition, the tow yard averages about two customers per day. For context, a typical single-family home averages 9.4 vehicle trips per day. On August 28, 2007, the Planning Commission approved a development plan (File No. PDEV06-035) to construct a 16,738 industrial warehouse building on the west parcel of the Project site. In comparison, that industrial building would have generated approximately 29 vehicle trips per day (based on the Institute of Transportation (ITE) Trip Generation Manual of 1.74 vehicle trips per 1,000 SF of warehouse building).

The Project will also be required to widen Phillips Street and construct and improve the sidewalk and landscape parkways along Sultana Avenue, Phillips Street, and Belmont Street. In addition, the Project will only be accessed from Belmont Street. No access will be taken from Sultana Avenue (excepting emergency vehicle access) and Phillips Street. The Project has been conditioned to require that all tow trucks entering and exiting the site use Campus Avenue by way of Belmont Street. Tow trucks will not be allowed to use Sultana Avenue or Phillips Street.

e) The Health Risk Assessment ("HRA") may have been biased in favor of Pepe's Towing and it was requested that another HRA be prepared by another consultant in order to validate the current HRA findings.

The HRA was prepared by the Ganddini Group (dated February 15, 2021) and analyzed the potential impacts from the proposed Tow Yard Facility and concluded that the proposed Project will not impose significant health impacts on the surrounding environment. The Ganddini Group is a transportation and environmental consulting firm that utilized federal and State regulatory standards for analyzing the potential impacts from the proposed Project.

f) Relocate the towing and vehicle storage facility within an industrial area somewhere else in the City.

City records show that the Project site has been zoned for industrial uses since 1947. Vehicle storage yards and/or a towing facility is a conditionally permitted use within the Light Industrial zoning district. The Conditional Use Permit review process is intended to ensure the compatibility of adjacent uses by identifying potential nuisance activities and

establishes measures to mitigate those nuisance activities to an acceptable level. In response to community comments/concerns, the Project site has been designed to minimize impacts to surrounding residents by orienting Project access to the north, away from surrounding residential land uses. No access will be taken from Sultana Avenue (excepting emergency vehicle access) and Phillips Street. Also, the Project has been conditioned to require that all tow trucks entering and exiting the site use Campus Avenue by way of Belmont Street. Tow trucks will not be allowed to use Sultana Avenue or Phillips Street.

- g) Develop the subject site as green space or affordable housing.

City records show that the Project site has been zoned industrial since 1947. The Policy Plan (General Plan) land use designations surrounding the site are Industrial to the north, Low Density Residential/Industrial Overlay to the east, Low Density Residential to the south, and Low Medium Density Residential to the west. The Project site zoned IL (Light Industrial). The IL zoning district was established to accommodate lighter manufacturing and assembly activities, storage and warehousing activities, and other similar uses developed at a maximum intensity of 0.55 FAR. This zoning district is typically located within 500 feet of residentially zoned properties, public parks and schools, and mixed-use properties having a residential component. The IL district is intended to serve as a buffer between residentially zoned areas and heavier industrial zoning districts. Allowed uses are expected to have little or no impacts on nearby residential uses with regard to noise, odor, or hazards. The IL zoning district is consistent with, and implements, the Industrial land use designations of the Policy Plan component of The Ontario Plan. Also, the Project site is located within the Light Industrial zoning district and the surrounding area is located within the 65-70 dB CNEL noise impact zone of Ontario International Airport. Establishing new residential land uses is not a compatible or allowable land use.

(5) Health Risk Assessment — The Applicant was required to prepare a Health Risk Assessment ("HRA") to determine whether the proposed Project would pose a health risk to the existing residential land uses. The HRA prepared by the Ganddini Group (dated February 15, 2021) analyzed the potential impacts from the proposed Tow Yard Facility and concluded that the proposed Project will not impose significant health impacts on the surrounding environment (see Attachment A: Health Risk Assessment). The HRA analyzed the Project's cancer burden estimates as well as the operational Toxic Air Contaminants ("TACs") impacts from Diesel Particulate Matter ("DPM") emissions. Both analyses of the impacts concluded that these factors would be less than significant; therefore, no mitigation is required for the Project beyond that which was previously analyzed in The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), as certified by the Ontario City Council on January 27, 2010. Furthermore, the Project was designed to minimize any potential impacts to existing residential development. The Project entrances are oriented away from the existing residential uses and primary access will be taken from Belmont Street.

(6) Noise Assessment — The Applicant submitted an updated Noise Assessment prepared by Rincon Consultants (dated July 6, 2020), to include noise generated during typical tow-related activities. The initial Noise Assessment (Rincon Consultants, dated April 8, 2020) only analyzed noise levels from the proposed truck wash facility. The Noise Assessment concluded that the proposed tow operations and truck wash facility will not generate noise levels beyond the City's allowed noise levels for the adjacent residential zones to the west, south, and east of the Project site. Per the City's Noise Ordinance (Ontario Municipal Code Title 5, Chapter 29 (Noise), commencing with Section 5-29.01), the maximum exterior noise level allowed within residential zoning districts is 65 decibels (dBA), between the hours of 7:00 a.m. to 10:00 p.m. According to the Noise Assessment, the exterior noise level generated by the truck wash area will not exceed 59 dBA, the daytime tow operations would not exceed 55 dBA and the nighttime tow operations would not exceed 44 dBA. In addition, the Project site and the surrounding residential area are located within the 65-70 dB CNEL noise impact zone of Ontario International Airport and currently exposed to daily noise levels above 65 dBA. Therefore, the proposed outdoor truck washing and tow activities will comply with the City's maximum allowed exterior noise level for residential zoning districts and are not anticipated to adversely affect the adjacent residents (see Attachment B – Noise Study).

COMPLIANCE WITH THE ONTARIO PLAN: The proposed Project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed Project are as follows:

(1) City Council Goals.

- Invest in the Growth and Evolution of the City's Economy
- Operate in a Businesslike Manner
- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)

(2) Vision.

Distinctive Development:

- Commercial and Residential Development
 - Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

(3) Governance.

Decision Making:

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.

- G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision.

(4) Policy Plan (General Plan).

Land Use Element:

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.

- LU1-6 Complete Community: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).

- Goal LU2: Compatibility between a wide range of uses.

- LU2-6: Infrastructure Compatibility: We require infrastructure to be aesthetically pleasing and in context with the community character.

Community Economics Element:

- Goal CE1: A complete community that provides for all incomes and stages of life.

- Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.

- CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.

- CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.

- CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Safety Element:

- Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.
 - S1-1 Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

Community Design Element:

- Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.
 - CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.
 - CD1-3 Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.
- Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.
 - CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:
 - Building volume, massing, and height to provide appropriate scale and proportion;
 - A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
 - Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.
 - CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.
 - CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways,

corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

➤ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➤ CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.

➤ CD2-12 Site and Building Signage. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.

➤ CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

▪ Goal CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

➤ CD3-1 Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.

➤ CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.

➤ CD3-3 Building Entrances. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces.

➤ CD3-5 Paving. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.

➤ CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.

- Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.

- CD5-1 Maintenance of Buildings and Property. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.

- CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

HOUSING ELEMENT COMPLIANCE: The Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the Project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed Project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

ENVIRONMENTAL REVIEW: The Project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines and meets each of the following conditions: [1] the Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations; [2] the proposed development occurs within city limits, on a Project site of no more than five acres, and is substantially surrounded by urban uses; [3] the Project site has no value as habitat for endangered, rare, or threatened species; [4] approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and [5] the Project site can be adequately served by all required utilities and public services.

CONDITIONS OF APPROVAL: See attached department reports.

TECHNICAL APPENDIX

Surrounding Zoning and Land Use:

	Existing Land Use	General Plan Designation	Zoning Designation	Specific Plan Land Use
Site	Vacant Land	Industrial	IL (Light Industrial)	N/A
North	Metal Rebar Storage Yard	Industrial	IL (Light Industrial)	N/A
South	Single-Family Residential Homes	Low Density Residential	LDR5 (Low Density Residential – 2.1 to 5.0 DUs/Acre)	N/A
East	Railroad & Single-Family Residential Homes	Low Density Residential/Industrial Overlay	LDR5 (Low Density Residential – 2.1 to 5.0 DUs/Acre)	N/A
West	Single-Family Residential Homes	Low Medium Density Residential	MDR11 (Medium Density Residential – 5.1 to 11 DUs/Acre)	N/A

General Site & Building Statistics

Item	Proposed	Min./Max. Standard	Meets Y/N
Project Area:	3.14 Ac (136,778 SF)	10,000 SF (Min.)	Y
Building Area	6,870 SF	75,228 SF (Max.)	Y
Floor Area Ratio (FAR)	0.05	0.55 (Max.)	Y
Building Height:	34 FT	55 FT (Max.)	Y

Off-Street Parking:

Type of Use	Building Area	Parking Ratio	Spaces Required	Spaces Provided
Warehouse Building	5,814 SF	1 per 1,000 for first 20,000 GFA and 0.5 per 1,000 for building GFA greater than 20,000; plus, required parking for "general business offices" when those uses exceed 10% of building GFA (Gross Floor Area).	6	7
Office	1,056 SF	4 spaces per 1,000 of building GFA	2	13
Outdoor Vehicle Storage Area	79,569 SF	0.1 space per 1,000 SF of GFA	8	324
TOTAL Building Area	6,870 SF		16	20

Exhibit A—AERIAL VIEW

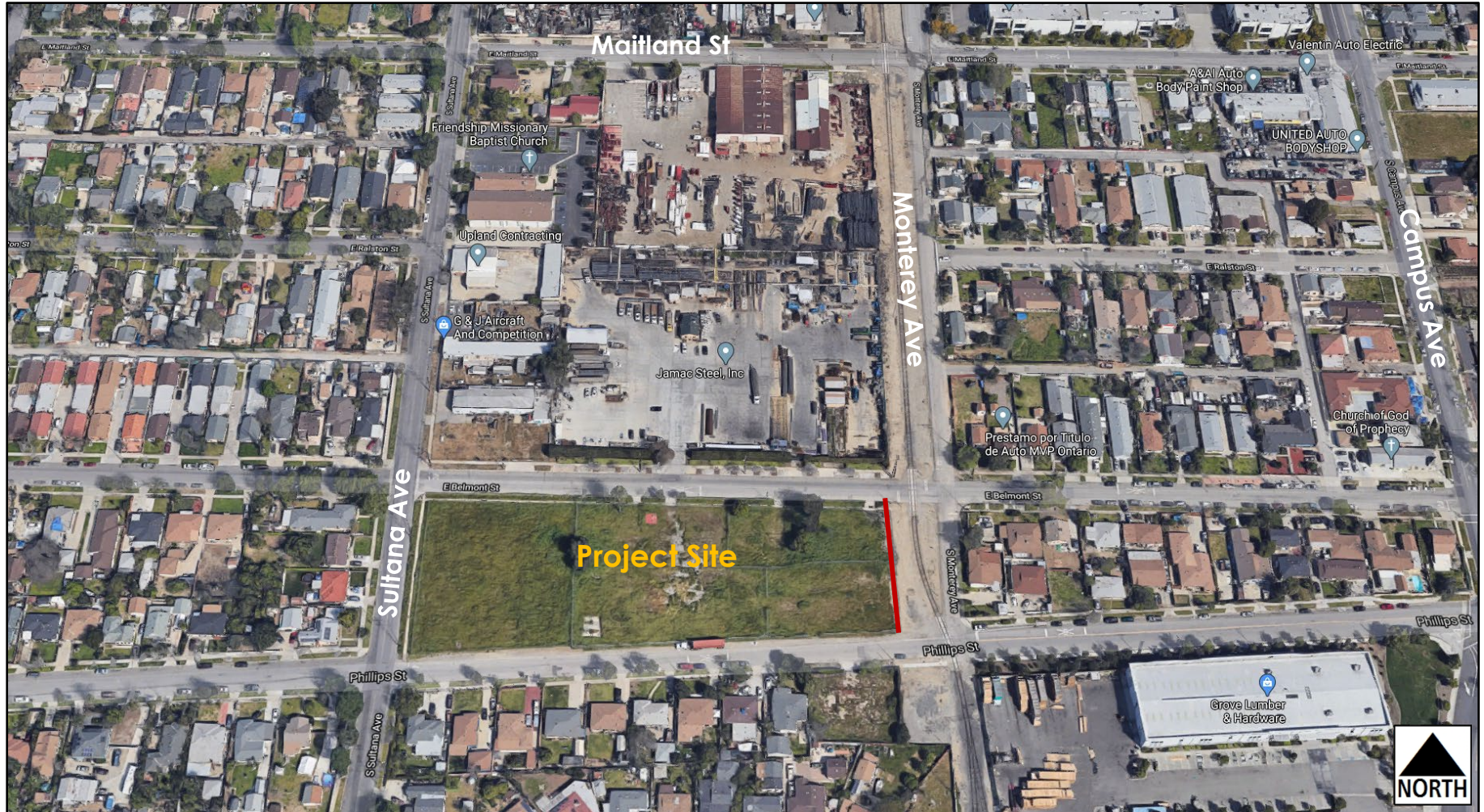


Exhibit B—SITE PHOTOGRAPHS



Southeast Corner View of Project Site (Phillips St & Railroad Tracks)



Northeast Corner of Project Site (Railroad Easement and Belmont Street)

Exhibit B1—SITE PHOTOGRAPHS



Northwest Corner View of Project Site (Belmont Street and Sultana Avenue)



Southwest Corner View of Project Site (Sultana Ave & Phillips St)

Exhibit C—SITE PLAN

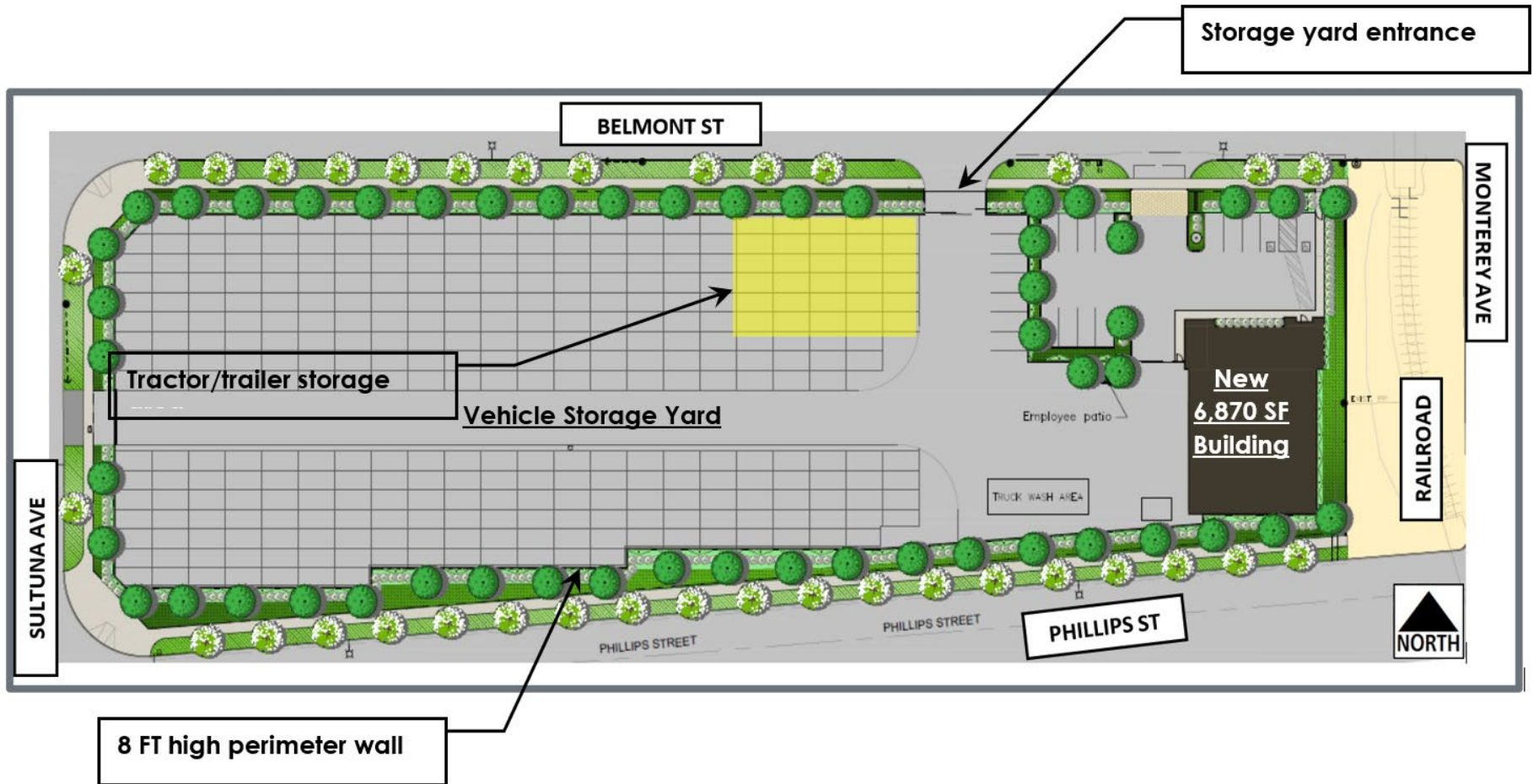


Exhibit D—BUILDING FLOOR PLANS

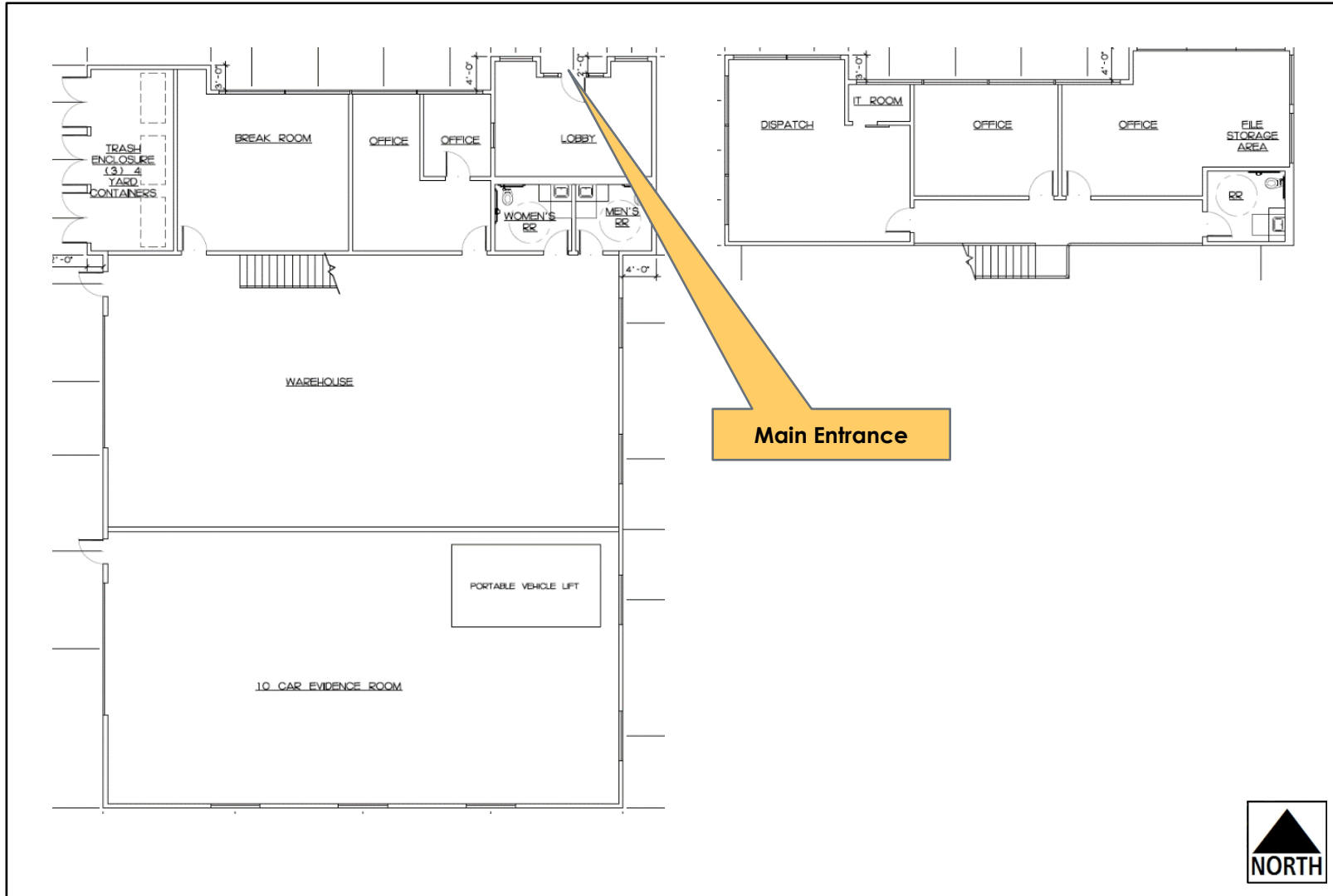
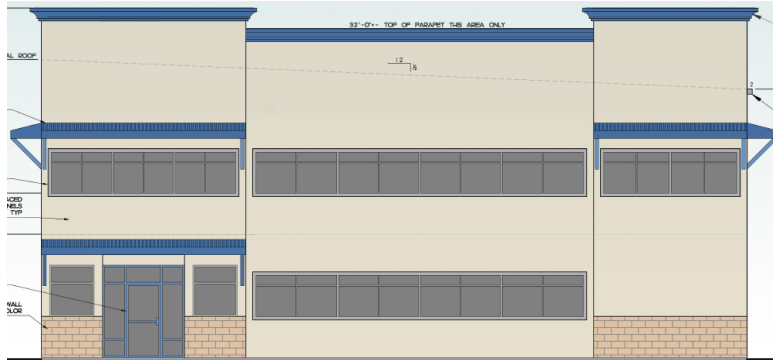


Exhibit E—BUILDING ELEVATIONS



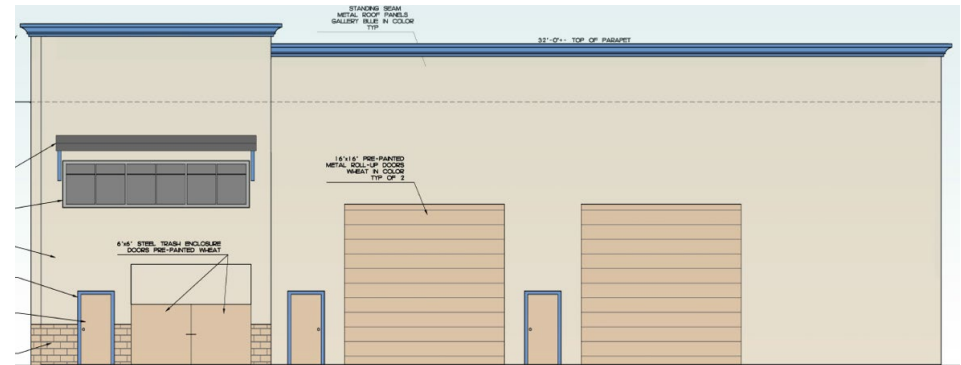
North (Belmont Street) Elevation



East Side Elevation



South (Rear) Elevation



West Elevation

Exhibit F—BUILDING PERSPECTIVES



View from Belmont Street

Exhibit F1—BUILDING PERSPECTIVES



View from Belmont/Monterey Intersection

Exhibit F2—BUILDING PERSPECTIVES



View from Monterey/Phillips Intersection

Exhibit F3—BUILDING PERSPECTIVES



View from Sultana/Belmont Intersection

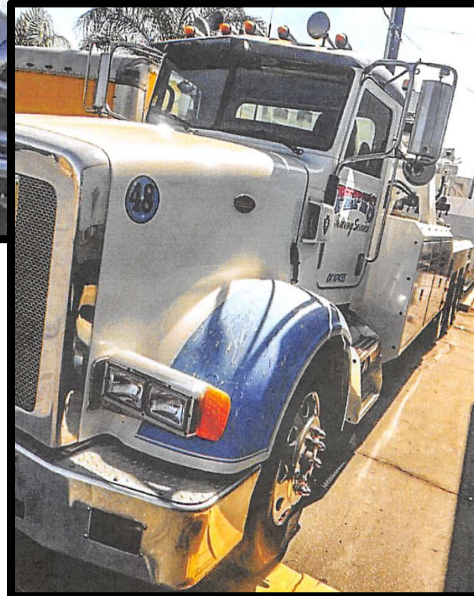
Exhibit G—TOW VEHICLES USED BY PEPE'S TOWING



Light Duty Flatbed Truck



Light Duty Wheel Lift Truck



Heavy Duty Truck

ATTACHMENT A:
HEALTH RISK ASSESSMENT

(Document follows this page)

500 BELMONT STREET TOW YARD HEALTH RISK ASSESSMENT ANALYSIS

City of Ontario

February 15, 2021



Traffic Engineering • Transportation Planning • Parking • Noise & Vibration
Air Quality • Global Climate Change • Health Risk Assessment

500 BELMONT STREET TOW YARD HEALTH RISK ASSESSMENT ANALYSIS

City of Ontario

February 15, 2021

prepared by
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Katie Wilson, MS



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Project No. 19350

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EXECUTIVE SUMMARY

The purpose of this health risk assessment analysis is to provide an assessment of the impacts resulting from the operation of the proposed 500 Belmont Street Tow Yard project and to identify measures that may be necessary to reduce potentially significant impacts.

Cancer and Non-Cancer-Related Health Risk Impacts

The analysis contained in this report shows that the existing sensitive receptors, within the vicinity of the proposed 500 Belmont Street Tow Yard project, would not be exposed to a cancer risk in excess of 10 in a million from operation of the project. Impacts are considered to be less than significant.

The operational health risk impacts for non-cancer related impacts are less than 1.0; therefore, they are also considered to be less significant. No mitigation is required.

1. INTRODUCTION AND SETTING

This section describes the purpose of this health risk assessment, project location, proposed development, and study area. Figure 1 shows the project location map and Figure 2 illustrates the project site plan.

PURPOSE AND OBJECTIVES

This study was performed to address the possibility of cancer and non-cancer risk from project related diesel emissions. The objectives of the study include:

- discussion of the cancer risk thresholds of significance
- analysis of the operations related cancer risk from diesel emissions
- recommendations for mitigation measures

The City of Ontario is the lead agency for this health risk assessment, in accordance with the California Environmental Quality Act authorizing legislation. Although this is a technical report, every effort has been made to write the report clearly and concisely. To assist the reader with terms unique to air quality, a definition of terms has been provided in Appendix A.

PROJECT LOCATION

The approximately 3.01-acre project site is located at 550 East Belmont Street in the City of Ontario, California. The project site is currently vacant. A vicinity map showing the project location is provided on Figure 1.

PROJECT DESCRIPTION

The proposed project involves development of the project site with a 6,600 square foot industrial building and towing yard facility for the relocation of an existing towing storage yard currently operating on an unimproved lot at 810 East Main Street in the City of Ontario. The proposed towing facility is anticipated to continue comparably to the existing towing storage yard in terms of impounds. The proposed project will operate 24/7 with three dispatch staff working three 8-hour shifts per day and three office/yard staff working Monday through Friday from 8:00 AM to 5:00 PM. Vehicular access to the site is proposed by a new full access 27-foot driveway on Belmont Street, a gated restricted access 30-foot driveway on Belmont Street, and an emergency access 25-foot driveway on Sultana Avenue. Figure 2 illustrates the proposed site plan.

According to the SCAQMD's MATES-IV study, the project area has an estimated, ambient cancer risk of 1,144.96 in one million. In comparison, the average cancer risk for the South Coast Air Basin portion of San Bernardino County is 339 in one million. This increased cancer risk is largely due to the proximity to the Union Pacific Railroad rail line, State Route 60, and Interstate 10 Freeway.

PHASING AND TIMING

The proposed project is anticipated to be operational in 2021.

SENSITIVE RECEPTORS IN PROJECT VICINITY

Sensitive receptors include residential land uses, schools, day care centers, and other places where people reside, including prisons. The nearest sensitive receptors to the proposed project are the single-family residential dwelling units located approximately 50 feet to the south (across E Phillips Street), 65 feet to the west (across Sultana Avenue), and approximately 80 feet to the east (across Monterey Avenue) of the project site.

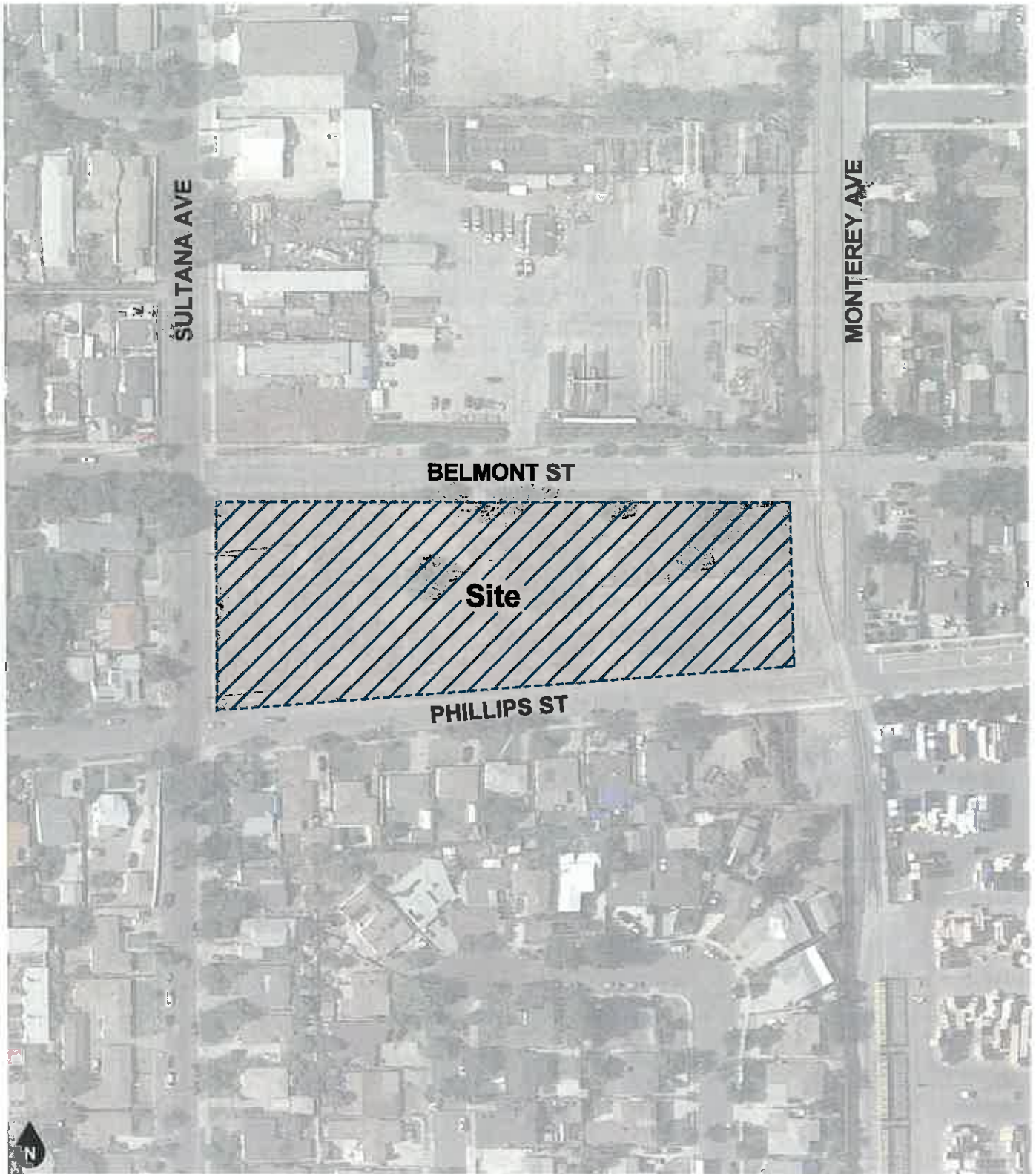


Figure 1
Project Location Map

500 Belmont Street Tow Yard
Health Risk Assessment Analysis
19350

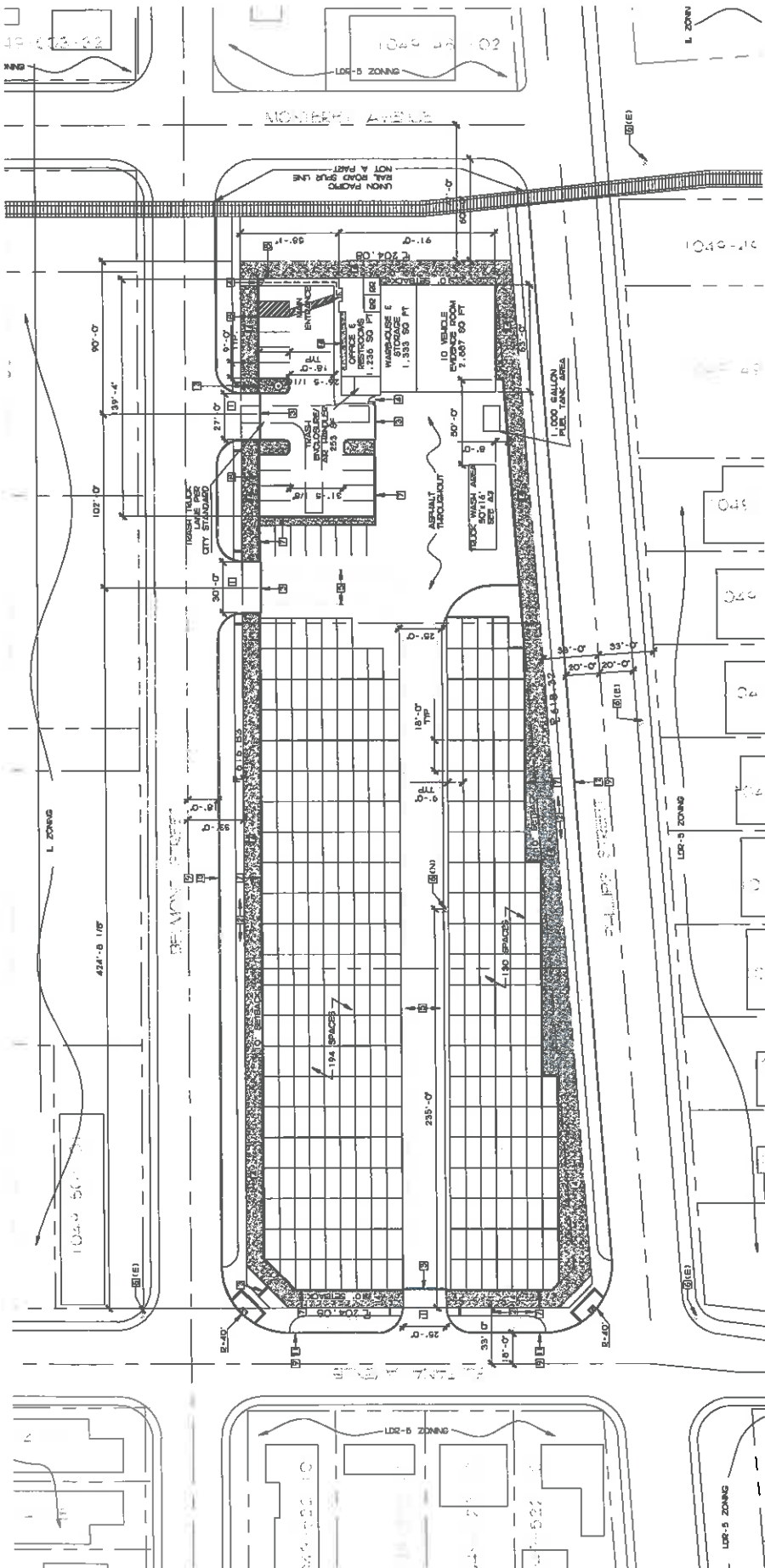


Figure 2
Site Plan

500 Belmont Street Tow Yard
Health Risk Assessment Analysis
19350



2. POLLUTANTS AND REGULATORY SETTING

POLLUTANTS

Pollutants are generally classified as either criteria pollutants or non-criteria pollutants. Federal ambient air quality standards have been established for criteria pollutants, whereas no ambient standards have been established for non-criteria pollutants. For some criteria pollutants, separate standards have been set for different periods. Most standards have been set to protect public health. For some pollutants, standards have been based on other values (such as protection of crops, protection of materials, or avoidance of nuisance conditions). A summary of federal and state ambient air quality standards is provided in the Regulatory Framework section.

Toxic Air Contaminants

In addition to the above-listed criteria pollutants, toxic air contaminants (TACs) are another group of pollutants of concern. Sources of toxic air contaminants include industrial processes such as petroleum refining and chrome plating operations, commercial operations such as gasoline stations and dry cleaners, and motor vehicle exhaust. Cars and trucks release at least forty different toxic air contaminants. The most important of these toxic air contaminants, in terms of health risk, are diesel particulates, benzene, formaldehyde, 1,3-butadiene, and acetaldehyde. Public exposure to toxic air contaminants can result from emissions from normal operations as well as from accidental releases. Health effects of toxic air contaminants include cancer, birth defects, neurological damage, and death.

Toxic air contaminants are less pervasive in the urban atmosphere than criteria air pollutants, however they are linked to short-term (acute) or long-term (chronic or carcinogenic) adverse human health effects. There are hundreds of different types of toxic air contaminants with varying degrees of toxicity. Sources of toxic air contaminants include industrial processes, commercial operations (e.g., gasoline stations and dry cleaners), and motor vehicle exhaust.

According to the 2013 California Almanac of Emissions and Air Quality, the majority of the estimated health risk from toxic air contaminants can be attributed to relatively few compounds, the most important of which is diesel particulate matter (DPM). Diesel particulate matter is a subset of PM_{2.5} because the size of diesel particles are typically 2.5 microns and smaller. The identification of diesel particulate matter as a toxic air contaminant in 1998 led the California Air Resources Board (CARB) to adopt the Risk Reduction Plan to Reduce Particulate Matter Emissions from Diesel-fueled Engines and Vehicles in September 2000. The plan's goals are a 75-percent reduction in diesel particulate matter by 2010 and an 85-percent reduction by 2020 from the 2000 baseline. Diesel engines emit a complex mixture of air pollutants, composed of gaseous and solid material. The visible emissions in diesel exhaust are known as particulate matter or PM, which includes carbon particles or "soot". Diesel exhaust also contains a variety of harmful gases and over 40 other cancer-causing substances. California's identification of diesel particulate matter as a toxic air contaminant was based on its potential to cause cancer, premature deaths, and other health problems. Exposure to diesel particulate matter is a health hazard, particularly to children whose lungs are still developing and the elderly who may have other serious health problems. Overall, diesel engine emissions are responsible for the majority of California's potential airborne cancer risk from combustion sources.

The California Air Resources Board (CARB) have monitoring networks that measure ambient concentrations of certain TACs that are associated with important health-related effects and are present in appreciable concentrations in the area. The CARB publishes annual Statewide, air basin, and location-specific summaries of the concentration levels of several TACs and their resulting cancer risks¹. The most recent summary is the CARB Air Quality Almanac for 2013 (CARB 2013). The Almanac presents the relevant concentration and

¹ Cancer risk is expressed as a probability of an individual out of a population of one million contracting cancer via a continuous exposure to TACs over a 30-year lifetime.

cancer risk data for the ten TACs that pose the most substantial health risk in California based on available data. These TACs are: acetaldehyde, benzene, 1,3-butadiene, carbon tetrachloride, hexavalent chromium, para-dichlorobenzene, formaldehyde, methylene chloride, and perchloroethylene. DPM is not directly measured but is indirectly estimated based on fine particulate matter measurements and special studies on the chemical speciation of ambient fine particulate data along with receptor modeling techniques. CARB showed that Diesel PM emissions decreased 37 percent from 2000 to 2010 primarily as a result of more stringent emissions standards and the introduction of cleaner burning diesel fuel. Emissions from diesel mobile sources are projected to continue to decrease after 2010. Overall, statewide emissions are forecasted to decline by 71 per cent between 2000 and 2035. CARB estimates that 78 percent of the known statewide cancer risks are from the top 10 outdoor air toxics in addition to DPM.

Estimates of total cancer risk Statewide have shown a steady decline from the early 1990s when the cancer risk from DPM was estimated to be 1,696 in one million. By the year 2000, the cancer risk was estimated to be 1,005 in one million or a reduction of 41 percent. Reductions in cancer risk are expected to continue into the future as new emission controls are implemented that further reduce DPM emissions, the major component of the total airborne cancer risk. Table 1 provides this summary of TACs and health risk information from the ARB Annual Toxic Summary for the most recent three-year period, 2017-2019 for the Riverside-Rubidoux air monitoring station, the closest air monitoring station to the project site with recent data, located approximately 13.61 miles southeast of the project site. The cancer risk attributable to the non-DPM chemicals (i.e., the 10 TACs measured by the ARB described above) have also shown significant reductions at the Riverside-Rubidoux location declining from an estimated cancer risk of 397 in one million in 2017, to 260 in one million in 2018.

Asbestos

Asbestos is listed as a TAC by the CARB and as a Hazardous Air Pollutant by the United States Environmental Protection Agency (EPA). Asbestos occurs naturally in mineral formations and crushing or breaking these rocks, through construction or other means, can release asbestiform fibers into the air. Asbestos emissions can result from the sale or use of asbestos-containing materials, road surfacing with such materials, grading activities, and surface mining. The risk of disease is dependent upon the intensity and duration of exposure. When inhaled, asbestos fibers may remain in the lungs and with time may be linked to such diseases as asbestosis, lung cancer, and mesothelioma. Naturally occurring asbestos is not present in San Bernardino County. The nearest likely locations of naturally occurring asbestos, as identified in the [General Location Guide for Ultramafic Rocks in California](#) prepared by the California Division of Mines and Geology, is located in Santa Barbara County. Due to the distance to the nearest natural occurrences of asbestos, the project site is not likely to contain asbestos.

REGULATORY SETTING

The proposed project is addressed through the efforts of various international, federal, state, regional, and local government agencies. These agencies work jointly, as well as individually, to improve air quality through legislation, regulations, planning, policy making, education, and a variety of programs. The agencies responsible for improving the air quality are discussed below.

Federal – United States Environmental Protection Agency (EPA)

The EPA is responsible for setting and enforcing the National Ambient Air Quality Standards (NAAQS) for atmospheric pollutants. It regulates emission sources that are under the exclusive authority of the federal government, such as aircraft, ships, and certain locomotives. The National Ambient Air Quality Standards (NAAQS) pollutants were identified using medical evidence.

As part of its enforcement responsibilities, the EPA requires each state with federal nonattainment areas to prepare and submit a State Implementation Plan (SIP) that demonstrates the means to attain the national standards. The State Implementation Plan (SIP) must integrate federal, state, and local components and

regulations to identify specific measures to reduce pollution, using a combination of performance standards and market-based programs within the timeframe identified in the State Implementation Plan (SIP).

State - California Air Resources Board

The CARB, which is a part of the California Environmental Protection Agency, is responsible for the coordination and administration of both federal and state air pollution control programs within California. In this capacity, the CARB conducts research, sets the California Ambient Air Quality Standards (CAAQS), compiles emission inventories, develops suggested control measures, provides oversight of local programs, and prepares the State Implementation Plan (SIP). In addition, the CARB establishes emission standards for motor vehicles sold in California, consumer products (e.g., hairspray, aerosol paints, and barbeque lighter fluid), and various types of commercial equipment. It also sets fuel specifications to further reduce vehicular emissions.

CARB Airborne Toxic Control Measure to Limit Diesel-Fueled Commercial Motor Vehicle Idling adopts new section 2485 within Chapter 10, Article 1, Division 3, title 13 in the California Code of Regulations. The measure limits the idling of diesel vehicles (i.e., commercial trucks over 10,000 pounds) to reduce emissions of toxics and criteria pollutants. The driver of any vehicle subject to this section: (1) shall not idle the vehicle's primary diesel engine for greater than five minutes at any location; and (2) shall not idle a diesel-fueled auxiliary power system for more than five minutes to power a heater, air conditioner, or any ancillary equipment on the vehicle if it has a sleeper berth and the truck is located within 100 feet of a restricted area (homes and schools).

CARB Requirements to Reduce Idling Emissions from New and In-Use Trucks. Amendments were made to Title 13 in California Code of Regulations in Sections 1956.8, 2404, 2424, 2425, and 2485. The amendment states: "all new 2008 and subsequent model-year heavy-duty diesel engines shall be equipped with an engine shutdown system that automatically shuts down the engine after 300 seconds of continuous idling operation once the vehicle is stopped, the transmission is set to 'neutral' or 'park,' and the parking brake is engaged. If the parking brake is not engaged, then the engine shutdown system shall shut down the engine after 900 seconds of continuous idling operation once the vehicle is stopped and the transmission is set to 'neutral' or 'park.'" There are a few conditions where the engine shutdown system can be overridden to prevent engine damage. Any project trucks manufactured after 2008 would be consistent with this rule, which would ultimately reduce air emissions.

Statewide Truck and Bus Regulation (Regulation to Reduce Emissions of DPM, Oxides of Nitrogen and Other Criteria Pollutants, from In-Use Heavy-Duty Diesel-Fueled Vehicles, Title 13, California Code of Regulations, Section 2025). On December 12, 2008, the ARB approved this regulation to reduce emissions from existing on-road diesel trucks and buses operating in California. This regulation applies to all on-road heavy-duty diesel-fueled vehicles with a gross vehicle weight rating greater than 14,000 pounds, agricultural yard trucks with off-road certified engines, and certain diesel fueled shuttle vehicles of any gross vehicle weight rating. Out-of-state trucks and buses that operate in California are also subject. Under the regulation, older, heavier trucks (i.e. those with pre-2000 year engines and a gross vehicle weight rating greater than 26,000 pounds), are required to have installed a particulate matter filter and must be replaced with a 2010 engine between 2015 and 2020, depending on the model year. By 2015, all heavier pre-1994 trucks must be upgraded to 2010 engines and newer trucks are thereafter required to be replaced over the next eight years. Older, more polluting trucks are required to be replaced first, while trucks that already have relatively clean 2007-2009 engines are not required to be replaced until 2023. Lighter trucks (14,001-26,000 pounds) must adhere to a similar schedule. Furthermore, nearly all trucks that are not required under the Truck and Bus Regulation to be replaced by 2015 were required to be upgraded with a particulate matter filter by that date.

The CARB is also responsible for regulations pertaining to toxic air contaminants. The Air Toxics "Hot Spots" Information and Assessment Act (AB 2588, 1987, Connelly) was enacted in 1987 as a means to establish a formal air toxics emission inventory risk quantification program. AB 2588, as amended, establishes a process that requires stationary sources to report the type and quantities of certain substances their facilities routinely

release into the air basin. The data is ranked by high, intermediate, and low categories, which are determined by: the potency, toxicity, quantity, volume, and proximity of the facility to nearby receptors.

AB 617 Nonvehicular air pollution: criteria air pollutants and toxic air contaminants

This bill requires the state board to develop a uniform statewide system of annual reporting of emissions of criteria air pollutants and toxic air contaminants for use by certain categories of stationary sources. The bill requires those stationary sources to report their annual emissions of criteria air pollutants and toxic air contaminants, as specified. This bill required the state board, by October 1, 2018, to prepare a monitoring plan regarding technologies for monitoring criteria air pollutants and toxic air contaminants and the need for and benefits of additional community air monitoring systems, as defined. The bill requires the state board to select, based on the monitoring plan, the highest priority locations in the state for the deployment of community air monitoring systems. The bill requires an air district containing a selected location, by July 1, 2019, to deploy a system in the selected location. The bill would authorize the air district to require a stationary source that emits air pollutants in, or that materially affect, the selected location to deploy a fence-line monitoring system, as defined, or other specified real-time, on-site monitoring. The bill authorizes the state board, by January 1, 2020, and annually thereafter, to select additional locations for the deployment of the systems. The bill would require air districts that have deployed a system to provide to the state board air quality data produced by the system. By increasing the duties of air districts, this bill would impose a state-mandated local program. The bill requires the state board to publish the data on its Internet Web site.

Regional

The project site is located within the City of Ontario, in San Bernardino County, which is part of the South Coast Air Basin (SCAB) that includes all of Orange County as well as the non-desert portions of Los Angeles, Riverside, and San Bernardino Counties. The South Coast Air Basin is located on a coastal plain with connecting broad valleys and low hills to the east. Regionally, the South Coast Air Basin is bounded by the Pacific Ocean to the southwest and high mountains to the east forming the inland perimeter.

SCAQMD

The SCAQMD is the agency principally responsible for comprehensive air pollution control in the South Coast Air Basin. To that end, as a regional agency, the SCAQMD works directly with the Southern California Association of Governments (SCAG), county transportation commissions, and local governments and cooperates actively with all federal and state agencies.

In addition to attaining and maintaining air quality standards set by State and Federal Governments, the District is also responsible for ensuring that toxic air pollutants do not pose a nuisance or significant health threat to the surrounding community. Every year, the State's Air Toxics Hot Spots program (AB 2588) requires the District to quantify and assess health risks from subject facilities to nearby residents, notify affected residents of significant risks, and to reduce those significant health risks to acceptable levels.

Health Risk Significant Thresholds

According to the SCAQMD CEQA Handbook, any project that has the potential to expose the public to toxic air contaminants in excess of the following thresholds would be considered to have a significant air quality impact:

- If the Maximum Incremental Cancer Risk is 10 in one million or greater; or
- Toxic air contaminants from the proposed project would result in a Hazard Index increase of 1 or greater.

In order to determine if the proposed project may have a significant impact related to hazardous air pollutants (HAP), the Health Risk Assessment Guidance for analyzing Cancer Risks from Mobile Source Diesel Idling

Emissions for CEQA Air Quality Analysis. (Diesel Analysis), prepared by SCAQMD, August 2003, recommends that if the proposed project is anticipated to create hazardous air pollutants through stationary sources or regular operations of diesel trucks on the project site, then the proximity of the nearest receptors to the source of the hazardous air pollutants and the toxicity of the hazardous air pollutants should be analyzed through a comprehensive facility-wide health risk assessment (HRA).

As determined in the *California Building Industry Association v. Bay Area Air Quality Management District* (2015) 62 Cal. 4th 369 (CBIA) case the California Supreme Court determined that CEQA does not generally require an impact analysis of the existing environmental conditions on the future residents of a proposed project and generally only requires an analysis of the proposed project's impact on the environment. However, the CBIA case also stated that when a proposed project brings development and people into an area already subject to specific hazards and the new development/people exacerbate the existing hazards, then CEQA requires an analysis of the hazards and the proposed project's effect in terms of increasing the risks related to those hazards. Regarding air quality hazards, TACs are defined as substances that may cause or contribute to an increase in deaths or in serious illness, or that may pose a present or potential hazard to human health. As such, if a proposed project would not exacerbate pre-existing hazards (e.g., TAC health risks) then an analysis of those hazards and the proposed project's effect on increasing those hazards is not required.

However, the project is an industrial tow yard project and will be a source of toxic air contaminants; therefore, per City of Ontario request, an HRA was conducted.

Table 1
TAC Concentration Levels and Associated Risks - Riverside-Rubidoux

TAC	Concentration ¹ Risk ²	Year		
		2017	2018	2019
Acetaldehyde	Annual Average	1.080	ND	ND
	Health Risk	16	ND	ND
Benzene	Annual Average	0.271	0.239	ID
	Health Risk	70	62	ID
1,3-Butadiene	Annual Average	0.044	0.043	ID
	Health Risk	48	46	ID
Carbon Tetrachloride	Annual Average	0.090	0.073	ID
	Health Risk	69	56	ID
Chromium, Hex	Annual Average	6.7	ID	ND
	Health Risk	ND	ND	ND
Para-Dichlorobenzene	Annual Average	ID	ID	ID
	Health Risk	ID	ID	ID
Formaldehyde	Annual Average	3.350	ND	ND
	Health Risk	70	ND	ND
Methylene Chloride	Annual Average	12.300	9.590	ID
	Health Risk	122	95	ID
Perchloroethylene	Annual Average	0.013	0.011	ID
	Health Risk	2	1	ID
Diesel PM	Annual Average	No monitoring data available		
	Health Risk			
Total Health Risk (without DPM)		397	260	-

Notes:

ND = no data reported; ID = insufficient data

Source: <http://www.arb.ca.gov/adam/toxics/toxics.html> (for Riverside-Rubidoux- 5558 California Avenue Air Monitoring Station)

1. Concentrations for Hexavalent Chromium are expressed as ng/m³, and concentrations for Diesel PM are expressed as µg/m³. Concentrations for all other TACs are expressed as ppb.

2. Health Risk represents the number of excess cancer cases per million people based on a lifetime (30-year) exposure to the annual average concentration. Total Health Risk represents only those compounds listed in this table and only those with data for the year. There may be other significant compounds for which monitoring and/or health risk information is not available.

3. DIESEL EMISSIONS HEALTH RISK ASSESSMENT

The on-going operation of the proposed project would generate toxic air contaminant emissions from diesel truck emissions created by the on-going operations of the proposed project. According to SCAQMD methodology, health effects from carcinogenic air toxics are usually described in terms of individual cancer risk. "Individual Cancer Risk" is the likelihood that a person exposed to concentrations of toxic air contaminants over a 30-year lifetime will contract cancer, based on the use of revised Office of Environmental Health Hazard Assessment (OEHHA) risk-assessment methodology.²

A health risk assessment requires the completion and interaction of four general steps:

- (1) Quantify project-generated TAC emissions.
- (2) Identify nearby ground-level receptor locations that may be affected by the emissions (including any special sensitive receptor locations such as residences, schools, hospitals, convalescent homes, and daycare centers).
- (3) Perform air dispersion modeling analyses to estimate ambient pollutant concentrations at each receptor location using project TAC emissions and representative meteorological data to define the transport and dispersion of those emissions in the atmosphere.
- (4) Characterize and compare the calculated health risks with the applicable health risk significance thresholds.

EMISSIONS INVENTORY DEVELOPMENT

Important issues that affect the dispersion modeling include the following: (1) Model Selection, (2) Source Treatment, (3) Meteorological Data, and (4) Receptor Grid. Each of these issues is addressed below.

Emission Source Estimates - DPM for Motor Vehicles

DPM emissions from the various sources were calculated using information derived from the project description, and mobile source emission factors from the CARB EMFAC2017 emissions factor model. Truck mix information was obtained from the *500 Belmont Tow Yard Trip Generation Analysis* prepared by Ganddini Group, Inc. (February 4, 2021).

Four pieces of information are required to generate the mobile source emissions from the proposed project:

- Number of vehicle trips for each component of the proposed project;
- Types of vehicles that access the proposed project (passenger car vs. heavy-duty truck and gasoline vs. diesel);
- The allocation of the vehicle trips to each building that comprises the proposed project; and
- Estimate of the vehicle emission factors for estimating exhaust and idling emissions.

Estimate of Vehicle Trips and Vehicle Types

The Trip Generation Analysis showed the project is expected to generate approximately 31 (non-passenger car equivalents) vehicle trips per day. Of those vehicle trips, for the tow yard use 21 are automobile round

² In February 2015, the Office of Environmental Health Hazard Assessment updated their "Air Toxics Hot Spots Program, Risk Assessments Guidelines, Guidance Manual for Preparation of Health Risk Assessments; however, the updated OEHHA guidance states in the page footers "do not cite or quote." SCAQMD staff have incorporated the updates into their methodology for SCAQMD's Rules 1401, 1401.1, 1402, and 212, and have updated their HRA Guidance for permitting; however they are still in the process of updating the guidance for CEQA analyses (via working group sessions); however, to be conservative, the new OEHHA guidance was used to assess HRA impacts in this analysis. Per SCAQMD staff (personal communication with Dr. Jillian Wong 6-19-2015 and 12-22-15), updated SCAQMD HRA guidance will be forthcoming.

trips, 8 are 2-axle truck round trips, 0 are 3-axle truck round trips, and 2 are 4+-axle truck round trips per day (non-passenger car equivalents).

Per the Trip Generation Analysis, as the Institute of Transportation Engineers (ITE) *Trip Generation Manual* (10th Edition, 2017) does not include data for tow and storage yard facilities, trip rates were derived from impound and release records provided by the project applicant for their existing facility in the City of Ontario. The impound and release records provided were for January 2020, which is prior to the pandemic related “stay-at-home” orders that disrupted regular service for many businesses. The total impounds and releases during the first four weeks of January 2020 were divided by the number of days to determine the average daily trip generation for this land use during typical weekday conditions.

Estimate of Emission Factors

The DPM emission factors for the various vehicle types were derived from the CARB EMFAC2017 mobile source emission model. The emissions factors were derived for San Bernardino County. Third trimester exposure used opening year (2021) emissions factors, 2-year factors (for infant exposure) reflect years 2022 and 2023, 14-year average factors (for child exposure during years 2-16) reflect emissions during the first 14 years of operation (2024 to 2037), the second 14 years of exposure (years 2038-2051) were used for assessment of exposure during years 16 to 30.

Emissions factors were estimated to establish the emissions generated while the vehicles travel off-site, along travel links from the entrance to the loading docks, and while idling at the loading dock during loading or unloading materials. All vehicles were assumed to travel on-site at a speed of 10 miles per hour. Off-site, the speeds along the roads were anticipated to average 35 miles per hour. Delivery vehicles were assumed to idle for a maximum of 15 minutes per vehicle per day (5 minutes per location: at the truck wash area and throughout the tow yard parking areas), in keeping with the CARB Air Toxic Control Measure (ATCM), which regulates truck idling time (CARB 2005). The four different sets of emissions factors used in this assessment are detailed in Table 2. It should be noted that the DPM emissions on both the gram per mile and gram per idle hour bases decline beyond 2021 for all vehicle classes and in particular the heavy-heavy-duty truck class (the 4+ axle “big rig” trucks). This is due to the CARB emissions’ requirements on heavy-duty trucks that call for either the replacement of older trucks with cleaner trucks or the installation of diesel particulate matter filters on the truck fleet.

Emission Source Characterization

Each of the emission source types described above also requires geometrical and emission release specifications for use in the air dispersion model. Table 3 provides a summary of the assumptions used to configure the various emission sources. The following definitions are used to characterize the emission source geometrical configurations referred to in Table 3:

- Point source: A single, identifiable, local source of emissions; it is approximated in the AERMOD air dispersion model as a mathematical point in the modeling region with a location and emission characteristics such as height of release, temperature, etc., for example, a truck idle location where emissions are sourced from the truck’s exhaust stack while the vehicle is stationary.
- Line source: A series of volume sources along a path, for example, vehicular traffic volumes along a roadway.

Figure 3 provides the location of the project buildings, emission source locations, and the locations of the nearest sensitive receptors (single-family detached residential dwelling units located approximately 50 feet south of the project’s southern property line, 80 feet east of the project’s eastern property line, and approximately 65 feet west of the project site’s western property line and along Belmont St, Sultana Avenue, Mission Boulevard, and Euclid Avenue). Receptors are shown as orange triangles. Residential receptors are

shown as orange triangles labeled 1 through 10. The direction of on-site and off-site truck travel were obtained from the site plan and City truck routes.

RECEPTOR NETWORK

The assessment requires that a network of receptors be specified where the impacts can be computed at the various locations surrounding the project. Receptors were located at existing sensitive receptors surrounding the proposed project (as detailed above). In addition, the identified sensitive receptors locations were supplemented by the specification of a modeling grid that extended around the proposed project to identify other potential locations of impact. The locations of the receptors are shown as orange triangles on Figure 3.

DISPERSION MODELING

The next step in the assessment process utilizes the emissions inventory along with a mathematical air dispersion model and representative meteorological data to calculate impacts at the various receptor locations. The dispersion model used in this assessment is described below.

Model Selection

The assessment of air quality and health risk impacts from pollutant emissions from this project applied the USEPA AERMOD Model, which is the air dispersion model accepted by the SCAQMD for performing air quality impact analyses. AERMOD predicts pollutant concentrations from point, area, volume, line, and flare sources with variable emissions in terrain from flat to complex with the inclusion of building downwash effects from buildings on pollutant dispersion. It captures the essential atmospheric physical processes and provides reasonable estimates over a wide range of meteorological conditions and modeling scenarios.

General Model Assumptions

A summary of Emission Configurations is shown in Table 3. The basic options used in the dispersion modeling are summarized in Table 4.

As indicated in Table 4 the analysis takes into account the effects of building downwash on the dispersion of emissions from the various sources located on the project's property. Building downwash occurs when the aerodynamic turbulence, induced by nearby buildings, causes pollutants emitted from an elevated source to be mixed rapidly toward the ground (downwash), resulting in potentially higher ground-level concentrations than if the buildings were not present. The AERMOD dispersion model contains algorithms to account for building downwash effects. The required information includes the location of the emission source; the location of adjacent buildings; and the building geometry in terms of length, width, and height. For purposes of this analysis, the emission source and building locations were taken from the project site plan. The proposed building geometries were estimated from the project plans, assuming a building height of 24 feet.

Meteorological Data

Meteorological data (processed with the ADJ_U option) from the Air District's Ontario International Airport monitoring site was selected for this modeling application. Five full years of sequential meteorological data was collected at the site from January 1, 2012 to December 31, 2016 by the SCAQMD. The SCAQMD processed the data for input to the model. The data was obtained at SCAQMD's <https://www.aqmd.gov/home/air-quality/air-quality-data-studies/meteorological-data/data-for-aermod> (see Figure 4).

ESTIMATION OF HEALTH RISKS

Health risks from diesel particulate matter are twofold. First, diesel particulate matter is a carcinogen according to the State of California. Second, long-term chronic exposure to diesel particulate matter can cause health effects to the respiratory system. Each of these health risks is discussed below.

Cancer Risks

According to the *Risk Assessment Guidelines: Guidance Manual for Preparation of Health Risk Assessments*, released by the Office of Environmental Health Hazard Assessment (OEHHHA) in February 2015 and formally adopted in March 2015, the residential inhalation dose for cancer risk assessment should be calculated using the following formula:

$$[\text{Dose-air (mg)/(Kg-day)}] * \text{Cancer Potency} * [1 \times 10^{-6}] = \text{Potential Cancer Risk}$$

Where:

Cancer Potency Factor = 1.1

$$\text{Dose-inh} = (\text{C-air} * \text{DBR} * \text{A} * \text{EF} * \text{ED} * \text{ASF} * \text{FAH} * 10^{-6}) / \text{AT}$$

Where:

Cair [Concentration in air ($\mu\text{g}/\text{m}^3$)] = (Calculated by AERMOD Model)

DBR [Daily breathing rate (L/kg body weight – day)] = 261 for adults, 572 for children, and 1,090 for infants, and 361 for 3rd trimester per SCAQMD Permit Application Package "N" Table 4.1 D guidance.

A [Inhalation absorption factor] = 1

EF [Exposure frequency (days/year)] = 350

ED [Exposure duration (years)] = 30 for adults (for an individual who is an adult at opening year), 14 for children (from 2-16 years), 14 for adults (from 16-30 years), 2 for infants, and 1 for 3rd Trimester

ASF [Age sensitivity factor] = 10 for 3rd trimester to 2 years of age, 3 for 2 to 16 years of age, and 1 for 16 to 30 years of age

FAH [Fraction of time spent at home] = 1 for 3rd trimester to 2 years of age, 1 for 2 to 16 years of age, and 0.73 for 16 to 30 years of age

10^6 [Micrograms to milligrams conversion]

AT [Average time period over which exposure is averaged in days] = 25,550

The model run results are shown in Appendix B. Figure 5 illustrates the cancer risk to the most affected age-group, infants (0-2 years).

Table 5 show the cancer risk for the unborn child during the 3rd trimester, Table 6 shows the cancer risk to infants (0-2 years), Table 7 shows the cancer risk to children ages 2 to 16 years and Table 8 shows the cancer risk as that child becomes an adult (years 16-30). The highest cancer risk corresponds to child cancer risk 2-16 years (see Table 7), and is at receptor 3, with a maximum risk of 0.45 in one million. The highest infant cancer risk 0-2 years is also at receptor 3; with a maximum risk of 0.41 in one million. Therefore, no children or infants are exposed to cancer risks in excess of 10 in a million.

The assessment of cancer-related health risk to sensitive receptors within the project vicinity is based on the following most-conservative scenario:

An unborn child in its 3rd trimester is potentially exposed to DPM emissions (via exposure of the mother) during the opening year. That child is born opening year and then remains at home for the entire first two years of life. From age 2 to 16, the child remains at home 100 percent of the time. From age 16 to 30, the child continues to live at home, growing into an adult that spends 73 percent of its time at home and lives there until age 30.

Based on the above, ultra-conservative assumptions, the 30.25-year, cumulative carcinogenic health risk (3rd trimester [-0.25 to 0 years] + infant [0-2 years] + child [2-16 years] + adult [16-30 years]) to an individual born during the opening year of the project and located in the project vicinity for the entire 30-year duration, is a maximum of 0.93 in a million at receptor location 3, as shown in Table 9. Therefore, the on-going operations of the proposed project would result in a less than significant impact due to the cancer risk from diesel emissions created by the proposed project. As the residential cancer risk does not exceed 10 in a million.

Non-Cancer Risks

The relationship for non-cancer health effects is given by the equation:

$$\text{HIDPM} = \text{CDPM} / \text{RELDPM}$$

Where,

- HIDPM = Hazard Index; an expression of the potential for non-cancer health effects.
- CDPM = Annual average diesel particulate matter concentration in $\mu\text{g}/\text{m}^3$.
- RELDPM = Reference Exposure Level (REL) for diesel particulate matter; the diesel particulate matter concentration at which no adverse health effects are anticipated.

The non-carcinogenic hazards to adult, child and infant receptors are also detailed in Tables 5 through 8 column (j). The RELDPM is $5 \mu\text{g}/\text{m}^3$. The Office of Environmental Health Hazard Assessment as protective for the respiratory system has established this concentration. Using the maximum DPM concentration from years 2021-2051, the resulting Hazard Index is:

$$\text{HIDPM} = 0.00159 / 5 = 0.0003$$

The criterion for significance is a Hazard Index increase of 1.0 or greater. Therefore, the on-going operations of the proposed project would result in a less than significant impact due to the non-cancer risk from diesel emissions created by the proposed project.

Table 2
DPM Emissions Factors for the Proposed Project

Vehicle Class	14-Year Average (First 14 years of Operation - 2024-2037)		
	Idling (g/hr)	On-Site Travel (g/mi)	Off-Site Travel (g/mi)
Light Heavy Duty Truck 2	0.79348	0.03822	0.01657
Medium Heavy Duty Truck	0.01881	0.00501	0.00387
Heavy Heavy Duty Truck	0.01113	0.01049	0.00853

Vehicle Class	14-Year Average (Second 14 years of Operation - 2038-2051)		
	Idling (g/hr)	On-Site Travel (g/mi)	Off-Site Travel (g/mi)
Light Heavy Duty Truck 2	0.79386	0.02939	0.01440
Medium Heavy Duty Truck	0.00786	0.00437	0.00368
Heavy Heavy Duty Truck	0.01017	0.00959	0.00810

Vehicle Class	2-Year Average (2022-2023)		
	Idling (g/hr)	On-Site Travel (g/mi)	Off-Site Travel (g/mi)
Light Heavy Duty Truck 2	0.78735	0.05065	0.01953
Medium Heavy Duty Truck	0.09549	0.03406	0.01910
Heavy Heavy Duty Truck	0.01380	0.02424	0.01325

Vehicle Class	1-Year Average (Opening Year-2021)		
	Idling (g/hr)	On-Site Travel (g/mi)	Off-Site Travel (g/mi)
Light Heavy Duty Truck 2	0.78639	0.05383	0.02026
Medium Heavy Duty Truck	0.24358	0.17489	0.06839
Heavy Heavy Duty Truck	0.02224	0.07840	0.03670

Notes:

Source: EMFAC2017.

**Table 3
Summary of Emission Configurations**

Emission Source Type	Geometric Configuration	Relevant Assumptions
Off-Site Diesel Truck Traffic	Line Sources	Stack release height: 12 feet
		Vehicle speed: 35 mph
		Length of the line source (along Belmont St from Project Driveway to Sultana Ave, Belmont Street Sultana Ave to Euclid Ave, Euclid Ave south of Belmont St, Sultana Ave Belmont St to Mission Blvd, Mission Blvd to Euclid Ave, and Euclid Ave north of Mission Blvd)
		Vehicle types: heavy-heavy-duty, medium-heavy-duty and light-heavy-duty diesel delivery trucks
		Emission factor: CARB EMFAC2017
On-Site Diesel Truck Traffic	Line Sources	Stack release height: 12 feet
		Vehicle speed: 10 mph
		Length of the line source (distance from the facility entrance on Belmont Street to parking lot area)
		Vehicle types: heavy-heavy-duty and light-heavy-duty diesel tow trucks
		Emission factor: CARB EMFAC2017
On-Site Diesel Truck Idling	Point Sources located at various locations on-site.	Stack release height: 12 feet
		Stack release characteristics
		> Stack diameter: 0.1 meter (0.3 feet)
		> Stack velocity: 51.9 mps (170 feet/sec)
		> Stack temperature: 366 °k (200° F)
		Idle time: 15 minutes per truck per day
		Vehicle types: heavy-heavy-duty and light-heavy-duty diesel tow trucks Emission factor: CARB EMFAC2017

Table 4
General Modeling Assumptions - AERMOD Model

Feature	Option Selected
Terrain processing	AERMAP - NED GEOTIFF 30 min
Emission source configuration	See Table 3
Regulatory dispersion options	Default
Land use	Urban
Coordinate system	UTM, Zone 11 north
Building downwash	Included in calculations
Receptor height	0 meters above ground (per OEHHA methodology)
Meteorological data	SCAQMD Ontario International Airport Meteorological Data

**Table 5
Carcinogenic Risks and Non-Carcinogenic 3rd Trimester Exposure Scenario (0.25-Year)**

Receptor ID (a)	Maximum Concentration		Weight Fraction (d)	Contaminant (e)	Carcinogenic Hazards		Noncarcinogenic Hazards		
	(ug/m3) (b)	(mg/m3) (c)			CPF (mg/kg/day) (f)	RISK (per million) (g)	REL (ug/m3) (h)	RfD (mg/kg/day) (i)	Index (j)
1	0.00054	5.4E-07	1.00E+00	DPM	1.1E+00	0.01	5.0E+00	1.4E-03	0.0001
2	0.00067	6.7E-07	1.00E+00	DPM	1.1E+00	0.01	5.0E+00	1.4E-03	0.0001
3	0.00129	1.3E-06	1.00E+00	DPM	1.1E+00	0.02	5.0E+00	1.4E-03	0.0003
4	0.00037	3.7E-07	1.00E+00	DPM	1.1E+00	0.01	5.0E+00	1.4E-03	0.0001
5	0.00015	1.5E-07	1.00E+00	DPM	1.1E+00	0.00	5.0E+00	1.4E-03	0.0000
6	0.00008	8.0E-08	1.00E+00	DPM	1.1E+00	0.00	5.0E+00	1.4E-03	0.0000
7	0.00009	9.0E-08	1.00E+00	DPM	1.1E+00	0.00	5.0E+00	1.4E-03	0.0000
8	0.00007	7.0E-08	1.00E+00	DPM	1.1E+00	0.00	5.0E+00	1.4E-03	0.0000
9	0.00011	1.1E-07	1.00E+00	DPM	1.1E+00	0.00	5.0E+00	1.4E-03	0.0000
10	0.00022	2.2E-07	1.00E+00	DPM	1.1E+00	0.00	5.0E+00	1.4E-03	0.0000

Notes:

OEHHA 95th percentile Exposure factors used to calculate TAC intake:

Exposure Frequency (days/year)	350
Exposure Duration (years)	0.25
Daily Breathing Rate	361
Age Sensitivity Factor	10
Fraction of Time At Home (FAH)	1
Averaging Time _(cancer) (days)	25550
Averaging Time _(non-cancer) (days)	91.25

E = 10^X, i.e. E-02 = 10⁻²

**Table 6
Carcinogenic Risks and Non-Carcinogenic Infant Exposure Scenario (2-Year)**

Receptor ID (a)	Maximum Concentration		Weight Fraction (d)	Contaminant (e)	Carcinogenic Hazards		Noncarcinogenic Hazards		
	(ug/m3) (b)	(mg/m3) (c)			CPF (mg/kg/day) (f)	RISK (per million) (g)	REL (ug/m3) (h)	RfD (mg/kg/day) (i)	Index (j)
1	0.00051	5.1E-07	1.00E+00	DPM	1.1E+00	0.17	5.0E+00	1.4E-03	0.0001
2	0.00064	6.4E-07	1.00E+00	DPM	1.1E+00	0.21	5.0E+00	1.4E-03	0.0001
3	0.00126	1.3E-06	1.00E+00	DPM	1.1E+00	0.41	5.0E+00	1.4E-03	0.0003
4	0.00034	3.4E-07	1.00E+00	DPM	1.1E+00	0.11	5.0E+00	1.4E-03	0.0001
5	0.00012	1.2E-07	1.00E+00	DPM	1.1E+00	0.04	5.0E+00	1.4E-03	0.0000
6	0.00007	7.0E-08	1.00E+00	DPM	1.1E+00	0.02	5.0E+00	1.4E-03	0.0000
7	0.00008	8.0E-08	1.00E+00	DPM	1.1E+00	0.03	5.0E+00	1.4E-03	0.0000
8	0.00006	6.0E-08	1.00E+00	DPM	1.1E+00	0.02	5.0E+00	1.4E-03	0.0000
9	0.00009	9.0E-05	1.00E+00	DPM	1.1E+00	0.03	5.0E+00	1.4E-03	0.0000
10	0.00019	1.9E-04	1.00E+00	DPM	1.1E+00	0.06	5.0E+00	1.4E-03	0.0000

Notes:

OEHHA 95th percentile Exposure factors used to calculate TAC intake

Exposure Frequency (days/year)	350
Exposure Duration (years)	2
Daily Breathing Rate	1090
Age Sensitivity Factor	10
Fraction of Time At Home (FAH)	1
Averaging Time _(cancer) (days)	25550
Averaging Time _(non-cancer) (days)	730

E= 10^X, i.e. E-02 = 10⁻²

Table 7
Carcinogenic Risks and Non-Carcinogenic Child Exposure Scenario (2-16 Years)

Receptor ID (a)	Maximum Concentration		Weight Fraction (d)	Contaminant (e)	Carcinogenic Hazards		Noncarcinogenic Hazards		
	(ug/m3) (b)	(mg/m3) (c)			CPF (mg/kg/day) (f)	RISK (per million) (g)	REL (ug/m3) (h)	RfD (mg/kg/day) (i)	Index (j)
1	0.00049	4.9E-07	1.00E+00	DPM	1.1E+00	0.18	5.0E+00	1.4E-03	0.0001
2	0.00061	6.1E-07	1.00E+00	DPM	1.1E+00	0.22	5.0E+00	1.4E-03	0.0001
3	0.00124	1.2E-06	1.00E+00	DPM	1.1E+00	0.45	5.0E+00	1.4E-03	0.0002
4	0.00032	3.2E-07	1.00E+00	DPM	1.1E+00	0.12	5.0E+00	1.4E-03	0.0001
5	0.00011	1.1E-07	1.00E+00	DPM	1.1E+00	0.04	5.0E+00	1.4E-03	0.0000
6	0.00007	7.0E-08	1.00E+00	DPM	1.1E+00	0.03	5.0E+00	1.4E-03	0.0000
7	0.00007	7.0E-08	1.00E+00	DPM	1.1E+00	0.03	5.0E+00	1.4E-03	0.0000
8	0.00006	6.0E-08	1.00E+00	DPM	1.1E+00	0.02	5.0E+00	1.4E-03	0.0000
9	0.00008	8.0E-08	1.00E+00	DPM	1.1E+00	0.03	5.0E+00	1.4E-03	0.0000
10	0.00017	1.7E-07	1.00E+00	DPM	1.1E+00	0.06	5.0E+00	1.4E-03	0.0000

Notes:

OEHHA 95th percentile Exposure factors used to calculate TAC intake

Exposure Frequency (days/year)	350
Exposure Duration (years)	14
Daily Breathing Rate	572
Age Sensitivity Factor	3
Fraction of Time At Home (FAH)	1
Averaging Time _(cancer) (days)	25550
Averaging Time _(non-cancer) (days)	5110

E = 10^X, i.e. E-02 = 10⁻²

**Table 8
Carcinogenic Risks and Non-Carcinogenic Hazards Adult Exposure Scenario (16-30 Years)**

Receptor ID (a)	Maximum Concentration		Weight Fraction (d)	Contaminant (e)	Carcinogenic Hazards		Noncarcinogenic Hazards		
	(ug/m3) (b)	(mg/m3) (c)			CPF (mg/kg/day) (f)	RISK (per million) (g)	REL (ug/m3) (h)	RfD (mg/kg/day) (i)	Index (j)
1	0.00047	4.7E-07	1.00E+00	DPM	1.1E+00	0.02	5.0E+00	1.4E-03	0.0001
2	0.00059	5.9E-07	1.00E+00	DPM	1.1E+00	0.02	5.0E+00	1.4E-03	0.0001
3	0.00123	1.2E-06	1.00E+00	DPM	1.1E+00	0.05	5.0E+00	1.4E-03	0.0002
4	0.0003	3.0E-07	1.00E+00	DPM	1.1E+00	0.01	5.0E+00	1.4E-03	0.0001
5	0.0001	1.0E-07	1.00E+00	DPM	1.1E+00	0.00	5.0E+00	1.4E-03	0.0000
6	0.00007	7.0E-08	1.00E+00	DPM	1.1E+00	0.00	5.0E+00	1.4E-03	0.0000
7	0.00006	6.0E-08	1.00E+00	DPM	1.1E+00	0.00	5.0E+00	1.4E-03	0.0000
8	0.00005	5.0E-08	1.00E+00	DPM	1.1E+00	0.00	5.0E+00	1.4E-03	0.0000
9	0.00007	7.0E-08	1.00E+00	DPM	1.1E+00	0.00	5.0E+00	1.4E-03	0.0000
10	0.00011	1.1E-07	1.00E+00	DPM	1.1E+00	0.00	5.0E+00	1.4E-03	0.0000

Notes:

OEHHA 95th percentile Exposure factors used to calculate TAC intake

Exposure Frequency (days/year)	350
Exposure Duration (years)	14
Daily Breathing Rate	261
Age Sensitivity Factor	1
Fraction of Time At Home (FAH)	0.73
Averaging Time _(cancer) (days)	25550
Averaging Time _(non-cancer) (days)	5110

E= 10^x, i.e. E-02 = 10⁻²

Table 9
Cumulative Carcinogenic Risk 30.25-Year Exposure Scenario

Receptor ID	Cumulative RISK (per million)
1	0.37
2	0.46
3	0.93
4	0.24
5	0.09
6	0.05
7	0.06
8	0.04
9	0.06
10	0.13

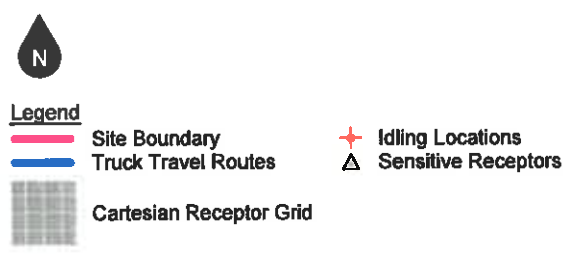


Figure 3
AERMOD Model Source and Receptor Placement

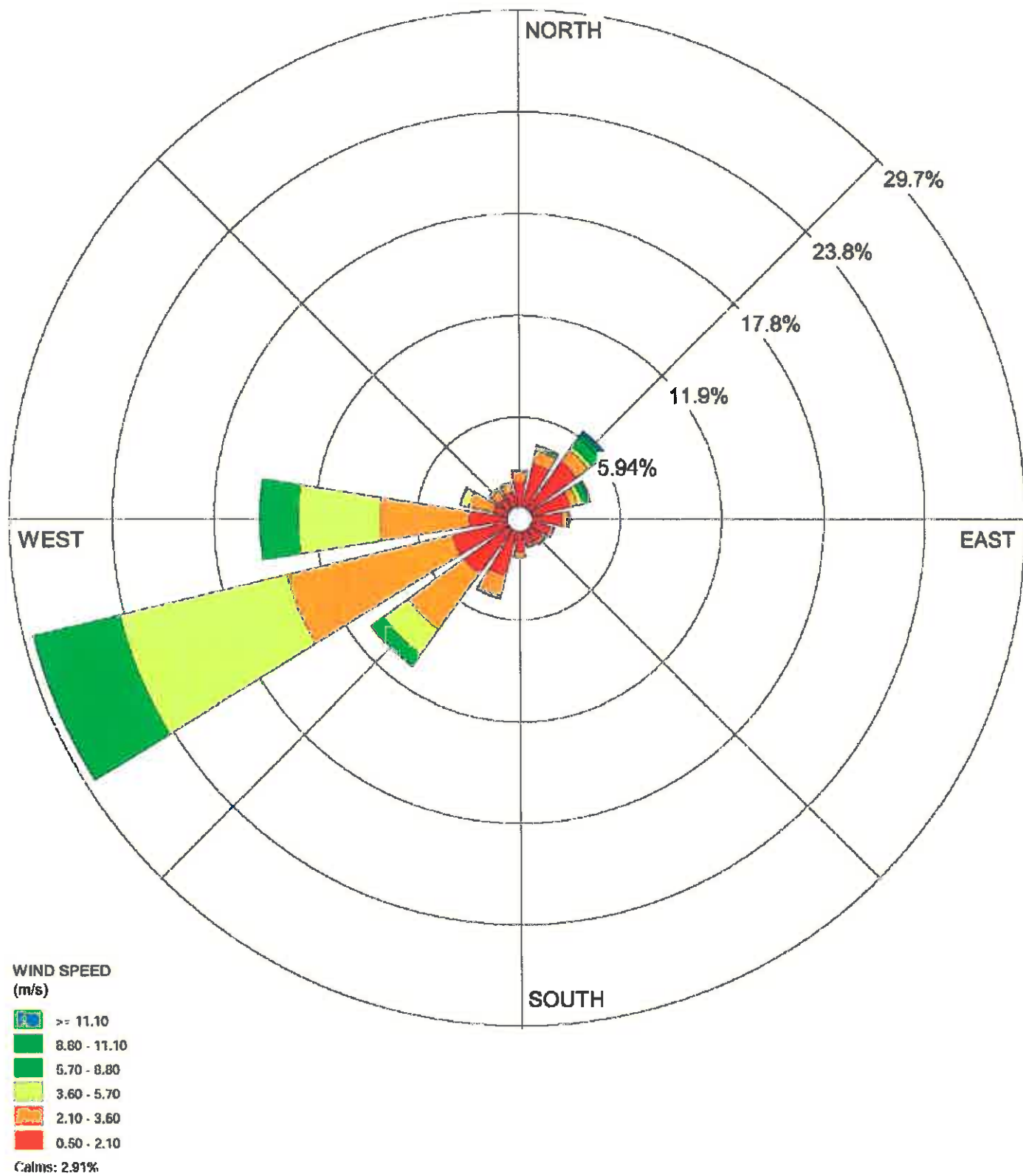
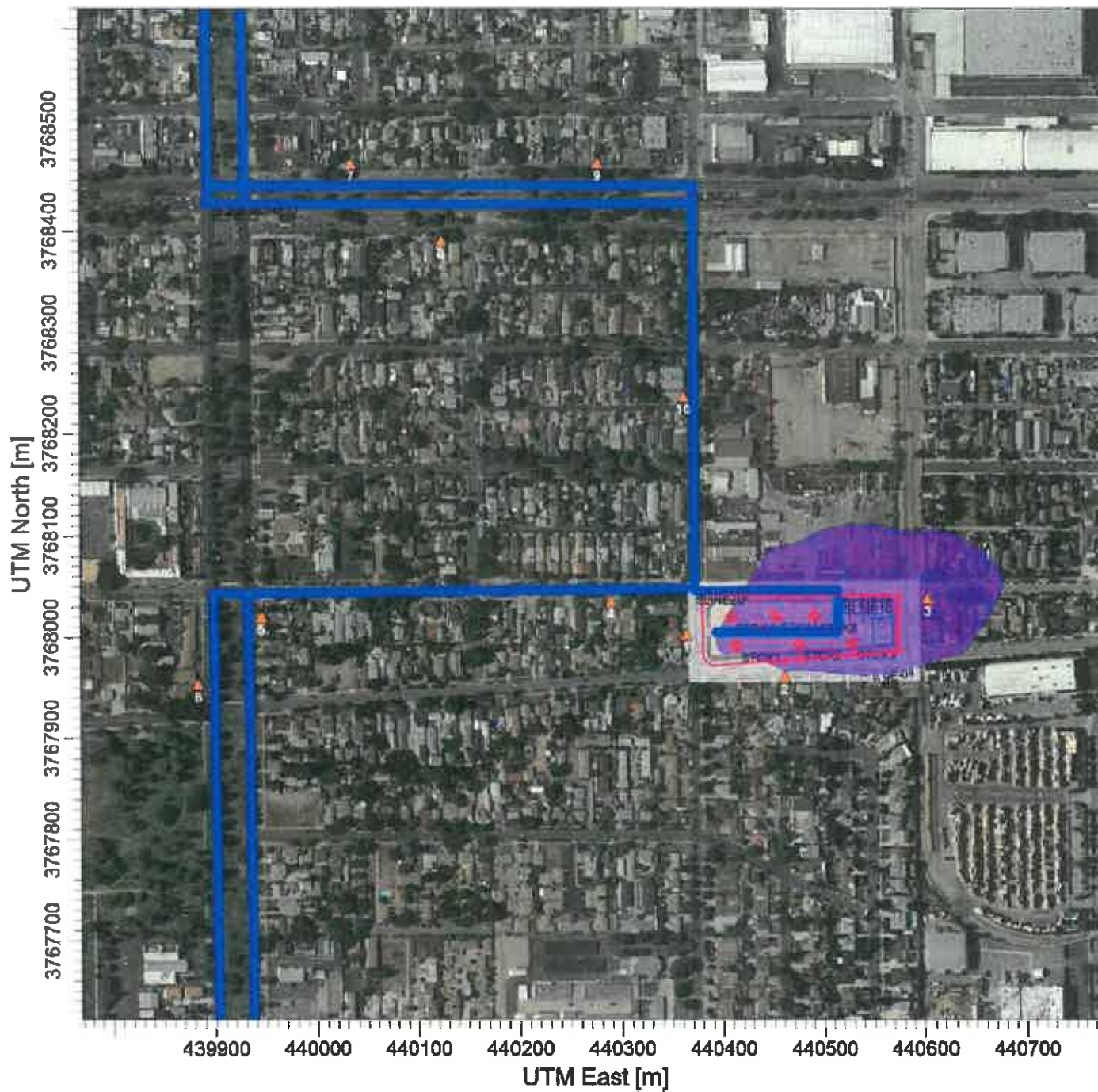


Figure 4
Wind Rose: Ontario International Airport



Legend

- Cancer Risk**
- 1 in a million
 - 0.75 in a million
 - 0.5 in a million
 - 0.25 in a million

Figure 5
Modeled Study Area Highest Annual DPM Emissions

4. MITIGATION MEASURES

OPERATIONAL MEASURES

Health risk impacts are less than significant. No operational mitigation is required.

5. REFERENCES

California Air Pollution Control Officers Association

2009 Health Risk Assessments for Proposed Land Use Projects

California Air Resources Board

2008 Resolution 08-43

2008 Airborne Toxic Control Measure for in-use Diesel-Fueled Transport Refrigeration Units (TRU) and TRU Generator Sets, Section 2477 of Division 3, Chapter 9, Title 13, California Code of Regulations

2008 ARB Recommended Interim Risk Management Policy for Inhalation-Based Residential Cancer Risk – Frequently Asked Questions

2013 Almanac of Emissions and Air Quality.
Source: <https://www.arb.ca.gov/aqd/almanac/almanac13/almanac13.htm>

Ganddini Group, Inc.

2021 500 Belmont Tow Yard Trip Generation Analysis. February 4.

Office of Environmental Health Hazard Assessment

2015 Air Toxics Hot Spots Program Risk Assessment Guidelines

Ontario, City of

2016 The Ontario Plan Figure M-5 Truck Routes, Revised September 20.

South Coast Air Quality Management District

2003 Health Risk Assessment Guidance for Analyzing Cancer Risks from Mobile Source Diesel Idling Emissions for CEQA Air Quality Analysis

2015 Final MATES-IV Multiple Air Toxics Exposure Study in the South Coast Air Basin. May.

U.S. Geological Survey

2011 Reported Historic Asbestos Mines, Historic Asbestos Prospects, and Other Natural Occurrences of Asbestos in California

APPENDICES

Appendix A Glossary

Appendix B AERMOD Model Printout

APPENDIX A

GLOSSARY

AQMP	Air Quality Management Plan
BACT	Best Available Control Technologies
CAAQS	California Ambient Air Quality Standards
CalEPA	California Environmental Protection Agency
CARB	California Air Resources Board
CCAA	California Clean Air Act
CCAR	California Climate Action Registry
CEQA	California Environmental Quality Act
CFCs	Chlorofluorocarbons
CH ₄	Methane
CNG	Compressed natural gas
CO	Carbon monoxide
CO ₂	Carbon dioxide
CO _{2e}	Carbon dioxide equivalent
DPM	East Kern Air Pollution Control District
EKAPCD	Diesel particulate matter
EPA	U.S. Environmental Protection Agency
GHG	Greenhouse gas
GWP	Global warming potential
HIDPM	Hazard Index Diesel Particulate Matter
HFCs	Hydrofluorocarbons
IPCC	International Panel on Climate Change
LCFS	Low Carbon Fuel Standard
LST	Localized Significant Thresholds
MTCO _{2e}	Metric tons of carbon dioxide equivalent
MMTCO _{2e}	Million metric tons of carbon dioxide equivalent
MPO	Metropolitan Planning Organization
NAAQS	National Ambient Air Quality Standards
NO _x	Nitrogen Oxides
NO ₂	Nitrogen dioxide
N ₂ O	Nitrous oxide
OEHHA	Office of Environmental Health Hazard Assessment
O ₃	Ozone
OPR	Governor's Office of Planning and Research
PFCs	Perfluorocarbons
PM	Particle matter
PM ₁₀	Particles that are less than 10 micrometers in diameter
PM _{2.5}	Particles that are less than 2.5 micrometers in diameter
PMI	Point of maximum impact
PPM	Parts per million
PPB	Parts per billion
SF ₆	Sulfur hexafluoride
SIP	State Implementation Plan
SJVAPCD	San Joaquin Valley Air Pollution Control District
SO _x	Sulfur Oxides
TAC	Toxic air contaminants
VOC	Volatile organic compounds

APPENDIX B
AERMOD MODEL PRINTOUT

Please Note:

Appendix B, AERMOD Model Printout is available at the Planning Department counter.

ATTACHMENT B:
NOISE ASSESSMENT

(Document follows this page)



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July 6, 2020
Project No. 20-09388

Pepe's Towing Service
Attn: Lorenzo Navarro
14351 Veterans Way
Moreno Valley, CA 92553

**Subject: Noise Assessment for Pepe's Towing Service Ontario Towing Yard Project
550 East Belmont Street, Ontario, California 91761**

Dear Mr. Navarro:

This Noise Memorandum summarizes the results of the noise analysis for the Pepe's Towing Service Ontario Towing Yard Project (herein referred to as "proposed project" or "project"). The project involves construction of a 6,600-square foot industrial building and towing yard facility on a 3.01-acre project site at 550 East Belmont Street (Assessor's Parcel Numbers 1049-491-01, -02, and -03) in the city of Ontario. The towing yard facility would include a truck wash area where employees would hand wash trucks with hoses and hand cloths; no stationary car wash equipment or drying fans are proposed. The project would include an area to unload or onload towed vehicles. The project would also include an approximately eight-foot-high concrete masonry unit (CMU) wall that would surround the majority of the project site with the exception of access driveways, the employee and visitor parking area, and the industrial building. See Attachment 1 for the project site plans. It is our understanding that the City of Ontario (City) has requested a noise assessment for the truck wash area in accordance with the City's exterior noise standards in its project review comments dated February 18, 2020. In addition, per follow up City comments, the City has requested an analysis of noise generated during typical tow-related activities (i.e., unloading of a towed vehicle). As detailed in the analysis below, the project would be consistent with the City's exterior and interior noise standards pursuant to Ontario Municipal Code (OMC) Sections 5-29.04 and 5-29.05.

Noise Overview

Sound is a vibratory disturbance created by a moving or vibrating source, which is capable of being detected by the hearing organs (e.g., the human ear). Noise is defined as sound that is loud, unpleasant, unexpected, or undesired and may therefore be classified as a more specific group of sounds. The effects of noise on people can include general annoyance, interference with speech communication, sleep disturbance, and, in the extreme, hearing impairment.¹

Noise levels are commonly measured in decibels (dB) using the A-weighted sound pressure level (dBA). The A-weighting scale is an adjustment to the actual sound pressure levels so that they are consistent with the human hearing response, which is most sensitive to frequencies around 4,000 Hertz (Hz) and

¹ Crocker, Malcolm J. (Editor). 2007. Handbook of Noise and Vibration Control Book, ISBN: 978-0-471-39599-7, Wiley-VCH, October.



less sensitive to frequencies around and below 100 Hz.² Decibels are measured on a logarithmic scale that quantifies sound intensity in a manner similar to the Richter scale used to measure earthquake magnitudes. A doubling of the energy of a noise source, such as a doubling of traffic volume, would increase the noise level by 3 dB; similarly, dividing the energy in half would result in a decrease of 3 dB.³

Human perception of noise has no simple correlation with acoustical energy. The perception of noise is not linear in terms of dBA or in terms of acoustical energy. Two equivalent noise sources combined do not sound twice as loud as one source. It is widely accepted that the average healthy ear can barely perceive changes of 3 dBA, increase or decrease; that a change of 5 dBA is readily perceptible; and that an increase (decrease) of 10 dBA sounds twice (half) as loud.⁴

The impact of noise is not a function of loudness alone. The time of day when noise occurs and the duration of the noise are also important. In addition, most noise that lasts for more than a few seconds is variable in its intensity. Consequently, a variety of noise descriptors has been developed. The noise descriptors used for this analysis are the one-hour equivalent noise level ($L_{eq(1h)}$) and the community noise equivalent level (CNEL).

- The L_{eq} is the level of a steady sound that, in a stated time period and at a stated location, has the same A-weighted sound energy as the time-varying sound. For example, $L_{eq(1h)}$ is the equivalent noise level over a 1-hour period and is a common metric for limiting nuisance noise.
- The CNEL is a 24-hour equivalent sound level. The CNEL calculation applies an additional 5 dBA penalty to noise occurring during evening hours (i.e., 7:00 p.m. to 10:00 p.m.) and an additional 10 dBA penalty is added to noise occurring during nighttime hours (i.e., 10:00 p.m. to 7:00 a.m.). These increases for certain times are intended to account for the added sensitivity of humans to noise during the evening and nighttime periods.

Sound from a small, localized source (approximating a “point” source) decreases or drops off at a rate of 6 dBA for each doubling of the distance from the source.

Existing Noise Setting

The primary sources of noise in the project site vicinity are vehicular traffic on Phillips Street and railroad operations on the Union Pacific Railroad track that runs parallel to South Monterey Avenue. Figure S-3a of the Ontario Policy Plan Safety Element *Future Roadway Noise Contours* map identifies ambient traffic noise levels at the project site and surrounding area to be between 60 and 65 CNEL.⁵ In addition, Map 2-3, *Compatibility Policy Map: Noise Impact Zones*, of the Los Angeles/Ontario International Airport Land Use Compatibility Plan identifies airport noise levels at the project site and surrounding area to be between 65 and 70 CNEL.^{6, 7} These noise level ranges may be a conservative underestimate of noise

² Kinsler, Lawrence E., Austin R. Frey, Alan B. Coppens, and James V. Sanders. 1999. *Fundamentals of Acoustics*, 4th Edition. ISBN 0-471-84789-5. Wiley-VCH, December 1999.

³ Crocker, Malcolm J. (Editor). 2007. *Handbook of Noise and Vibration Control Book*, ISBN: 978-0-471-39599-7, Wiley-VCH, October.

⁴ California Department of Transportation (Caltrans). 2013a. *Technical Noise Supplement to the Traffic Noise Analysis Protocol*. (CT-HWANP-RT-13-069.25.2) September. http://www.dot.ca.gov/hq/env/noise/pub/TeNS_Sept_2013B.pdf (accessed August 2019).

⁵ Ontario, City of. 2019. *City of Ontario Policy Plan*. Adopted January 27, 2010. Last amended July 16, 2019. <http://www.ontarioplan.org/policy-plan/> (accessed March 2020).

⁶ Ontario, City of. 2011. *Ontario International Airport Land Use Compatibility Plan*. Adopted April 19, 2011. <http://www.ontarioplan.org/alucp-for-ontario-international-airport> (accessed March 2020).



levels at the project site and surrounding area because they do not include noise generated by rail operations on the Union Pacific Railroad track.

The City of Ontario Policy Plan and OMC do not define noise-sensitive land uses. However, the most stringent noise standards in OMC Sections 5-29.05, 5-29.05, and 5-29.11 and Ontario Policy Plan Table LU-7 are applied to single-family and multi-family residential land uses, mobile home parks, schools, day care centers, hospitals and similar health care institutions, churches, libraries, museums, auditoriums, concert halls, outdoor music shells, and amphitheatres.⁸ Therefore, for the purposes of this analysis, these land uses are considered to be noise-sensitive land uses. The nearest noise-sensitive land uses to the project site are single-family residences located to the west, south, and east. Other noise-sensitive land uses within 0.25 mile include Friendship Missionary Baptist Church (approximately 460 feet to the west), Delgado Family Child Care (approximately 530 feet to the northeast), Church of God of Prophecy (approximately 600 feet to the east), Love Chapel Church (approximately 760 feet to the west), De Anza Middle School (approximately 900 feet to the south), and Hope Chapel (approximately 1,200 feet to the northwest).

City of Ontario Policy Plan

The following goal and policy of the City of Ontario Policy Plan Safety Element would be applicable to the proposed project:⁹

Goal S4 An environment where noise does not adversely affect the public's health, safety, and welfare.

Policy S4-1 Noise Mitigation. We utilize the City's Noise Ordinance, building codes and subdivision and development codes to mitigate noise impacts.

In addition, Table LU-7 of the City of Ontario Policy Plan Land Use Element includes noise/land use compatibility guidelines, which are reproduced herein as Figure 1.

⁷ Due to the Executive Order N-33-20, which was in effect at the time of this study, noise measurements were not taken at the project site because this activity does not fall under a critical infrastructure sector and therefore Rincon staff were not exempt from stay-at-home requirements.

⁸ Ontario, City of. 2019. City of Ontario Policy Plan. Adopted January 27, 2010. Last amended July 16, 2019. <http://www.ontarioplan.org/policy-plan/> (accessed March 2020).

⁹ Ontario, City of. 2019. City of Ontario Policy Plan. Adopted January 27, 2010. Last amended July 16, 2019. <http://www.ontarioplan.org/policy-plan/> (accessed March 2020).



Figure 1 Noise Level Exposure and Land Use Compatibility Guidelines

LAND USE CATEGORIES		COMMUNITY NOISE EQUIVALENT LEVEL (CNEL)					
Category	Land Use	55	60	65	70	75	80
Residential/ Lodging	Single Family / Duplex	Green	Green	Yellow	Orange	Red	Red
	Multi-Family	Green	Green	Yellow	Orange	Red	Red
	Mobile Homes	Green	Green	Yellow	Red	Red	Red
	Hotel/Motels	Green	Green	Yellow	Orange	Orange	Red
Public/Institutional	Schools/Hospitals	Green	Green	Yellow	Orange	Red	Red
	Churches/ Libraries	Green	Green	Yellow	Orange	Red	Red
	Auditoriums/Concert Halls	Green	Yellow	Orange	Orange	Red	Red
Commercial	Offices	Green	Green	Yellow	Yellow	Orange	Red
	Retail	Green	Green	Green	Yellow	Orange	Red
Industrial	Manufacturing	Green	Green	Green	Yellow	Orange	Orange
	Warehousing	Green	Green	Green	Yellow	Yellow	Orange
Recreational/ Open Space	Parks/Playgrounds	Green	Green	Yellow	Orange	Red	Red
	Golf Courses/ Riding Stables	Green	Green	Yellow	Orange	Red	Red
	Outdoor Spectator Sports	Green	Green	Yellow	Orange	Red	Red
	Outdoor Music Shells/ Amphitheaters	Yellow	Yellow	Orange	Red	Red	Red
	Livestock/Wildlife Preserves	Green	Green	Green	Green	Orange	Red
	Crop Agriculture	Green	Green	Green	Green	Green	Green

LEGEND

	Clearly Acceptable:	No special noise insulation required, assuming buildings of normal conventional construction.
	Normally Acceptable:	Acoustical reports will be required for major new residential construction. Conventional construction with closed windows and fresh air supply systems of air conditioning will normally suffice.
	Normally Unacceptable:	New construction should be discouraged. Noise/aviation easements required for all new construction. If new construction does proceed, a detailed analysis of noise reduction requirements must be made and necessary noise insulation features included.
	Clearly Unacceptable:	No new construction should be permitted.

Source: Table LU-7 of the City of Ontario Policy Plan Land Use Element



City of Ontario Municipal Code

The City's Noise Ordinance is codified as OMC Chapter 29. OMC Section 5-29.04 states:

- a. The following exterior noise standards (reproduced herein as Table 1), unless otherwise specifically indicated, shall apply to all properties within a designated noise zone.
- b. It is unlawful for any person at any location within the incorporated area of the City to create noise, or to allow the creation of any noise on property owned, leased, occupied or otherwise controlled by such person, which noise causes the noise level, when measured at any location on any other property, to exceed either of the following:
 - 1. The noise standard for the applicable zone for any 15-minute period; and
 - 2. A maximum instantaneous (single instance) noise level equal to the value of the noise standard plus 20 dBA for any period of time (measured using A-weighted slow response).
 - a. In the event the ambient noise level exceeds the noise standard, the maximum allowable noise level under such category shall be increased to reflect the maximum ambient noise level.
 - b. The Noise Zone IV standard shall apply to that portion of residential property falling within 100 feet of a commercial property or use, if the noise originates from that commercial property or use.
 - c. If the measurement location is on a boundary between two different noise zones, the lower noise level standard applicable to the noise zone shall apply.

Table 1 OMC Exterior Noise Standards¹

Noise Zone	Type of Land Use	Allowable Equivalent Noise Level (dBA L _{eq}) ²	
		7:00 a.m. – 10:00 p.m.	10:00 p.m. – 7:00 a.m.
I	Single-Family Residential	65	45
II	Multi-Family Residential, Mobile Home Parks	65	50
III	Commercial Property	65	60
IV	Residential Portion of Mixed Use	70	70
V	Manufacturing and Industrial, Other Uses	70	70

¹ If the ambient noise level exceeds the resulting standard, the ambient noise level shall be the standard.

² Measurements for compliance are made on the affected property pursuant to OMC Section 5-29.15. The noise standard for the applicable zone is for any 15-minute period

dBA = A-weighted decibel; L_{eq} = equivalent noise level

Source: OMC Section 5-29.04(a)

OMC Section 5-29.05 states:

- a. The following interior noise standards (reproduced herein as Table 2), unless otherwise specifically indicated, shall apply to all properties within a designated noise zone.
- b. It is unlawful for any person at any location within the incorporated area of the City to create noise, or to allow the creation of any noise on property owned, leased, occupied or otherwise



controlled by such person, which noise causes the noise level, when measured at any location on any other property, to exceed either of the following:

1. The noise standard for the applicable zone for any 15-minute period; and
2. A maximum instantaneous (single instance) noise level equal to the value of the noise standard plus 20 dBA for any period of time (measured using A-weighted slow response).
 - a. In the event the ambient noise level exceeds the noise standard, the maximum allowable noise level under such category shall be increased to reflect the maximum ambient noise level.
 - b. The Noise Zone IV standard shall apply to that portion of residential property falling within 100 feet of a commercial property or use, if the noise originates from that commercial property or use.
 - c. If the measurement location is on a boundary between two different noise zones, the lower noise level standard applicable to the noise zone shall apply.

Table 2 OMC Interior Noise Standards¹

Noise Zone	Type of Land Use	Allowable Equivalent Noise Level (dBA L _{eq}) ²	
		7:00 a.m. – 10:00 p.m.	10:00 p.m. – 7:00 a.m.
I	Single-Family Residential	45	40
II	Multi-Family Residential, Mobile Home Parks	45	40
IV	Residential Portion of Mixed Use	45	40

¹ If the ambient noise level exceeds the resulting standard, the ambient noise level shall be the standard.

² Measurements for compliance are made on the affected property pursuant to OMC Section 5-29.15. The noise standard for the applicable zone is for any 15-minute period

dBA = A-weighted decibel; L_{eq} = equivalent noise level

Source: OMC Section 5-29.05(a)

OMC Section 5-29.11 states:

“It is unlawful for any person to create any noise that causes the outdoor noise level at any school, day care center, hospital or similar health care institution, church, library or museum while the same is in use, to exceed the noise standards specified in OMC Section 5-29.04 prescribed for the assigned Noise Zone I.”

Impact Analysis

Methodology

The proposed truck wash area to be located on the southern portion of the project site approximately 140 feet west of the intersection of Phillips Street and South Monterey Avenue. A reference noise level for the truck wash area was obtained from a noise assessment performed for a similar hand car wash in Cudworth, Barnsley, United Kingdom. The reference noise level measurement was taken for a facility comprised of two car wash bays at which vehicles are hand washed using jet sprays and hand cloths, then dried, polished, and vacuumed. Based on the noise assessment, a hand car wash generates average



noise levels of up to approximately 70 dBA L_{eq} at 25 feet.¹⁰ Use of the reference noise level from this noise assessment provides a conservative overestimate of project-generated noise levels because the project would not include the use of jet sprays or vacuums, which generate higher levels of noise than hoses and hand cloths used by the proposed project.

To capture noise levels from towed vehicle unloading and onloading, noise measurements were conducted on July 2, 2020 at a Pepe's Towing Service location at 8828 Beech Avenue in Fontana, California. These activities typically occur over a three- to four-minute period. Sources observed making noise during an unloading or onloading activity include hydraulics, chains, truck bed moving, and truck engine idling. The truck bed is tilted and the vehicle is removed from the e-brake and quietly backed off the track during unloading. These noise levels are shown in Table 3; when accounting for distance, the loudest measurement occurred during unloading for Measurement #3. This measurement was used in the impact analysis. Per OMC Section 5-29.05, the noise standards are for a 15-minute period; therefore, the three- to four-minute measurements were analyzed over a 15-minute time period (i.e., assuming three to four minutes of activity and 11 to 12 minutes of inactivity). See Attachment 2 for detailed noise measurements.

Table 3 Tow Vehicle Measurements

Measurement	Description	Distance	Time Period	dBA L_{eq}	dBA L_{eq} over 15 minutes
#1	Truck unloading	20 feet	4 minutes	66.7	60.9
#2	Truck onloading	20 feet	4 minutes	65.3	59.1
#3	Truck unloading	40 feet	3 minutes	63.9	57.4

See Attachment 2 for detailed noise measurements

dBA = A-weighted decibel; L_{eq} = equivalent noise level

The project would involve two types of work weeks during the year, as detailed below. Per this information, during the busiest week of the year there would typically be one vehicle dropped off per two hours:

- A regular work week that occurs 42 times in one calendar year.
 - **Hours of operation:** Monday thru Friday 8 a.m. to 5 p.m. Saturday and Sunday closed.
 - **Number of vehicles drop off:** Between two or three cars per day (approximately one vehicle per eight hours).
- A rotation week that occurs 10 times in one calendar year (contract with a police department)
 - **Hours of operation:** 24/7 from Monday morning thru Sunday Night until 11:59 p.m.
 - **Number of vehicles drop off:** Between 10 to 12 per each day (approximately one vehicle per two hours).

It was conservatively assumed that the vehicle drop off and the car wash would occur simultaneously in approximately the same area. Exterior noise levels at the receivers nearest to the project site were calculated using standard distance attenuation for point sources of 6 dBA per doubling of distance. The noise analysis also takes into account the presence of the proposed eight-foot-high CMU wall that would surround the majority of the project site as well as the proposed 32-foot-high industrial building to be

¹⁰ Waterman Infrastructure & Environment Limited. 2016. Noise Assessment – Hand Car Wash and Valet Centre, Cudworth, Barnsley, S72 8YA. November 2016.



located on the eastern portion of the project site. The CMU wall would block the line-of-sight between the project and residences to the south, and the industrial building would block line-of-sight between the project and residences to the east. The proposed CMU wall is calculated to provide an approximately 8-dBA reduction in ambient noise levels at receivers to the south of the project site and the proposed industrial building is calculated to provide an approximately 20-dBA reduction in ambient noise levels at receivers to the east of the project site. Barrier modeling outputs are included in Attachment 3.

According to the Federal Highway Administration, buildings provide exterior-to-interior noise level reductions ranging from 10 dBA for buildings with open windows to 35 dBA for masonry buildings with double glazed windows.¹¹ Interior noise levels at the nearest receivers to the project site were calculated using the estimated exterior noise levels and conservatively assuming an exterior-to-interior reduction of 10 dBA.

Project Impacts

Table 4 and Table 5 summarize exterior and interior noise levels, respectively, generated by the proposed project at the nearest receivers, including residences to the west, south, and east and industrial properties to the north. Noise levels for nighttime hours (i.e., 10:00 p.m. to 7:00 a.m.) did not include the truck wash area because the wash would not be in operation during those hours; towing activities would occur during the nighttime hours for approximately 10 weeks a year. As shown in Table 4 and Table 5, respectively, exterior and interior noise levels from the truck wash and vehicle unloading and onloading areas would not exceed the City's daytime exterior and interior noise level standards for the surrounding land uses, and exterior and interior noise levels from the vehicle unloading and onloading areas would not exceed the City's nighttime exterior and interior noise level standards for the surrounding land use.

¹¹ Federal Highway Administration. 2011. Highway Traffic Noise: Analysis and Abatement Guidance. December 2011. <https://www.in.gov/indot/files/FHWA-Highway-Traffic-Noise-Analysis-and-Abatement-Guidance-December-2011.pdf> (accessed March 2020).



Table 4 Exterior Noise Levels at Nearest Receivers

	Noise Level (dBA L _{eq})			
	550 Phillips Street (85 feet to the south)	1214 S. Sultana Street (460 feet to the west)	Industrial Properties (180 feet to the north)	604 S. Monterey Avenue (170 feet to the east)
Truck Wash Area ¹	59	45	53	53
Towed Vehicle Unloading ²	51	36	44	45
Combined Wash and Unloading (Daytime Only)	60	46	54	54
CMU Wall ^{3, 4}	(8)	n/a	n/a	n/a
Buildings ⁵	n/a	n/a	n/a	(20)
Estimated Noise Level - Daytime⁶	52	46	54	34
Exterior Noise Standard - Daytime ⁷	65	65	70	65
Standard Exceeded?	No	No	No	No
Estimated Noise Level - Nighttime⁸	43	36	44	25
Exterior Noise Standard - Nighttime ⁷	45	45	70	45
Standard Exceeded?	No	No	No	No

¹ Based on a reference noise level of 70 dBA L_{eq} at 25 feet and a standard distance attenuation for point sources of 6 dBA per doubling of distance.

² Based on a reference noise level of 57.4 dBA L_{eq} at 40 feet and a standard distance attenuation for point sources of 6 dBA per doubling of distance.

³ See Attachment 3 for barrier modeling results.

⁴ The CMU wall would only partially block the line of sight between the project area and residences to the west/industrial properties to the north; therefore, noise reductions from this wall are conservatively not included in the estimate of noise levels at these properties.

⁵ The proposed industrial building would block the line of sight between the project area and residences to the east along South Monterey Avenue. See Attachment 3 for barrier modeling results.

⁶ Daytime noise includes a conservative assumption of the truck wash and unloading of a towed vehicle occurring at the same time in approximately the same location.

⁷ See Table 1.

⁸ Nighttime noise includes unloading of a towed vehicle.

() denotes a negative number.

dBA = A-weighted decibel; L_{eq} = equivalent noise level; n/a = not applicable



Table 5 Interior Noise Levels at Nearest Receivers

	Noise Level (dBA L _{eq})			
	550 Phillips Street (85 feet to the south)	1214 S. Sultana Street (460 feet to the west)	Industrial Properties (180 feet to the north)	604 S. Monterey Avenue (170 feet to the east)
Truck Wash Area ¹	59	45	53	53
Towed Vehicle Unloading ²	51	36	44	45
Combined Wash and Unloading (Daytime Only)	60	46	54	54
CMU Wall ^{3, 4}	(8)	n/a	n/a	n/a
Buildings ⁵	n/a	n/a	n/a	(20)
Interior-to-Exterior Reduction	(10)	(10)	(10)	(10)
Estimated Noise Level - Daytime⁶	42	36	44	24
Interior Noise Standard ⁷	45	45	n/a	45
Standard Exceeded?	No	No	No	No
Estimated Noise Level - Nighttime⁸	33	26	34	15
Interior Noise Standard-Nighttime ⁷	40	40	n/a	40
Standard Exceeded?	No	No	No	No

¹ Based on a reference noise level of 70 dBA L_{eq} at 25 feet and a standard distance attenuation for point sources of 6 dBA per doubling of distance.

² Based on a reference noise level of 57.4 dBA L_{eq} at 40 feet and a standard distance attenuation for point sources of 6 dBA per doubling of distance.

³ See Attachment 3 for barrier modeling results.

⁴ The CMU wall would only partially block the line of sight between the project area and residences to the west/industrial properties to the north; therefore, noise reductions from this wall are conservatively not included in the estimate of noise levels at these properties.

⁵ The proposed industrial building would block the line of sight between the project area and residences to the east along South Monterey Avenue. See Attachment 3 for barrier modeling results.

⁶ Daytime noise includes a conservative assumption of the truck wash and unloading of a towed vehicle occurring at the same time in approximately the same location

⁷ See Table 2. The OMC does not establish interior noise standards for industrial properties.

⁸ Nighttime noise includes unloading of a towed vehicle.

() denotes a negative number.

dBA = A-weighted decibel; L_{eq} = equivalent noise level; n/a = not applicable



Conclusion

Noise levels generated by the project's truck wash area and towing activities would not conflict with the City of Ontario's exterior and interior noise standards.

Thank you for the opportunity to assist with this assignment. Please do not hesitate to contact us if you have questions about this report.

Sincerely,
Rincon Consultants, Inc.

Annaliese Miller

Annaliese Miller
Associate Environmental Planner
(805) 644-4455 ext. 429
amiller@rinconconsultants.com

Joe Power

Joe Power
Principal/Sr. Vice President
(805) 644-4455
ipower@rinconconsultants.com

Attachments

- | | |
|--------------|-------------------------------------|
| Attachment 1 | Project Site Plans |
| Attachment 2 | Towing Activities Noise Measurement |
| Attachment 3 | Barrier Modeling Results |

Attachment 1

Project Site Plans

PEPE'S TOWING COMPANY

500 E. BELMONT ST. ONTARIO, CA

STRUCTURAL ENGINEER:
RICHARD HERMAN
STRUCTURAL ENGINEER
4854 MAIN ST
YORBA LINDA, CA 92886
714-903-1073
RHERMAN@RHERMANSE.COM

GENERAL NOTES

PROPRIETARY NOTICE

The designs, drawings and/or written materials herein constitute the original and unpublished property of THE HOWARD PARSELL COMPANY. Said materials may not be reproduced, utilized or disclosed without written permission of THE HOWARD PARSELL COMPANY. Actions contrary to the aforementioned may constitute legal recourse.

GENERAL NOTES

- The following notes and typical details apply to all drawings unless noted otherwise.
- The Architect/Engineer assumes no responsibility for the completeness of plans for bid purposes prior to issuance of building permit.
- The intent of these drawings and specifications is to include all labor, materials and services necessary for the completion of all work shown, prescribed or reasonably implied, but not limited to that explicitly indicated in the contract documents.
- All construction and workmanship shall conform to:
 - 2016 CALIFORNIA BUILDING CODE
 - 2016 CALIFORNIA MECHANICAL CODE
 - 2016 CALIFORNIA PLUMBING CODE
 - 2016 CALIFORNIA ELECTRICAL CODE
 - 2016 CALIFORNIA ENERGY CODES (2016 IBC, 2016 UMC, 2016 UPC AND 2016 NEC AS AMENDED BY THE STATE OF CALIFORNIA)
 - 2016 CALIFORNIA GREEN BUILDING CODE
- Dimensions shown in figures take precedence over dimensions scaled from drawings. Large scale drawings take precedence over smaller scale drawings. Any discrepancy or conflict of dimensions shall be brought to the attention of the Architect/Engineer by the contractor prior to implementation.
- Notes and details on drawings shall take precedence over these general notes.
- Unless otherwise noted or indicated all dimensions on the plans shall be from centerline of column, face of concrete, or face of stud (for exterior walls), centerline of stud wall (for interior walls) or face of mullions.
- The General Contractor and all subcontractors shall verify all dimensions and conditions on the job site prior to beginning of construction and report any discrepancies to the Architect/Engineer.
- The Architect/Engineer's approval of shop drawings shall not relieve the General Contractor from responsibility for deviations from drawings or specifications unless he has (in writing) called the Architect/Engineer's attention to such deviations at the time of submission, nor shall it relieve him of responsibility for errors of any sort in the shop drawings.
- For sizes, locations and specific details of Mechanical, Plumbing and Electrical equipment, refer to appropriate sub-contractor drawings and specifications.

CONSTRUCTION RESPONSIBILITY

- The contractor shall have sole responsibility for site safety. Any fabricator and erector shall review the contract documents and if the structure, as shown on those documents, is in conflict with the requirements of any safety regulation, the fabricator shall notify the Structural Engineer of Record (EOR) prior to commencing shop drawing production. If the fabricator and/or erector fail to notify EOR, as stated above, they shall become responsible for all costs for correcting such conflicts with the requirements of any and all safety regulations.
- It shall be the responsibility of the contractor to locate all existing utilities whether shown hereon or not and to protect them from damage. The contractor shall bear all expense of repair or replacement in conjunction with the execution of this work.
- The Contract Drawings and specifications represent the finished structure. Unless otherwise shown, they do not indicate the method of construction. The Contractor shall supervise and direct the work and shall be solely responsible for all construction means, methods, techniques, sequences and procedures. Observation visits to the site by field representatives of the Architect/Engineer shall not include inspections of the protective measures or the construction procedures required for same, which are furnished by others. These support services performed by the Architect/Engineer, whether for material or work, and whether performed prior to, during, or after completion of construction are performed solely for the purpose of assisting in quality control and in achieving conformance with the Contract Drawings and Specifications, but they do not guarantee Contractor performance and shall not be construed as supervision of construction.
- All drawings, specifications and other work product of the Architect/Engineer for this project are instruments of service for this project only and shall remain the property of the Architect/Engineer whether the project is completed or not. Reuse of any of the instruments of service of the Architect/Engineer by the Owner/Client on extensions of this project or on any project without the written permission of the Architect/Engineer shall be at the Owners/Clients own risk, and the Owner/Client agrees to defend, indemnify and hold harmless the Architect/Engineer from all claims, damages, and expenses including attorneys fees arising out of such unauthorized reuse of the Architects/Engineers instruments of service by the Owner/Client or by others acting through the Owner/Client.
- The Contractor agrees that he shall assume sole and complete responsibility for job site conditions during the course of construction of this entire project, including safety of all persons and property; that these requirements shall apply continuously and not be limited to normal working hours; and that the Owner/Client(s), Designer(s), Architect(s), Engineer(s) and Surveyor(s) are harmless from any and all liability during the course of construction.
- ALL trades and contractors shall review ALL sheets of these drawings as well as any shop drawings provided by others for CONFLICT WITH OR APPLIANCES TO THEIR TRADE. COSTS ARISING FROM FAILURE TO DO THIS SHALL BE BORN BY THE CONTRACTOR AT FAULT. IT SHALL BE THE RESPONSIBILITY OF THE GENERAL CONTRACTOR to distribute copies of drawings to the sub-contractors and see that they comply with the above (in particular: Notes and dimensions, etc. on one sheet may effect the work shown on another sheet, but may not be repeated on the other sheet. Therefore it is essential that all contractors are familiar with all sheets of all drawings).

- The design, adequacy & safety of erection bracing, shoring, temporary supports, form etc., is the sole responsibility of the contractor, and has not been considered by the Structural Engineer. The contractor is responsible for means and methods of construction to insure the stability of the structure prior to the application of all walls, roof & floor sheathing and finish materials. He shall provide the necessary bracing to provide stability prior to the application of the aforementioned materials. The contractor is responsible for compliance with OSHA requirements including, but not limited to, the addition of anchor bolts and/or temporary bracing to insure column stability during construction. Observation visits to the site by the Structural Engineer shall not include inspection of the above items.

SITE

- The Contractor, at his own expense, shall keep the project area and surrounding area free from dust nuisance. The work shall be in conformance with the Air Pollution Control Standards and regulations of the State Department of Health.
- If existing facilities are related to the work, the Contractor shall not disrupt the existing services or utilities without obtaining Owners/Clients prior approval and instructions in each case.
- All grading operations shall be performed in conformance with the applicable provisions of the water quality standards contained in the Public Health Regulations, State Department of Health on water pollution control and water quality standards.
- The Contractor shall furnish adequate shoring, bracing, barricades and protective measures, etc., as required to safely execute all work, and shall be fully responsible for same.
- The Contractor, at his own expense, shall keep the project area and surrounding Construction debris and wastes shall be deposited at an appropriate site, the Contractor shall inform the Architect/Engineer of the location of disposal sites. The disposal site must also fulfill the requirements of the grading ordinances.
- This project shall comply with CBC Section 3306. The Contractor shall ensure that all pedestrians are protected during construction, remodeling, and demolition activities as required by CBC Section 3306 and Table 3306.1. Signs shall be provided to direct pedestrian traffic.

FINISH WORK

- ALL REQUIRED PERMITS MUST BE OBTAINED FROM THE FIRE PREVENTION BUREAU BEFORE THE BUILDING IS OCCUPIED.
- Unless noted otherwise, all surfaces to be painted shall receive three (3) coats.

TRADE WORK

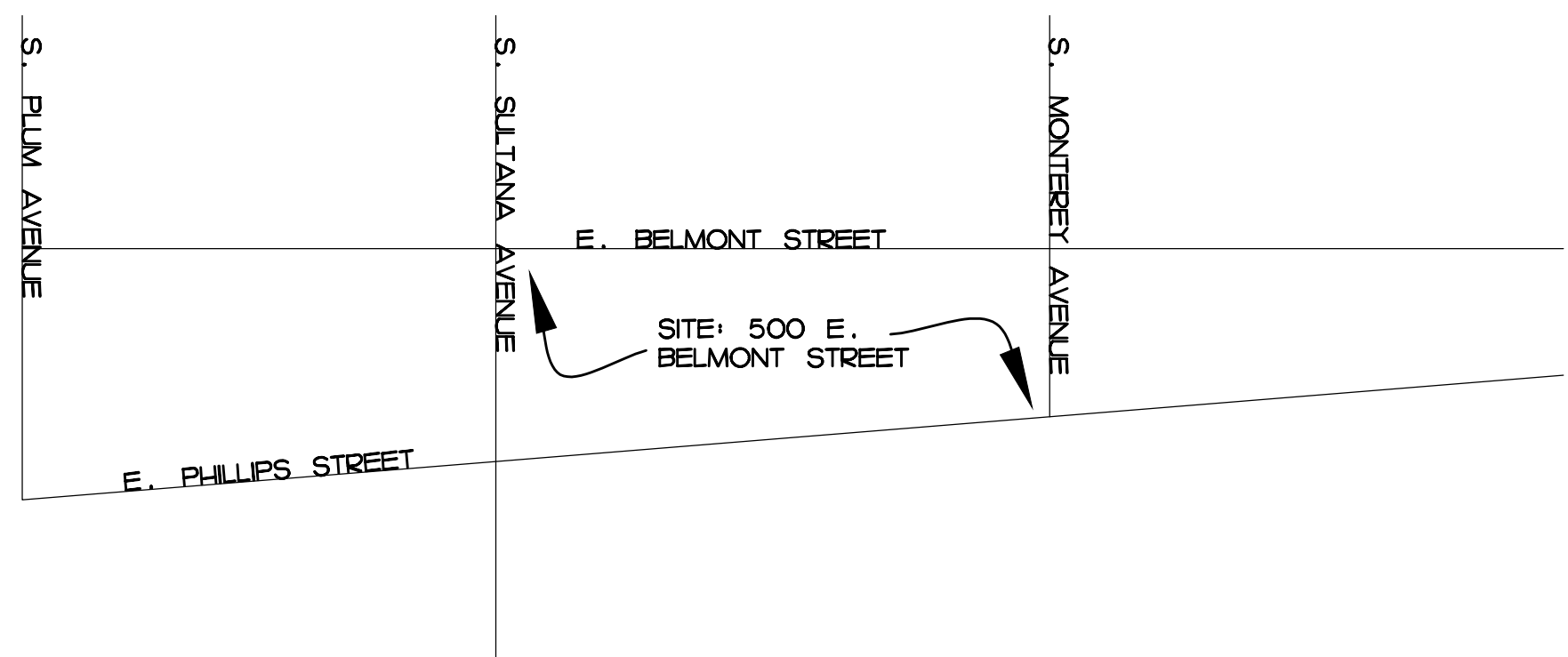
- The Contractor shall verify sizes and locations of all mechanical, electrical, plumbing and specialty equipment before construction of cabinets.
- The Contractor shall insure that all under-floor rough plumbing and/or mechanical, electrical and specialty equipment installations are in place prior to placement of concrete slab floors.
- The Contractor shall verify locations of all openings and pads required with Mechanical, Electrical, Plumbing or Specialty equipment manufacturer's data sheets and plans.
- The Contractor shall provide and locate access panels as required after installation of mechanical ducts, plumbing and electrical work.
- The Contractor shall provide all necessary blocking and backing framing for light fixtures, electrical units and A/C equipment.

AMERICANS WITH DISABILITIES ACT (ADA)

With specific respect to design requirements of the AMERICANS WITH DISABILITIES ACT OF 1990 (ADA) or certified state or local accessibility regulations, Owner/Client agrees to waive any action against the Design Professional, and to indemnify and defend the Design Professional against any claim arising from Design Professional's alleged failure to meet ADA design requirements prescribed. Owner/Client understands that interpretation of ADA is a legal issue and not a design issue, therefore retention of legal counsel for purposes of interposition is advisable.

CALIFORNIA ACCESSIBILITY STANDARDS

THE CALIFORNIA BUILDING CODE ACCESSIBILITY STANDARDS ARE SPECIFIED AND ILLUSTRATED ON SHEETS REFERENCED AS DA1, DA2, ETC., AND ARE NOT NECESSARILY NOTED ELSEWHERE.
The Contractor is responsible for reviewing these specifications and incorporating them into this project where applicable.



SITE VICINITY MAP 500 E. BELMONT ST.

SCALE: NOT TO SCALE

INDEX TO DRAWINGS

- 1 COVER SHEET, GENERAL NOTES
- 2 STRUCTURAL SPECIFICATIONS & NOTES
- SP1 SITE PLAN
- SUP1 SITE UTILIZATION PLAN
- SUP2 SITE UTILIZATION PLAN CONT.
- G1 PRELIMINARY GRADING PLAN
- G2 PRELIMINARY GRADING PLAN CONT.
- L COLORED LANDSCAPE PLAN
- L1 PRELIMINARY LANDSCAPE PLAN
- L2 PRELIMINARY LANDSCAPE PLAN
- L3 TREE INVENTORY PLAN
- A1 FLOOR PLAN
- A2 SECOND FLOOR FRAMING PLAN
- A3 CANOPY PLAN
- A4 FUEL AREA - FLAG POLE
- A5 NORTH/SOUTH ELEVATIONS
- A6 EAST/WEST ELEVATIONS
- A7 FENCE/BLOCK WALL ELEVATIONS
- A8 CANOPY ELEVATIONS
- A9 SECTIONS
- A10 ROOF PLAN
- S1 FOUNDATION PLAN
- D1 DETAIL SHEET

SITE SUMMARY:

APN: 104-491-01,02,03
SITE AREA: 111,963SF=2.57AC

PROP BLDG AREA: 5460 SF 1st FLOOR
1200 SF 2nd FLOOR (OFFICES)
6660 SF TOTAL

PROP TRUCK WASH: 800 SF

FLOOR AREA RATIO: $\frac{800+5460}{111,963} = 5.6\%$

LANDSCAPE AREA: 15,500 SF
PAVED AREA: 90,600 SF

LANDSCAPE COVERAGE RATIO: $\frac{15,500}{111,963} = 13.8\%$

TYPE OF CONSTRUCTION: V
OCCUPANCY: B,S2

PARKING NEEDED: 12 STALLS
PARKING PROVIDED: 21 STALLS

OWNER:

MANUEL ACOSTA
810 E. MAIN STREET
ONTARIO, CA

NOTES:

PRE-ENGINEERED METAL BUILDING COMPANY TO PROVIDE FULL ENGINEERING AND CONSTRUCTION DRAWINGS.

SECOND FLOOR TO BE INTEGRATED AND ENGINEERED IN TO STEEL FRAMING BY METAL BUILDING COMPANY.

MEP SHALL BE DESIGN BUILD.

CONTRACTOR:

PROJECT NAME:

New Development For:

Pepe's Towing Co.

500 East Belmont Street
Ontario, California

SEAL / SIGNATURE



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COVER GENERAL NOTES

DATE: 07/31/19

PROJECT NO: 19-006

CONTRACTOR:

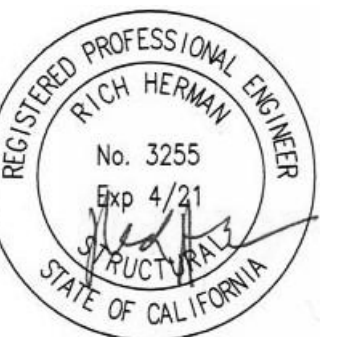
PROJECT NAME:

New Development For:

Pepe's Towing Co.

500 East Belmont Street
Ontario, California

SEAL / SIGNATURE



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PROPERTY OWNERS
WITHIN 300' RADIUS

DATE: 07/31/19

PROJECT NO: 19-006

SUP2

1049-491-01 300ft. Radius, 1/4/2018, Page 1-1

Parcel #*	Owner's Name*	#	Dir.	Street	Type	City	St.	Zip
10495117	MC KERNAN FAMILY LIMITED PTRNSHP	5390000	E	BELMONT	ST	ONTARIO	CA	91761
104950104	SILVA, ALICE	1115	E	SULTANA	AVE	ONTARIO	CA	91761
104951417	MORALES, FELIPE D J L	427	E	BELMONT	ST	ONTARIO	CA	91761
104951420	TORRES, JAVIER	419	E	BELMONT	ST	ONTARIO	CA	91761
104951421	LAY, NAT SOK	413	E	BELMONT	ST	ONTARIO	CA	91761
104950101	SILVA, ALICE	0						00000
104952214	RIVAS, DOROTEO R TR	0						00000
104952216	MONTERO, SANDRA	407	E	PHILLIPS	ST	ONTARIO	CA	91761
104952213	RIVAS, DOROTEO R TR	1218	S	SULTANA	AVE	ONTARIO	CA	91761
104949215	CHAVEZ, JOSEFINA ROMAN DE	542	E	PHILLIPS	ST	ONTARIO	CA	91761
104949213	CERVANTES, STEVE B	532	E	PHILLIPS	ST	ONTARIO	CA	00000
104953126	ZAMORA, LOUIE L	424	E	PHILLIPS	ST	ONTARIO	CA	00000
104950118	MC KERNAN FAMILY LIMITED PTRNSHP	533	E	BELMONT	ST	ONTARIO	CA	91761
104950116	MC KERNAN FAMILY LIMITED PTRNSHP	543	E	BELMONT	ST	ONTARIO	CA	91761
104949101	FOUR SISTERS ENTERPRISES LLC	500	E	BELMONT	ST	ONTARIO	CA	91761
104953125	JIMENEZ, VERONICA	416	E	PHILLIPS	ST	ONTARIO	CA	91761
104949237	LORENZANO, CATALINO	1306	S	PLEASANT	AVE	ONTARIO	CA	91761
104949207	LEE S LEUNG FAMILY TRUST 4/22/17	1313	S	SULTANA	AVE	ONTARIO	CA	91761
104953128	MEDINA, VICTOR	1329	S	SULTANA	AVE	ONTARIO	CA	91761
104951418	VAZQUEZ, HECTOR	425	E	BELMONT	ST	ONTARIO	CA	91761
104951419	ULTIMATE INVESTMENT ENTERPRISES INC	423	E	BELMONT	ST	ONTARIO	CA	91761
104951416	RAMIREZ, RICARDO	428	E	RALSTON	ST	ONTARIO	CA	91761
104953124	RAMOS, ROSEMARY	410	E	PHILLIPS	ST	ONTARIO	CA	00000
104949236	ORTIZ, FRANCISCO	1302	S	MONTEREY	AVE	ONTARIO	CA	91761
104949238	MUNOZ, VICTOR	1310	S	PLEASANT	AVE	ONTARIO	CA	00000
104950103	SILVA, ALICE	0						00000
104951433	SANCHEZ, MARK A	409	E	BELMONT	ST	ONTARIO	CA	91761
104950102	SILVA, ALICE	1125	E	SULTANA	AVE	ONTARIO	CA	91761
104949102	FOUR SISTERS ENTERPRISES LLC	539	E	BELMONT	ST	ONTARIO	CA	91761
104952208	ESPINOZA, THERESA O TR	408	E	BELMONT	ST	ONTARIO	CA	00000
104952215	CHAVEZ, JESUS	411	E	PHILLIPS	ST	ONTARIO	CA	91761
104949211	DE SANTIAGO, BAUDELIO	522	E	PHILLIPS	ST	ONTARIO	CA	00000
104953133	DAVIS, JEFFREY G TR	419	E	ACACIA	ST	ONTARIO	CA	91761
104951415	IZAGUIRRE, ADEL OSMAR	426	E	RALSTON	ST	ONTARIO	CA	91761
104951414	RONS, SHUANG YI LIVING TRUST 4/16/15	422	E	RALSTON	ST	ONTARIO	CA	91761
104952212	SOTO, JUAN H	1214	S	SULTANA	AVE	ONTARIO	CA	91761
104949208	QUEVEDO, MIGUEL	504	E	PHILLIPS	ST	ONTARIO	CA	91761
104949239	RODRIGUEZ, MANUEL M	1314	S	PLEASANT	AVE	ONTARIO	CA	00000
104949242	YANG, ALAN YUNGLIN	1326	S	PLEASANT	AVE	ONTARIO	CA	91761
104950119	MC KERNAN FAMILY LIMITED PTRNSHP	525	E	BELMONT	ST	ONTARIO	CA	00000
104949103	FOUR SISTERS ENTERPRISES LLC	544	E	BELMONT	ST	ONTARIO	CA	91761
104952209	IARUSSI, DONNALD	414	E	BELMONT	ST	ONTARIO	CA	91761
104949216	PHAM, KHANG KHAI	550	E	PHILLIPS	ST	ONTARIO	CA	91761
104949212	GADNA, ESTHELA A	526	E	PHILLIPS	ST	ONTARIO	CA	91761
104949235	BELTRAL, VICTOR	1313	S	MONTEREY	AVE	ONTARIO	CA	91761
104949241	GOMEZ, VICTOR M	1322	S	PLEASANT	AVE	ONTARIO	CA	91761
104952210	GUTIERREZ, ROSA M	424	E	BELMONT	ST	ONTARIO	CA	91761
104952211	GONZALES, FELICITAS P FAMILY TRUST 6	1208	S	SULTANA	AVE	ONTARIO	CA	00000
104949214	JAJIME, ARMANDO	538	E	PHILLIPS	ST	ONTARIO	CA	91761

1049-491-01 300ft. Radius, 1/4/2018, Page 2-1

Parcel #*	Owner's Name*	#	Dir.	Street	Type	City	St.	Zip
104949210	VARELA, CALLETIANO	516	E	PHILLIPS	ST	ONTARIO	CA	91761
104949239	GADNA, PABLO	510	E	PHILLIPS	ST	ONTARIO	CA	91761
104953127	RIVAS, DOROTEO R TR	430	E	PHILLIPS	ST	ONTARIO	CA	91761
104949240	PUGA, DANIEL	1318	S	PLEASANT	AVE	ONTARIO	CA	91761
104949206	PEREZ, ANTONIO TAPIA	1319	S	SULTANA	AVE	ONTARIO	CA	91761
104949205	HERNANDEZ, XAVIER	1323	S	SULTANA	AVE	ONTARIO	CA	91761
104953129	M2T INVESTMENTS LLC	1328	S	SULTANA	AVE	ONTARIO	CA	91761

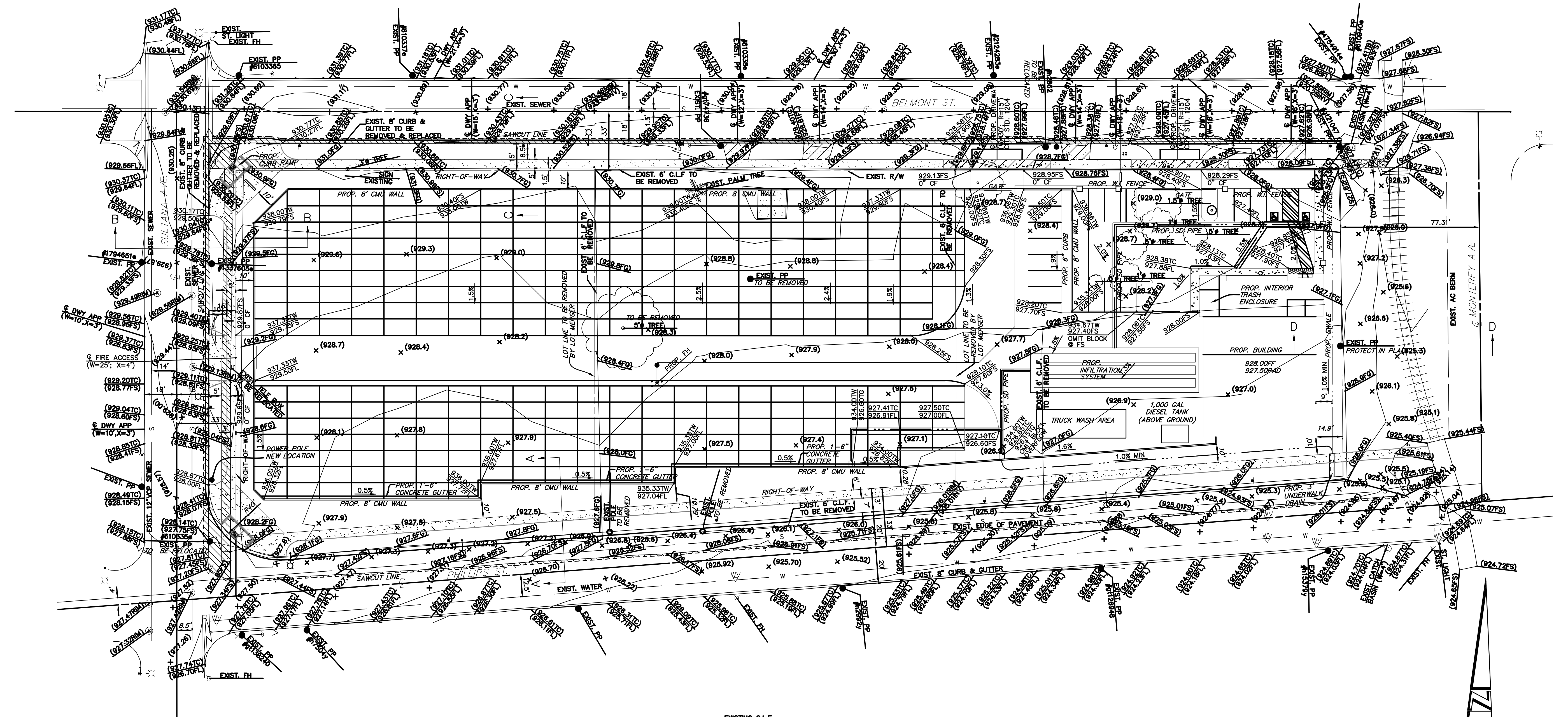
1049-491-01 300ft. Radius, 1/4/2018, Page 1-2

Owner's Address	City-St-Zip
P O BOX 3877	ONTARIO CA
14135 VALLEY BLVD	FONTANA CA 92335
2034 S SULTANA AVE	ONTARIO CA 91761
419 E BELMONT AVE	ONTARIO CA 91761
1440 DELAMERE DR	ROWLAND HEIGHTS CA 91748
14135 VALLEY BLVD	FONTANA CA 92335
1218 S SULTANA AVE	ONTARIO CA 91761
407 E PHILLIPS ST	ONTARIO CA 91761
1218 S SULTANA AVE	ONTARIO CA 91761
542 E PHILLIPS ST	ONTARIO CA 91761
532 E PHILLIPS ST	ONTARIO CA 91761
424 E PHILLIPS ST	ONTARIO CA
P O BOX 3877	ONTARIO CA
23792 ROCKFIELD BLVD # 140	LAKE FOREST CA 92630
416 E PHILLIPS ST	ONTARIO CA 91761
352 W 41 PL	LOS ANGELES CA 90037
4805 WILLMONTE AVE	TEMPLE CITY CA 91780
12338 DAHLIA AVE	EL MONTE CA 91732
425 E BELMONT ST	ONTARIO CA 91761
9125 EGLISE AVE	DOWNEY CA 90240
428 E RALSTON ST	ONTARIO CA 91761
410 E PHILLIPS ST	ONTARIO CA 91761
1302 S MONTEREY AVE	ONTARIO CA 91761
1310 S PLEASANT AVE	ONTARIO CA 91762
14135 VALLEY BLVD	FONTANA CA 92335
11239 WHITEWATER AVE	MONTCLAIR CA 91763
14135 VALLEY BLVD	FONTANA CA 92335
23792 ROCKFIELD BLVD # 140	LAKE FOREST CA 92630
312 W MAITLAND ST	ONTARIO CA 91762
411 E PHILLIPS ST	ONTARIO CA 91761
522 E PHILLIPS ST	ONTARIO CA 91761
530 STRATFORD DR	ENCINITAS CA 92024
426 E RALSTON ST	ONTARIO CA 91761
6760 ARDENDALE AVE	MARADIA CA 91007
1325 W RALSTON	ONTARIO CA 91762
504 E PHILLIPS ST	ONTARIO CA 91761
1314 S PLEASANT AVE	ONTARIO CA 91761
7248 PINEWOOD CT	CORONA CA 92680
P O BOX 3877	ONTARIO CA
23792 ROCKFIELD BLVD # 140	LAKE FOREST CA 92630
414 E BELMONT ST	ONTARIO CA 91761
908 JACARANDA ST	ONTARIO CA 91762
526 E PHILLIPS	ONTARIO CA 91761
1313 S MONTEREY DR	ONTARIO CA 91761
1322 S PLEASANT AVE	ONTARIO CA 91761
424 AND 428 E BELMONT ST	ONTARIO, CA
1208 S SULTANA	ONTARIO CA 91761
538 E PHILLIPS ST	ONTARIO CA 91761

1049-491-01 300ft. Radius, 1/4/2018, Page 2-2

Owner's Address	City-St-Zip
516 E PHILLIPS ST	ONTARIO CA 91761
510 E PHILLIPS ST	ONTARIO CA 91761
1218 S SULTANA AVE	ONTARIO CA 91761
1318 S PLEASANT AVE	ONTARIO CA 91761
1318 S SULTANA AVE	ONTARIO CA 91761
1323 S SULTANA AVE	ONTARIO CA 91761
236 MILTON DR	SAN GABRIEL CA 91775

- LEGEND**
- BW BACK OF WALK
 - EG EXISTING GRADE
 - FG FINISH GRADE
 - FS FINISH SURFACE
 - TC TOP OF CURB
 - FL FLOW LINE
 - EP EDGE OF PAVEMENT
 - FF FINISH FLOOR
 - PAD PAD GRADE
 - TW TOP OF WALL
 - TF TOP OF FOOTING
 - HP HIGH POINT
 - LP LOW POINT
 - CF CURB FACE
 - TG TOP OF GRATE
 - WI WROUGHT IRON
 - TRW TOP OF RETAINING WALL
 - EDF EXTRA DEPTH FOOTING
 - SCO SEWER CLEANOUT
 - INV INVERT OF PIPE
 - TOF TOP OF SLOPE
 - RSO RATE OF SLOPE
 - TSO TOE OF SLOPE
 - S- EXISTING SEWER
 - W- EXISTING WATER
 - E- EXISTING ELEC.
 - G- EXISTING GAS
 - T- EXISTING COMM.
 - X- EXISTING CHAIN-LINK FENCE
 - X- EXISTING WROUGHT IRON FENCE
 - X- EXISTING EDGE OF PAVEMENT
 - X- EXISTING FIRE HYDRANT
 - X- EXISTING FDC
 - X- EXISTING POWER POLE
 - X- EXISTING WATER VALVE
 - X- EXISTING GAS METER
 - X- EXISTING WATER METER
 - X- EXISTING PULL BOX
 - X- EXISTING ICV
 - D- EXISTING STORM DRAIN
 - S- EXISTING SEWER DRAIN
 - T- EXISTING TELECOMM MANHOLE
 - FOUND MONUMENT AS NOTED
 - EXISTING HANDICAP SIGN
 - PROPOSED NO PARKING SIGN
 - EXISTING TREE
 - EXISTING GUY-WIRE
 - EXISTING STREET LIGHT
 - EXISTING CONTOUR
 - FINISH CONTOURS
 - DAYLIGHT LINE
 - EXIST. CONCRETE
 - PROPOSED CONCRETE
 - PROPOSED TURF
 - PROPOSED CONCRETE BLOCK WALL
 - PROPOSED WROUGHT IRON FENCE



LEGAL DESCRIPTION:

PARCEL 1:
 THAT PORTION OF BLOCK 180, VILLA PLOTS SOUTH SIDE TRACT, IN THE CITY OF ONTARIO, COUNTY OF SAN BERNARDINO, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 6, PAGE(S) 11, OF MAPS, IN THE OFFICE OF HT COUNTY RECORDER OF SAID COUNTY, DESCRIBED AS FOLLOWS:
 BEGINNING AT A POINT ON THE NORTH LINE OF SAID BLOCK 180 WHICH IS 200 FEET EAST OF THE EAST LINE OF SULTANA AVENUE AS SAID AVENUE IS EXTENDED THROUGH SAID BLOCK; THENCE WESTERLY ALONG THE SOUTHERLY LINE OF SAID BLOCK 200.57 FEET, MORE OR LESS, TO THE EAST LINE OF SAID SULTANA AVENUE, THENCE NORTH ALONG THE EAST LINE OF SULTANA AVENUE TO THE NORTH LINE OF SAID BLOCK 180; THENCE EAST ALONG SAID NORTH LINE, 200 FEET TO THE POINT OF BEGINNING.

PARCEL 2:
 PORTION OF BLOCKS 180 AND 181, VILLA PLOTS, SOUTH SIDE TRACT, IN THE CITY OF ONTARIO, COUNTY OF SAN BERNARDINO, STATE OF CALIFORNIA, AS SHOWN BY MAP ON FILE IN BOOK 6, PAGE(S) 11, OF MAPS, RECORDS OF SAN BERNARDINO COUNTY, CALIFORNIA DESCRIBED AS FOLLOWS:
 BEGINNING AT A POINT IN THE NORTH LINE OF SAID BLOCK 181, 213.26 FEET WEST OF THE INTERSECTION OF THE SAID NORTH LINE OF SAID BLOCK WITH THE WEST LINE OF THE RIGHT OF WAY OF THE SOUTHERN PACIFIC RAILROAD COMPANY, AS DESCRIBED IN DEED SAID COMPANY RECORDED MARCH 16, 1891, IN BOOK 128 OF DEEDS, PAGE 55 RECORDS OF SAN BERNARDINO COUNTY, CALIFORNIA; THENCE WEST ALONG THE NORTH LINE OF SAID BLOCKS 181 AND 180, 203.42 FEET, MORE OR LESS, TO A POINT 200 FEET EAST OF THE EAST LINE OF SULTANA AVENUE, AS SAID AVENUE IS EXTENDED THROUGH SAID BLOCK 180; THENCE SOUTH 221.59 FEET TO A POINT IN THE SOUTHERLY LINE OF SAID BLOCK 180; THENCE IN AN EASTERLY DIRECTION ALONG THE SOUTHERLY LINE OF BLOCKS 180 AND 181, 203.96 FEET, MORE OR LESS, TO A POINT DUE SOUTH OF THE POINT OF BEGINNING; THENCE NORTH 206.73 FEET, MORE OR LESS, TO THE POINT OF BEGINNING.

PARCEL 3:
 THE EAST 213.26 FEET OF LOT 181 OF VILLA PLOTS, SOUTH SIDE TRACT, IN THE CITY OF ONTARIO, COUNTY OF SAN BERNARDINO, STATE OF CALIFORNIA, AS PER PLAT RECORDED IN BOOK 6 OF MAPS, RECORDS OF SAID COUNTY.
 EXCEPT THEREFROM THAT PORTION OF LAND LYING EAST OF THE WESTERLY LINE OF THE LAND CONVEYED TO THE SOUTHERN PACIFIC RAILROAD COMPANY IN DEED RECORDED MARCH 16, 1891 IN BOOK 128, PAGE 55 OF DEEDS

EASEMENT NOTES:

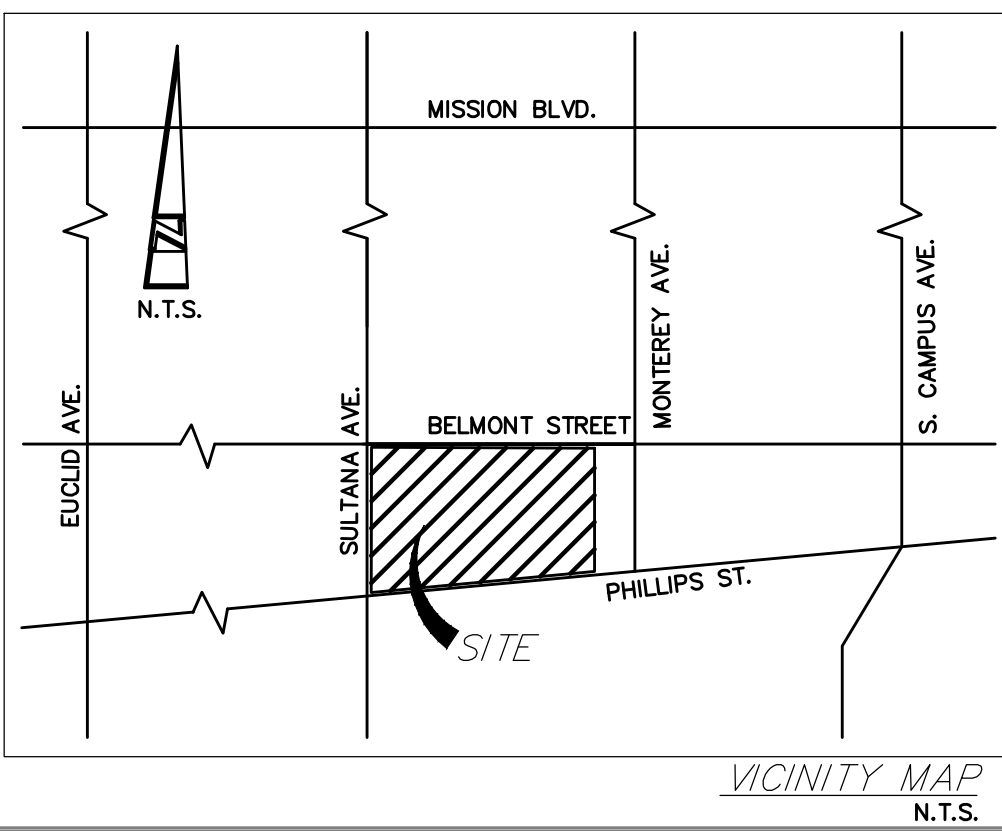
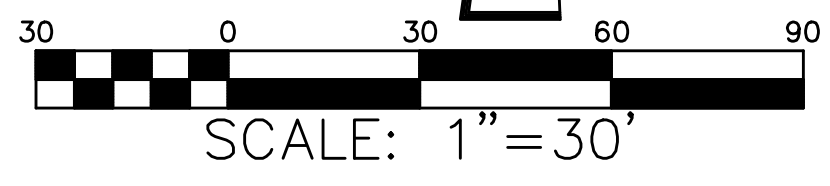
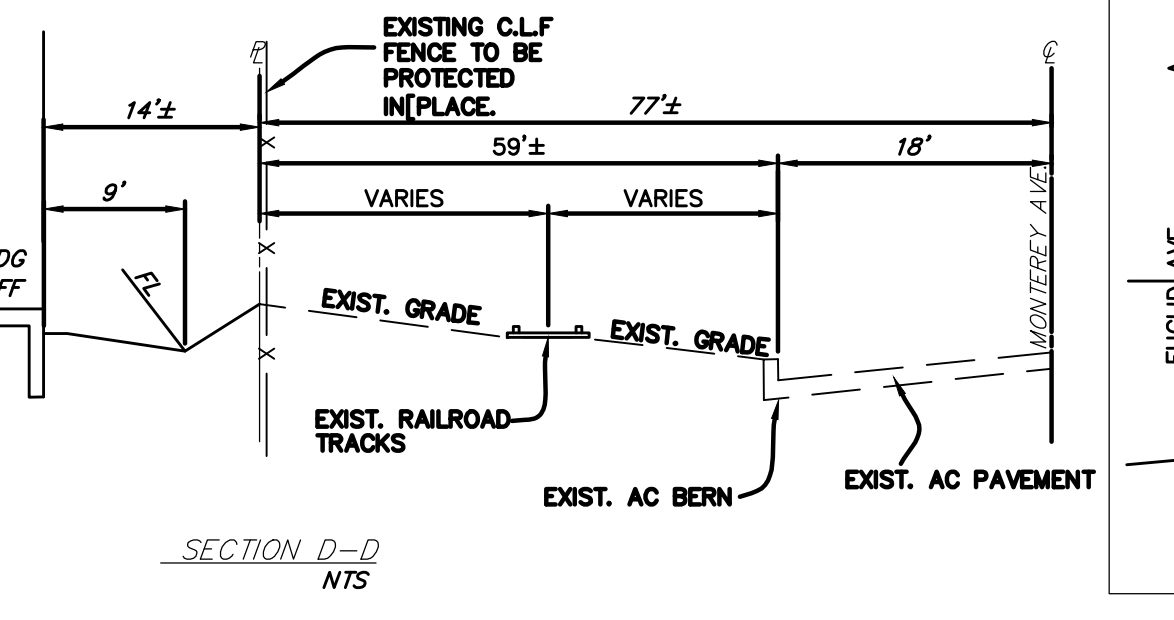
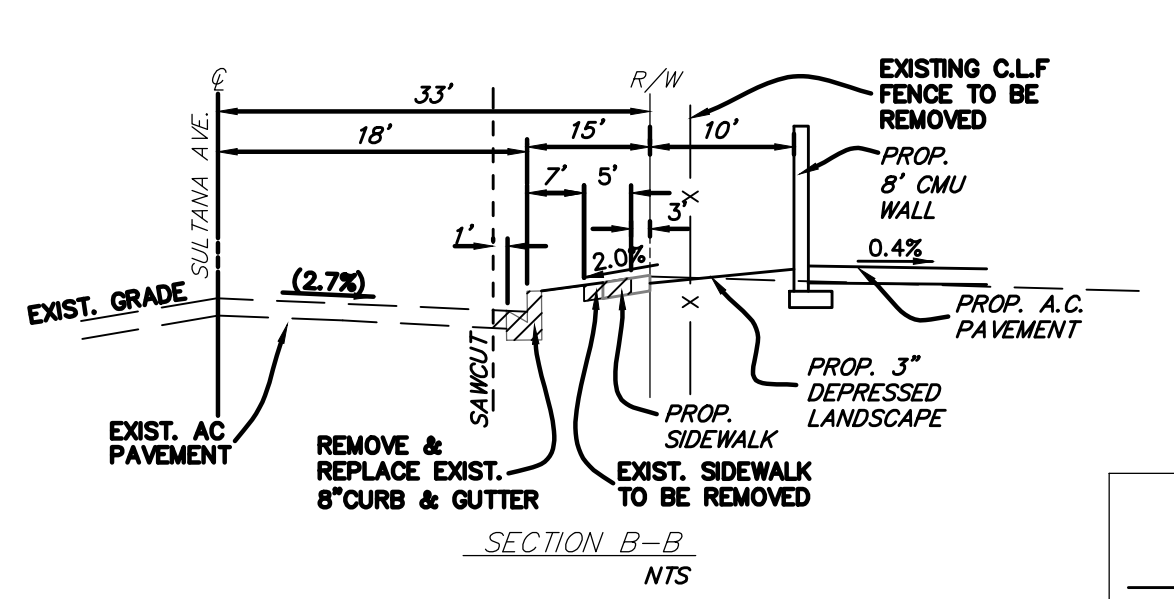
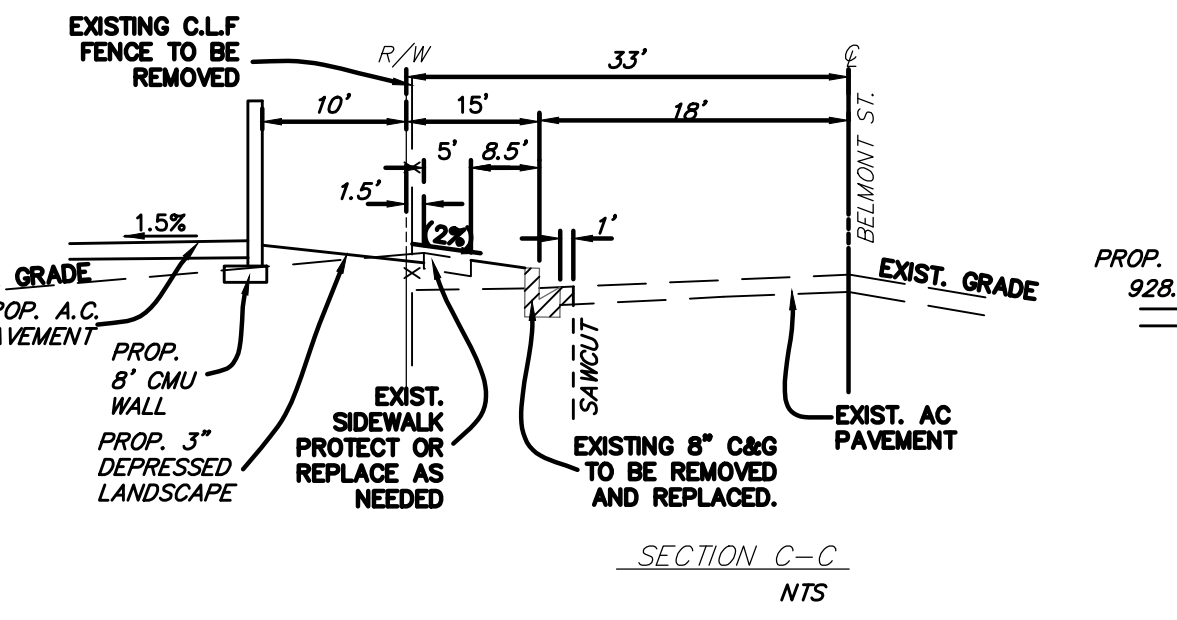
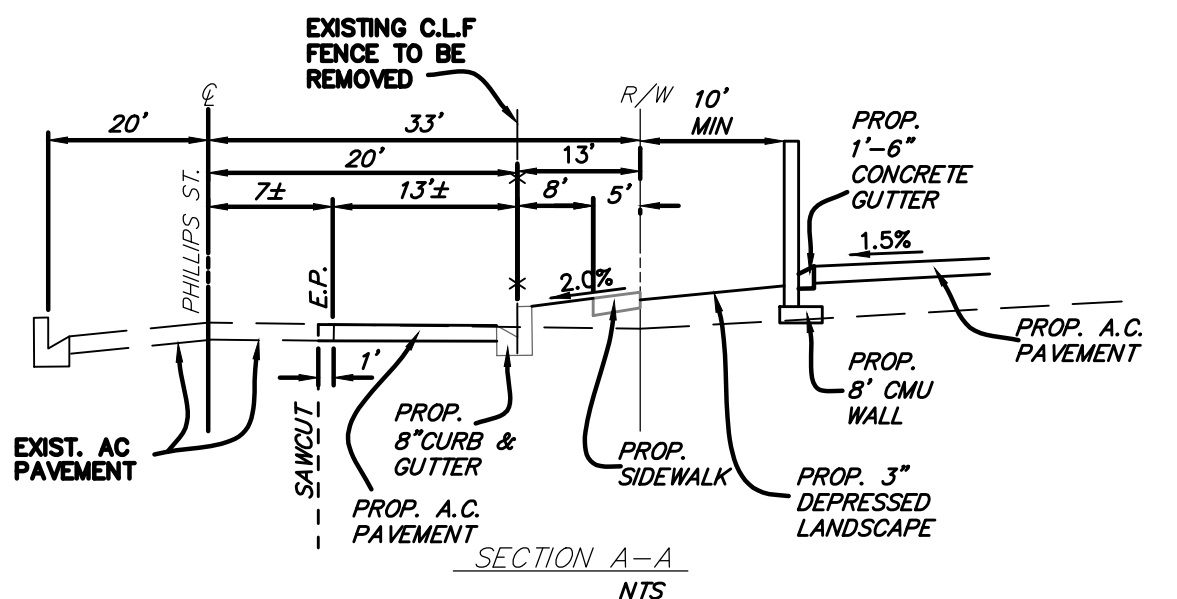
- BASES ON FIRST AMERICA TITLE INSURANCE COMPANY, TITLE COMMITMENT ISSUED BY FIRST AMERICA TITLE INSURANCE COMPANY, AS AGENT FILE NO. NCS-842567-ONTI DATED MARCH 24, 2017
1. AN EASEMENT FOR PIPELINES AND INCIDENTAL PURPOSES IN THE DOCUMENT RECORDED MAY 28, 1888 AS BOOK 41, PAGE 363 OF OFFICIAL RECORDS. (THE LOCATION OF EASEMENT CANNOT BE DETERMINED)
 2. A RIGHT OF WAY OVER AND THROUGH SAID LAND RESERVED TO THE SAN ANTONIO WATER COMPANY A CORPORATION AND ASSIGNS, ALSO TO THE ONTARIO, LAND AND IMPROVEMENT COMPANY, A CORPORATION AND ASSIGNS, FOR WATER PIPES AND OTHER CONDUITS USED IN THE DISTRIBUTION OF WATER BY SAID COMPANIES, RECORDED NOVEMBER 27, 1901, IN BOOK 304, PAGE 195 OF DEEDS. (THE LOCATION OF THE EASEMENT CANNOT BE DETERMINED).
 3. AN EASEMENT GRANTED TO CHINO BASIN MUNICIPAL WATER DISTRICT FOR PIPE LINES AND INCIDENTAL PURPOSES, RECORDED OCTOBER 17, 1968 AS BOOK 7113, PAGE 219 OF OFFICIAL RECORDS.
 4. AN EASEMENT GRANTED TO CHINO BASIN MUNICIPAL WATER DISTRICT FOR PIPELINES AND INCIDENTAL PURPOSES, RECORDED OCTOBER 29, 1968 AS BOOK 7119, PAGE 902 OF OFFICIAL RECORDS.
 5. AN EASEMENT GRANTED TO CHINO BASIN MUNICIPAL WATER DISTRICT FOR PIPE LINES AND INCIDENTAL PURPOSES, RECORDED AS BOOK 7121, PAGE 866 OF OFFICIAL RECORDS

BASIS OF BEARINGS:

THE BEARING OF NORTH 85°04' 10" EAST FOR THE CENTERLINE OF PHILLIPS STREET AS PER TRACT NO. 4047, MAP BOOK 51, PAGES 63, IN THE CITY OF ONTARIO, COUNTY OF SAN BERNARDINO, STATE OF CALIFORNIA, WAS USED AS THE BASIS OF BEARINGS FOR THESE PLANS.

NOTES:

1. BELMONT STREET AND SULTANA STREET WILL BE REMOVED OR GRINS AND OVERLAY TO THE CENTERLINE BASED ON SOILS ENGINEER RECOMMENDATION.
2. ALL EXISTING POWER POLE NOT BEHIND THE PROPOSED CURB AND GUTTER WILL BE RELOCATED.
3. ALL EXISTING DRIVEWAY ALONG BELMONT STREET TO BE REMOVED.
4. ALL EXISTING TREES TO BE REMOVED.



DIGALERT
 DIAL TOLL FREE 811
 AT LEAST TWO DAYS BEFORE YOU DIG
 UNDERGROUND SERVICE ALERT OF SOUTHERN CALIFORNIA

SEE SHEET 2 FOR PROPOSED WATER, SEWER, STREET LIGHT AND PARKING SIGNS



BENCHMARK #M-20-1
 ELEV. 949.265'
 2 IN. BRASS DISK STAMPED "CITY OF ONTARIO M20-1." SET ON CURB TOP CURB APPROXIMATE 3 FEET WEST OF BOR @ SOUTHWEST RETURN OF MISSION BLVD. AND CAMPUS AVE. RECOVERED MARCH 2018.

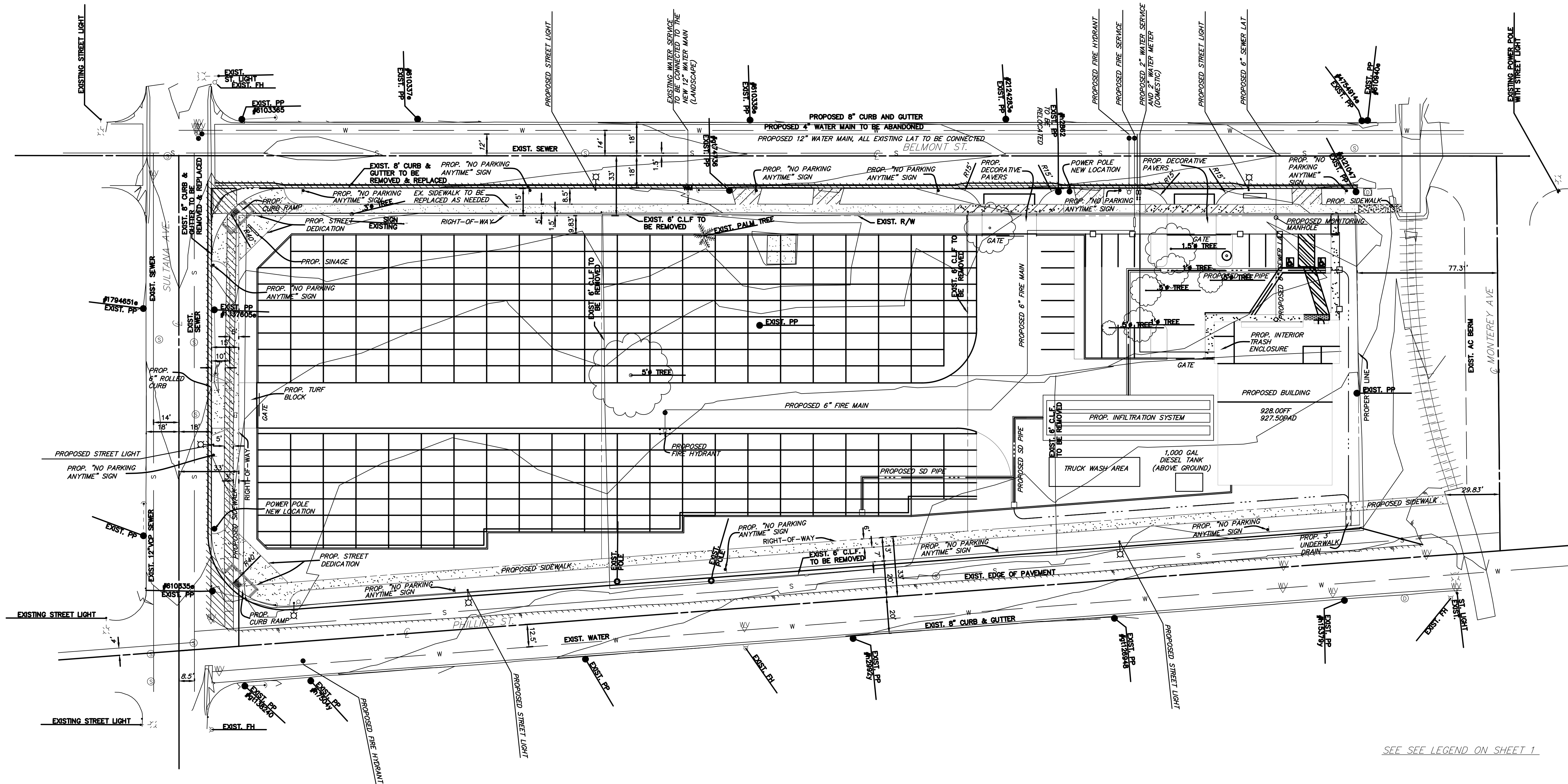
REVISIONS:	DATE

PREPARED UNDER THE SUPERVISION OF:
 KEVIN J. RICHER, R.C.E. 43714, LIC. EXP. 3/31/21
 APPROVED BY: _____ DATE: _____

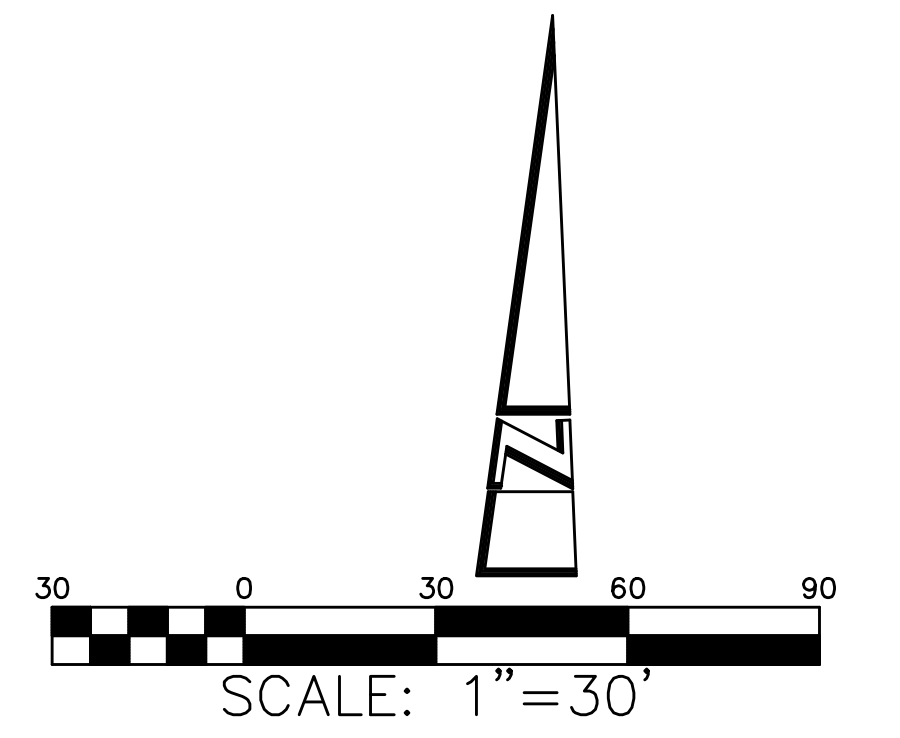
PRELIMINARY GRADING

SITE: PEPE'S TOWING, 500 BELMONT ST., ONTARIO, CA
 PREPARED FOR: MANUEL ACOSTA, 23792 ROCKFIELD BLVD., LAKE FOREST, CA

DATE: 9/04/19	JOB NO. 6078
DRAWN BY: KR	SCALE 1"=30'
DESIGNED BY: KR	SHEET 1 OF 2 SHEETS
CHECKED BY: KJR	PLANNING • CIVIL • SURVEYING



SEE LEGEND ON SHEET 1



LAND DEVELOPMENT DESIGN COMPANY, LLC
 2313 E. Philadelphia St., Ste. F
 Ontario, CA 91761
 (909) 930-1466 • FAX (909) 930-1468
 PLANNING • CIVIL • SURVEYING

DIGALERT
 DIAL TOLL FREE 811
 AT LEAST TWO DAYS BEFORE YOU DIG
 UNDERGROUND SERVICE ALERT OF SOUTHERN CALIFORNIA

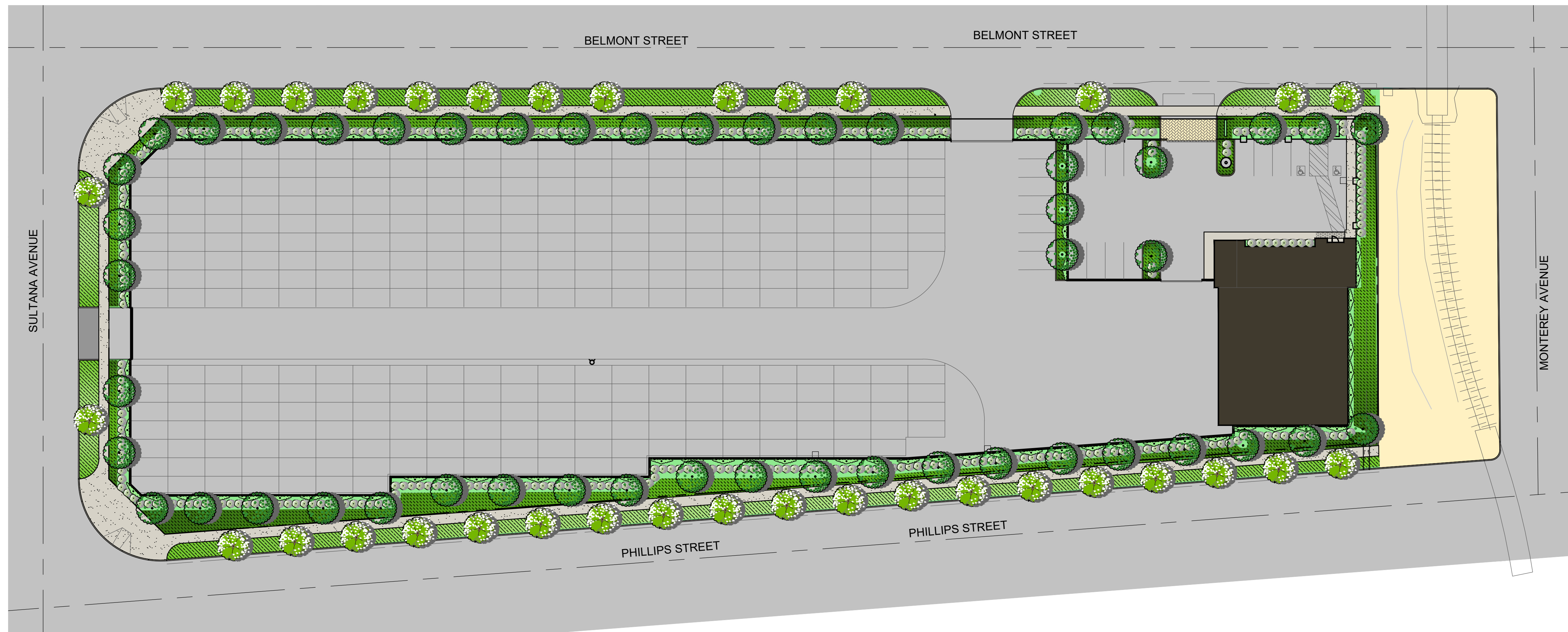


BENCHMARK #M-20-1
 ELEV. 949.265'
 2 IN. BRASS DISK STAMPED "CITY OF ONTARIO M20-1." SET ON CURB TOP CURB APPROXIMATE 3 FEET ± WEST OF BOR @ SOUTHWEST RETURN OF MISSION BLVD. AND CAMPUS AVE. RECOVERED MARCH 2018.

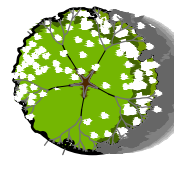
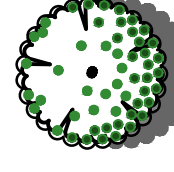


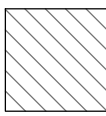
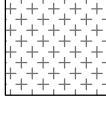
REVISIONS:	DATE

PREPARED UNDER THE SUPERVISION OF:
 KEVIN J. RICHER
 R.G.E. 43714 LIC. EXP. 3/31/21 DATE
 APPROVED BY:
 DATE

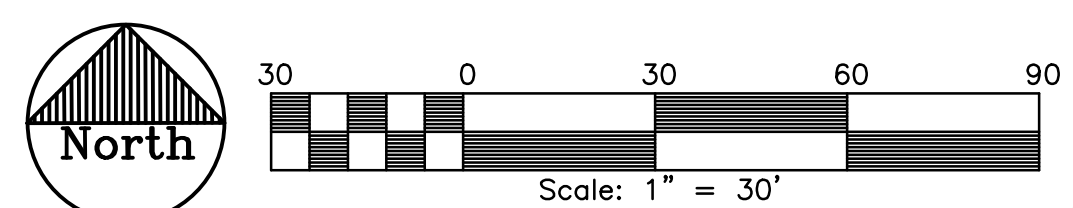
PRELIMINARY GRADING	
SITE PEPE'S TOWING 500 BELMONT ST. ONTARIO, CA	PREPARED FOR MANUEL ACOSTA 23792 ROCKFIELD BLVD. LAKE FOREST, CA
DATE: 9/04/19	JOB NO. 6078
DRAWN BY: KJR	SCALE 1"=30'
DESIGNED BY: KJR	SHEET 2 OF 2 SHEETS
CHECKED BY: KJR	



CONCEPTUAL PLANT LEGEND

	PARKWAY TREE (24" box) To coordinate with City of Ontario's Master Street Tree Plan Belmont Street: Pistachia chinensis (M) Sultana Avenue: Chitalpa tashkentensis (L) Phillips Street: Fraxinus oxycarpa 'Raywood Ash' (M)	36
	ON SITE TREE (3 @ 48" box, 5 @ 36" box, 15 @ 24" box, 27 @ 15-gal) Cercis occidentalis / Western Redbud (L) Koelerutera bipinnata / Chinese Flame Tree (M) Lagerstroemia indica / Grape Myrtle (M) Lagunana patersonii / Primrose Tree (L) Pistacia chinensis / Chinese Pistache (M) Platanus racemosa / California Sycamore (M) Prunus cerasifera 'Krauter Vesuvius' / Purple Leaf Plum (M) Quercus agrifolia / Coast Live Oak (L) Tristania conferta / Brisbane Box (M)	48
	SHRUBS Arbutus unedo 'Compacta' / Dwarf Strawberry Tree (L) Callistemon viminalis 'Little John' / Dwarf Weeping Bottlebrush (L) Cistus pulchellus 'Sunset' / Rockrose (L) Dianella revoluta 'Little Rev' / Little Rev Flax Lily (L) Elaeagnus pungens 'Fruitlandi' / Silverberry (L) Leucophyllum frutescens 'Compacta' / Compact Texas Ranger (L) Olea europaea 'Little Ollie' / Little Ollie Olive (L) Westringia fruticosa 'Wynabbie Gem' / Wynabbie Gem Coast Rosemary (L)	353
	VINES Ficus pumila / Creeping Fig (M) Parthenocissus tricuspidata / Japanese Creeper (M)	91
	PARKWAY LOW GROUNDCOVER Achillea millefolium / Common Yarrow (L) Dymondia margaritae / Dymondia (L) Myoporum parvifolium / Trailing Myoporum (L)	8,497 sf
	GROUNDCOVER Loniceria japonica 'Halliana' / Halls Honeysuckle Flowering Vine (L) Osteospermum fruticosum / Freeway Daisy (L) Rosmannus officinalis 'Huntington Carpet' / Huntington Carpet Rosemary (L)	8,480 sf

Note:
(L) Low water use plant per WUCOLS
(M) Medium water use plant per WUCOLS

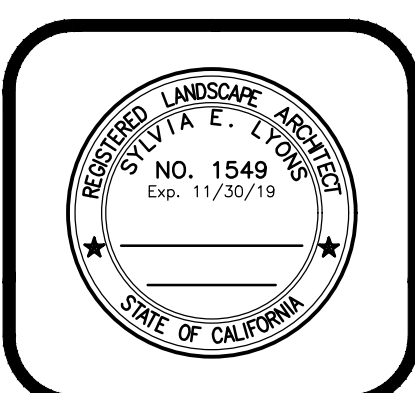


REVISIONS	BY

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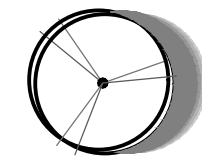
ROYAL OAK DESIGN
Sylvia E. Lyons, Landscape Architect #1549
2456 Hummingbird Way
La Verne, CA 91750-2371
Telephone: (909)593-4158
Email: royaloakdesign@verizon.net

COLORED PRELIMINARY LANDSCAPE PLAN
PROJECT: PEPE'S TOWING CO.
ADDRESS: 500 EAST BELMONT STREET, ONTARIO, CA

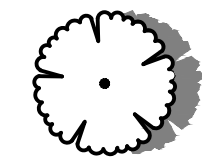


DRAWN
S.L./J.S.
DATE
9/13/2019
SCALE
1" = 20'
JOB NO.
5277
SHEET
L

CONCEPTUAL PLANT LEGEND



PARKWAY TREE (24" box)
 To coordinate with City of Ontario's Master Street Tree Plan
 Belmont Street: Pistacia chinensis (M)
 Sultana Avenue: Chitalpa tashkentensis (L)
 Phillips Street: Fraxinus oxycarpa 'Raywood Ash' (M)



ON SITE TREE
 3 @ 48" box, 5 @ 36" box, 15 @ 24" box, 27 @ 15-gal
 Cercis occidentalis / Western Redbud* (L)
 Koelerutena bipinnata / Chinese Flame Tree (M)
 Lagerstroemia indica / Crape Myrtle (M)
 Lagunana patersonii / Pinrose Tree (L)
 Pistacia chinensis / Chinese Pistache (M)
 Platanus racemosa / California Sycamore* (M)
 Prunus cerasifera 'Krauter Vesuvius' / Purple Leaf Plum (M)
 Quercus agrifolia / Coast Live Oak* (L)
 Tristiana conferta / Bnsbane Box (M)

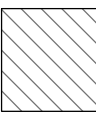
*25% on site trees shall be California natives
 On site trees will meet Minimum Tree Species Mix requirements.



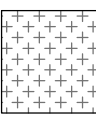
SHRUBS
 Arbutus unedo 'Compacta' / Dwarf Strawberry Tree (L)
 Callistemon viminalis 'Little John' / Dwarf Weeping Bottlebrush (L)
 Cistus pulchellus 'Sunset' / Rockrose (L)
 Dianella revoluta 'Little Rev' / Little Rev Flax Lily (L)
 Eleaegnis pungens 'Fruitlandi' / Silverberry (L)
 Leucophyllum frutescens 'Compacta' / Compact Texas Ranger (L)
 Olea europaea 'Little Olive' / Little Olive Olive (L)
 Westringia fruticosa 'Wynabbie Gem' / Wynabbie Gem Coast Rosemary (L)



VINES
 Ficus pumila / Creeping Fig (M)
 Parthenocissus tricuspidata / Japanese Creeper (M)



PARKWAY LOW GROUND COVER
 Achillea millefolium / Common Yarrow (L)
 Dymondia margaritae / Dymondia (L)
 Myoporum parvifolium / Trailing Myoporum (L)



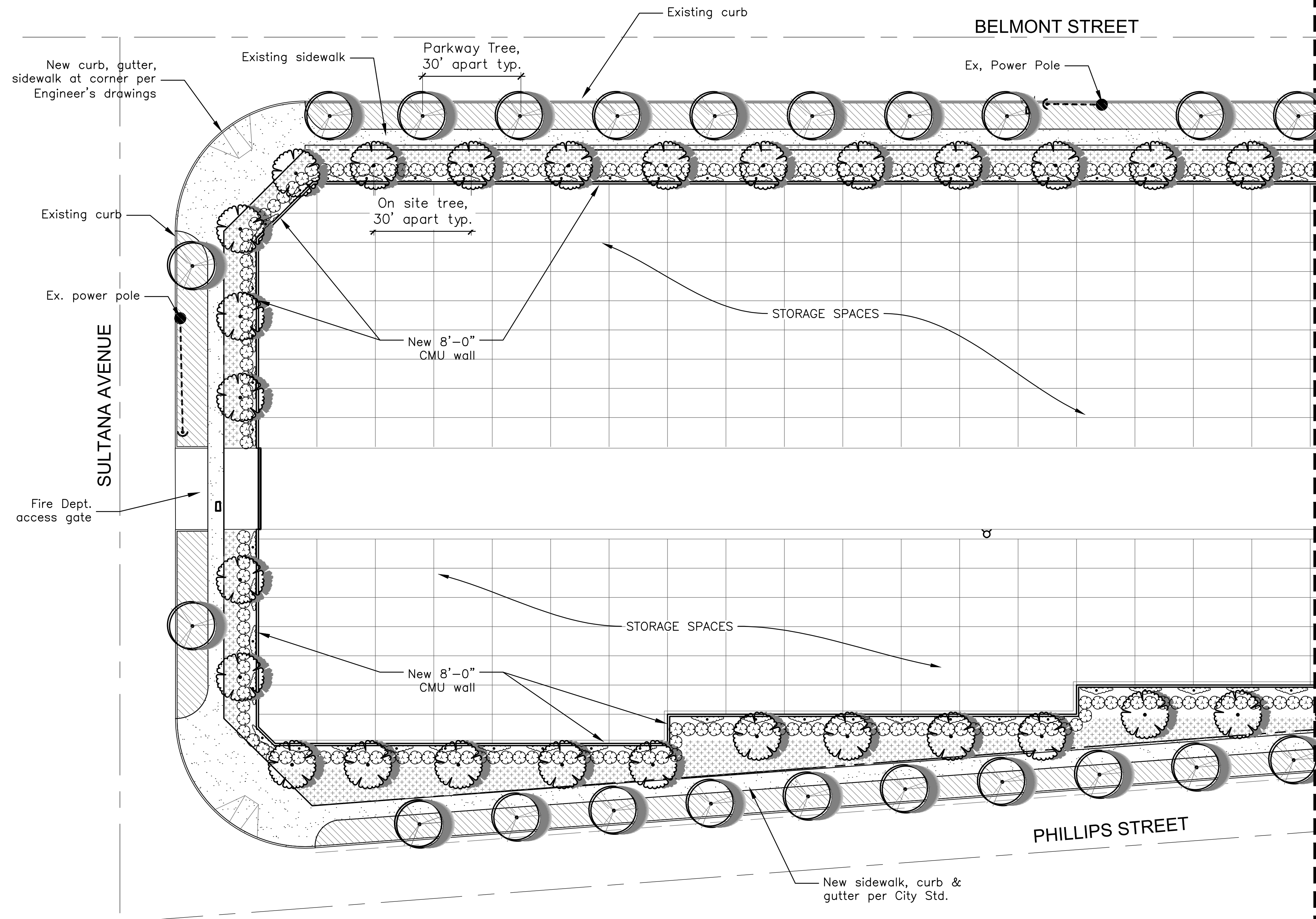
GROUND COVER
 Lonicera japonica 'Halliana' / Halls Honeysuckle Flowering Vine (L)
 Osteospermum fruticosum / Freeway Daisy (L)
 Rosmannia officinalis 'Huntington Carpet' / Huntington Carpet Rosemary (L)

Note:
 (L) Low water use plant per WUCOLS
 (M) Medium water use plant per WUCOLS

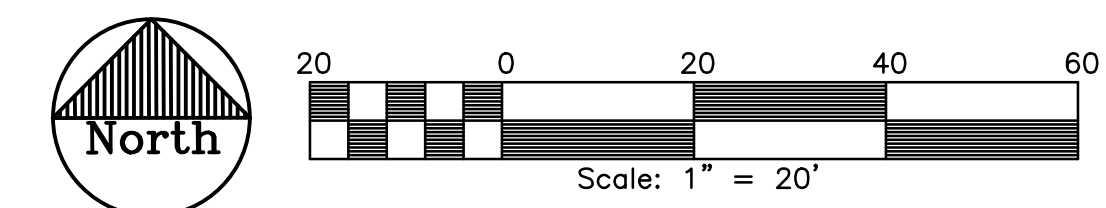
LANDSCAPE NOTES:

Landscape Tabulations:
 Total site: 111,960 sq ft (2.57 acres)
 Total landscape: 16,721 sq. ft. (15.0% of site)

1. Agronomical soil testing is required; report of soils test will be included on landscape construction plans.
2. Landscape construction plans shall meet the requirements of City of Ontario's Landscape Development Guidelines.
3. Landscape areas where compaction has occurred due to grading activities and where trees or storm water infiltration areas are located shall be loosened by soil fracturing. For trees a 12'x12'x18" deep area; for storm water infiltration the entire area shall be loosened.
4. The backhoe method of soil fracturing shall be used to break up compaction. A 4" layer of Compost is spread over the soil surface before fracturing is begun. The back hoe shall dig into the soil lifting and then drop the soil immediately back into the hole. The bucket then moves the adjacent soil and repeats. The compost falls into the spaces between the soil chunks created. Fracturing shall leave the soil surface quite rough with large soil clods. These must be broken by additional tilling. Tilling in more Compost to the surface after fracturing per the soil report will help create an A horizon soil. Imported or reused Topsoil can be added on top of the fractured soil as needed for grading. The Landscape Architect shall be present during this process and provide certification of the soil fracturing. For additional references see Urban Tree Foundation - Planting Soil Specifications.
5. All above-ground utilities such as transformers (screened w/ 4' evergreen shrubs) and backflow devices (screened w/ 3' high strappy leaved shrubs) shall be screened from public view. Trash enclosure is located inside building.
6. Water and Sewer lines will not conflict with required tree locations.



SEE SHEET L-2

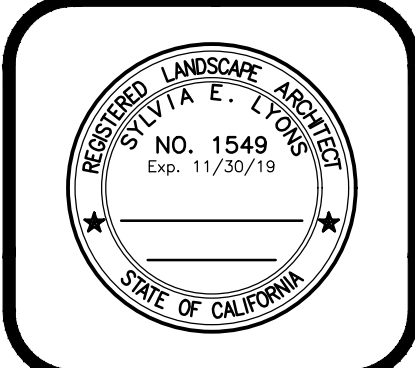


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PRELIMINARY LANDSCAPE PLAN
PROJECT: PEPE'S TOWING CO.
 ADDRESS: 500 EAST BELMONT STREET, ONTARIO, CA



DRAWN
 S.L./J.S.
 DATE
 9/13/2019
 SCALE
 1" = 20'
 JOB NO.
 5277
 SHEET

L-1

PARKWAY - WATER EFFICIENT LANDSCAPE WORKSHEET								
ONTARIO, CA								
Reference Evapotranspiration (ET ₀):		54.60						
Hydrozone #/Planting Description ^A	Plant Factor (PF)	Irrigation Method ^B	Irrigation Efficiency (IE) ^C	ETAF (PF/IE)	Landscape Area (sq. ft.)	ETAF x Area	Estimated Total Water Use (ETWU) ^D	Valves
REGULAR LANDSCAPE AREAS								
LW	0.30	DRIP	0.81	0.37	7917.00	2932.22	99261.59	
MWTR	0.40	BUBBLERS	0.80	0.50	825.00	412.50	13963.95	
TOTALS:					8742.00	3344.72		
ETWU TOTAL							113225.54	
Maximum Allowed Water Allowance (MAWAF) ^E							133170.38	
ETAF CALCULATIONS								
REGULAR LANDSCAPE AREAS					TOTAL ETAF x AREA	3344.72		
					TOTAL AREA	8742.00		
					AVERAGE ETAF	0.38		

ON SITE WATER EFFICIENT LANDSCAPE WORKSHEET								
ONTARIO, CA								
Reference Evapotranspiration (ET ₀):		54.60						
Hydrozone #/Planting Description ^A	Plant Factor (PF)	Irrigation Method ^B	Irrigation Efficiency (IE) ^C	ETAF (PF/IE)	Landscape Area (sq. ft.)	ETAF x Area	Estimated Total Water Use (ETWU) ^D	Valves
REGULAR LANDSCAPE AREAS								
LW	0.30	ROTOR	0.78	0.38	14314.00	5505.38	186368.28	A-4
LW	0.30	DRIP	0.81	0.37	831.00	307.78	10418.89	
MWTR/MW	0.40	BUBBLERS	0.80	0.50	1596.00	798.00	27013.90	
TOTALS:					16741.00	6611.16		
ETWU TOTAL							223801.07	
Maximum Allowed Water Allowance (MAWAF) ^E							255022.35	
ETAF CALCULATIONS								
REGULAR LANDSCAPE AREAS					TOTAL ETAF x AREA	6611.16		
					TOTAL AREA	16741.00		
					AVERAGE ETAF	0.39		

^A Hydrozone # / Planting Description
 MW = Moderate Water use Plants
 LW = Low Water use Plants
 LWTR = Low Water use Trees
 MWTR = Moderate Water use Trees

^B Irrigation Method
 B = Bubbler
 D = Drip
 O = Other

^C Irrigation Efficiency
 0.80 for bubbler
 0.81 for drip
 0.78 Hunter MP Rotator

^D ETWU (Annual Gallons Required)
 ET₀ x 0.62 x ETAF x Area

^E MAWA (Annual Gallons Allowed)
 (ET₀)(0.62)((ETAF x LA)

Where 0.62 is a conversion factor that converts acre - inches per acre per year to gallons per square foot per year.

Where 0.62 is a conversion factor that converts acre-inches per acre per year to gallons per square foot per year. LA is the total landscape area in square feet. ETAF is 0.55 for residential areas and 0.45 for non-residential areas.

CONCEPTUAL PLANT LEGEND

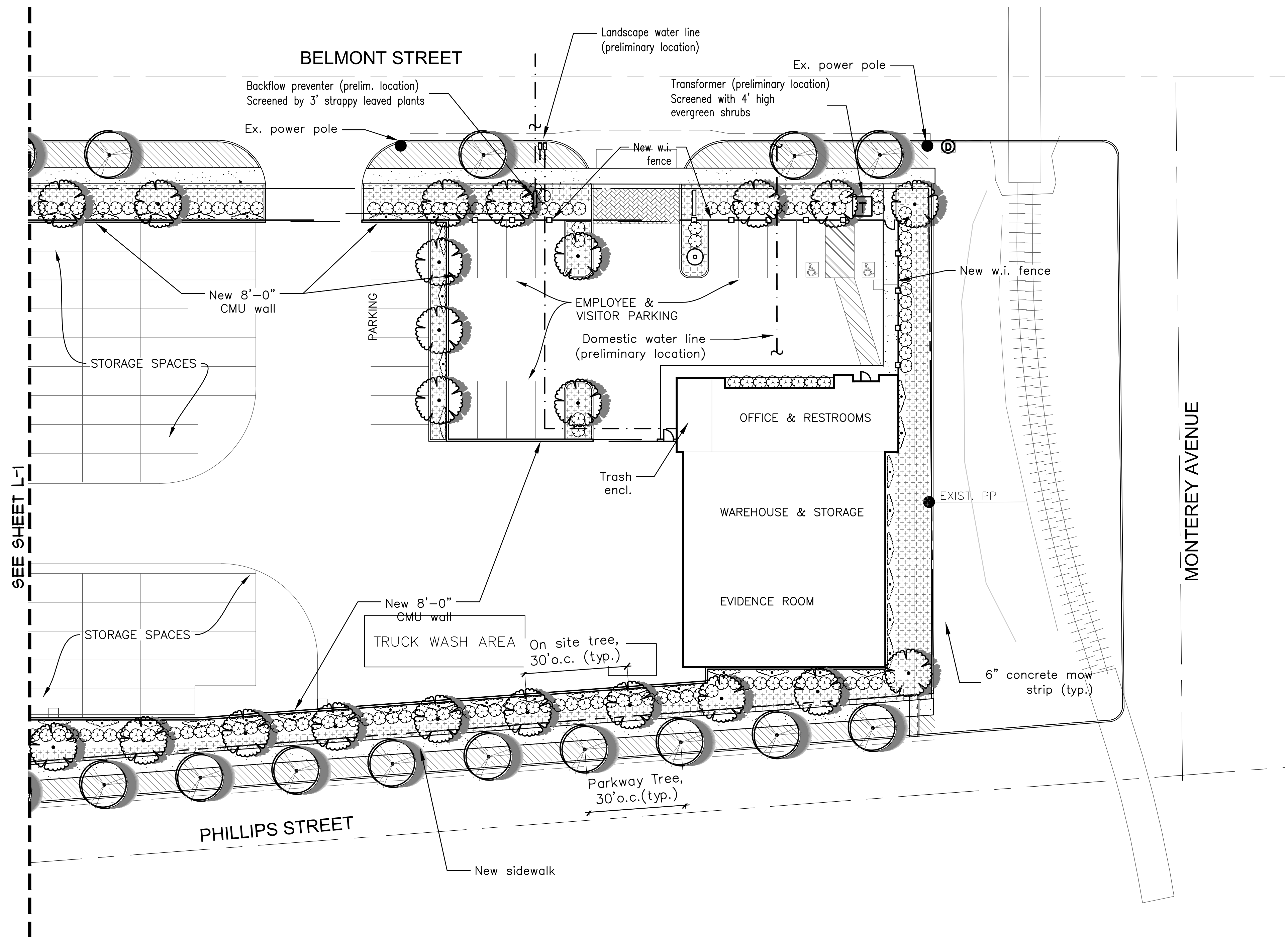
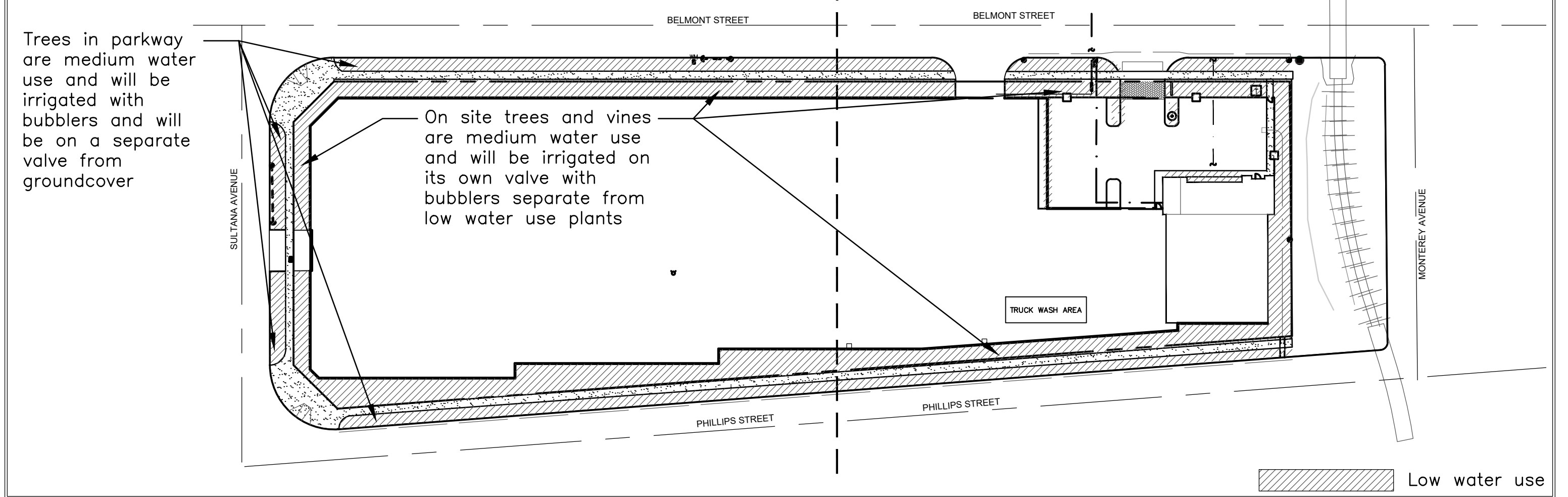
- PARKWAY TREE (24" box)**
To coordinate with City of Ontario's Master Street Tree Plan
Belmont Street: Pistachia chinensis (M)
Sultana Avenue: Chitalpa tashkentensis (L)
Phillips Street: Fraxinus oxycarpa 'Raywood Ash' (M) 36
- ON SITE TREE**
(3 @ 48" box, 5 @ 36" box, 15 @ 24" box, 27 @ 15-gal)
Cercis occidentalis / Western Redbud* (L)
Koeleria bipinnata / Chinese Flame Tree (M)
Lagerstroemia indica / Crape Myrtle (M)
Lagunaria patersonii / Pinrose Tree (L)
Pistacia chinensis / Chinese Pistache (M)
Platanus racemosa / California Sycamore* (M)
Prunus cerasifera 'Krauter Vesuvius' / Purple Leaf Plum (M)
Quercus agrifolia / Coast Live Oak* (L)
Tristania conferta / Brisbane Box (M) 49
- *25% on site trees shall be California natives
On site trees will meet Minimum Tree Species Mix requirements.
- SHRUBS** 353
Arbutus unedo 'Compacta' / Dwarf Strawberry Tree (L)
Callistemon viminalis 'Little John' / Dwarf Weeping Bottlebrush (L)
Cistus pulchellus 'Sunset' / Rockrose (L)
Daniella revoluta 'Little Rev' / Little Rev Flax Lily (L)
Elaeagnus pungens 'Fruitlandi' / Silverberry (L)
Leucophyllum frutescens 'Compacta' / Compact Texas Ranger (L)
Olea europaea 'Little Olive' / Little Olive Olive (L)
Westringia fruticosa 'Wynabbie Gem' / Wynabbie Gem Coast Rosemary (L)
- VINES** 91
Ficus pumila / Creeping Fig (M)
Parthenocissus tricuspidata / Japanese Creeper (M)
- PARKWAY LOW GROUNDCOVER**
Achillea millefolium / Common Yarrow (L)
Dymondia margaritae / Dymondia (L)
Myoporum parvifolium / Trailing Myoporum (L)
- GROUNDCOVER**
Lonicera japonica 'Halliana' / Halls Honeysuckle Flowering Vine (L)
Osteospermum fruticosum / Freeway Daisy (L)
Rosmarinus officinalis 'Huntington Carpet' / Huntington Carpet Rosemary (L)

Note:
 (L) Low water use plant per WUCOLS
 (M) Medium water use plant per WUCOLS

IRRIGATION NOTES:

- Automatic irrigation system to be installed that is water efficient, appropriate for the landscape hydrozones and provides 100% coverage.
- Landscape planting will meet MAWA water use requirements by using a low water efficient irrigation system and weather based smart controller.
- Landscape planting to be irrigated the following ways:
 - Trees: stream bubblers with PC Screens
 - Vines: bubblers
 - All other planting areas: drip irrigation
- Irrigation plans shall provide separate systems for tree stream bubblers with pc screens.
- Weather based smart controller such as Hunter X-Core, or Hunter PCC will be used.

IRRIGATION HYDROZONE KEY MAP



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PROJECT: PEPE'S TOWING CO.
 ADDRESS: 500 EAST BELMONT STREET, ONTARIO, CA



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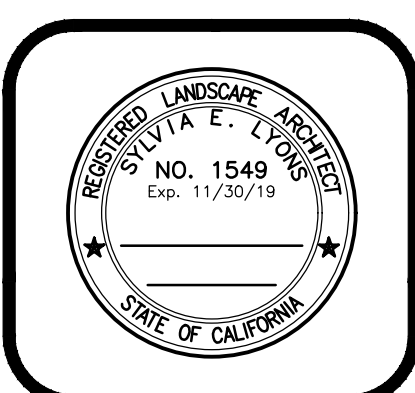
L-2

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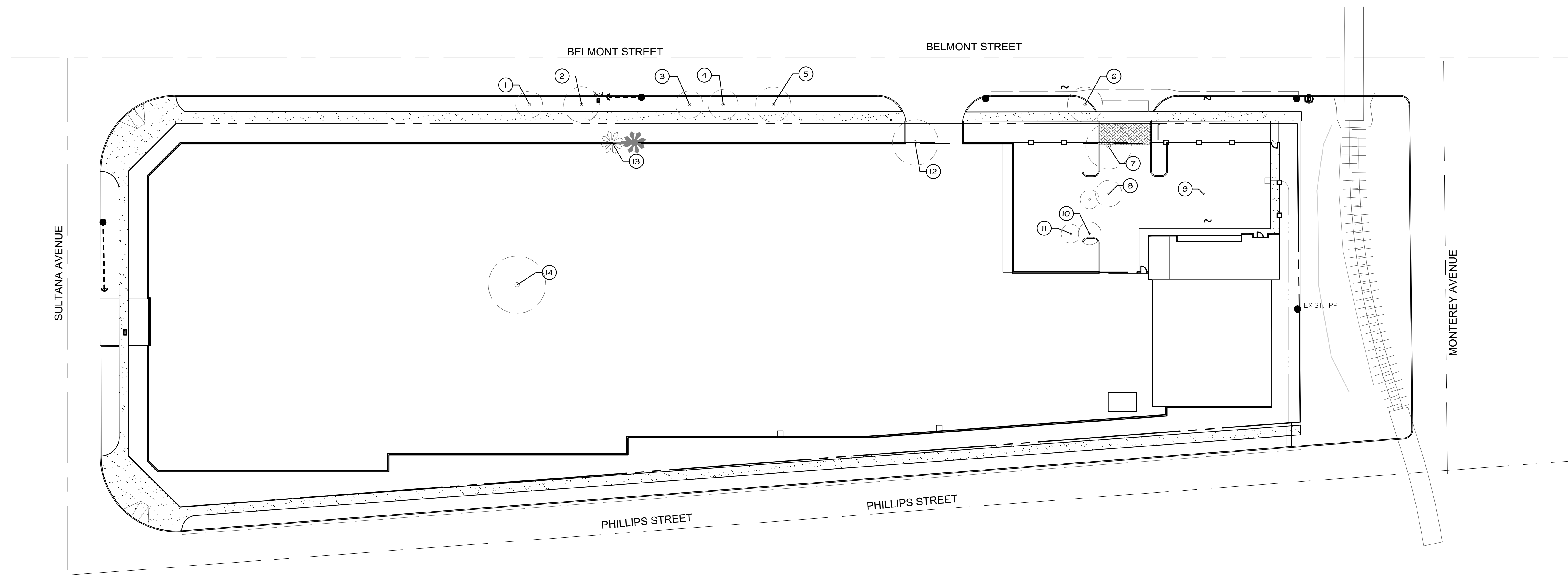
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TREE INVENTORY PLAN
 PROJECT: PEPE'S TOWING CO.
 ADDRESS: 500 EAST BELMONT STREET, ONTARIO, CA



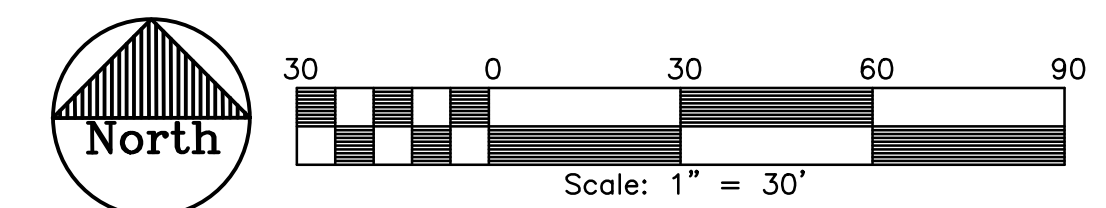
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L-3

LIST OF EXISTING TREES					
NO.	SPECIES	CALIPER	CANOPY	CONDITION	PROPOSED ACTION
1	Chitalpa x tashkentensis	11"	10'	Poor form (leaning)	Remove
2	Pistacia chinensis	1'-0"	15'	Good	Protect in place
3	Chitalpa x tashkentensis	10"	8'	Poor form (leaning)	Remove
4	Olea europaea	9"	8'	Fair	Protect in place
5	Olea europaea		8'	Fair	Protect in place
6	Magnolia grandiflora	6"	10'	Good	Remove
7	Eucalyptus sideroxylon	1'-4"	20'	Good	Remove
8	Eucalyptus sideroxylon	1'-0"	10'	Good	Remove
9	Unknown	6"		Dead	Remove
10	Ficus nitida	1'-0"	10'	Good	Remove
11	Tristania conferta	6"	8'	Poor - bark damage	Remove
12	Ficus nitida	10"	18'	Good	Remove
13	Phoenix canariensis	2'-0"	10'	Good	Relocate off site
14	Schinus molle	4'-0"	25'	Healthy but low branches	Remove - mitigation req'd

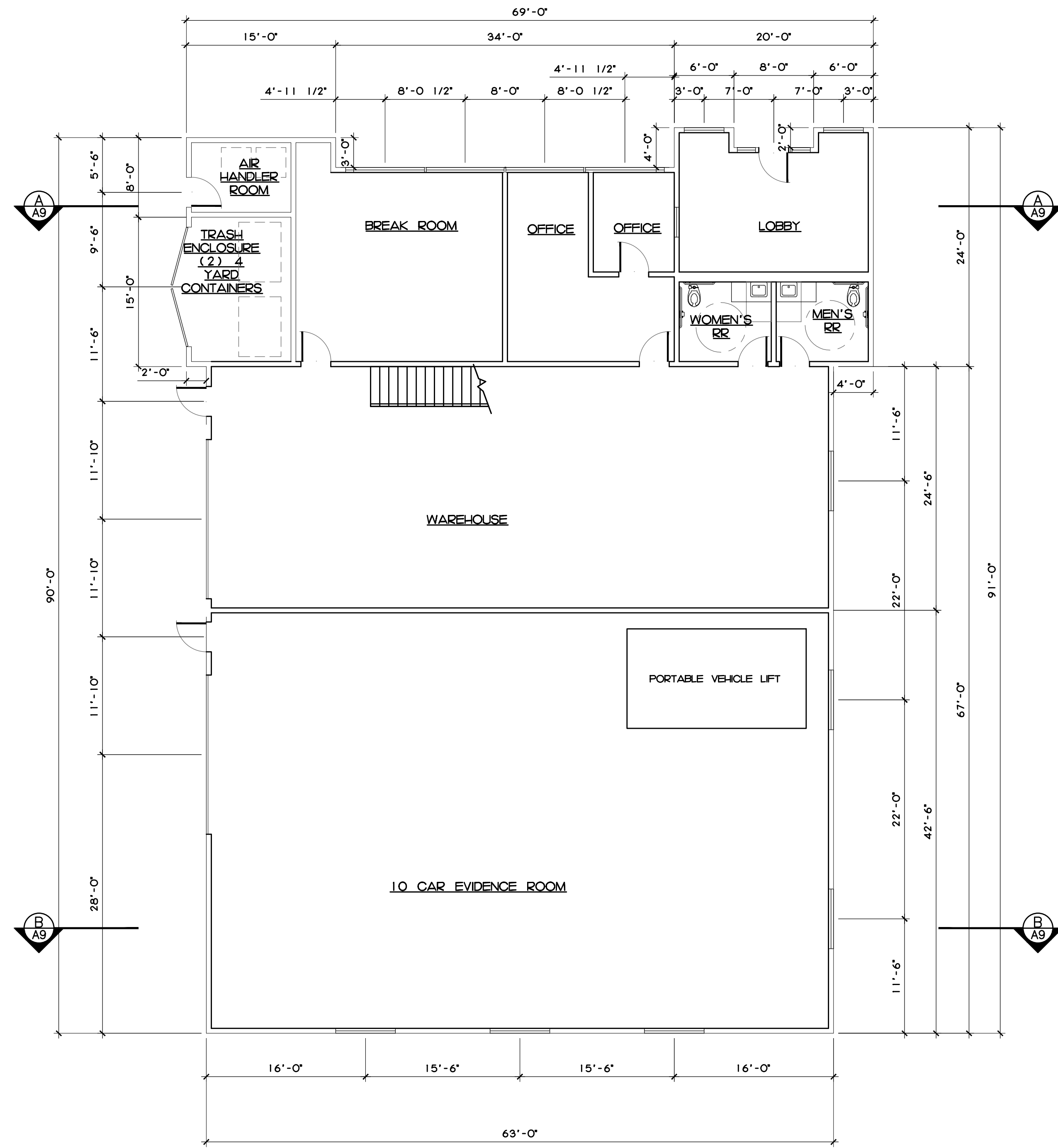


TREE INVENTORY NOTES:

- All existing trees within this property (eucalyptus, ficus, phoenix, schinus, tristania) are to be removed.
- Parkway trees to be replaced where noted.
- Mitigation measures will be required in final landscape plans for trees 13 & 14 per the Tree Preservation Policy and Protection Measures.

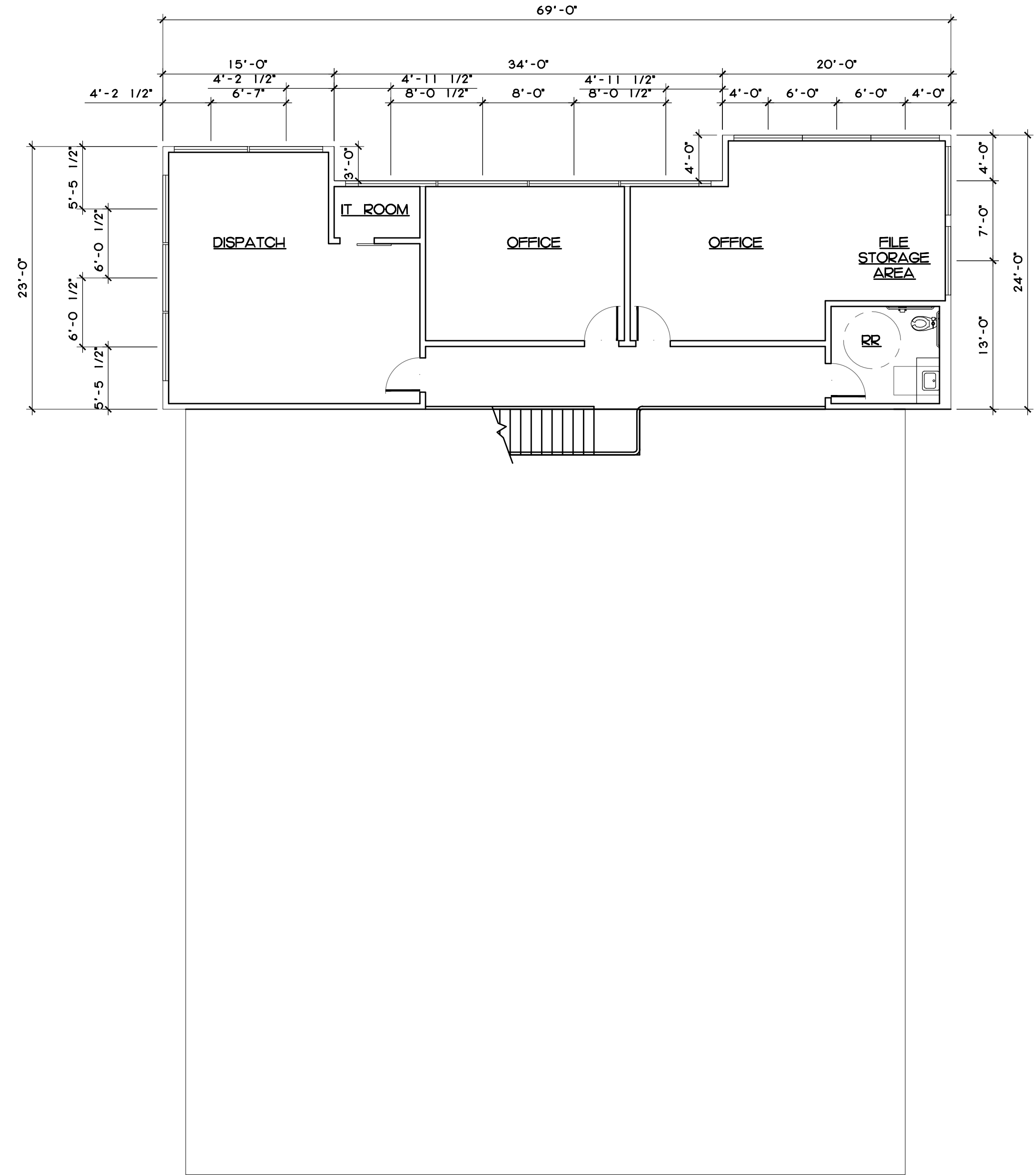


NOTE:
 TRASH ENCLOSURE SHALL BE PER CITY OF ONTARIO COMMERCIAL REFUSE
 AND RECYCLING ENCLOSURE (2) 4 YARD BINS



BUILDING FIRST FLOOR PLAN

SCALE: 1/8 INCH = ONE FOOT



BUILDING SECOND FLOOR PLAN

SCALE: 1/8 INCH = ONE FOOT



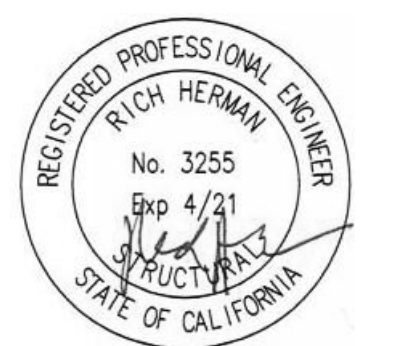
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 RHERMAN@RHERMANSE.COM

CONTRACTOR:

PROJECT NAME:

New Development For:
Pepe's Towing Co.
 500 East Belmont Street
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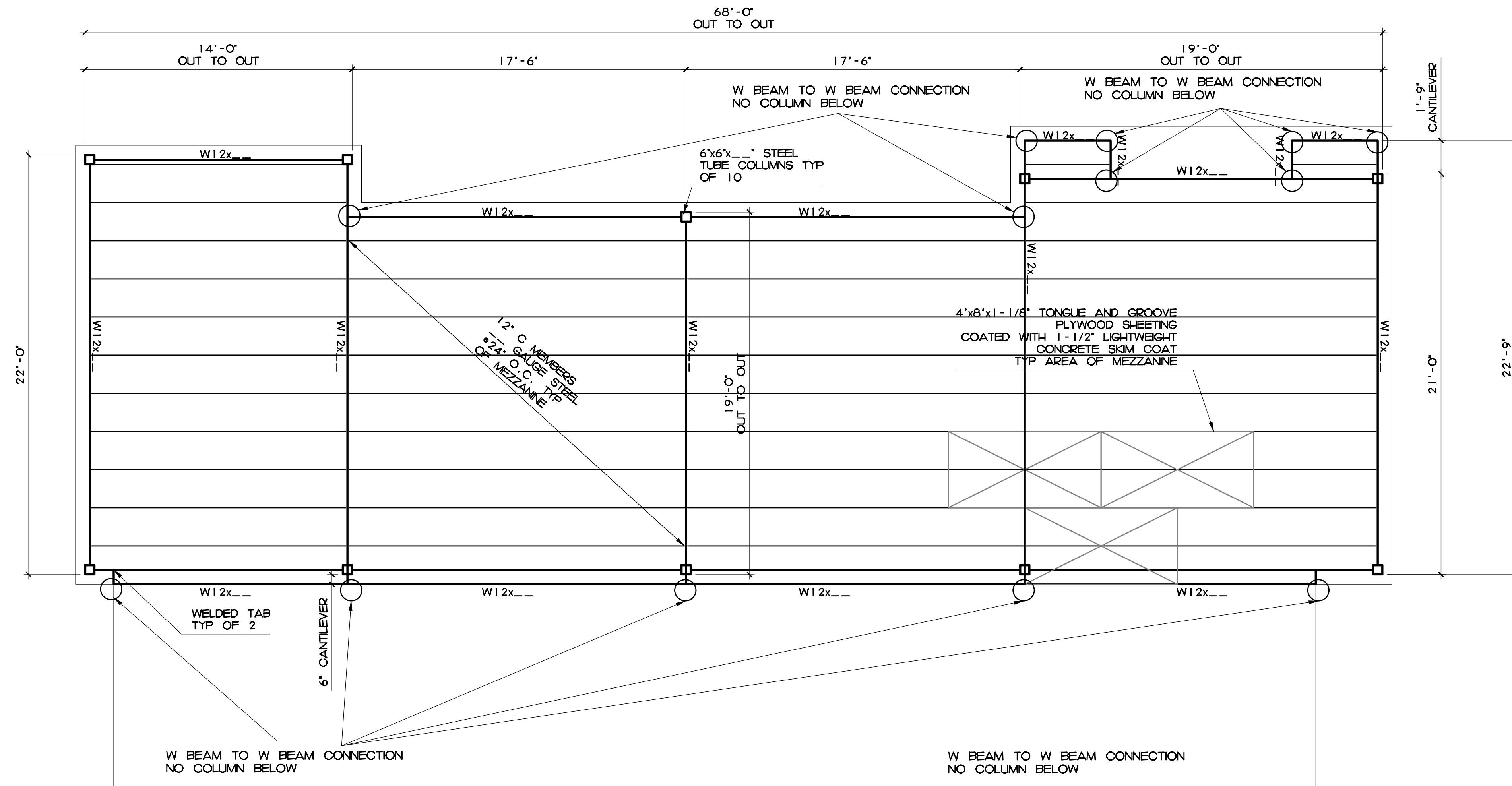
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BUILDING FLOOR PLANS

DATE: 07/31/19

PROJECT NO: 19-006

A1



INTEGRATED FLOOR FRAMING AS PER METAL BUILDING COMPANY

(PRELIMINARY FRAMING TO CHANGE UPON METAL BUILDING COMPANY'S ENGINEERING)

SCALE: 1/4 INCH = ONE FOOT



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SECOND FLOOR FRAMING PLAN

DATE: 07/31/19

PROJECT NO: 19-006

A2

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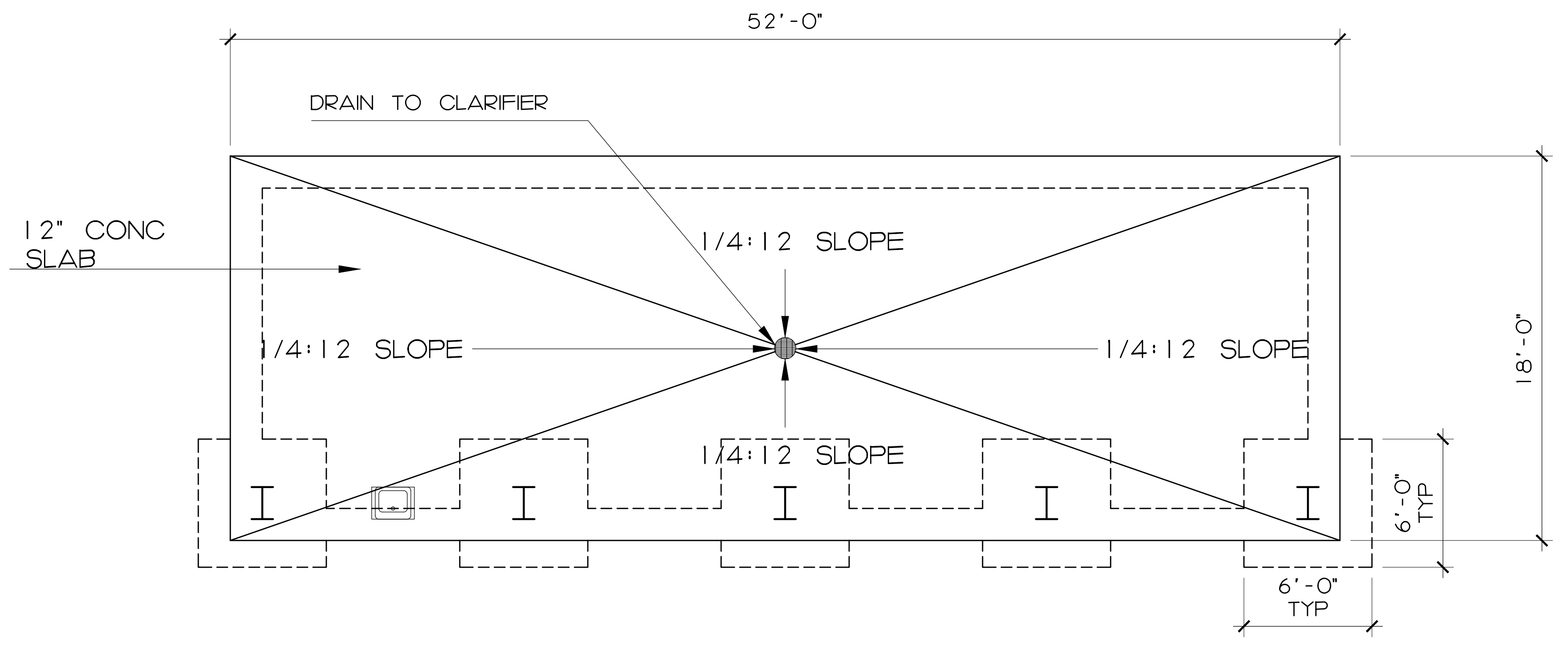
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CANOPY PLAN

DATE: 07/31/19

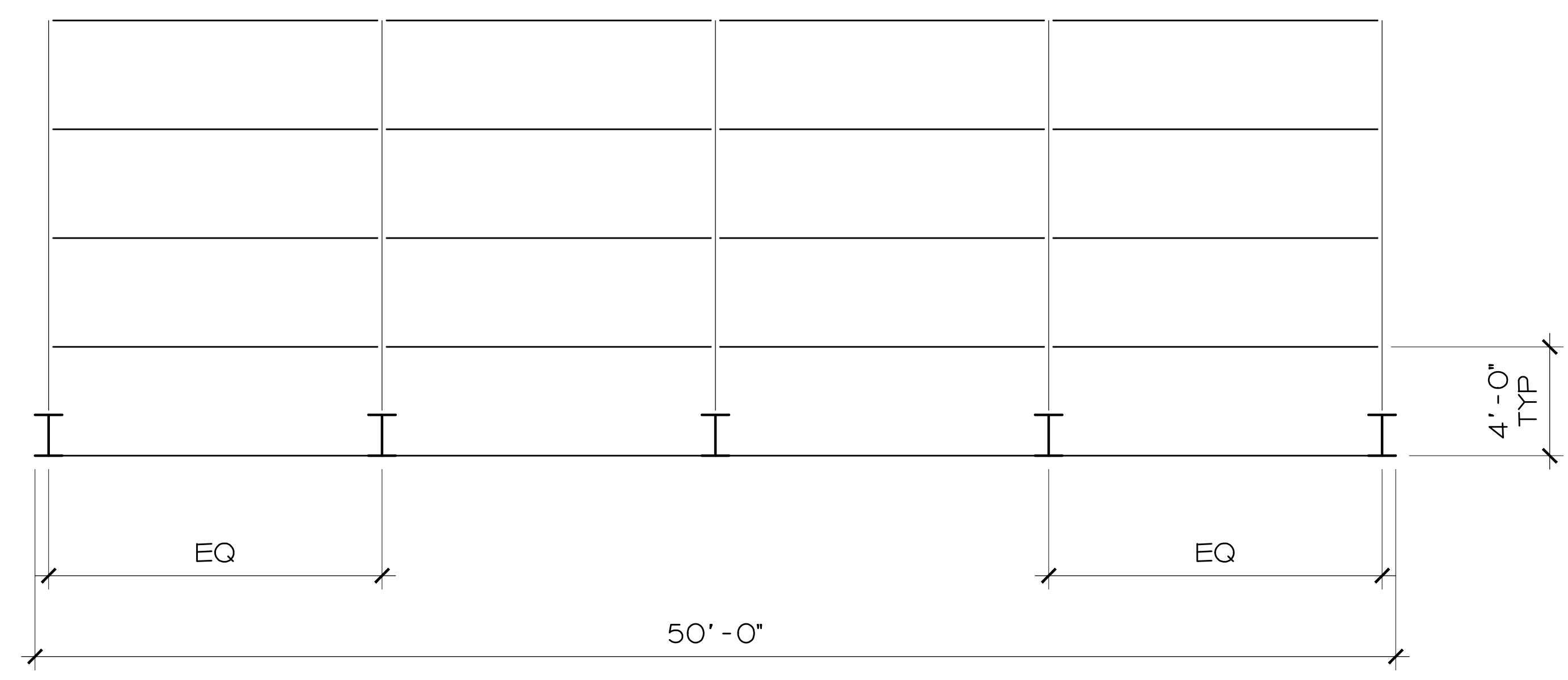
PROJECT NO: 19-006

A3



CANOPY FLOOR/FOUNDATION PLAN

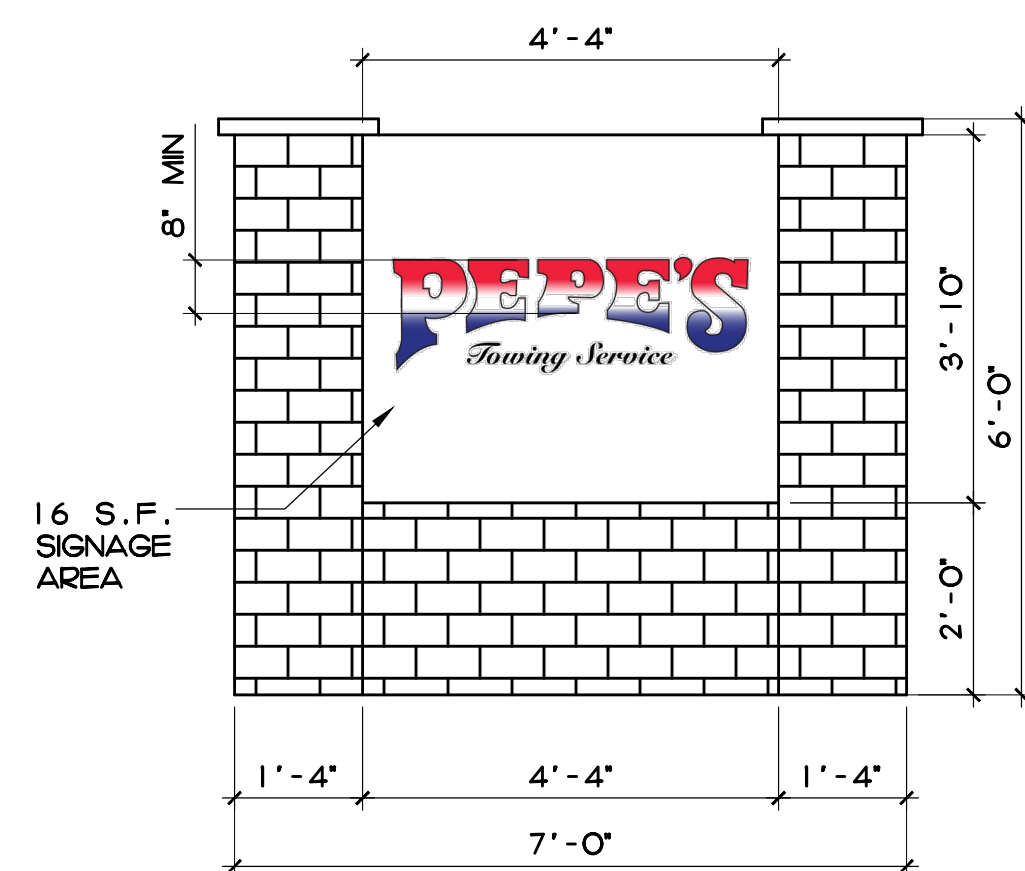
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CANOPY FRAMING PLAN

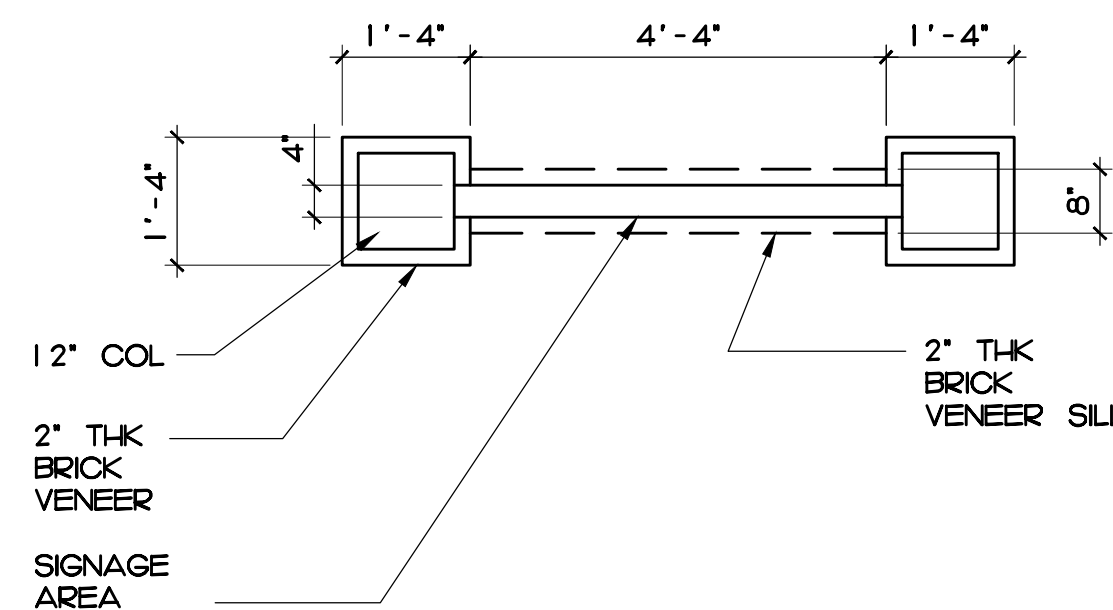
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NOTE: ALL LETTERING ON MONUMENT SIGN SHALL BE 8" MIN HEIGHT

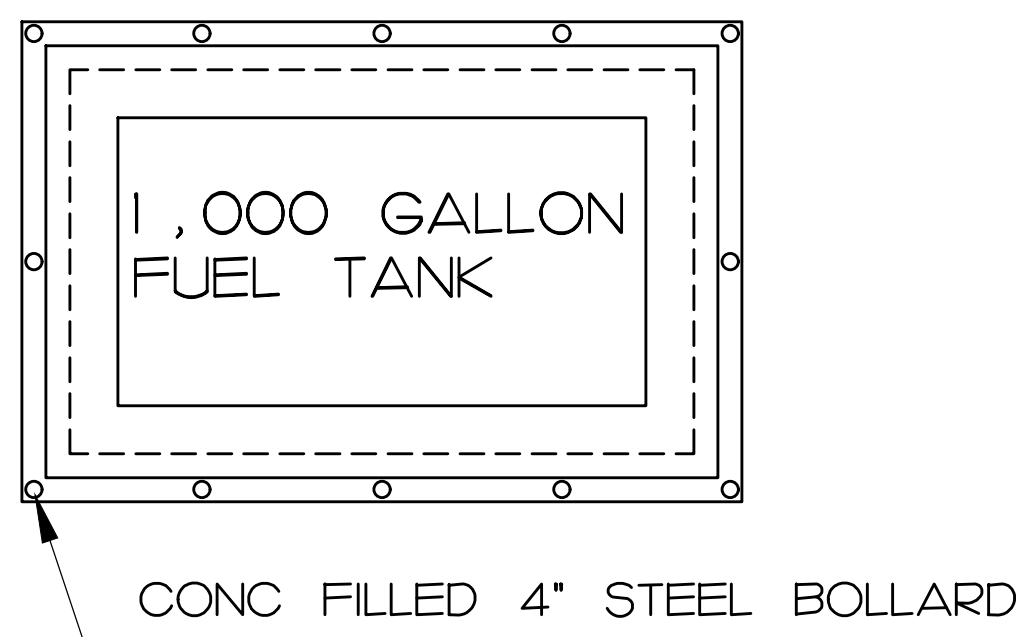
ELEVATION



PLAN

MONUMENT SIGNS

SCALE: 1/4 INCH = ONE FOOT



FUEL TANK PLAN

SCALE: 1/4 INCH = ONE FOOT



FLAG POLE PLAN

SCALE: 1/4 INCH = ONE FOOT

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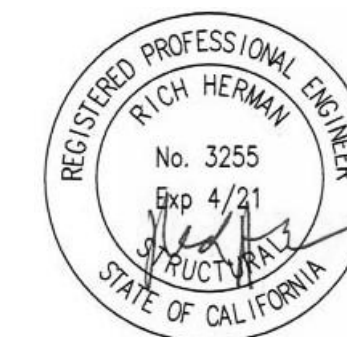
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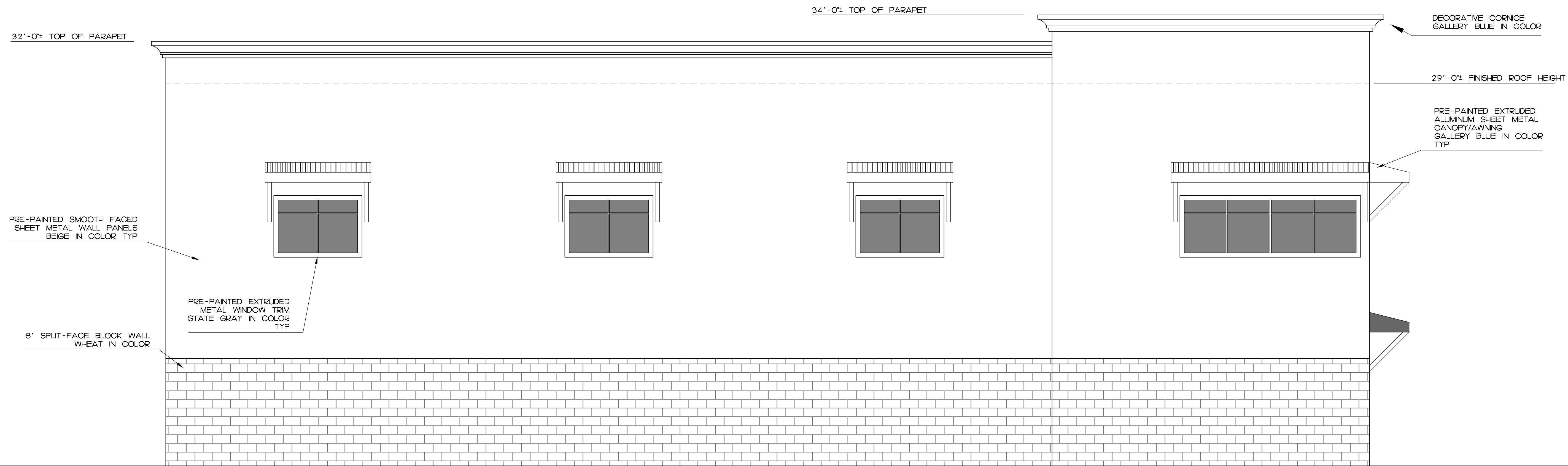
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**FUEL AREA
FLAG POLE
SIGNAGE**

DATE: 07/31/19

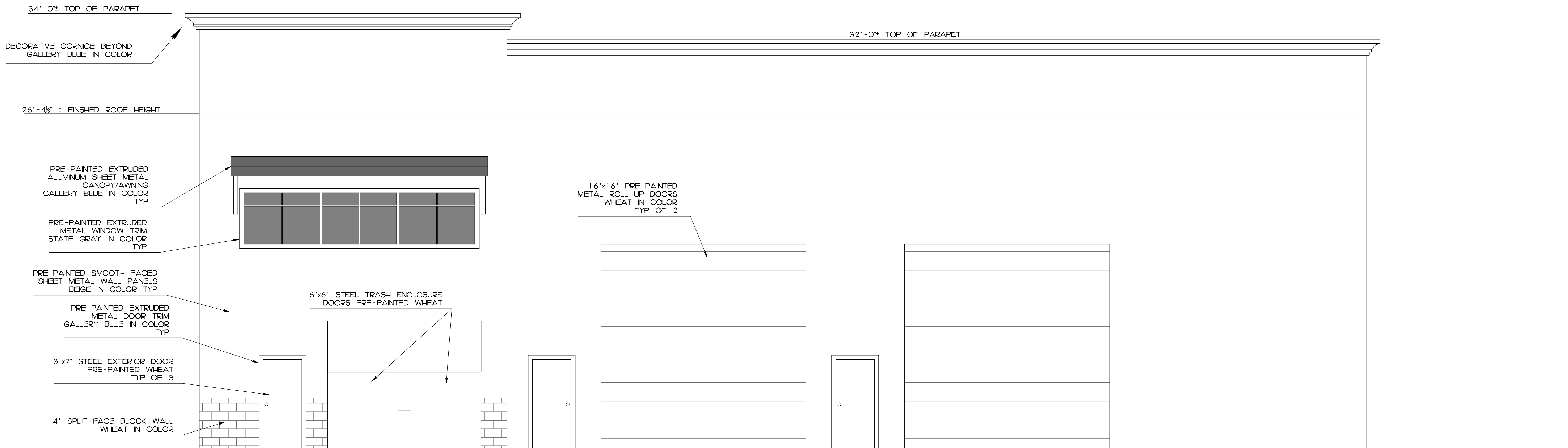
PROJECT NO: 19-006

A4



EAST ELEVATION

SCALE: 1/4 INCH = ONE FOOT



WEST ELEVATION

SCALE: 1/4 INCH = ONE FOOT

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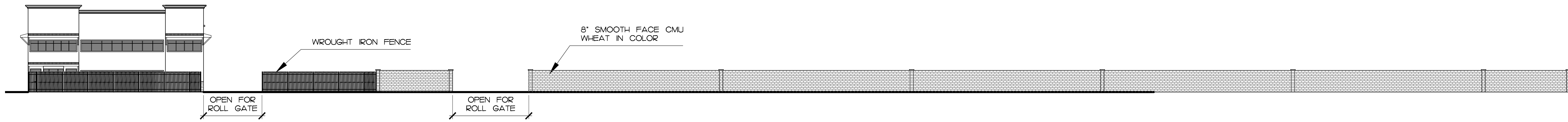
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BUILDING ELEVATIONS

DATE: 07/31/19

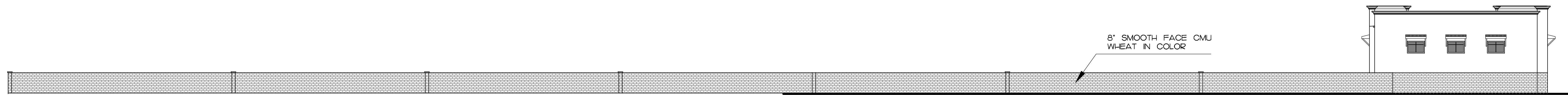
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A6



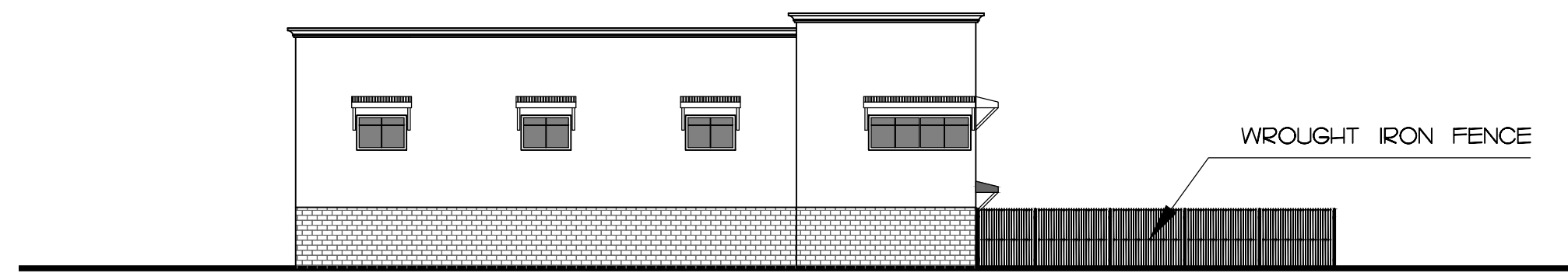
NORTH ELEVATION

SCALE: 1 INCH = 20 FEET



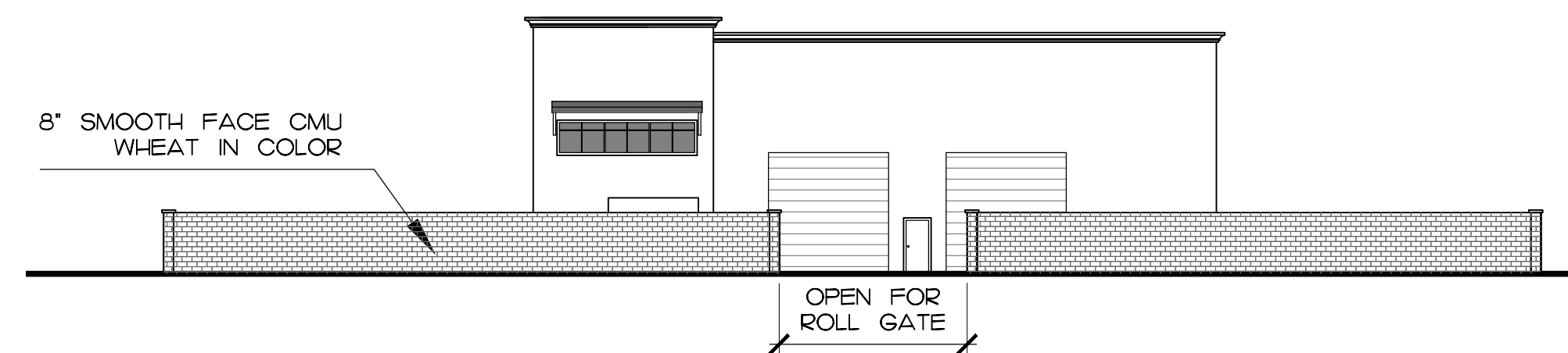
SOUTH ELEVATION

SCALE: 1 INCH = 20 FEET



EAST ELEVATION

SCALE: 1 INCH = 20 FEET



WEST ELEVATION

SCALE: 1 INCH = 20 FEET

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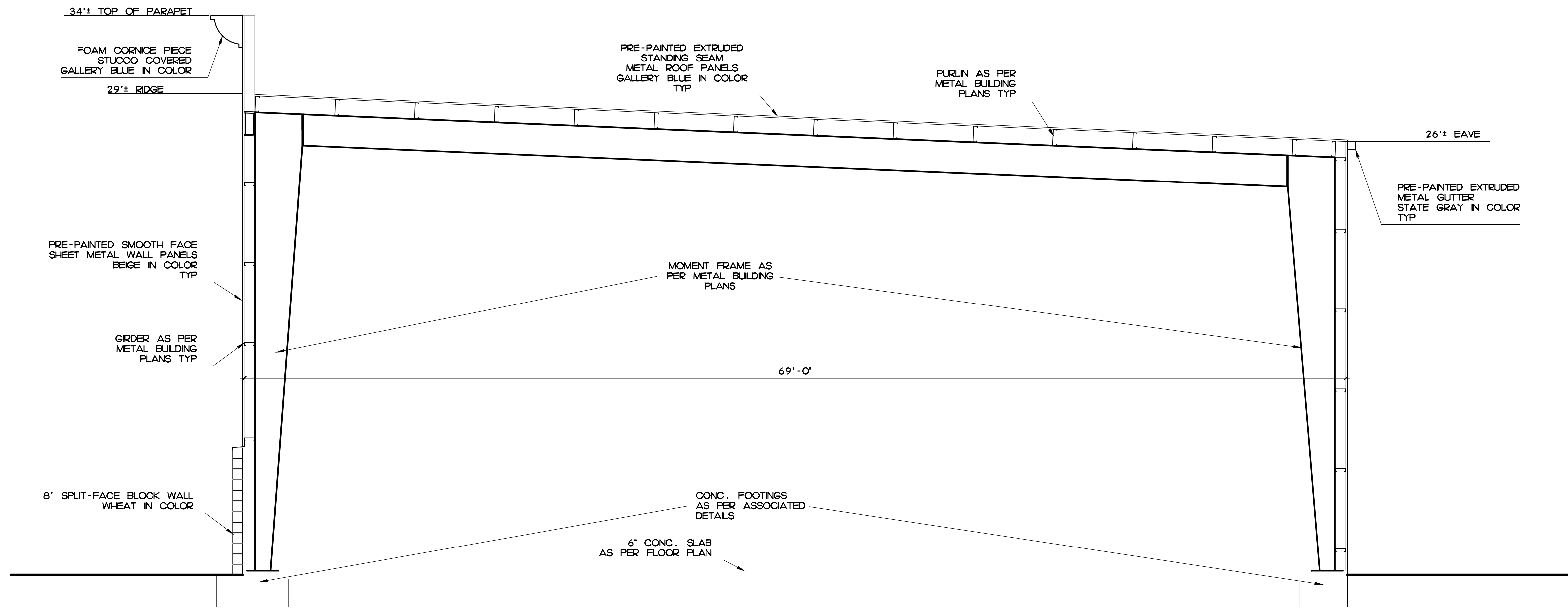
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FENCING ELEVATIONS

DATE: 07/31/19

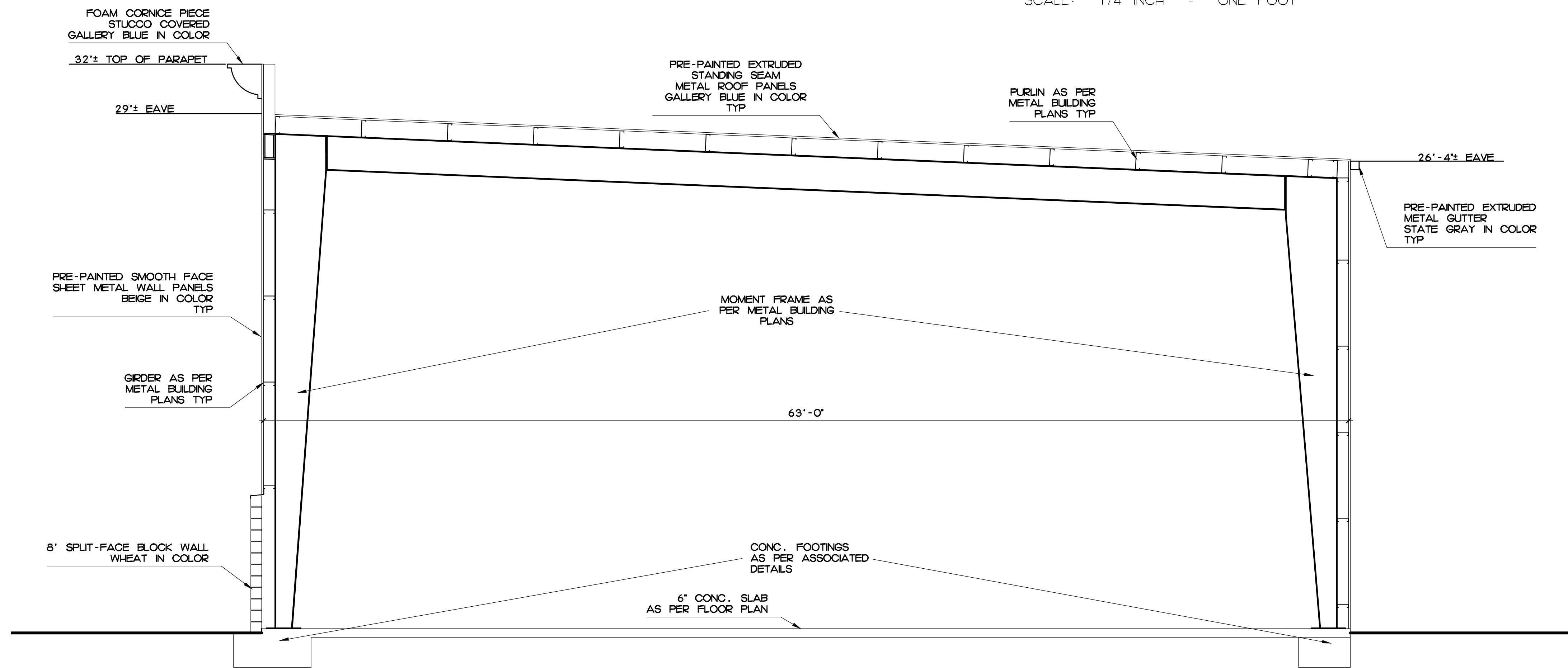
PROJECT NO: 19-006

A7



SECTION A

SCALE: 1/4 INCH = ONE FOOT



SECTION B

SCALE: 1/4 INCH = ONE FOOT

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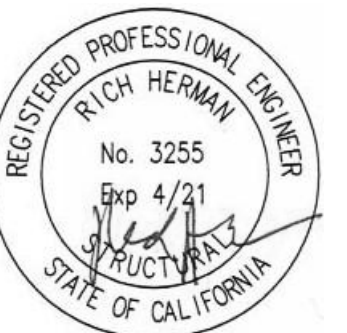
CONTRACTOR:

PROJECT NAME:

New Development For:
Pepe's Towing Co.
 500 East Belmont Street
 Ontario, California

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BUILDING SECTIONS

DATE: 07/31/19

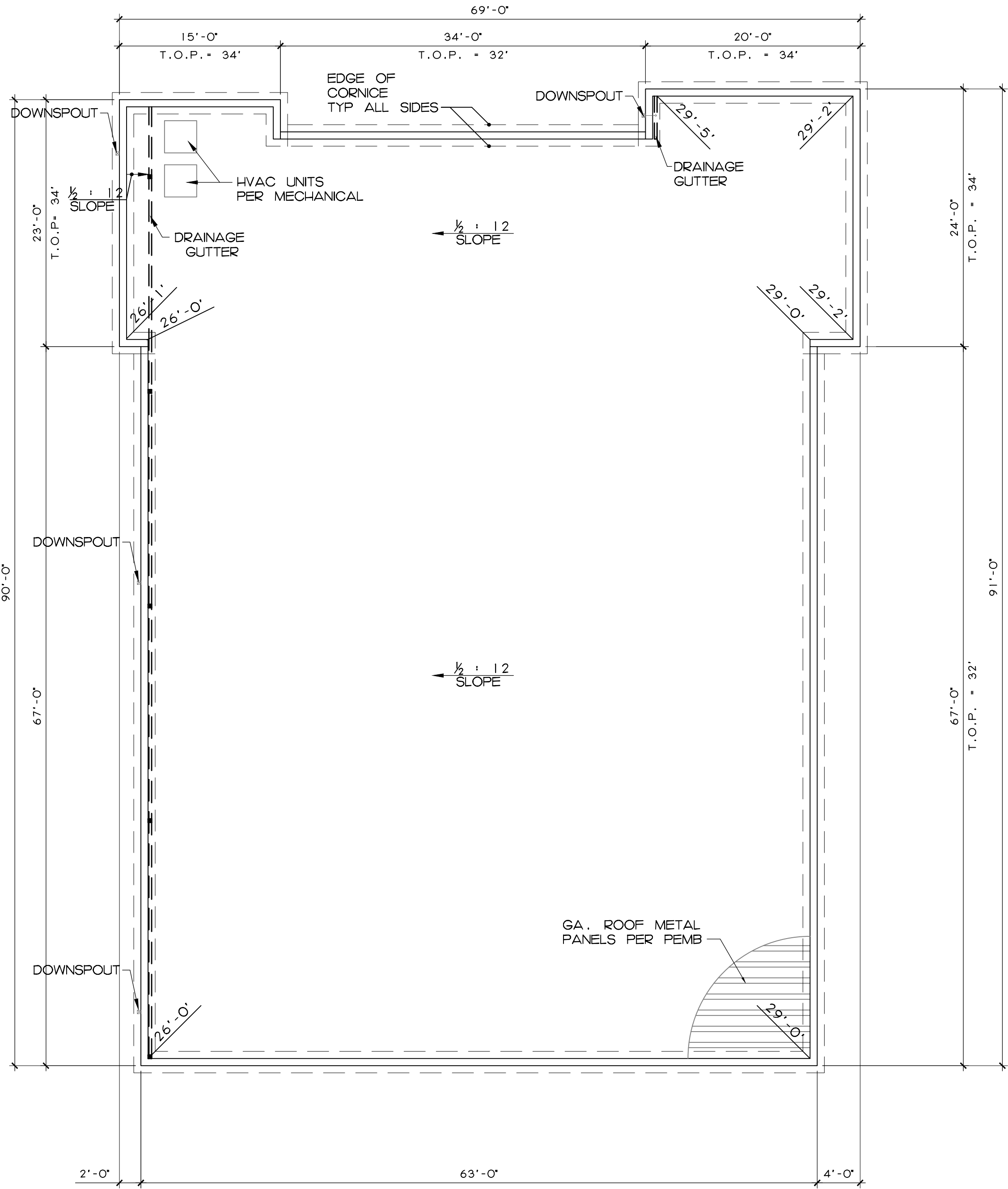
PROJECT NO: 19-006

A9

ROOF LAYOUT PLAN



SCALE: 1/8 INCH = ONE FOOT



STRUCTURAL ENGINEER
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STRUCTURAL ENGINEER
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CONTRACTOR:

PROJECT NAME:

New Development For:
Pepe's Towing Co.
500 East Belmont Street
Ontario, California

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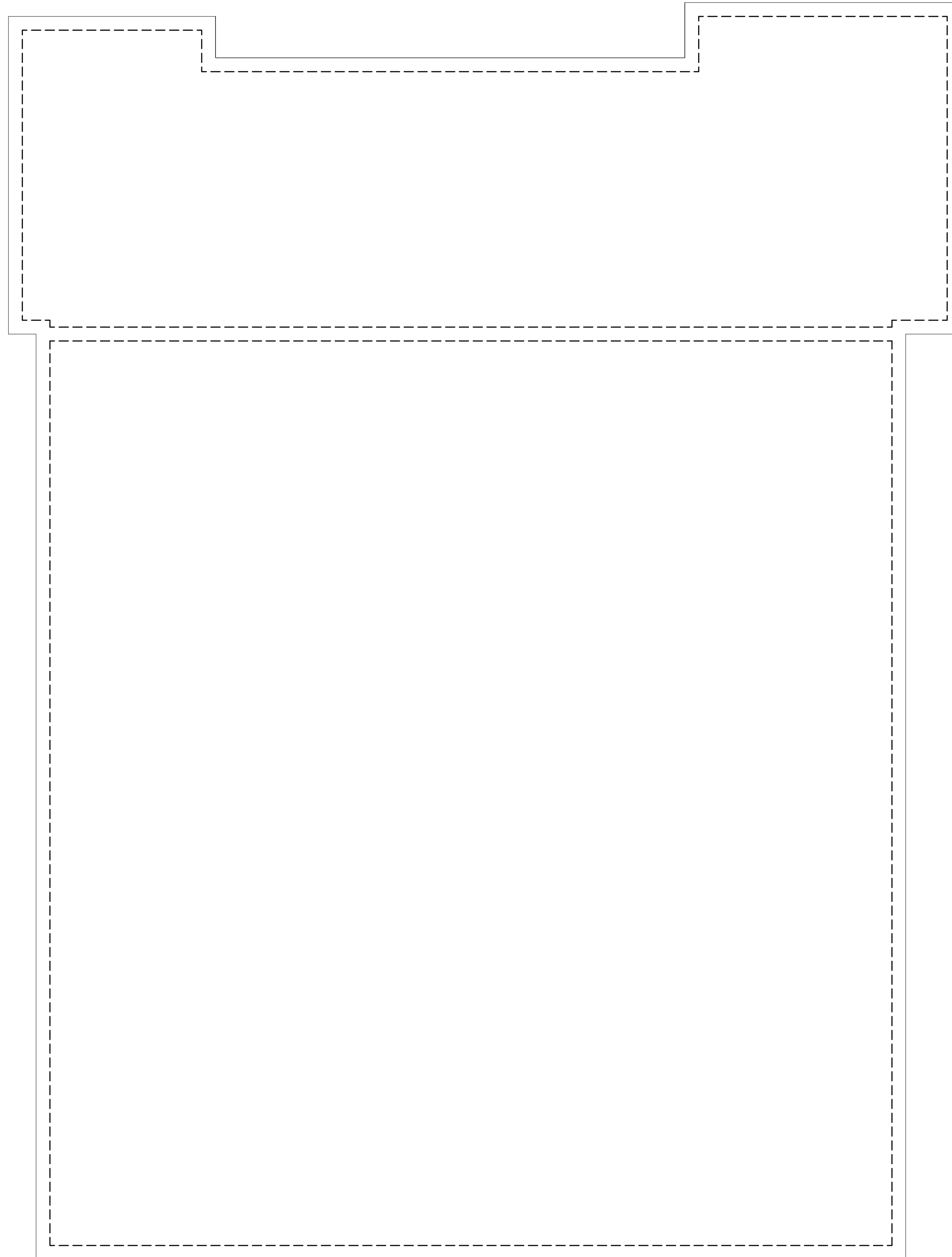
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ROOF LAYOUT

DATE: 07/31/19

PROJECT NO: 19-006

A10



FOUNDATION PLAN

(PRELIMINARY, WILL CHANGE AS PER METAL BUILDING ENGINEERING CALCS)

SCALE: 3/16 INCH = ONE FOOT



STRUCTURAL ENGINEER:
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CONTRACTOR:

PROJECT NAME:

New Development For:
Pepe's Towing Co.
 500 East Belmont Street
 Ontario, California

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FOUNDATION PLAN

DATE: 07/31/19

PROJECT NO: 19-006

S1

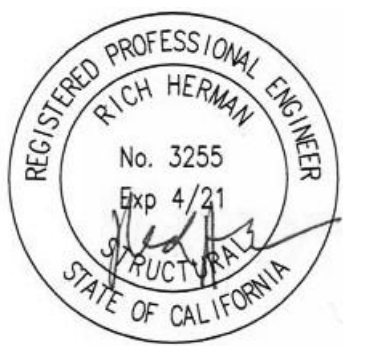
CONTRACTOR:

PROJECT NAME:

New Development For:
Pepe's Towing Co.
 500 East Belmont Street
 Ontario, California

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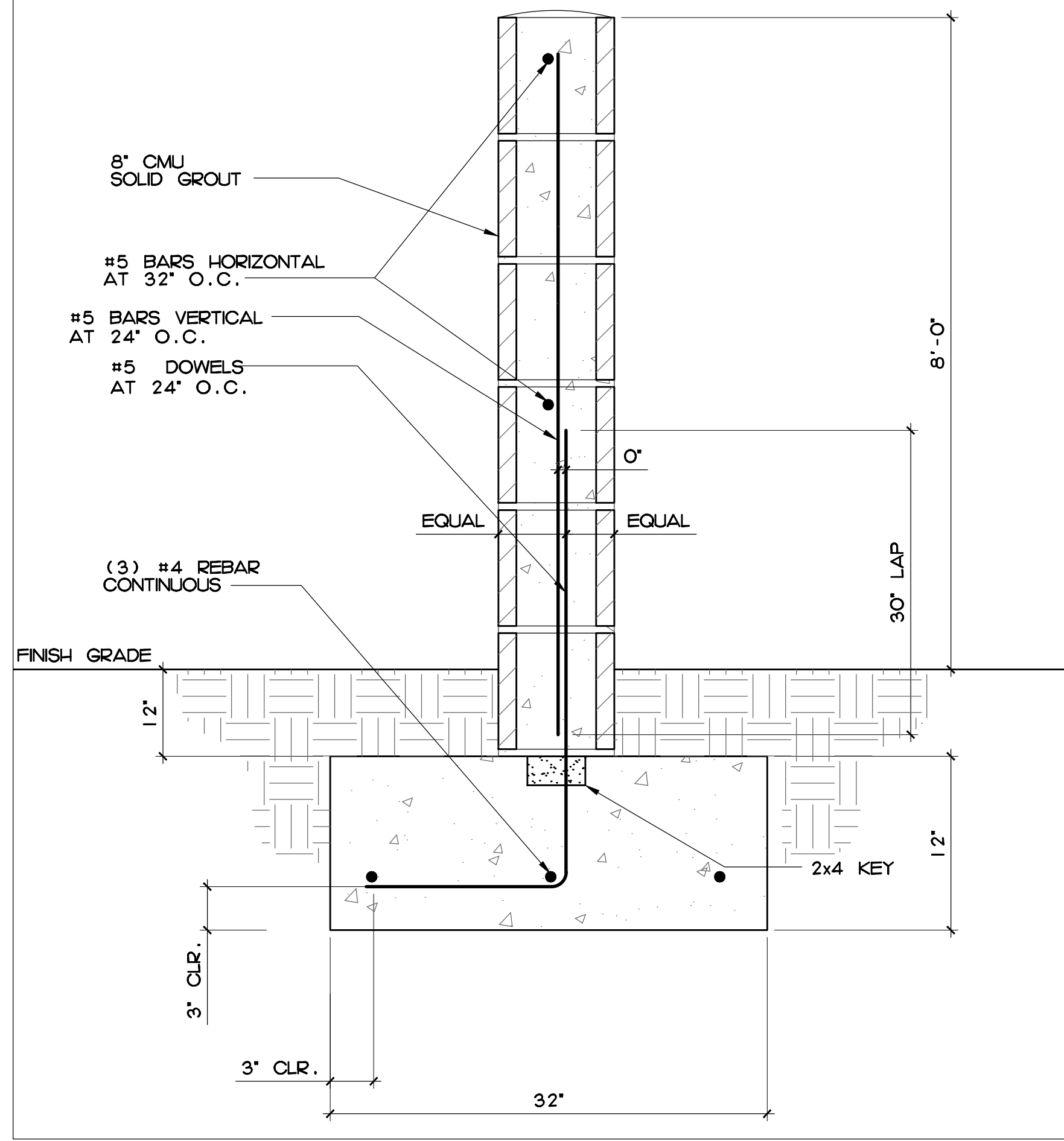
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DETAILS

DATE: 07/31/19

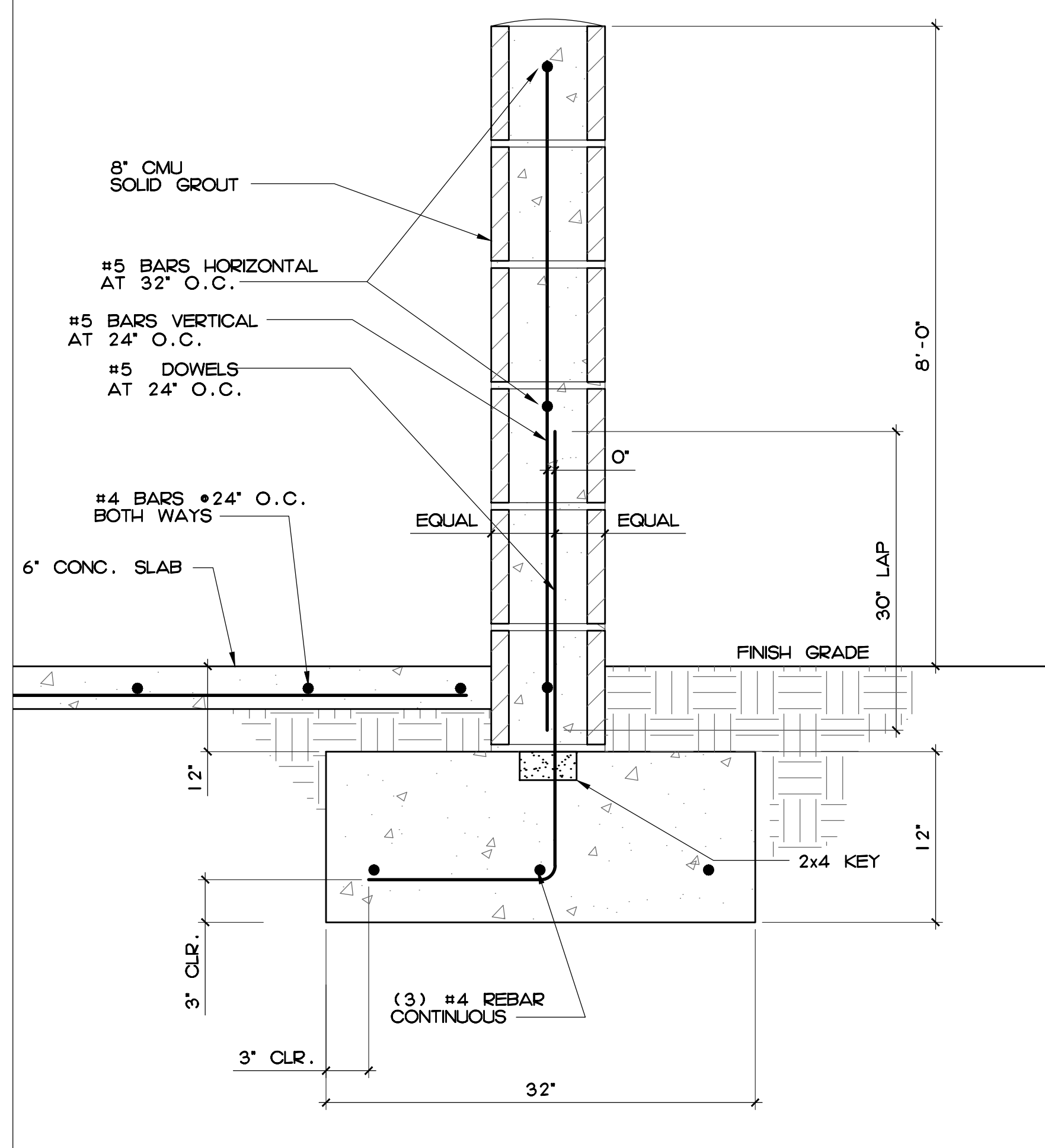
PROJECT NO: 19-006

D1



BLOCK WALL / FOOTING DETAIL

SCALE
NOT TO SCALE



BLOCK WALL AND FOOTING DETAIL WITH SLAB

SCALE
NOT TO SCALE

SCALE

17

SCALE

13

SCALE

9

SCALE

18

SCALE

14

SCALE

10

SCALE

19

SCALE

15

SCALE

11

SCALE

20

SCALE

16

SCALE

12

Attachment 2



Sound Study ↓



Attachment 3

Barrier Modeling Results

Barrier Calculations - CMU Wall

Input Variables

Reference Noise Level (dBA)	70
Reference Distance (ft)	25
Site Conditions <i>(Choice: Hard or Soft)</i>	Hard

Output Calculations

Distance from Barrier to Source (ft)	Distance from Barrier to Receiver (ft)	Distance from Source to Receiver (ft)	Height of Source (ft)	Height of Wall (ft)	Height of Receiver (ft)	Noise Level Reduction (dBA)	Unabated Noise Level (dBA)	Resultant Noise Level (dBA)
10	75	85	6	8	5	8.19	59	51.18

Barrier Calculations - Industrial Building

Input Variables

Reference Noise Level (dBA)	70
Reference Distance (ft)	25
Site Conditions <i>(Choice: Hard or Soft)</i>	Hard

Output Calculations

Distance from Barrier to Source (ft)	Distance from Barrier to Receiver (ft)	Distance from Source to Receiver (ft)	Height of Source (ft)	Height of Wall (ft)	Height of Receiver (ft)	Noise Level Reduction (dBA)	Unabated Noise Level (dBA)	Resultant Noise Level (dBA)
95	75	170	6	32	5	20.00	53	33.35



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April 8, 2020
Project No. 20-09388

Pepe's Towing Service
Attn: Lorenzo Navarro
14351 Veterans Way
Moreno Valley, CA 92553

**Subject: Noise Assessment for Pepe's Towing Service Ontario Towing Yard Project
550 East Belmont Street, Ontario, California 91761**

Dear Mr. Navarro:

This Noise Memorandum summarizes the results of the noise analysis for the Pepe's Towing Service Ontario Towing Yard Project (herein referred to as "proposed project" or "project"). The project involves construction of a 6,600-square foot industrial building and towing yard facility on a 3.01-acre project site at 550 East Belmont Street (Assessor's Parcel Numbers 1049-491-01, -02, and -03) in the city of Ontario. The towing yard facility would include a truck wash area where employees would hand wash trucks with hoses and hand cloths; no stationary car wash equipment or drying fans are proposed. The project would also include an approximately eight-foot-high concrete masonry unit (CMU) wall that would surround the majority of the project site with the exception of access driveways, the employee and visitor parking area, and the industrial building. See Attachment 1 for the project site plans. It is our understanding that the City of Ontario (City) has requested a noise assessment for the truck wash area in accordance with the City's exterior noise standards in its project review comments dated February 18, 2020. As detailed in the analysis below, the project would be consistent with the City's exterior and interior noise standards pursuant to Ontario Municipal Code (OMC) Sections 5-29.04 and 5-29.05.

Noise Overview

Sound is a vibratory disturbance created by a moving or vibrating source, which is capable of being detected by the hearing organs (e.g., the human ear). Noise is defined as sound that is loud, unpleasant, unexpected, or undesired and may therefore be classified as a more specific group of sounds. The effects of noise on people can include general annoyance, interference with speech communication, sleep disturbance, and, in the extreme, hearing impairment.¹

Noise levels are commonly measured in decibels (dB) using the A-weighted sound pressure level (dBA). The A-weighting scale is an adjustment to the actual sound pressure levels so that they are consistent with the human hearing response, which is most sensitive to frequencies around 4,000 Hertz (Hz) and less sensitive to frequencies around and below 100 Hz.² Decibels are measured on a logarithmic scale that quantifies sound intensity in a manner similar to the Richter scale used to measure earthquake

1 Crocker, Malcolm J. (Editor). 2007. Handbook of Noise and Vibration Control Book, ISBN: 978-0-471-39599-7, Wiley-VCH, October.

2 Kinsler, Lawrence E., Austin R. Frey, Alan B. Coppens, and James V. Sanders. 1999. Fundamentals of Acoustics, 4th Edition. ISBN 0-471-84789-5. Wiley-VCH, December 1999.



magnitudes. A doubling of the energy of a noise source, such as a doubling of traffic volume, would increase the noise level by 3 dB; similarly, dividing the energy in half would result in a decrease of 3 dB.³

Human perception of noise has no simple correlation with acoustical energy. The perception of noise is not linear in terms of dBA or in terms of acoustical energy. Two equivalent noise sources combined do not sound twice as loud as one source. It is widely accepted that the average healthy ear can barely perceive changes of 3 dBA, increase or decrease; that a change of 5 dBA is readily perceptible; and that an increase (decrease) of 10 dBA sounds twice (half) as loud.⁴

The impact of noise is not a function of loudness alone. The time of day when noise occurs and the duration of the noise are also important. In addition, most noise that lasts for more than a few seconds is variable in its intensity. Consequently, a variety of noise descriptors has been developed. The noise descriptors used for this analysis are the one-hour equivalent noise level ($L_{eq[1h]}$) and the community noise equivalent level (CNEL).

- The L_{eq} is the level of a steady sound that, in a stated time period and at a stated location, has the same A-weighted sound energy as the time-varying sound. For example, $L_{eq(1h)}$ is the equivalent noise level over a 1-hour period and is a common metric for limiting nuisance noise.
- The CNEL is a 24-hour equivalent sound level. The CNEL calculation applies an additional 5 dBA penalty to noise occurring during evening hours (i.e., 7:00 p.m. to 10:00 p.m.) and an additional 10 dBA penalty is added to noise occurring during nighttime hours (i.e., 10:00 p.m. to 7:00 a.m.). These increases for certain times are intended to account for the added sensitivity of humans to noise during the evening and nighttime periods.

Sound from a small, localized source (approximating a “point” source) decreases or drops off at a rate of 6 dBA for each doubling of the distance from the source.

Existing Noise Setting

The primary sources of noise in the project site vicinity are vehicular traffic on Phillips Street and railroad operations on the Union Pacific Railroad track that runs parallel to South Monterey Avenue. Figure S-3a of the Ontario Policy Plan Safety Element *Future Roadway Noise Contours* map identifies ambient traffic noise levels at the project site and surrounding area to be between 60 and 65 CNEL.⁵ In addition, Map 2-3, *Compatibility Policy Map: Noise Impact Zones*, of the Los Angeles/Ontario International Airport Land Use Compatibility Plan identifies airport noise levels at the project site and surrounding area to be between 65 and 70 CNEL.^{6, 7} These noise level ranges may be a conservative underestimate of noise levels at the project site and surrounding area because they do not include noise generated by rail operations on the Union Pacific Railroad track.

³ Crocker, Malcolm J. (Editor). 2007. Handbook of Noise and Vibration Control Book, ISBN: 978-0-471-39599-7, Wiley-VCH, October.

⁴ California Department of Transportation (Caltrans). 2013a. Technical Noise Supplement to the Traffic Noise Analysis Protocol. (CT-HWANP-RT-13-069.25.2) September. http://www.dot.ca.gov/hq/env/noise/pub/TeNS_Sept_2013B.pdf (accessed August 2019).

⁵ Ontario, City of. 2019. City of Ontario Policy Plan. Adopted January 27, 2010. Last amended July 16, 2019. <http://www.ontarioplan.org/policy-plan/> (accessed March 2020).

⁶ Ontario, City of. 2011. Ontario International Airport Land Use Compatibility Plan. Adopted April 19, 2011. <http://www.ontarioplan.org/alucp-for-ontario-international-airport> (accessed March 2020).

⁷ Due to the Executive Order N-33-20, which was in effect at the time of this study, noise measurements were not taken at the project site because this activity does not fall under a critical infrastructure sector and therefore Rincon staff were not exempt from stay-at-home requirements.



The City of Ontario Policy Plan and OMC do not define noise-sensitive land uses. However, the most stringent noise standards in OMC Sections 5-29.05, 5-29.05, and 5-29.11 and Ontario Policy Plan Table LU-7 are applied to single-family and multi-family residential land uses, mobile home parks, schools, day care centers, hospitals and similar health care institutions, churches, libraries, museums, auditoriums, concert halls, outdoor music shells, and amphitheaters.⁸ Therefore, for the purposes of this analysis, these land uses are considered to be noise-sensitive land uses. The nearest noise-sensitive land uses to the project site are single-family residences located to the west, south, and east. Other noise-sensitive land uses within 0.25 mile include Friendship Missionary Baptist Church (approximately 460 feet to the west), Delgado Family Child Care (approximately 530 feet to the northeast), Church of God of Prophecy (approximately 600 feet to the east), Love Chapel Church (approximately 760 feet to the west), De Anza Middle School (approximately 900 feet to the south), and Hope Chapel (approximately 1,200 feet to the northwest).

City of Ontario Policy Plan

The following goal and policy of the City of Ontario Policy Plan Safety Element would be applicable to the proposed project:⁹

Goal S4 An environment where noise does not adversely affect the public's health, safety, and welfare.

Policy S4-1 Noise Mitigation. We utilize the City's Noise Ordinance, building codes and subdivision and development codes to mitigate noise impacts.

In addition, Table LU-7 of the City of Ontario Policy Plan Land Use Element includes noise/land use compatibility guidelines, which are reproduced herein as Figure 1.

⁸ Ontario, City of. 2019. City of Ontario Policy Plan. Adopted January 27, 2010. Last amended July 16, 2019. <http://www.ontarioplan.org/policy-plan/> (accessed March 2020).

⁹ Ontario, City of. 2019. City of Ontario Policy Plan. Adopted January 27, 2010. Last amended July 16, 2019. <http://www.ontarioplan.org/policy-plan/> (accessed March 2020).



Figure 1 Noise Level Exposure and Land Use Compatibility Guidelines

LAND USE CATEGORIES		COMMUNITY NOISE EQUIVALENT LEVEL (CNEL)					
Category	Land Use	55	60	65	70	75	80
Residential/ Lodging	Single Family / Duplex	Green	Green	Yellow	Orange	Red	Red
	Multi-Family	Green	Green	Yellow	Orange	Orange	Red
	Mobile Homes	Green	Green	Yellow	Red	Red	Red
	Hotel/Motels	Green	Green	Green	Yellow	Orange	Orange
Public/Institutional	Schools/Hospitals	Green	Green	Yellow	Orange	Red	Red
	Churches/ Libraries	Green	Green	Yellow	Orange	Red	Red
	Auditoriums/Concert Halls	Green	Yellow	Orange	Orange	Red	Red
Commercial	Offices	Green	Green	Green	Yellow	Yellow	Orange
	Retail	Green	Green	Green	Yellow	Orange	Red
Industrial	Manufacturing	Green	Green	Green	Yellow	Orange	Orange
	Warehousing	Green	Green	Green	Yellow	Yellow	Orange
Recreational/ Open Space	Parks/Playgrounds	Green	Green	Green	Yellow	Orange	Red
	Golf Courses/ Riding Stables	Green	Green	Green	Yellow	Orange	Red
	Outdoor Spectator Sports	Green	Green	Yellow	Orange	Orange	Red
	Outdoor Music Shells/ Amphitheaters	Yellow	Yellow	Orange	Red	Red	Red
	Livestock/Wildlife Preserves	Green	Green	Green	Green	Orange	Red
	Crop Agriculture	Green	Green	Green	Green	Green	Green

LEGEND

	Clearly Acceptable:	No special noise insulation required, assuming buildings of normal conventional construction.
	Normally Acceptable:	Acoustical reports will be required for major new residential construction. Conventional construction with closed windows and fresh air supply systems of air conditioning will normally suffice.
	Normally Unacceptable:	New construction should be discouraged. Noise/aviation easements required for all new construction. If new construction does proceed, a detailed analysis of noise reduction requirements must be made and necessary noise insulation features included.
	Clearly Unacceptable:	No new construction should be permitted.

Source: Table LU-7 of the City of Ontario Policy Plan Land Use Element



City of Ontario Municipal Code

The City's Noise Ordinance is codified as OMC Chapter 29. OMC Section 5-29.04 states:

- a. The following exterior noise standards (reproduced herein as Table 2), unless otherwise specifically indicated, shall apply to all properties within a designated noise zone.
- b. It is unlawful for any person at any location within the incorporated area of the City to create noise, or to allow the creation of any noise on property owned, leased, occupied or otherwise controlled by such person, which noise causes the noise level, when measured at any location on any other property, to exceed either of the following:
 1. The noise standard for the applicable zone for any 15-minute period; and
 2. A maximum instantaneous (single instance) noise level equal to the value of the noise standard plus 20 dBA for any period of time (measured using A-weighted slow response).
 - a. In the event the ambient noise level exceeds the noise standard, the maximum allowable noise level under such category shall be increased to reflect the maximum ambient noise level.
 - b. The Noise Zone IV standard shall apply to that portion of residential property falling within 100 feet of a commercial property or use, if the noise originates from that commercial property or use.
 - c. If the measurement location is on a boundary between two different noise zones, the lower noise level standard applicable to the noise zone shall apply.

Table 1 OMC Exterior Noise Standards¹

Noise Zone	Type of Land Use	Allowable Equivalent Noise Level (dBA L _{eq}) ²	
		7:00 a.m. – 10:00 p.m.	10:00 p.m. – 7:00 a.m.
I	Single-Family Residential	65	45
II	Multi-Family Residential, Mobile Home Parks	65	50
III	Commercial Property	65	60
IV	Residential Portion of Mixed Use	70	70
V	Manufacturing and Industrial, Other Uses	70	70

¹ If the ambient noise level exceeds the resulting standard, the ambient noise level shall be the standard.

² Measurements for compliance are made on the affected property pursuant to OMC Section 5-29.15.

dBA = A-weighted decibel; L_{eq} = equivalent noise level

Source: OMC Section 5-29.04(a)

OMC Section 5-29.05 states:

- a. The following interior noise standards (reproduced herein as Table 3), unless otherwise specifically indicated, shall apply to all properties within a designated noise zone.
- b. It is unlawful for any person at any location within the incorporated area of the City to create noise, or to allow the creation of any noise on property owned, leased, occupied or otherwise



controlled by such person, which noise causes the noise level, when measured at any location on any other property, to exceed either of the following:

1. The noise standard for the applicable zone for any 15-minute period; and
2. A maximum instantaneous (single instance) noise level equal to the value of the noise standard plus 20 dBA for any period of time (measured using A-weighted slow response).
 - a. In the event the ambient noise level exceeds the noise standard, the maximum allowable noise level under such category shall be increased to reflect the maximum ambient noise level.
 - b. The Noise Zone IV standard shall apply to that portion of residential property falling within 100 feet of a commercial property or use, if the noise originates from that commercial property or use.
 - c. If the measurement location is on a boundary between two different noise zones, the lower noise level standard applicable to the noise zone shall apply.

Table 2 OMC Interior Noise Standards¹

Noise Zone	Type of Land Use	Allowable Equivalent Noise Level (dBA L _{eq}) ²	
		7:00 a.m. – 10:00 p.m.	10:00 p.m. – 7:00 a.m.
I	Single-Family Residential	45	40
II	Multi-Family Residential, Mobile Home Parks	45	40
IV	Residential Portion of Mixed Use	45	40

¹ If the ambient noise level exceeds the resulting standard, the ambient noise level shall be the standard.

² Measurements for compliance are made on the affected property pursuant to OMC Section 5-29.15.

dBA = A-weighted decibel; L_{eq} = equivalent noise level

Source: OMC Section 5-29.05(a)

OMC Section 5-29.11 states:

“It is unlawful for any person to create any noise that causes the outdoor noise level at any school, day care center, hospital or similar health care institution, church, library or museum while the same is in use, to exceed the noise standards specified in OMC Section 5-29.04 prescribed for the assigned Noise Zone I.”

Impact Analysis

Methodology

The City of Ontario has requested a noise assessment for the proposed truck wash area to be located on the southern portion of the project site approximately 140 feet west of the intersection of Phillips Street and South Monterey Avenue. A reference noise level for the truck wash area was obtained from a noise assessment performed for a similar hand car wash in Cudworth, Barnsley, United Kingdom. The reference noise level measurement was taken for a facility comprised of two car wash bays at which vehicles are hand washed using jet sprays and hand cloths, then dried, polished, and vacuumed. Based on the noise assessment, a hand car wash generates average noise levels of up to approximately 70 dBA



L_{eq} at 25 feet.¹⁰ Use of the reference noise level from this noise assessment provides a conservative overestimate of project-generated noise levels because the project would not include the use of jet sprays or vacuums, which generate higher levels of noise than hoses and hand cloths used by the proposed project. See Appendix B for the full noise assessment.

Exterior noise levels at the receivers nearest to the project site were calculated using standard distance attenuation for point sources of 6 dBA per doubling of distance. The noise analysis also takes into account the presence of the proposed eight-foot-high CMU wall that would surround the majority of the project site as well as the proposed 32-foot-high industrial building to be located on the eastern portion of the project site. The CMU wall would block the line-of-sight between the truck wash area and residences to the south, and the industrial building would block line-of-sight between the truck wash area and residences to the east. The proposed CMU wall is calculated to provide an approximately 8-dBA reduction in ambient noise levels at receivers to the south of the project site and the proposed industrial building is calculated to provide an approximately 20-dBA reduction in ambient noise levels at receivers to the east of the project site. Barrier modeling outputs are included in Attachment 3.

According to the Federal Highway Administration, buildings provide exterior-to-interior noise level reductions ranging from 10 dBA for buildings with open windows to 35 dBA for masonry buildings with double glazed windows.¹¹ Interior noise levels at the nearest receivers to the project site were calculated using the estimated exterior noise levels and conservatively assuming an exterior-to-interior reduction of 10 dBA.

Project Impacts

Table 4 and Table 5 summarize daytime hourly exterior and interior noise levels, respectively, generated by the proposed project at the nearest receivers, including residences to the west, south, and east and industrial properties to the north. Noise levels were not estimated for nighttime hours (i.e., 10:00 p.m. to 7:00 a.m.) because the truck wash area would not be in operation during those hours. As shown in Table 4 and Table 5, respectively, exterior and interior noise levels from the truck wash area would not exceed the City's daytime exterior and interior noise level standards for the surrounding land uses.

¹⁰ Waterman Infrastructure & Environment Limited. 2016. Noise Assessment – Hand Car Wash and Valet Centre, Cudworth, Barnsley, S72 8YA. November 2016.

¹¹ Federal Highway Administration. 2011. Highway Traffic Noise: Analysis and Abatement Guidance. December 2011. <https://www.in.gov/indot/files/FHWA-Highway-Traffic-Noise-Analysis-and-Abatement-Guidance-December-2011.pdf> (accessed March 2020).



Table 3 Exterior Noise Levels at Nearest Receivers

	Noise Level (dBA L _{eq})			
	550 Phillips Street (85 feet to the south)	1214 S. Sultana Street (460 feet to the west)	Industrial Properties (180 feet to the north)	604 S. Monterey Avenue (170 feet to the east)
Truck Wash Area ¹	59	45	53	53
CMU Wall ^{2, 3}	(8)	n/a	n/a	n/a
Buildings ⁴	n/a	n/a	n/a	(20)
Estimated Noise Level	51	45	53	33
Exterior Noise Standard ⁵	65	65	70	65
Standard Exceeded?	No	No	No	No

¹ Based on a reference noise level of 70 dBA L_{eq} at 25 feet and a standard distance attenuation for point sources of 6 dBA per doubling of distance.

² See Attachment 3 for barrier modeling results.

³ The CMU wall would only partially block the line of sight between the truck wash area and residences to the west/industrial properties to the north; therefore, noise reductions from this wall are conservatively not included in the estimate of noise levels at these properties.

⁴ The proposed industrial building would block the line of sight between the truck wash area and residences to the east along South Monterey Avenue. See Attachment 3 for barrier modeling results.

⁵ See Table 2.

() denotes a negative number.

dBA = A-weighted decibel; L_{eq} = equivalent noise level; n/a = not applicable



Table 4 Interior Noise Levels at Nearest Receivers

	Noise Level (dBA L _{eq})			
	550 Phillips Street (85 feet to the south)	1214 S. Sultana Street (460 feet to the west)	Industrial Properties (180 feet to the north)	604 S. Monterey Avenue (170 feet to the east)
Truck Wash Area ¹	59	45	53	53
CMU Wall ^{2, 3}	(8)	n/a	n/a	n/a
Buildings ⁴	n/a	n/a	n/a	(20)
Interior-to-Exterior Reduction	(10)	(10)	(10)	(10)
Estimated Noise Level	41	35	43	23
Interior Noise Standard ⁵	45	45	n/a	45
Standard Exceeded?	No	No	No	No

¹ Based on a reference noise level of 70 dBA L_{eq} at 25 feet and a standard distance attenuation for point sources of 6 dBA per doubling of distance.

² See Attachment 3 for barrier modeling results.

³ The CMU wall would only partially block the line of sight between the truck wash area and residences to the west/industrial properties to the north; therefore, noise reductions from this wall are conservatively not included in the estimate of noise levels at these properties.

⁴ The proposed industrial building would block the line of sight between the truck wash area and residences to the east along South Monterey Avenue. See Attachment 3 for barrier modeling results.

⁵ See Table 3. The OMC does not establish interior noise standards for industrial properties.

() denotes a negative number.

dBA = A-weighted decibel; L_{eq} = equivalent noise level; n/a = not applicable

Conclusion

Noise levels generated by the project's truck wash area would not conflict with the City of Ontario's exterior and interior noise standards.

Thank you for the opportunity to assist with this assignment. Please do not hesitate to contact us if you have questions about this report.

Sincerely,
 Rincon Consultants, Inc.

Annaliese Miller
 Associate Environmental Planner
 (805) 644-4455 ext. 429
amiller@rinconconsultants.com

Joe Power
 Principal/Sr. Vice President
 (805) 644-4455
jpower@rinconconsultants.com



Attachments

- Attachment 1 Project Site Plans
- Attachment 2 Barrier Modeling Results

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PCUP18-021, A CONDITIONAL USE PERMIT TO OPERATE A TOWING SERVICE AND SHORT-TERM STORAGE OF AUTOMOBILES, VANS, LIGHT TRUCKS, AND TRACTOR TRUCKS ON 3.1 ACRES OF LAND LOCATED AT 580 EAST BELMONT STREET, WITHIN THE IL (LIGHT INDUSTRIAL) ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF (APNS: 1049-491-01, 1049-491-02 & 1049-491-03).

WHEREAS, Four Sisters Enterprises, LLC ("Applicant") has filed an Application for the approval of a Conditional Use Permit, File No. PCUP18-021, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 3.1 acres of land located at the southwest corner of Belmont Street and Monterey Avenue, within the IL (Light Industrial) zoning district; and

WHEREAS, the property to the north of the project site is within the IL (Light Industrial) zoning district and is developed with industrial buildings and a metal rebar storage yard. The properties to the east are within the LDR-5 (Low Density Residential – 2.1 to 5.0 DUs/Acre) and IL (Light Industrial) zoning districts and are developed with the Southern Pacific Transportation Company railroad right-of way and single-family homes. The properties to the south are located within the LDR-5 (Low Density Residential – 2.1 to 5.0 DUs/Acre) zoning district and are developed with single-family homes. The properties to the west are within the MDR-11 (Medium Density Residential – 5.1 to 11.0 DUs/Acre) zoning district and are developed with single-family homes; and

WHEREAS, pursuant to the Ontario Development Code, Conditional Use Permit approval is required to establish an outdoor automobile storage yard (tow yard) within the IL (Light Industrial) zoning district. A Conditional Use permit is required in order adequately review the potential impacts the use might have on the surrounding properties and eliminate or mitigate those impacts; and

WHEREAS, on June 21, 2018, the applicant submitted a Conditional Use Permit (File No. PCUP18-021) to establish and operate an outdoor automobile storage yard (tow yard) in conjunction with a Development Plan (File No. PDEV18-022) to construct a 6,870 square foot industrial building on the Project site; and

WHEREAS, Pepe's Towing currently operates another towing facility in the City, located at 810 East Main Street. The towing facility was approved with a Conditional Use Permit (File No. PCUP17-016) on February 21, 2018, by the Zoning Administrator; and

WHEREAS, the applicant is requesting to establish and operate a towing service and short-term storage of automobiles, vans, light trucks, and tractor trucks on 3.1 acres of land located at a 580 East Belmont Street, within the IL (Light Industrial) zoning district; and

WHEREAS, the proposed towing facility and vehicle impound storage yard, consists of a 79,569 square foot outdoor vehicle storage area that will accommodate up to 324 motor vehicles, including towing services and short-term storage of automobiles, vans, light trucks, and tractor trucks; and

WHEREAS, the towing service operation will also include a 6,870 square foot, two-story industrial building, and an 800 square foot outdoor truck wash area. The industrial building is comprised of an office area that includes a dispatch office, warehouse/storage area, and a vehicle evidence room. The evidence room will serve as a staging and storage area for non-operational vehicles that were involved in serious accidents that need to be secured for the Police Department inspections; and

WHEREAS, the towing service operation will have three full-time employees (dispatchers), one clerk, and two yardmen. Normal business office hours are from 8:00 a.m. to 5:00 p.m., Monday thru Friday. After hours customer services may be arranged on an appointment-only basis; and

WHEREAS, Pepe's Towing provides 24-hour roadside towing services, tire changes, fuel delivery, lock-out, and police storage services every day of the week. Vehicles from Ontario Police Department, Chino Hills Sheriff Department, Rancho Cucamonga California Highway Patrol (CHP), as well as vehicles within a 10-mile radius will also be towed to the subject location; and

WHEREAS, tow vehicles will have access to the facility on a 24 hour basis, every day of the week. Three tow vehicles will be used by the business, including flatbed and wheel-lift trucks to accommodate automobiles, vans, and light trucks, and a heavy-duty truck to accommodate tractor trucks; and

WHEREAS, the Applicant submitted an updated Noise Assessment prepared by Rincon Consultants (dated July 6, 2020) to include noise generated during typical tow-related activities. The initial Noise assessment (Rincon Consultants, dated April 8, 2020) only analyzed noise levels from the proposed truck wash facility. The Noise Assessment concluded that the proposed tow operations and truck wash facility will not generate noise levels beyond the City's allowed noise levels for the adjacent residential zones to the west, south and east of the Project site. Per the Ontario Municipal Code Noise Ordinance (Ontario Municipal Code Title 5, Chapter 29 (Noise), commencing with Section 5-29.01), the maximum exterior noise level allowed within residential zoning districts is 65 decibels (dBA), between the hours of 7:00 a.m. to 10:00 p.m. According to the Noise Assessment,

the exterior noise level generated by the truck wash area will not exceed 59 dBA, the daytime tow operations would not exceed 55 dBA and the nighttime tow operations would not exceed 44 dBA. Therefore, the proposed outdoor truck washing and tow activities will comply with the City's maximum allowed exterior noise level for residential zoning districts and are not anticipated to adversely affect the adjacent residents; and

WHEREAS, on May 26, 2020, the Planning Commission approved the proposed Project, subject to conditions. Subsequently, the project was appealed to City Council on June 5, 2020; and

WHEREAS, on July 7, 2020, the City Council held a Public Hearing to hear the appeal and recommended that the project return to Planning Commission to provide the Commission the opportunity to review new project information presented by the applicant, regarding the operation of the towing facility; and

WHEREAS, the applicant submitted updated information for the towing operation, including a Health Risk Assessment (HRA). The HRA prepared by the Ganddini Group (dated February 15, 2021) analyzed the potential impacts from the proposed Tow Yard Facility and concluded that the proposed Project will not impose significant health impacts on the surrounding environment; and

WHEREAS, a notice was mailed on March 18, 2021, to all property owners beyond 500-foot radius of the Project site, in both English and Spanish. The intent of the notification was to inform the surrounding residents and property owners of the proposed Project Area and to address questions and/or concerns they may have regarding the Project; and

WHEREAS, on March 30, 2021, the Planning Department hosted a virtual Zoom Community meeting for Pepe's Towing Project. The purpose of the community meeting was to present the community with the proposed project and the new information provided by the applicant and to gather the community's input and comments. The community meeting was held in both English and Spanish; and

WHEREAS, in order to help residents that did not have access to a computer, the Dorothy Quesada Community Center was provided as a location to view and participate in the virtual Zoom meeting. Planning Department staff were on hand to provide Spanish translation assistance and ensure that proper social distancing measures were followed. A total of 10 people attended the meeting at the Quesada Community Center and 18 people logged on to the virtual Zoom meeting; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA"); and

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan ("ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 18, 2020, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision DAB20-023, recommending the Planning Commission approve the Application; and

WHEREAS, on May 26, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to approve Decisions PC20-023 and PC20-024; and

WHEREAS, the project was appealed to City Council on June 5, 2020; and

WHEREAS, on July 7, 2020, the City Council held a Public Hearing to hear the appeal. After the applicant and community testimony, the City Council recommended that the project return to Planning Commission to provide the Commission the opportunity to

review new project information presented by the applicant, regarding the operation of the towing facility; and

WHEREAS, on May 25, 2021, the Planning Commission of the City of Ontario conducted a hearing to reconsider the Project with new project information, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the decision-making body for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(2) The Project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines and meets each of the following conditions: (a) the Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations; (b) the proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses; (c) the Project site has no value as habitat for endangered, rare, or threatened species; (d) approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and (e) the Project site can be adequately served by all required utilities and public services; and

(3) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

(4) The determination of CEQA exemption reflects the independent judgment of the Planning Commission.

SECTION 2: Housing Element Compliance. Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based on

the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 3: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan (“ALUCP”), establishing the Airport Influence Area for Ontario International Airport (“ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making body for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the Planning Commission, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 4: *Concluding Facts and Reasons.* Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing and upon the specific findings set forth in Sections 1 through 3, above, the Planning Commission hereby concludes as follows:

(1) ***The scale and intensity of the proposed land use would be consistent with the scale and intensity of land uses intended for the particular zoning or land use district.*** The proposed location of the Conditional Use Permit is in accord with the objectives and purposes of the City of Ontario Development Code and the IL (Light Industrial) zoning district, and the scale and intensity of land uses intended for the zoning district in which the use is proposed to be located. Furthermore, the proposed outdoor automobile storage yard (tow yard) will be established and operated consistent with the objectives and purposes, and development standards and guidelines, of the IL (Light Industrial) zoning district; and

(2) ***The proposed use at the proposed location, and the manner in which it will be operated and maintained, is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed industrial building and outdoor automobile storage yard (tow yard) will be located within the industrial land use district of the Policy Plan Land Use Map, and the IL (Light Industrial) zoning district. The development standards, and the conditions of approval under which the proposed land use will be established, operated, and maintained, are consistent with the goals, policies, plans, and exhibits of the Vision, City Council Priorities, and Policy Plan (General Plan) components of The Ontario Plan. The proposed facility will provide infrastructure compatibility and will be aesthetically pleasing and in context with the community character (LU2-6). Furthermore, the proposed facility is consistent with City Council goals, such as: invest in the growth and evolution of the City's Economy, and focus resources in Ontario's commercial and residential neighborhoods; and

(3) ***The proposed use at the proposed location, and the manner in which it will be operated and maintained, is consistent with the objectives and requirements of the Development Code and any applicable specific plan or planned unit development.*** The proposed outdoor automobile storage yard (tow yard) is located within the Industrial land use district, and the IL (Light Industrial) zoning district, and has been reviewed and conditioned to ensure the establishment, operation and maintenance of the proposed land use is consistent with all applicable objectives, purposes, standards, and guidelines of the Development Code and IL (Light Industrial) zoning district. The proposed outdoor automobile storage yard (tow yard) and industrial building will be located on the southeast corner of the Project site and adjacent to the Southern Pacific Railroad. The towing yard area will be screened by 10-foot landscaping and an 8-foot tall, decorative split-face block wall, which will screen the towing yard parking area from public view; and

(4) ***The establishment, maintenance, and operation of the proposed use at the proposed location would not be detrimental or injurious to property and improvements within the vicinity, nor would it be detrimental to the health, safety, or general welfare of persons residing or working in the surrounding neighborhood.*** The Development Advisory Board has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: (a) the purposes of the Development Code are maintained; (b) the Project will not endanger the public health, safety or general welfare; (c) the Project will not result in any significant environmental impacts; and (d) the Project will be in harmony with the surrounding area in which it is proposed to be located.

SECTION 5: Planning Commission Action. Based on the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby

APPROVES the Application, subject to each and every condition set forth in the Department reports included as "Attachment A" and incorporated herein by this reference.

SECTION 6: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 7: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario. The records are available for inspection by any interested person, upon request.

SECTION 8: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of May 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary of Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution PC No. ___, was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 25, 2021, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**File No. PCUP18-021
Departmental Conditions of Approval**

(Departmental conditions of approval to follow this page)



City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

Planning Department Land Development Division Conditions of Approval

Meeting Date: May 25, 2021

File No: PCUP18-021

Related Files: PDEV18-022

Project Description: A Conditional Use Permit (File No. PCUP18-021) to establish and operate a towing service and short-term storage of automobiles, vans, light trucks, and tractor trucks on 3.1 acres of land located at 580 East Belmont Street, within the IL (Light Industrial) zoning district (APNs: 1049-491-01, 1049-491-02 and 1049-491-03); **submitted by Four Sisters Enterprises LLC.**

Prepared By: Denny D. Chen, Associate Planner
Phone: 909.395.2424 (direct)
Email: dchen@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits.

(a) Conditional Use Permit approval shall become null and void two years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director, except that a Conditional Use Permit approved in conjunction with a Development Plan shall have the same time limits as said Development Plan. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

2.2 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.3 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

2.4 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

2.5 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

2.6 Site Lighting.

(a) All off-street parking facilities shall be provided with nighttime security lighting pursuant to Ontario Municipal Code Section 4-11.08 (Special Residential Building Provisions) and Section 4-11.09 (Special Commercial/Industrial Building Provisions), designed to confine emitted light to the parking areas. Parking facilities shall be lighted from sunset until sunrise, daily, and shall be operated by a photocell switch.

(b) Unless intended as part of a master lighting program, no operation, activity, or lighting fixture shall create illumination on any adjacent property.

2.7 Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

2.8 Signs. All Project signage shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations).

2.9 Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noise levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

2.10 Environmental Review.

(a) The proposed project is categorically exempt from the requirements of the California Environmental Quality Act of 1970 (CEQA), as amended, and the Guidelines promulgated thereunder, pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines and meets all of the following conditions:

(i) The Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations;

(ii) The proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses;

(iii) The project site has no value as habitat for endangered, rare, or threatened species;

(iv) Approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and

(v) The Project site can be adequately served by all required utilities and public services.

2.11 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.12 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Exemption ("NOE") filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act ("CEQA"). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit against the Project.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

2.13 Additional Requirements.

(a) A final site inspection shall be required from the Planning Department prior to the grant of an Occupancy Permit for the proposed building, by the Building Department.

(b) The business shall obtain and maintain an active City Business License.

(c) All applicable Conditions of Approval from other City departments shall be met and addressed by the applicant.

(d) The Conditional Use Permit is subject to all applicable provisions of the City's Rotational Towing Services Ordinance No. 3008.

(e) No access to or from the Project site shall be taken from Sultana Avenue, excepting for emergency vehicle access only. Furthermore, no access to or from the Project site shall be taken from Phillips Street.

(f) All tow trucks entering and exiting the Project site shall utilize as the primary route, Belmont Street by way of Campus Avenue. Additionally, tow trucks shall not use Sultana Avenue, Monterey Avenue, or Phillips Street to access the Project site.

(g) All tow vehicles entering and maneuvering towed vehicles on-site, shall turn-off the truck's back-up beeper/vehicle's back-up alarm and revolving safety lights after 8:00 p.m., in order to control additional noise generated by tow trucks.

(h) Normal business office hours of operation shall be from 8:00 a.m. to 5:00 p.m., Monday thru Friday. After hours customer services may be arranged on an appointment-only basis.

(i) The above ground fuel tank shall be relocated to the northeast area of the site to a more appropriate location across from the exiting industrial development, subject to Planning Department review and approval.

(j) Three types of tow-vehicles shall be used by the business, limited to flatbed and wheel-lift trucks to accommodate automobiles, vans, and light trucks, and one heavy-duty tow truck to accommodate tractor trucks.

(k) The business activities for which the Conditional Use Permit has been granted shall not commence until an Occupancy Permit has been issued by the Building Department for the proposed building and all required on-site improvements have been completed to the satisfaction of the Planning Director and Building Official, and all off-site improvements have been completed to the satisfaction of the City Engineer.



ENGINEERING DEPARTMENT CONDITIONS OF APPROVAL

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Information Technology & Management Services Department Conditions incorporated)

<input checked="" type="checkbox"/> DEVELOPMENT PLAN <input type="checkbox"/> OTHER	<input type="checkbox"/> PARCEL MAP <input type="checkbox"/> TRACT MAP <input type="checkbox"/> FOR CONDOMINIUM PURPOSES
PROJECT FILE NO. PDEV18-022 RELATED FILE NO(S). PCUP18-021	
<input type="checkbox"/> ORIGINAL <input checked="" type="checkbox"/> REVISED: 5/6/21	

CITY PROJECT ENGINEER & PHONE NO: **Jesus Plasencia, (909) 395-2128**

CITY PROJECT PLANNER & PHONE NO: **Denny Chen, (909) 395-2424**

DAB MEETING DATE: **May 18, 2020**

PROJECT NAME / DESCRIPTION: **A Development Plan to construct a 6,870 square foot industrial building in conjunction with a CUP to establish and operate a towing service on 3.1 acres of land.**

LOCATION: **580 East Belmont Street**

APPLICANT: **Four Sisters Enterprises, LLC**

REVIEWED BY: Raymond Lee 5/10/21
 Raymond Lee, P.E. Date
 Assistant City Engineer

APPROVED BY: [Signature] 5-11-21
 Khoi Do, P.E. Date
 City Engineer



THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.

1. PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, APPLICANT SHALL: **Check When Complete**

- 1.01 Dedicate to the City of Ontario, the right-of-way, described below:
_____ feet on _____
Property line corner 'cut-back' required at the intersection of _____
and _____.
- 1.02 Dedicate to the City of Ontario, the following easement(s): _____

- 1.03 Restrict vehicular access to the site as follows: _____
- 1.04 Vacate the following street(s) and/or easement(s): _____
- 1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.
- 1.06 Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.
- 1.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.
- 1.08 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Management Services Department at (909) 395-2124 regarding this requirement.
(1) _____
(2) _____
- 1.09 Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.



- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ci.ontario.ca.us) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of three (3) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Management Services at (909) 395-2353 to initiate the CFD application process.
- 1.13 New Model Colony (NMC) Developments:
 - 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
 - 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
 - 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions: _____

2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:

**A. GENERAL
 (Permits includes Grading, Building, Demolition and Encroachment)**

- 2.01 Record Parcel Map/Tract Map No. _____ pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 Submit a duplicate photo mylar of the recorded map to the City Engineer's office.
- 2.03 Note that the subject parcel is a recognized parcel in the City of Ontario per _____
- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of _____
- 2.05 Apply for a: Conforming Grant Deed; Lot Line Adjustment
 - Make a Dedication of Easement (see Condition 2.11)
- 2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.



- 2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.
- 2.08 **Submit a soils/geology report.**
- 2.09 **Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:**
 - State of California Department of Transportation (Caltrans)
 - San Bernardino County Road Department (SBCRD)
 - San Bernardino County Flood Control District (SBCFCD)
 - Federal Emergency Management Agency (FEMA)
 - Cucamonga Valley Water District (CVWD) for sewer/water service
 - United States Army Corps of Engineers (USACE)
 - California Department of Fish & Game
 - Inland Empire Utilities Agency (IEUA)
 - Other: Union Pacific Railroad (UPRR): For proposed improvements in UPRR right-of-way.**
- 2.10 **Dedicate to the City of Ontario the right-of-way described below:**
 1. **33 feet north of centerline on Phillips Street along the project frontage.**
 2. **Property line corner 'cut-back' required at the intersection of Sultana Avenue and Belmont Street.**
 3. **Property line corner 'cut-back' required at the intersection of Sultana Avenue and Phillips Street.**
- 2.11 **Dedicate to the City of Ontario the following easement(s) only if necessary:**
 1. **Sidewalk easement behind the proposed drive approaches located along Sultana Avenue and Belmont Street.**
- 2.12 **New Model Colony (NMC) Developments:**
 - 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.
 - 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.
 - 3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall).
- 2.13 **Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at 100% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.**



- 2.14** The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.

- 2.15** Pay all Development Impact Fees (DIF) to the Building Department. Storm Drain Development Impact Fee, approximately \$69,139, shall be paid to the Building Department. Final fee shall be determined based on the approved site plan.

- 2.16** Other conditions:
 - 1.** The applicant/developer shall process a Lot Line Adjustment and a Conforming Grant Deed in order to consolidate the site into one (1) parcel and render the subject property as a recognized parcel in the City of Ontario. The Lot Line Adjustment and Conforming Grant Deed shall be recorded prior to issuance of a building permit.



B. PUBLIC IMPROVEMENTS

(See attached Exhibit 'A' for plan check submittal requirements.)

- 2.17 Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):**

Improvement	Phillips Street	Sultana Avenue	Belmont Street	Monterey Avenue
Curb and Gutter	<input checked="" type="checkbox"/> New; 20 ft. from C/L north <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New; 18 ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New; 18 ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
AC Pavement (see Condition 2.19)	<input checked="" type="checkbox"/> Replacement <input checked="" type="checkbox"/> Widen 18 feet from C/L north along frontage, including pavm't Transitions	<input checked="" type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input checked="" type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Drive Approach	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Sidewalk	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
ADA Access Ramp	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Parkway	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
Raised Landscaped Median	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Fire Hydrant	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation



Sewer (see Sec. 2.C)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Water (see Sec. 2.D)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Street Light (see Sec. 2.F)	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2G)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Fiber Optics (see Sec. 2K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input checked="" type="checkbox"/> Relocate ⁽¹⁾	<input checked="" type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
Removal of Improvements	Remove existing metal poles and fences	_____	_____	_____
Other Improvements	_____	_____	_____	_____

Specific notes for improvements listed in item no. 2.17, above:

- Relocate existing power poles as needed to accommodate the construction of curb & gutter along the project frontage.

- 2.18 Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s):
- 2.19 Reconstruction of the full pavement structural section on Phillips Street, Sultana Avenue, and Belmont Street, per City of Ontario Standard Drawing number 1011, is required unless a grind and overly is recommended by the soils engineer based on the existing pavement conditions.



Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.

- 2.20 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide water service sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.
- 2.21 **Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892).**
- 2.22 Other conditions: _____

C. SEWER

- 2.23 **An 8-inch sewer main is available for connection by this project in Belmont Street. (Ref: Sewer plan bar code: S12680)**
- 2.24 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.25 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.
- 2.26 **Other conditions:**
 1. **The Occupant of the building shall apply for a Wastewater Discharge Permit for their Establishment, and shall comply with all the requirements of their Wastewater Discharge Permit. Requirements of Wastewater Discharge Permit may include, but not limited to including: possibly installing a monitoring manhole, clarifier, or other sewer pretreatment equipment.**

D. WATER

- 2.27 A _____ inch water main is available for connection by this project in _____. (Ref: Water plan bar code: _____)
- 2.28 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.29 **Other conditions:**
 1. **Replace the existing 4" water line in Belmont, between Sultana and Monterey, with a new 12". All existing appurtenances shall be reconnected to the new 12" main via the installation of new service laterals (no coupling) from main to appurtenance. Upgrade any/all hydrants with breakaway check valve per City standards.**
 2. **Install one fire hydrant on the north side of Phillips Street (south side of property), connecting off the 1074 PZ water line, fronting the property.**

E. RECYCLED WATER

- 2.30 A _____ inch recycled water main is available for connection by this project in _____. (Ref: Recycled Water plan bar code: _____)
- 2.31 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.
- 2.32 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.



- 2.33 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.

Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.

- 2.34 Other conditions: _____

F. TRAFFIC / TRANSPORTATION

- 2.35 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer:
1. On-site and off-site circulation
 2. Traffic level of service (LOS) at 'build-out' and future years
 3. Impact at specific intersections as selected by the City Engineer

- 2.36 New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.

- 2.37 Other conditions:

1. The applicant/developer shall be responsible to design and construct the following streets to their ultimate half-width along the project frontage:

- Sultana Avenue (66' R/W, 36' C/C)
- Belmont Street (66' R/W, 36' C/C)
- Phillips Street (66' R/W, 40' C/C)

Improvements shall include, but not be limited to, concrete curb and gutter, sidewalk, LED street lights, landscaped parkways, signing & striping, and necessary pavement transitions as deemed necessary by the City Engineer.

2. The Applicant/Developer shall be responsible to design and construct in-fill public street lights along the property frontages of Sultana Avenue, Belmont Street and Phillips Street, in accordance with the Traffic and Transportation Design Guidelines Section 1.4 Street Light Plans, City of Ontario Standard Drawing No. 5101, and to the satisfaction of the City Engineer.

3. Driveways shall be designed and constructed in accordance with City of Ontario Standard Drawing No. 1204 for Commercial Driveway Approach.

4. Property frontages along Sultana Avenue, Belmont Street, Phillips Street and Monterey Avenue shall be signed "No Parking Anytime."

5. Proposed gates on Belmont Street shall remain open at all times during business hours.

6. The applicant/developer's engineer-of-record shall meet with City Engineering staff prior to starting signing/striping and/or street lighting design.

7. The applicant/developer shall obtain all rights-of-way necessary to construct all required improvements.

8. All landscaping, block walls, and other obstructions shall be compatible with the stopping sight distance requirements per City of Ontario Standard Drawing No. 1309.

G. DRAINAGE / HYDROLOGY

- 2.38 A _____ inch storm drain main is available to accept flows from this project in _____.
(Ref: Storm Drain plan bar code: _____)



- 2.39** Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.
- 2.40** An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100-year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.
- 2.41** Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.
- 2.42** Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.
- 2.43** Other conditions:
 - 1. The applicant/developer shall pay an in-lieu fee for future storm drain in Sultana Avenue along project frontage valued at \$75,454.

H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)

- 2.44** 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels. If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted. Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.
- 2.45** Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: <http://www.sbcounty.gov/dpw/land/npdes.asp>.
- 2.46** Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.
- 2.47** Other conditions:
 - 1. Activities resulting in land disturbance of one acre or more is required to obtain coverage under the Construction General Permit (CGP). The owner is the legally responsible person (LRP) of the site and shall have a Stormwater Pollution Prevention Plan (SWPPP) developed and submitted through the SMARTS website at <https://smarts.waterboards.ca.gov/smarts/faces/SwSmartsLogin.xhtml>



J. SPECIAL DISTRICTS

- 2.48 File an application, together with an initial payment deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community facilities District Act of 1982. The application and fee shall be submitted a minimum four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact the Financial Services Department at (909) 395-2353 to initiate the CFD application process.

- 2.49 Other conditions: _____

K. FIBER OPTIC

- 2.50 Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole if within close proximity. If there is no OntarioNet handhole, conduit infrastructure shall terminate at either end of project frontage in a handhole.

- 2.51 Refer to the City's Fiber Optic Master Plan and other related documents on the City website for design and layout guidelines. Contact the Information Technology Department at (909) 395-2000, for more information regarding the requirement to install fiber optic improvements.

L. Solid Waste

- 2.52 Onsite solid waste shall be designed in accordance with the City's Solid Waste Manual location at:
<http://www.ontarioca.gov/municipal-utilities-company/solid-waste>

- 2.53 Other conditions:
 - 1. Prior to approval of the any building permits, the Conceptual Solid Waste Handling Plan needs to be updated and converted into a Final Solid Waste Handling Plan Sheet and shall be submitted accompanying the Precise Grading Plan Submittal to the City/OMUC for review and approval. See "Solid Waste Handling Plan (SWHP) Requirements" document for details.
 - 2. The applicant shall submit an Integrated Waste Management Report for review and approval with the Precise Grading Plan. This report shall address the management of all integrated waste (Refuse, Recycling, Organics, etc.). The IWMR shall demonstrate compliance with the "Integrated Waste Management Report Requirements" document.



3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:

- 3.01 **Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.**
- 3.02 Complete all requirements for recycled water usage.
 - 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.
 - 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.
 - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.
- 3.03 **The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.**
- 3.04 NMC Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.
- 3.05 **Confirm payment of all Development Impact Fees (DIF) to the Building Department.**
- 3.06 **Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).**

4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:

- 4.01 **Complete all Conditions of Approval listed under Sections 1-3 above.**
- 4.02 **Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.**
- 4.03 **The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall include a completed Acceptance and Bond Release Checklist, state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process**
- 4.04 **Submit record drawings (PDF) for all public improvements identified within Section 2 of these Conditions of Approval.**



EXHIBIT 'A'
ENGINEERING DEPARTMENT
First Plan Check Submittal Checklist

Project Number: PDEV18-022

The following items are required to be included with the first plan check submittal:

1. **A copy of this check list**
2. **Payment of fee for Plan Checking**
3. **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4. **One (1) copy of project Conditions of Approval**
5. **Include a PDF (electronic submittal) of each required improvement plan at every submittal.**
6. **Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).**
7. **Three (3) sets of Public Street improvement plan with street cross-sections**
8. **Three (3) sets of Private Street improvement plan with street cross-sections**
9. **Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)**
10. **Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)**
11. **Four (4) sets of Public Sewer improvement plan**
12. **Five (5) sets of Public Storm Drain improvement plan**
13. **Three (3) sets of Public Street Light improvement plan**
14. **Three (3) sets of Signing and Striping improvement plan**
15. **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
16. **Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)**
17. **Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.**
18. **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**
19. **One (1) copy of Hydrology/Drainage study**
20. **One (1) copy of Soils/Geology report**
21. **Payment for Final Map/Parcel Map processing fee**




- 22. Three (3) copies of Final Map/Parcel Map
- 23. One (1) copy of approved Tentative Map
- 24. **One (1) copy of Preliminary Title Report (current within 30 days)**
- 25. One (1) copy of Traverse Closure Calculations
- 26. One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.
- 27. Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use.
- 28. **Other: Update the sewer improvement plan for Belmont Street to show the proposed service connection to the project site.**

**CITY OF ONTARIO
LANDSCAPE PLANNING
DIVISION**

303 East "B" Street, Ontario, CA 91764

CONDITIONS OF APPROVAL

Sign Off


Jamie Richardson, Sr. Landscape Planner

2/13/20
Date

Reviewer's Name:
Jamie Richardson, Sr. Landscape Planner

Phone:
(909) 395-2615

D.A.B. File No.:
PDEV18-022 Rev 4

Case Planner:
Denny Chen

Project Name and Location:

Pepe's Towing
554 E Belmont St.

Applicant/Representative:
Howard Parcell Company
4854 Main St
Yorba Linda, CA 92886

A Preliminary Landscape Plan (dated 2/13/20) meets the Standard Conditions for New Development and has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.

A Preliminary Landscape Plan (dated) has not been approved. Corrections noted below are required prior to Preliminary Landscape Plan approval.

A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED AS INCOMPLETE

PREVIOUS COMMENTS – 11/13/19

Civil/ Site Plans

1. The tree inventory identifies 48" of trunk diameter to be mitigated; Schinus mole No. 14.
 - a. New 15 gallon trees min 1" diameter trunk, in addition to trees required; total of 48 trees.
 - b. New 24" box trees min 1.5" diameter trunk, in addition to trees required; total of 32 trees
 - c. Upsizing trees on the plan one size larger such as 15 gallon to 24" box, or 24" to 36" box size.
 - d. Monetary value of the trees removed as identified in the "Guide for Plant Appraisal", approved certified arborist plant appraiser, or may be equal to the value of the installation cost of planting, fertilizing, staking and irrigating 15 gallon trees, (100\$ each) to the City of Ontario Historic Preservation Fund for city tree planting or city approved combination of the above items; total of \$4,800.
2. Show transformers and backflow devices located in planter areas and set back 4' from paving; dimension on plan. Locate on level grade. Coordinate with landscape plans.
3. ~~2/13/20 Spoke with Mr. George Estrada over the counter; locate the wall at the Belmont entry of the tow yard so the landscape planter is in the public parking lot. Provide an employee break area on the inside of the tow yard adjacent to the 4 public parking spaces include a table and/or bench and 2 broad canopy shade trees such as Platanus racemosa 24" box. Provide irrigation to trees and mulch.~~ 3/12/20 updated plans received. Condition has been addressed.

Landscape Plans

4. Locate light standards, fire hydrants, water and sewer lines to not conflict with required tree locations. Coordinate civil plans with landscape plans



CITY OF ONTARIO MEMORANDUM

Development Plan Review

Project: PDEV18-022 (PCUP18-021)

Date: February 13th, 2020

Location: 554 East Belmont Street

By: Nathan Kuan

The following shall become conditions of approval for the project.

Conditions:

1. The applicant/developer shall be responsible to design and construct the following streets to their ultimate half-width along the project frontage:
 - Sultana Avenue (66' R/W, 36' C/C)
 - Belmont Street (66' R/W, 36' C/C)
 - Phillips Street (66' R/W, 40' C/C)Improvements shall include, but not be limited to: concrete curb and gutter, sidewalk, LED street lights, landscaped parkways, signing & striping, and necessary pavement transitions as deemed necessary by the City Engineer.
2. The Applicant/Developer shall be responsible to design and construct in-fill public street lights and service pedestals along the property frontages of Sultana Avenue, Belmont Street and Phillips Street, in accordance with the Traffic and Transportation Design Guidelines Section 1.4 Street Light Plans, City of Ontario Standard Drawing No. 5101, and to the satisfaction of the City Engineer.
3. Driveways shall be designed and constructed in accordance with City of Ontario Standard Drawing No. 1204 for Commercial Driveway Approach.
4. Property frontages along Sultana Avenue, Belmont Street, Phillips Street and Monterey Avenue shall be signed "No Parking Anytime."
5. Proposed gates on Belmont Street shall remain open at all times during business hours.
6. The applicant/developer's engineer-of-record shall meet with City Engineering staff prior to starting signing/striping and/or street lighting design.
7. The applicant/developer shall obtain all rights-of-way necessary to construct all required improvements.
8. All landscaping, block walls, and other obstructions shall be compatible with the stopping sight distance requirements per City of Ontario Standard Drawing No. 1309.

nk;

CITY OF ONTARIO MEMORANDUM

TO: PLANNING DEPARTMENT, Denny Chen
FROM: BUILDING DEPARTMENT, Kevin Shear
DATE: June 11, 2019
SUBJECT: PDEV18-022

-
- The plan **does** adequately address the departmental concerns at this time.
- No comments
- Report below.

Conditions of Approval

1. See previous report for Conditions.

KS:lr

CITY OF ONTARIO

MEMORANDUM

TO: PLANNING DEPARTMENT, Denny Chen
FROM: BUILDING DEPARTMENT, Kevin Shear
DATE: June 27, 2018
SUBJECT: PDEV18-022

-
- The plan **does** adequately address the departmental concerns at this time.
- No comments
 - Report below.

Conditions of Approval

1. The site address for this project will be 580 E Belmont St.
2. The lot lines are to be removed.

KS:lm



CITY OF ONTARIO

MEMORANDUM

TO: Denny Chen, Associate Planner
Planning Department

FROM: Paul Ehrman, Deputy Fire Chief/Fire Marshal
Fire Department

DATE: July 6, 2018

SUBJECT: PDEV18-022 - A Development Plan to construct a 6,660-square foot industrial building on 3.01 acres of land located at the southwest corner of Belmont Street and Monterey Avenue, at 554 East Belmont Street, within the IL (Light Industrial) zoning district (APNs: 1049-491-01, 1049-491-02 and 1049-491-03). Related File: PCUP18-021

-
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.
-

SITE AND BUILDING FEATURES:

- A. 2016 CBC Type of Construction: V
- B. Type of Roof Materials: Ordinary
- C. Ground Floor Area(s): 5,460 Sq. Ft.
- D. Number of Stories: 2
- E. Total Square Footage: 6,660 Sq. Ft.
- F. 2016 CBC Occupancy Classification(s): B, S

CONDITIONS OF APPROVAL:

1.0 GENERAL

- 1.1 The following are the Ontario Fire Department ("Fire Department") requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards ("Standards.") It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at www.ontarioca.gov, click on "Fire Department" and then on "Standards and Forms."
- 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

2.0 FIRE DEPARTMENT ACCESS

- 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25') inside and forty-five feet (45') outside turning radius per Standard #B-005.
- 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150') in length shall have an approved turn-around per Standard #B-002.
- 2.4 Access drive aisles which cross property lines shall be provided with CC&Rs, access easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of building plan check.
- 2.5 "No Parking-Fire Lane" signs and /or red painted curbs with lettering are required to be installed in interior access roadways, in locations where vehicle parking would obstruct the minimum clear width requirement. Installation shall be per Standard #B-001.
- 2.6 Security gates or other barriers on fire access roadways shall be provided with a Knox brand key switch or padlock to allow Fire Department access. See Standards #B-003, B-004 and H-001.
- 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-six (26) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by fire department and other emergency services..

3.0 WATER SUPPLY

- 3.1 The required fire flow per Fire Department standards, based on the 2016 California Fire Code, Appendix B, is 2000 gallons per minute (g.p.m.) for 2 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.
- 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.
- 3.4 The water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

4.0 FIRE PROTECTION SYSTEMS

- 4.1 On-site private fire hydrants are required per Standard #D-005, and identified in accordance with Standard #D-002. Installation and location(s) are subject to the approval of the Fire Department. An application with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- 4.2 Underground fire mains which cross property lines shall be provided with CC & R, easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of fire department plan check. The shared use of private fire mains or fire pumps is allowable only between immediately adjacent properties and shall not cross any public street.
- 4.4 Wood frame buildings that are to be sprinkled shall have these systems in service (but not necessarily finalized) before the building is enclosed.

5.0 BUILDING CONSTRUCTION FEATURES

- 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- 5.3 Single station smoke alarms and carbon monoxide alarms are required to be installed per the California Building Code and the California Fire Code.

- ☒ 5.6 Knox ® brand key-box(es) shall be installed in location(s) acceptable to the Fire Department. All Knox boxes shall be monitored for tamper by the building fire alarm system. See Standard #H-001 for specific requirements.
- ☒ 5.7 Placards shall be installed in acceptable locations on buildings that store, use or handle hazardous materials in excess of the quantities specified in the CFC. Placards shall meet the requirements of National Fire Protection Association (NFPA) Standard 704.

6.0 OTHER SPECIAL USES

- ☒ 6.1 The storage, use, dispensing, or handling of any hazardous materials shall be approved by the Fire Department, and adequate fire protection features shall be required. If hazardous materials are proposed, a Fire Department Hazardous Materials Information Packet, including Disclosure Form and Information Worksheet, shall be completed and submitted with Material Safety Data Sheets to the Fire Department along with building construction plans.
- ☒ 6.3 Underground fuel tanks, their associated piping and dispensers shall be reviewed, approved, and permitted by Ontario Building Department, Ontario Fire Department, and San Bernardino County Fire Department Hazardous Materials Division. In fueling facilities, an exterior emergency pump shut-off switch shall be provided.



CITY OF ONTARIO

MEMORANDUM

TO: Denny Chen, Planning Department

FROM: Douglas Sorel, Police Department

DATE: July 26, 2018

SUBJECT: PDEV18-022 & PCUP18-021: A DEVELOPMENT PLAN AND
CONDITIONAL USE PERMIT TO ESTABLISH A TOW YARD AT 554
EAST BELMONT STREET

The “Standard Conditions of Approval” contained in Resolution No. 2017-027 and Ontario Development Code Section 5.03.310 apply. The applicant shall read and be thoroughly familiar with these conditions, including but not limited to, the requirements listed below.

- Required lighting for all walkways, driveways, doorways, parking areas, and other areas used by the public shall be provided and operate on photosensor. Photometrics shall be provided to the Police Department. Photometrics shall include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- Rooftop addresses shall be installed on the building as stated in the Standard Conditions. The numbers shall be at a minimum 3 feet tall and 1 foot wide, in reflective white paint on a flat black background, and oriented with the bottom of the numbers towards the addressed street.
- The Applicant shall comply with construction site security requirements as stated in the Standard Conditions.
- The indoor storage of motor vehicles shall comply with all requirements of the fire and building codes.
- All vehicles stored outdoors shall be screened from public view by a minimum 8-foot high decorative masonry block wall.
- All vehicles stored shall comply with the requirements for the base zoning district.

In addition to the Standard Conditions, the Ontario Police Department places the following conditions on the project:

- While the Applicant serves as a contracted Rotational Towing Service with the City of Ontario they shall comply with all terms of Title 4-19 of the Ontario Municipal Code and their current Professional Services Agreement with the City.

- The applicant will be responsible for keeping the grounds of the business clean from debris and litter.
- Graffiti abatement by the business owner/licensee, or management shall be immediate and on-going on the premises, but in no event shall graffiti be allowed unabated on the premises for more than 72 hours. Abatement shall take the form of removal, or shall be covered/painted over with a color reasonably matching the color of the existing building, structure, or other surface being abated. Additionally, the business owner/licensee, or management shall notify the City within 24 hours at (909) 395-2626 (graffiti hotline) of any graffiti elsewhere on the property not under the business owner/licensee's or management control so that it may be abated by the property owner and/or the City's graffiti team.

The Applicant is invited to contact Douglas Sorel at (909) 408-1873 with any question or concerns regarding these conditions.

AIRPORT LAND USE COMPATIBILITY PLANNING CONSISTENCY DETERMINATION REPORT



Project File No.: PDEV18-022 & PCUP18-021
 Address: 554 East Belmont Street
 APN: 1049-491-01, 02 & 03
 Existing Land Use: Vacant
 Proposed Land Use: Development Plan and CUP to establish a tow yard and 6,600 SF 2-story industrial/warehouse building
 Site Acreage: 3.01 ac Proposed Structure Height: 33 FT
 ONT-IAC Project Review: N/A
 Airport Influence Area: ONT

Reviewed By: Lorena Mejia
 Contact Info: 909-395-2276
 Project Planner: Rudy Zeledon
 Date: 7/30/18
 CD No.: 2018-036
 PALU No.: n/a

The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="radio"/> High Terrain Zone	<input type="radio"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input checked="" type="checkbox"/> FAA Notification Surfaces	<input checked="" type="checkbox"/> Recorded Overflight Notification
<input type="radio"/> Zone 2	<input checked="" type="checkbox"/> 65 - 70 dB CNEL	<input checked="" type="checkbox"/> Airspace Obstruction Surfaces	<input type="radio"/> Real Estate Transaction Disclosure
<input type="radio"/> Zone 3	<input type="radio"/> 60 - 65 dB CNEL	<input checked="" type="checkbox"/> Airspace Avigation Easement Area	
<input type="radio"/> Zone 4		Allowable Height: 110 FT	
<input type="radio"/> Zone 5			

The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1
 Zone 2
 Zone 3
 Zone 4
 Zone 5
 Zone 6
 Allowable Height: _____

CONSISTENCY DETERMINATION

This proposed Project is: Exempt from the ALUCP
 Consistent
 Consistent with Conditions
 Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

See Attached Conditions

Airport Planner Signature: _____

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT

CD No.: 2018-036

PALU No.: _____

PROJECT CONDITIONS

1. This project is located within Airspace Avigation Easement Area and is required to file and record an Avigation Easement with the Ontario International Airport Authority prior to obtaining a Certificate of Occupancy.

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PDEV18-022, A DEVELOPMENT PLAN TO CONSTRUCT A 6,870 SQUARE FOOT INDUSTRIAL BUILDING ON 3.1 ACRES OF LAND LOCATED AT 580 EAST BELMONT STREET, WITHIN THE IL (LIGHT INDUSTRIAL) ZONING DISTRICT AND MAKING FINDINGS IN SUPPORT THEREOF (APNS: 1049-491-01, 1049-491-02 & 1049-491-03).

WHEREAS, Four Sisters Enterprises, LLC ("Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV18-022, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 3.1 acres of land located on the southwest corner of Belmont Street and Monterey Avenue, at 580 East Belmont Street, within the IL (Light Industrial) zoning district; and

WHEREAS, on June 21, 2018, the Applicant submitted a Development Plan application requesting approval to construct a 6,870 square foot industrial building having a floor area ratio (FAR) of 0.05, in conjunction with a Conditional Use Permit (File No. PCUP18-021) to establish and operate a towing service and motor vehicle impound storage yard on the Project site; and

WHEREAS, the property to the north of the project site is within the IL (Light Industrial) zoning district and is developed with industrial buildings and a metal rebar storage yard. The properties to the east are within the LDR-5 (Low Density Residential – 2.1 to 5.0 DUs/Acre) and IL (Light Industrial) zoning districts and are developed with the Southern Pacific Railroad rail line located in the Monterey Avenue right-of-way and single-family homes across Monterey Avenue. The property to the south is located within the LDR-5 (Low Density Residential – 2.1 to 5.0 DUs/Acre) and is developed with single-family homes. The property to the west is within the MDR-11 (Medium Density Residential – 5.1 to 11.0 DUs/Acre) zoning district and is developed with single-family homes; and

WHEREAS, proposed building is located at the southeast corner of the Project site, fronting on to Monterey Avenue (to the east) and Phillips Street (to the south). The building is setback 48 feet from the Belmont Street (north) property line, 10 feet from the east property line, 10 feet from Phillips Street (south) property line, and approximately 540 feet from Sultana Avenue (west) property line; and

WHEREAS, the building's office and main entrance is situated at the northeast corner of the building, fronting onto Belmont Street. The employee and visitor parking areas are located north of the building on the northeast corner of the project site and are secured by an 8-foot-tall decorative tube steel fence; and

WHEREAS, the motor vehicle impound yard is proposed on the westerly two-thirds of the Project site, covering 79,569 square feet and is designed to park 324 impounded vehicles. The yard area will be screened from public view by a decorative 8-foot-tall wall consisting of split-face masonry block with decorative pilasters and cap, located along the north, west, and south sides of the Project site; and

WHEREAS, the Project is required to provide a minimum of 16 off-street parking spaces and a total of 20 off-street parking spaces have been provided, exceeding the minimum number of required parking spaces; and

WHEREAS, primary vehicle access is provided by two driveways located along Belmont Street, immediately west of Monterey Avenue. A secondary vehicle access point is provided from Sultana Avenue, mid-block between Belmont and Phillips Streets; and

WHEREAS, the proposed building elevations illustrate the type of high-quality architecture promoted by the Ontario Development Code's development standards and design guidelines. The building will be composed of pre-painted/smooth faced sheet metal wall panels that incorporate a contemporary architectural style. The building design incorporates articulation in the building footprint, incorporating a combination of recessed and popped-out wall areas, and articulation in the building parapet/roof line, which serves to accentuate the building's roofline; and

WHEREAS, the Project proposes 15 percent landscape coverage, which meets the minimum Development Code requirement for corner lots located within the IL (Light Industrial) zoning district. The landscape pallet for the Project incorporates a mixture of 24-inch, 36-inch, and 48-inch box accent and shade trees; and

WHEREAS, public utilities (water and sewer) are available to serve the Project. Additionally, the Applicant has submitted a Preliminary Water Quality Management Plan (PWQMP), which establishes the Project's compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizing low impact development (LID) best management practices (BMPs), such as retention and infiltration, biotreatment, and evapotranspiration. The PWQMP proposes a vegetated swale designed to accept runoff from building roofs, parking lots and Project roadways, which lead to an underground stormwater infiltration system. The proposed underground infiltration system is located along the southeast portion of the site and any overflow drainage will be conveyed to the surrounding streets; and

WHEREAS, on May 26, 2020, the Planning Commission approved the proposed Project, subject to conditions. Subsequently, the project was appealed to City Council on June 5, 2020; and

WHEREAS, on July 7, 2020, the City Council held a Public Hearing to hear the appeal and recommended that the project return to Planning Commission to provide the Commission the opportunity to review new project information presented by the applicant, regarding the operation of the towing facility; and

WHEREAS, the applicant submitted updated information for the towing operation, including a Health Risk Assessment (HRA). The HRA prepared by the Ganddini Group (Dated: February 15, 2021) analyzed the potential impacts from the proposed Tow Yard Facility and concluded that the proposed Project will not impose significant health impacts on the surrounding environment; and

WHEREAS, the Applicant submitted an updated Noise Assessment prepared by Rincon Consultants (Dated: July 6, 2020) to include noise generated during typical tow-related activities. The initial Noise assessment (Rincon Consultants - Dated: April 8, 2020) only analyzed noise levels from the proposed truck wash facility. The Noise Assessment concluded that the proposed tow operations and truck wash facility will not generate noise levels beyond the City's allowed noise levels for the adjacent residential zones to the west, south and east of the Project site. Per the Ontario Municipal Code Noise Ordinance, the maximum exterior noise level allowed within residential zoning districts is 65 decibels (dBA), between the hours of 7 a.m. to 10 p.m. According to the Noise Assessment, the exterior noise level generated by the truck wash area will not exceed 59 dBA, the daytime tow operations would not exceed 55 dBA and the nighttime tow operations would not exceed 44 dBA. Therefore, the proposed outdoor truck washing and tow activities will comply with the City's maximum allowed exterior noise level for residential zoning districts and are not anticipated to adversely affect the adjacent residents; and

WHEREAS, a notice was mailed on March 18, 2021, to all property owners beyond 500-feet of the Project site, in both English and Spanish. The intent of the notification was to inform the surrounding residents and property owners of the proposed Project Area and to address questions and/or concerns they may have regarding the Project; and

WHEREAS, on March 30, 2021, the Planning Department hosted a virtual Zoom Community meeting for Pepe's Towing Project. The purpose of the community meeting was to present the community with the proposed project and the new information provided by the applicant and to gather the community's input and comments. The community meeting was held in both English and Spanish; and

WHEREAS, in order to help residents that did not have access to a computer, the Dorothy Quesada Community Center was provided as a location to view and participate in the virtual Zoom meeting. Planning Department staff were on hand to provide Spanish translation assistance and ensure that proper social distancing measures were followed. A total of 10 people attended the meeting at the Quesada Community Center and 18 people logged on to the virtual Zoom meeting; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA"); and

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan ("ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 18, 2020, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision DAB20-024, recommending the Planning Commission approve the Application; and

WHEREAS, on May 26, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to approve Decisions PC20-023 and PC20-024; and

WHEREAS, the project was appealed to City Council on June 5, 2020; and

WHEREAS, on July 7, 2020, the City Council held a Public Hearing to hear the appeal. After the applicant and community testimony, the City Council recommended that the project return to Planning Commission to provide the Commission the opportunity to review new project information presented by the applicant, regarding the operation of the towing facility; and

WHEREAS, on May 25, 2021, the Planning Commission of the City of Ontario conducted a hearing to reconsider the Project with new project information, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the decision-making body for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The administrative record has been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(2) The Project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines and meets each of the following conditions: (a) the Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations; (b) the proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses; (c) the Project site has no value as habitat for endangered, rare, or threatened species; (d) approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and (e) the Project site can be adequately served by all required utilities and public services; and

(3) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

(4) The determination of CEQA exemption reflects the independent judgment of the Planning Commission.

SECTION 2: *Housing Element Compliance.* Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 3: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan (“ALUCP”), establishing the Airport Influence Area for Ontario International Airport (“ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making body for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the Planning Commission, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 4: *Concluding Facts and Reasons.* Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing and upon the specific findings set forth in Sections 1 through 3, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Project is located within the Industrial land use district of the Policy Plan Land Use Map, and the IL (Light Industrial) zoning district. The development standards and conditions under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan. The proposed towing yard facility will provide

infrastructure compatibility and will be aesthetically pleasing and in context with the community character (LU2-6). Furthermore, the proposed development is consistent with City Council goals, such as: invest in the growth and evolution of the City's Economy, maintain the current high level of public safety, and focus resources in Ontario's commercial and residential neighborhoods; and

(2) ***The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located.*** The Project has been designed consistent with the requirements of the City of Ontario Development Code and the IL (Light Industrial) zoning district, as-well-as building intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions. In addition, the proposed 6,870 square foot industrial building will be constructed on the southeast corner of the Project site to minimize any potential impacts on the residential homes. In addition, the proposed vehicle impound yard will be screened from public views by a combination of landscaping and an 8-foot tall, decorative split face block wall; and

(3) ***The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed project.*** The Planning Commission has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: (a) the purposes of the IL (Light Industrial) zoning district are maintained; (b) the Project will not endanger the public health, safety or general welfare; (c) the Project will not result in any significant environmental impacts; (d) the Project will be in harmony with the area in which it is located; and (e) the Project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan; and

(4) ***The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development.*** The proposed Project has been reviewed for consistency with the general development standards and guidelines of the IL (Light Industrial) zoning district that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking and loading spaces, parking lot dimensions, design and landscaping, bicycle parking, on-site landscaping, and fences and walls, as-well-as those development standards and guidelines. As a result of this review, the Planning Commission has determined that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the development standards and guidelines described in the IL (Light Industrial) zoning district.

SECTION 5: *Planning Commission Action.* Based on the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby APPROVES the Application, subject to each and every condition set forth in the Department reports included as “Attachment A” and incorporated herein by this reference.

SECTION 6: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 7: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East “B” Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario. The records are available for inspection by any interested person, upon request.

SECTION 8: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 25th day of May 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage
Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary of Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution PC No.____, was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 25, 2021, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**File No. PDEV18-022
Departmental Conditions of Approval**

(Departmental conditions of approval to follow this page)



City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

Planning Department Land Development Division Conditions of Approval

Meeting Date: May 25, 2021

File No: PDEV18-022

Related Files: PCUP18-021

Project Description: A Development Plan (File No. PDEV18-022) to construct a 6,870 square-foot industrial building on 3.1 acres of land located at 580 East Belmont Street, within the IL (Light Industrial) zoning district (APNs: 1049-491-01, 1049-491-02 and 1049-491-03); **submitted by Four Sisters Enterprises LLC.**

Prepared By: Denny D. Chen, Associate Planner
Phone: 909.395.2424 (direct)
Email: dchen@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits.

(a) Development Plan approval shall become null and void two years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

2.2 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.3 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b) Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

(c) Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(d) Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

2.4 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

2.5 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b) All drive approaches shall be provided with an enhanced pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.

(c) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(d) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

(e) Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).

(f) Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11).

2.6 Outdoor Loading and Storage Areas.

(a) Loading facilities shall be designed and constructed pursuant to Development Code Division 6.03 (Off-Street Parking and Loading).

(b) Areas designated for off-street parking, loading, and vehicular circulation and maneuvering, shall not be used for the outdoor storage of materials or equipment.

(c) Outdoor loading and storage areas, and loading doors, shall be screened from public view pursuant to the requirements of Development Code Paragraph 6.02.025.A.2 (Screening of Outdoor Loading and Storage Areas, and Loading Doors) Et Seq.

(d) Outdoor loading and storage areas shall be provided with gates that are view-obstructing by one of the following methods:

(i) Construct gates with a perforated metal sheet affixed to the inside of the gate surface (50 percent screen); or

(ii) Construct gates with minimum one-inch square tube steel pickets spaced at maximum 2-inches apart.

(e) The minimum gate height for screen wall openings shall be established based upon the corresponding wall height, as follows:

<i>Screen Wall Height</i>	<i>Minimum Gate Height</i>
14 feet:	10 feet
12 feet:	9 feet
10 feet:	8 feet
8 feet:	8 feet
6 feet:	6 feet

2.7 Site Lighting.

(a) All off-street parking facilities shall be provided with nighttime security lighting pursuant to Ontario Municipal Code Section 4-11.08 (Special Residential Building Provisions) and Section 4-11.09 (Special Commercial/Industrial Building Provisions), designed to confine emitted light to the parking areas. Parking facilities shall be lighted from sunset until sunrise, daily, and shall be operated by a photocell switch.

(b) Unless intended as part of a master lighting program, no operation, activity, or lighting fixture shall create illumination on any adjacent property.

2.8 Mechanical and Rooftop Equipment.

(a) All exterior roof-mounted mechanical, heating and air conditioning equipment, and all appurtenances thereto, shall be completely screened from public view by parapet walls or roof screens that are architecturally treated so as to be consistent with the building architecture.

(b) All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.

2.9 Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

2.10 Signs. All Project signage shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations).

2.11 Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

2.12 Environmental Review.

(a) The proposed project is categorically exempt from the requirements of the California Environmental Quality Act of 1970 (CEQA), as amended, and the Guidelines promulgated thereunder, pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines and meets all of the following conditions:

(i) The Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations;

(ii) The proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses;

(iii) The project site has no value as habitat for endangered, rare, or threatened species;

(iv) Approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and

(v) The Project site can be adequately served by all required utilities and public services.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

2.13 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.14 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

2.15 Additional Requirements.

(a) The proposed metal canopy over the office entrance and all canopies located on the first floor (North Elevation) shall project from the face of the building a minimum of 5 feet. All other metal canopies proposed on the second floor shall project a minimum of 4 feet. The applicant shall work with staff, during the plan check process, to finalize the dimensions/projections of the metal canopies.

(b) An 8-foot tall decorative masonry block wall with decorative cap, shall be constructed along the north, west, and south sides of the property to screen the towing yard area from public view. The minimum height of the proposed tubular steel/metal fence to be constructed along the east and north sides of the property shall be 8 feet, measured from finished grade level.

(c) All walls shall be painted with anti-graffiti coating to facilitate the removal of graffiti.

(d) The maximum height of fences and walls within the project's front setback area, shall be 6 feet and shall be decorative and nonview-obstructing, such as wrought iron or tubular steel. The installation of chainlink fences around the perimeter of the project site shall be prohibited, except during Project construction. All steel/metal fencing shall be powder coated to prevent rust.

(e) Building roof projections shall have returns of a minimum length of 6 feet, to avoid a false front/unfinished appearance. The Applicant shall work with staff during the plan check process to finalize the minimum length of the roof returns along the building's north, east, south, and west elevations, and wherever roof returns are required to be provided. If the rear of towers will be visible, towers shall be enclosed on all four sides.

(f) Applicant shall work with Planning Department staff during the plan check process, to ensure that all building elevations and architectural treatments are consistent with the approved on file with the Planning Department.

(g) A minimum of 16 on-site parking spaces shall be provided and maintained.

(h) The Applicant shall work with Planning Department staff during the plan check process, to add a metal cover on the south side of the truck wash canopy, in order to screen the steel columns and the trucks utilizing the truck wash area. The metal cover and/or screen material shall match the proposed industrial building's color, texture, and architecture.

(i) Additional decorative split-face block shall be added, along the building's north elevation, below the window area. The decorative block shall match the material and color used on the building two towers.

(j) The above ground fuel tank shall be relocated to the northeast area of the site to a more appropriate location across from the exiting industrial development, subject to Planning Department review and approval.

(k) All new and proposed signage shall be reviewed and approved by the Planning Department and shall comply with the requirements of the Ontario Development Code – Sign Regulations (Division 8.1).

(l) A final site inspection shall be required from the Planning Department prior to Building Department issuance of an Occupancy Permit.



ENGINEERING DEPARTMENT CONDITIONS OF APPROVAL

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Information Technology & Management Services Department Conditions incorporated)

<input checked="" type="checkbox"/> DEVELOPMENT PLAN <input type="checkbox"/> OTHER	<input type="checkbox"/> PARCEL MAP <input type="checkbox"/> TRACT MAP <input type="checkbox"/> FOR CONDOMINIUM PURPOSES
PROJECT FILE NO. PDEV18-022 RELATED FILE NO(S). PCUP18-021	
<input type="checkbox"/> ORIGINAL <input checked="" type="checkbox"/> REVISED: 5/6/21	

CITY PROJECT ENGINEER & PHONE NO: **Jesus Plasencia, (909) 395-2128**

CITY PROJECT PLANNER & PHONE NO: **Denny Chen, (909) 395-2424**

DAB MEETING DATE: **May 18, 2020**

PROJECT NAME / DESCRIPTION: **A Development Plan to construct a 6,870 square foot industrial building in conjunction with a CUP to establish and operate a towing service on 3.1 acres of land.**

LOCATION: **580 East Belmont Street**

APPLICANT: **Four Sisters Enterprises, LLC**

REVIEWED BY: Raymond Lee 5/10/21
 Raymond Lee, P.E. Date
 Assistant City Engineer

APPROVED BY: [Signature] 5-11-21
 Khoi Do, P.E. Date
 City Engineer



THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.

1. PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, APPLICANT SHALL: Check When Complete

- 1.01 Dedicate to the City of Ontario, the right-of-way, described below:
_____ feet on _____
Property line corner 'cut-back' required at the intersection of _____
and _____.
- 1.02 Dedicate to the City of Ontario, the following easement(s): _____

- 1.03 Restrict vehicular access to the site as follows: _____
- 1.04 Vacate the following street(s) and/or easement(s): _____
- 1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.
- 1.06 Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.
- 1.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.
- 1.08 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Management Services Department at (909) 395-2124 regarding this requirement.
(1) _____
(2) _____
- 1.09 Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.



- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ci.ontario.ca.us) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of three (3) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Management Services at (909) 395-2353 to initiate the CFD application process.
- 1.13 New Model Colony (NMC) Developments:
 - 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
 - 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
 - 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions: _____

2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:

**A. GENERAL
 (Permits includes Grading, Building, Demolition and Encroachment)**

- 2.01 Record Parcel Map/Tract Map No. _____ pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 Submit a duplicate photo mylar of the recorded map to the City Engineer's office.
- 2.03 Note that the subject parcel is a recognized parcel in the City of Ontario per _____
- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of _____
- 2.05 Apply for a: Conforming Grant Deed; Lot Line Adjustment
 - Make a Dedication of Easement (see Condition 2.11)
- 2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.



- 2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.

- 2.08 **Submit a soils/geology report.**

- 2.09 **Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:**
 - State of California Department of Transportation (Caltrans)
 - San Bernardino County Road Department (SBCRD)
 - San Bernardino County Flood Control District (SBCFCD)
 - Federal Emergency Management Agency (FEMA)
 - Cucamonga Valley Water District (CVWD) for sewer/water service
 - United States Army Corps of Engineers (USACE)
 - California Department of Fish & Game
 - Inland Empire Utilities Agency (IEUA)
 - Other: Union Pacific Railroad (UPRR): For proposed improvements in UPRR right-of-way.**

- 2.10 **Dedicate to the City of Ontario the right-of-way described below:**
 1. **33 feet north of centerline on Phillips Street along the project frontage.**
 2. **Property line corner 'cut-back' required at the intersection of Sultana Avenue and Belmont Street.**
 3. **Property line corner 'cut-back' required at the intersection of Sultana Avenue and Phillips Street.**

- 2.11 **Dedicate to the City of Ontario the following easement(s) only if necessary:**
 1. **Sidewalk easement behind the proposed drive approaches located along Sultana Avenue and Belmont Street.**

- 2.12 **New Model Colony (NMC) Developments:**
 - 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.
 - 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.
 - 3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall).

- 2.13 **Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at 100% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.**



- 2.14** The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.

- 2.15** Pay all Development Impact Fees (DIF) to the Building Department. Storm Drain Development Impact Fee, approximately \$69,139, shall be paid to the Building Department. Final fee shall be determined based on the approved site plan.

- 2.16** Other conditions:
 - 1.** The applicant/developer shall process a Lot Line Adjustment and a Conforming Grant Deed in order to consolidate the site into one (1) parcel and render the subject property as a recognized parcel in the City of Ontario. The Lot Line Adjustment and Conforming Grant Deed shall be recorded prior to issuance of a building permit.



B. PUBLIC IMPROVEMENTS

(See attached Exhibit 'A' for plan check submittal requirements.)

- 2.17 Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):**

Improvement	Phillips Street	Sultana Avenue	Belmont Street	Monterey Avenue
Curb and Gutter	<input checked="" type="checkbox"/> New; 20 ft. from C/L north <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New; 18 ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New; 18 ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
AC Pavement (see Condition 2.19)	<input checked="" type="checkbox"/> Replacement <input checked="" type="checkbox"/> Widen 18 feet from C/L north along frontage, including pavm't Transitions	<input checked="" type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input checked="" type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Drive Approach	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Sidewalk	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
ADA Access Ramp	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Parkway	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
Raised Landscaped Median	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Fire Hydrant	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation



Sewer (see Sec. 2.C)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Water (see Sec. 2.D)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Street Light (see Sec. 2.F)	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2G)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Fiber Optics (see Sec. 2K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input checked="" type="checkbox"/> Relocate ⁽¹⁾	<input checked="" type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
Removal of Improvements	Remove existing metal poles and fences	_____	_____	_____
Other Improvements	_____	_____	_____	_____

Specific notes for improvements listed in item no. 2.17, above:

- Relocate existing power poles as needed to accommodate the construction of curb & gutter along the project frontage.

- 2.18 Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s):
- 2.19 Reconstruction of the full pavement structural section on Phillips Street, Sultana Avenue, and Belmont Street, per City of Ontario Standard Drawing number 1011, is required unless a grind and overly is recommended by the soils engineer based on the existing pavement conditions.



Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.

- 2.20 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide water service sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.
- 2.21 **Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892).**
- 2.22 Other conditions: _____

C. SEWER

- 2.23 **An 8-inch sewer main is available for connection by this project in Belmont Street. (Ref: Sewer plan bar code: S12680)**
- 2.24 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.25 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.
- 2.26 **Other conditions:**
 1. **The Occupant of the building shall apply for a Wastewater Discharge Permit for their Establishment, and shall comply with all the requirements of their Wastewater Discharge Permit. Requirements of Wastewater Discharge Permit may include, but not limited to including: possibly installing a monitoring manhole, clarifier, or other sewer pretreatment equipment.**

D. WATER

- 2.27 A _____ inch water main is available for connection by this project in _____. (Ref: Water plan bar code: _____)
- 2.28 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.29 **Other conditions:**
 1. **Replace the existing 4" water line in Belmont, between Sultana and Monterey, with a new 12". All existing appurtenances shall be reconnected to the new 12" main via the installation of new service laterals (no coupling) from main to appurtenance. Upgrade any/all hydrants with breakaway check valve per City standards.**
 2. **Install one fire hydrant on the north side of Phillips Street (south side of property), connecting off the 1074 PZ water line, fronting the property.**

E. RECYCLED WATER

- 2.30 A _____ inch recycled water main is available for connection by this project in _____. (Ref: Recycled Water plan bar code: _____)
- 2.31 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.
- 2.32 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.



- 2.33 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.

Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.

- 2.34 Other conditions: _____

F. TRAFFIC / TRANSPORTATION

- 2.35 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer:
1. On-site and off-site circulation
 2. Traffic level of service (LOS) at 'build-out' and future years
 3. Impact at specific intersections as selected by the City Engineer

- 2.36 New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.

- 2.37 Other conditions:

1. The applicant/developer shall be responsible to design and construct the following streets to their ultimate half-width along the project frontage:

- Sultana Avenue (66' R/W, 36' C/C)
- Belmont Street (66' R/W, 36' C/C)
- Phillips Street (66' R/W, 40' C/C)

Improvements shall include, but not be limited to, concrete curb and gutter, sidewalk, LED street lights, landscaped parkways, signing & striping, and necessary pavement transitions as deemed necessary by the City Engineer.

2. The Applicant/Developer shall be responsible to design and construct in-fill public street lights along the property frontages of Sultana Avenue, Belmont Street and Phillips Street, in accordance with the Traffic and Transportation Design Guidelines Section 1.4 Street Light Plans, City of Ontario Standard Drawing No. 5101, and to the satisfaction of the City Engineer.
3. Driveways shall be designed and constructed in accordance with City of Ontario Standard Drawing No. 1204 for Commercial Driveway Approach.
4. Property frontages along Sultana Avenue, Belmont Street, Phillips Street and Monterey Avenue shall be signed "No Parking Anytime."
5. Proposed gates on Belmont Street shall remain open at all times during business hours.
6. The applicant/developer's engineer-of-record shall meet with City Engineering staff prior to starting signing/striping and/or street lighting design.
7. The applicant/developer shall obtain all rights-of-way necessary to construct all required improvements.
8. All landscaping, block walls, and other obstructions shall be compatible with the stopping sight distance requirements per City of Ontario Standard Drawing No. 1309.

G. DRAINAGE / HYDROLOGY

- 2.38 A _____ inch storm drain main is available to accept flows from this project in _____.
(Ref: Storm Drain plan bar code: _____)



- 2.39 **Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.**
- 2.40 **An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100-year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.**
- 2.41 **Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.**
- 2.42 **Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.**
- 2.43 **Other conditions:**
 1. **The applicant/developer shall pay an in-lieu fee for future storm drain in Sultana Avenue along project frontage valued at \$75,454.**

H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)

- 2.44 **401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels. If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted. Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.**
- 2.45 **Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: <http://www.sbcounty.gov/dpw/land/npdes.asp>.**
- 2.46 **Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.**
- 2.47 **Other conditions:**
 1. **Activities resulting in land disturbance of one acre or more is required to obtain coverage under the Construction General Permit (CGP). The owner is the legally responsible person (LRP) of the site and shall have a Stormwater Pollution Prevention Plan (SWPPP) developed and submitted through the SMARTS website at <https://smarts.waterboards.ca.gov/smarts/faces/SwSmartsLogin.xhtml>**



J. SPECIAL DISTRICTS

- 2.48 File an application, together with an initial payment deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community facilities District Act of 1982. The application and fee shall be submitted a minimum four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact the Financial Services Department at (909) 395-2353 to initiate the CFD application process.

- 2.49 Other conditions: _____

K. FIBER OPTIC

- 2.50 Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole if within close proximity. If there is no OntarioNet handhole, conduit infrastructure shall terminate at either end of project frontage in a handhole.

- 2.51 Refer to the City's Fiber Optic Master Plan and other related documents on the City website for design and layout guidelines. Contact the Information Technology Department at (909) 395-2000, for more information regarding the requirement to install fiber optic improvements.

L. Solid Waste

- 2.52 Onsite solid waste shall be designed in accordance with the City's Solid Waste Manual location at:
<http://www.ontarioca.gov/municipal-utilities-company/solid-waste>

- 2.53 Other conditions:
 - 1. Prior to approval of the any building permits, the Conceptual Solid Waste Handling Plan needs to be updated and converted into a Final Solid Waste Handling Plan Sheet and shall be submitted accompanying the Precise Grading Plan Submittal to the City/OMUC for review and approval. See "Solid Waste Handling Plan (SWHP) Requirements" document for details.
 - 2. The applicant shall submit an Integrated Waste Management Report for review and approval with the Precise Grading Plan. This report shall address the management of all integrated waste (Refuse, Recycling, Organics, etc.). The IWMR shall demonstrate compliance with the "Integrated Waste Management Report Requirements" document.



3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:

- 3.01 **Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.**
- 3.02 Complete all requirements for recycled water usage.
 - 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.
 - 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.
 - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.
- 3.03 **The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.**
- 3.04 NMC Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.
- 3.05 **Confirm payment of all Development Impact Fees (DIF) to the Building Department.**
- 3.06 **Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).**

4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:

- 4.01 **Complete all Conditions of Approval listed under Sections 1-3 above.**
- 4.02 **Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.**
- 4.03 **The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall include a completed Acceptance and Bond Release Checklist, state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process**
- 4.04 **Submit record drawings (PDF) for all public improvements identified within Section 2 of these Conditions of Approval.**



EXHIBIT 'A'
ENGINEERING DEPARTMENT
First Plan Check Submittal Checklist

Project Number: PDEV18-022

The following items are required to be included with the first plan check submittal:

1. **A copy of this check list**
2. **Payment of fee for Plan Checking**
3. **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4. **One (1) copy of project Conditions of Approval**
5. **Include a PDF (electronic submittal) of each required improvement plan at every submittal.**
6. **Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).**
7. **Three (3) sets of Public Street improvement plan with street cross-sections**
8. **Three (3) sets of Private Street improvement plan with street cross-sections**
9. **Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)**
10. **Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)**
11. **Four (4) sets of Public Sewer improvement plan**
12. **Five (5) sets of Public Storm Drain improvement plan**
13. **Three (3) sets of Public Street Light improvement plan**
14. **Three (3) sets of Signing and Striping improvement plan**
15. **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
16. **Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)**
17. **Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.**
18. **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**
19. **One (1) copy of Hydrology/Drainage study**
20. **One (1) copy of Soils/Geology report**
21. **Payment for Final Map/Parcel Map processing fee**




- 22. Three (3) copies of Final Map/Parcel Map
- 23. One (1) copy of approved Tentative Map
- 24. **One (1) copy of Preliminary Title Report (current within 30 days)**
- 25. One (1) copy of Traverse Closure Calculations
- 26. One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.
- 27. Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use.
- 28. **Other: Update the sewer improvement plan for Belmont Street to show the proposed service connection to the project site.**

**CITY OF ONTARIO
LANDSCAPE PLANNING
DIVISION**

303 East "B" Street, Ontario, CA 91764

CONDITIONS OF APPROVAL

Sign Off


Jamie Richardson, Sr. Landscape Planner

2/13/20
Date

Reviewer's Name:
Jamie Richardson, Sr. Landscape Planner

Phone:
(909) 395-2615

D.A.B. File No.:
PDEV18-022 Rev 4

Case Planner:
Denny Chen

Project Name and Location:

Pepe's Towing
554 E Belmont St.

Applicant/Representative:
Howard Parcell Company
4854 Main St
Yorba Linda, CA 92886

A Preliminary Landscape Plan (dated 2/13/20) meets the Standard Conditions for New Development and has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.

A Preliminary Landscape Plan (dated) has not been approved. Corrections noted below are required prior to Preliminary Landscape Plan approval.

A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED AS INCOMPLETE

PREVIOUS COMMENTS – 11/13/19

Civil/ Site Plans

1. The tree inventory identifies 48" of trunk diameter to be mitigated; Schinus mole No. 14.
 - a. New 15 gallon trees min 1" diameter trunk, in addition to trees required; total of 48 trees.
 - b. New 24" box trees min 1.5" diameter trunk, in addition to trees required; total of 32 trees
 - c. Upsizing trees on the plan one size larger such as 15 gallon to 24" box, or 24" to 36" box size.
 - d. Monetary value of the trees removed as identified in the "Guide for Plant Appraisal", approved certified arborist plant appraiser, or may be equal to the value of the installation cost of planting, fertilizing, staking and irrigating 15 gallon trees, (100\$ each) to the City of Ontario Historic Preservation Fund for city tree planting or city approved combination of the above items; total of \$4,800.
2. Show transformers and backflow devices located in planter areas and set back 4' from paving; dimension on plan. Locate on level grade. Coordinate with landscape plans.
3. ~~2/13/20 Spoke with Mr. George Estrada over the counter; locate the wall at the Belmont entry of the tow yard so the landscape planter is in the public parking lot. Provide an employee break area on the inside of the tow yard adjacent to the 4 public parking spaces include a table and/or bench and 2 broad canopy shade trees such as Platanus racemosa 24" box. Provide irrigation to trees and mulch.~~ 3/12/20 updated plans received. Condition has been addressed.

Landscape Plans

4. Locate light standards, fire hydrants, water and sewer lines to not conflict with required tree locations. Coordinate civil plans with landscape plans



CITY OF ONTARIO MEMORANDUM

Development Plan Review

Project: PDEV18-022 (PCUP18-021)

Date: February 13th, 2020

Location: 554 East Belmont Street

By: Nathan Kuan

The following shall become conditions of approval for the project.

Conditions:

1. The applicant/developer shall be responsible to design and construct the following streets to their ultimate half-width along the project frontage:
 - Sultana Avenue (66' R/W, 36' C/C)
 - Belmont Street (66' R/W, 36' C/C)
 - Phillips Street (66' R/W, 40' C/C)Improvements shall include, but not be limited to: concrete curb and gutter, sidewalk, LED street lights, landscaped parkways, signing & striping, and necessary pavement transitions as deemed necessary by the City Engineer.
2. The Applicant/Developer shall be responsible to design and construct in-fill public street lights and service pedestals along the property frontages of Sultana Avenue, Belmont Street and Phillips Street, in accordance with the Traffic and Transportation Design Guidelines Section 1.4 Street Light Plans, City of Ontario Standard Drawing No. 5101, and to the satisfaction of the City Engineer.
3. Driveways shall be designed and constructed in accordance with City of Ontario Standard Drawing No. 1204 for Commercial Driveway Approach.
4. Property frontages along Sultana Avenue, Belmont Street, Phillips Street and Monterey Avenue shall be signed "No Parking Anytime."
5. Proposed gates on Belmont Street shall remain open at all times during business hours.
6. The applicant/developer's engineer-of-record shall meet with City Engineering staff prior to starting signing/striping and/or street lighting design.
7. The applicant/developer shall obtain all rights-of-way necessary to construct all required improvements.
8. All landscaping, block walls, and other obstructions shall be compatible with the stopping sight distance requirements per City of Ontario Standard Drawing No. 1309.

nk;

CITY OF ONTARIO

MEMORANDUM

TO: PLANNING DEPARTMENT, Denny Chen
FROM: BUILDING DEPARTMENT, Kevin Shear
DATE: June 11, 2019
SUBJECT: PDEV18-022

-
- The plan **does** adequately address the departmental concerns at this time.
- No comments
- Report below.

Conditions of Approval

1. See previous report for Conditions.

KS:lr

CITY OF ONTARIO MEMORANDUM

TO: PLANNING DEPARTMENT, Denny Chen
FROM: BUILDING DEPARTMENT, Kevin Shear
DATE: June 27, 2018
SUBJECT: PDEV18-022

-
- The plan **does** adequately address the departmental concerns at this time.
- No comments
- Report below.

Conditions of Approval

1. The site address for this project will be 580 E Belmont St.
2. The lot lines are to be removed.

KS:lm



CITY OF ONTARIO

MEMORANDUM

TO: Denny Chen, Associate Planner
Planning Department

FROM: Paul Ehrman, Deputy Fire Chief/Fire Marshal
Fire Department

DATE: July 6, 2018

SUBJECT: PDEV18-022 - A Development Plan to construct a 6,660-square foot industrial building on 3.01 acres of land located at the southwest corner of Belmont Street and Monterey Avenue, at 554 East Belmont Street, within the IL (Light Industrial) zoning district (APNs: 1049-491-01, 1049-491-02 and 1049-491-03). Related File: PCUP18-021

-
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.
-

SITE AND BUILDING FEATURES:

- A. 2016 CBC Type of Construction: V
- B. Type of Roof Materials: Ordinary
- C. Ground Floor Area(s): 5,460 Sq. Ft.
- D. Number of Stories: 2
- E. Total Square Footage: 6,660 Sq. Ft.
- F. 2016 CBC Occupancy Classification(s): B, S

CONDITIONS OF APPROVAL:

1.0 GENERAL

- 1.1 The following are the Ontario Fire Department ("Fire Department") requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards ("Standards.") It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at www.ontarioca.gov, click on "Fire Department" and then on "Standards and Forms."
- 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

2.0 FIRE DEPARTMENT ACCESS

- 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25') inside and forty-five feet (45') outside turning radius per Standard #B-005.
- 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150') in length shall have an approved turn-around per Standard #B-002.
- 2.4 Access drive aisles which cross property lines shall be provided with CC&Rs, access easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of building plan check.
- 2.5 "No Parking-Fire Lane" signs and /or red painted curbs with lettering are required to be installed in interior access roadways, in locations where vehicle parking would obstruct the minimum clear width requirement. Installation shall be per Standard #B-001.
- 2.6 Security gates or other barriers on fire access roadways shall be provided with a Knox brand key switch or padlock to allow Fire Department access. See Standards #B-003, B-004 and H-001.
- 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-six (26) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by fire department and other emergency services..

3.0 WATER SUPPLY

- 3.1 The required fire flow per Fire Department standards, based on the 2016 California Fire Code, Appendix B, is 2000 gallons per minute (g.p.m.) for 2 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.
- 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.
- 3.4 The water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

4.0 FIRE PROTECTION SYSTEMS

- 4.1 On-site private fire hydrants are required per Standard #D-005, and identified in accordance with Standard #D-002. Installation and locations(s) are subject to the approval of the Fire Department. An application with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- 4.2 Underground fire mains which cross property lines shall be provided with CC & R, easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of fire department plan check. The shared use of private fire mains or fire pumps is allowable only between immediately adjacent properties and shall not cross any public street.
- 4.4 Wood frame buildings that are to be sprinkled shall have these systems in service (but not necessarily finalized) before the building is enclosed.

5.0 BUILDING CONSTRUCTION FEATURES

- 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- 5.3 Single station smoke alarms and carbon monoxide alarms are required to be installed per the California Building Code and the California Fire Code.

- ☒ 5.6 Knox ® brand key-box(es) shall be installed in location(s) acceptable to the Fire Department. All Knox boxes shall be monitored for tamper by the building fire alarm system. See Standard #H-001 for specific requirements.
- ☒ 5.7 Placards shall be installed in acceptable locations on buildings that store, use or handle hazardous materials in excess of the quantities specified in the CFC. Placards shall meet the requirements of National Fire Protection Association (NFPA) Standard 704.

6.0 OTHER SPECIAL USES

- ☒ 6.1 The storage, use, dispensing, or handling of any hazardous materials shall be approved by the Fire Department, and adequate fire protection features shall be required. If hazardous materials are proposed, a Fire Department Hazardous Materials Information Packet, including Disclosure Form and Information Worksheet, shall be completed and submitted with Material Safety Data Sheets to the Fire Department along with building construction plans.
- ☒ 6.3 Underground fuel tanks, their associated piping and dispensers shall be reviewed, approved, and permitted by Ontario Building Department, Ontario Fire Department, and San Bernardino County Fire Department Hazardous Materials Division. In fueling facilities, an exterior emergency pump shut-off switch shall be provided.



CITY OF ONTARIO

MEMORANDUM

TO: Denny Chen, Planning Department

FROM: Douglas Sorel, Police Department

DATE: July 26, 2018

SUBJECT: PDEV18-022 & PCUP18-021: A DEVELOPMENT PLAN AND
CONDITIONAL USE PERMIT TO ESTABLISH A TOW YARD AT 554
EAST BELMONT STREET

The "Standard Conditions of Approval" contained in Resolution No. 2017-027 and Ontario Development Code Section 5.03.310 apply. The applicant shall read and be thoroughly familiar with these conditions, including but not limited to, the requirements listed below.

- Required lighting for all walkways, driveways, doorways, parking areas, and other areas used by the public shall be provided and operate on photosensor. Photometrics shall be provided to the Police Department. Photometrics shall include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- Rooftop addresses shall be installed on the building as stated in the Standard Conditions. The numbers shall be at a minimum 3 feet tall and 1 foot wide, in reflective white paint on a flat black background, and oriented with the bottom of the numbers towards the addressed street.
- The Applicant shall comply with construction site security requirements as stated in the Standard Conditions.
- The indoor storage of motor vehicles shall comply with all requirements of the fire and building codes.
- All vehicles stored outdoors shall be screened from public view by a minimum 8-foot high decorative masonry block wall.
- All vehicles stored shall comply with the requirements for the base zoning district.

In addition to the Standard Conditions, the Ontario Police Department places the following conditions on the project:

- While the Applicant serves as a contracted Rotational Towing Service with the City of Ontario they shall comply with all terms of Title 4-19 of the Ontario Municipal Code and their current Professional Services Agreement with the City.

- The applicant will be responsible for keeping the grounds of the business clean from debris and litter.
- Graffiti abatement by the business owner/licensee, or management shall be immediate and on-going on the premises, but in no event shall graffiti be allowed unabated on the premises for more than 72 hours. Abatement shall take the form of removal, or shall be covered/painted over with a color reasonably matching the color of the existing building, structure, or other surface being abated. Additionally, the business owner/licensee, or management shall notify the City within 24 hours at (909) 395-2626 (graffiti hotline) of any graffiti elsewhere on the property not under the business owner/licensee's or management control so that it may be abated by the property owner and/or the City's graffiti team.

The Applicant is invited to contact Douglas Sorel at (909) 408-1873 with any question or concerns regarding these conditions.

AIRPORT LAND USE COMPATIBILITY PLANNING CONSISTENCY DETERMINATION REPORT



Project File No.: PDEV18-022 & PCUP18-021

Address: 554 East Belmont Street

APN: 1049-491-01, 02 & 03

Existing Land Use: Vacant

Proposed Land Use: Development Plan and CUP to establish a tow yard and 6,600 SF 2-story industrial/warehouse building

Site Acreage: 3.01 ac Proposed Structure Height: 33 FT

ONT-IAC Project Review: N/A

Airport Influence Area: ONT

Reviewed By:

Lorena Mejia

Contact Info:

909-395-2276

Project Planner:

Rudy Zeledon

Date: 7/30/18

CD No.: 2018-036

PALU No.: n/a

The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="radio"/> High Terrain Zone	<input type="radio"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input checked="" type="checkbox"/> FAA Notification Surfaces	<input checked="" type="checkbox"/> Recorded Overflight Notification
<input type="radio"/> Zone 2	<input checked="" type="checkbox"/> 65 - 70 dB CNEL	<input checked="" type="checkbox"/> Airspace Obstruction Surfaces	<input type="radio"/> Real Estate Transaction Disclosure
<input type="radio"/> Zone 3	<input type="radio"/> 60 - 65 dB CNEL	<input checked="" type="checkbox"/> Airspace Avigation Easement Area	
<input type="radio"/> Zone 4		Allowable Height: 110 FT	
<input type="radio"/> Zone 5			

The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1
 Zone 2
 Zone 3
 Zone 4
 Zone 5
 Zone 6

Allowable Height: _____

CONSISTENCY DETERMINATION

This proposed Project is: Exempt from the ALUCP Consistent Consistent with Conditions Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

See Attached Conditions

Airport Planner Signature: _____

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT

CD No.: 2018-036
PALU No.: _____

PROJECT CONDITIONS

1. This project is located within Airspace Avigation Easement Area and is required to file and record an Avigation Easement with the Ontario International Airport Authority prior to obtaining a Certificate of Occupancy.



PLANNING DEPARTMENT ACTIVITY REPORT

Month of April 2021

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

TO: Chairman and Members of the Planning Commission
FROM: Rudy Zeledon, Planning Director *RZ*
DATE: May 25, 2021

Attached, you will find the Planning Department Monthly Activity Report for the month of April 2021. The report describes all new applications received by the Planning Department and actions taken on applications during the month. Please contact me if you have any questions regarding this information.

The attached reports, along with reports from past months, may also be viewed on the City's web site at: <https://www.ontarioca.gov/Planning/Reports/MonthlyActivity>.



Monthly Activity Report: Actions

Month of April 2021

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

DEVELOPMENT ADVISORY BOARD MEETING April 5, 2021

ENVIRONMENTAL ASSESSMENT, TENTATIVE TRACT MAP, AND DEVELOPMENT PLAN REVIEW FOR FILE

NOS. PMTT20-003 AND PDEV20-007: A Tentative Tract Map (File No. PMTT20-003/TT 20345) to subdivide 6.65 acres of land into 1 numbered lot for condominium purposes, 26 numbered lots for single-family dwellings and 20 lettered lots in conjunction with a Development Plan (File No. PDEV20-007) to construct 26 detached single-family units (6-Pack Cluster) and 77 multiple family units (14-plex Courtyard Townhomes), located at the northeast corner of Ontario Ranch Road and Haven Avenue, within the within the Mixed Use District Planning Area 6A of the Rich Haven Specific Plan. Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by City Council on January 27, 2010. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 218-393-07, 218-393-06, 218-393-10, 218-393-22, 218-393-36, 218-393-38 and 218-393-39) **submitted by BrookCal Ontario LLC. and Brookfield Properties Development. Planning Commission action is required.**

Action: The Development Advisory Board adopted a decision recommending the Planning Commission approve the Tentative Tract Map and Development Plan, subject to conditions.

ENVIRONMENTAL ASSESSMENT, TENTATIVE PARCEL MAP AND DEVELOPMENT PLAN REVIEW FOR FILE

NOS. PMTT20-004 AND PDEV20-014: A Development Plan (File No. PDEV20-014) to relocate a Tier III historic single-family residence from its current location approximately 130 feet southeast to the corner of the site and a Tentative Parcel Map-TPM 20255 (File No. PMTT20-004) to subdivide 1.1-acres of land into 4 lots within the LDR-5 (Low Density Residential -2.1 to 5.0 DU/Acre) zoning district located at 730 West Fourth Street. A Mitigated Negative Declaration of environmental effects has been prepared for this project. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP). (APN: 1047-594-52) **submitted by Fred Herzog. Planning Commission action is required. Related File No. PHP20-008.**

Action: The Development Advisory Board adopted a decision recommending the Planning Commission approve the Tentative Parcel Map and Development Plan, subject to conditions.

ZONING ADMINISTRATOR MEETING April 5, 2021

ENVIRONMENTAL ASSESSMENT AND CONDITIONAL USE PERMIT REVIEW FOR FILE NO. PCUP20-018:

A Conditional Use Permit to establish alcoholic beverage sales for consumption on the premises. The request includes rescinding File No. PCUP08-006, previously approved for a Type 47 ABC License (On Sale General), and establishing a Type 70 ABC License (On-Sale General Restrictive Service) in conjunction with an existing 82-room hotel (Comfort Inn) on 1.81 acres of land located at 3333



Monthly Activity Report: Actions

Month of April 2021

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

East Shelby Street, within the Garden Commercial land use district of the R.H. Wagner Properties Specific Plan. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15301 (Class 1, Existing Facilities) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport, and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0210-182-71) **submitted by Fine Hospitality Investment Group Inc. This item was continued from the March 15, 2021 meeting.**

Action: The Zoning Administrator adopted a decision approving the Conditional Use Permit, subject to conditions.

CITY COUNCIL/HOUSING AUTHORITY MEETING April 6, 2021

ENVIRONMENTAL ASSESSMENT AND PLANNED UNIT DEVELOPMENT AMENDMENT REVIEW FOR FILE NO. PUD20-002: An Amendment to the Downtown Civic Center Planned Unit Development, to allow for ground floor residential land use, at grade stand-alone parking structures, and increase height of building element projections from 60 feet to 65 feet, reconfigure vehicular access points, and modify parking stall and drive aisle width development standards. The environmental impacts of this project were previously reviewed in conjunction with an amendment to the Ontario Downtown Civic Center PUD, File No. PUD08-001, for which an Addendum to the Ontario Downtown Civic Center Environmental Impact Report (State Clearinghouse No. 200405115, certified on November 16, 2004), was adopted by the City Council on June 21, 2011. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP). (APNs: 1048-551-10 through 13; 1048-552-13 through 19; 1048-553-01 through 17; 1048-547-04 through 94; and 1048-548-01 through 54). **The Planning Commission recommended approval of this item on February 23, 2021 with a vote of 6 to 0.**

Action: The City Council adopted and waived further reading of an ordinance approving the Planned Unit Development Amendment.

DEVELOPMENT ADVISORY BOARD MEETING April 19, 2021

ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV19-057: A Development Plan to construct a 281,000 square foot industrial warehouse building on 14.29 acres of land generally located at the northeast corner of Haven Avenue and SR-60 Freeway, within the Industrial land use district of the Haven Gateway Centre Specific Plan. Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by City Council on January 27, 2010. This application introduces no new

significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN:108-332-01) **submitted by Executive Development, LLC.**

Action: The Development Advisory Board adopted a decision approving the Development Plan, subject to conditions.

ENVIRONMENTAL ASSESSMENT AND TENTATIVE PARCEL MAP REVIEW FOR FILE NO. PMT21-001: A Tentative Parcel Map (TPM 20328) to subdivide 0.49 acres of land into 4 parcels generally located at the northeast corner of Euclid Avenue and Acacia Avenue, at 1325 and 1329 South Euclid Avenue, within the MDR-11 (Medium Density Residential – 5.1 to 11.0 du/ac) zoning district. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15315 (Class 15, Minor Land Divisions) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1049-531-01 & -02) **submitted by Alex Espinoza. Planning Commission action is required.**

Action: The Development Advisory Board adopted a decision recommending the Planning Commission approve the Tentative Parcel Map, subject to conditions.

ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV20-016: A Development Plan to construct a 74-foot collocated monopine wireless communications facility (T-Mobile and Verizon) on 0.176 acres of land located at 617 East Park Street within the IL (Light Industrial) zoning district. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP) provided certain conditions are met; (APN: 1049-233-13) **submitted by Joel Taubman, Crown Castle Towers. Planning Commission action is required.**

Action: The Development Advisory Board adopted a decision recommending the Planning Commission approve the Development Plan, subject to conditions.

ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV20-030: A Development Plan to construct 224 dwellings, including 87 single-family and 137 multiple-family dwellings, on 21.10 acres of land located at the northeast corner of East Edison and South Mill Creek Avenues. The environmental impacts of this project were previously reviewed in conjunction with File No. PSP05-004, the Rich Haven Specific Plan, for which an Environmental Impact Report (State Clearinghouse No. 2006051081) was previously certified by the City Council on December 4, 2007. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0218-211-12) **submitted by Shea Homes. Planning Commission action is required.**



Monthly Activity Report: Actions

Month of April 2021

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

Action: The Development Advisory Board adopted a decision recommending the Planning Commission approve the Development Plan, subject to conditions.

ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV20-032: A Development Plan to construct 106 single-family dwellings on 10.49 acres of land located at the northeast corner of South Manitoba Place and East La Avenida Drive, within the Low-Medium Density land use district of The Avenue Specific Plan. The environmental impacts of this project were previously reviewed in conjunction with File No. PGPA19-008, for which an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was prepared. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0218-652-27) **submitted by Woodside 05S, LP. Planning Commission action is required.**

Action: The Development Advisory Board adopted a decision recommending the Planning Commission approve the Development Plan, subject to conditions.

ZONING ADMINISTRATOR MEETING April 19, 2021

Meeting Cancelled

CITY COUNCIL/HOUSING AUTHORITY MEETING April 20, 2021

ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT AGREEMENT AMENDMENT REVIEW FOR FILE NO. PDA16-003: A First Amendment to the Development Agreement (File No. PDA16-003) to defer the commencement of certain street improvements and establish the terms and conditions for the development of Tentative Tract Map 20265 (File No. PMTT19-006), a 21.10 acre property located on the east side of Mill Creek Avenue, approximately 670 feet south of Ontario Ranch Road, within the Standalone Residential land use district of the Rich-Haven Specific Plan. The environmental impacts of this project were previously reviewed in conjunction with File No. PSP05-004, the Rich-Haven Specific Plan, for which an Environmental Impact Report (State Clearinghouse No. 2006051081) was certified by the City Council on December 4, 2007. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0218-652-27) **submitted by GDCI-RCCD 2, LP. The Planning Commission recommend approval of this item on March 23, 2021, with a vote of 5 to 0.**

Action: The City Council introduced and waived further reading of an ordinance approving the First Amendment to the Development Agreement.

ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT AGREEMENT REVIEW FOR FILE NO. PDA18-004: A Development Agreement (File No. PDA18-004) between the City of Ontario and Merrill Commerce Center East LLC, to establish the terms and conditions for the development of Tentative Parcel Map 20273 (File No. PMTT20-010), a 366.65 acre property generally bordered by Eucalyptus Avenue to the north, Merrill Avenue to the south, Carpenter Avenue to the east, and Grove Avenue to the west, within the Business Park and Industrial land use districts of the Merrill Commerce Center Specific Plan. The environmental impacts of this project were previously reviewed in conjunction with the Merrill Commerce Center Specific Plan (File No. PSP18-001), for which an Environmental Impact Report (State Clearinghouse No. 2019049079) was certified by the City Council on February 2, 2021. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP). The project site is also located within the Airport Influence area of Chino Airport and is consistent with policies and criteria set forth within the 2011 California Airport Land Use Planning Handbook published by the California Department of Transportation, Division of Aeronautics; (APNs: 1054-111-01; 1054-111-02; 1054-121-01; 1054-121-02; 1054-131-01; 1054-131-02; 1054-141-01; 1054-141-02; 1054-151-01; 1054-151-02; 1054-161-01; 1054-161-02; 1054-161-03; 1054-171-01; 1054-171-02; 1054-171-03; 1054-171-04; 1054-181-01; 1054-181-02; 1054-191-01; 1054-191-02; 1054-201-01; 1054-201-02; 1054-211-01, 1054-211-02; 1054-221-01; 1054-221-02; 1054-331-01; 1054-331-02; 1054-341-01; 1054-341-02; 1054-351-01; 1054-351-02; 1054-361-01; 1054-361-02; 1073-111-01; 1073-111-02; 1073-111-03; 1073-111-04; 1073-111-05; 1073-111-06) **submitted by Merrill Commerce Center East LLC. The Planning Commission recommended approval of this item on March 23, 2021, with a vote of 6 to 0.**

Action: The City Council introduced and waived further reading of an ordinance approving the Development Agreement.

ENVIRONMENTAL ASSESSMENT AND REVIEW FOR TENTATIVE CANCELLATION OF WILLIAMSON ACT CONTRACTS NUMBERS 69-147 (FILE NO. PWIL 20-001) AND 70-167 (FILE PWIL20-002): A Tentative Cancellation of Williamson Act Contracts 69-147 (File No. PWIL20-001) and 70-167 (File No. PWIL20-002), for properties located south of Eucalyptus Avenue and north of Merrill Avenue, approximately 552 feet west of Carpenter Avenue and 630 feet east of Walker Avenue, within Planning Areas 4, 4A, 6, and 6A, of the Merrill Commerce Center Specific Plan. The environmental impacts of this project were previously reviewed in conjunction with the Merrill Commerce Center Specific Plan EIR (State Clearing House #2019049079). The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP). The project site is also located within the Airport Influence area of Chino Airport and is consistent with policies and criteria set forth within the 2011 California Airport Land Use Planning Handbook published by the California Department of Transportation, Division of Aeronautics; (APNs:1054-151-02, 1054-161-03, 1054-201-02, 1054-351-02 and 1073-111-02) **submitted by 9052 Merrill Ave Ontario LLC and Liberty Property Limited Partnership (Prologis). The Planning Commission recommended approval of this item on March 23, 2021, with a vote of 6 to 0.**

Action: The City Council adopted a resolution approving the Tentative Cancellation of the Land Conservation Contracts.

PLANNING/HISTORIC PRESERVATION COMMISSION MEETING
April 27, 2021

ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV20-005: A Development Plan to construct a 256,711 square foot industrial building on 11.3 acres of land located at 875 West State Street, within the IL (Light Industrial) zoning district. The environmental impacts of this project were previously reviewed in conjunction with The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), which was certified by City Council on January 27, 2010. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 1011-161-04 and 1011-161-05) **submitted by Inland Harbor LLC. This item was continued from the March 23, 2021 Planning Commission meeting.**

Action: Continued to the May 25, 2021, Planning Commission meeting.

ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV20-030: A Development Plan to construct 224 dwellings, including 87 single-family and 137 multiple-family dwellings, on 21.10 acres of land located at the northeast corner of East Edison and South Mill Creek Avenues. The environmental impacts of this project were previously reviewed in conjunction with File No. PSP05-004, the Rich Haven Specific Plan, for which an Environmental Impact Report (State Clearinghouse No. 2006051081) was previously certified by the City Council on December 4, 2007. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0218-211-12) **submitted by Shea Homes.**

Action: The Planning Commission adopted a resolution approving the Development Plan, subject to conditions.

ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV20-032: A Development Plan to construct 106 single-family dwellings on 10.49 acres of land located at the northeast corner of South Manitoba Place and East La Avenida Drive, within the Low-Medium Density land use district of The Avenue Specific Plan. The environmental impacts of this project were previously reviewed in conjunction with File No. PGPA19-008, for which an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was prepared. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0218-652-27) **submitted by Woodside 05S, LP.**

Action: The Planning Commission adopted a resolution approving the Development Plan, subject to conditions.

ENVIRONMENTAL ASSESSMENT, CERTIFICATE OF APPROPRIATENESS, DEVELOPMENT PLAN, AND TENTATIVE PARCEL MAP REVIEW FOR FILE NOS. PHP20-008, PDEV20-014 AND PMTT20-004: A Certificate of Appropriateness (File No. PHP20-008) and a Development Plan (File No. PDEV20-014)

to relocate a Tier III historic single-family residence from its current location approximately 130 feet southeast, to the corner of the site, in conjunction with a Tentative Parcel Map (File No. PMTT20-004/TPM 20255) to subdivide 1.1 acres of land into 4 lots within the LDR-5 (Low Density Residential –2.1 to 5.0 du/ac) zoning district located at 730 West Fourth Street. A Mitigated Negative Declaration of environmental effects has been prepared for this project. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP). (APN: 1047-594-52) **submitted by Fred Herzog. This item was continued from the March 23, 2021 Planning Commission meeting.**

Action: The Planning/Historic Preservation Commission adopted resolutions approving the Certificate of Appropriateness, Development Plan, and Tentative Parcel Map, subject to conditions.

ENVIRONMENTAL ASSESSMENT AND TENTATIVE PARCEL MAP REVIEW FOR FILE NO. PMTT21-001: A Tentative Parcel Map (TPM 20328) to subdivide 0.49 acres of land into 4 parcels generally located at the northeast corner of Euclid Avenue and Acacia Avenue, at 1325 and 1329 South Euclid Avenue, within the MDR-11 (Medium Density Residential – 5.1 to 11.0 du/ac) zoning district. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15315 (Class 15, Minor Land Divisions) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1049-531-01 & -02) **submitted by Alex Espinoza.**

Action: The Planning Commission adopted a resolution approving the Tentative Parcel Map, subject to conditions.

ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV20-016: A Development Plan to construct a 74-foot collocated monopine wireless communications facility (T-Mobile and Verizon) on 0.176-acre of land located at 617 East Park Street within the IL (Light Industrial) zoning district. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP) provided certain conditions are met; (APN: 1049-233-13) **submitted by Joel Taubman, Crown Castle Towers.**

Action: The Planning Commission adopted a resolution approving the Development Plan, subject to conditions.

ENVIRONMENTAL ASSESSMENT, TENTATIVE TRACT MAP, AND DEVELOPMENT PLAN REVIEW FOR FILE NOS. PMTT20-003 AND PDEV20-007: A Tentative Tract Map (File No. PMTT20-003/TT 20345) to subdivide 6.65 acres of land into 1 numbered lot for condominium purposes, 26 numbered lots for single-family dwellings and 20 lettered lots in conjunction with a Development Plan (File No. PDEV20-007) to construct 26 detached single-family units (6-Pack Cluster) and 77 multiple family units (14-plex Courtyard Townhomes), located at the northeast corner of Ontario Ranch Road



Monthly Activity Report: Actions

Month of April 2021

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

and Haven Avenue, within the within the Mixed Use District Planning Area 6A of the Rich Haven Specific Plan. Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by City Council on January 27, 2010. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 218-393-07, 218-393-06, 218-393-10, 218-393-22, 218-393-36, 218-393-38 and 218-393-39) **submitted by BrookCal Ontario LLC. and Brookfield Properties Development.**

Action: The Planning Commission adopted resolutions approving the Tentative Tract Map and Development Plan, subject to conditions.

ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT AGREEMENT REVIEW FOR FILE NO. PDA20-002: A Development Agreement (File No. PDA20-002) between the City of Ontario and Rich Haven Marketplace LLC, to establish the terms and conditions for the development of Planning Areas 7A & 7B pursuant to the proposed Rich-Haven Specific Plan Amendment (File No. PSPA19-006), an 81.1 acre property located at the northwest corner of Hamner Avenue and Ontario Ranch Road, within the proposed Light Industrial and Regional Commercial land use districts of the Rich-Haven Specific Plan. Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by City Council on January 27, 2010. This application introduces no new significant environmental impacts, and all previously-adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP). (APNs: 0218-211-17; 0218-211-24; and 0218-211-27) **submitted by Rich Haven Marketplace, LLC. City Council action is required.**

Action: The Planning Commission adopted a resolution recommending the City Council approve the Development Agreement.

ENVIRONMENTAL ASSESSMENT, GENERAL PLAN AMENDMENT, AND SPECIFIC PLAN AMENDMENT REVIEW FOR FILE NOS. PGPA19-005 AND PSPA19-006: A request for approval of the following:

[1] A General Plan Amendment (File No. PGPA19-005) to modify Policy Plan (general plan) Exhibit LU-01, Policy Plan Land Use Plan, changing the land use designation on 105.4 acres of Low Density Residential (2.1 – 5 du/ac), 66.01 acres of Mixed Use (9 – NMC East) and 10.36 acres of Open Space – Non Recreation designated property, to 23.41 acres of Low Density Residential (2.1 – 5 du/ac), 24.16 acres of Low-Medium Density Residential (5.1 – 11 du/ac), 57.83 acres of Medium Density Residential (11.1 – 25 du/ac), 20.46 acres of Mixed Use (9 – NMC East), 48.61 acres of Industrial, and 7.3 acres of Open Space – Non Recreation designated property; and modify Policy Plan (general plan) Exhibit LU-03, Future Buildout, to be consistent with the herein described land use changes; and

[2] An amendment (File No. PSPA19-006) to the Rich Haven Specific Plan, which includes the following map and text revisions:

[A] Change the land use designation on 110.1 gross acres of land from Planning Area 1A – 1F (Residential - SFD), to 25.5 gross acres of Planning Area 1A (Residential - SFD), 24.5 gross acres of Planning Area 1B (Residential – SFD/SFA) and 60.6 gross acres of Planning Area 1C (Residential

- SFD/SFA);

[B] Change the land use designation on 81.1 gross acres of land within Planning Area 7 (Stand-Alone Residential Overlay, Mixed-Use Overlay, Regional Commercial, and SCE Easement/Gas Easement) to, Planning Area 7A (49.4 gross acres of Light Industrial and 6.6 gross acres of Open Space – Non Recreation) and Planning Area 7B (25.1 gross acres of Regional Commercial);

[C] Change the land use designation on 4.13 acres of land within Planning Area 6A from Regional Commercial to Stand-Alone Residential Overlay;

[D] Change the land use designation on 4.13 acres of land within Planning Area 9A from Stand-Alone Residential Overlay to Regional Commercial; and

[E] Various changes to the Specific Plan development standards, exhibits, and text, to reflect the proposed land uses.

The Rich Haven Specific Plan is generally bounded by Riverside Drive, Colony High School and the SCE substation to the north, Hamner Avenue to the east, Old Edison Road to the south, and Hamner Avenue to the west. Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by City Council on January 27, 2010. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 0218-161-04, 0218-161-05, 0218-161-10, 0218-161-11, 0218-211-17, 0218-211-24, 0218-211-27, 0218-211-01 and 0218-393-10) **submitted by Rich Haven Marketplace LLC and Brookcal Ontario, LLC. City Council action is required. This item was continued from the March 23, 2021 Planning Commission meeting.**

Action: The Planning Commission adopted resolutions recommending the City Council approve the General Plan and Specific Plan Amendments.



Monthly Activity Report: New Applications

Month of April 2021

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

PCUP21-006:

Submitted by CATHAY BANK

A Conditional Use Permit to establish a bank (Cathay Bank) on 1.16 acres of land located at 2195 South Grove Avenue, within the Business Park land use district of the Grove Avenue Specific Plan (APN: 0113-641-15). **Zoning Administrator action required.**

PDA-21-002:

Submitted by Inland Harbor Com, LLC

A Development Agreement with Inland Harbor COM, LLC, to establish the terms and conditions of development associated with a Development Plan (File No. PDEV20-018) on 40.07 acres of land generally bordered by Sultana Avenue to the west, Schaefer Avenue to the north, Campus Avenue to the east, and Edison Avenue approximately 1,300 feet to the south, within the proposed Schaefer Avenue Storage Center Specific Plan. **City Council action required.**

PDEV21-016:

Submitted by OC Engineering

A Development Plan to construct one 38,445 square foot industrial building on 1.6 acres of land located at 635 South Taylor Avenue, within the IG (General Industrial) zoning district (APNs: 1049-212-06 and 1049-212-05). **Planning Commission action required.**

PDEV21-017:

Submitted by Tipping Development

A Development Plan to construct two commercial buildings totaling 30,971 square feet on 0.86-acre of land located at 125 West Emporia Street, within LAU-2N (Arts District - North) of the MU-1 (Downtown Mixed Use) zoning district (APN: 1049-059-14). Related File: PMTT21-009. **Planning Commission action required.**

PHP-21-006:

Submitted by Bryan and Monica Sinsabaugh

A Mills Act Contract for a Contributor to the Euclid Avenue Historic District located at 1240 North Euclid Avenue, within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) zoning district (APN: 1047-531-07). **City Council action required.**

PHP-21-007:

Submitted by BRYAN SINSABAUGH

A request for a plaque for a Contributor to the Euclid Avenue Historic District located at 1240 North Euclid Avenue, within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) zoning district (APN: 1047-531-07). **Staff action required.**

PLDG21-002:

Submitted by JULIAN VARGAS

A Boarding House Permit for 6 or fewer occupants on property located within the LDR-5 Zoning District at 1440 N Helen Avenue, within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) zoning district (APN: 1008-441-09). **Staff action required.**

PMTT21-008:

Submitted by Investment City, LLC

A Tentative Parcel Map (TPM 20376) to subdivide 0.998-acre of land into two parcels located at the northwest corner of Oaks Avenue and Spruce Street, within the AR-2 (Residential-Agricultural – 0 to 2.0 du/ac) zoning district (APN: 1014-561-30). Related File PVAR 21-002. **Planning Commission action required.**



Monthly Activity Report: New Applications

Month of April 2021

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

PMTT21-009:

Submitted by RWSS Development LLC

A Tentative Parcel Map to subdivide 0.86-acre of land into 22 commercial airspace condominium units located at 125 West Emporia Street, within LAU-2N (Arts District - North) of the MU-1 (Downtown Mixed Use) zoning district (APN: 1049-059-14). Related File: PDEV21-017. **Planning Commission action required.**

PPRE21-002:

Submitted by Richland Communities

A Preliminary Plan for a proposed Tentative Tract Map to subdivide 19.7 acres of land in conjunction with a Development Plan to construct 191 residential dwelling units, 1.5 acre private park, and several pocket parks, at a density of 10.3 du/ac, within PA 3 of the West Haven Specific Plan (APN: 0218-151-10). **Staff action required.**

PSGN21-023:

Submitted by Enchanted Crystals

A Sign Plan to install a temporary grand opening wall-mounted banner (3 FT x 5 FT) for ENCHANTED CRYSTALS located at 1744 South Euclid Avenue, within the CN (Neighborhood Commercial) zoning district. Banner will be up from 4/1/2021 to 5/1/2021 (APN: 1050-281-02). **Staff action required.**

PSGN21-024:

Submitted by Victor Chen

A Sign Plan to install a non-illuminated wall sign for XIERUN GARDEN USA, INC., located at 2181 East Francis Street, within the IG (General Industrial) zoning district (APN: 113-395-07). **Staff action required.**

PSGN21-025:

Submitted by Elite Sign Services, Inc

A Sign Plan to install one building identification wall sign and a series of loading dock directional wall signs for PROLOGIS, located at 1383 South Cucamonga Avenue, within the IG (General Industrial) zoning district (APN: 1049-411-01). **Staff action required.**

PSGN21-026:

Submitted by Eddy

A Sign Plan to install two wall signs and reface an existing monument sign for CATHAY BANK, located at 2195 South Grove Avenue, within the Grove Avenue Specific Plan (APN: 0113-641-15). **Staff action required.**

PSGN21-027:

Submitted by Electricore Signs

A Sign Plan to install two illuminated wall signs for UP CLINICS, located at 1236 West Brooks Street, within the IP (Industrial Park) zoning district (APN: 1011-121-14). **Staff action required.**

PSGN21-028:

Submitted by Downtown Ontario Business Association

A Sign Plan for the installation of 20 temporary light pole banners to be installed along Euclid Avenue between Holt Boulevard and G Street, for the DOWNTOWN ONTARIO IMPROVEMENT ASSOCIATION (DOIA). Banners will be up from 5/10/21 to 1/31/22. **Staff action required.**



Monthly Activity Report: New Applications

Month of April 2021

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

PSGN21-029:

Submitted by Sign Art Co

A Sign Plan to install two nonilluminated wall signs for iSTAR USA, located at 3500 East Francis Street, within the California Commerce Center Specific Plan (APN: 0211-281-56). **Staff action required.**

PSGN21-030:

Submitted by Trulite Signs Inc

A Sign Plan to install one illuminated wall sign and one nonilluminated monument sign for WIELAND, located at 5100 South Archibald Avenue, within the Colony Commerce Center East Specific Plan (APN: 0218-311-22). **Staff action required.**

PSGN21-031:

Submitted by Renogy

A Sign Plan to install one new nonilluminated wall sign for RENOGY, located at 5050 South Archibald Avenue, within the Colony Commerce Center East Specific Plan (APN: 0218-311-21). **Staff action required.**

PSGN21-032:

Submitted by New Sign Solution

A Sign Plan to install one illuminated wall sign for CHANGAN KITCHEN, located at 701 North Milliken Avenue, within The Ontario Center Specific Plan (APN: 0210-211-43). **Staff action required.**

PSGN21-033:

Submitted by David Hernandez

A Sign Plan to install one temporary banner sign for THREE ANGELS MEDICAL, located at 517 East Holt Boulevard, within the CN (Neighborhood Commercial) zoning district (APN: 1048-522-10). Banner will be up from 4/30/21 to 5/30/22. **Staff action required.**

PSGN21-034:

Submitted by Signarama

A Sign Plan to install two illuminated wall signs for ORANGE COAST TITLE, located at 2151 East Convention Center Way, within the CCS (Convention Center Support Commercial) zoning district. **Staff action required.**

PSGN21-035:

Submitted by Signarama

A Sign Plan to install two wall signs (one illuminated and one nonilluminated) for LANDMARK WEALTH MANAGEMENT GROUP, located at 3333 West Concourse Street, Suite 8100, within The Ontario Center Specific Plan (APN: 0110-261-16). **Staff action required.**

PSGN21-036:

Submitted by The Signs and Services Company

A Sign Plan to install five new wall signs and replacement of text for two existing monument signs for WALMART, located at 1333 North Mountain Avenue, within the Main Street land use district of the Mountain Village Specific Plan (APN: 1008-431-27). **Staff action required.**

PSGN21-037:

Submitted by Premier Sign Service Inc.

A Sign Plan to install three illuminated wall signs for CROWN LEXUS, located at 1125 South Kettering Drive (APN: 0238-251-13). **Staff action required.**



Monthly Activity Report: New Applications

Month of April 2021

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

PSGN21-038:

Submitted by T. Sign

A Sign Plan to install one illuminated wall sign for DEMEANOR BARBERSHOP, located at 628 West Holt Boulevard (APN: 1048-591-31). **Staff action required.**

PSGN21-039:

Submitted by Swain Sign Inc.

A Sign Plan to install one wall-mounted, internally illuminated sign for ROCKLER WOODWORKING AND HARDWARE, located at 4320 East Mills Circle, Unit G, within the Ontario Mills/California Commerce Center North Specific Plan (APN: 0238-014-46). **Staff action required.**

PTUP21-016:

Submitted by T-Mobile West

A Temporary Use Permit to establish a temporary wireless facility (T-Mobile) consisting of a 55 FT monopole antenna, 8-foot high chain link security fence with mesh screening, and supporting equipment, located at 1155 South Wanamaker Avenue, within the California Commerce Center Specific Plan (APN: 0238-221-36). Duration: 06/01/2021 to TBD. **Staff action required.**

PTUP21-017:

Submitted by Juicy's LLC

A Temporary Use Permit for an outdoor event with games, rides, and food (no alcohol) within the parking lot of the Toyota Arena, located at 4000 Ontario Center Parkway, within The Ontario Center Specific Plan (APN: 0210-205-01). Event to be held on Saturdays and Sundays only, noon to 10:00PM, 4/3/2021 thru 4/25/2021. **Staff action required.**

PTUP21-018:

Submitted by Iglesia de Dios de la Profecia

A Temporary Use Permit for an outdoor flower sales event for Mother's Day in conjunction with Iglesia de Dios de la Profecia, located at 1130 South Campus Avenue (APN: 1049-503-33). Event to be held on 5/8 and 5/9/2021. **Staff action required.**

PTUP21-019:

Submitted by Church in the Valley

A Temporary Use Permit for Church in the Valley to conduct church services at Celebration Park, located at 4980 South Celebration Avenue. Event to be held 5/9/2021 thru 7/25/2021, Sundays only, 10:00AM to 12:00PM. **Staff action required.**

PTUP21-020:

Submitted by Ben Spell

A Temporary Use Permit for a charitable fundraising event within Ontario Mills parking lot, located at 1 East Mills Circle, within the Regional Commercial land use district of the Ontario Mills Specific Plan (APN: 0238-014-36). Event to be held 5/27/2021 thru 6/7/2021. **Staff action required.**

PTUP21-021:

Submitted by International Line Builders

A Temporary Use Permit for a temporary SCE service yard consisting of two office trailers, storage containers, materials, and equipment on property located at 13545 South Walker Avenue, within the SP/AG (Specific Plan/Agricultural Overlay) zoning district (APN: 0216-213-02). Temporary facility duration: 5/1/2021 thru 5/1/2022. **Staff action required.**



Monthly Activity Report: New Applications

Month of April 2021

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

PVAR21-002:

Submitted by Investment City, LLC

A Variance to reduce the corner lot width dimension from 120 feet to 117.8 feet, filed in conjunction with a Tentative Parcel Map (TPM 20376) to subdivide 0.998-acre of land into two Parcels, within the AR-2 (Residential-Agricultural – 0 to 2.0 du/ac) zoning district, located at the NWC of Oaks Avenue and Spruce (APN: 1014-561-30). Related File: PMTT21-008. **Planning Commission action required.**

PVER21-018:

Submitted by Global Zoning

A Zoning Verification for property located at 1525 and 1541 West Brooks Street, within the IG (General Industrial) zoning district (APNs: 1011-112-07, 1011-112-08, 1011-112-09, and 1011-112-10). **Staff action required.**

PVER21-019:

Submitted by Johnathan Speaks

A Zoning Verification for property located at 3787 East Guasti Road, within the Ontario Gateway Specific Plan (APN: 0210-212-55). **Staff action required.**

PVER21-021:

Submitted by InSite Real Estate LLC

A Zoning Verification for property located at on the east side of Archibald Avenue, approximately, 225 feet south of Philadelphia Street, within the California Commerce Center South Specific Plan (APN: 1083-071-19). **Staff action required.**

PVER21-022:

Submitted by Jordan Bartell

A Zoning Verification for property located at 4621 East Guasti Road, within the IG (General Industrial) zoning district (APN: 0238-042-19). **Staff action required.**

PVER21-023:

Submitted by PZR

A Zoning Verification for property located at 2151 South Vintage Avenue, within the IH (Heavy Industrial) zoning district (APN: 0238-161-71). **Staff action required.**