

**CITY OF ONTARIO
CITY COUNCIL AND HOUSING AUTHORITY
AGENDA
JUNE 16, 2020**

Paul S. Leon
Mayor

Debra Dorst-Porada
Mayor pro Tem

Alan D. Wapner
Council Member

Jim W. Bowman
Council Member

Ruben Valencia
Council Member



Scott Ochoa
City Manager

Scott E. Huber
City Attorney

Sheila Mautz
City Clerk

James R. Milhiser
Treasurer

WELCOME to a meeting of the Ontario City Council.

- All documents for public review are on file with the Records Management/City Clerk's Department located at 303 East B Street, Ontario, CA 91764.
- Anyone wishing to speak during public comment or on a particular item will be required to fill out a blue slip. Blue slips must be turned in prior to public comment beginning or before an agenda item is taken up. The Clerk will not accept blue slips after that time.
- Comments will be limited to 3 minutes. Speakers will be alerted when they have 1 minute remaining and when their time is up. Speakers are then to return to their seats and no further comments will be permitted.
- In accordance with State Law, remarks during public comment are to be limited to subjects within Council's jurisdiction. Remarks on other agenda items will be limited to those items.
- Remarks from those seated or standing in the back of chambers will not be permitted. All those wishing to speak including Council and Staff need to be recognized by the Chair before speaking.

ORDER OF BUSINESS The regular City Council and Housing Authority meeting begins with Public Comment at 6:30 p.m. immediately followed by the Regular Meeting and Public Hearings. No agenda item will be introduced for consideration after 10:00 p.m. except by majority vote of the City Council.

(EQUIPMENT FOR THE HEARING IMPAIRED AVAILABLE IN THE RECORDS MANAGEMENT OFFICE)

CALL TO ORDER (*OPEN SESSION*)

6:30 p.m.

ROLL CALL

Dorst-Porada, Wapner, Bowman, Valencia, Mayor/Chairman Leon

PLEDGE OF ALLEGIANCE

Council Member Wapner

INVOCATION

PUBLIC COMMENTS

6:30 p.m.

The Public Comment portion of the Council/Housing Authority meeting is limited to 30 minutes with each speaker given a maximum of 3 minutes. An opportunity for further Public Comment may be given at the end of the meeting. Under provisions of the Brown Act, Council is prohibited from taking action on oral requests.

As previously noted -- if you wish to address the Council, fill out one of the blue slips at the rear of the chambers and give it to the City Clerk.

AGENDA REVIEW/ANNOUNCEMENTS The City Manager will go over all updated materials and correspondence received after the Agenda was distributed to ensure Council Members have received them. He will also make any necessary recommendations regarding Agenda modifications or announcements regarding Agenda items to be considered.

SPECIAL CEREMONIES

TWENTIETH ANNUAL MODEL COLONY AWARDS

That the City Council present the 2020 “Model Colony” Awards for Historic Preservation.

CONSENT CALENDAR

All matters listed under CONSENT CALENDAR will be enacted by one motion in the form listed below – there will be no separate discussion on these items prior to the time Council votes on them, unless a member of the Council requests a specific item be removed from the Consent Calendar for a separate vote.

Each member of the public wishing to address the City Council on items listed on the Consent Calendar will be given a total of 3 minutes.

1. APPROVAL OF MINUTES

Minutes for the regular meeting of the City Council and Housing Authority of May 19, 2020, approving same as on file in the Records Management Department.

2. BILLS/PAYROLL

Bills May 15, 2020 through May 28, 2020 and **Payroll** May 10, 2020 through May 23, 2020, when audited by the Finance Committee.

3. A RESOLUTION CONTINUING THE EXISTENCE OF A LOCAL EMERGENCY FOR THE 2020 CORONAVIRUS PANDEMIC

That the City Council adopt a resolution continuing the Proclamation of Local Emergency for the 2020 Coronavirus Pandemic until further notice with an update scheduled for the July 7, 2020, City Council Meeting, unless cancelled sooner.

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, PROCLAIMING THE CONTINUED EXISTENCE OF A LOCAL EMERGENCY AND DIRECT THE EMERGENCY ORGANIZATION OF THE CITY OF ONTARIO TO TAKE THE NECESSARY STEPS FOR THE PROTECTION OF LIFE, HEALTH AND SAFETY IN THE CITY OF ONTARIO.

4. A RESOLUTION OF THE CITY COUNCIL REQUESTING A DELAY IN THE IMPLEMENTATION OF TITLE 14 OF THE CALIFORNIA CODE OF REGULATIONS, SECTION 15064.3, ESTABLISHING THRESHOLDS OF SIGNIFICANCE FOR VEHICLE MILES TRAVELED

That the City Council adopt a resolution requesting the State delay the implementation of Title 14 of the California Code of Regulations, Section 15064.3.

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, REQUESTING A DELAY OF THE IMPLEMENTATION OF TITLE 14 OF THE CALIFORNIA CODE OF REGULATIONS, SECTION 15064.3, ESTABLISHING A THRESHOLD OF SIGNIFICANCE FOR VEHICLE MILES TRAVELED (VMT).

5. A RESOLUTION ADOPTING VEHICLE MILES TRAVELED THRESHOLDS FOR DETERMINING SIGNIFICANCE OF TRANSPORTATION IMPACTS THROUGH THE CALIFORNIA ENVIRONMENTAL QUALITY ACT IN CONFORMANCE WITH SB 743

That the City Council consider and adopt a resolution adopting Vehicle Miles Traveled (VMT) Thresholds for determining significance of transportation impacts through the California Environmental Quality Act (CEQA) in conformance with Senate Bill 743 (SB 743).

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, ADOPTING VEHICLES MILES TRAVELED THRESHOLDS OF SIGNIFICANCE FOR PURPOSES OF ANALYZING TRANSPORTATION IMPACTS UNDER THE CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA).

6. AN AMENDMENT TO THE COOPERATIVE AGREEMENT FOR THE SR-60 AT ARCHIBALD AVENUE INTERCHANGE PROJECT/SAN BERNARDINO COUNTY TRANSPORTATION AUTHORITY (SBCTA)

That the City Council approve Amendment No. 4 in the amount of \$610,000 to Cooperative Agreement R14138 (15-1001150) (on file in the Records Management Department) with San Bernardino County Transportation Authority (SBCTA) for funding the State Route 60 (SR-60) at Archibald Avenue Interchange Project (Project); and authorize the City Manager to execute said agreement and future amendments within the authorization limits.

7. A COOPERATIVE AGREEMENT FOR THE DESIGN, RIGHT OF WAY AND CONSTRUCTION OF PHASE 1 OF THE WEST VALLEY CONNECTOR BUS RAPID TRANSIT PROJECT/SAN BERNARDINO COUNTY TRANSPORTATION AUTHORITY (SBCTA)

That the City Council approve a Cooperative Agreement (20-1002422) (on file in the Records Management Department) with San Bernardino County Transportation Authority (SBCTA) for the design, right of way and construction of Phase 1 of the West Valley Connector Bus Rapid Transit Project; and authorize the City Manager to execute said agreement, future amendments and related documents.

8. A RESOLUTION ORDERING THE SUMMARY VACATION OF A PUBLIC EASEMENT FOR FIRE ACCESS PURPOSES GENERALLY LOCATED SOUTH OF GUAISTI ROAD APPROXIMATELY 1,515 FEET EAST OF HAVEN AVENUE

That the City Council adopt a resolution ordering the summary vacation of a public easement for fire access purposes generally located south of Guasti Road approximately 1,515 feet east of Haven Avenue.

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, ORDERING THE SUMMARY VACATION OF A PUBLIC EASEMENT FOR FIRE ACCESS PURPOSES GENERALLY LOCATED SOUTH OF GUAISTI ROAD APPROXIMATELY 1,515 FEET EAST OF HAVEN AVENUE.

9. ANNUAL LEVY OF ASSESSMENTS WITHIN ONTARIO PARKWAY MAINTENANCE ASSESSMENT DISTRICT NOS. 1, 2, 3 AND 4 FOR FISCAL YEAR 2020-21

That the City Council adopt resolutions pertaining to the levy of assessments within Ontario Parkway Maintenance Assessment District (OPMAD) Nos. 1, 2, 3 and 4:

- (A) A resolution initiating proceedings and ordering the preparation of an Engineer's Report for each district relating to the annual levy of assessments; and
- (B) A resolution giving preliminary approval of the Engineer's Report; and
- (C) A resolution for District Nos. 1, 2 and 3, and a resolution for District No. 4, declaring the City's intention to levy the special assessments for Fiscal Year 2020-21 and setting the date of July 21, 2020 for a Public Hearing on each district in accordance with the Landscaping and Lighting Act of 1972.

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, ORDERING THE PREPARATION OF THE ASSESSMENT ENGINEER'S REPORTS PURSUANT TO THE PROVISIONS OF DIVISION 15, PART 2, OF THE STREETS AND HIGHWAYS CODE OF THE STATE OF CALIFORNIA, FOR PROCEEDINGS FOR THE ANNUAL ASSESSMENT LEVY WITHIN ONTARIO PARKWAY MAINTENANCE ASSESSMENT DISTRICTS NO. 1, 2, 3 AND 4 FOR FISCAL YEAR 2020-21.

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING THE ASSESSMENT ENGINEER'S REPORTS FOR THE ANNUAL LEVY OF ASSESSMENTS WITHIN ONTARIO PARKWAY MAINTENANCE ASSESSMENT DISTRICTS NO. 1, 2, 3 AND 4 FOR FISCAL YEAR 2020-21.

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, DECLARING ITS INTENTION TO PROVIDE FOR AN ANNUAL LEVY AND COLLECTION OF ASSESSMENTS FOR FISCAL YEAR 2020-21 IN ONTARIO PARKWAY MAINTENANCE ASSESSMENT DISTRICTS NO. 1, 2 AND 3, PURSUANT TO THE PROVISIONS OF DIVISION 15, PART 2, OF THE STREETS AND HIGHWAYS CODE OF THE STATE OF CALIFORNIA, AND SETTING A TIME AND PLACE FOR A PUBLIC HEARING THEREON.

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, DECLARING ITS INTENTION TO PROVIDE FOR AN ANNUAL LEVY AND COLLECTION OF ASSESSMENTS FOR FISCAL YEAR 2020-21 IN ONTARIO PARKWAY MAINTENANCE ASSESSMENT DISTRICT NO. 4, PURSUANT TO THE PROVISIONS OF DIVISION 15, PART 2, OF THE STREETS AND HIGHWAYS CODE OF THE STATE OF CALIFORNIA, AND SETTING A TIME AND PLACE FOR A PUBLIC HEARING THEREON.

10. ANNUAL LEVY OF ASSESSMENTS WITHIN STREET LIGHTING MAINTENANCE DISTRICT NOS. 1 AND 2 FOR FISCAL YEAR 2020-21

That the City Council adopt resolutions pertaining to the levy of assessments within Street Lighting Maintenance District (SLMD) Nos. 1 and 2:

- (A) A resolution initiating proceedings and ordering the preparation of an Engineer's Report for each district relating to the annual levy of assessments;
- (B) A resolution giving preliminary approval of the Engineer's Reports; and
- (C) A resolution for each district declaring the City's intention to levy the special assessments for Fiscal Year 2020-21 and setting the date of July 21, 2020 for a Public Hearing on each district in accordance with the Landscaping and Lighting Act of 1972.

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, ORDERING THE PREPARATION OF THE ASSESSMENT ENGINEER'S REPORTS PURSUANT TO THE PROVISIONS OF DIVISION 15, PART 2, OF THE STREETS AND HIGHWAYS CODE OF THE STATE OF CALIFORNIA, FOR PROCEEDINGS FOR THE ANNUAL ASSESSMENT LEVY WITHIN ONTARIO STREET LIGHTING MAINTENANCE DISTRICTS NO. 1 AND NO. 2 FOR FISCAL YEAR 2020-21.

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING THE ASSESSMENT ENGINEER'S REPORTS FOR THE ANNUAL LEVY OF ASSESSMENTS WITHIN ONTARIO STREET LIGHTING MAINTENANCE DISTRICTS NO. 1 AND NO. 2 FOR FISCAL YEAR 2020-21.

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, DECLARING ITS INTENTION TO PROVIDE FOR AN ANNUAL LEVY AND COLLECTION OF ASSESSMENTS FOR FISCAL YEAR 2020-21 IN ONTARIO STREET LIGHTING MAINTENANCE DISTRICT NO. 1 PURSUANT TO THE PROVISIONS OF DIVISION 15, PART 2, OF THE STREETS AND HIGHWAYS CODE OF THE STATE OF CALIFORNIA, AND SETTING A TIME AND PLACE FOR A PUBLIC HEARING THEREON.

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, DECLARING ITS INTENTION TO PROVIDE FOR AN ANNUAL LEVY AND COLLECTION OF ASSESSMENTS FOR FISCAL YEAR 2020-21 IN ONTARIO STREET LIGHTING MAINTENANCE DISTRICT NO. 2, PURSUANT TO THE PROVISIONS OF DIVISION 15, PART 2, OF THE STREETS AND HIGHWAYS CODE OF THE STATE OF CALIFORNIA, AND SETTING A TIME AND PLACE FOR A PUBLIC HEARING THEREON.

11. FIVE-YEAR CONSOLIDATED PLAN FOR FISCAL YEARS 2020-2024 AND ONE-YEAR ACTION PLAN FOR THE COMMUNITY DEVELOPMENT BLOCK GRANT, HOME INVESTMENT PARTNERSHIPS, AND EMERGENCY SOLUTIONS GRANT PROGRAMS FOR FISCAL YEARS 2020-21, AND ASSESSMENT OF FAIR HOUSING

That the City Council:

- (A) Approve the Five-Year Consolidated Plan for Fiscal Years 2020-24, One-Year Action Plan for Fiscal Years 2020-21 for the Community Development Block Grant (“CDBG”), HOME Investment Partnerships (“HOME”), and Emergency Solutions Grant (“ESG”) Programs (on file in the Records Management Department), and 2020 Assessment of Fair Housing (“AFH”); and
- (B) Direct staff to prepare and transmit the final documents to the U.S. Department of Housing and Urban Development (“HUD”); and
- (C) Authorize the City Manager, or his designee, to take all actions necessary or desirable to implement the Five-Year Consolidated Plan, Subrecipient Agreements, and the Assessment of Fair Housing.

12. AN AMENDMENT TO THE AGREEMENT WITH TRUEPOINT SOLUTIONS FOR REMOTE ACCELA SUPPORT

That the City Council authorize the City Manager to execute an amendment (on file in the Records Management Department) to the agreement with TruePoint Solutions, LLC, of Loomis, California, for Accela remote support extending the term by three years to June 2023 and increasing the revised authorized contract amount by \$300,000, for a new total of \$400,000.

13. AUTHORIZE THE PURCHASE OF LICENSING AND EQUIPMENT FOR A SECURITY LOGGING SYSTEM/SHI INTERNATIONAL CORP.

That the City Council award Bid No. 1317 from SHI International Corp. of Somerset, New Jersey and authorize the purchase of additional licensing and equipment in the amount of \$190,220 for a security logging system.

14. A MAINTENANCE SERVICE AGREEMENT FOR THE POLICE DEPARTMENT FIREARMS TRAINING FACILITY/GREEN GLOBAL ENVIRONMENTAL

That the City Council authorize the City Manager to execute a five-year maintenance service agreement (on file in Records Management Department) with Green Global Environmental, of Claremont, CA, for range cleaning and maintenance at an annual estimated cost of \$30,564, or a total of \$152,820 for the term of the agreement.

15. AN AMENDMENT TO EXTEND THE PROFESSIONAL SERVICES AGREEMENT FOR BOOKING, JAIL AND TRANSPORTATION SERVICES/GEO CORRECTIONS AND DETENTION, LLC

That the City Council authorize the City Manager to execute a three-year extension of the current professional services agreement (on file in the Records Management Department) with GEO Corrections and Detention, LLC, of Boca Raton, Florida, for booking, jail and transportation services with a not to exceed total cost of \$1,676,702 for the three-year period.

16. APPROVAL OF PRE-AUTHORIZED VENDORS TO PROVIDE SERVICES AND PARTS FOR SPECIALIZED FLEET AND EQUIPMENT

That the City Council approve the pre-authorized vendors to provide parts and maintenance services for the following specialized fleet and equipment: automotive vehicles, paving equipment, Integrated Waste vehicles, John Deere and Toro mowers.

17. AWARD OF A PROFESSIONAL SERVICES AGREEMENT TO DUDEK FOR THE SOUTH ARCHIBALD PLUME REPORTING

That the City Council approve a five-year Professional Services Agreement (on file in the Records Management Department) with Dudek of Encinitas, California, for the Annual South Archibald Plume Monitoring and Reporting in the amount of \$63,500; and authorize the City Manager to execute the agreement and any future amendments within the authorization limits.

18. AWARD OF DESIGN SERVICE AGREEMENTS FOR ON-CALL WATER SUPPLY STUDIES/ALBERT A. WEBB/MICHAEL BAKER INC./PLACEWORKS

That the City Council approve and authorize the City Manager to execute three-year Design Services Agreements (on file in the Records Management Department) with Albert A. Webb, of Riverside, California, Michael Baker Inc., of Ontario, California, and PlaceWorks of Santa Ana, California; and authorize the City Manager to extend the agreements for up to two (2) additional one (1) year periods consistent with City Council approved budgets.

PUBLIC HEARINGS

Pursuant to Government Code Section 65009, if you challenge the City's zoning, planning or any other decision in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the City Council at, or prior to the public hearing.

19. A PUBLIC HEARING TO CONSIDER AN ORDINANCE APPROVING THE FOURTH AMENDMENT TO THE DEVELOPMENT AGREEMENT (FILE NO. PDA05-002) BETWEEN THE CITY OF ONTARIO AND SC ONTARIO DEVELOPMENT COMPANY, LLC, TO MODIFY THE COMMENCEMENT OF CERTAIN SPECIFIC INFRASTRUCTURE ASSOCIATED WITH THE DEVELOPMENT OF TENTATIVE TRACT MAP 20316 (FILE NO. PMTT19-020), AND CONFORM TO THE REVISED DEVELOPMENT STANDARDS ESTABLISHED BY THE PARKSIDE SPECIFIC PLAN AMENDMENT (FILE NO. PSPA19-007), LOCATED AT THE NORTHWEST CORNER OF EUCALYPTUS AVENUE AND ARCHIBALD AVENUE, WITHIN PLANNING AREAS 1 THROUGH 26 OF THE PARKSIDE SPECIFIC PLAN (APNS: 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, AND 0218-221-10)

That the City Council introduce and waive further reading of an ordinance approving the Fourth Amendment to the Development Agreement (File No. PDA05-002) between the City of Ontario and SC Ontario Development Company, LLC, to modify the commencement of certain specific infrastructure associated with the development of Tentative Tract Map 20316 (File No. PMTT19-020) and conform to the revised development standards established by the Parkside Specific Plan Amendment (File No. PSPA19-007).

Notice of public hearing has been duly given and affidavits of compliance are on file in the Records Management Department.

Written communication.

Oral presentation.

Public hearing closed.

ORDINANCE NO. _____

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING A FOURTH AMENDMENT TO THE DEVELOPMENT AGREEMENT (FILE NO. PDA05-002), BETWEEN THE CITY OF ONTARIO AND SC ONTARIO DEVELOPMENT COMPANY, LLC, TO MODIFY THE COMMENCEMENT OF CERTAIN SPECIFIC INFRASTRUCTURE ASSOCIATED WITH THE DEVELOPMENT OF TENTATIVE TRACT MAP 20316 (FILE NO. PMTT19-020), AND CONFORM TO THE REVISED DEVELOPMENT STANDARDS ESTABLISHED BY THE PARKSIDE SPECIFIC PLAN AMENDMENT (PSPA19-007), LOCATED AT THE NORTHWEST CORNER OF EUCALYPTUS AVENUE AND ARCHIBALD AVENUE, WITHIN PLANNING AREAS 1 THROUGH 26 OF THE PARKSIDE SPECIFIC PLAN, AND MAKING FINDINGS IN SUPPORT THEREOF—APNS: 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, AND 0218-221-10.

20. A PUBLIC HEARING TO CONSIDER AN AMENDMENT TO THE PARKSIDE SPECIFIC PLAN (FILE NO. PSPA19-007) TO: [1] RECONFIGURE RESIDENTIAL PLANNING AREAS 1 THROUGH 4, AND 17 THROUGH 19; [2] RECONFIGURE THE GREAT PARK PLANNING AREA 22 (EAST OF THE CUCAMONGA CREEK CHANNEL); [3] REVISE INTERNAL CIRCULATION TO IMPROVE ACCESS INTO THE NEIGHBORHOOD COMMERCIAL PLANNING AREA 21; [4] UPDATE AND REVISE RESIDENTIAL DESIGN GUIDELINES (SECTIONS 7.1 THROUGH 7.6) TO INTRODUCE NEW HOUSING TYPES AND ARCHITECTURAL STYLES; AND [5] UPDATE AND REVISE LANDSCAPE STANDARDS (SECTION 7.7). THE PROJECT SITE IS BOUNDED BY ONTARIO RANCH ROAD TO THE NORTH, EUCALYPTUS AVENUE TO SOUTH, ARCHIBALD AVENUE TO THE EAST AND CARPENTER AVENUE TO THE WEST. (APNS): 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, AND 0218-221-10

That the City Council consider and adopt a resolution approving an Addendum to the Parkside Specific Plan Environmental Impact Report (SCH# 2004011008), analyzing the environmental effects of the Project, pursuant to State CEQA Guidelines section 15164; and adopt a resolution approving an Amendment to the Parkside Specific Plan (File No. PSPA19-007).

Notice of public hearing has been duly given and affidavits of compliance are on file in the Records Management Department.

Written communication.

Oral presentation.

Public hearing closed.

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING AN ADDENDUM TO THE PARKSIDE SPECIFIC PLAN ENVIRONMENTAL IMPACT REPORT (SCH#2004011008), PURSUANT TO THE REQUIREMENTS OF THE CALIFORNIA ENVIRONMENTAL QUALITY ACT, AS AMENDED, FOR FILE NO. PSPA19-007, AND MAKING FINDINGS IN SUPPORT THEREOF — APNS: 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, AND 0218-221-10.

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, APPROVAL OF FILE NO. PSPA19-007, AN AMENDMENT TO THE PARKSIDE SPECIFIC PLAN TO: [1] RECONFIGURE THE RESIDENTIAL PLANNING AREAS 1 THROUGH 4, AND 17 THROUGH 19; [2] RECONFIGURE THE GREAT PARK PLANNING AREA 22 (EAST OF THE CUCAMONGA CREEK CHANNEL); [3] REVISE INTERNAL CIRCULATION TO IMPROVE ACCESS INTO THE NEIGHBORHOOD COMMERCIAL PLANNING AREA 19; [4] UPDATE AND REVISE RESIDENTIAL DESIGN GUIDELINES (SECTIONS 7.1 THROUGH 7.6) TO INTRODUCE NEW HOUSING TYPES AND ARCHITECTURAL STYLES; AND [5] UPDATE AND REVISE LANDSCAPE STANDARDS (SECTION 7.7). THE PROJECT SITE IS BOUNDED BY ONTARIO RANCH ROAD TO THE NORTH, EUCALYPTUS AVENUE TO THE SOUTH, ARCHIBALD AVENUE TO THE EAST AND CARPENTER AVENUE TO THE WEST, AND MAKING FINDINGS IN SUPPORT THEREOF—APNS: 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, AND 0218-221-10.

21. ECONOMIC DEVELOPMENT SUBSIDY FIVE-YEAR REPORTS FOR SIGMANET INTERNET, INC., HENRY SCHEIN, INC., PAG ONTARIO B1, INC., QVC, INC., AND STAPLES, INC. AS REQUIRED BY GOVERNMENT CODE 53083 (D)

That the City Council hold a public hearing to consider any written or oral comments on the information contained in the Economic Development Subsidy five-year reports for SIGMANet Internet, Inc., Henry Schein, Inc., PAG Ontario B1, Inc., QVC, Inc., and Staples, Inc., as required by Government Code 53083(d).

Notice of public hearing has been duly given and affidavits of compliance are on file in the Records Management Department.

Written communication.
Oral presentation.
Public hearing closed.

STAFF MATTERS

City Manager Ochoa

COUNCIL MATTERS

Mayor Leon
Mayor pro Tem Dorst-Porada
Council Member Wapner
Council Member Bowman
Council Member Valencia

ADJOURNMENT

CITY OF ONTARIO

Agenda Report
June 16, 2020

SECTION:
SPECIAL CEREMONIES

SUBJECT: TWENTIETH ANNUAL MODEL COLONY AWARDS

RECOMMENDATION: That the City Council present the 2020 “Model Colony” Awards for Historic Preservation.

COUNCIL GOALS: Focus Resources in Ontario’s Commercial and Residential Neighborhoods Encourage, Provide or Support Enhanced Recreational, Educational, Cultural and Healthy City Programs, Policies and Activities

FISCAL IMPACT: None.

BACKGROUND: The Planning Commission, acting as the Historic Preservation Commission, developed the “Model Colony” Awards program to recognize outstanding achievement in the preservation of Ontario’s historic resources.

The “Model Colony” Awards are presented by the City Council each spring to coincide with National Historic Preservation Month. Two awards will be presented in the categories of preservation. On April 28, 2020, the Planning/Historic Preservation Commission considered each category and chose the following recipients:

Restoration Award: For achievement in the restoration of a historic resource.

Recipient: Gardiner W. Spring Auditorium
Chaffey Joint Union High School District

Rehabilitation Award: For achievement in the rehabilitation of a historic resource.

Recipient: Chaffey High School Math and Science Building
Chaffey Joint Union High School District

STAFF MEMBER PRESENTING: Scott Murphy, AICP, Executive Director Development Agency

Prepared by: Elly Antuna
Department: Planning

City Manager Approval: 

Submitted to Council/O.H.A. 06/16/2020

Approved: _____

Continued to: _____

Denied: _____

PLANNING / HISTORIC PRESERVATION COMMISSION STAFF REPORT



DATE: April 28, 2020
FILE NO: PHP20-004
SUBJECT: 2020 "Model Colony" Awards
LOCATION: Citywide
APPLICANT: City Initiated
PROPERTY OWNER: N/A

RECOMMENDATION:

That the Planning/Historic Preservation Commission consider and approve the 2020 "Model Colony" Award nominations.


BACKGROUND:

In 2000, the City Council adopted the Model Colony Awards to recognize outstanding efforts to restore, rehabilitate, and preserve Ontario's historic places. This is the twentieth consecutive year that the City has conducted the awards program. The award categories include: Restoration, Rehabilitation, John S. Armstrong Landscape, Founder's Heritage Award, George Chaffey Memorial, and Merit. Past Model Colony Award recipients included Ontario's schools, churches, single-family residences, multi-family properties, and joint public/private preservation projects.

There are 2 nominations this year, both for projects at historic Chaffey High School. The first is for the successful infill construction of the new Math and Science Building on the south edge of the campus. The second is for the restoration of the Gardiner W. Spring Auditorium. The 2020 Model Colony Awards will be presented to award recipients by the City Council during a special ceremony and reception which has been tentatively scheduled for June 2, 2020.

HISTORIC CONTEXT:

Ontario was founded in September of 1882 by George and William B. Chaffey. The Chaffey's established three principles for the "Colony" that had social and economic implications including a mutual water company concept, a grand thoroughfare, and an agricultural college

Case Planner:	Elly Antuna, Associate Planner	Hearing Body	Date	Decision	Action
Planning Director Approval:		HPSC:			
Submittal Date:	N/A	PC / HPC:	04/28/2020	APPROVED	Final
Hearing Deadline:	N/A	CC:	06/02/2020		Presentation

for general education. In 1882, the first year of the Model Colony, George Chaffey set aside a 20-acre site at the corner of Euclid Avenue and Fourth Street for an agricultural college.

The site of Chaffey College was established in 1885 as an affiliate of USC. Both high school and college courses were offered here from 1901 to 1960. Between 1911 and 1933 several new buildings and additions were constructed on the campus to accommodate the growing needs of the surrounding communities. Then, in 1933 the Long Beach earthquake shook Ontario, damaging most of the buildings on the high school and junior college campus, resulting in the condemnation of all the high school buildings. This disaster occurred during the Great Depression when the school district was in dire financial condition.

By taking full advantage of recovery act funding sources such as State Emergency Relief Act (SERA), Public Works Administration (PWA), and Work Projects Administration (WPA), school superintendent Gardiner W. Spring obtained funding to rebuild all the high school buildings and add new buildings to the junior college. The availability of these funding sources leveled the playing field for communities throughout the region that would not typically be able to secure well-known architects, let alone rebuild an entire school campus.

Gardiner W. Spring Auditorium, designed by famed architects Allison and Allison of Los Angeles, was one of the buildings constructed in 1939 by the WPA and the PWA. The reinforced concrete auditorium replaced the 1912 auditorium and library building that was condemned after the earthquake. Allison and Allison belonged to a preeminent school of architects that have been characterized as “true romantics” leading the revival period of architecture in Southern California. The buildings they designed were adapted and heavily influenced by Spanish, Mexican and Italian architecture because of the similar climate to Southern California. Just a few of the buildings that bare their name include schools and collegiate projects, churches, libraries, post offices, factories and warehouses. Allison and Allison helped to shape Southern California architecture, leaving their mark at Chaffey High School with Gardiner W. Spring Auditorium and Tower Hall.

Since then, the high school has continued to evolve to accommodate a growing and changing community. The Chaffey High School campus had been determined to meet the National Register of Historic Places holding historic significance on a local, state and national level. On January 19, 1999, the Ontario City Council designated the campus as Local Landmark No. 58, and on June 4, 2013 it was designated as a Contributor to the Euclid Avenue Historic District.

2020 AWARD NOMINEES:

For their outstanding efforts in the field of historic preservation, the nominees are:

Restoration Award:	Gardiner W. Spring Auditorium, Chaffey High School
Award Recipient:	Chaffey Joint Union High School District

Since its construction and its very first show in 1939, the Gardiner W. Spring Auditorium has been revered by the community, which is evident by the continued efforts of not only the school district, but local organizations, to preserve, restore and protect this iconic historic resource.



In 2017, with funds from Measure P, efforts began to completely restore and update the Spanish Colonial style 2,400 seat auditorium. One of many significant interior features is a massive 20,000 square foot Art Deco mural on the acoustic plaster ceiling. The mural was originally painted on the auditorium ceiling by artist E. Peterson. Over time, the mural had turned from blue to a bluish green, mostly as a result of a period where smoking was allowed in the auditorium. Architectural arts restoration experts were brought on to conduct a finishes investigation and conservation testing of the auditorium. It was determined that the acoustic plaster substrate contained asbestos and required abatement along with the complete replacement of the ceiling and mural. The architectural art restoration team carefully measured and documented the entire ceiling by digital photography. Pounce patterns were created, and a grid system was used to layout and recreate the mural on the new acoustic plaster surface. Additionally, all the original decorative painting was reinstated throughout the auditorium and public spaces.



The modernization to the auditorium also presented a unique set of challenges, one being that there were no as-built drawings of the historic structure to guide the project. The complexity of the auditorium building required a sophisticated approach to accurately capture the building dimensions. To achieve this, 3D laser scanning was used to scan the entire structure and provide complete as-built drawings. The modernization of the historic theater provides not only improvements of the existing structure, but structural repairs and a new cooling and ventilation system. Renovations to the structure include theatrical and acoustical upgrades, a new catwalk and steel stairway, two new elevators, accessibility upgrades and a lobby restroom expansion. The sound system is especially impressive as it was completely upgraded with professional, state-of-the-art equipment, making it one of the best equipped venues in the region.

Throughout the project, every effort was made to preserve and protect original features, and where necessary, replace with materials that were custom made to replicate the original. The original exterior doors featured an ornate design and when new doors needed to be made, the original design was replicated on each door. The auditorium seats were also special ordered to closely replicate the intricate designs on the aisle seats. The restoration project not only preserved this iconic auditorium but brought it into the 21st century with state-of-the-art technology, making it a resource that the community

will cherish and enjoy for generations to come.

Rehabilitation Award: Chaffey High School Math & Science Building
Award Recipient: Chaffey Joint Union High School District

One of the newest additions to the Chaffey High School campus is a state of the art, three-story Math and Science building located at the southern edge of the campus along East Fourth Street. The school district was committed to constructing a new building that did not compromise the historic integrity of the campus and knew that the campus was not only important to the district, but the community. With input from community members, students, teachers, parents, and other stakeholders and through a facilities bond (Measure P), the new 55,000 square foot, three-story, \$20 Million Math and Science Building was completed and welcomed students for the 2017-2018 academic year. The structural steel frame building with an exterior plaster finish contains 18 state-of-the-art classrooms and 12 science laboratories. The design of the new building is both respectful and complementary of the historic campus and incorporates the latest technology for the classrooms. Rather than compete with the unique Mission Revival architectural styled, WPA era buildings and their ornate churrigueresque entryways, the new building exhibits a quiet elegance that blends in effortlessly with the existing historic buildings. The new building is rectangular in plan with a flat roof and features a smooth plaster finish with powder coated steel windows, matching the finishes on the existing historic buildings. The Art Deco style entryway is in a central tower element covered with a hipped tile roof and wide overhanging boxed eaves. Secondary entrances are framed by deep recessed arched entryways.



The successful infill project is an excellent example of appropriate infill on a historic property and in a Historic District. The sensitive infill construction of this state-of-the-art building ensures that Chaffey High School will remain one of the City's premium historic properties.

COMPLIANCE WITH THE ONTARIO PLAN:

The Model Colony Awards Program is consistent with the principles, goals and policies contained in the following components of The Ontario Plan (TOP), including: (1) Vision, (2) Governance, and (3) Policy Plan (General Plan):

[1] City Council Priorities

Goals:

- Invest in the Growth and Evolution of the City's Economy
- Focus Resources in Ontario's Commercial and Residential Neighborhoods
- Encourage, Provide or Support Enhanced Recreational, Educational, Cultural and Healthy City Programs, Policies and Activities.

[2] Vision

Distinctive Development

- Development Quality: A community that is so well maintained and litter-free that its properties uniformly convey a sense of prosperity that is readily apparent and a symbol of community pride.

Dynamic Balance

- An appreciation for the “personality and charm” of this community, preserving important characteristics and values even as growth and change occur, all the while retaining a distinctive local feel where people love to be.

[3] Governance

Governance – Decision Making

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices
 - G1-1 Consistency with Policies. We require that staff recommendations to the City Council be consistent with adopted City Council Priorities (Goals and Objectives) and the Policy Plan.
 - G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision.

[4] Policy Plan (General Plan)

Community Design – Image & Identity

- **Goal CD1:** A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses
 - **CD1-3: Neighborhood Improvement.** We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.

Community Design – Historic Preservation

- **Goal CD4:** Historic buildings, streets, landscapes and neighborhoods, as well as the story of Ontario's people, businesses, and social and community organizations, that have been preserved and serve as a focal point for civic pride and identity.
 - **CD4-6: Promotion of Public Involvement in Preservation.** We engage in programs to publicize and promote the City's and the public's involvement in preservation efforts.
 - **CD4-7: Public Outreach.** We provide opportunities for our residents to research and learn about the history of Ontario through the Planning Department, Museum of History and Art, Ontario and the Robert E. Ellingwood Model Colony History Room.

Community Design – Protection of Investment

- **Goal CD5:** A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.
 - **CD5-4: Neighborhood Involvement.** We encourage active community involvement to implement programs aimed at the beautification and improvement of neighborhoods.

CITY OF ONTARIO

Agenda Report
June 16, 2020

SECTION:
CONSENT CALENDAR

SUBJECT: A RESOLUTION CONTINUING THE EXISTENCE OF A LOCAL EMERGENCY FOR THE 2020 CORONAVIRUS PANDEMIC

RECOMMENDATION: That the City Council adopt a resolution continuing the Proclamation of Local Emergency for the 2020 Coronavirus Pandemic until further notice with an update scheduled for the July 7, 2020, City Council Meeting, unless cancelled sooner.

COUNCIL GOALS: Maintain the Current High Level of Public Safety


FISCAL IMPACT: This resolution continues the existence of a declared local emergency and the City's compliance with State and Federal requirements for emergency response. Furthermore, this action allows continued eligibility for State and Federal reimbursement for eligible costs associated with Coronavirus Disease 2019 (abbreviated COVID-19).

BACKGROUND: Governments worldwide are responding to an outbreak of respiratory disease caused by a novel (new) coronavirus that was first detected in China and which has now been detected in nearly 100 countries, including in the United States. The virus has been named "SARS-CoV-2"; and the disease COVID-19).

COVID-19 is a serious disease that as of June 4, 2020 has killed over 391,000 people worldwide with over 1,870,000 confirmed cases and over 108,000 deaths in the United States. On March 4, 2020, the Governor of the State of California declared a State of Emergency. On March 10, 2020, the San Bernardino County Health Department proclaimed a Public Health Emergency; and the County Board of Supervisors issued a Proclamation of Local Emergency due to COVID-19. On March 11, 2020, the World Health Organization declared the disease a global pandemic. On March 13, 2020, the President of the United States declared a National Emergency.

Through Ontario Ordinance No. 2990, the City Manager, as the Director of Emergency Services, proclaimed a local emergency on March 14, 2020 in order to enhance the City's ability to mobilize local

STAFF MEMBER PRESENTING: Jordan Villwock, Fire Administrative Director

Prepared by: Raymond Cheung
Department: Fire
City Manager Approval: 

Submitted to Council/O.H.A. 06/16/2020
Approved: _____
Continued to: _____
Denied: _____

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resources, coordinate interagency response, accelerate procurement of vital supplies, use mutual aid, and seek potential reimbursement by the State and Federal governments. On March 17, 2020 the City Council ratified the local emergency proclamation; and then on April 7, 2020, May 5, 2020, June 2, 2020, the City Council issued additional proclamations to continue the local emergency.

The City of Ontario regularly participates in disaster drills to maintain its preparedness. The City's first and second response agencies are prepared for the impact of COVID-19; however, continuing the existence of a local emergency continues to unlock resources and legal authority to quickly respond to changing conditions.

Per California Government Code § 8630, a proclamation of local emergency must be ratified by the governing body within 7 days of issuance and reviewed and continued every 30 days thereafter until termination of the local emergency as conditions warrant.

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, PROCLAIMING THE CONTINUED EXISTENCE OF A LOCAL EMERGENCY AND DIRECT THE EMERGENCY ORGANIZATION OF THE CITY OF ONTARIO TO TAKE THE NECESSARY STEPS FOR THE PROTECTION OF LIFE, HEALTH AND SAFETY IN THE CITY OF ONTARIO.

WHEREAS, City of Ontario Ordinance No. 2990 empowers the City Manager as the Director of Emergency Services to proclaim the existence or threatened existence of a local emergency when said city is affected or likely affected by a public calamity and the City Council is not in session; and

WHEREAS, conditions of disaster or of extreme peril to the safety of persons and property have arisen both Internationally and within the United States as a result of the introduction of the novel coronavirus (COVID-19), a novel communicable disease, which was first detected in Wuhan City, Hubei Province, China in December 2019; and

WHEREAS COVID-19 has spread globally to over 70 countries, infecting more than 153,000 persons in the United States and killing more than 2,800. COVID-19 has created conditions that are likely to be beyond the control of local resources and require the combined forces of other political subdivisions to combat; and

WHEREAS, on February 26, 2020, the CDC confirmed the first case of local person-to-person transmission of COVID-19 in the United States and this case raises the possibility of community transmission occurring in the general public, the Health Officer of San Bernardino County and the San Bernardino County Board of Supervisors has determined that there is an imminent threat to the public health from the introduction of COVID-19 in the City of Ontario and has declared a Local Health Emergency; and

WHEREAS, the City's ability to mobilize local resources, coordinate interagency response, accelerate procurement of vital supplies, use mutual aid, and seek future reimbursement by the State and Federal governments will be critical to successfully responding to COVID-19; and

WHEREAS, these conditions warrant and necessitate that the City of Ontario declare the existence of a local emergency.

WHEREAS, on March 17, 2020 the Ontario City Council ratified Resolution No. 2020-027 proclaiming the existence of local emergency.

WHEREAS, on April 7, 2020 the Ontario City Council continued the existence of a local emergency through Resolution No. 2020-028.

WHEREAS, on May 5, 2020 the Ontario City Council continued the existence of a local emergency through Resolution No. 2020-038.

WHEREAS, on June 2, 2020 the Ontario City Council continued the existence of a local emergency through Resolution No. 2020-062.

NOW THEREFORE, BE IT RESOLVED, by the City Council of the City of Ontario does hereby proclaim the continued existence of a local emergency and direct the Emergency Organization of the City of Ontario to take the necessary steps for the protection of life, health and safety in the City of Ontario.

IT IS FURTHER ORDERED, that during the existence of said local emergency the powers, functions, and duties of the Emergency Organization of the City shall be those prescribed by state law, by ordinances, and resolutions of the City; and

THE ONTARIO CITY COUNCIL FURTHER DIRECTS, that all City Departments shall review and revise their department emergency and contingency plans to address the risks COVID-19 poses to their critical functions in coordination with the Office of Emergency Management (OEM); and

THE ONTARIO CITY COUNCIL FURTHER DIRECTS, that all City Departments shall track costs for staffing, supplies, and equipment related to COVID-19 preparation and prevention and forward that information to the Financial Services Agency; and

THE ONTARIO CITY COUNCIL FURTHER DIRECTS, that OEM shall coordinate Citywide planning, preparedness and response efforts regarding COVID-19 with the San Bernardino County Department of Public Health and the San Bernardino County Office of Emergency Services (OES).

NOW, THEREFORE, BE IT FURTHER RESOLVED, by the City Council of the City of Ontario, that the Resolution of Local Emergency shall take effect immediately and that widespread publicity and notice shall be given said Declaration through the most feasible and adequate means of disseminating such notice throughout the City.

IT IS FURTHER RESOLVED AND ORDERED, by the City Council of the City of Ontario, that a copy of this Resolution be forwarded to the San Bernardino County Office of Emergency Services to be forwarded to the Director of the California Governor's Office of Emergency Services requesting that the Director find it acceptable in accordance with State law; that the Governor of California pursuant to the Emergency Services Act, issue a resolution declaring an emergency in San Bernardino County; that the Governor waive regulations that may hinder response and recovery efforts; that response and recovery assistance be made available under the California Disaster Assistance Act; and that the State expedite access to State and Federal resources and any other appropriate federal disaster relief programs.

The City Clerk of the City of Ontario shall certify as to the adoption of this Resolution.

PASSED, APPROVED, AND ADOPTED this 16th day of June 2020.

PAUL S. LEON, MAYOR

ATTEST:

SHEILA MAUTZ, CITY CLERK

APPROVED AS TO FORM:

COLE HUBER, LLP
CITY ATTORNEY

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, SHEILA MAUTZ, City Clerk of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. 2020- was duly passed and adopted by the City Council of the City of Ontario at their regular meeting held June 16, 2020 by the following roll call vote, to wit:

AYES: COUNCIL MEMBERS:

NOES: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

SHEILA MAUTZ, CITY CLERK

(SEAL)

The foregoing is the original of Resolution No. 2020- duly passed and adopted by the Ontario City Council at their regular meeting held June 16, 2020.

SHEILA MAUTZ, CITY CLERK

(SEAL)

CITY OF ONTARIO

Agenda Report
June 16, 2020

SECTION:
CONSENT CALENDAR

SUBJECT: A RESOLUTION OF THE CITY COUNCIL REQUESTING A DELAY IN THE IMPLEMENTATION OF TITLE 14 OF THE CALIFORNIA CODE OF REGULATIONS, SECTION 15064.3, ESTABLISHING THRESHOLDS OF SIGNIFICANCE FOR VEHICLE MILES TRAVELED

RECOMMENDATION: That the City Council adopt a resolution requesting the State delay the implementation of Title 14 of the California Code of Regulations, Section 15064.3.

COUNCIL GOALS: Operate in a Businesslike Manner

FISCAL IMPACT: None.

BACKGROUND: In 2013, SB 743 was adopted which directed the Office of Planning and Research (OPR) to examine alternative ways to analyze transportation impacts of projects during the California Environmental Quality Act (CEQA) process. OPR determined that using Vehicle Miles Traveled (VMT) is a more suitable metric than Level of Service (LOS) in determining transportation impacts of a project. In 2018, the VMT guidelines were added to the CEQA Guidelines. By July 1, 2020, all cities in the State of California are required to adopt VMT thresholds for passenger vehicle and light duty trucks and determine the methodology to be used during this analysis.

Over the past several months, City staff has been working with San Bernardino County Transportation Authority (SBCTA) to develop VMT thresholds for the City of Ontario. And while staff has continued to work to provide the thresholds, the Building Industry Association (BIA) and California Chamber of Commerce have expressed concerns about the timing of the VMT implementation. As a result, they have requested that the Governor postpone the implementation of the VMT thresholds (see exhibits to this report). The BIA notes that the COVID-19 pandemic has had a dramatic impact on development and businesses and the implementation would better serve the State by delaying the start date to July 1, 2021.

STAFF MEMBER PRESENTING: Scott Murphy, AICP, Executive Director Development Agency

Prepared by: Scott Murphy
Department: Development
City Manager Approval: 

Submitted to Council/O.H.A. 06/16/2020
Approved: _____
Continued to: _____
Denied: _____

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In that there is currently no extension in place, staff has developed a VMT threshold to comply with SB 743. A delay in implementation would provide staff additional time to work with the BIA and our development partners to refine our VMT analysis and threshold criteria.



Baldy View Chapter

May 27, 2020

Dear Mayors and City Council members,

The Building Industry Association of Southern California, Baldy View Chapter (BIA) is a leading advocate for thousands of building industry leaders who are committed to a better future for California by building communities, creating jobs and ensuring housing opportunities for everyone. **We respectfully request that all Cities in San Bernardino County add the attached resolution to your upcoming June meeting agenda for a vote by your City Council.**

Specifically, the resolution requests that Governor Newsom extend the implementation date of Senate Bill (SB) 743 for 1-year until July 1, 2021. As you are aware SB 743 creates a process to change the way that transportation impacts are analyzed under CEQA. This new law eliminates vehicular congestion, traditionally expressed as Level of Service (LOS), as the operative metric for identifying transportation impacts. In its place, SB 743 identifies Vehicle Miles Traveled (VMT) as the key impact criterion under CEQA.

The adoption of the VMT threshold by cities is a very complex matter and requires the services of expensive expert traffic engineers, tremendous staff time and resources and multiple public meetings to ensure public input on the adoption of the threshold. As noted in the resolution, a 1-year extension on the July 1, 2020, implementation date is prudent considering:

The COVID-19 pandemic is resulting in dramatic changes in transportation patterns such as the expansion of working from home, learning from home, and even receiving medical care from home via remote communication technologies, as well as 90% decreases in public transit ridership based on social distancing and related health priorities, the requested extension will allow for the informed consideration of VMT and other potential changes to CEQA.

Thank you for your consideration and please contact me to confirm the status of our request to adopt a VMT delay resolution next month. In the meantime, we look forward to continuing the collaborative dialogue with your staff concerning each of your respective VMT plans that are due on July 1. Please have your staff contact me regarding the status of your respective VMT plans and proposed adoption dates at their earliest convenience. I can be reached at carlos@biabuild.com and/or 909-641-4630.

Sincerely,

Carlos Rodriguez,
Chapter Executive Officer





April 15, 2020

The Honorable Gavin Newsom
 Governor, State of California
 State Capitol
 Sacramento, CA 95814

Dear Governor Newsom:

We deeply appreciate your aggressive leadership responding to the coronavirus pandemic. The business community is also helping where possible by providing essential services, having employees work remotely, and of course providing support for our workers and communities. Like you, our businesses are keenly focused on meeting this crisis and finding a path to recovery. The interconnectedness of our economy and its supply chains means the reverberation of shutting down public gatherings and non-essential outings has been felt by every Californian.

You have already taken preliminary actions to mitigate some of the burdens on individuals and private employers as they deal with the vast economic consequences, including directing tax agencies to delay the income tax filing deadlines and suspending the California WARN act for employers who have unexpectedly been required to shut down their businesses with little notice. We are grateful for these efforts so far but

believe there is much more to be done to ease the economic pain being visited on employers and their workers throughout the state.

Our organizations recommend the Administration urgently act to pause non-essential state regulatory functions that will not impact measures designed to protect imminent harm to human health. Specifically, we urge you to issue an Executive Order temporarily suspending all pending new rulemaking, as well as any rules or amendments first taking effect after your stay-at-home order of March 19, 2020, not urgently needed to protect public health, for all state agencies and commissions for at least six months.

Since public participation is an integral part of the development of new regulations and proposed rules, regulators must protect due process rights of regulated entities and the public. As businesses direct their focus to providing essential services and protecting their workforce, they will lose the capacity to meaningfully contribute to draft regulations or proposals from state agencies, boards, and commissions. Nor is there an accessible platform for stakeholders to participate on a virtual basis that is also equipped to accommodate widespread public comments or participation.

We are aware of the following agencies that are still moving forward with proposed rulemaking, which can be postponed during this emergency without affecting public health and safety, or the state's response to the COVID-19 crisis. As we become aware of others, we will advise you.

- Draft Supplemental Guidance by the Department of Toxic Substances Control: Screening and Evaluating Vapor Intrusion, for which public comment is due April 30.
- Department of Fish & Wildlife Commission - Petition to List the Southern California/Central Coast Evolutionarily Significant Unit (ESU) of Mountain Lions as Threatened under the California Endangered Species Act (CESA), submitted by the Center for Biological Diversity and the Mountain Lion Foundation – Hearing April 16.
- California Department of Housing and Community Development (HCD) - Multifamily Housing Program (MHP) 2020 Draft Guideline amendments: Comments deadline May 5.
- Air Resources Board (ARB) - Vessels at berth regulation: Comments deadline May 1.
- ARB Transportation Refrigeration Unit regulation: Comments deadline April 27 (extended 30 days from March 27)
- ARB Advanced Clean Trucks: Expected public comment deadline approximately week of May 11-22.
- ARB Public Hearing on adoption of the proposed amendments to the Regulation on the Commercialization of Alternative Diesel Fuels (ADF) – April 23.
- State Water Resources Control Board (SWRCB) Microplastics in Drinking Water: Comments deadline April 24. On Board agenda 6/16/20.
- SWRCB Hexvalent Chromium MCL Staff workshop 4/27/20: Comments deadline May 15.
- SWRCB Drinking Water Fees: Comments deadline May 15.
- Water Board Once Through Cooling Extensions: Comments deadline May 11.
- CalRecycle – SB 1383 Regulatory Language – Comments deadline May 20.

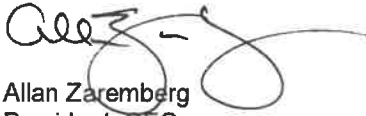
Employers should also have an extended period of time to implement regulations that have not yet taken effect, for at least 90 days following the end of both a local and statewide shelter-in-place order. We are aware of the following upcoming regulations and will advise of others as we become aware:

- CEQA guidelines adopted by the Natural Resources Agency implementing SB 743 changes on analysis and mitigation of VMT thresholds.
- Suspend the enforcement of AB 827 that is set to take effect July 1, 2020, as it applies to amusement parks. Before the Legislature went into emergency recess, the author of AB 827 was moving rapidly his clean-up urgency legislation, AB 1506, to clarify application of the original bill. With the Legislature on indefinite recess, amusement parks would be expected to spend substantial sums for compliance – even if not open for business;
- California Consumer Privacy Act regulations, set to take effect July 1, 2020.

- State Wetland Definition and Procedures for Discharges of Dredged or Fill Material to Waters of the State – implementation date May 28, 2020.

We believe that Californians' health and safety will not suffer from a pause in these regulatory processes, and that state regulators can accommodate these changes to their schedules. We appreciate your urgent attention to this matter.

Sincerely,

A handwritten signature in black ink, appearing to read "Allan Zaremberg", with a large, stylized flourish extending to the right.

Allan Zaremberg
President, CEO
California Chamber of Commerce

On behalf of the above organizations

AZ:JB:ll

California Legislature

May 25th, 2020

The Honorable Gavin Newsom
California State Capitol
Sacramento, California 95814

RE: Extension of the July 2020 Implementation Date for the Vehicle Miles Travelled Regulations

Dear Governor Newsom:

We would like to thank you for your leadership and prompt and decisive action to protect our state from the COVID-19 pandemic. We applaud you for taking this threat seriously, and taking the necessary actions to prevent its rapid spread thereby reducing the impacts on our healthcare system, economy, and housing that are sure to come. It is in this spirit that we bring to your attention a regulation that we believe will dramatically impact the ability for many Californians, including millennials and communities of color in particular, to purchase a home – Vehicle Miles Travelled (VMT), and respectfully request the extension of the implementation date to July 1st, 2022.

In December 2018, the California Natural Resources Agency adopted new regulations for the implementation of the California Environmental Quality Act (CEQA). The CEQA Guidelines contain many provisions that will increase the likelihood that housing projects will be subject to litigation and cost increases. Among them was the newly proposed impact known as VMT.

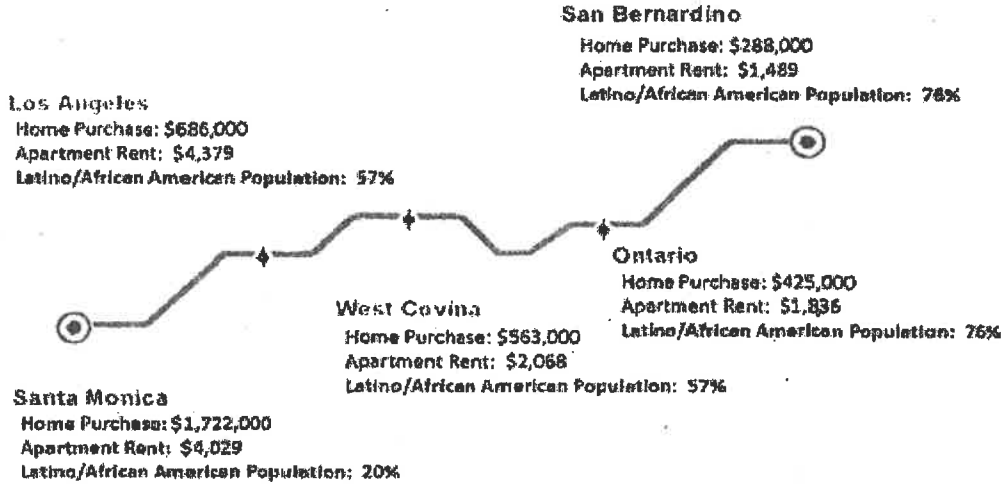
VMT requires residential project developers to quantify how many miles home buyers or renters drive to work, to the store, for recreation, and other purposes. This new impact is in addition to existing laws which require the quantification and mitigation of air quality impacts, including greenhouse gas emissions, from tail pipes. Accordingly, it will apply even once we convert to 100% zero emission vehicles.

The ideological approach of VMT is to get people to abandon their individual vehicles and utilize multimodal transit opportunities such as walking, biking, and using public transit. The regulation views road congestion as a good thing, since it slows down traffic and incentivizes individuals to use alternative forms of transit. Improvements like road widening is considered a negative impact on greenhouse gas reductions because it increases commuter speeds which the regulation assumes will encourage people to drive longer distances. The new regulation advocates that California go on a "road diet" and calls into question whether the voters understood this when they approved an increase in the gas tax.

The highest costs imposed by the VMT regulation is in areas farther away from job centers. This is where housing can be produced at the lowest cost, and is the primary source of housing for low and middle class Californians. However, measures to mitigate VMT, especially in rural areas, significantly drive up the costs of residential development. VMT also disproportionately impacts low- and middle-class Californians who are predominately communities of color. This point is illustrated in the graph below which shows that as people drive "until they qualify", home prices drop by \$19,000 per mile as they move farther way from Santa Monica towards San Bernardino.



Figure 1: Geography of Southern California Region's Housing Cost Crisis
Housing Costs increase \$19,000 per Mile
Median 2BR Apartment Rents Increase \$33 per month per mile
(77 Mile Commute Distance to Coast)



Communities of color depend on cars more than non-protected classes to get to their jobs which are often not at fixed locations served by transit (construction, farmworkers, janitors, etc.). The VMT regulation will increase, not decrease, the cost of housing and will have its greatest impacts on classes protected by the federal and state constitutions and a variety of federal and state laws prohibiting housing discrimination. Moreover, a \$1,000 increase in the cost of a home eliminates 8,870 households from the ability to afford a home and puts the American dream of homeownership – the primary method of establishing economic stability, community participation and economic growth – further out of reach for those struggling to afford a home today.

During the COVID-19 health crisis, which has already produced dramatic reductions in VMT, we believe that you should pause this regulation for cities and counties until a more equitable solution can be achieved. Therefore, we respectfully request that you extend the implementation of the VMT regulation for two years. Thank you again for your leadership during these challenging times, and know that we stand ready to assist you in our mutual effort to address the housing crisis in California.

Sincerely,

Anna M. Caballero

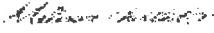
Anna Caballero
 Senator, 12th District

Frank Bigelow

Frank Bigelow
 Assemblymember, 5th District



Andreas Borgeas
Senator, 8th District



Melissa Hurtado
Senator, 14th District



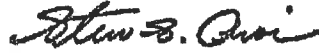
Richard Roth
Senator, 31st District



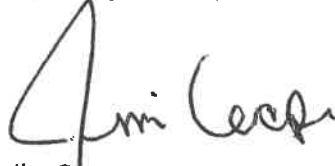
Susan Rubio
Senator, 22nd District



Scott Wilk
Senator, 21st District



Steven Choi
Assemblymember, 68th District



Jim Cooper
Assemblymember, 9th District



Tom Daly
Assemblymember, 69th District



James Gallagher
Assemblymember, 3rd District



Mike Gipson
Assemblymember, 64th District



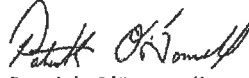
Adam Gray
Assemblymember, 21st District



Chad Mayes
Assemblymember, 42nd District



Jay Obernolte
Assemblymember, 33rd District



Patrick O'Donnell
Assemblymember, 70th District



Jim Patterson
Assemblymember, 23rd District



Sharon Quirk-Silva
Assemblymember, 65th District



Blanca Rubio
Assemblymember, 48th District



Rudy Salas
Assemblymember, 32nd District

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, REQUESTING A DELAY OF THE IMPLEMENTATION OF TITLE 14 OF THE CALIFORNIA CODE OF REGULATIONS, SECTION 15064.3, ESTABLISHING A THRESHOLD OF SIGNIFICANCE FOR VEHICLE MILES TRAVELED (VMT).

WHEREAS, on January 30, 2020, the World Health Organization (WHO) declared COVID-19 a Public Health Emergency of International Concern, and on January 31, 2020, the United States Secretary of Health and Human Services declared a Public Health Emergency; and

WHEREAS, On March 4, 2020, California Governor Gavin Newsom declared a State of Emergency to make additional resources available, formalize emergency actions already underway across multiple state agencies and departments, and help the state prepare for a broader spread of COVID-19; and

WHEREAS, the City of Ontario is experiencing a decline in revenues, unanticipated COVID-19 expenses have been incurred including expenses related to disinfection of public facilities, purchase of personal protective equipment, new technology for remote operations, increased public safety costs (including overtime), increased personnel costs (including overtime), purchasing supplies for residents and financial support to residents and businesses; and

WHEREAS, the City of Ontario is required by a new state law (Title 14 of the California Code of Regulations, section 15064.3) to adopt a threshold of significance for vehicle miles traveled (VMT), to be imposed on new housing development projects, among others; and

WHEREAS, the adoption of the VMT threshold is a very complex matter and requires the services of expensive expert traffic engineers, tremendous City staff time and resources and multiple public meetings to ensure public input on the adoption of the threshold; and

WHEREAS, the COVID-19 pandemic is resulting in dramatic changes in transportation patterns such as the expansion of working from home, learning from home, and even receiving medical care from home via remote communication technologies, as well as 90% decreases in public transit ridership based on social distancing and related health priorities, the requested extension will allow for the informed consideration of VMT and other potential changes to CEQA;

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the City Council of the City of Ontario, that the City of Ontario hereby requests that Governor Newsom extend the implementation date of Title 14 of the California Code of Regulations, section 15064.3 until July 1, 2021.

BE IT FURTHER RESOLVED that the action taken today be transmitted to Governor Newsom immediately.

The City Clerk of the City of Ontario shall certify as to the adoption of this Resolution.

PASSED, APPROVED, AND ADOPTED this 16th day of June 2020.

PAUL S. LEON, MAYOR

ATTEST:

SHEILA MAUTZ, CITY CLERK

APPROVED AS TO FORM:

COLE HUBER LLP
CITY ATTORNEY

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, SHEILA MAUTZ, City Clerk of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. 2020- was duly passed and adopted by the City Council of the City of Ontario at their regular meeting held June 16, 2020, by the following roll call vote, to wit:

AYES: COUNCIL MEMBERS:

NOES: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

SHEILA MAUTZ, CITY CLERK

(SEAL)

The foregoing is the original of Resolution No. 2020- duly passed and adopted by the Ontario City Council at their regular meeting held June 16, 2020.

SHEILA MAUTZ, CITY CLERK

(SEAL)

CITY OF ONTARIO

Agenda Report
June 16, 2020

SECTION:
CONSENT CALENDAR

SUBJECT: A RESOLUTION ADOPTING VEHICLE MILES TRAVELED THRESHOLDS FOR DETERMINING SIGNIFICANCE OF TRANSPORTATION IMPACTS THROUGH THE CALIFORNIA ENVIRONMENTAL QUALITY ACT IN CONFORMANCE WITH SB 743

RECOMMENDATION: That the City Council consider and adopt a resolution adopting Vehicle Miles Traveled (VMT) Thresholds for determining significance of transportation impacts through the California Environmental Quality Act (CEQA) in conformance with Senate Bill 743 (SB 743).


**COUNCIL GOALS: Operate in a Businesslike Manner
Pursue City's Goals and Objectives by Working with Other Governmental Agencies**

FISCAL IMPACT: None.

BACKGROUND: In 2013, SB 743 was adopted which directed the Office of Planning and Research (OPR) to examine alternative ways to analyze transportation impacts of projects during the CEQA process. OPR determined that using VMT is a more suitable metric than Level of Service (LOS) in determining transportation impacts of a project. By July 1, 2020, all cities in the State of California are required to adopt VMT thresholds for passenger vehicle and light duty trucks and determine the methodology to be used during this analysis. These decisions are at the sole discretion of each jurisdiction.

City staff participated in a collaborative study led by San Bernardino County Transportation Authority (SBCTA) which evaluated the tools, thresholds, and mitigation options appropriate for jurisdiction in this county. This study allowed for the unique characteristics of the region to be examined and provided broad guidance to the local jurisdictions within it. The VMT thresholds and methodology being proposed exempt projects that are not likely to generate significant VMT by applying the OPR and SBCTA collaborative recommendations, and it will rely on the current General Plan (The Ontario Plan-"TOP").

STAFF MEMBER PRESENTING: Scott Murphy, AICP, Executive Director Development Agency

Prepared by: Melanie Mullis
Department: Engineering
City Manager Approval: 

Submitted to Council/O.H.A. 06/16/2020
Approved: _____
Continued to: _____
Denied: _____

5

While LOS will no longer be used to assess the impacts of a project during the CEQA process, use of LOS to comply with Congestion Management Plan requirements, assess a project's consistency with TOP and determine project specific transportation improvements will continue.

On May 26, 2020, the Planning Commission considered the proposed VMT thresholds and voted 6-0 to issue a resolution recommending the City Council approve them.

ENVIRONMENTAL REVIEW: The project is exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to § 15061(b)(3) (General Rule) of the CEQA Guidelines based on the fact that there is no possibility that adoption of VMT Thresholds will result in a significant effect on the environment.



PLANNING COMMISSION STAFF REPORT

May 26, 2020

FILE NO.: SB 743 VMT Thresholds

SUBJECT: Adoption of Vehicle Miles Traveled Thresholds (VMT) for determining significance of transportation impacts through the California Environmental Quality Act in conformance with SB 743; **City Initiated. City Council action is required.**


RECOMMENDED ACTION: That the Planning Commission consider and recommend City Council adoption of Vehicle Miles Traveled (VMT) Thresholds for Implementing the California Environmental Quality Act (CEQA) pursuant to SB 743, pursuant to the facts and reasons contained in the staff report and attached resolution.

PROJECT ANALYSIS:

(1) Background - The California Environmental Quality Act (CEQA) is California's comprehensive environmental law. It requires public agencies to evaluate the environmental effects of a project before action is taken. CEQA also aims to prevent significant environmental effects from occurring as a result of agency actions by requiring agencies avoid or reduce, when feasible, the significant environmental impacts of their decisions.

With the adoption of SB 743 in 2013, Office of Planning and Research (OPR) was directed to examine alternative ways to analyze transportation impacts of projects during the CEQA process. OPR determined that use of Vehicle Miles Traveled (VMT) rather than Level of Service (LOS) is most suitable when analyzing the environmental impacts of a project since LOS assesses automobile delay and roadway efficiency rather than environmental impacts on transportation from projects. The use of VMT thresholds for passenger vehicle and light duty trucks was incorporated into revised state CEQA Guidelines that were adopted in December 2018. In addition, OPR adopted guidelines for use of VMT as the transportation metric to assist local governments when preparing their VMT thresholds. This VMT analysis does not apply to large trucks typically used for transporting of goods.

All cities in the State of California are required to adopt VMT thresholds for use during the CEQA process no later than July 1, 2020. The thresholds will determine what non-exempt projects may have a significant impact upon transportation. The decision on what the thresholds shall be is at the sole discretion of each jurisdiction. City staff participated in a collaborative study led by San Bernardino County Transportation Authority (SBCTA) which evaluated the tools, thresholds, and mitigation options appropriate for San

Case Planner:	Melanie Mullis, Principal Planner
Planning Director Approval:	
Submittal Date:	05/11/2020

Hearing Body	Date	Decision	Action
DAB			
PC	05/26/2020	Approval	Recommend
CC			Final

Bernardino County. This study allowed for the unique characteristics of our region to be examined and provide broad guidance to the local jurisdictions within it.

While LOS will no longer be used to assess the impacts of a project during the CEQA process, it does not preclude the City from using LOS analysis to comply with Congestion Management Plan requirements, assess a project's consistency with The Ontario Plan or to conduct project specific transportation analysis.

Local CEQA Guidelines

In July 2019, City Council adopted the current City CEQA Guidelines which includes provisions for using VMT thresholds. This proposed action would establish the thresholds that the City of Ontario will use during the review of projects for their environmental impacts to transportation.

These VMT Thresholds will become effective upon adoption. New projects or projects that have not circulated CEQA documents for public review before the effective date must comply with the City's new VMT Thresholds.

[2] Analysis — The proposed VMT Impact Thresholds contained in Exhibit A attached to the draft resolution identify the thresholds being proposed. The proposed VMT thresholds are based on the City's average VMT for Service Population (Residents and Employees) and includes trips both within the County and between the City and locations outside the County. The combination of the high number of jobs in Ontario, reasonable housing costs (compared to counties to the west), the presence of various regional uses such as Ontario International Airport, Ontario Mills, Toyota Arena and various large distribution facilities, and its proximity to Los Angeles, Riverside and Orange counties, make Ontario's VMT characteristics unique. Residents who work in Los Angeles or Orange counties can find more affordable housing in Ontario than locations to the west yet are within a commutable distance. Conversely, Ontario is within a reasonable commute distance from locations outside of Ontario for the more than 100,000 people who work in Ontario. This results in an average Service Population VMT higher than both the County and SCAG region because of these unique characteristics.

While Ontario's VMT for Service Population rate is higher than the County or SCAG region, The Ontario Plan land use and mobility plans concentrates balanced growth in strategic areas resulting in an overall Ontario and regional reduction in both VMT (0.2%) and GHG (0.2%). In addition, The Ontario Plan land uses have been incorporated into the County transportation model (SBTAM) which have been incorporated into SCAG's regional transportation model. As such, projects consistent with The Ontario Plan are consistent with the Regional Transportation Plan (RTP) and Sustainable Communities Strategy (SCS). Therefore, with the incorporation of the VMT reduction within The Ontario Plan land use analysis, staff is recommending that the City maintain TOP VMT reduction goal of 0.2%. Future projects consistent with TOP and in conformance with the VMT Impact Thresholds (Exhibit A) are determined to have less than significant impacts for transportation. Projects that exceed the TOP VMT Citywide average for Service

Population under General Plan Buildout conditions and/or RTP/SCS are determined to be significant and require additional CEQA analysis.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

[1] City Council Goals.

- Operate in a Businesslike Manner
- Pursue City's Goals and Objectives by Working with Other Governmental Agencies

[2] Policy Plan (General Plan)

Mobility Element:

- Goal M1: A system of roadways that meets the mobility needs of a dynamic and prosperous Ontario.
 - M1-2 Mitigation of Impacts. We require development to mitigate its traffic impacts.

HOUSING ELEMENT COMPLIANCE: The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The project site is located within the Airport Influence Area of the Ontario International Airport and has been found to be consistent with the policies and criteria set forth within the Ontario International Airport Land Use Compatibility Plan.

ENVIRONMENTAL REVIEW: The project is exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to § 15061(b)(3) (General Rule) of the CEQA Guidelines based on the fact that there is no possibility that adoption of VMT Thresholds will result in a significant effect on the environment.

RESOLUTION NO. PC20-021

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING CITY COUNCIL ADOPT “VEHICLES MILES TRAVELED” THRESHOLDS OF SIGNIFICANCE FOR PURPOSES OF ANALYZING TRANSPORTATION IMPACTS UNDER THE CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA).

WHEREAS, the California Environmental Quality Act Guidelines (“CEQA Guidelines”) encourage public agencies to develop and publish generally applicable “thresholds of significance” to be used in determining the significance of a project’s environmental effects; and

WHEREAS, CEQA Guidelines section 15064.7(a) defines a threshold of significance as “an identifiable quantitative, qualitative or performance level of a particular environmental effect, noncompliance with which means the effect will normally be determined to be significant by the agency and compliance with which means the effect normally will be determined to be less than significant”; and

WHEREAS, CEQA Guidelines section 15064.7(b) requires that thresholds of significance must be adopted by ordinance, resolution, rule, or regulations, developed through a public review process, and be supported by substantial evidence; and

WHEREAS, pursuant to CEQA Guidelines section 15064.7(c), when adopting thresholds of significance, a public agency may consider thresholds of significance adopted or recommended by other public agencies provided that the decision of the agency is supported by substantial evidence; and

WHEREAS, Senate Bill 743, enacted in 2013 and codified in Public Resources Code section 21099, required changes to the CEQA Guidelines regarding the criteria for determining the significance of transportation impacts of projects; and

WHEREAS, in 2018, the Governor’s Office of Planning and Research (“OPR”) proposed, and the California Natural Resources Agency certified and adopted, new CEQA Guidelines section 15064.3 that identifies vehicle miles traveled (“VMT”) – meaning the amount and distance of automobile travel attributable to a project – as the most appropriate metric to evaluate a project’s transportation impacts; and

WHEREAS, as a result, automobile delay, as measured by “level of service” (“LOS”) and other similar metrics, will generally no longer constitute a significant environmental effect under CEQA; and

WHEREAS, CEQA Guidelines section 15064.3 requires agencies to stop treating automobile delay/LOS as an environmental impact effective on July 1, 2020, though public agencies may elect to be governed by this section immediately; and

WHEREAS, the City of Ontario, following a public review process consisting of staff presentations before the Planning Commission, wishes to adopt the VMT thresholds of significance for determining the significance of transportation impacts that are recommended in an analysis conducted by the San Bernardino County Transportation Authority on behalf of its member jurisdictions; and

WHEREAS, the project is exempt from the requirements of the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA") pursuant to § 15061(b)(3) (General Rule) of the CEQA Guidelines based on the fact that there is no possibility that adoption of VMT Thresholds will result in a significant effect on the environment; and

WHEREAS, on May 26, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1. *Environmental Determination and Findings.* As the recommending body for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

a. The Project is exempt from environmental review pursuant to Section 15601(b)(3) (General Rule) of the CEQA Guidelines; and

b. The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

c. The determination of CEQA exemption reflects the independent judgment of the Planning Commission.

SECTION 2. *Concluding Facts and Reasons.* Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing,

and upon the specific findings set forth in Section 1, above, the Planning Commission hereby concludes as follows:

a. Adoption of the Vehicle Miles Traveled (VMT) Thresholds is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan; and

b. The Vehicle Miles Traveled (VMT) Thresholds would not be detrimental to the public interest, health, safety, convenience, or general welfare of the City.

SECTION 3. *Planning Commission Action.* Based upon the findings and conclusions set forth in Sections 1 through 2, above, the Planning Commission hereby RECOMMENDS THE CITY COUNCIL APPROVE

1. Utilizing the San Bernardino County Travel Demand Model (SBTAM) as its preferred methodology to measure VMT.
2. Utilizing the San Bernardino County Travel Demand Model (SBTAM) as its preferred method to analyze a project's VMT impact.
3. Utilizing a threshold consistent with Table 1 in Exhibit A

SECTION 4: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 5: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 6: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

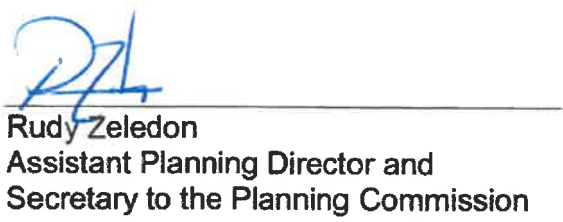
The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 26th day of May, 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.



Jim Willoughby
Planning Commission Chairman

ATTEST:



Rudy Zeledon
Assistant Planning Director and
Secretary to the Planning Commission

Planning Commission Resolution
File No. SB 743 VMT Thresholds
May 26, 2020
Page 5

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. PC20-021, was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on May 26, 2020, by the following roll call vote, to wit:

AYES: DeDiemar, Gage, Gregorek, Reyes, Ricci, Willoughby

NOES: None

ABSENT: Downs

ABSTAIN: None



Gwen Berendsen
Secretary Pro Tempore

Exhibit A

VMT Impact Thresholds		
Methods	Project Threshold	Cumulative Threshold
Land Use Plans (such as General Plans and Specific Plans)		
<ul style="list-style-type: none"> • San Bernardino Traffic Analysis Model (SBTAM) forecast of total daily VMT/SP. To capture project effect, the same cumulative year population and employment growth totals should be used. The 'project' only influences land use allocation. 	<p>A significant impact would occur if the project VMT/SP (for the land use plan) exceeds the Citywide average for Service Population under General Plan Buildout Conditions.</p>	<p>A significant impact would occur if the project caused total daily VMT within the City to be higher than the no project alternative under cumulative conditions.</p>
<ul style="list-style-type: none"> • Consistency check with SCAG RTP/SCS. Is the proposed project within the growth projections in the RTP/SCS? 	<p>NA</p>	<p>A significant impact would occur if the project is determined to be inconsistent with the RTP/SCS.</p>
Land Use Projects		
<ul style="list-style-type: none"> • Transit Priority Area (TPA) screening. 	<p>Presumed less than significant VMT impact for projects located in TPAs if the project:</p> <ul style="list-style-type: none"> ○ Has a Floor Area Ratio 0.75 or higher; ○ Includes no more parking for residents, customers or employees than the City Development Code mandates; ○ Is consistent with the Sustainable Communities Strategy (SCS) as determined by the City with input from SCAG or SBCTA; and ○ If there are existing affordable residential units, replaces those residential units with at least as many similarly affordable units and not with a smaller number of moderate or high-income residential units. 	<p>Project presumption applies under cumulative conditions as long as project is consistent with SCAG RTP/SCS.</p>
<ul style="list-style-type: none"> • Low VMT area screening. 	<p>Presumed less than significant VMT impact for projects located in low VMT generating model traffic analysis zones (TAZs). These TAZs generate total daily VMT/SP that is 15% less than the baseline level for the County.</p>	<p>Project presumption applies under cumulative conditions as long as project is consistent with SCAG RTP/SCS.</p>
<ul style="list-style-type: none"> • Low Trip Generating Uses 	<p>Projects below 110 Average Daily Trips (ADT) are presumed to be less than significant, such as:</p> <ul style="list-style-type: none"> ○ 11 single family housing units; ○ 16 multi-family, condominiums or townhouse housing units; ○ 10,000 sq. ft. of office; ○ 15,000 sq. ft. of light industrial; 	<p>Project presumption applies under cumulative conditions as long as project is consistent with SCAG RTP/SCS.</p>

VMT Impact Thresholds		
Methods	Project Threshold	Cumulative Threshold
	<ul style="list-style-type: none"> o 63,000 sq. ft. of warehousing; and 79,000 sq. ft. of high cube transload and short-term storage warehouse. 	
<ul style="list-style-type: none"> • Project type screening by land use type. 	<p>Projects that meet the criteria describe below can be screened from further VMT review during the CEQA process and are presumed to have less than significant impact upon transportation (determination to be made by City staff).</p> <ol style="list-style-type: none"> 1. Residential, office, retail or mix of these uses within ½ mile of an existing major transit stop or a stop along a high-quality transit corridor as long as the use: <ol style="list-style-type: none"> a. Has at least 0.75 FAR; b. Provides no more parking than the Development Code requires; and c. Is consistent with the RTP/SCS and The Ontario Plan. 2. Local serving retail: <ol style="list-style-type: none"> a. Not greater than 50,000 sq. ft. in size b. Projects with a Neighborhood Commercial TOP Land Use designation since this designation is intended to be local serving (Neighborhood Commercial is defined as “Local serving retail, personal service, office, and dining uses, typically located within a predominantly residential neighborhood”). 3. Certain Transportation projects that do not add vehicle capacity such as: <ol style="list-style-type: none"> a. Roadway maintenance; b. Right or left turn lanes (no through traffic); c. Addition of transit only lanes; d. Traffic signal timing to optimize vehicle, bicycle or pedestrian flow; e. Installation of roundabouts or traffic circles; f. Installation of traffic calming devices; and g. Installation of bicycle or pedestrian facilities and other active transportation projects. 4. Local Serving K – 12 Public Schools (not magnet or charter schools); 5. Local/Neighborhood Parks; 6. Daycare / Childcare/Pre-Kindergarten; 7. Affordable or Supportive Housing; 8. Student Housing on or adjacent to college campuses; 9. Community Institutions (e.g. Public Library, Fire Station, local government facilities); 10. Senior Housing (as defined by HUD) or Assisted Living facilities; HUD defines Senior Housing as projects that are: 	<p>Project presumption applies under cumulative conditions as long as project is consistent with SCAG RTP/SCS.</p>

<u>VMT Impact Thresholds</u>		
<u>Methods</u>	<u>Project Threshold</u>	<u>Cumulative Threshold</u>
	a. Provided under any state or federal program that the Secretary of HUD has determined to be specifically designed and operated to assist elderly persons (as defined in the state or federal program); b. Intended for, and solely occupied by persons 62 years of age or older; or c. Intended and operated for occupancy by persons 55 years of age or older. (most commonly used) 11. Redevelopment of a site to residential or office that would generate fewer VMT than the existing use; and 12. Non-Destination Small Hotels (with 150 or fewer rooms and no Banquet Facilities). This is appropriate given locating small hotels in Ontario will serve the airport and local businesses and reduce the VMT of these customers from having to travel further from the airport or their business destination.	
<ul style="list-style-type: none"> VMT analysis using SBTAM forecast of total daily VMT/SP. 	A significant impact would occur if the project VMT/SP exceeds the Citywide average for Service Population under General Plan Buildout Conditions.	A significant impact would occur if the project is determined to be inconsistent with the RTP/SCS. A significant impact would occur if the project causes total daily VMT within the City to be higher than the no project alternative under cumulative conditions. This analysis should be performed using the 'project effect' or 'boundary' method.
<u>Transportation Projects (thresholds may apply for SB 743 or GHG purposes)</u>		
<ul style="list-style-type: none"> SBTAM forecast of total citywide daily VMT¹ 	A significant impact would occur if the project increased the baseline VMT within the City for Service Population.	A significant impact would occur if the project caused total daily VMT within the City to be higher than the no build alternative under cumulative conditions.
<ul style="list-style-type: none"> Consistency check with SCAG RTP/SCS 	NA	A significant impact would occur if the project is determined to be inconsistent with the RTP/SCS.

¹ It is recommended that SBTAM is used to develop VMT estimates for transportation project impact assessment. However, the analyst must verify the model results for sensitivity to changes in VMT. Alternatively, if the model is not deemed appropriate, Robert Cevero's research on lane-mile elasticity and its relationship to VMT can be referenced.

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, ADOPTING VEHICLES MILES TRAVELED THRESHOLDS OF SIGNIFICANCE FOR PURPOSES OF ANALYZING TRANSPORTATION IMPACTS UNDER THE CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA).

WHEREAS, the California Environmental Quality Act Guidelines (“CEQA Guidelines”) encourage public agencies to develop and publish generally applicable “thresholds of significance” to be used in determining the significance of a project’s environmental effects; and

WHEREAS, CEQA Guidelines section 15064.7(a) defines a threshold of significance as, “an identifiable quantitative, qualitative or performance level of a particular environmental effect, noncompliance with which means the effect will normally be determined to be significant by the agency and compliance with which means the effect normally will be determined to be less than significant”; and

WHEREAS, CEQA Guidelines section 15064.7(b) requires that thresholds of significance must be adopted by ordinance, resolution, rule, or regulations, developed through a public review process and be supported by substantial evidence; and

WHEREAS, pursuant to CEQA Guidelines section 15064.7(c), when adopting thresholds of significance, a public agency may consider thresholds of significance adopted or recommended by other public agencies provided that the decision of the agency is supported by substantial evidence; and

WHEREAS, Senate Bill 743, enacted in 2013 and codified in Public Resources Code section 21099, required changes to the CEQA Guidelines regarding the criteria for determining the significance of transportation impacts of projects; and

WHEREAS, in 2018, the Governor’s Office of Planning and Research (“OPR”) proposed, and the California Natural Resources Agency certified and adopted, new CEQA Guidelines Section 15064.3 that identifies vehicle miles traveled (“VMT”) - meaning the amount and distance of automobile travel attributable to a project – as the most appropriate metric to evaluate a project’s transportation impacts; and

WHEREAS, as a result, automobile delay, as measured by “level of service” (“LOS”) and other similar metrics, will generally no longer constitute a significant environmental effect under CEQA; and

WHEREAS, CEQA Guidelines Section 15064.3 requires agencies to stop treating automobile delay/LOS as an environmental impact effective on July 1, 2020, though public agencies may elect to be governed by this section immediately; and

WHEREAS, the City of Ontario, following a public review process consisting of staff presentations before the Planning Commission, wishes to adopt the VMT thresholds of significance for determining the significance of transportation impacts that are recommended in an analysis conducted by the San Bernardino County Transportation Authority on behalf of its member jurisdictions; and

WHEREAS, the project is exempt from the requirements of the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA") pursuant to § 15061(b)(3) (General Rule) of the CEQA Guidelines based on the fact that there is no possibility that adoption of VMT Thresholds will result in a significant effect on the environment; and

WHEREAS, on May 26, 2020, the Planning Commission of the City of Ontario considered the proposed VMT Thresholds and recommended the City Council adopt the VMT Thresholds; and

WHEREAS, on June 16, 2020, the City Council of the City of Ontario considered the proposed VMT Thresholds; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the City Council of the City of Ontario ("City"), as follows:

SECTION 1. Environmental Determination and Findings. As the decision-making body for the Project, the City Council has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the City Council, the City Council finds as follows:

a. The Project is exempt from environmental review pursuant to Section 15601(b)(3) (General Rule) of the CEQA Guidelines; and

b. The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

c. The determination of CEQA exemption reflects the independent judgment of the City Council.

SECTION 2. Concluding Facts and Reasons. Based upon the substantial evidence presented to the City Council during the above-referenced hearing, and upon the specific findings set forth in Section 1, above, the City Council hereby concludes as follows:

a. Adoption of the Vehicle Miles Traveled (VMT) Thresholds is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan; and

b. The Vehicle Miles Traveled (VMT) Thresholds would not be detrimental to the public interest, health, safety, convenience, or general welfare of the City.

SECTION 3. *City Council Action.* Based upon the findings and conclusions set forth in Sections 1 through 2, above, the City Council hereby ADOPTS:

1. Utilizing the San Bernardino County Travel Demand Model (SBTAM) as its preferred methodology to measure VMT.
2. Utilizing the San Bernardino County Travel Demand Model (SBTAM) as its preferred method to analyze a project's VMT impact.
3. Utilizing a threshold consistent with Table 1 in Exhibit A

SECTION 4. *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 5. *Certification to Adoption.* The City Clerk shall certify to the adoption of the Resolution.

PASSED, APPROVED, AND ADOPTED this 16th day of June 2020.

PAUL S. LEON, MAYOR

ATTEST:

SHEILA MAUTZ, CITY CLERK

APPROVED AS TO FORM:

COLE HUBER, LLP
CITY ATTORNEY

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, SHEILA MAUTZ, City Clerk of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. 2020- was duly passed and adopted by the City Council of the City of Ontario at their regular meeting held June 16, 2020, by the following roll call vote, to wit:

AYES: COUNCIL MEMBERS:

NOES: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

SHEILA MAUTZ, CITY CLERK

(SEAL)

The foregoing is the original of Resolution No. 2020- duly passed and adopted by the Ontario City Council at their regular meeting held June 16, 2020.

SHEILA MAUTZ, CITY CLERK

(SEAL)

Exhibit A

<u>VMT Impact Thresholds</u>		
<u>Methods</u>	<u>Project Threshold</u>	<u>Cumulative Threshold</u>
Land Use Plans (such as General Plans and Specific Plans)		
<ul style="list-style-type: none"> • San Bernardino Traffic Analysis Model (SBTAM) forecast of total daily VMT/SP. To capture project effect, the same cumulative year population and employment growth totals should be used. The 'project' only influences land use allocation. 	<p>A significant impact would occur if the project VMT/SP (for the land use plan) exceeds the Citywide average for Service Population under General Plan Buildout Conditions.</p>	<p>A significant impact would occur if the project caused total daily VMT within the City to be higher than the no project alternative under cumulative conditions.</p>
<ul style="list-style-type: none"> • Consistency check with SCAG RTP/SCS. Is the proposed project within the growth projections in the RTP/SCS? 	<p>NA</p>	<p>A significant impact would occur if the project is determined to be inconsistent with the RTP/SCS.</p>
Land Use Projects		
<ul style="list-style-type: none"> • Transit Priority Area (TPA) screening. 	<p>Presumed less than significant VMT impact for projects located in TPAs if the project:</p> <ul style="list-style-type: none"> ○ Has a Floor Area Ratio 0.75 or higher; ○ Includes no more parking for residents, customers or employees than the City Development Code mandates; 	<p>Project presumption applies under cumulative conditions as long as project is consistent with SCAG RTP/SCS.</p>

<u>VMT Impact Thresholds</u>		
<u>Methods</u>	<u>Project Threshold</u>	<u>Cumulative Threshold</u>
	<ul style="list-style-type: none"> ○ Is consistent with the Sustainable Communities Strategy (SCS) as determined by the City with input from SCAG or SBCTA; and ○ If there are existing affordable residential units, replaces those residential units with at least as many similarly affordable units and not with a smaller number of moderate or high-income residential units. 	
<ul style="list-style-type: none"> • Low VMT area screening. 	Presumed less than significant VMT impact for projects located in low VMT generating model traffic analysis zones (TAZs). These TAZs generate total daily VMT/SP that is 15% less than the baseline level for the County.	Project presumption applies under cumulative conditions as long as project is consistent with SCAG RTP/SCS.
<ul style="list-style-type: none"> • Low Trip Generating Uses 	<p>Projects below 110 Average Daily Trips (ADT) are presumed to be less than significant, such as:</p> <ul style="list-style-type: none"> ○ 11 single family housing units; ○ 16 multi-family, condominiums or townhouse housing units; ○ 10,000 sq. ft. of office; ○ 15,000 sq. ft. of light industrial; ○ 63,000 sq. ft. of warehousing; and ○ 79,000 sq. ft. of high cube transload and short-term storage warehouse. 	Project presumption applies under cumulative conditions as long as project is consistent with SCAG RTP/SCS.
<ul style="list-style-type: none"> • Project type screening by land use type. 	<p>Projects that meet the criteria describe below can be screened from further VMT review during the CEQA process and can be determined to not have a significant impact upon transportation (determination to be made by City staff).</p> <ol style="list-style-type: none"> 1. Residential, office, retail or mix of these uses within ½ mile of an existing major transit stop or a stop along a high-quality transit corridor as long as the use: 	Project presumption applies under cumulative conditions as long as project is consistent with SCAG RTP/SCS.

VMT Impact Thresholds		
<u>Methods</u>	<u>Project Threshold</u>	<u>Cumulative Threshold</u>
	<ul style="list-style-type: none"> a. Has at least 0.75 FAR; b. Provides no more parking than the Development Code requires; and c. Is consistent with the RTP/SCS and The Ontario Plan. <ul style="list-style-type: none"> 2. Local serving retail (not greater than 50,000 sq. ft. in size) or projects with a Neighborhood Commercial TOP Land Use designation since this designation is intended to be local serving (Neighborhood Commercial is defined as “Local serving retail, personal service, office, and dining uses, typically located within a predominantly residential neighborhood”). <ul style="list-style-type: none"> 3. Certain Transportation projects that do not add vehicle capacity such as: <ul style="list-style-type: none"> a. Roadway maintenance; b. Right or left turn lanes (no through traffic); c. Addition of transit only lanes; d. Traffic signal timing to optimize vehicle, bicycle or pedestrian flow; e. Installation of roundabouts or traffic circles; f. Installation of traffic calming devices; and g. Installation of bicycle or pedestrian facilities and other active transportation projects. <ul style="list-style-type: none"> 4. Local Serving K – 12 Public Schools (not magnet or charter schools); 5. Local/Neighborhood Parks; 6. Daycare / Childcare/Pre-Kindergarten; 	

<u>VMT Impact Thresholds</u>		
<u>Methods</u>	<u>Project Threshold</u>	<u>Cumulative Threshold</u>
	<p>7. Affordable or Supportive Housing;</p> <p>8. Student Housing on or adjacent to college campuses;</p> <p>9. Community Institutions (e.g. Public Library, Fire Station, local government facilities);</p> <p>10. Senior Housing (as defined by HUD) or Assisted Living facilities; HUD defines Senior Housing as projects that are:</p> <ul style="list-style-type: none"> a. Provided under any state or federal program that the Secretary of HUD has determined to be specifically designed and operated to assist elderly persons (as defined in the state or federal program); b. Intended for, and solely occupied by persons 62 years of age or older; or c. Intended and operated for occupancy by persons 55 years of age or older. (most commonly used) <p>11. Redevelopment of a site to residential or office that would generate fewer VMT than the existing use; and</p> <p>12. Non-Destination Small Hotels (with 150 or fewer rooms and no Banquet Facilities). This is appropriate given locating small hotels in Ontario will serve the airport and local businesses and reduce the VMT of these customers from having to travel further from the airport or their business destination.</p>	
<ul style="list-style-type: none"> • VMT analysis using SBTAM forecast of total daily VMT/SP. 	<p>A significant impact would occur if the project VMT/SP exceeds the Citywide average for Service Population under General Plan Buildout Conditions.</p>	<p>A significant impact would occur if the project is determined to</p>

VMT Impact Thresholds		
<u>Methods</u>	<u>Project Threshold</u>	<u>Cumulative Threshold</u>
		<p>be inconsistent with the RTP/SCS.</p> <p>A significant impact would occur if the project causes total daily VMT within the City to be higher than the no project alternative under cumulative conditions. This analysis should be performed using the 'project effect' or 'boundary' method.</p>
Transportation Projects (thresholds may apply for SB 743 or GHG purposes)		
<ul style="list-style-type: none"> • SBTAM forecast of total citywide daily VMT¹ 	A significant impact would occur if the project increased the baseline VMT within the City for Service Population.	A significant impact would occur if the project caused total daily VMT within the City to be higher than the no build alternative under cumulative conditions.
<ul style="list-style-type: none"> • Consistency check with SCAG RTP/SCS 	NA	A significant impact would occur if the project is determined to be inconsistent with the RTP/SCS.

¹ It is recommended that SBTAM is used to develop VMT estimates for transportation project impact assessment. However, the analyst must verify the model results for sensitivity to changes in VMT. Alternatively, if the model is not deemed appropriate, Robert Cervero's research on lane-mile elasticity and its relationship to VMT can be referenced.

Exhibit A

<u>VMT Impact Thresholds</u>		
<u>Methods</u>	<u>Project Threshold</u>	<u>Cumulative Threshold</u>
<u>Land Use Plans (such as General Plans and Specific Plans)</u>		
<ul style="list-style-type: none"> • San Bernardino Traffic Analysis Model (SBTAM) forecast of total daily VMT/SP. To capture project effect, the same cumulative year population and employment growth totals should be used. The 'project' only influences land use allocation. 	<p>A significant impact would occur if the project VMT/SP (for the land use plan) exceeds the Citywide average for Service Population under General Plan Buildout Conditions.</p>	<p>A significant impact would occur if the project caused total daily VMT within the City to be higher than the no project alternative under cumulative conditions.</p>
<ul style="list-style-type: none"> • Consistency check with SCAG RTP/SCS. Is the proposed project within the growth projections in the RTP/SCS? 	<p>NA</p>	<p>A significant impact would occur if the project is determined to be inconsistent with the RTP/SCS.</p>
<u>Land Use Projects</u>		
<ul style="list-style-type: none"> • Transit Priority Area (TPA) screening. 	<p>Presumed less than significant VMT impact for projects located in TPAs if the project:</p> <ul style="list-style-type: none"> ○ Has a Floor Area Ratio 0.75 or higher; ○ Includes no more parking for residents, customers or employees than the City Development Code mandates; ○ Is consistent with the Sustainable Communities Strategy (SCS) as determined by the City with input from SCAG or SBCTA; and ○ If there are existing affordable residential units, replaces those residential units with at least as many similarly affordable units and not with a smaller number of moderate or high-income residential units. 	<p>Project presumption applies under cumulative conditions as long as project is consistent with SCAG RTP/SCS.</p>
<ul style="list-style-type: none"> • Low VMT area screening. 	<p>Presumed less than significant VMT impact for projects located in low VMT generating model traffic analysis zones (TAZs). These TAZs generate total daily VMT/SP that is 15% less than the baseline level for the County.</p>	<p>Project presumption applies under cumulative conditions as long as project is consistent with SCAG RTP/SCS.</p>
<ul style="list-style-type: none"> • Low Trip Generating Uses 	<p>Projects below 110 Average Daily Trips (ADT) are presumed to be less than significant, such as:</p> <ul style="list-style-type: none"> ○ 11 single family housing units; ○ 16 multi-family, condominiums or townhouse housing units; ○ 10,000 sq. ft. of office; ○ 15,000 sq. ft. of light industrial; ○ 63,000 sq. ft. of warehousing; and 79,000 sq. ft. of high cube transload and short-term storage warehouse. 	<p>Project presumption applies under cumulative conditions as long as project is consistent with SCAG RTP/SCS.</p>

VMT Impact Thresholds

<u>Methods</u>	<u>Project Threshold</u>	<u>Cumulative Threshold</u>
<ul style="list-style-type: none"> • Project type screening by land use type. 	<p>Projects that meet the criteria describe below can be screened from further VMT review during the CEQA process and can be determined to not have a significant impact upon transportation (determination to be made by City staff).</p> <ol style="list-style-type: none"> 1. Residential, office, retail or mix of these uses within ½ mile of an existing major transit stop or a stop along a high-quality transit corridor as long as the use: <ol style="list-style-type: none"> a. Has at least 0.75 FAR; b. Provides no more parking than the Development Code requires; and c. Is consistent with the RTP/SCS and The Ontario Plan. 2. Local serving retail (not greater than 50,000 sq. ft. in size) or projects with a Neighborhood Commercial TOP Land Use designation since this designation is intended to be local serving (Neighborhood Commercial is defined as “Local serving retail, personal service, office, and dining uses, typically located within a predominantly residential neighborhood”). 3. Certain Transportation projects that do not add vehicle capacity such as: <ol style="list-style-type: none"> a. Roadway maintenance; b. Right or left turn lanes (no through traffic); c. Addition of transit only lanes; d. Traffic signal timing to optimize vehicle, bicycle or pedestrian flow; e. Installation of roundabouts or traffic circles; f. Installation of traffic calming devices; and g. Installation of bicycle or pedestrian facilities and other active transportation projects. 4. Local Serving K – 12 Public Schools (not magnet or charter schools); 5. Local/Neighborhood Parks; 6. Daycare / Childcare/Pre-Kindergarten; 7. Affordable or Supportive Housing; 8. Student Housing on or adjacent to college campuses; 9. Community Institutions (e.g. Public Library, Fire Station, local government facilities); 10. Senior Housing (as defined by HUD) or Assisted Living facilities; HUD defines Senior Housing as projects that are: <ol style="list-style-type: none"> a. Provided under any state or federal program that the Secretary of HUD has determined to be specifically designed and operated to assist elderly persons (as defined in the state or federal program); b. Intended for, and solely occupied by persons 62 years of age or older; or 	<p>Project presumption applies under cumulative conditions as long as project is consistent with SCAG RTP/SCS.</p>

<u>VMT Impact Thresholds</u>		
<u>Methods</u>	<u>Project Threshold</u>	<u>Cumulative Threshold</u>
	<ul style="list-style-type: none"> c. Intended and operated for occupancy by persons 55 years of age or older. (most commonly used) 11. Redevelopment of a site to residential or office that would generate fewer VMT than the existing use; and 12. Non-Destination Small Hotels (with 150 or fewer rooms and no Banquet Facilities). This is appropriate given locating small hotels in Ontario will serve the airport and local businesses and reduce the VMT of these customers from having to travel further from the airport or their business destination. 	
<ul style="list-style-type: none"> • VMT analysis using SBTAM forecast of total daily VMT/SP. 	A significant impact would occur if the project VMT/SP exceeds the Citywide average for Service Population under General Plan Buildout Conditions.	<p>A significant impact would occur if the project is determined to be inconsistent with the RTP/SCS.</p> <p>A significant impact would occur if the project causes total daily VMT within the City to be higher than the no project alternative under cumulative conditions. This analysis should be performed using the 'project effect' or 'boundary' method.</p>
<u>Transportation Projects (thresholds may apply for SB 743 or GHG purposes)</u>		
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<ul style="list-style-type: none"> • Consistency check with SCAG RTP/SCS 	NA	A significant impact would occur if the project is determined to be inconsistent with the RTP/SCS.

¹ It is recommended that SBTAM is used to develop VMT estimates for transportation project impact assessment. However, the analyst must verify the model results for sensitivity to changes in VMT. Alternatively, if the model is not deemed appropriate, Robert Cevero's research on lane-mile elasticity and its relationship to VMT can be referenced.

CITY OF ONTARIO

Agenda Report
June 16, 2020

SECTION:
CONSENT CALENDAR

SUBJECT: AN AMENDMENT TO THE COOPERATIVE AGREEMENT FOR THE SR-60 AT ARCHIBALD AVENUE INTERCHANGE PROJECT

RECOMMENDATION: That the City Council approve Amendment No. 4 in the amount of \$610,000 to Cooperative Agreement R14138 (15-1001150) (on file in the Records Management Department) with San Bernardino County Transportation Authority (SBCTA) for funding the State Route 60 (SR-60) at Archibald Avenue Interchange Project (Project); and authorize the City Manager to execute said agreement and future amendments within the authorization limits.

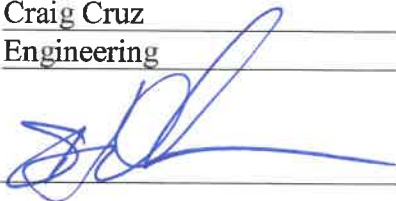
COUNCIL GOALS: Pursue City's Goals and Objectives by Working with Other Governmental Agencies

Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)

Ensure the Development of a Well Planned, Balanced, and Self-Sustaining Community in Ontario Ranch

FISCAL IMPACT: This Amendment No. 4 with SBCTA increases the City's share of the Project by \$610,000. The scope of improvements included in this amendment are City betterments that the City is fully responsible for. If approved, appropriations in the amount of \$190,000 from the Road Maintenance & Rehab Account (RMRA) Fund and \$115,000 from the Ontario Municipal Utility Company (OMUC) Water Capital Fund will be included in the Fiscal Year 2020-21 Proposed Budget to the City Council to fund the City's increased share of the Project. The RMRA Fund will pay for construction management services associated with the betterment work for the replacement of asphalt concrete to full depth concrete pavement. The OMUC Water Capital Fund will finance the installation of a recycled water lateral located on the west side of Archibald Avenue, north of the interchange. The remaining \$305,000 will have no direct fiscal impact on City funds as it will be comprised of fees collected from the development on the northwest corner of Archibald Avenue and the SR-60 westbound on-ramp. These fees will finance the installation of water, sewer, and gas laterals to service the development at this location.

STAFF MEMBER PRESENTING: Scott Murphy, AICP, Executive Director Development Agency

Prepared by: Craig Cruz
Department: Engineering
City Manager Approval: 

Submitted to Council/O.H.A. 06/16/2020
Approved: _____
Continued to: _____
Denied: _____

6

BACKGROUND: The Project will widen the existing Archibald Avenue bridge undercrossing at SR-60 to accommodate additional left-turn pockets and will widen the existing freeway on and off ramps. Anticipated traffic generated by development in Ontario Ranch will create impacts at the SR-60 at Archibald Avenue interchange that will be mitigated by this Project.

In August 2014, the City entered into a Cooperative Agreement with SBCTA for the Project and committed funds for the environmental and design phases at that time. Project funding was subsequently added in the FY 2017-18 Adopted Budget for the right-of-way phase. In February 2019, funds were again added for the construction phase. In September 2019, SBCTA received bids for the Project which exceeded the value of the current Cooperative Agreement, thus requiring Amendment No. 3 in November 2019.

Amendment No. 4 adds construction management services for full depth concrete pavement on Archibald Avenue and various service laterals needed on the northwest corner of Archibald Avenue and the SR-60 westbound on-ramp.

CITY OF ONTARIO

Agenda Report
June 16, 2020

SECTION:
CONSENT CALENDAR

SUBJECT: A COOPERATIVE AGREEMENT FOR THE DESIGN, RIGHT OF WAY AND CONSTRUCTION OF PHASE 1 OF THE WEST VALLEY CONNECTOR BUS RAPID TRANSIT PROJECT

RECOMMENDATION: That the City Council approve a Cooperative Agreement (20-1002422) (on file in the Records Management Department) with San Bernardino County Transportation Authority (SBCTA) for the design, right of way and construction of Phase 1 of the West Valley Connector Bus Rapid Transit Project; and authorize the City Manager to execute said agreement, future amendments and related documents.

COUNCIL GOALS: Pursue City's Goals and Objectives by Working with Other Governmental Agencies

Focus Resources in Ontario's Commercial and Residential Neighborhoods

Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)

FISCAL IMPACT: None. The project is funded through the Los Angeles County Metropolitan Transportation, Omnitrans, SBCTA, State and Federal sources.

BACKGROUND: The West Valley Connector Project (WVC) is the second out of ten planned bus rapid transit (BRT) corridors identified by Omnitrans in their 2010 System-Wide Transit Corridor Plan for the San Bernardino Valley. WVC is a 35 mile long BRT project that travels from Pomona to Fontana, through the cities of Montclair, Ontario and Rancho Cucamonga. It will decrease travel times and improve the existing public transit system within the West Valley of San Bernardino County. The Project is proposed to be constructed in two phases:

Phase I: Pomona Regional Transit Center to Victoria Gardens in Rancho Cucamonga via Milliken Avenue, and

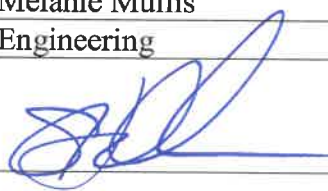
Phase II: Ontario International Airport to Kaiser Permanente Medical Center in Fontana via Haven Avenue and Foothill Boulevard.

STAFF MEMBER PRESENTING: Scott Murphy, AICP, Executive Director Development Agency

Prepared by: Melanie Mullis

Department: Engineering

City Manager

Approval: 

Submitted to Council/O.H.A. 06/16/2020

Approved: _____

Continued to: _____

Denied: _____

7

Phase I is scheduled to start operation in early 2024. Construction of Phase II/Haven Alignment is scheduled to occur after the completion of Phase I, when funding is available.

The joint Environmental Impact Report and Environmental Assessment (EIR/EA) were approved by SBCTA and the Federal Transit Administration (FTA) in May 2020. These environmental documents analyzed the impacts of two build alternatives including:

- Alternative A Rapid line with no dedicated bus-only lanes, would include the full 35 mile long BRT corridor (Phase I and II) with 60 side-running station platforms at 33 station locations/major intersections; and

- Alternative B Full BRT is similar to Alternative A, with the exception that a 3.5 mile long dedicated bus-only lane would be constructed between Benson Avenue and Vine Avenue and between Euclid Avenue and Vineyard Avenue in Ontario and include 5 center-running station platforms and 50 side-running station platforms at 33 station locations/major intersections.

With the environmental clearances complete, WVC can begin final design, right-of-way acquisition and advanced utility relocation. Once operational, WVC would generally operate 6:00 a.m. to 8:00 p.m. on weekdays with service frequencies of 10 minutes during peak hours and 15 minutes during off-peak hours. Stations would be “rapid bus” style stations designed for fast boarding.

The proposed agreement identifies the specific roles and responsibilities for both parties to ensure the Project is completed in a timely and cost-effective manner. In general, SBCTA will take on the role and responsibilities of managing the project including seeking funding, final design, right of way acquisition and construction. The City of Ontario will continue to provide input into project design, provide no-cost plan check and permit issuance, and utilize its social media accounts for community outreach and engagement.

CITY OF ONTARIO

Agenda Report
June 16, 2020

SECTION:
CONSENT CALENDAR

SUBJECT: A RESOLUTION ORDERING THE SUMMARY VACATION OF A PUBLIC EASEMENT FOR FIRE ACCESS PURPOSES GENERALLY LOCATED SOUTH OF GUASTI ROAD APPROXIMATELY 1,515 FEET EAST OF HAVEN AVENUE

RECOMMENDATION: That the City Council adopt a resolution ordering the summary vacation of a public easement for fire access purposes generally located south of Guasti Road approximately 1,515 feet east of Haven Avenue.

COUNCIL GOALS: Invest in the Growth and Evolution of the City's Economy
Focus Resources in Ontario's Commercial and Residential Neighborhoods

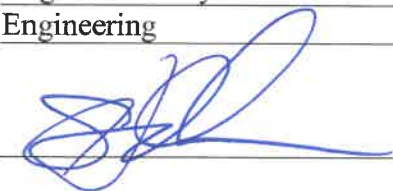
FISCAL IMPACT: None. The City did not pay to acquire this easement and will not incur any cost for vacating this easement. The applicant has paid the applicable processing fees to defray the City's cost to process this request.

BACKGROUND: The applicant, Costco Wholesale Corporation, has requested that the City vacate the unimproved public easement for fire access purposes within the property generally located south of Guasti Road approximately 1,515 feet east of Haven Avenue, as shown on "Exhibit A", to facilitate the development of the area. The subject easement, neither now nor in the future, will be needed for fire access purposes. The subject easement has no public utilities and is determined to be excess.

Section 8333(c) of the California Streets and Highways Code authorizes the City to summarily vacate (by resolution with no public hearing) a public service easement that has been superseded by relocation or determined to be excess and there are no public facilities located within the easement.

This summary vacation meets the California Government Code, Streets and Highways Code and Ontario Municipal Code and has been reviewed and approved by the City Engineer.

STAFF MEMBER PRESENTING: Scott Murphy, AICP, Executive Director Development Agency

Prepared by: Miguel Sotomayor
Department: Engineering
City Manager Approval: 

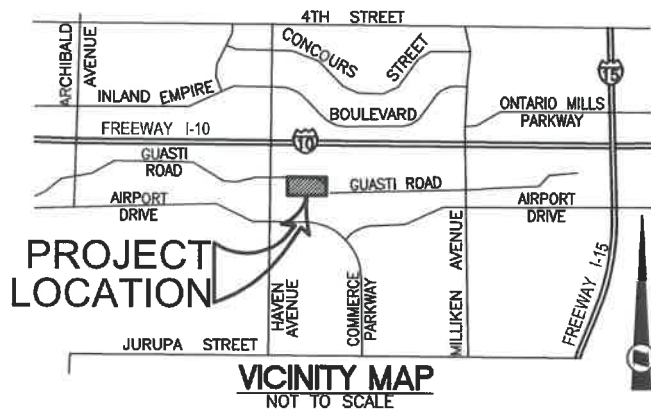
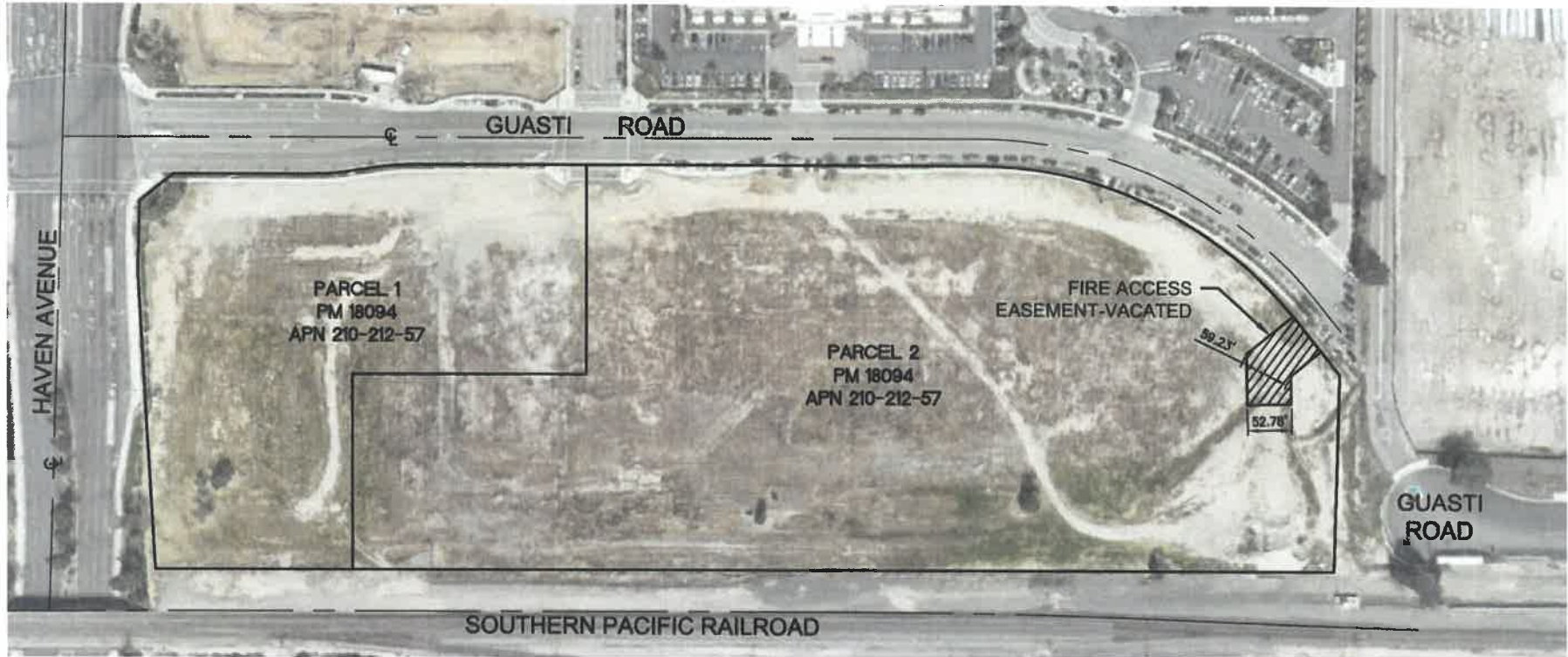
Submitted to Council/O.H.A. 06/16/2020

Approved: _____

Continued to: _____

Denied: _____

8



LEGEND



EXISTING FIRE ACCESS
EASEMENT TO BE VACATED.
AREA=0.13 AC

May, 2020

FUSCOE
ENGINEERING

2850 Inland Empire Blvd. Suite B
Ontario, California 91764
tel 909.581.0676 • fax 909.581.0696
www.fuscoe.com

CITY OF ONTARIO
ENGINEERING DEPARTMENT

COSTCO WHOLESALE
PROJECT: V-286

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, ORDERING THE SUMMARY VACATION OF A PUBLIC EASEMENT FOR FIRE ACCESS PURPOSES GENERALLY LOCATED SOUTH OF GUASTI ROAD APPROXIMATELY 1,515 FEET EAST OF HAVEN AVENUE.

WHEREAS, the City Council of the City of Ontario, California, pursuant to Division 9, Part 3, Chapter 4, of the California Streets and Highways Code, may summarily vacate an easement under certain conditions specified therein; and

WHEREAS, the public easement for fire access purposes dedicated by Parcel Map No. 18094, generally located south of Guasti Road Approximately 1,515 feet east of Haven Avenue, over a portion of parcel 2 as shown on said parcel map, recorded in Book 224, pages 67 through 70 of Parcel Maps, in the Office of the Recorder of San Bernardino County, State of California, is not needed for any present or future fire access purposes and is determined to be excess; and

WHEREAS, Section 8333(c) of the California Streets and Highways Code authorizes the City to summarily vacate (by resolution with no public hearing) a public service easement that has been superseded by relocation or determined to be excess and there are no public facilities located within the easement; and

WHEREAS, the property owner, Costco Wholesale Corporation, a Washington corporation, has requested a vacation of said easement.

NOW, THEREFORE, BE IT RESOLVED, by the City Council of the City of Ontario, California:

1. That the above recitals are true and correct.
2. That title to the above-described said easement, more specifically described in Exhibit "A" and depicted on Exhibit "B" attached hereto, shall be vacated.
3. That the City Clerk of the City of Ontario, California, shall cause a copy of this resolution to be recorded in the office of the County Recorder of San Bernardino County, California.

The City Clerk of the City of Ontario shall certify as to the adoption of this Resolution.

PASSED, APPROVED, AND ADOPTED this 16th day of June 2020.

PAUL S. LEON, MAYOR

ATTEST:

SHEILA MAUTZ, CITY CLERK

APPROVED AS TO LEGAL FORM:

COLE HUBER, LLP
CITY ATTORNEY

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, SHEILA MAUTZ, City Clerk of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. 2020- was duly passed and adopted by the City Council of the City of Ontario at their regular meeting held June 16, 2020 by the following roll call vote, to wit:

AYES: COUNCIL MEMBERS:

NOES: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

SHEILA MAUTZ, CITY CLERK

(SEAL)

The foregoing is the original of Resolution No. 2020- duly passed and adopted by the Ontario City Council at their regular meeting held June 16, 2020.

SHEILA MAUTZ, CITY CLERK

(SEAL)

**EXHIBIT A
EASEMENT VACATION
V-286**

BEING AN EASEMENT OVER PARCEL A, LOT LINE ADJUSTMENT LLA-19-006 IN THE CITY OF ONTARIO, COUNTY OF SAN BERNARDINO, STATE OF CALIFORNIA, PER INSTRUMENT NO. 2020-0005758 RECORDED ON JANUARY 7, 2020, RECORDS OF SAID COUNTY.

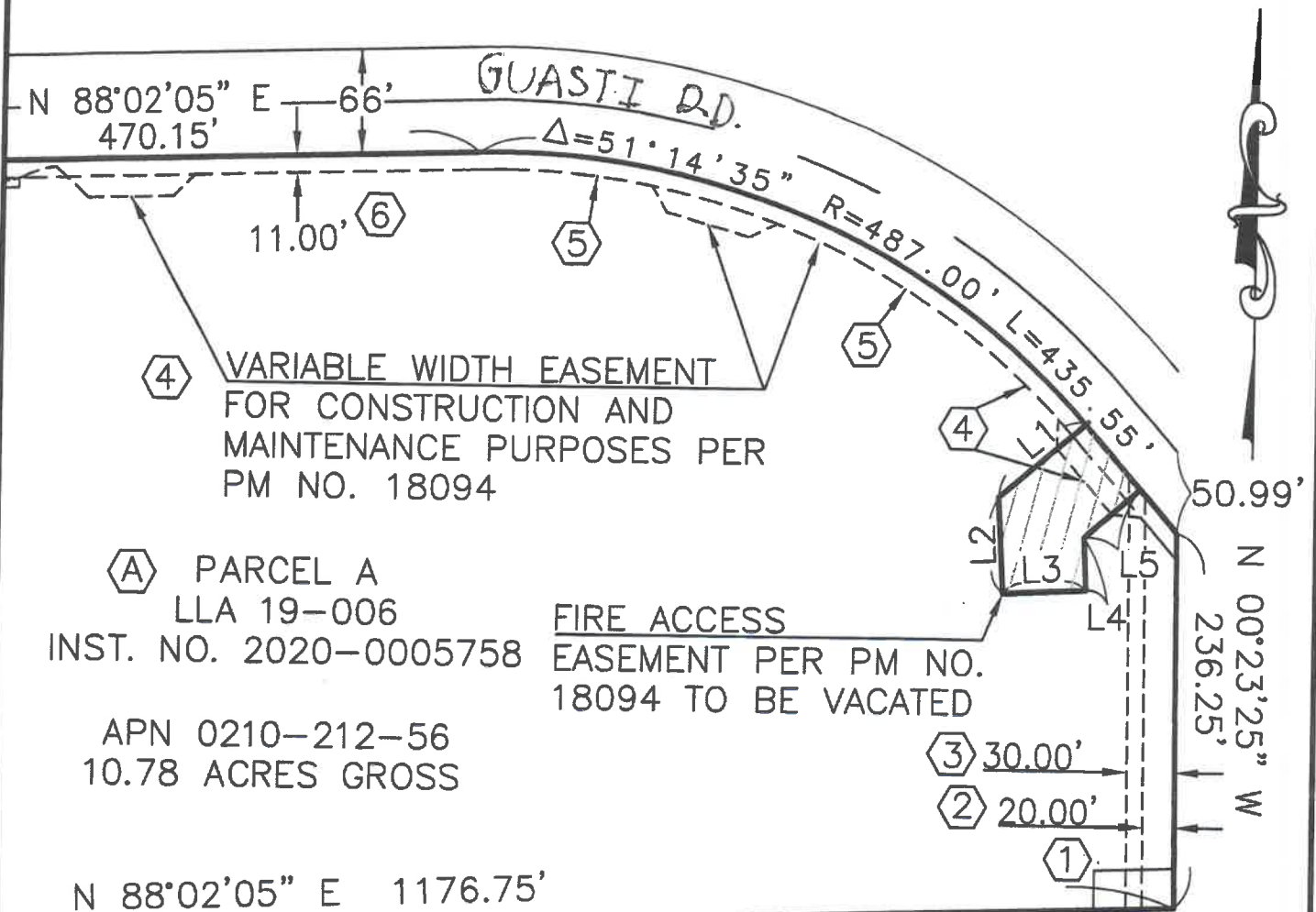
SAID EASEMENT DEDICATED TO THE CITY OF ONTARIO FOR FIRE ACCESS PURPOSES OVER SAID PARCEL A, AS SHOWN ON PARCEL MAP NO. 18094, RECORDED IN BOOK 224, PAGES 67, 68, 69, AND 70 OF PARCEL MAPS, RECORDS OF SAID COUNTY.

ALL SHOWN ON EXHIBIT B, ATTACHED HERETO AND BY THIS REFERENCE MADE APART OF.

CONTAINS: 5,625 S.F., 0.13 ACRES



EXHIBIT "B" EASEMENT VACATION V-286



(A) PARCEL A
LLA 19-006
INST. NO. 2020-0005758
APN 0210-212-56
10.78 ACRES GROSS

FIRE ACCESS
EASEMENT PER PM NO.
18094 TO BE VACATED

LEGEND

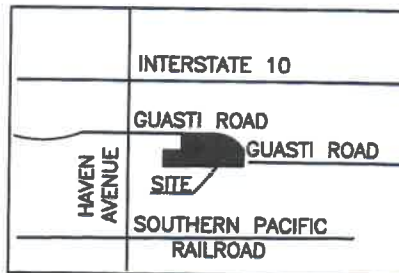
EXISTING EASEMENT DEDICATED TO THE CITY OF ONTARIO FOR FIRE ACCESS PURPOSES PER PARCEL MAP NO. 18094, PMB 224, PAGES 67-70, TO BE VACATED.

NOTE

(A) PARCEL A OF LOT LINE ADJUSTMENT LLA19-006 RECORDED JANUARY 7, 2020 AS INSTRUMENT NO. 2020-0005758.

LINE TABLE

LINE	BEARING	DISTANCE
L1	N49°16'40"E	73.93'
L2	N02°59'47"W	60.00'
L3	N87°00'13"E	52.78'
L4	N02°59'47"W	33.12'
L5	N49°16'40"E	46.51'



VICINITY MAP
NTS



Prepared by HILLWIG-GOODROW, INC.:
31407 Outer Hwy. 10, Redlands, CA 92373 (888) 626-5137

ALAN C. HILLWIG, PLS 5137
LICENSE EXPIRES: 6-30-21

Alan C. Hillwig 4/20/20
DATE

SCALE: 1" = 110'

FILE NO.: 228-208

DATE: APRIL 2020

SHEET 1 OF 2

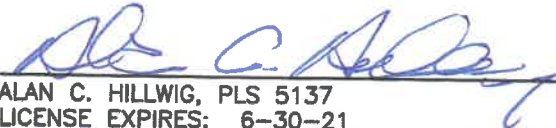
EXHIBIT "B"

EASEMENT VACATION V-286

EASEMENTS

- ① EASEMENT FOR SANITARY SEWER PURPOSES TO THE CITY OF ONTARIO PER BK. 7651/646, O.R.
- ② 20 FOOT WIDE EASEMENT FOR PUBLIC UTILITY DEDICATED PER PM NO. 18094.
- ③ 30 FOOT WIDE EASEMENT FOR PUBLIC UTILITY, WATER, STORM DRAIN, AN INCIDENTAL PURPOSES PER INSTRUMENT NO. 2008-0473116 OF OFFICIAL RECORDS.
- ④ VARIABLE WIDTH EASEMENT FOR CONSTRUCTION AND MAINTENANCE PURPOSES DEDICATED PER PM NO. 18094.
- ⑤ EASEMENT FOR PUBLIC STREET AND PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED OCTOBER 4, 2011 AS INSTRUMENT NO. 2011-0416326 OF OFFICIAL RECORDS.
- ⑥ 11 FOOT WIDE EASEMENT FOR PUBLIC STREET AND ROAD, PUBLIC UTILITIES, WIRES, CABLES, CONDUITS, STORM DRAINS, SANITARY SEWERS, WATER PIPELINES (FOR ANY PURPOSE, POTABLE OR NON-POTABLE, DOMESTIC OR RECYCLED) AND INCIDENTAL PURPOSES, RECORDED NOVEMBER 9, 2010 AS INSTRUMENT NO. 2010-0465740 OF OFFICIAL RECORDS.
- ⑦ PUBLIC TRAFFIC SIGNAL CONTRUCTION AND MAINTENANCE EASEMENT DEDICATED PER PM. NO. 18094.



Prepared by HILLWIG-GOODROW, INC.: 31407 Outer Hwy. 10, Redlands, CA 92373 (888) 626-5137  ALAN C. HILLWIG, PLS 5137 LICENSE EXPIRES: 6-30-21	DATE	SCALE: N/A
	4/20/20	FILE NO.: 228-208
		DATE: APRIL 2020
		SHEET <u>2</u> OF <u>2</u>

CITY OF ONTARIO

Agenda Report
June 16, 2020

SECTION:
CONSENT CALENDAR

SUBJECT: ANNUAL LEVY OF ASSESSMENTS WITHIN ONTARIO PARKWAY MAINTENANCE ASSESSMENT DISTRICT NOS. 1, 2, 3 AND 4 FOR FISCAL YEAR 2020-21


RECOMMENDATION: That the City Council adopt resolutions pertaining to the levy of assessments within Ontario Parkway Maintenance Assessment District (OPMAD) Nos. 1, 2, 3 and 4:

- (A) A resolution initiating proceedings and ordering the preparation of an Engineer's Report for each district relating to the annual levy of assessments; and
- (B) A resolution giving preliminary approval of the Engineer's Report; and
- (C) A resolution for District Nos. 1, 2 and 3, and a resolution for District No. 4, declaring the City's intention to levy the special assessments for Fiscal Year 2020-21 and setting the date of July 21, 2020 for a Public Hearing on each district in accordance with the Landscaping and Lighting Act of 1972.

COUNCIL GOALS: Operate in a Businesslike Manner
Focus Resources in Ontario's Commercial and Residential Neighborhoods

FISCAL IMPACT: As proposed, special assessment revenue will be generated in the amount of \$285,616 from OPMAD Nos. 1, 2 and 3, and \$339,006 from OPMAD No. 4 during Fiscal Year 2020-21. The total cost of \$607,398 for operation and maintenance within OPMAD Nos. 1, 2 and 3 will exceed the assessment revenues by \$321,781. The differential will be funded from the General Fund. If approved, the General Fund portion will be included in the City's proposed budget for Fiscal Year 2020-21. The total proposed special assessment for OPMAD No. 4 is sufficient to pay all expenses within this service area.

STAFF MEMBER PRESENTING: Armen Harkalyan, Executive Director of Finance

Prepared by: Jason Jacobsen
Department: Investments & Revenue Resources
City Manager Approval: 

Submitted to Council/O.H.A. 06/16/2020
Approved: _____
Continued to: _____
Denied: _____

9

BACKGROUND: The first of these districts was formed in 1976 pursuant to the Landscaping and Lighting Act of 1972 (the “1972 Act”). Additional territories are annexed to the districts from time to time as development proceeds. The districts were formed to help minimize the continually increasing cost of maintaining and operating the landscaping along public streets where the property owners do not have direct access to the landscaping. A special assessment is levied annually on benefiting properties for the cost of operation and maintenance of certain parkway landscaping facilities within the districts. The City’s Public Works Department establishes the annual operation and maintenance costs and administers the maintenance of the landscaping. Annually, an Engineer’s Report for each district must be prepared which apportions these costs to each parcel within the districts. The locations of the districts are shown on the attached maps.

OPMAD Nos. 1, 2, and 3 are comprised of single-family residential developments. Due to varying characteristics, the districts are organized into benefit zones and/or maintenance areas. Each zone/maintenance area is assessed for only the operation and maintenance of the facilities from which it receives special direct benefit. The total assessment for each maintenance area is then apportioned to its constituent parcels.

Special assessment revenue will be generated in the combined amount of \$285,616 from OPMAD Nos. 1, 2 and 3. The assessed tax rates for Fiscal Year 2019-20, and the proposed and maximum tax rates for Fiscal Year 2020-21, are as follows:

	Prior Year 2019-20	Maximum 2020-21	Proposed 2020-21
OPMAD No. 1	\$ 66.32 per Lot	\$ 66.32 per Lot	\$ 66.32 per Lot
OPMAD No. 2	\$ 34.04 per Lot	\$ 34.04 per Lot	\$ 34.04 per Lot
OPMAD No. 3, Zone 2000-1	\$ 41.29 per Lot	\$ 41.29 per Lot	\$ 41.29 per Lot
OPMAD No. 3, Zone 2000-2	\$ 186.38 per Lot	\$ 240.27 per Lot	\$ 186.38 per Lot

OPMAD No. 4 is comprised of single-family residential tracts and the Ontario Mills development area. The district is organized into 6 Maintenance Areas (MAs). Each maintenance area is assessed for only the operation and maintenance of the facilities from which it receives direct special benefit. The total assessment for each maintenance area is then apportioned to its constituent parcels.

The assessments for OPMAD No. 4 will generate a total of \$339,006. OPMAD No. 4 will generate sufficient revenue to pay all expenses within the service area. The assessed tax rates for Fiscal Year 2019-20, and the maximum and proposed tax rates for Fiscal Year 2020-21, are as follows:

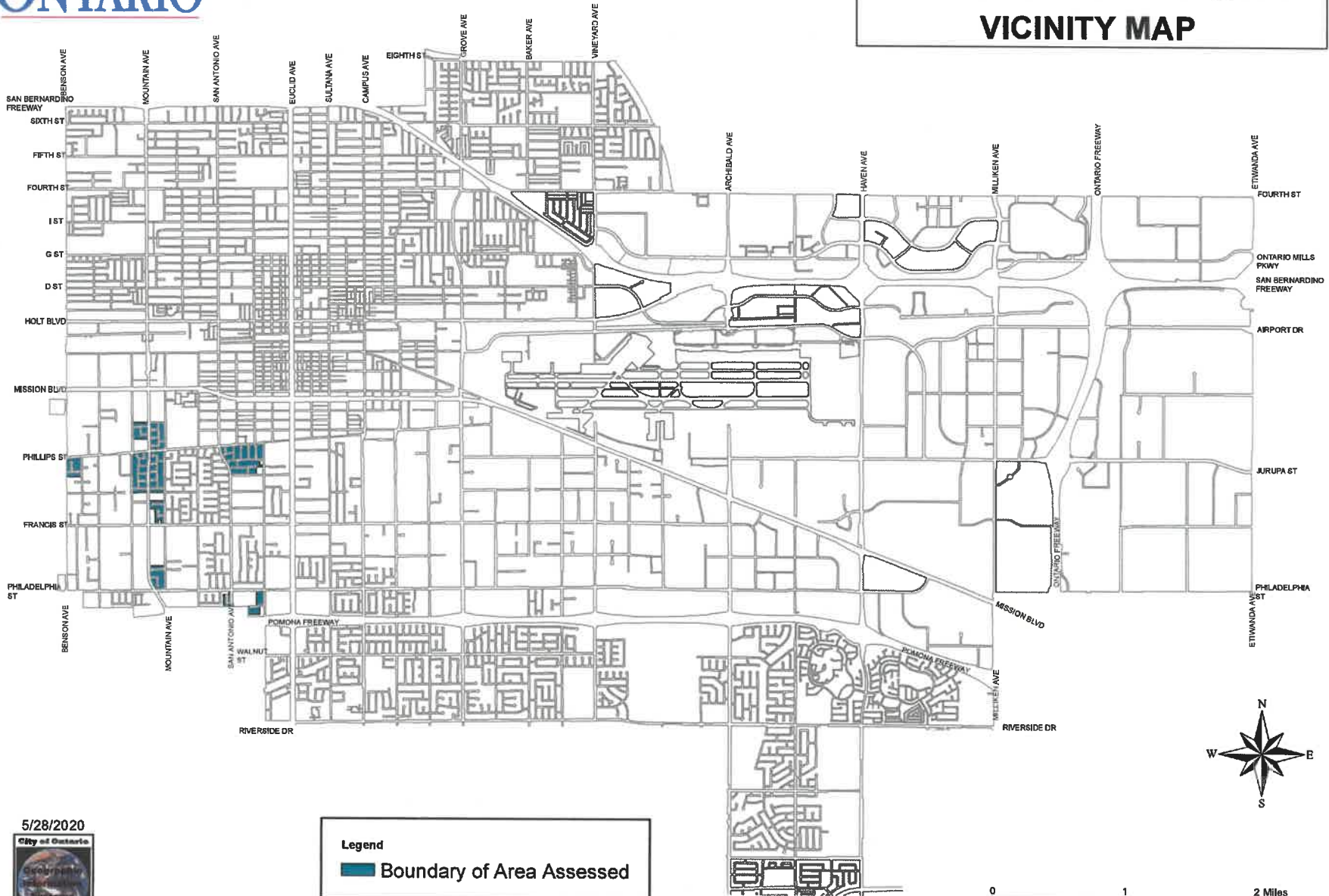
	Prior Year 2019-20	Maximum 2020-21	Proposed 2020-21
MA#99-1	\$ 55.65 per Lot	\$ 58.65 per Lot	\$ 55.65 per Lot
MA#99-2	\$ 352.00 per Lot	\$ 368.79 per Lot	\$ 362.55 per Lot
MA#99-3	\$ 1,093.77 per Acre	\$ 1,324.21 per Acre	\$ 1,126.58 per Acre
MA#00-1	\$ 264.84 per Lot	\$ 363.20 per Lot	\$ 264.84 per Lot
MA#04-1	\$ 1,065.26 per Lot	\$ 2,313.51 per Lot	\$ 1,065.26 per Lot
MA#05-1	\$ 1,463.53 per Lot	\$ 2,867.72 per Lot	\$ 1,463.53 per Lot

Pursuant to the "1972 Act," Engineer's Reports have been prepared for each district and are on file with the Records Management Department. Each report includes plans and specifications for the improvements and maintenance, an estimate of costs, a diagram of the area proposed to be assessed, and an assessment of the costs to the various parcels within each district.

A public hearing is required for each district prior to adoption of the proposed assessments. A combined public hearing will be held on July 21, 2020 for the purpose of accepting comments and written protests. At the conclusion of the public hearing, the City Council will consider adoption of resolutions confirming the assessments and ordering the levy of the assessments for tax year 2020-21. Notice of the public hearing for the levy of assessments will be published in the local newspaper, in accordance with the applicable statutes.



Ontario Parkway Maintenance Assessment District No. 1 VICINITY MAP



Legend
■ Boundary of Area Assessed

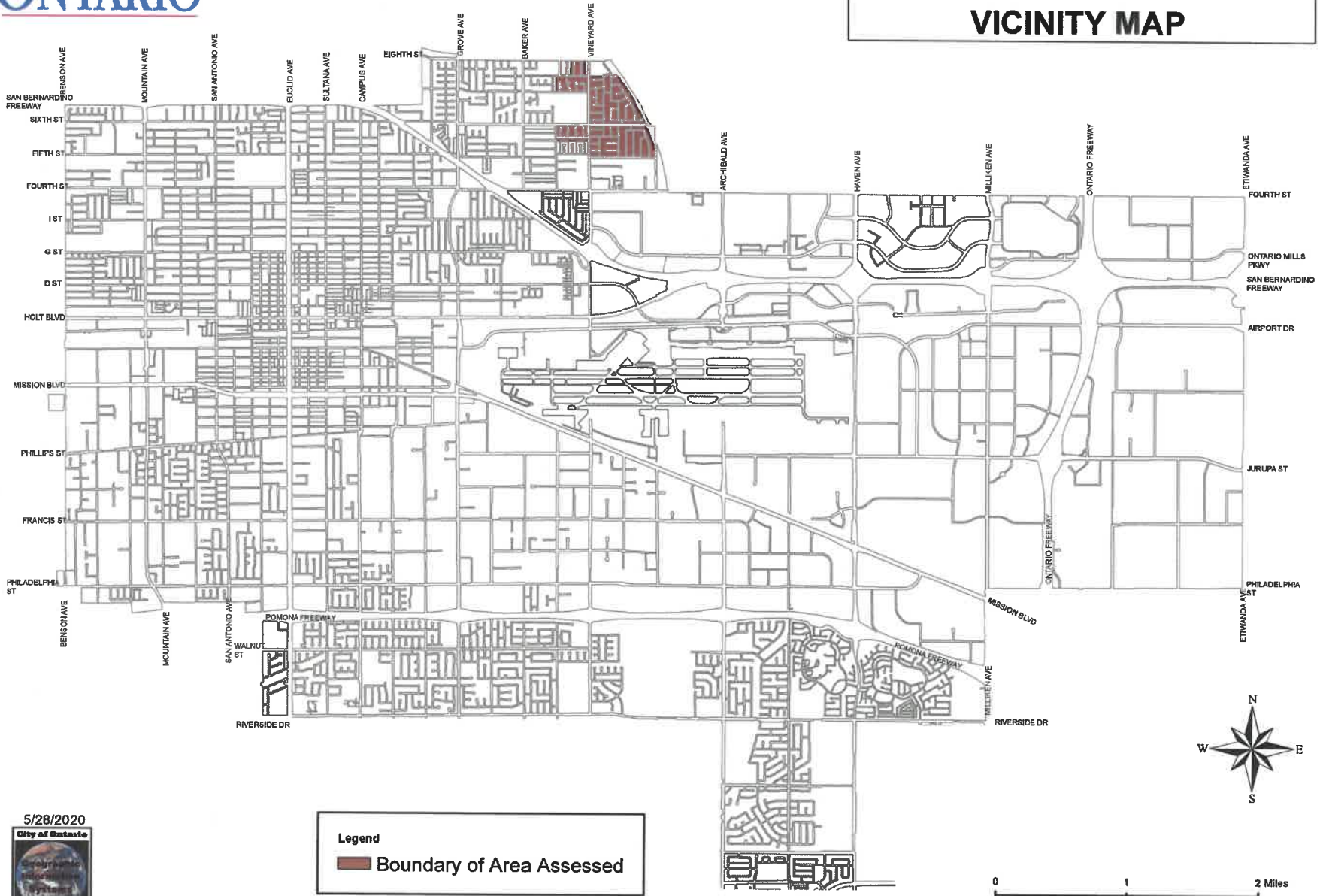


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Ontario Parkway Maintenance Assessment District No. 2 VICINITY MAP



5/28/2020

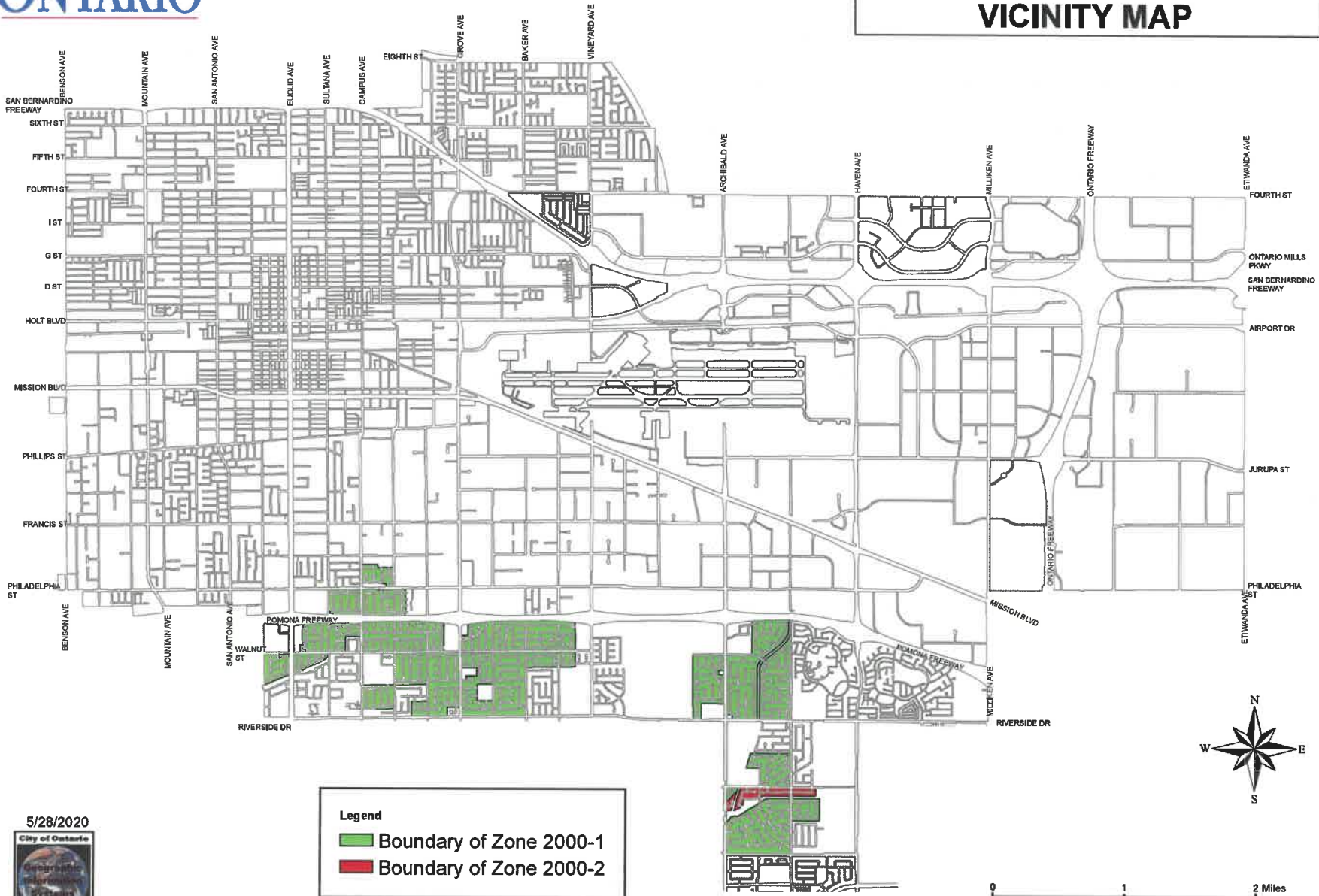


Legend
■ Boundary of Area Assessed

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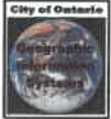
Ontario Parkway Maintenance Assessment District No. 3 VICINITY MAP



Legend

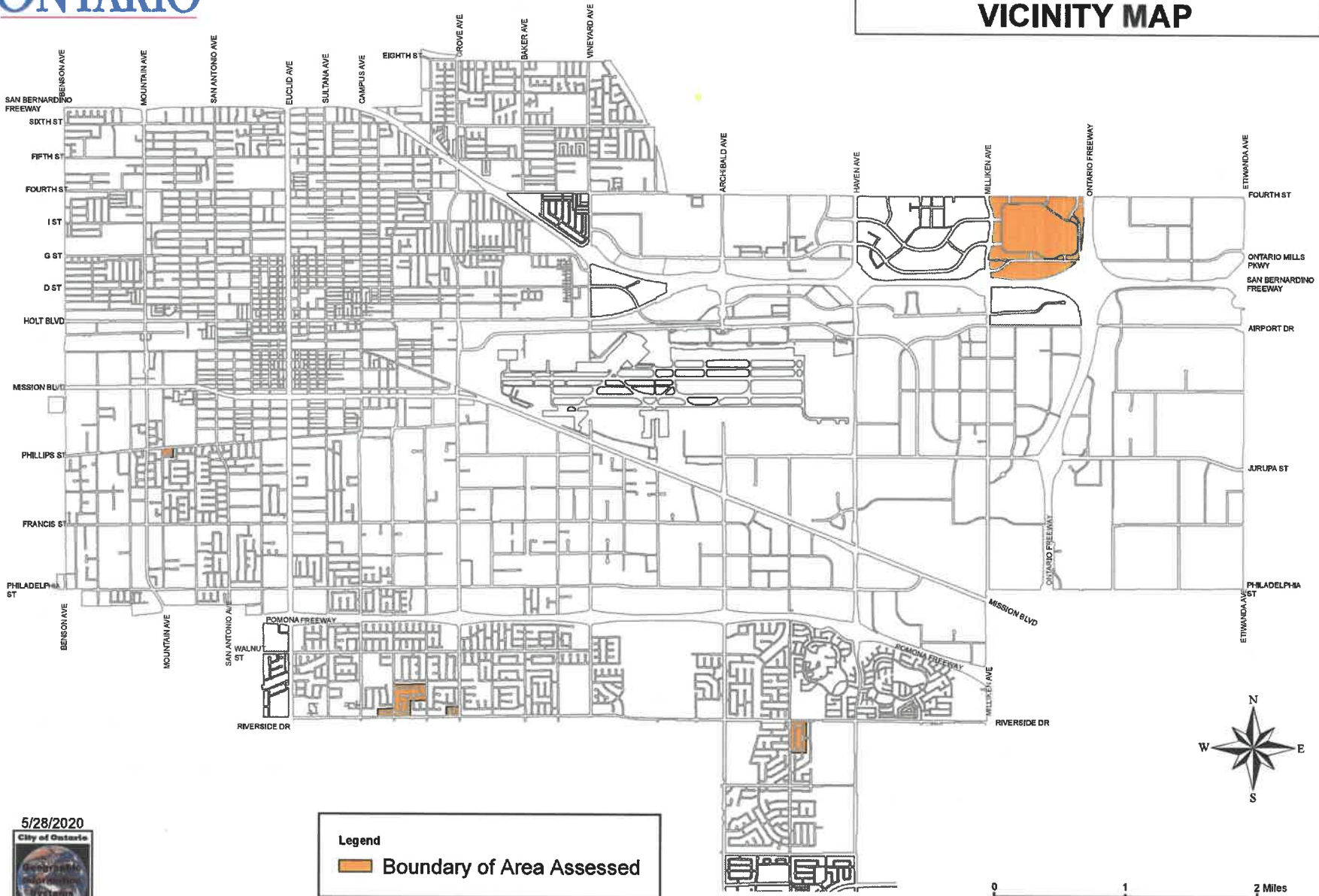
- Boundary of Zone 2000-1
- Boundary of Zone 2000-2

5/28/2020





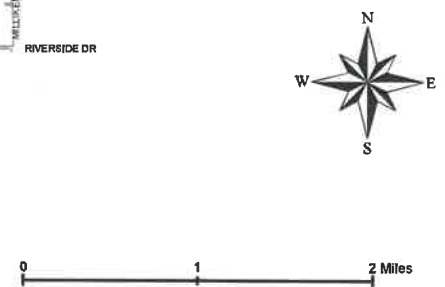
Ontario Parkway Maintenance Assessment District No. 4 VICINITY MAP



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Legend
Boundary of Area Assessed



RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, ORDERING THE PREPARATION OF THE ASSESSMENT ENGINEER'S REPORTS PURSUANT TO THE PROVISIONS OF DIVISION 15, PART 2, OF THE STREETS AND HIGHWAYS CODE OF THE STATE OF CALIFORNIA, FOR PROCEEDINGS FOR THE ANNUAL ASSESSMENT LEVY WITHIN ONTARIO PARKWAY MAINTENANCE ASSESSMENT DISTRICTS NO. 1, 2, 3 AND 4 FOR FISCAL YEAR 2020-21.

WHEREAS, the City Council of the City of Ontario, California, previously formed maintenance districts pursuant to the terms and provisions of the "Landscaping and Lighting Act of 1972," being Division 15, Part 2, of the Streets and Highways Code of the State of California (the "1972 Act"), known and designated as

CITY OF ONTARIO
PARKWAY MAINTENANCE ASSESSMENT DISTRICTS NO. 1, 2, 3 AND 4
(the "Districts"); and

WHEREAS, at this time the City Council desires to initiate proceedings to provide for the annual levy of assessments for the ensuing fiscal year to provide for the annual costs for maintenance of improvements within the Districts; and

WHEREAS, the proceedings for the annual levy of assessments shall relate to the fiscal year commencing July 1, 2020, and ending June 30, 2021; and

WHEREAS, there has been submitted to this City Council for its consideration at this time, maps showing the boundaries of the area affected by the levy of the assessment for the above-referenced fiscal year, and inventory maps showing and describing in general the works of improvement proposed to be maintained in said Districts, said description being sufficient to identify the works of improvement and the areas proposed to be assessed for said maintenance thereof.

NOW, THEREFORE, IT IS HEREBY RESOLVED as follows:

SECTION 1. That the above recitals are all true and correct.

SECTION 2. That the maps entitled

CITY OF ONTARIO
PARKWAY MAINTENANCE ASSESSMENT DISTRICTS NO. 1, 2, 3 AND 4
BOUNDARY AND INVENTORY MAPS

attached hereto, as submitted to this City Council, showing the boundaries of the proposed areas to be assessed and showing the works of improvement to be maintained, are hereby approved, and a copy thereof shall be on file in the Records Management

Department and open to public inspection. The parcels and properties within said area are those proposed to be assessed to pay the costs and expenses for said maintenance work.

SECTION 3. That the proposed maintenance work within the area proposed to be assessed shall be for certain landscaping and appurtenant improvements, as said maintenance work is set forth in the Reports to be presented to this City Council for consideration.

SECTION 4. That the Assessment Engineer, Harris & Associates, is hereby ordered to prepare and file with this City Council the Assessment Engineer's Reports relating to said annual assessment and levy in accordance with the provisions of 1972 Act, Article XIID of the Constitution of the State of California ("Article XIID"), and the Proposition 218 Omnibus Implementation Act (Government Code Section 53750 and following) (the "Implementation Act") (the 1972 Act, Article XIID and the Implementation Act are referred to collectively as the "Assessment Law").

SECTION 5. That upon completion, said Assessment Engineer's Reports shall be filed with the City Clerk, who shall then submit the same to this City Council for its consideration pursuant to the Assessment Law.

The City Clerk of the City of Ontario shall certify as to the adoption of this Resolution.

PASSED, APPROVED, AND ADOPTED this 16th day of June 2020.

PAUL S. LEON, MAYOR

ATTEST:

SHEILA MAUTZ, CITY CLERK

APPROVED AS TO LEGAL FORM:

COLE HUBER LLP
CITY ATTORNEY

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, SHEILA MAUTZ, City Clerk of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. 2020- was duly passed and adopted by the City Council of the City of Ontario at their regular meeting held June 16, 2020 by the following roll call vote, to wit:

AYES: COUNCIL MEMBERS:

NOES: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

SHEILA MAUTZ, CITY CLERK

(SEAL)

The foregoing is the original of Resolution No. 2020- duly passed and adopted by the Ontario City Council at their regular meeting held June 16, 2020.

SHEILA MAUTZ, CITY CLERK

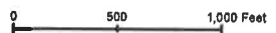
(SEAL)



Ontario Parkway Maintenance Assessment District No. 1 Boundary and Inventory Map



5/28/2020
City of Ontario
Geographic Information Systems



- Legend**
- Areas Maintained
 - Boundary of Area Assessed

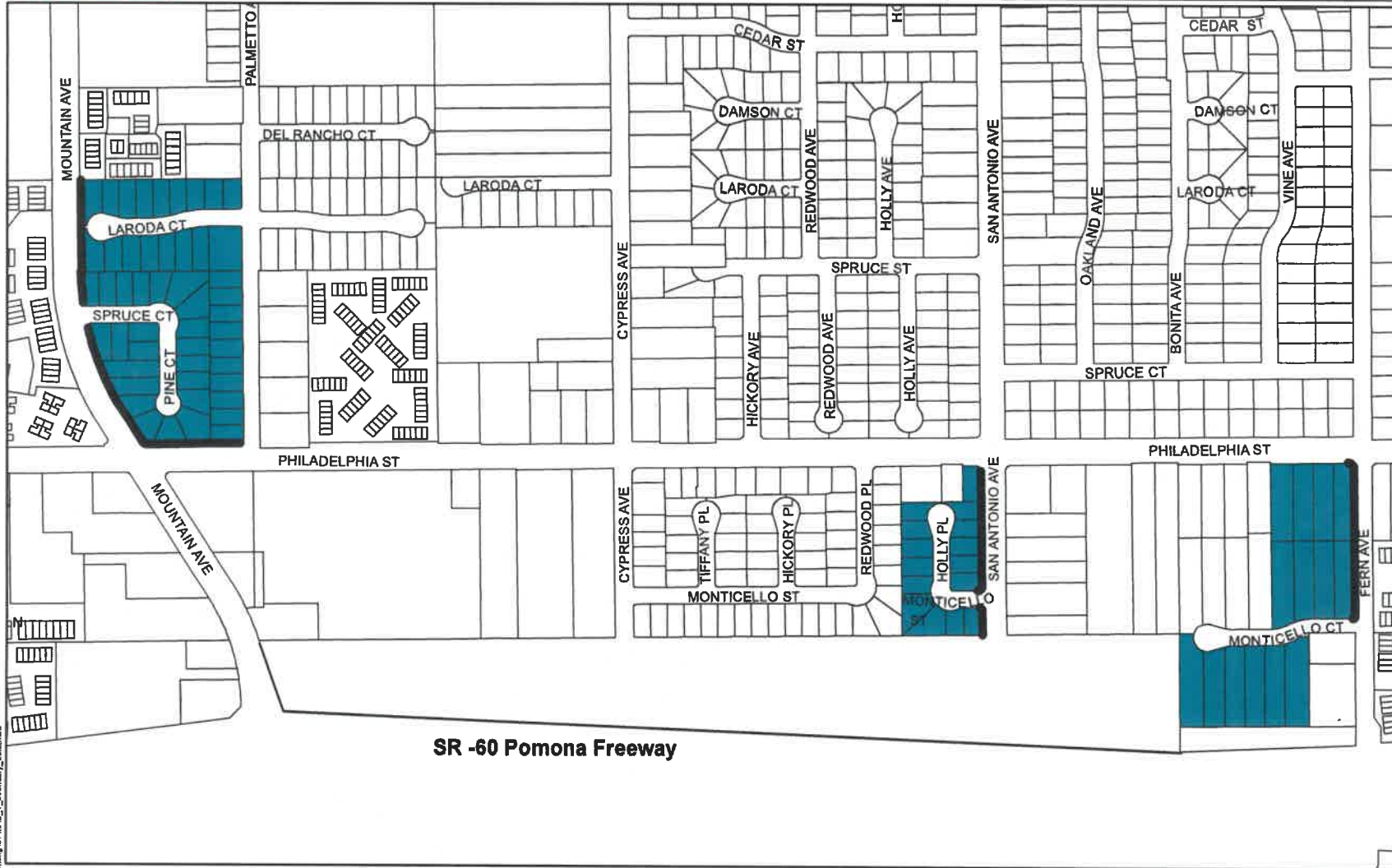
Map 1 of 2

Note:
Reference is hereby made to the Maps of the Assessor of the County of San Bernardino, California, for detailed descriptions of the lines and dimensions of the parcels shown hereon.

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Ontario Parkway Maintenance Assessment District No. 1 Boundary and Inventory Map

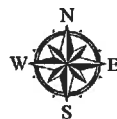


SR -60 Pomona Freeway

5/28/2020



0 500 1,000 Feet



Legend

- Areas Maintained
- Boundary of Area Assessed

Map 2 of 2

Note:

Reference is hereby made to the Maps of the Assessor of the County of San Bernardino, California, for detailed descriptions of the lines and dimensions of the parcels shown hereon.



Ontario Parkway Maintenance Assessment District No. 2 Boundary and Inventory Map



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5/28/2020



0 500 1,000 Feet



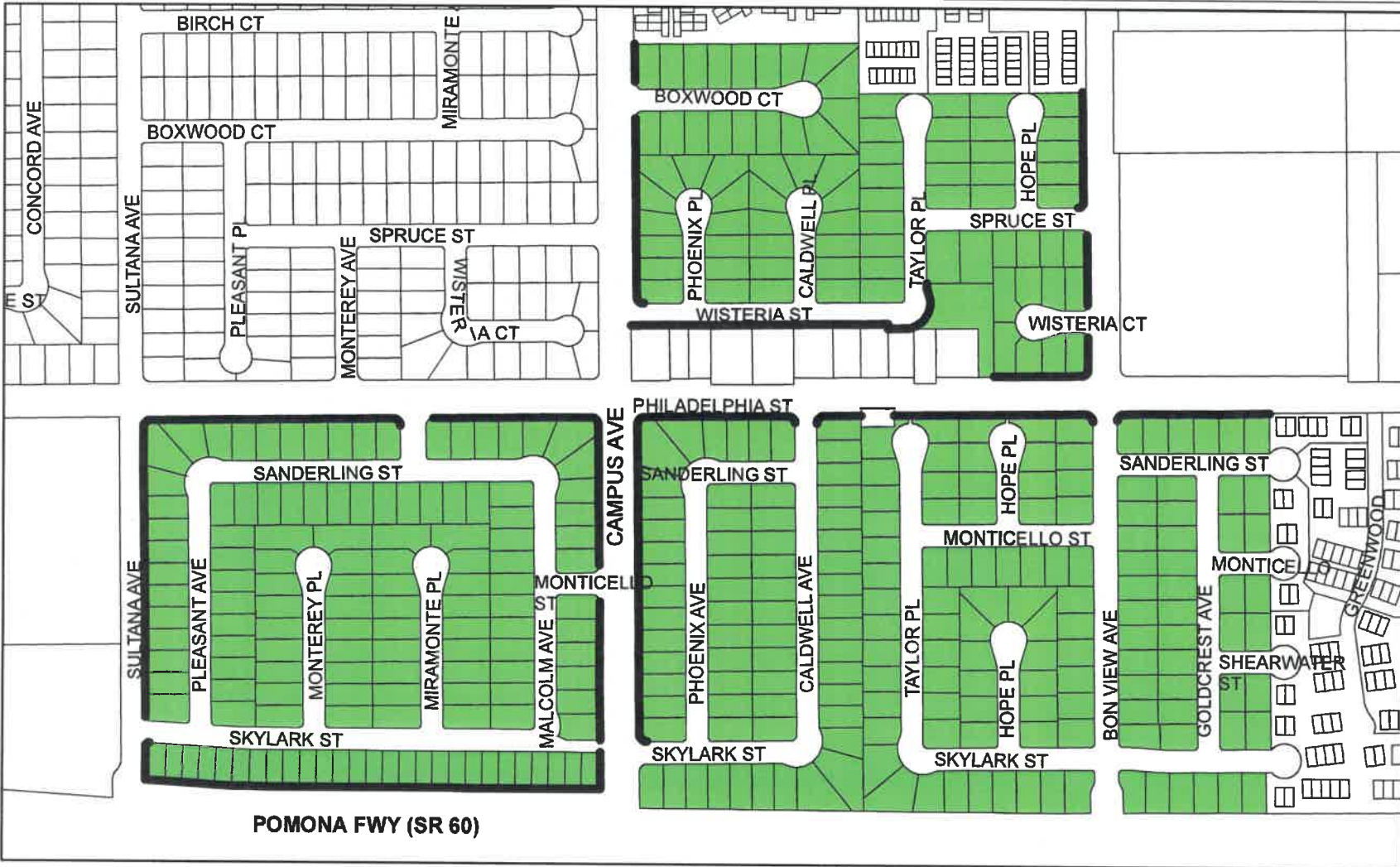
Legend **Map 1 of 1**

- Areas Maintained
- Boundary of Areas Assessed

Note: Reference is hereby made to the Maps of the Assessor of the County of San Bernardino, California, for detailed descriptions of the lines and dimensions of the parcels shown herein.

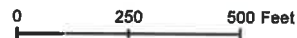


Ontario Parkway Maintenance Assessment District No. 3 Boundary and Inventory Map



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5/28/2020



Legend **Map 1 of 5**

- Areas Maintained
- Boundary of Zone 2000-1

Note:
Reference is hereby made to the Maps of the Assessor of the County of San Bernardino, California, for detailed descriptions of the lines and dimensions of the parcels shown hereon.

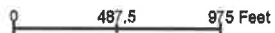


Ontario Parkway Maintenance Assessment District No. 3 Boundary and Inventory Map



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5/28/2020



Legend

- Areas Maintained
- █ Boundary of Zone 2000-1

Map 2 of 5

Note:
Reference is hereby made to the Maps of the Assessor of the County of San Bernardino, California, for detailed descriptions of the lines and dimensions of the parcels shown hereon.



Ontario Parkway Maintenance Assessment District No. 3 Boundary and Inventory Map



5/28/2020
City of Ontario



Legend Map 3 of 5

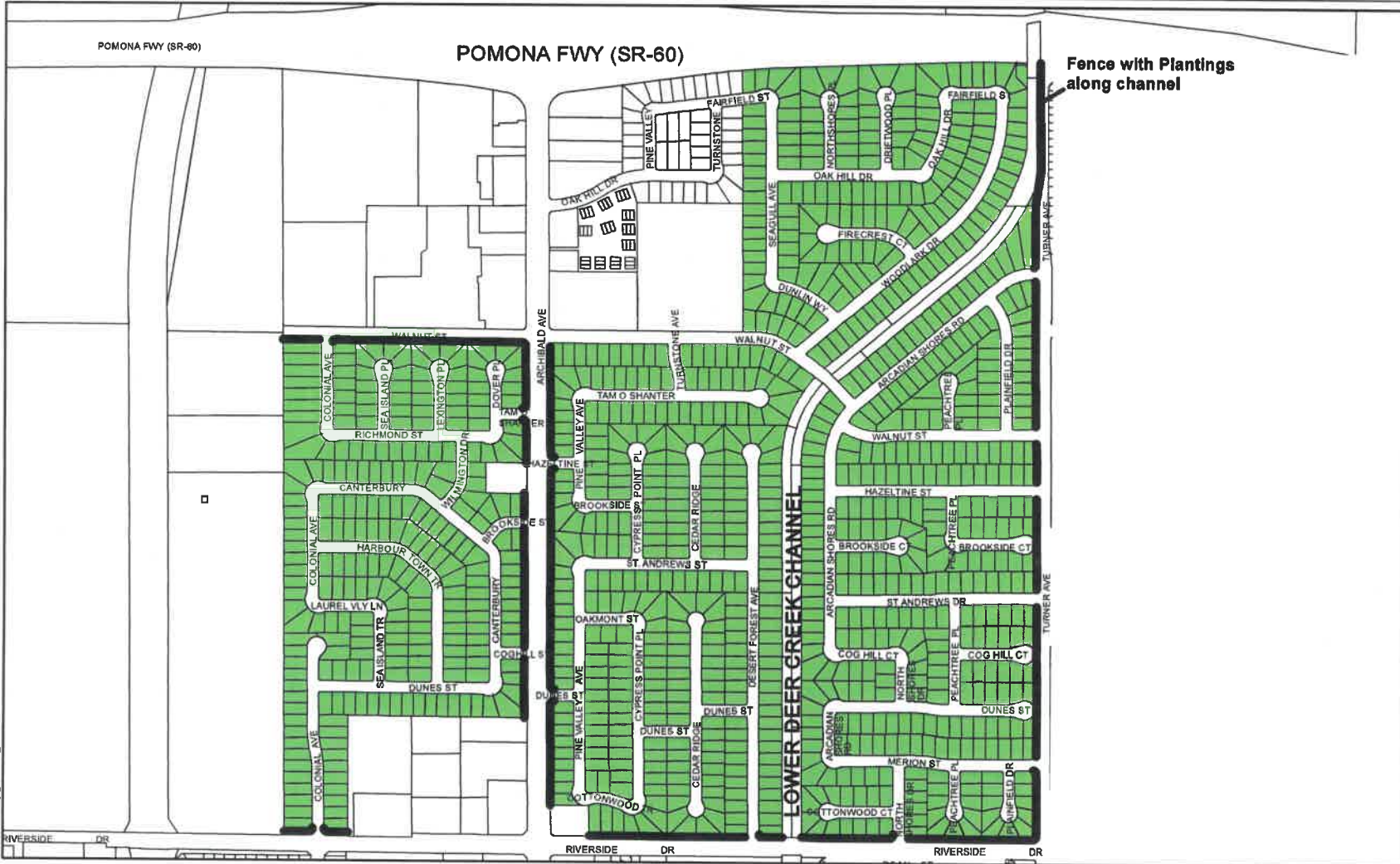
- Areas Maintained
- Boundary of Zone 2000-1

Note:
Reference is hereby made to the Maps of the Assessor of the County of San Bernardino, California, for detailed descriptions of the lines and dimensions of the parcels shown herein.

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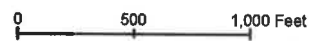


Ontario Parkway Maintenance Assessment District No. 3 Boundary and Inventory Map



Fence with Plantings along channel

5/28/2020
City of Ontario
Geographic Information System



Legend Map 4 of 5

- Areas Maintained
- Boundary of Zone 2000-1

Note:
Reference is hereby made to the Maps of the Assessor of the County of San Bernardino, California, for detailed descriptions of the lines and dimensions of the parcels shown hereon.



Ontario Parkway Maintenance Assessment District No. 3 Boundary and Inventory Map

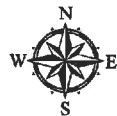
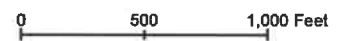
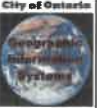


Entry Statement in easement only

Future Bike Path

Landscape buffer in S.C.E. RW not yet installed

5/28/2020

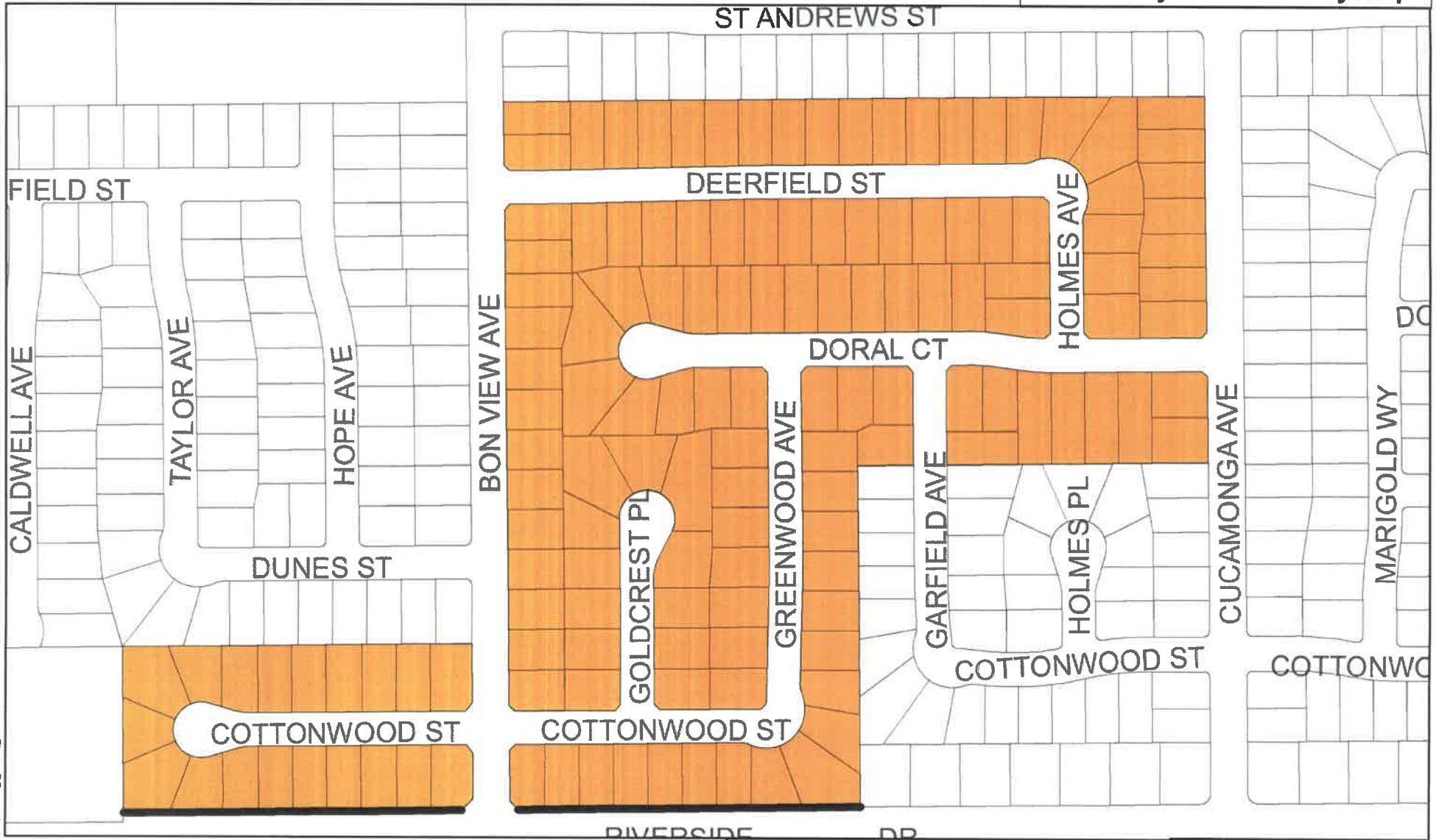


Legend		Map 5 of 5
	Areas Maintained	
	Boundary of Zone 2000-1	
	Boundary of Zone 2000-2	

Note:
Reference is hereby made to the Maps of the Assessor of the County of San Bernardino, California, for detailed descriptions of the lines and dimensions of the parcels shown hereon.

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**Ontario Parkway Maintenance
Assessment District No. 4
Boundary and Inventory Map**



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5/28/2020



0 237.5 475 Feet



Legend

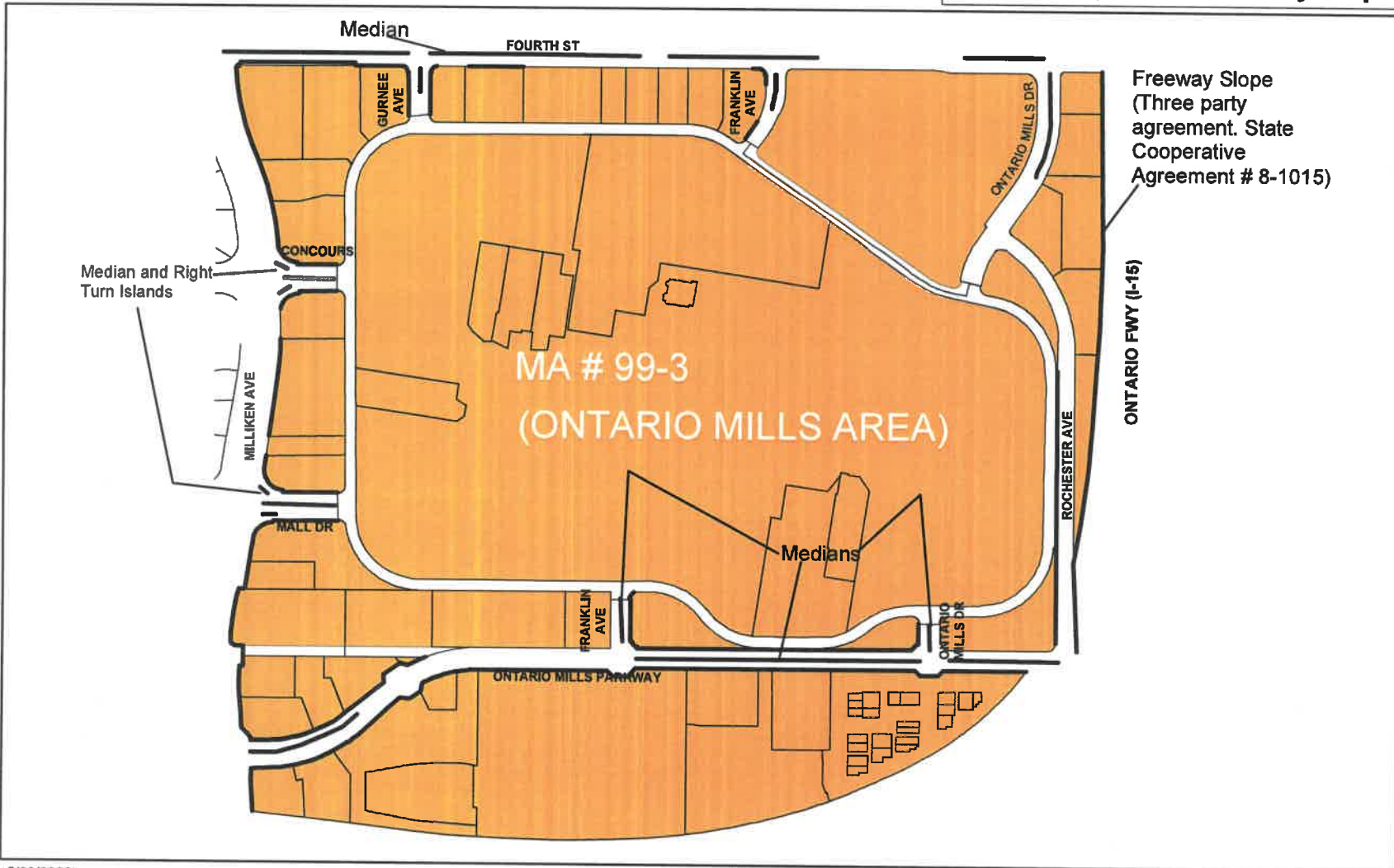
-  Areas Maintained
-  MA - # Boundary and Maintenance Area #

Map 1 of 5

Note:
Reference is hereby made to the Maps of the Assessor of the County of San Bernardino, California, for detailed descriptions of the lines and dimensions of the parcels shown hereon.



Ontario Parkway Maintenance Assessment District No. 4 Boundary and Inventory Map



5/28/2020
City of Ontario
Geographic Information Systems

0 500 1,000 Feet



Legend

- Areas Maintained
- MA - # Boundary and Maintenance Area #

Map 2 of 5

Note:
Reference is hereby made to the Maps of the Assessor of the County of San Bernardino, California, for detailed descriptions of the lines and dimensions of the parcels shown hereon.



Ontario Parkway Maintenance Assessment District No. 4 Boundary and Inventory Map



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5/28/2020
City of Ontario



0 195 390 Feet



Legend

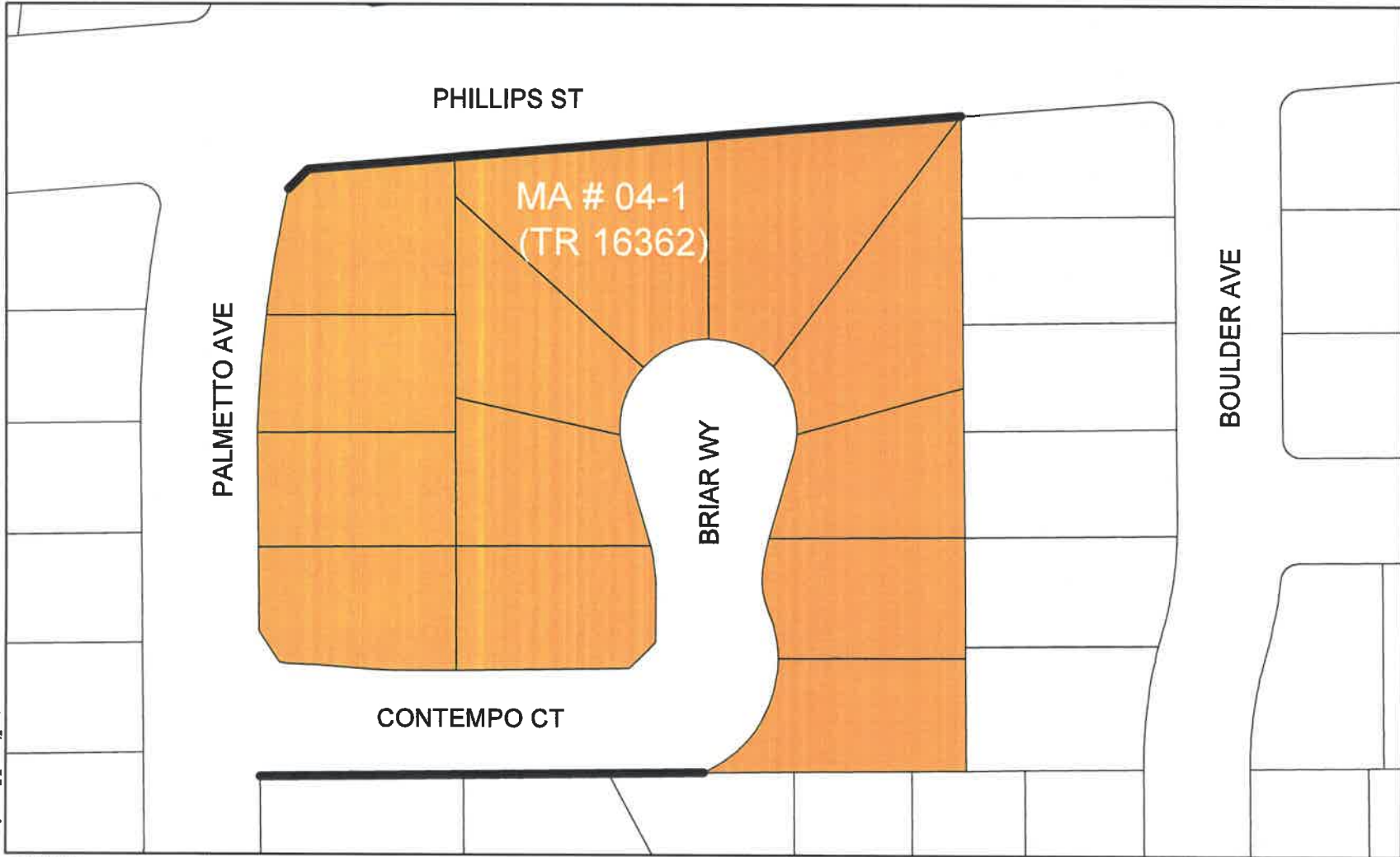
-  Areas Maintained
-  MA - # Boundary and Maintenance Area #

Map 3 of 5

Note:
Reference is hereby made to the Maps of the Assessor of the County of San Bernardino, California, for detailed descriptions of the lines and dimensions of the parcels shown hereon.



Ontario Parkway Maintenance Assessment District No. 4 Boundary and Inventory Map



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5/28/2020



0 45 90 Feet



Legend

- Areas Maintained
- MA - # Boundary and Maintenance Area #

Map 4 of 5

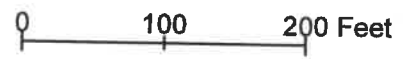
Note:
Reference is hereby made to the Maps of the Assessor of the County of San Bernardino, California, for detailed descriptions of the lines and dimensions of the parcels shown hereon.



**Ontario Parkway Maintenance Assessment District No. 4
Boundary and Inventory Map**



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Legend

- Areas Maintained
- MA - # Boundary and Maintenance Area #

Map 5 of 5

Note:
Reference is hereby made to the Maps of the Assessor of the County of San Bernardino, California, for detailed descriptions of the lines and dimensions of the parcels shown hereon.

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING THE ASSESSMENT ENGINEER'S REPORTS FOR THE ANNUAL LEVY OF ASSESSMENTS WITHIN ONTARIO PARKWAY MAINTENANCE ASSESSMENT DISTRICTS NO. 1, 2, 3 AND 4 FOR FISCAL YEAR 2020-21.

WHEREAS, the City Council of the City of Ontario, California, pursuant to the provisions of Division 15, Part 2, of the Streets and Highways Code of the State of California (the "1972 Act"), Article XIID of the Constitution of the State of California ("Article XIID"), and the Proposition 218 Omnibus Implementation Act (Government Code Section 53750 and following) (the "Implementation Act") (the 1972 Act, Article XIID and the Implementation Act are referred to collectively as the "Assessment Law"), did, by previous Resolution, order the preparation of the Assessment Engineer's Reports for the annual levy of assessments for the referenced Fiscal Year in the maintenance assessment districts known and designated as

CITY OF ONTARIO
PARKWAY MAINTENANCE ASSESSMENT DISTRICTS NO. 1, 2, 3 AND 4
(the "Districts"); and

WHEREAS, there has now been presented to this City Council the Assessment Engineer's Reports as required by the Assessment Law and as previously directed by Resolution; and

WHEREAS, this City Council has now examined and reviewed the Assessment Engineer's Reports as presented, and is satisfied with each and all of the items and documents as set forth therein, and is satisfied that the assessments on a preliminary basis, have been spread in accordance with the special benefits received from the improvements to be maintained, as set forth in said Assessment Engineer's Reports.

NOW, THEREFORE, IT IS HEREBY RESOLVED as follows:

SECTION 1. That the above recitals are all true and correct.

SECTION 2. That the Assessment Engineer's Reports as presented, consisting of the following:

- A. Plans and specifications describing the general nature, location, and extent of the improvements to be maintained and the extent of such maintenance;
- B. An estimate of the cost of the maintenance of the improvements for the Districts for the referenced fiscal year;

- C. A diagram for each District, showing the area and properties proposed to be assessed; and
- D. An annual assessment for the referenced Fiscal Year of the estimated costs of the maintenance of those improvements to be maintained during such Fiscal Year, assessing the net amount upon all assessable lots and/or parcels within the Districts in proportion to the special benefits received;

are hereby approved on a preliminary basis and are ordered to be filed in the Records Management Department as a permanent record and to remain open to public inspection.

SECTION 3. That the City Clerk shall certify to the passage and adoption of this Resolution, and the minutes of this meeting shall so reflect the presentation of the Assessment Engineer's Reports.

The City Clerk of the City of Ontario shall certify as to the adoption of this Resolution.

PASSED, APPROVED, AND ADOPTED this 16th day of June 2020.

PAUL S. LEON, MAYOR

ATTEST:

SHEILA MAUTZ, CITY CLERK

APPROVED AS TO LEGAL FORM:

COLE HUBER
CITY ATTORNEY

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, SHEILA MAUTZ, City Clerk of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. 2020- was duly passed and adopted by the City Council of the City of Ontario at their regular meeting held June 16, 2020 by the following roll call vote, to wit:

AYES: COUNCIL MEMBERS:

NOES: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

SHEILA MAUTZ, CITY CLERK

(SEAL)

The foregoing is the original of Resolution No. 2020- duly passed and adopted by the Ontario City Council at their regular meeting held June 16, 2020.

SHEILA MAUTZ, CITY CLERK

(SEAL)

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, DECLARING ITS INTENTION TO PROVIDE FOR AN ANNUAL LEVY AND COLLECTION OF ASSESSMENTS FOR FISCAL YEAR 2020-21 IN ONTARIO PARKWAY MAINTENANCE ASSESSMENT DISTRICTS NO. 1, 2 AND 3, PURSUANT TO THE PROVISIONS OF DIVISION 15, PART 2, OF THE STREETS AND HIGHWAYS CODE OF THE STATE OF CALIFORNIA, AND SETTING A TIME AND PLACE FOR A PUBLIC HEARING THEREON.

WHEREAS, the City Council of the City of Ontario, California, has previously formed landscaping maintenance assessment districts pursuant to the terms and provisions of the "Landscaping and Lighting Act of 1972," being Division 15, Part 2, of the Streets and Highways Code of the State of California, in what is known and designated as

CITY OF ONTARIO
PARKWAY MAINTENANCE ASSESSMENT DISTRICTS NO. 1, 2 AND 3

(hereinafter referred to as the Districts); and

WHEREAS, at this time, this City Council desires to conduct proceedings to provide for the annual levy of assessments for the ensuing fiscal year, to provide for the costs and expenses necessary for continual maintenance of improvements within said Districts; and

WHEREAS, at this time, there has been presented and approved by this City Council, the Engineer's Reports as required by law, and this City Council desires to conduct the proceedings for said annual levy.

NOW, THEREFORE, IT IS HEREBY RESOLVED as follows:

SECTION 1. That the above recitals are all true and correct.

SECTION 2. That the public interest and convenience requires, and it is the intention of this City Council, to undertake proceedings for the annual levy and collection of special assessments for the continual maintenance of certain improvements, all to service and benefit said Districts as said area is shown and delineated on the maps as previously approved by this City Council and on file in the Records Management Department, open to public inspection, and herein so referenced and made a part hereof, and proposed changes thereto are set forth in the Engineer's Reports, incorporated herein as a part hereof.

REPORT

SECTION 3. That the Engineer's Reports regarding the annual levy for said Districts, which Reports are for maintenance for said fiscal year, are hereby approved and are directed to be filed in the Records Management Department.

ASSESSMENT

SECTION 4. That the public interest and convenience requires, and it is the intention of this City Council to order the annual assessment levy for the Districts as set forth and described in said Engineer's Reports, and further it is determined to be in the best public interest and convenience to levy and collect annual assessments to pay the costs and expenses of said maintenance and improvement as estimated in said Reports.

DESCRIPTION OF MAINTENANCE

SECTION 5. The assessments levied and collected shall be for the maintenance of certain parkway landscaping and appurtenant improvements, as set forth in the Engineer's Reports, referenced and so incorporated herein.

COUNTY AUDITOR

SECTION 6. The County Auditor shall enter on the County Assessment Roll the amount of the assessments, and the County Tax Collector shall collect said assessments at the time and in the same manner as County taxes are collected. After collection by the County, the net amount of the assessments, after the deduction of any compensation due to the County for collection, shall be paid to the Treasurer for purposes of paying for the costs and expenses of said Districts.

SPECIAL FUND

SECTION 7. That all monies collected shall be deposited in a special fund. Payment shall be made out of said fund only for the purpose provided for in this Resolution, and in order to expedite the making of this maintenance and improvement, the City Council may transfer into said fund as it may deem necessary to expedite the proceedings. Any such transfer shall be repaid out of the proceeds of the assessments provided for in this Resolution.

BOUNDARIES OF DISTRICTS

SECTION 8. Said contemplated maintenance work is in the opinion of this City Council, of direct benefit to the properties within the boundaries of the Districts, and this City Council makes the costs and expenses of said maintenance chargeable upon the Districts, which Districts said City Council hereby declares to be the Districts benefited by said improvement and maintenance, and to be further assessed to pay the costs and expenses thereof. Said Districts shall include each and every parcel of land within the boundaries of said Districts, as said Districts are shown on the maps as approved by this City Council and on file in the Records Management Department.

PUBLIC PROPERTY

SECTION 9. Any lots or parcels of land known as public property, as the same are defined in Section 22663 of Division 15, Part 2, of the Streets and Highways Code of the State of California, which are included within the boundaries of the Districts, shall be omitted and exempt from any assessment to be made under these proceedings to cover any of the costs and expenses of said improvement and maintenance work.

PUBLIC HEARING

SECTION 10. NOTICE IS HEREBY GIVEN THAT A PUBLIC HEARING WILL BE HELD THE 21st DAY OF JULY, 2020, AT THE HOUR OF 6:30 O'CLOCK P.M., IN THE REGULAR MEETING OF THE CITY COUNCIL, BEING IN THE COUNCIL CHAMBERS, WHICH IS THE TIME AND PLACE FIXED BY THIS CITY COUNCIL FOR THE HEARING OF PROTESTS OR OBJECTIONS IN REFERENCE TO THE ANNUAL LEVY OF ASSESSMENTS, TO THE EXTENT OF THE MAINTENANCE, BY PROPERTY OWNERS AFFECTED HEREBY AND ANY OTHER MATTERS CONTAINED IN THIS RESOLUTION; PROVIDED, THAT, IN THE EVENT THE JULY 21, 2020 CITY COUNCIL MEETING IS HELD VIA TELECONFERENCE AND/OR VIDEO CONFERENCE ONLY, THE MEANS BY WHICH THE PUBLIC MAY OBSERVE SUCH PUBLIC HEARING AND OFFER PUBLIC COMMENT SHALL BE PRESCRIBED IN THE NOTICE AND AGENDA FOR SUCH CITY COUNCIL MEETING. ANY PERSONS WHO WISH TO OBJECT TO THE PROCEEDINGS FOR THE ANNUAL LEVY SHOULD FILE A WRITTEN PROTEST WITH THE CITY CLERK PRIOR TO THE TIME SET AND SCHEDULED FOR SAID PUBLIC HEARING.

NOTICE

SECTION 11. That the City Clerk is hereby authorized and directed to publish, pursuant to Government Code Section 6061, a copy of this Resolution in the INLAND VALLEY DAILY BULLETIN, a newspaper of general circulation within said City, said publication shall be made one time and not less than ten (10) days before the date set for the Public Hearing.

SECTION 12. That this Resolution shall take effect immediately upon its adoption.

SECTION 13. For any and all information relating to the proceedings, protest procedure, any documentation and/or information of a procedural or technical nature, your attention is directed to the below listed person at the local agency or department so designated:

Jason Jacobsen
City of Ontario
303 East "B" Street
Ontario, California 91764
(909) 395-2253

The City Clerk of the City of Ontario shall certify as to the adoption of this Resolution.

PASSED, APPROVED, AND ADOPTED this 16th day of June 2020

PAUL S. LEON, MAYOR

ATTEST:

SHEILA MAUTZ, CITY CLERK

APPROVED AS TO LEGAL FORM:

COLE HUBER LLP
CITY ATTORNEY

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, SHEILA MAUTZ, City Clerk of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. 2020- was duly passed and adopted by the City Council of the City of Ontario at their regular meeting held June 16, 2020 by the following roll call vote, to wit:

AYES: COUNCIL MEMBERS:

NOES: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

SHEILA MAUTZ, CITY CLERK

(SEAL)

The foregoing is the original of Resolution No. 2020- duly passed and adopted by the Ontario City Council at their regular meeting held June 16, 2020.

SHEILA MAUTZ, CITY CLERK

(SEAL)

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, DECLARING ITS INTENTION TO PROVIDE FOR AN ANNUAL LEVY AND COLLECTION OF ASSESSMENTS FOR FISCAL YEAR 2020-21 IN ONTARIO PARKWAY MAINTENANCE ASSESSMENT DISTRICT NO. 4, PURSUANT TO THE PROVISIONS OF DIVISION 15, PART 2, OF THE STREETS AND HIGHWAYS CODE OF THE STATE OF CALIFORNIA, AND SETTING A TIME AND PLACE FOR A PUBLIC HEARING THEREON.

WHEREAS, the City Council of the City of Ontario, California, has previously formed a landscaping maintenance assessment district, and authorized the levy of assessments therein pursuant to the terms and provisions of the "Landscaping and Lighting Act of 1972," being Division 15, Part 2, of the Streets and Highways Code of the State of California (the "1972 Act"), Article XIID of the Constitution of the State of California ("Article XIID"), and the Proposition 218 Omnibus Implementation Act (Government Code Section 53750 and following) (the "Implementation Act") (the 1972 Act, Article XIID and the Implementation Act are referred to collectively as the "Assessment Law") in what is known and designated as

CITY OF ONTARIO
PARKWAY MAINTENANCE ASSESSMENT DISTRICT NO. 4
(the "District"); and

WHEREAS, the City Council previously undertook proceedings as required by the Assessment Law to consider the approval of the levy of (a) maximum annual assessments to finance the estimated costs of the maintenance of all of the improvements to ultimately be maintained upon the completion and acceptance thereof for maintenance, assessing the net amount upon all assessable lots and/or parcels within the District in proportion to the special benefits received; together with a formula pursuant to which such maximum assessment may be adjusted annually for inflation, and (b) an initial annual assessment of the estimated costs of the maintenance of those improvements to be maintained during such Fiscal Year, assessing the net amount upon all assessable lots and/or parcels within the District in proportion to the special benefits received; and

WHEREAS, upon the conclusion of the public hearing related to the proposed levy of the maximum annual assessments referred to in the preceding recital, the assessment ballots submitted in favor of the maximum annual assessments and the assessment ballots received in opposition to the levy of the maximum annual assessments were tabulated with assessment ballots weighted according to the proportional financial obligation of the affected properties; and

WHEREAS, as a result of the tabulation of the assessment ballots submitted, it was determined that the assessment ballots submitted, and not withdrawn, in favor of the proposed maximum annual assessment exceeded the assessment ballots submitted, and not withdrawn, in opposition to the levy of the maximum annual assessment; and

WHEREAS, this City Council has initiated proceedings to provide for the annual levy of assessments for the referenced Fiscal Year, to finance the costs and expenses necessary for continual maintenance of improvements within said District; and

WHEREAS, at this time, there has been presented and approved by this City Council, the Assessment Engineer's Report as required by the Assessment Law, and this City Council desires to conduct the proceedings to authorize said annual levy; and

WHEREAS, the proposed annual assessments for the referenced Fiscal Year as set forth in the Assessment Engineer's Report do not exceed the maximum annual assessments as previously authorized to be levied and, therefore, the proposed levy of assessments for the referenced Fiscal Year are not deemed to be "increased" over the maximum annual assessments.

NOW, THEREFORE, IT IS HEREBY RESOLVED as follows:

RECITALS

SECTION 1. That the above recitals are all true and correct.

IMPROVEMENTS AND MAINTENANCE

SECTION 2. That the public interest and convenience requires, and it is the intention of this City Council, to undertake proceedings for the annual levy and collection of special assessments for the continual maintenance of the improvements described below, all to service and specially benefit the properties within said District as said area is shown and delineated on a map as previously approved by this City Council and on file in the Records Management Department, open to public inspection, and herein so referenced and made a part hereof.

The improvements to be maintained include the landscaping, planting, shrubbery, trees, irrigation systems, hardscapes, fixtures, sidewalks within designated medians, parkways, other public rights-of-way and dedicated easements specially benefiting the properties within the District.

The maintenance of such landscaping shall include the furnishing of services and materials for the ordinary and usual maintenance and servicing of the improvements, including:

- (a) Repair, removal, or replacement of all or any part of any improvement;

- (b) Providing for the life, growth, health, and beauty of the improvements, including cultivation, irrigation, trimming, spraying, fertilizing, and/or treating for disease and injury; and
- (c) The removal of trimmings, rubbish, and debris.

The annual assessment for the referenced Fiscal Year shall be levied only for those improvements completed or projected to be completed and accepted by this City Council for maintenance prior to or during said Fiscal Year.

ASSESSMENT ENGINEER'S REPORT

SECTION 3. That the Assessment Engineer's Report regarding the annual levy for said District, which Report is for maintenance for the said Fiscal Year, is hereby approved and is directed to be filed in the Records Management Department. Reference is made to such Report for a full and detailed description of the improvements to be maintained, the boundaries of the District and any zones therein, and the proposed assessments upon assessable lots and parcels of land within the District.

ASSESSMENT

SECTION 4. That the public interest and convenience requires, and it is the intention of this City Council to order the annual assessment levy for the District as set forth and described in said Assessment Engineer's Report, and further it is determined to be in the best public interest and convenience to levy and collect annual assessments to pay the costs and expenses of said maintenance and improvement as estimated in the Assessment Engineer's Report.

BOUNDARIES OF DISTRICT

SECTION 5. Said contemplated maintenance work is in the opinion of this City Council, of direct benefit to the properties within the boundaries of the District, and this City Council makes the costs and expenses of said maintenance chargeable upon a District, which District said City Council hereby declares to be the District benefited by said improvement and maintenance, and to be further assessed to pay the costs and expenses thereof. Said District shall include each and every parcel of land within the boundaries of said District, as said District is shown on a map as approved by this City Council and on file in the Records Management Department, and so designated by the name of the District.

PUBLIC HEARING

SECTION 6. NOTICE IS HEREBY GIVEN THAT A PUBLIC HEARING WILL BE HELD THE 21st DAY OF JULY, 2020, AT THE HOUR OF 6:30 O'CLOCK P.M., IN THE REGULAR MEETING OF THE CITY COUNCIL, BEING IN THE COUNCIL CHAMBERS, WHICH IS THE TIME AND PLACE FIXED BY THIS CITY COUNCIL FOR THE HEARING OF PROTESTS OR OBJECTIONS IN REFERENCE TO THE ANNUAL LEVY OF ASSESSMENTS, TO THE EXTENT OF THE MAINTENANCE, BY ANY INTERESTED PERSON AND ANY OTHER MATTERS CONTAINED IN THIS RESOLUTION; PROVIDED, THAT, IN THE EVENT THE JULY 21, 2020 CITY COUNCIL MEETING IS HELD VIA TELECONFERENCE AND/OR VIDEO CONFERENCE ONLY, THE MEANS BY WHICH THE PUBLIC MAY OBSERVE SUCH PUBLIC HEARING AND OFFER PUBLIC COMMENT SHALL BE PRESCRIBED IN THE NOTICE AND AGENDA FOR SUCH CITY COUNCIL MEETING. ANY PERSONS WHO WISH TO OBJECT TO THE PROCEEDINGS FOR THE ANNUAL LEVY SHOULD FILE A WRITTEN PROTEST WITH THE CITY CLERK PRIOR TO THE TIME SET AND SCHEDULED FOR SAID PUBLIC HEARING.

NOTICE

SECTION 7. That the City Clerk is hereby authorized and directed to publish, pursuant to Government Code Section 6061, a copy of this Resolution in the INLAND VALLEY DAILY BULLETIN, a newspaper of general circulation within said City, said publication shall be made one time and not less than ten (10) days before the date set for the Public Hearing.

EFFECTIVE DATE OF RESOLUTION

SECTION 8. That this Resolution shall take effect immediately upon its adoption.

PROCEEDING INQUIRIES

SECTION 9. For any and all information relating to the proceedings, protest procedure, any documentation and/or information of a procedural or technical nature, your attention is directed to the below listed person at the local agency or department so designated:

Jason Jacobsen
City of Ontario
303 East "B" Street
Ontario, California 91764
(909) 395-2253

The City Clerk of the City of Ontario shall certify as to the adoption of this Resolution.

PASSED, APPROVED, AND ADOPTED this 16th day of June 2020.

PAUL S. LEON, MAYOR

ATTEST:

SHEILA MAUTZ, CITY CLERK

APPROVED AS TO LEGAL FORM:

COLE HUBER, LLP
CITY ATTORNEY

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, SHEILA MAUTZ, City Clerk of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. 2020- was duly passed and adopted by the City Council of the City of Ontario at their regular meeting held June 16, 2020 by the following roll call vote, to wit:

AYES: COUNCIL MEMBERS:

NOES: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

SHEILA MAUTZ, CITY CLERK

(SEAL)

The foregoing is the original of Resolution No. 2020- duly passed and adopted by the Ontario City Council at their regular meeting held June 16, 2020.

SHEILA MAUTZ, CITY CLERK

(SEAL)

CITY OF ONTARIO

Agenda Report
June 16, 2020

SECTION:
CONSENT CALENDAR

SUBJECT: ANNUAL LEVY OF ASSESSMENTS WITHIN STREET LIGHTING MAINTENANCE DISTRICT NOS. 1 AND 2 FOR FISCAL YEAR 2020-21

RECOMMENDATION: That the City Council adopt resolutions pertaining to the levy of assessments within Street Lighting Maintenance District (SLMD) Nos. 1 and 2:

- (A) A resolution initiating proceedings and ordering the preparation of an Engineer's Report for each district relating to the annual levy of assessments;
- (B) A resolution giving preliminary approval of the Engineer's Reports; and
- (C) A resolution for each district declaring the City's intention to levy the special assessments for Fiscal Year 2020-21 and setting the date of July 21, 2020 for a Public Hearing on each district in accordance with the Landscaping and Lighting Act of 1972.

**COUNCIL GOALS: Operate in a Businesslike Manner
Focus Resources on Ontario's Commercial and Residential Neighborhoods**

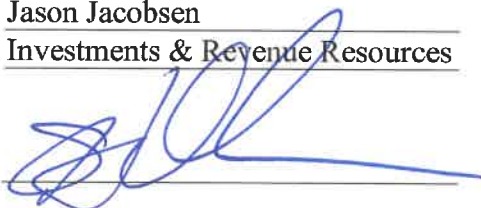
FISCAL IMPACT: As proposed, special assessment revenue will be generated in the amount of \$300,045 from SLMD No. 1 and \$92,752 from SLMD No. 2 during Fiscal Year 2020-21. A portion of the facilities within SLMD No. 1 provide general benefit to the public at large and cannot be funded with the assessment. The amount of general benefit is \$7,986.

BACKGROUND: Street Lighting Maintenance District No. 1 was formed in 1984, and Street Lighting Maintenance District No. 2 was formed in 1999, pursuant to the Landscaping and Lighting Act of 1972 (the "1972 Act"), to help minimize the continually increasing cost for maintaining and operating the City's street lighting system. A special assessment is levied annually on benefiting properties for the cost of maintenance and operation of certain street lighting facilities within the districts. The locations of the districts are shown on the attached maps.

STAFF MEMBER PRESENTING: Armen Harkalyan, Executive Director of Finance

Prepared by: Jason Jacobsen
Department: Investments & Revenue Resources

City Manager
Approval:



Submitted to Council/O.H.A. 06/16/2020

Approved: _____

Continued to: _____

Denied: _____

10

The City's Public Works Department establishes the annual maintenance costs and maintains the street lighting system. An Engineer's Report for each district must be prepared annually, apportioning the costs to each parcel within the districts. The districts are comprised of commercial, industrial and some multi-family residential properties. Due to varying characteristics, the districts are organized into benefit zones and/or maintenance areas. Each zone/maintenance area is assessed for only the operation and maintenance of the facilities from which it receives direct special benefit. The total assessment for each zone/maintenance area is then apportioned to its constituent parcels. Therefore, the assessments are unique and specific to each individual parcel of property.

The total assessment during tax year 2019-20 was \$422,888 in SLMD No. 1, and \$102,365 in SLMD No. 2. The City's proposed Fiscal Year 2020-21 levy for the two Street Light maintenance districts is based on the previous year's actual expenses. The proposed total assessments for tax year 2020-21 are \$300,045 in SLMD No. 1, and \$92,752 in SLMD No. 2. A portion of the facilities within SLMD No. 1 provide general benefit to the public at large and cannot be funded with the assessment. The amount of this general benefit is \$7,986 for Fiscal Year 2020-21. The assessed tax rates for Fiscal Year 2019-20, and the proposed and maximum tax rates for Fiscal Year 2020-21, are as follows:

		Prior Year 2019-20	Maximum 2020-21	Proposed 2020-21
SLMD No. 1, Zone 2000-1	per AU	\$ 147	\$ 155	\$ 103
SLMD No. 1, Zone 2000-2	per AU	\$ 74	\$ 127	\$ 60
SLMD No. 2, MA# 99-1		\$ 4,551	\$ 11,076	\$ 4,242
SLMD No. 2, MA# 99-2		\$ 2,351	\$ 5,508	\$ 2,261
SLMD No. 2, MA# 99-3		\$ 4,697	\$ 9,178	\$ 4,523
SLMD No. 2, MA# 99-4		\$ 1,573	\$ 3,676	\$ 1,508
SLMD No. 2, MA# 99-5		\$ 1,436	\$ 3,446	\$ 1,247
SLMD No. 2, MA# 99-6		\$ 14,065	\$ 32,166	\$ 12,213
SLMD No. 2, MA# 99-7		\$ 422	\$ 995	\$ 409
SLMD No. 2, MA# 99-8		\$ 3,135	\$ 7,343	\$ 3,016
SLMD No. 2, MA# 00-1		\$ 3,000	\$ 3,379	\$ 2,755
SLMD No. 2, MA# 00-2		\$ 1,573	\$ 2,254	\$ 1,507
SLMD No. 2, MA# 00-3		\$ 8,602	\$ 12,390	\$ 8,292
SLMD No. 2, MA# 00-4		\$ 56,959	\$ 70,951	\$ 50,781

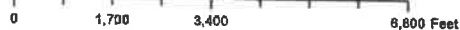
AU-Assessment Unit (e.g. – Zone 2000-1: 1 vacant acre = 0.25 AU; Zone 2000-2: 1 vacant acre = 1 AU)

Pursuant to the "1972 Act," Engineer's Reports have been prepared for each district, and are on file with the Records Management Department. Each report includes plans and specifications for the improvements being operated and maintained, an estimate of costs, a diagram of the area proposed to be assessed, and an assessment of the costs to the various parcels within the districts.

A public hearing is required for each district prior to adoption of the proposed assessments. A combined public hearing will be held on July 21, 2020 for the purpose of accepting comments and written protests. At the conclusion of the public hearing the City Council will consider adoption of resolutions confirming the assessments and ordering the levy of the assessments for tax year 2020-21. Notice of the public hearing for the levy of assessments will be published in the local newspaper, in accordance with the applicable statutes



Street Light Maintenance District 1 Vicinity Map



Legend

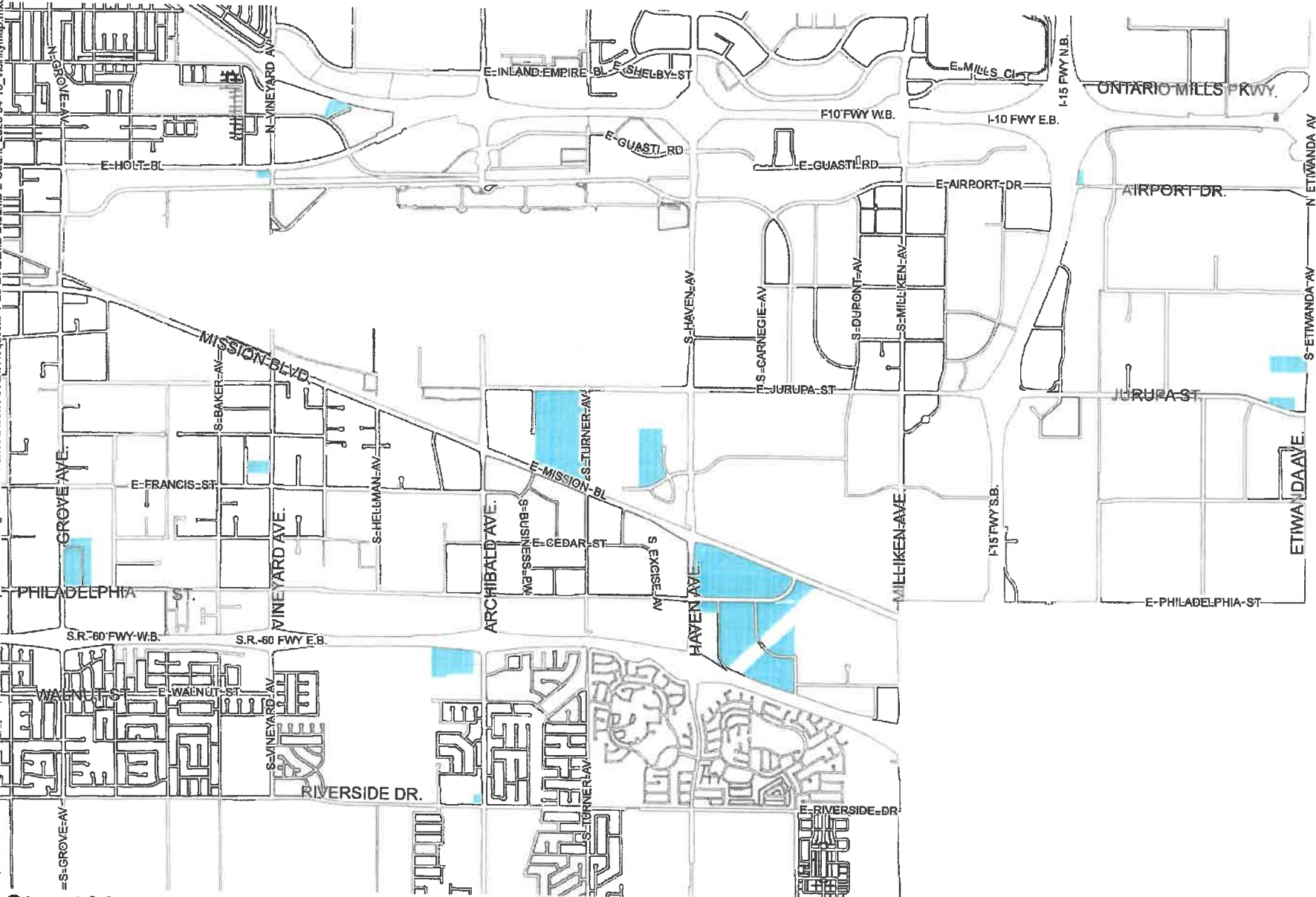
- Zone 2000-1 (Safety Light: 310; Cobrahead: 1363)
- Zone 2000-2 (Safety Light: 14; Cobrahead: 43)

Total Lights: 1730 (Safety Lights: 324; Cobrahead: 1406)



Note:
Reference is hereby made to the Maps of The Assessor of the County of San Bernardino, California, for a detailed description of the lines and dimensions of the parcels shown hereon.

Date: 04/16/2020



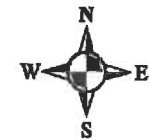
Street Light Maintenance District 2 Vicinity Map



Legend

Maintenance District 2

Total Lights: 141; Safety Lights: 18; Cobrahead: 123



Note:
Reference is hereby made to the Maps of The Assessor of the County of San Bernardino, California, for a detailed description of the lines and dimensions of the parcels shown hereon.

Date: 04/16/2020

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, ORDERING THE PREPARATION OF THE ASSESSMENT ENGINEER'S REPORTS PURSUANT TO THE PROVISIONS OF DIVISION 15, PART 2, OF THE STREETS AND HIGHWAYS CODE OF THE STATE OF CALIFORNIA, FOR PROCEEDINGS FOR THE ANNUAL ASSESSMENT LEVY WITHIN ONTARIO STREET LIGHTING MAINTENANCE DISTRICTS NO. 1 AND NO. 2 FOR FISCAL YEAR 2020-21.

WHEREAS, the City Council of the City of Ontario, California, previously formed maintenance districts pursuant to the terms and provisions of the "Landscaping and Lighting Act of 1972," being Division 15, Part 2, of the Streets and Highways Code of the State of California (the "1972 Act"), known and designated as

CITY OF ONTARIO
STREET LIGHTING MAINTENANCE DISTRICTS NO. 1 AND 2
(the "Districts"); and

WHEREAS, at this time the City Council desires to initiate proceedings to provide for the annual levy of assessments for the referenced Fiscal Year to provide for the annual costs for maintenance of improvements within the Districts; and

WHEREAS, the proceedings for the annual levy of assessments shall relate to the Fiscal Year commencing July 1, 2020, and ending June 30, 2021; and

WHEREAS, there has been submitted to this City Council for its consideration at this time, maps showing the boundaries of the areas affected by the levy of the assessments for the above-referenced Fiscal Year, and inventory maps showing and describing in general the works of improvement proposed to be maintained in said Districts, said description being sufficient to identify the works of improvement and the areas proposed to be assessed for said maintenance thereof.

NOW, THEREFORE, IT IS HEREBY RESOLVED as follows:

SECTION 1. That the above recitals are all true and correct.

SECTION 2. That the maps entitled

CITY OF ONTARIO
STREET LIGHTING MAINTENANCE DISTRICTS NO. 1 AND 2
BOUNDARY MAP AND INVENTORY MAPS

attached hereto, as submitted to this City Council, showing the boundaries of the proposed areas to be assessed and showing the works of improvement to be maintained, are hereby approved, and a copy thereof shall be on file in the Records

Management Department and open to public inspection. The proposed parcels and properties within said areas are those to be assessed to pay the costs and expenses for said maintenance work.

SECTION 3. That the proposed maintenance work within the areas proposed to be assessed shall be for street lighting and signal and appurtenant improvements, as said maintenance work is set forth in the Reports to be presented to this City Council for consideration.

SECTION 4. That the Assessment Engineer, Harris & Associates, is hereby ordered to prepare and file with this City Council the Assessment Engineer's Reports relating to said annual assessment and levy in accordance with the provisions of 1972 Act, Article XIID of the Constitution of the State of California ("Article XIID"), and the Proposition 218 Omnibus Implementation Act (Government Code Section 53750 and following) (the "Implementation Act") (the 1972 Act, Article XIID and the Implementation Act are referred to collectively as the "Assessment Law").

SECTION 5. That upon completion, said Assessment Engineer's Reports shall be filed with the City Clerk, who shall then submit the same to this City Council for its consideration pursuant to the Assessment Law.

The City Clerk of the City of Ontario shall certify as to the adoption of this Resolution.

PASSED, APPROVED, AND ADOPTED this 16th day of June 2020.

PAUL S. LEON, MAYOR

ATTEST:

SHEILA MAUTZ, CITY CLERK

APPROVED AS TO LEGAL FORM:

COLE HUBER LLP
CITY ATTORNEY

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, SHEILA MAUTZ, City Clerk of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. 2020- was duly passed and adopted by the City Council of the City of Ontario at their regular meeting held June 16, 2020 by the following roll call vote, to wit:

AYES: COUNCIL MEMBERS:

NOES: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

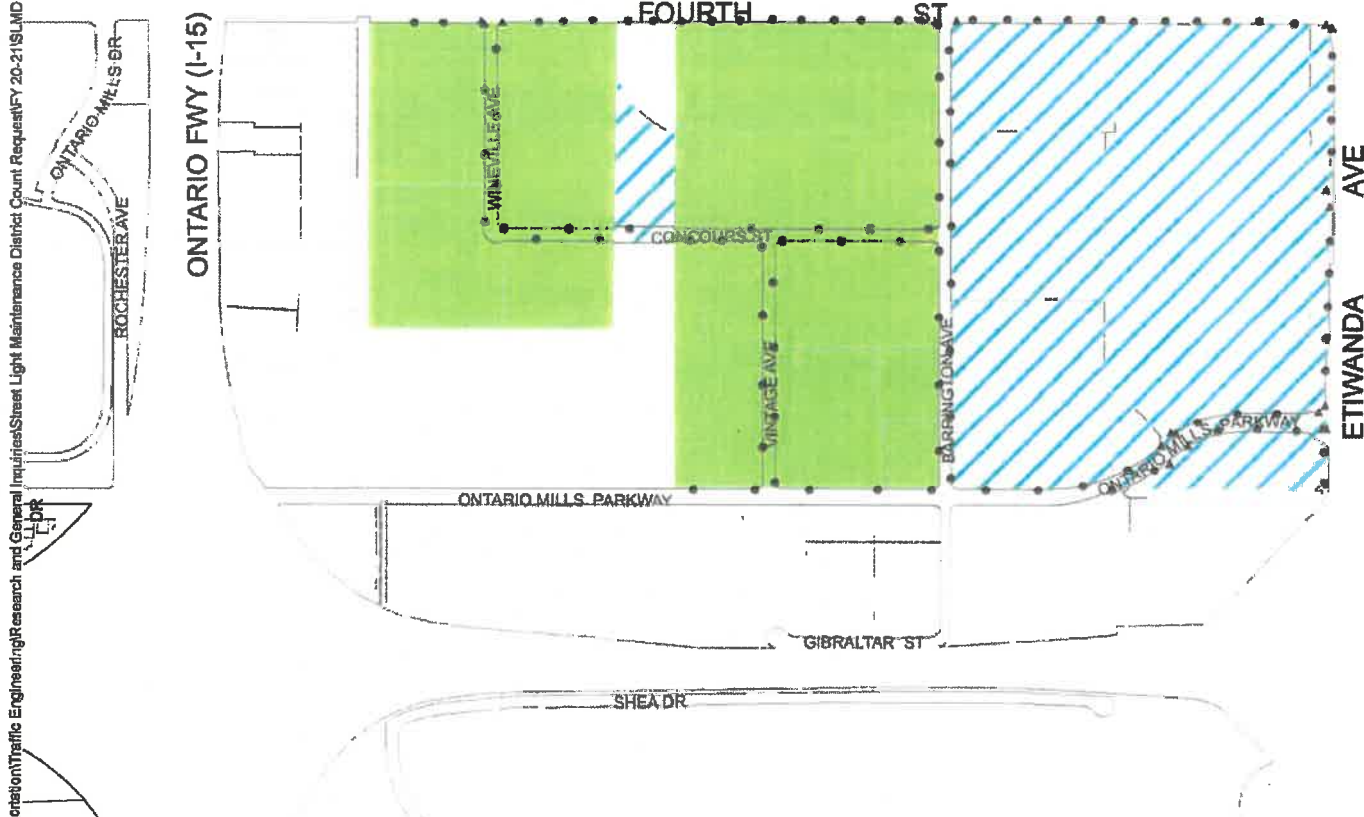
SHEILA MAUTZ, CITY CLERK

(SEAL)

The foregoing is the original of Resolution No. 2020- duly passed and adopted by the Ontario City Council at their regular meeting held June 16, 2020.

SHEILA MAUTZ, CITY CLERK

(SEAL)



Street Light Maintenance District 1 Boundary and Inventory Map



Legend

- ▲ Safety Light
- Conventional
- Zone 2000-1
- ▨ Zone 2000-2



Note:
Reference is hereby made to the Maps of The Assessor of the County of San Bernardino, California, for a detailed description of the lines and dimensions of the parcels shown hereon.

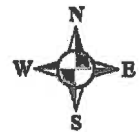


Street Light Maintenance District 1 Boundary and Inventory Map



Legend

- ▲ Safety Light
- Conventional
- Zone 2000-1

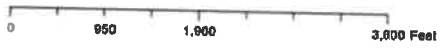


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description of the lines and dimensions
of the parcels shown hereon.



S:\Engineering\Traffic & Transportation\Traffic Engineering\Research and General Inquiries\Street Light Maintenance District Court Request\FY 20-21\SLMD 1\SLMD 1 Court_2020-04-16_mapbook.mxd

Street Light Maintenance District 1 Boundary and Inventory Map

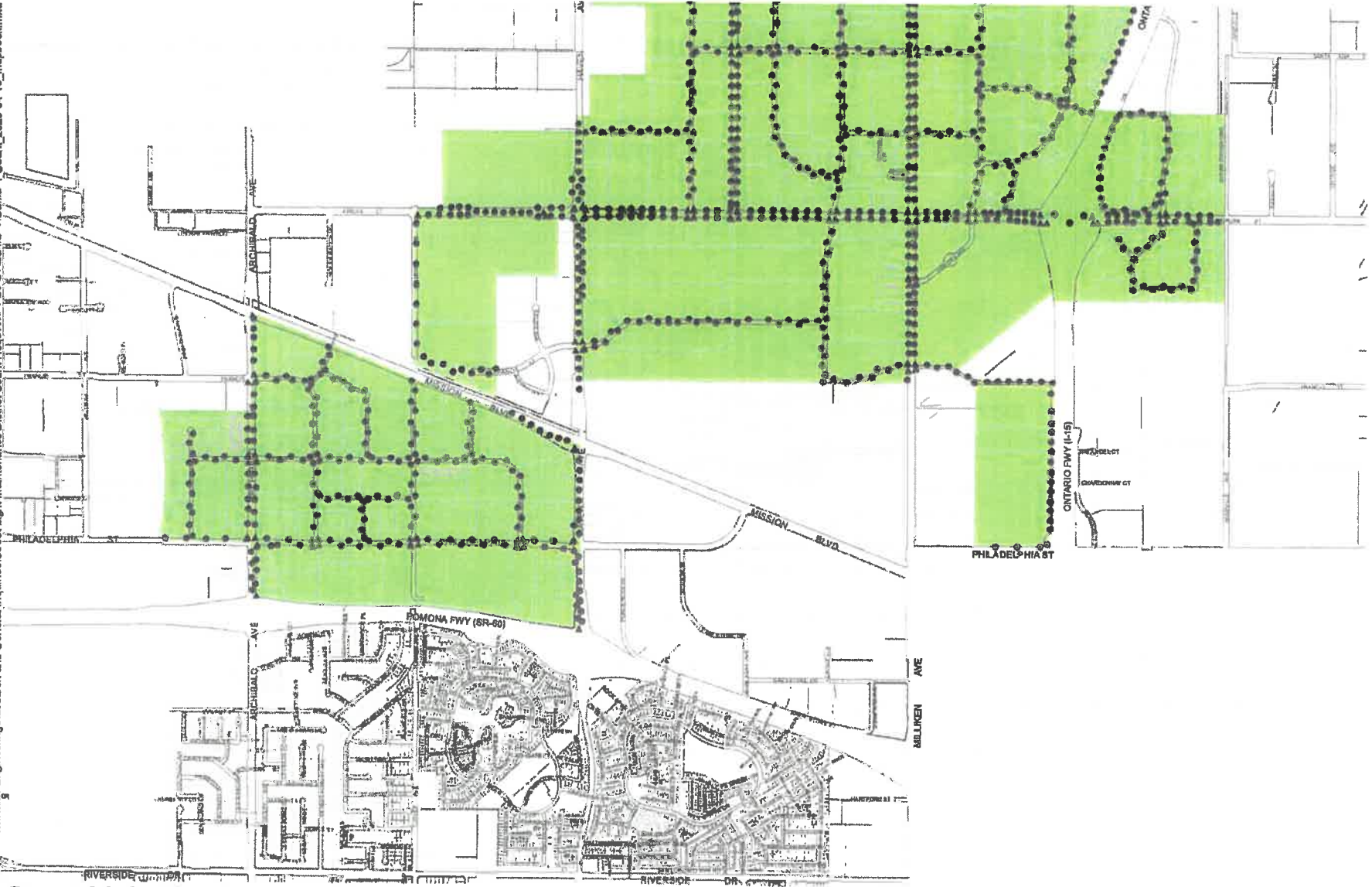


- Legend**
- ▲ Safety Light
 - Conventional
 - Zone 2000-1



Note:
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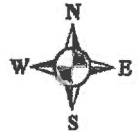
Date: 04/16/2020



Street Light Maintenance District 1 Boundary and Inventory Map



- Legend**
- ▲ Safety Light
 - Conventional
 - Zone 2000-1



Note:
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Street Light Maintenance District 1 Boundary and Inventory Map



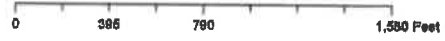
Legend

- ▲ Safety Light
- Conventional
- Zone 2000-1



Note:
Reference is hereby made to the Maps of The Assessor of the County of San Bernardino, California, for a detailed description of the lines and dimensions of the parcels shown hereon.

Street Light Maintenance District 2 Boundary and Inventory Map



- Legend**
- ▲ Safety Light
 - Conventional
 - Maintenance District 2



Note:
Reference is hereby made to the
Maps of The Assessor of the County of
San Bernardino, California, for a detailed
description of the lines and dimensions
of the parcels shown hereon.

Date: 04/16/2020

Page 1 of 9



Street Light Maintenance District 2 Boundary and Inventory Map



Legend

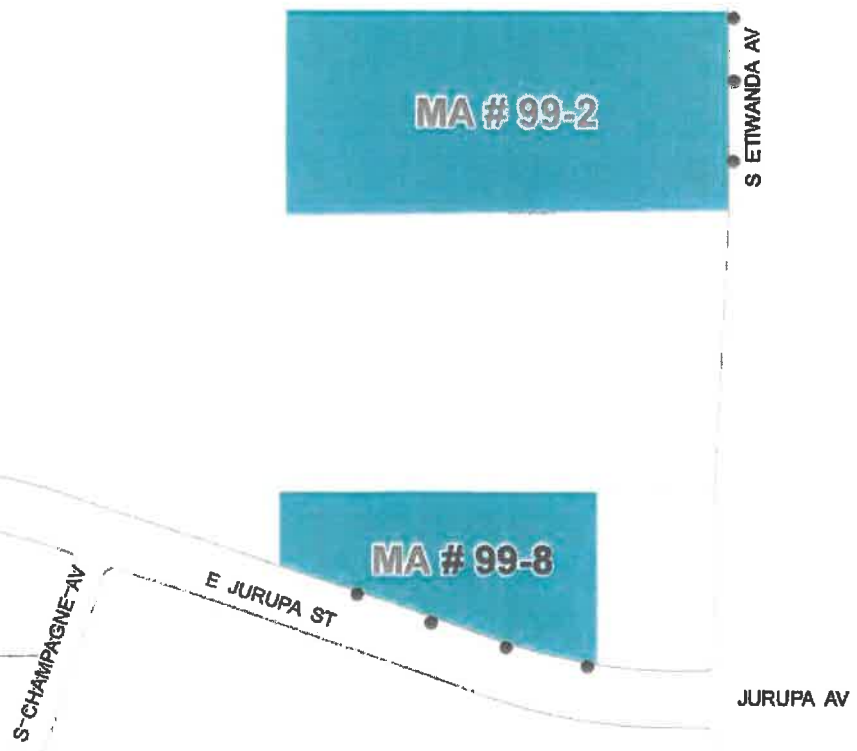
- Conventional
- Maintenance District 2

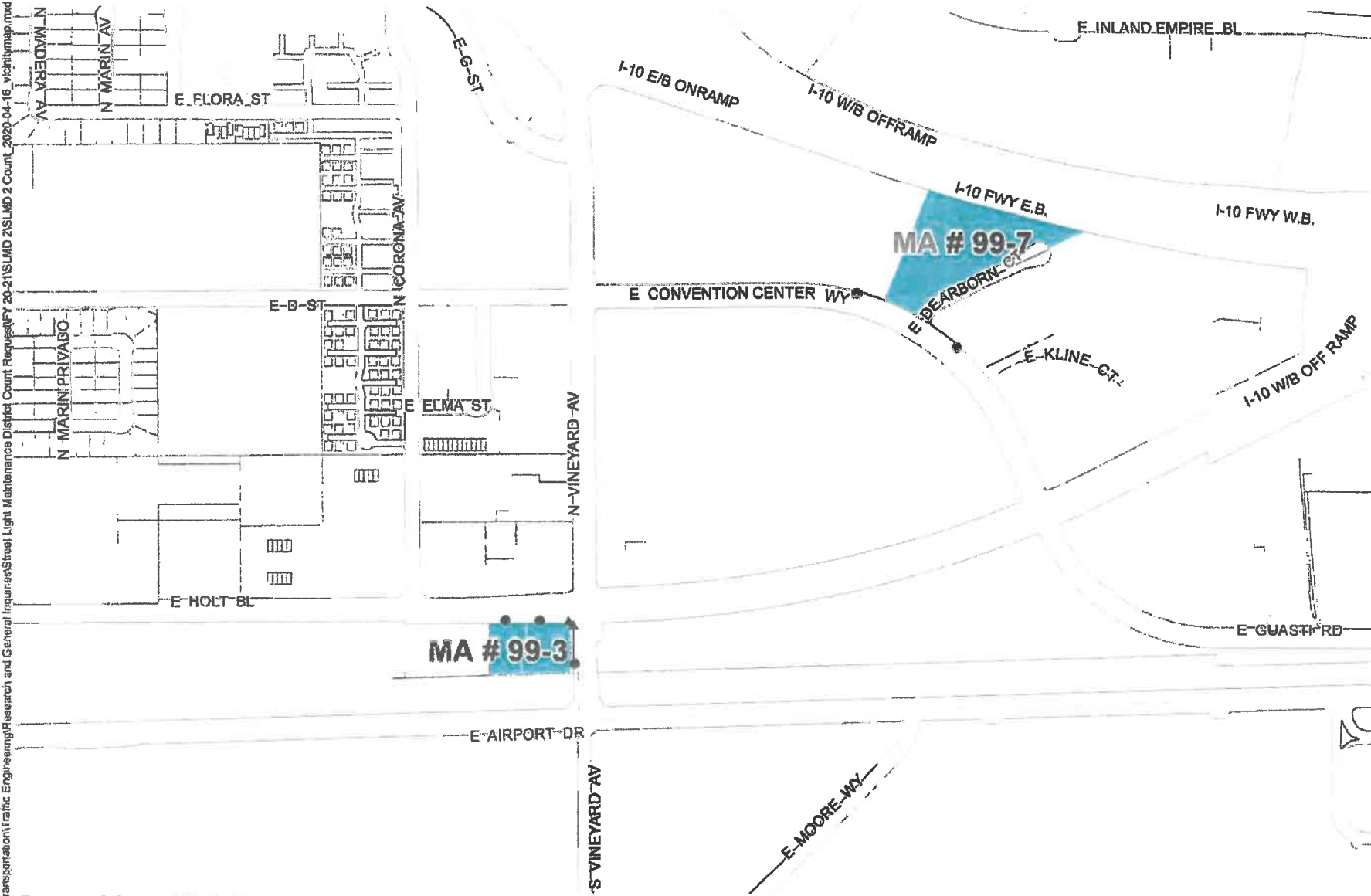


Note:
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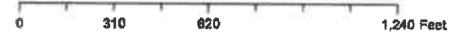
Date: 04/16/2020

Page 2 of 9





Street Light Maintenance District 2 Boundary and Inventory Map



- Legend**
- ▲ Safety Light
 - Conventional
 - Maintenance District 2



Note:
Reference is hereby made to the Maps of The Assessor of the County of San Bernardino, California, for a detailed description of the lines and dimensions of the parcels shown hereon.

Street Light Maintenance District 2 Boundary and Inventory Map



Legend

- Conventional
- Maintenance District 2



Note:
Reference is hereby made to the
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San Bernardino, California, for a detailed
description of the lines and dimensions
of the parcels shown hereon.

S.R.-60 FWY E.B.

S.R.-60 FWY W.B.

SR-60 W/B ONRAMP

SR-60 W/B OFFRAMP

SR-60 E/B OFFRAMP

SR-60 E/B ONRAMP

MA # 99-4

E-WALNUT-ST

E-RICHMOND-ST

S-COLONIAL-AV

S DOVER PL

S ARCHIBALD AV

E-TAM-O'SHANTER LCT

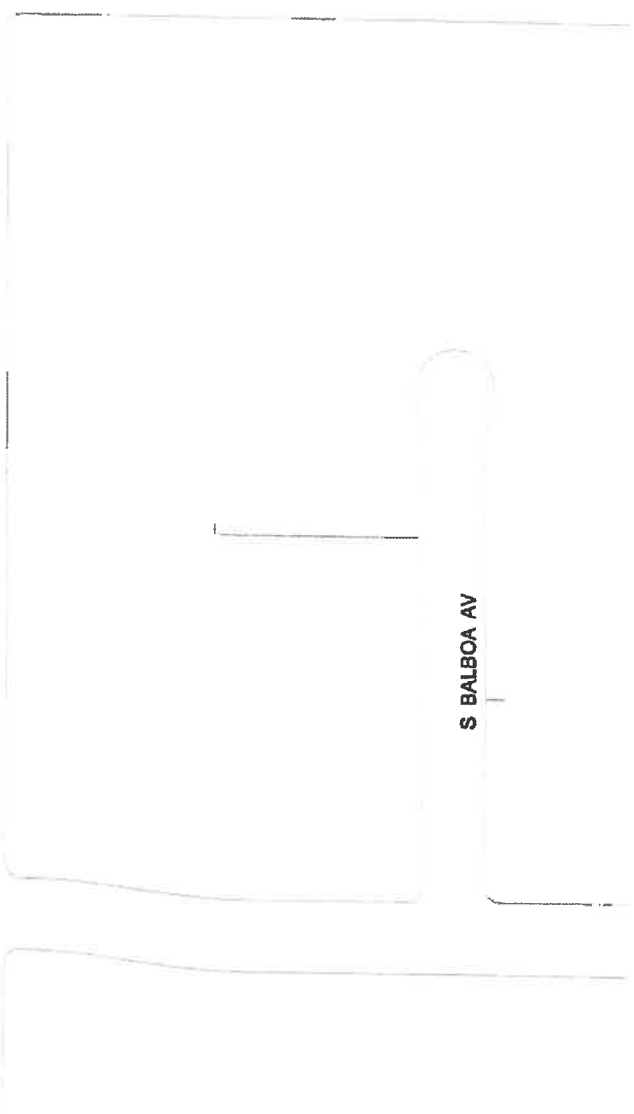
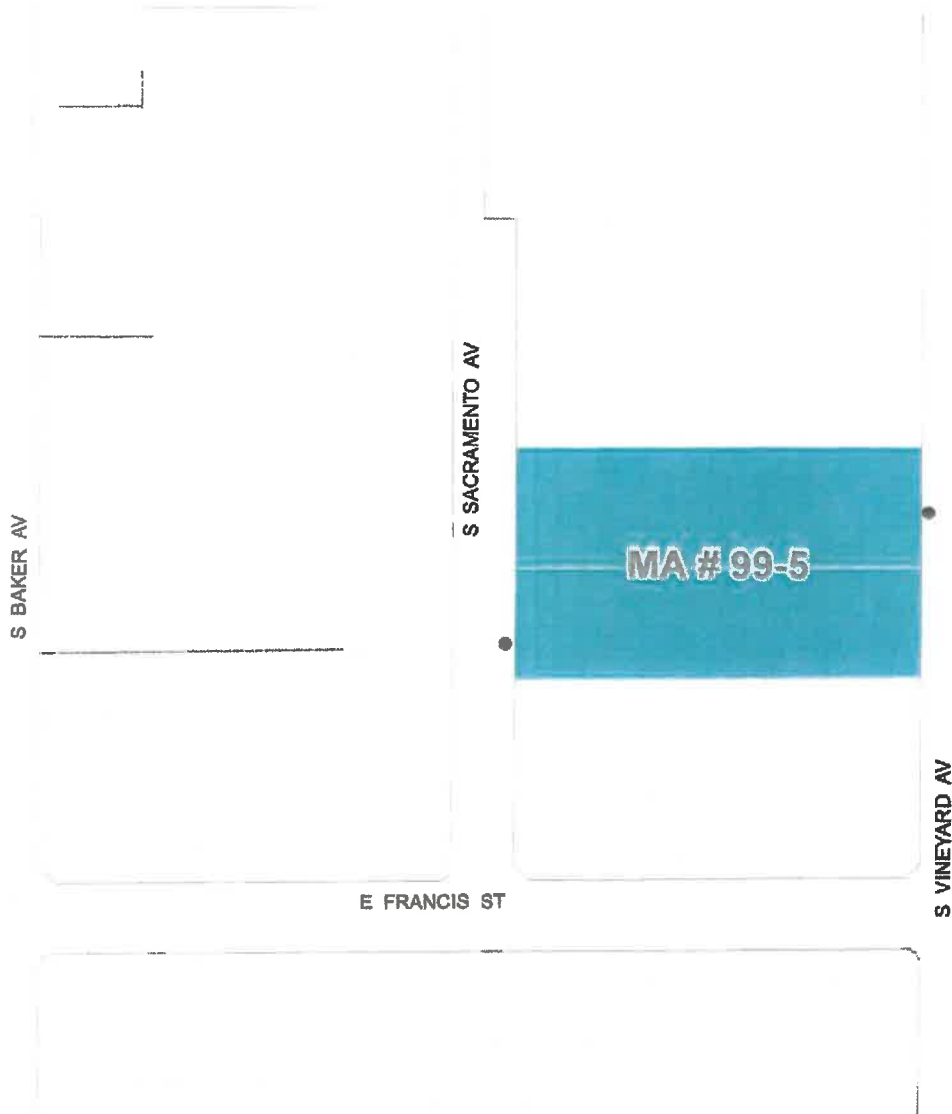
E-FAIRFIELD-ST

E-OAK-HILL DR

S-SEAGULL-AV

NORTH SHORES PL

S-WOODLARK-DR



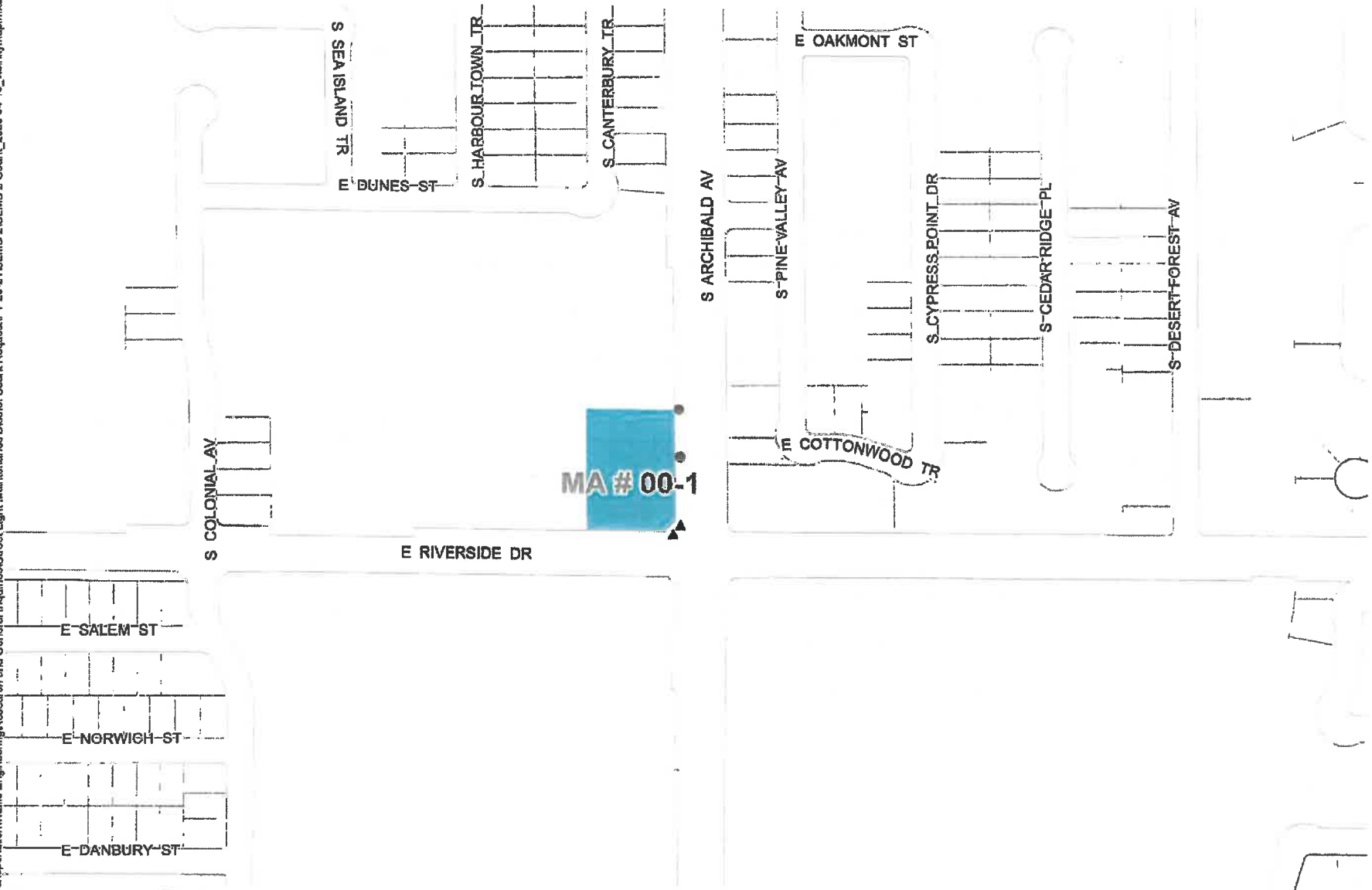
Street Light Maintenance District 2 Boundary and Inventory Map



- Legend**
- Conventional
 - Maintenance District 2



Note:
Reference is hereby made to the Maps of The Assessor of the County of San Bernardino, California, for a detailed description of the lines and dimensions of the parcels shown hereon.



Street Light Maintenance District 2 Boundary and Inventory Map



- Legend**
- ▲ Safety Light
 - Conventional
 - Maintenance District 2



Note:
Reference is hereby made to the Maps of The Assessor of the County of San Bernardino, California, for a detailed description of the lines and dimensions of the parcels shown hereon.

S:\Engineering\Traffic & Transportation\Traffic Engineering\Research and General Inquiries\Street Light Maintenance District Count Request\FY 20-21\SLMD 2\SLMD 2 Count_2020-04-16_vicinitymap.mxd

I-10 FWY E.B.

I-10 FWY W.B.

E-SHEA-CENTER-DR

I-15 FWY S.B.

E-GUASTI RD

I-15 FWY N.B.

MA # 00-2

N WINEVILLE AV

E AIRPORT DR

S-WANAMAKER AV

E-BRICKELL-ST

S ROCHESTER AV

S KETTERING DR

"drainage road"

S WINEVILLE AV

Street Light Maintenance District 2 Boundary and Inventory Map



Legend

- Conventional
- Maintenance District 2



Note:
Reference is hereby made to the Maps of The Assessor of the County of San Bernardino, California, for a detailed description of the lines and dimensions of the parcels shown hereon.

Street Light Maintenance District 2 Boundary and Inventory Map



Legend

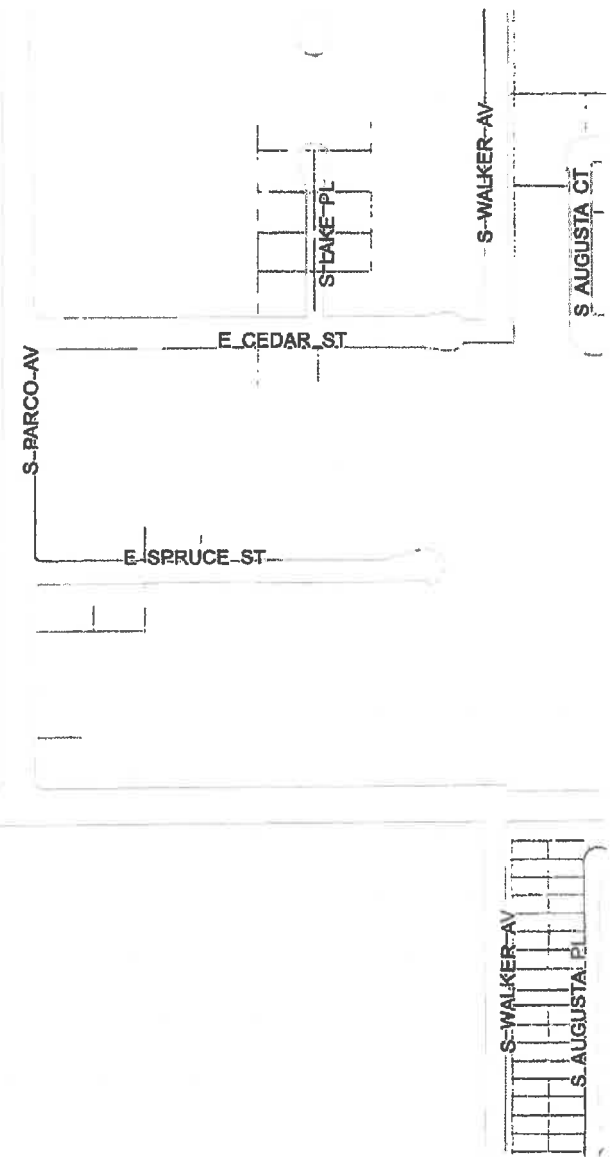
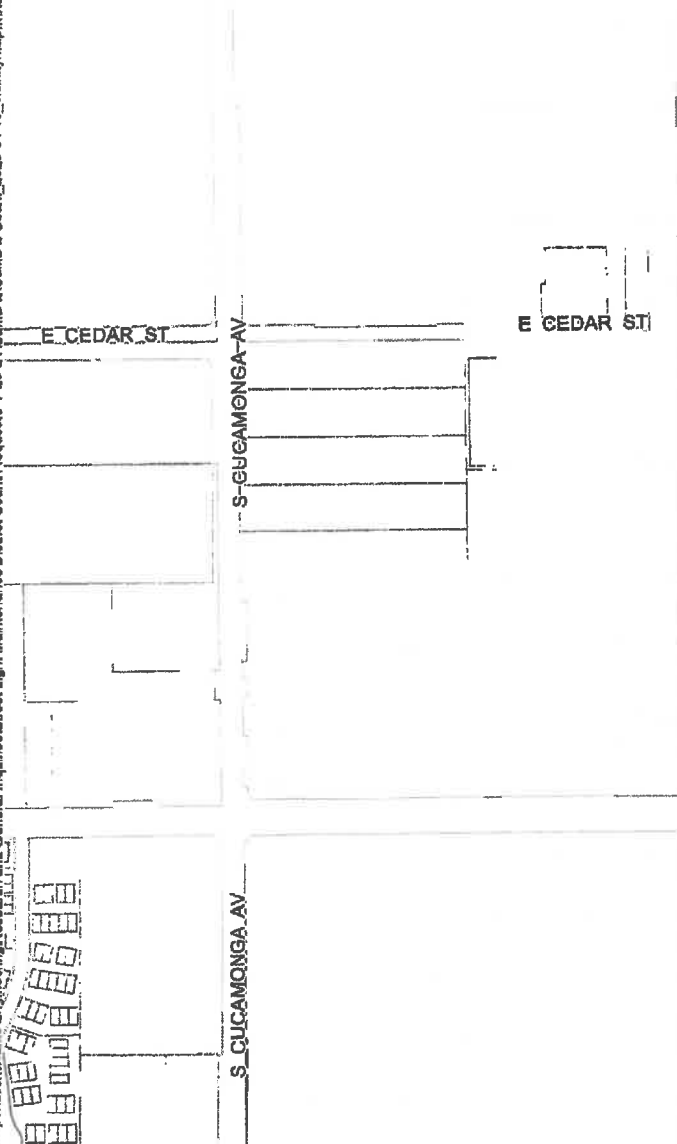
- ▲ Safety Light
- Conventional
- Maintenance District 2

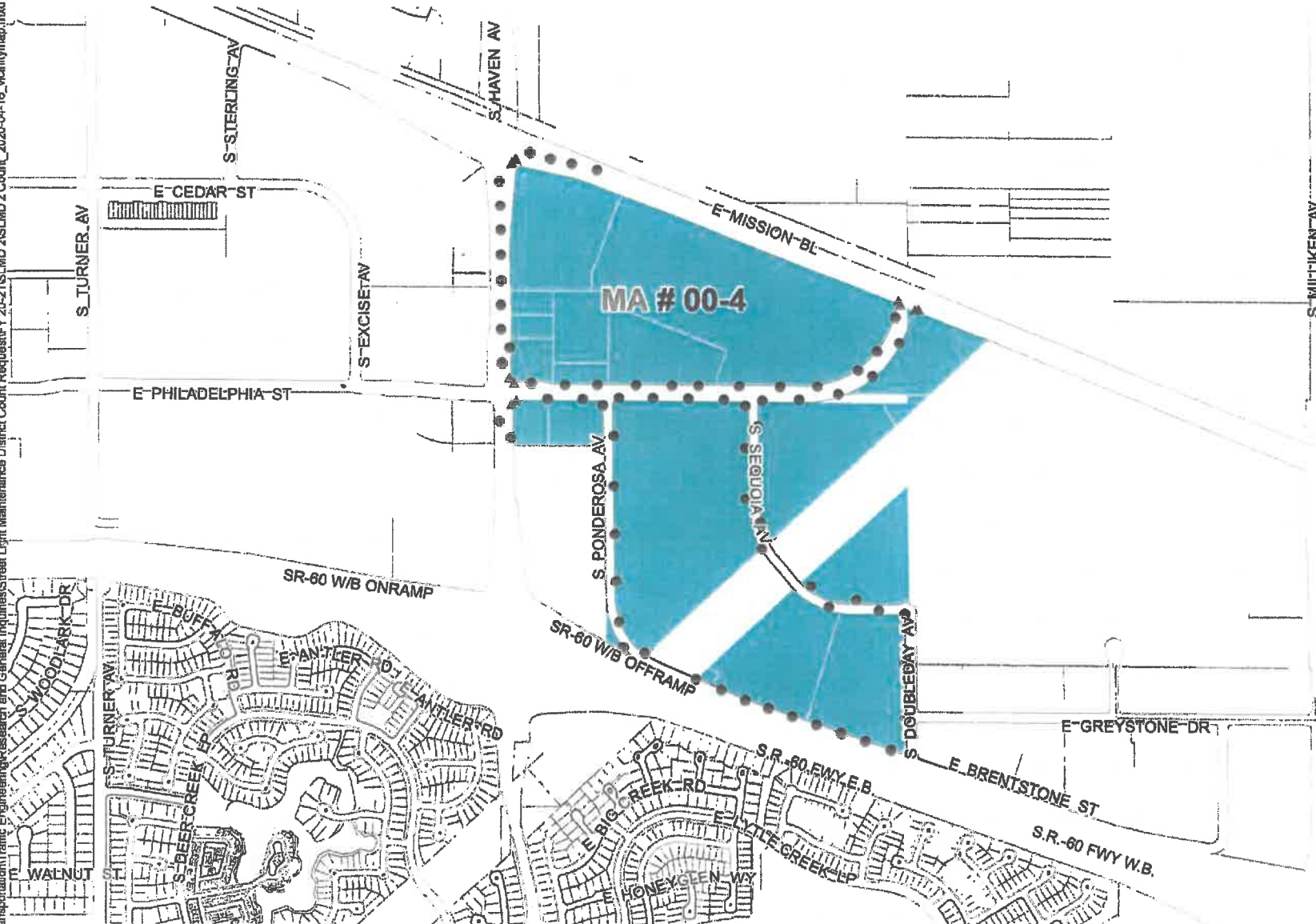


Note:
Reference is hereby made to the
Maps of The Assessor of the County of
San Bernardino, California, for a detailed
description of the lines and dimensions
of the parcels shown hereon.

Date: 04/16/2020

Page 8 of 9





**Street Light Maintenance District 2
Boundary and Inventory Map**

- Legend**
- ▲ Safety Light
 - Conventional
 - Maintenance District 2



Note:
Reference is hereby made to the Maps of The Assessor of the County of San Bernardino, California, for a detailed description of the lines and dimensions of the parcels shown hereon.



0 400 800 1,900 Feet

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING THE ASSESSMENT ENGINEER'S REPORTS FOR THE ANNUAL LEVY OF ASSESSMENTS WITHIN ONTARIO STREET LIGHTING MAINTENANCE DISTRICTS NO. 1 AND NO. 2 FOR FISCAL YEAR 2020-21.

WHEREAS, the City Council of the City of Ontario, California, pursuant to the provisions of Division 15, Part 2, of the Streets and Highways Code of the State of California (the "1972 Act"), Article XIID of the Constitution of the State of California ("Article XIID"), and the Proposition 218 Omnibus Implementation Act (Government Code Section 53750 and following) (the "Implementation Act") (the 1972 Act, Article XIID and the Implementation Act are referred to collectively as the "Assessment Law"), did, by previous Resolution, order the preparation of the Assessment Engineer's Reports for the annual levy of assessments for the referenced Fiscal Year in the maintenance assessment districts known and designated as

CITY OF ONTARIO
STREET LIGHTING MAINTENANCE DISTRICTS NO. 1 AND 2
(the "Districts"); and

WHEREAS, there has now been presented to this City Council the Assessment Engineer's Reports as required by the Assessment Law and as previously directed by Resolution; and

WHEREAS, this City Council has now examined and reviewed the Assessment Engineer's Reports as presented, and is satisfied with each and all of the items and documents as set forth therein, and is satisfied that the assessments on a preliminary basis, have been assessed in accordance with the special benefits received from the improvements to be maintained, as set forth in said Assessment Engineer's Reports.

NOW, THEREFORE, IT IS HEREBY RESOLVED as follows:

SECTION 1. That the above recitals are all true and correct.

SECTION 2. That the Assessment Engineer's Reports as presented, consisting of the following:

- A. Plans and specifications describing the general nature, location, and extent of the improvements to be maintained and the extent of such maintenance for each District;
- B. An estimate of the cost of the maintenance of the improvements for each District for the referenced Fiscal Year;

- C. A diagram for each District, showing the area and properties proposed to be assessed; and
- D. An annual assessment within each District for the referenced Fiscal Year of the estimated costs of the maintenance of those improvements to be maintained during such Fiscal Year, assessing the net amount upon all assessable lots and/or parcels within each District in proportion to the special benefits received;

are hereby approved on a preliminary basis and are ordered to be filed in the Records Management Department as a permanent record and to remain open to public inspection.

SECTION 3. That the City Clerk shall certify to the passage and adoption of this Resolution, and the minutes of this meeting shall so reflect the presentation of the Assessment Engineer's Reports.

PASSED, APPROVED, AND ADOPTED this 16th day of June 2020.

PAUL S. LEON, MAYOR

ATTEST:

SHEILA MAUTZ, CITY CLERK

APPROVED AS TO LEGAL FORM:

COLE HUBER LLP
CITY ATTORNEY

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, SHEILA MAUTZ, City Clerk of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. 2020- was duly passed and adopted by the City Council of the City of Ontario at their regular meeting held June 16, 2020 by the following roll call vote, to wit:

AYES: COUNCIL MEMBERS:

NOES: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

SHEILA MAUTZ, CITY CLERK

(SEAL)

The foregoing is the original of Resolution No. 2020- duly passed and adopted by the Ontario City Council at their regular meeting held June 16, 2020.

SHEILA MAUTZ, CITY CLERK

(SEAL)

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, DECLARING ITS INTENTION TO PROVIDE FOR AN ANNUAL LEVY AND COLLECTION OF ASSESSMENTS FOR FISCAL YEAR 2020-21 IN ONTARIO STREET LIGHTING MAINTENANCE DISTRICT NO. 1 PURSUANT TO THE PROVISIONS OF DIVISION 15, PART 2, OF THE STREETS AND HIGHWAYS CODE OF THE STATE OF CALIFORNIA, AND SETTING A TIME AND PLACE FOR A PUBLIC HEARING THEREON.

WHEREAS, the City Council of the City of Ontario, California, has previously formed a street lighting maintenance assessment district pursuant to the terms and provisions of the "Landscaping and Lighting Act of 1972," being Division 15, Part 2, of the Streets and Highways Code of the State of California (the "1972 Act"), in what is known and designated as

CITY OF ONTARIO
STREET LIGHTING MAINTENANCE DISTRICT NO. 1
(hereinafter referred to as the District); and

WHEREAS, the City Council has previously undertaken proceedings as required by the 1972 Act, Article XIID of the Constitution of the State of California ("Article XIID"), and the Proposition 218 Omnibus Implementation Act (Government Code Section 53750 and following) (the "Implementation Act and, together with the 1972 Act and Article XIID, the "Assessment Law") to approve and has approved the levy of maximum annual assessments to finance the estimated costs of the maintenance of all of the improvements to be maintained, assessing the net amount upon all assessable lots and/or parcels within the District in proportion to the special benefits received; together with a formula pursuant to which such maximum assessments may be adjusted annually for inflation; and

WHEREAS, at this time, this City Council desires to conduct proceedings to provide for the annual levy of assessments for the referenced Fiscal Year, to provide for the costs and expenses necessary for continual maintenance of improvements within said District; and

WHEREAS, at this time, there has been presented and approved by this City Council, the Engineer's Report as required by law, and this City Council desires to conduct the proceedings for said annual levy; and

WHEREAS, the proposed annual assessments for the referenced Fiscal Year as set forth in the Assessment Engineer's Report do not exceed the maximum annual assessments as previously authorized to be levied and, therefore, the proposed levy of assessments for the referenced Fiscal Year are not deemed to be "increased" over the maximum annual assessments.

NOW, THEREFORE, IT IS HEREBY RESOLVED as follows:

SECTION 1. That the above recitals are all true and correct.

SECTION 2. That the public interest and convenience requires, and it is the intention of this City Council, to undertake proceedings for the annual levy and collection of special assessments for the continual maintenance of certain improvements, all to service and specially benefit said District as said area is shown and delineated on the maps as previously approved by this City Council and on file in the Records Management Department, open to public inspection, and herein so referenced and made a part hereof, and proposed changes thereto are set forth in the Engineer's Report, incorporated herein as a part hereof.

REPORT

SECTION 3. That the Engineer's Report regarding the annual levy for said District, which Report is for maintenance for said Fiscal Year, is hereby approved and is directed to be filed in the Records Management Department. Reference is made to such Report for a full and detailed description of the improvements to be maintained, the boundaries of the District and the zones therein, and the proposed assessments on assessable lots and parcels of land within the District.

ASSESSMENT

SECTION 4. That the public interest and convenience requires, and it is the intention of this City Council to order the annual assessment levy for the District as set forth and described in said Engineer's Report, and further it is determined to be in the best public interest and convenience to levy and collect annual assessments to pay the costs and expenses of said maintenance and improvement as estimated in said Report.

DESCRIPTION OF MAINTENANCE

SECTION 5. The assessments levied and collected shall be for the maintenance of certain street lighting and appurtenant improvements, as set forth in the Engineer's Report, referenced and so incorporated herein.

COUNTY AUDITOR

SECTION 6. The County Auditor shall enter on the County Assessment Roll the amount of the assessments, and the County Tax Collector shall collect said assessments at the time and in the same manner as County taxes are collected. After collection by the County, the net amount of the assessments, after the deduction of any compensation due to the County for collection, shall be paid to the Treasurer for purposes of paying for the costs and expenses of said District.

SPECIAL FUND

SECTION 7. That all monies collected shall be deposited in a special fund. Payment shall be made out of said fund only for the purpose provided for in this Resolution, and in order to expedite the making of this maintenance and improvement, the City Council may transfer into said fund as it may deem necessary to expedite the proceedings. Any such transfer shall be repaid out of the proceeds of the assessments provided for in this Resolution.

BOUNDARIES OF DISTRICT

SECTION 8. Said contemplated maintenance work is in the opinion of this City Council, of special benefit to the properties within the boundaries of the District, and this City Council makes the costs and expenses of said maintenance chargeable upon the District, which District said City Council hereby declares to be the District specially benefited by said improvement and maintenance, and to be further assessed to pay the costs and expenses thereof. Said District shall include each and every parcel of land within the boundaries of said District, as said District is shown on the maps as approved by this City Council and on file in the Records Management Department.

PUBLIC HEARING

SECTION 9. NOTICE IS HEREBY GIVEN THAT A PUBLIC HEARING WILL BE HELD THE 21st DAY OF JULY, 2020, AT THE HOUR OF 6:30 O'CLOCK P.M., IN THE REGULAR MEETING OF THE CITY COUNCIL, BEING IN THE COUNCIL CHAMBERS, WHICH IS THE TIME AND PLACE FIXED BY THIS CITY COUNCIL FOR THE HEARING OF PROTESTS OR OBJECTIONS IN REFERENCE TO THE ANNUAL LEVY OF ASSESSMENTS, TO THE EXTENT OF THE MAINTENANCE, BY PROPERTY OWNERS AFFECTED HEREBY AND ANY OTHER MATTERS CONTAINED IN THIS RESOLUTION; PROVIDED, THAT, IN THE EVENT THE JULY 21, 2020 CITY COUNCIL MEETING IS HELD VIA TELECONFERENCE AND/OR VIDEO CONFERENCE ONLY, THE MEANS BY WHICH THE PUBLIC MAY OBSERVE SUCH PUBLIC HEARING AND OFFER PUBLIC COMMENT SHALL BE PRESCRIBED IN THE NOTICE AND AGENDA FOR SUCH CITY COUNCIL MEETING. ANY PERSONS WHO WISH TO OBJECT TO THE PROCEEDINGS FOR THE ANNUAL LEVY SHOULD FILE A WRITTEN PROTEST WITH THE CITY CLERK PRIOR TO THE TIME SET AND SCHEDULED FOR SAID PUBLIC HEARING.

NOTICE

SECTION 10. That the City Clerk is hereby authorized and directed to publish, pursuant to Government Code Section 6061, a copy of this Resolution in the INLAND VALLEY DAILY BULLETIN, a newspaper of general circulation within said City, said publication shall be made one time and not less than ten (10) days before the date set for the Public Hearing.

SECTION 11. That this Resolution shall take effect immediately upon its adoption.

SECTION 12. For any and all information relating to the proceedings, protest procedure, any documentation and/or information of a procedural or technical nature, your attention is directed to the below listed person at the local agency or department so designated:

Jason Jacobsen
City of Ontario
303 East "B" Street
Ontario, California 91764
(909) 395-2253

The City Clerk of the City of Ontario shall certify as to the adoption of this Resolution.

PASSED, APPROVED, AND ADOPTED this 16th day of June 2020.

PAUL S. LEON, MAYOR

ATTEST:

SHEILA MAUTZ, CITY CLERK

APPROVED AS TO LEGAL FORM:

COLE HUBER LLP
CITY ATTORNEY

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, SHEILA MAUTZ, City Clerk of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. 2020- was duly passed and adopted by the City Council of the City of Ontario at their regular meeting held June 16, 2020 by the following roll call vote, to wit:

AYES: COUNCIL MEMBERS:

NOES: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

SHEILA MAUTZ, CITY CLERK

(SEAL)

The foregoing is the original of Resolution No. 2020- duly passed and adopted by the Ontario City Council at their regular meeting held June 16, 2020.

SHEILA MAUTZ, CITY CLERK

(SEAL)

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, DECLARING ITS INTENTION TO PROVIDE FOR AN ANNUAL LEVY AND COLLECTION OF ASSESSMENTS FOR FISCAL YEAR 2020-21 IN ONTARIO STREET LIGHTING MAINTENANCE DISTRICT NO. 2, PURSUANT TO THE PROVISIONS OF DIVISION 15, PART 2, OF THE STREETS AND HIGHWAYS CODE OF THE STATE OF CALIFORNIA, AND SETTING A TIME AND PLACE FOR A PUBLIC HEARING THEREON.

WHEREAS, the City Council of the City of Ontario, California, has previously formed a street lighting maintenance assessment district, and authorized the levy of assessments therein pursuant to the terms and provisions of the "Landscaping and Lighting Act of 1972," being Division 15, Part 2, of the Streets and Highways Code of the State of California (the "1972 Act"), Article XIID of the Constitution of the State of California ("Article XIID"), and the Proposition 218 Omnibus Implementation Act (Government Code Section 53750 and following) (the "Implementation Act") (the 1972 Act, Article XIID and the Implementation Act are referred to collectively as the "Assessment Law") in what is known and designated as

CITY OF ONTARIO
STREET LIGHTING MAINTENANCE DISTRICT NO. 2
(the "District"); and

WHEREAS, the City Council previously undertook proceedings as required by the Assessment Law to consider the approval of the levy of (a) maximum annual assessments to finance the estimated costs of the maintenance of all of the improvements to ultimately be maintained upon the completion and acceptance thereof for maintenance, assessing the net amount upon all assessable lots and/or parcels within the District in proportion to the special benefits received; together with a formula pursuant to which such maximum assessment may be adjusted annually for inflation, and (b) an initial annual assessment of the estimated costs of the maintenance of those improvements to be maintained during such Fiscal Year, assessing the net amount upon all assessable lots and/or parcels within the District in proportion to the special benefits received; and

WHEREAS, upon the conclusion of the public hearing related to the proposed levy of the maximum annual assessments referred to in the preceding recital, the assessment ballots submitted in favor of the maximum annual assessments and the assessment ballots received in opposition to the levy of the maximum annual assessments were tabulated with assessment ballots weighted according to the proportional financial obligation of the affected properties; and

WHEREAS, as a result of the tabulation of the assessment ballots submitted, it was determined that the assessment ballots submitted, and not withdrawn, in favor of the proposed maximum annual assessment exceeded the assessment ballots submitted, and not withdrawn, in opposition to the levy of the maximum annual assessment; and

WHEREAS, this City Council has initiated proceedings to provide for the annual levy of assessments for the referenced Fiscal Year, to finance the costs and expenses necessary for continual maintenance of improvements within said District; and

WHEREAS, at this time, there has been presented and approved by this City Council, the Assessment Engineer's Report as required by the Assessment Law, and this City Council desires to conduct the proceedings to authorize said annual levy; and

WHEREAS, the proposed annual assessments for the referenced Fiscal Year as set forth in the Assessment Engineer's Report do not exceed the maximum annual assessments as previously authorized to be levied and, therefore, the proposed levy of assessments for the referenced Fiscal Year are not deemed to be "increased" over the maximum annual assessments.

NOW, THEREFORE, IT IS HEREBY RESOLVED as follows:

RECITALS

SECTION 1. That the above recitals are all true and correct.

IMPROVEMENTS AND MAINTENANCE

SECTION 2. That the public interest and convenience requires, and it is the intention of this City Council, to undertake proceedings for the annual levy and collection of special assessments for the continual maintenance of the improvements described below, all to service and specially benefit the properties within said District as said area is shown and delineated on a map as previously approved by this City Council and on file in the Records Management Department, open to public inspection, and herein so referenced and made a part hereof:

The improvements to be maintained include street lighting facilities specially benefiting the properties within the District. Such street lighting facilities include all works or improvements used or useful for street lighting, including luminaires, poles, supports, tunnels, manholes, vaults, conduits, pipes, wires, conductors, guys, stubs, platforms, braces, transformers, insulators, contacts, switches, capacitors, meters, communications circuits, appliances, attachments and appurtenances.

The maintenance of such street lighting facilities shall include the furnishing of services and materials for the ordinary and usual maintenance and servicing of the street lighting facilities, including:

- (a) Repair, removal, or replacement of all or any part of any street lighting facility; and
- (b) The provision of electric current for the operation of such street lighting facilities.

Reference is made to the Assessment Engineer's Report for further information regarding the improvements to be maintained and the scope of such maintenance.

The annual assessment for the referenced Fiscal Year shall be levied only for those improvements completed or projected to be completed and accepted by this City Council for maintenance prior to or during said Fiscal Year.

ASSESSMENT ENGINEER'S REPORT

SECTION 3. That the Assessment Engineer's Report regarding the annual levy for said District, which Report is for maintenance for the said Fiscal Year, is hereby approved and is directed to be filed in the Records Management Department. Reference is made to such Report for a full and detailed description of the improvements to be maintained, the boundaries of the District and any zones therein, and the proposed assessments upon assessable lots and parcels of land within the District.

ASSESSMENT

SECTION 4. That the public interest and convenience requires, and it is the intention of this City Council to order the annual assessment levy for the District as set forth and described in said Assessment Engineer's Report, and further it is determined to be in the best public interest and convenience to levy and collect annual assessments to pay the costs and expenses of said maintenance and improvement as estimated in the Assessment Engineer's Report.

BOUNDARIES OF DISTRICT

SECTION 5. Said contemplated maintenance work is in the opinion of this City Council, of direct benefit to the properties within the boundaries of the District, and this City Council makes the costs and expenses of said maintenance chargeable upon a District, which District said City Council hereby declares to be the District benefited by said improvement and maintenance, and to be further assessed to pay the costs and expenses thereof. Said District shall include each and every parcel of land within the boundaries of said District, as said District is shown on a map as approved by this City Council and on file in the Records Management Department, and so designated by the name of the District.

PUBLIC HEARING

SECTION 6. NOTICE IS HEREBY GIVEN THAT A PUBLIC HEARING WILL BE HELD THE 21st DAY OF JULY, 2020, AT THE HOUR OF 6:30 O'CLOCK P.M., IN THE REGULAR MEETING OF THE CITY COUNCIL, BEING IN THE COUNCIL CHAMBERS, WHICH IS THE TIME AND PLACE FIXED BY THIS CITY COUNCIL FOR THE HEARING OF PROTESTS OR OBJECTIONS IN REFERENCE TO THE ANNUAL LEVY OF ASSESSMENTS, TO THE EXTENT OF THE MAINTENANCE, BY ANY INTERESTED PERSON AND ANY OTHER MATTERS CONTAINED IN THIS RESOLUTION; PROVIDED, THAT, IN THE EVENT THE JULY 21, 2020 CITY COUNCIL MEETING IS HELD VIA TELECONFERENCE AND/OR VIDEO CONFERENCE ONLY, THE MEANS BY WHICH THE PUBLIC MAY OBSERVE SUCH PUBLIC HEARING AND OFFER PUBLIC COMMENT SHALL BE PRESCRIBED IN THE NOTICE AND AGENDA FOR SUCH CITY COUNCIL MEETING. ANY PERSONS WHO WISH TO OBJECT TO THE PROCEEDINGS FOR THE ANNUAL LEVY SHOULD FILE A WRITTEN PROTEST WITH THE CITY CLERK PRIOR TO THE TIME SET AND SCHEDULED FOR SAID PUBLIC HEARING.

NOTICE

SECTION 7. That the City Clerk is hereby authorized and directed to publish, pursuant to Government Code Section 6061, a copy of this Resolution in the INLAND VALLEY DAILY BULLETIN, a newspaper of general circulation within said City, said publication shall be made one time and not less than ten (10) days before the date set for the Public Hearing.

EFFECTIVE DATE OF RESOLUTION

SECTION 8. That this Resolution shall take effect immediately upon its adoption.

PROCEEDING INQUIRIES

SECTION 9. For any and all information relating to the proceedings, protest procedure, any documentation and/or information of a procedural or technical nature, your attention is directed to the below listed person at the local agency or department so designated:

Jason Jacobsen
City of Ontario
303 East "B" Street
Ontario, California 91764
(909) 395-2253

The City Clerk of the City of Ontario shall certify as to the adoption of this Resolution.

PASSED, APPROVED, AND ADOPTED this 16th day of June 2020.

PAUL S. LEON, MAYOR

ATTEST:

SHEILA MAUTZ, CITY CLERK

APPROVED AS TO LEGAL FORM:

COLE HUBER LLP
CITY ATTORNEY

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, SHEILA MAUTZ, City Clerk of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. 2020- was duly passed and adopted by the City Council of the City of Ontario at their regular meeting held June 16, 2020 by the following roll call vote, to wit:

AYES: COUNCIL MEMBERS:

NOES: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

SHEILA MAUTZ, CITY CLERK

(SEAL)

The foregoing is the original of Resolution No. 2020- duly passed and adopted by the Ontario City Council at their regular meeting held June 16, 2020.

SHEILA MAUTZ, CITY CLERK

(SEAL)

CITY OF ONTARIO

Agenda Report
June 16, 2020

SECTION:
CONSENT CALENDAR

SUBJECT: FIVE-YEAR CONSOLIDATED PLAN FOR FISCAL YEARS 2020-2024 AND ONE-YEAR ACTION PLAN FOR THE COMMUNITY DEVELOPMENT BLOCK GRANT, HOME INVESTMENT PARTNERSHIPS, AND EMERGENCY SOLUTIONS GRANT PROGRAMS FOR FISCAL YEARS 2020-21, AND ASSESSMENT OF FAIR HOUSING

RECOMMENDATION: That the City Council:

- (A) Approve the Five-Year Consolidated Plan for Fiscal Years 2020-24, One-Year Action Plan for Fiscal Years 2020-21 for the Community Development Block Grant (“CDBG”), HOME Investment Partnerships (“HOME”), and Emergency Solutions Grant (“ESG”) Programs (on file in the Records Management Department), and 2020 Assessment of Fair Housing (“AFH”); and
- (B) Direct staff to prepare and transmit the final documents to the U.S. Department of Housing and Urban Development (“HUD”); and
- (C) Authorize the City Manager, or his designee, to take all actions necessary or desirable to implement the Five-Year Consolidated Plan, Subrecipient Agreements, and the Assessment of Fair Housing.

COUNCIL GOALS: Pursue City’s Goals and Objectives by Working with Other Governmental Agencies

FISCAL IMPACT: In Fiscal Year 2020-21, the City will receive approximately \$1,864,592 in CDBG funds, \$746,910 in HOME funds, and \$160,290 in ESG funds from HUD. Unallocated prior year CDBG funding of \$342,196 will increase planned CDBG spending for FY 2020-21 to \$2.2 M. There is no impact to the City’s General Fund.

STAFF MEMBER PRESENTING: Julie Bjork, Executive Director Housing and Neighborhood Preservation

Prepared by: Hannah Mac Kenzie
Department: Housing and Neighborhood Preservation

City Manager Approval: 

Submitted to Council/O.H.A. 06/16/2020
Approved: _____

Continued to: _____
Denied: _____

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BACKGROUND: HUD requires that cities prepare and adopt a Five-Year Consolidated Plan and a One-Year Action Plan, which provides for the expenditure of CDBG, HOME, and ESG funds.

FIVE-YEAR CONSOLIDATED PLAN: Provides a strategic plan to identify priorities for the utilization of CDBG, HOME, and ESG funds.

The top priorities are summarized below:

Housing Strategy

- Expand affordable rental housing opportunities, particularly for low-income persons.
- Preserve and rehabilitate existing rental and owner-occupied units.
- Increase affordable homeownership opportunities, particularly for low- and moderate-income persons.

Homeless Strategy

- Preserve and improve the supply of supportive housing and public services for the homeless.

Special Needs Strategy

- Provide supportive services for special needs populations.

Community Development Strategy

- Provide for needed infrastructure improvements in target lower- and moderate- income neighborhoods.
- Provide for new community facilities and improve the quality of existing community facilities to serve lower- and moderate- income neighborhoods.
- Provide needed community services to serve lower- and moderate- income residents.
- Expand the City’s economic base and promote greater employment opportunities.

One community forum/public hearing was held to solicit public participation in the development of the Consolidated Plan and Action Plan. The community forum/public hearing was held on February 3, 2020. Additionally, a community fair was hosted at De Anza park on February 29, 2020. Residents in attendance were invited to participate in an interactive needs assessment activity. Residents were also given the ability to complete a community needs assessment survey for the purpose of identifying needs and priorities. Comments received at the meetings and from the surveys were incorporated into the Consolidated Plan.

On June 5, 2020, the City advertised the availability of the draft Consolidated Plan/Action Plan for public review. The draft Consolidated Plan/Action Plan was available for public review from June 5, 2020 to June 11, 2020.

ACTION PLAN: The Action Plan allocates funds to programs designed to support strategies identified in the Consolidated Plan. Funding available in Fiscal Year 2020-21 for eligible programs in the Action Plan are as follows:

	CDBG	HOME	ESG
CDBG	\$1,864,592		
Unallocated prior year CDBG funds	\$342,196		
HOME		\$746,910	

ESG			\$160,290
TOTAL	\$2,206,788	\$746,910	\$160,290

The City’s CDBG, HOME, and ESG allocations have changed for Fiscal Year 2020-21 from last year’s allocation as illustrated below:

	FY 2019-20	FY 2020-21	Difference
CDBG	\$1,824,218	\$1,864,592	\$40,374
HOME	\$659,631	\$746,910	\$87,279
ESG	\$156,918	\$160,290	\$3,372
TOTAL	\$2,640,767	\$2,771,792	\$131,025

The detailed Fiscal Year 2020-21 funding allocations are provided in Exhibit A.

ASSESSMENT OF FAIR HOUSING:

In 2016, federal regulations pertaining to HUD grantee programs were updated to require grantees to implement their obligation to affirmatively further fair housing by preparing an Assessment of Fair Housing. The AFH replaced the previously required Analysis of Impediments to Fair Housing Choice. The new AFH sets forth a meaningful and transparent process to identify and understand local and regional fair housing issues and to set goals for improving fair housing choice and access to opportunity.

The City of Ontario’s 2020 Assessment of Fair Housing is a thorough examination of structural barriers to fair housing choice and access to opportunity for members of historically marginalized groups protected from discrimination by the federal Fair Housing Act (FHA).

To provide a foundation for the conclusions and recommendations presented in the AFH, the following information was reviewed and analyzed by staff:

- Data from the U.S. Census Bureau and other sources about the demographic, housing, economic, and educational landscape of Ontario and the broader region;
- Various city planning documents and ordinances;
- Data reflecting housing discrimination complaints; and
- The input of a broad range of stakeholders that deal with the realities of the housing market and the lives of members of protected classes in Ontario.

Using the above information, the City’s AFH plan developed four goals and priorities:

1. Goal #1: Increase the supply of affordable housing in high opportunity areas.
2. Goal #2: Prevent displacement of low- and moderate-income residents with protected characteristics, including Hispanic and Black residents, seniors, and people with disabilities.
3. Goal #3: Increase community integration for persons with disabilities.
4. Goal #4: Ensure equal access to housing for persons with protected characteristics, who are disproportionately likely to be lower-income and to experience homelessness.

EXHIBIT A**FY 2020-21 FUNDING ALLOCATION SUMMARY****RECOMMENDED CDBG FUNDING**

Proposed Programs	Implementing Agency	Amount
Administration	City of Ontario Housing & Neighborhood Preservation	\$ 372,918
Community Improvement Team (CIT)	City of Ontario Code Enforcement	\$ 100,000
Mission and Mountain Storefront Façade Improvement Program	City of Ontario Economic Development	\$ 125,000
LED Streetlight Conversion Project	City of Ontario Engineering	\$ 100,000
Rubber-Polymer Modified Slurry Seal Project	City of Ontario Engineering	\$ 300,000
Alley Pavement Rehabilitation	City of Ontario Engineering	\$ 235,000
Wheelchair Ramp Installation	City of Ontario Comm. & Public Svcs.	\$ 175,000
Park Improvements Project	City of Ontario Comm. & Public Svcs.	\$ 519,182
Neighborhood Preservation Program	City of Ontario Housing & Neighborhood Preservation	\$ 17,800
Ontario On The Go	City of Ontario Comm. Life and Culture	\$ 15,000
COVID-19 Response Team	City of Ontario Housing & Neighborhood Preservation	\$ 66,364
Assisi House	Mercy House	\$ 37,914
Stabilization at SOVA Program Center	Inland Valley Hope Partners	\$ 18,410
Enhanced Homeless Services	City of Ontario Housing & Neighborhood Preservation	\$ 60,000
Fair Housing (AFFH) Program	Inland Fair Housing and Mediation Board	\$ 22,000
Landlord/Tenant Mediation	Inland Fair Housing and Mediation Board	\$ 10,200
Senior Support Services	Inland Fair Housing and Mediation Board	\$ 10,000
Child Care, Youth, and Family Services	Ontario-Montclair YMCA	\$ 22,000
	TOTAL	\$ 2,206,788

RECOMMENDED HOME FUNDING

Proposed Programs	Implementing Agency	Amount
Tenant Based Rental Assistance Program	City of Ontario Housing & Neighborhood Preservation	\$ 336,109
Single Family/Multi-Family Housing Rehabilitation and New Construction	City of Ontario Housing & Neighborhood Preservation	\$ 336,110
Administration	City of Ontario Housing & Neighborhood Preservation	\$ 74,691
	TOTAL	\$ 746,910

RECOMMENDED ESG FUNDING

Proposed Programs	Implementing Agency	Amount
Administration	City of Ontario Housing & Neighborhood Preservation	\$ 5,757
Services for Battered Women and Children	House of Ruth	\$ 12,600
Ontario Access Center	Mercy House	\$ 135,669
Mercy House Continuum of Care Administration	Mercy House	\$ 6,264
	TOTAL	\$ 160,290

CITY OF ONTARIO

Agenda Report
June 16, 2020

SECTION:
CONSENT CALENDAR

SUBJECT: AN AMENDMENT TO THE AGREEMENT WITH TRUEPOINT SOLUTIONS FOR REMOTE ACCELA SUPPORT

RECOMMENDATION: That the City Council authorize the City Manager to execute an amendment (on file in the Records Management Department) to the agreement with TruePoint Solutions, LLC, of Loomis, California, for Accela remote support extending the term by three years to June 2023 and increasing the revised authorized contract amount by \$300,000, for a new total of \$400,000.

COUNCIL GOALS: Operate in a Businesslike Manner

FISCAL IMPACT: This amendment increases the agreement amount from \$100,000 to \$400,000 and extends the contract term for an additional three years. Appropriations for the current fiscal year are contained in the Information Technology Fund. Appropriations for future years of the amendment will be included in annual proposed budgets for the respective fiscal years.

BACKGROUND: In 2009, the City first contracted with TruePoint Solutions for support on the Permits Plus to Accela implementation. TruePoint Solutions provides specialized expertise and support of the Development Agency's land management system, Accela, on an-as needed basis. This amendment extends the term of the current agreement through June 30, 2023 and amends the authorized contract amount to \$400,000. Support from TruePoint Solutions ensures we are able to properly implementation changes to City land management ordinances and can continue to review plans and issue land development permits.

STAFF MEMBER PRESENTING: Toni McNaughton, IT Applications Director

Prepared by: Jose Andrade
Department: Information Technology

City Manager Approval: 

Submitted to Council/O.H.A. 06/16/2020

Approved: _____

Continued to: _____

Denied: _____

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CITY OF ONTARIO

Agenda Report
June 16, 2020

SECTION:
CONSENT CALENDAR

SUBJECT: AUTHORIZE THE PURCHASE OF LICENSING AND EQUIPMENT FOR A SECURITY LOGGING SYSTEM

RECOMMENDATION: That the City Council award Bid No. 1317 from SHI International Corp. of Somerset, New Jersey and authorize the purchase of additional licensing and equipment in the amount of \$190,220 for a security logging system.

COUNCIL GOALS: Operate in a Businesslike Manner

FISCAL IMPACT: The Fiscal Year 2019-20 Adopted Operating Budget includes appropriations from the Information Technology Fund for replacement of network infrastructure and security equipment. The bid amount of \$190,220 includes four physical servers, additional licensing and one year of maintenance for a security logging system.

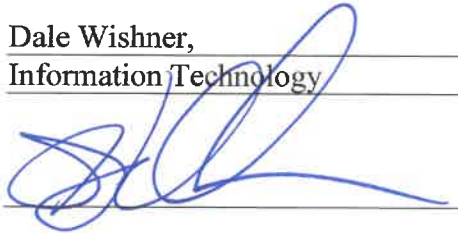
BACKGROUND: This purchase will provide additional capacity to the City's existing Logrhythm cyber security logging and alerting system. This system collects security logs from the City's servers and security equipment and will alert IT staff of an attempted security breach. In addition, the City is required to collect and maintain log history by the Department of Justice and Payment Card Industry for compliance purposes.

In May 2020, the City solicited bids for expanding and upgrading the Logrhythm solution, and two (2) bids were received.

<u>Vendor</u>	<u>Location</u>	<u>Amount</u>
SHI International Corp.	Somerset, NJ	\$190,220
Nth Generation Computing Inc.	San Diego, CA	\$195,825

Staff recommends awarding Bid No. 1317 to SHI International Corp. as they submitted the lowest responsive bid.

STAFF MEMBER PRESENTING: Colin Fernandes, Executive Director Information Technology

Prepared by: Dale Wishner,
Department: Information Technology
City Manager Approval: 

Submitted to Council/O.H.A. 06/16/2020
Approved: _____
Continued to: _____
Denied: _____

CITY OF ONTARIO

Agenda Report
June 16, 2020

SECTION:
CONSENT CALENDAR

SUBJECT: A MAINTENANCE SERVICE AGREEMENT FOR THE POLICE DEPARTMENT FIREARMS TRAINING FACILITY

RECOMMENDATION: That the City Council authorize the City Manager to execute a five-year maintenance service agreement (on file in Records Management Department) with Green Global Environmental, of Claremont, CA, for range cleaning and maintenance at an annual estimated cost of \$30,564, or a total of \$152,820 for the term of the agreement.

COUNCIL GOALS: Maintain the Current High Level of Public Safety
Operate in a Businesslike Manner


FISCAL IMPACT: The FY 2020-21 Proposed Operating Budget will include appropriations of \$30,564 for police firearms facility maintenance and cleaning. Appropriations for the remaining four years of the agreement will be included in future annual budgets to be considered by The City Council. The five-year estimated cost for services is \$152,820.

BACKGROUND: The Police Department Firearm Training Facility opened in 2012 and is operated on a continuing basis to provide firearm training opportunities for the Police Department and OFD Arson Investigators.

On May 22, 2020, 7 bids were received for the Maintenance Services of the facility. The bid results are summarized below.

Vendor	City, State	Bid Amount
Green Global Environmental	Claremont, CA	\$152, 820
North State	San Francisco, CA	\$152,940
HCI Environmental	Riverside, CA	\$182,165
NRC Environmental	Long Beach, CA	\$230,316

STAFF MEMBER PRESENTING: Derek Williams, Chief of Police

Prepared by: Christine Booker
Department: Police Department
City Manager Approval: 

Submitted to Council/O.H.A. 06/16/2020
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Sun Environmental	Torrance, CA	\$245,000
Metals Treatment	Arvada, CO	\$286,686
MEC, Inc.	Edwards, CA	\$442,200

Staff recommends award to Green Global Environmental as they submitted the lowest responsive bid and have performed similar maintenance work in the past that matches the project scope.

CITY OF ONTARIO

Agenda Report
June 16, 2020

SECTION:
CONSENT CALENDAR

SUBJECT: AN AMENDMENT TO EXTEND THE PROFESSIONAL SERVICES AGREEMENT FOR BOOKING, JAIL AND TRANSPORTATION SERVICES

RECOMMENDATION: That the City Council authorize the City Manager to execute a three-year extension of the current professional services agreement (on file in the Records Management Department) with GEO Corrections and Detention, LLC, of Boca Raton, Florida, for booking, jail and transportation services with a not to exceed total cost of \$1,676,702 for the three-year period.

COUNCIL GOALS: Maintain the Current High Level of Public Safety
Operate in a Businesslike Manner

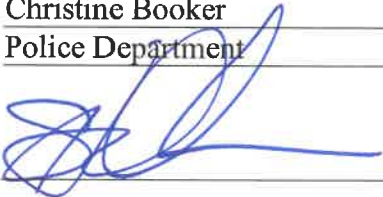
FISCAL IMPACT: The FY 2020-21 Proposed Operating Budget will include appropriations of \$542,464 for booking, jail, and transportation services. With the budgetary impacts of COVID-19 the GEO Group has maintained status quo for FY 2020-21 pricing. Appropriations for the remaining two years of the agreement will be included in future annual budgets as approved by City Council. The City will be billed for actual services provided in an amount not to exceed \$1,676,702 during the three-year extension period.

BACKGROUND: Booking, jail and transportation services are used to reduce the amount of time spent by sworn officers transporting prisoners to the West Valley Detention Center. First implemented in 2006 as a result of a comprehensive Police Department staffing study, the outsourcing of these services has reduced thousands of hours per year that officers would have spent providing these services.

GEO Correction and Detention, LLC. (GEO), formerly Correctional Systems, Inc. (CSI), has provided these services to the City of Ontario since 2006. GEO has demonstrated the ability to provide quality and responsive services to meet the needs of the Police Department.

GEO meets the standards of the California Boards of Corrections and is accredited with the American Correctional Association. GEO has been an outstanding vendor, and shown that during these uncertain

STAFF MEMBER PRESENTING: Derek Williams, Chief of Police

Prepared by: Christine Booker
Department: Police Department
City Manager Approval: 

Submitted to Council/O.H.A. 06/16/2020
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times they can be counted on to work for the good of all, and band with the City to ensure no shortage in services.

CITY OF ONTARIO

Agenda Report
June 16, 2020

SECTION:
CONSENT CALENDAR

SUBJECT: APPROVAL OF PRE-AUTHORIZED VENDORS TO PROVIDE SERVICES AND PARTS FOR SPECIALIZED FLEET AND EQUIPMENT

RECOMMENDATION: That the City Council approve the pre-authorized vendors to provide parts and maintenance services for the following specialized fleet and equipment: automotive vehicles, paving equipment, Integrated Waste vehicles, John Deere and Toro mowers.

COUNCIL GOALS: Maintain the Current High Level of Public Safety
Operate in a Businesslike Manner

FISCAL IMPACT: None. The designation of pre-authorized vendors to provide services and parts for specific types of fleet work does not affect appropriations and does not commit the City to any specific level of future expenditures with these vendors.

BACKGROUND: The Public Works Agency maintains the vehicles and equipment used by various City departments. To ensure that the vehicles and equipment are safe to operate, maintenance and repairs are necessary on a regular basis. Undue time delays affect the City's ability to provide services to the community. Due to specialized parts used on these vehicles and equipment, there is a limited number of authorized vendors that can provide them. In many cases, there is only a single authorized vendor in the Southern California area that is qualified or authorized to provide the part or service.

For these reasons, it is not cost-effective for the City to follow its standard purchasing procedures to solicit bids or requests for proposal. Establishing a list of pre-authorized vendors will facilitate the following:

- Ensure that the vehicles and equipment are serviced and repaired when needed;
- Services are performed by qualified vendors who are familiar with, paving equipment, Integrated Waste vehicles and cameras, John Deere and Toro mowers; and
- Better maintenance because the vendors are familiar with the service and repair history of the City's vehicles and equipment.

STAFF MEMBER PRESENTING: Tito Haes, Executive Director Public Works

Prepared by: Michael Johnson
Department: Fleet Services
City Manager Approval: 

Submitted to Council/O.H.A. 06/16/2020
Approved: _____
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The list of recommended vendors was developed from the manufacturers' list of authorized service centers, and from the City's experience with certain vendors.

Ontario Municipal Code Section 2-6.23(b) and 2-6.23(c) authorizes the Purchasing Officer to make purchases without following the standard purchasing procedures whenever (1) the goods can be obtained from only one source, and/or (2) a breakdown in machinery, equipment or an essential service which requires an immediate purchase of supplies and equipment to protect public health, safety and welfare generates circumstances that a competitive process would be unavailing or would not produce an advantage, and the advertisement for competitive bid would thus be undesirable, impractical, or impossible.

**APPROVED LIST OF AUTHORIZED VENDORS
FOR SERVICES AND MAINTENANCE OF FLEET SERVICES**

Sole Source Vendors

Service/Maintenance Performed

RDO Construction Equipment
20 Iowa Ave, Riverside, CA 92507

Large John Deere and Yellow Iron
Equipment parts and repairs

Stotz Equipment
4811 Brooks Street, Montclair, CA 91763

Small John Deere parts and repairs

Non-Sole Sources Vendor

Service/Maintenance Performed

Lu's Lighthouse
10230 Norwalk Blvd.,
Santa Fe Springs, CA 90670

Heavy Duty Truck Electrical Parts

Citrus Motors
1375 S Woodruff Way, Ontario, CA 91761

Ford and Kia parts and repairs

Penske Honda
1401 Auto Center Drive, Ontario, CA 91761

Honda parts and repairs

John Elway's Crown Toyota
1201 Kettering Drive, Ontario, CA 91761

Toyota parts and repairs

Oremor Automotive Group
1377 Kettering Drive, Ontario, CA 91761

Jeep, Dodge, and Nissan parts and
repairs

Subaru of Ontario
1195 Auto Center Drive, Ontario, CA 91761

Subaru parts and repairs

Inland Kenworth
9730 Cherry Ave, Fontana, CA 92335

Kenworth parts and repairs

Rush Truck Center
1440 Slover Ave, Fontana, CA 92335

Peterbilt parts and repairs

CITY OF ONTARIO

Agenda Report
June 16, 2020

SECTION:
CONSENT CALENDAR

SUBJECT: AWARD OF A PROFESSIONAL SERVICES AGREEMENT TO DUDEK FOR THE SOUTH ARCHIBALD PLUME REPORTING

RECOMMENDATION: That the City Council approve a five-year Professional Services Agreement (on file in the Records Management Department) with Dudek of Encinitas, California, for the Annual South Archibald Plume Monitoring and Reporting in the amount of \$63,500; and authorize the City Manager to execute the agreement and any future amendments within the authorization limits.

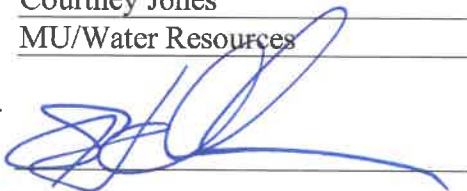
COUNCIL GOALS: Operate in a Businesslike Manner
Pursue City's Goals and Objectives by Working with Other Governmental Agencies
Invest in City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)

FISCAL IMPACT: Per the Stipulated Settlement and Cleanup and Abatement Order, the City of Ontario is responsible for 83.3 percent of the costs associated with the preparation of the annual South Archibald Plume Monitoring Report while the City of Upland is responsible for 16.7 percent of the costs. The City of Upland reimburses the City of Ontario at the end of each fiscal year.

Dudek has agreed to fixed billing rates, as set forth in their agreement, for five years; the annual cost is \$12,700. Ontario's net cost will be \$52,917 or 83.3% of the total contract cost for the five-year term. There is a six percent increase in the first year of service as compared to the current rates being billed for said services, and thereafter rates remain fixed for the duration of the agreement. Appropriations in the Water Operating Fund will be included in the Fiscal Year 2020-21 Proposed Budget. The total compensation under the terms of this agreement will be commensurate with City Council approved budgets in the Water Operational Fund to cover the costs. There is no impact to the General Fund.

BACKGROUND: The South Archibald groundwater trichloroethylene (TCE) Plume (Plume) was first identified in 1986 when samples from several wells south of the 60 Freeway had detectable concentrations of TCE. Subsequently, the Regional Water Quality Control Board (RWQCB) initiated an investigation to determine the extend and potential sources of the groundwater contamination in the area.

STAFF MEMBER PRESENTING: Scott Burton, Utilities General Manager

Prepared by: Courtney Jones
Department: MU/Water Resources
City Manager Approval: 

Submitted to Council/O.H.A. 06/16/2020
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In 2016, the Cities of Ontario and Upland entered into a Stipulated Settlement and Cleanup and Abatement Order (CAO) with the RWQCB. Stipulated CAO No. R8-2016-0016 holds the Cities of Ontario and Upland responsible for coordinating and conducting any and all ongoing monitoring of the Plume, unless the Regional Board orders or directs another public agency to conduct such monitoring. The Cities of Ontario and Upland jointly submit an annual Plume Monitoring Report to the Regional Board each year. Dudek was selected based on their qualifications and in coordination with Best Best & Krieger as a part of the negotiation leading up to the CAO. Since 2016, Dudek has prepared this report that presents and analyzes the water quality data generated by the private well sampling program for the South Archibald TCE Plume, consistent with the CAO.

Staff is recommending a five-year contract with Dudek to enhance the City's administrative efficiencies. Contracting with Dudek will increase cost effectiveness as a result of their previous experience and understanding of the City's needs specific to preparing and submitting the Annual Monitoring Report.

CITY OF ONTARIO

Agenda Report
June 16, 2020

SECTION:
CONSENT CALENDAR

SUBJECT: AWARD OF DESIGN SERVICE AGREEMENTS FOR ON-CALL WATER SUPPLY STUDIES

RECOMMENDATION: That the City Council approve and authorize the City Manager to execute three-year Design Services Agreements (on file in the Records Management Department) with Albert A. Webb, of Riverside, California, Michael Baker Inc., of Ontario, California, and PlaceWorks of Santa Ana, California; and authorize the City Manager to extend the agreements for up to two (2) additional one (1) year periods consistent with City Council approved budgets.

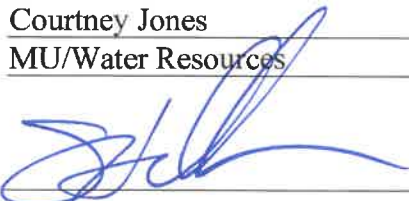
COUNCIL GOALS: Invest in the Growth and Evolution of the City's Economy
Operate in a Businesslike Manner
Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)

FISCAL IMPACT: Consultants will be compensated for on-call, as-needed services at the billing rates set forth in their respective agreements. Appropriations in the Water Operating Fund will be included in the Fiscal Year 2020-21 Proposed Budget. The total compensation paid to any of the consultants in any year under the term of these agreements will be commensurate with the City Council approved plans and budgets.

All firms have agreed to fixed billing rates set forth in their respective agreements for three years. At the City's discretion, up to two additional one-year extensions may be executed. Pricing for the option years will be negotiated and any increases will not exceed 3 percent per year. The Water Supply Assessments and Written Verification are prepared by the City on behalf of the various development projects and are reimbursable by the respective applicant. There is no impact to the General Fund.

BACKGROUND: The City receives development projects for review each year. Each project received by the City is evaluated to determine whether there is a need for a Water Supply Assessment (WSA) or Written Verification (WV) consistent with California Water Code Section 10910. The WSAs and WVs evaluate whether the City's total projected water supplies available during normal, single dry, and

STAFF MEMBER PRESENTING: Scott Burton, Utilities General Manager

Prepared by: Courtney Jones
Department: MU/Water Resources
City Manager Approval: 

Submitted to Council/O.H.A. 06/16/2020
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Continued to: _____
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multiple dry water years over a 20-year projection will meet the projected water demand of the proposed project, in addition to the City's existing and planned future uses.

In order to facilitate the workload associated with these projects, staff has identified a need to contract with firms to provide on-call professional services. Staff is recommending on-call contracts with three qualified firms. Contracting with three firms will assure a cost-effective option and sufficient resources are always available for projects as they arise. The execution of these agreements does not guarantee any payment or assignment to any firm. The pre-selection of firms on the basis of qualifications permits the assignment of tasks on the basis of lowest cost or on the basis of specialized expertise or availability, as needed.

On April 7, 2020, the City solicited statements of qualifications for water supply studies on-call services; and responses from the following were evaluated:

COMPANY	LOCATION
Albert A. Webb	Riverside, CA
Michael Baker Inc.	Ontario, CA
PlaceWorks	Santa Ana, CA
Tetra Tech	Pasadena, CA

Using a qualifications-based selection process, a panel of City staff from the Ontario Municipal Utilities Company reviewed the statements of qualifications. Albert A. Webb, Michael Baker, Inc., and PlaceWorks, were judged to be the most qualified firms. As such, they are being recommended based on their broad range of expertise as demonstrated by their services provided to municipal agencies and capability to perform the work in a timely manner.

CITY OF ONTARIO

Agenda Report
June 16, 2020

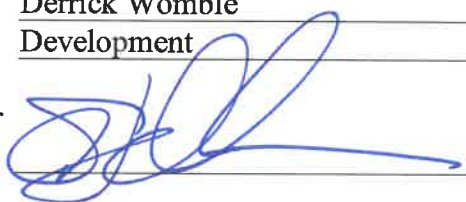
SECTION:
PUBLIC HEARINGS

SUBJECT: A PUBLIC HEARING TO CONSIDER AN ORDINANCE APPROVING THE FOURTH AMENDMENT TO THE DEVELOPMENT AGREEMENT (FILE NO. PDA05-002) BETWEEN THE CITY OF ONTARIO AND SC ONTARIO DEVELOPMENT COMPANY, LLC, TO MODIFY THE COMMENCEMENT OF CERTAIN SPECIFIC INFRASTRUCTURE ASSOCIATED WITH THE DEVELOPMENT OF TENTATIVE TRACT MAP 20316 (FILE NO. PMTT19-020), AND CONFORM TO THE REVISED DEVELOPMENT STANDARDS ESTABLISHED BY THE PARKSIDE SPECIFIC PLAN AMENDMENT (FILE NO. PSPA19-007), LOCATED AT THE NORTHWEST CORNER OF EUCALYPTUS AVENUE AND ARCHIBALD AVENUE, WITHIN PLANNING AREAS 1 THROUGH 26 OF THE PARKSIDE SPECIFIC PLAN (APNS: 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, and 0218-221-10)

RECOMMENDATION: That the City Council introduce and waive further reading of an ordinance approving the Fourth Amendment to the Development Agreement (File No. PDA05-002) between the City of Ontario and SC Ontario Development Company, LLC, to modify the commencement of certain specific infrastructure associated with the development of Tentative Tract Map 20316 (File No. PMTT19-020) and conform to the revised development standards established by the Parkside Specific Plan Amendment (File No. PSPA19-007).

COUNCIL GOALS: Invest in the Growth and Evolution of the City's Economy
Operate in a Businesslike Manner
Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)
Ensure the Development of a Well Planned, Balanced, and Self-Sustaining Community in Ontario Ranch

STAFF MEMBER PRESENTING: Scott Murphy, AICP, Executive Director Development Agency

Prepared by: Derrick Womble
Department: Development
City Manager Approval: 

Submitted to Council/O.H.A. 06/16/2020
Approved: _____
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Denied: _____

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FISCAL IMPACT: The proposed Fourth Amendment to the Development Agreement (File No. PDA05-002) will not have an immediate impact on the City's budget. The Development Agreement continues to provide the mechanism for funding from the formation of a Community Facilities District (CFD) for City services and facilities required to support the Parkside Specific Plan development, thereby mitigating the increased costs associated with such services. In addition, the City will receive public service funding fees plus development impact, compliance processing, licensing, and permitting fees.

BACKGROUND: On July 25, 2006, the Planning Commission approved Tentative Tract Map 18048 (File No. PMTT06-039). Tract Map 18048 proposed to subdivide 104.13 acres of land into 25 lots, facilitate the backbone infrastructure improvements (major streets, sewer, water, storm drain facilities) and the creation of residential neighborhoods, the commercial center, community facilities (fire station), and parks for the eastern portion of the Parkside Specific Plan ("Specific Plan"). On August 15, 2006, the City Council approved the Specific Plan and certified the Environmental Impact Report (EIR).

On September 19, 2006, the City Council approved a Development Agreement, File No. PDA05-002 ("Development Agreement"), between the City of Ontario and SC Ontario Development Company, LLC, ("Applicant") to provide the funding for additional City services required to support the Specific Plan residential development of up to 1,947 residential units and the infrastructure improvements required to support the related Tract Map No. 18048.

On July 7, 2009, the City Council approved an amendment to the Development Agreement to allow Tract Map 18048 to be recorded for the purpose of financing.

On February 3, 2015, the City Council approved a second amendment to the Development Agreement to update the Development Agreement to conform with the current Construction Agreement with NMC Builders, LLC, and to provide for the phasing of the construction of public infrastructure for 250.89 acres of the Specific Plan.

On June 18, 2019, the City Council approved a third amendment to the Development Agreement to remove approximately 2.43 acres of land from the Development Agreement and change the legal description in conjunction with the lot line adjustment (File No. LLA18-010) and sale of a portion of the property to Ontario Land Ventures, LLC, for the design, construction, and completion of street improvements necessary for the ultimate alignment of Eucalyptus Avenue.

The main points of the original Development Agreement continue to address Development Impact Fees (DIF), public service funding, Community Facilities District (CFD) for maintenance of public facilities, park/open space requirements, affordable housing fees, school facilities requirements, and remain in full force and effect. State law and Section 2.5 of the original Development Agreement provide that amendments may be made to the Agreement upon the mutual consent of both parties, using the same process and procedures as for the consideration and approval of the original Development Agreement.

On October 16, 2019, the Applicant submitted Tentative Tract Map 20316 (PMTT19-020) for condominium purposes to subdivide 56.99 acres of land into 4 numbered lots and 12 lettered lots within Planning Areas 1 through 4 of the Specific Plan. In addition, the Applicant has proposed the Parkside Specific Plan Amendment (File No. PSPA19-007) to: [a] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [b] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [c] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [d] Update and revise Residential Design Guidelines to introduce new housing types and architectural styles; [e] Update and revise Landscape Standards.

The new Tract Map and proposed revisions to the Specific Plan necessitates a fourth amendment to the Development Agreement (“Fourth Amendment”) to modify the commencement of certain specific infrastructure associated with the development of Tentative Tract Map 20316 (File No. PMTT19-020) and conform to the updated development standards established by the Parkside Specific Plan Amendment (File No. PSPA19-007).

Key points addressed in the Fourth Amendment are the following:

- Payment of the Public Service Funding Fee (PSFF) for residential and non-residential uses;
- Requirement to participate in a cost-sharing agreement with other developers for the construction of the Eucalyptus Avenue Bridge Improvements or deposit funds with the City for the proportional share of the cost to construct the bridge;
- Complete construction of the traffic signal at Eucalyptus Avenue and Parkhouse Privado (required with Phase 1) prior to the request for the first occupancy permit for Phase 1 Production Units or prior to completion of the Eucalyptus Bridge Improvements, whichever occurs first;
- Complete construction of the Cucamonga Creek Channel Trail Improvements and Neighborhood Edge Improvements, in phases, in conjunction with the development of the respective Phase (Phases 1 through 4);
- Continuing requirement to design, construct, and complete significant portions of the Great Park and allow for the City to acquire such portions, subject to the terms of a DIF Credit and Reimbursement Agreement; and
- Extension of the term of the Development Agreement another five (5) years until September 19, 2026.

In considering the application at their meeting on May 26, 2020, the Planning Commission found that the Fourth Amendment was consistent with State law, The Ontario Plan, and the City’s Development Agreement policies previously approved for Ontario Ranch developments. As a result, the Planning Commission adopted Resolution No. PC20-029 recommending City Council approval of the Fourth Amendment with a 6-0 vote.

HOUSING ELEMENT COMPLIANCE: The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the proposed project is consistent with the number of dwelling units (1,977) and density (15.73 DU/AC) specified in the Available Land Inventory.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan (“ALUCP”), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within

the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

ENVIRONMENTAL REVIEW: The environmental impacts of this project were analyzed in an Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006. This application is consistent with the previously adopted EIR and introduces no new significant environmental impacts. All previously adopted mitigation measures shall be a condition of project approval and are incorporated herein by reference. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP).

Exhibit "A" Tentative Tract Map 20316

V. H., COUNTY OF SAN BERNARDINO, CALIFORNIA

TENTATIVE TRACT MAP NO. 20316 FOR CONDOMINIUM PURPOSES

March 24, 2025

VAP SUMMARY

AREA: 60.47 AC
 LOTS: 10
 LOTS PER ACRE: 1.64
 TOTAL AREA: 60.47 AC
 TOTAL LOTS: 10
 TOTAL AREA PER LOT: 6.047 AC

LEGAL RESERVATION

BASIS OF BEARING

BEARINGS

ASSASSON'S PARCELS NUMBERS

UTILITIES

CAREY/DEVELOPER / CIVIL ENGINEER

LEGEND

APPROPRIATIONS

DATE OF SURVEY

EASEMENT NOTES

INDEX MAP

INDEX MAP

GENERAL NOTES

- All shown features to be done in accordance with the provisions of the California Subdivided Lands Act, Chapter 228, of the California Civil Code, and the provisions of the California Subdivided Lands Act, Chapter 228, of the California Civil Code.
- The map is prepared in accordance with the provisions of the California Subdivided Lands Act, Chapter 228, of the California Civil Code, and the provisions of the California Subdivided Lands Act, Chapter 228, of the California Civil Code.
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Exhibit "B"
Proposed Parkside Specific Plan Land Use Map

Parkside



Exhibit 1-3: Land Use Plan

Legend	
	Medium Density Residential (11.1 - 25 du/ac)
	Neighborhood Commercial (0.4 FAR)
	Public Facility
	Open Space - Parkland
	Open Space - Private/Village Edges
	Roads - Master Planned
	Roads - Local
	Cucamonga Creek Channel

ORDINANCE NO. _____

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING A FOURTH AMENDMENT TO THE DEVELOPMENT AGREEMENT (FILE NO. PDA05-002), BETWEEN THE CITY OF ONTARIO AND SC ONTARIO DEVELOPMENT COMPANY, LLC, TO MODIFY THE COMMENCEMENT OF CERTAIN SPECIFIC INFRASTRUCTURE ASSOCIATED WITH THE DEVELOPMENT OF TENTATIVE TRACT MAP 20316 (FILE NO. PMTT19-020), AND CONFORM TO THE REVISED DEVELOPMENT STANDARDS ESTABLISHED BY THE PARKSIDE SPECIFIC PLAN AMENDMENT (PSPA19-007), LOCATED AT THE NORTHWEST CORNER OF EUCALYPTUS AVENUE AND ARCHIBALD AVENUE, WITHIN PLANNING AREAS 1 THROUGH 26 OF THE PARKSIDE SPECIFIC PLAN, AND MAKING FINDINGS IN SUPPORT THEREOF—APNS: 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, AND 0218-221-10.

WHEREAS, SC Ontario Development Company, LLC ("Applicant") has filed an Application for the approval of a Fourth Amendment to the Development Agreement, File No. PDA05-002, as described in the title of this Ordinance (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 250.89 acres of land generally located at the northwest corner of Eucalyptus Avenue and Archibald Avenue, within Planning areas 1 through 26 of the Parkside Specific Plan; and

WHEREAS, the properties to the north, south, east, and west, are developed with agriculture and dairy uses and currently vacant. The property also encompasses approximately a half-mile portion of the Cucamonga Creek Channel from north to south; and

WHEREAS, on September 19, 2006, the City Council of the City of Ontario ("City"), adopted Ordinance No. 2841, approving the original Development Agreement between the City and Applicant, to provide the funding for additional City services required to support the Parkside Specific Plan (File No. PSP03-002) residential development of up to 1,947 residential units and the infrastructure improvements required to support the related Tract Map 18048 (File No. PMTT06-039); and

WHEREAS, on July 7, 2009, the City adopted Ordinance No. 2909, approving a First Amendment to the Development Agreement, to allow Tract Map 18048 to be recorded for the purpose of financing; and

WHEREAS, on February 3, 2015, the City adopted Ordinance No. 3012, approving a Second Amendment to the Development Agreement, to update the Development Agreement to conform with the Construction Agreement with NMC Builders, LLC, and to provide for the phasing of the construction of public infrastructure for 250.89 acres of the Parkside Specific Plan; and

WHEREAS, on June 18, 2019, the City adopted Ordinance No. 3134, approving the Third Amendment to the Development Agreement, to change the legal description in conjunction with the lot line adjustment (File No. LLA18-010) and sale of a portion of the property to Ontario Land Ventures, LLC, for the design, construction, and completion of street improvements necessary for the ultimate alignment of Eucalyptus Avenue; and

WHEREAS, on October 16, 2019, the Applicant submitted a new Tentative Tract Map 20316 (File No. PMTT19-020) for condominium purposes to subdivide 56.99 acres of land into 4 numbered lots and 12 lettered lots, within Planning Areas 1 through 4 of the Parkside Specific Plan. In addition, the Applicant has proposed the Parkside Specific Plan Amendment (File No. PSPA19-007) to: [a] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [b] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [c] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [d] Update and revise Residential Design Guidelines to introduce new housing types and architectural styles; [e] Update and revise Landscape Standards; and

WHEREAS, the new Tract Map and proposed Specific Plan Amendment necessitates a fourth amendment to the Development Agreement ("Fourth Amendment") to modify the commencement of certain specific infrastructure associated with the development of Tentative Tract Map 20316 (File No. PMTT19-020), and conform to the updated development standards established by the Parkside Specific Plan Amendment (PSPA19-007); and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an initial study has been prepared to determine possible environmental impacts; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, the environmental impacts of this project were analyzed in an addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006. This application is consistent with the previously adopted EIR and introduces no new significant environmental impacts. All previously adopted mitigation measures shall be a condition of project approval and are incorporated herein by reference; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and make recommendations to the City Council on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 26, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Resolution No. PC20-029 recommending the City Council approve the Application; and

WHEREAS, on June 16, 2020, the City Council of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND ORDAINED by the City Council of the City of Ontario, as follows:

SECTION 1. *Environmental Determination and Findings.* As the decision-making body for the Project, the City Council has reviewed and considered the information contained in the previous Certified EIR and supporting documentation. Based upon the facts and information contained in the previous Certified EIR and supporting documentation, the City Council finds as follows:

(1) The environmental impacts of this project were previously reviewed in conjunction with File No. PSP03-002, a(n) addendum to the Parkside Specific Plan for which a Certified EIR was adopted by the City Council on September 5, 2006; and

(2) The previous Certified EIR contains a complete and accurate reporting of the environmental impacts associated with the Project; and

(3) The previous Certified EIR was completed in compliance with CEQA and the Guidelines promulgated thereunder; and

(4) The previous Certified EIR reflects the independent judgment of the City Council; and

(5) The proposed project will introduce no new significant environmental impacts beyond those previously analyzed in the previous Certified EIR, and all mitigation measures previously adopted with the Certified EIR, are incorporated herein by this reference.

SECTION 2. *Subsequent or Supplemental Environmental Review Not Required.* Based on the information presented to the City Council, and the specific findings set forth in Section 1, above, the City Council finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and.

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

(d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3. *Housing Element Compliance.* Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the approving authority for the Project, the City Council finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy

Plan (General Plan) component of The Ontario Plan. The project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the proposed project is consistent with the number of dwelling units (1,977) and density (15.73 DU/AC) specified in the Available Land Inventory.

SECTION 4. *Ontario International Airport Land Use Compatibility Plan ("ALUCP") Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport ("ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making body for the Project, the City Council has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the City Council, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 5. *Concluding Facts and Reasons.* Based upon the substantial evidence presented to the City Council during the above-referenced hearing, and upon the specific findings set forth in Section 1 through 4, above, the City Council hereby concludes as follows:

(a) The Fourth Amendment applies to approximately 250.89 acres of land generally located at the northwest corner of Eucalyptus Avenue and Archibald Avenue, within Planning Areas 1 through 26 of the Parkside Specific Plan; and

(b) The properties to the north, south, east, and west, are developed with agriculture and dairy uses and currently vacant. The property also encompasses approximately one-half mile portion of the Cucamonga Creek Channel from north to south; and

(c) The main points of the original Development Agreement continue to address Development Impact Fees (DIF), public service funding, Community Facilities District (CFD) for maintenance of public facilities, park/open space requirements, affordable housing fees, school facilities requirements, and remain in full force and effect. State law and Section 2.5 of the original Development Agreement provide that amendments may be made to the Agreement upon the mutual consent of both parties, using the same process and procedures as for the consideration and approval of the original Development Agreement; and

(d) The new Tract Map and proposed Specific Plan Amendment necessitates a fourth amendment to the Development Agreement ("Fourth Amendment") to modify the commencement of certain specific infrastructure associated with the development of Tentative Tract Map 20316 (File No. PMTT19-020), and conform to the updated development standards established by the Parkside Specific Plan Amendment (File No. PSPA19-007).

(e) Key points addressed in the Fourth Amendment are the following:

- i. Payment of the Public Service Funding Fee (PSFF) for residential and non-residential uses;
- ii. Requirement to participate in a cost-sharing agreement with other developers for the construction of the Eucalyptus Avenue Bridge Improvements or, deposit funds with the City for the proportional share of the cost to construct the bridge;
- iii. Complete the construction of the traffic signal at Eucalyptus Avenue and Parkhouse Privado (required with Phase 1), prior to the request for the first occupancy permit for Phase 1 Production Units, or prior to completion of the Eucalyptus Bridge Improvements, whichever occurs first;
- iv. Construct the Cucamonga Creek Channel Trail Improvements and Neighborhood Edge Improvements in phases, in conjunction with the development of the respective Phase (Phases 1 through 4);
- v. Design, construct, and complete significant portions of the Great Park and allow for the City to acquire such portions, subject to the terms of a DIF Credit and Reimbursement Agreement; and
- vi. Extension of the term of the Development Agreement another five (5) years until September 19, 2026.

(f) This Amendment will not be materially injurious or detrimental to the adjacent properties and will not have a significant impact on the environment or the surrounding properties. The environmental impacts of this project were previously reviewed in conjunction with addendum to the Parkside Specific Plan EIR (SCH#20040111008) that was adopted by the City Council on September 5, 2006. This application introduces no new significant environmental impacts; and

(g) All adopted mitigation measures of the related EIR shall be a condition of project approval and are incorporated herein by reference.

SECTION 6. City Council Action. Based upon the findings and conclusions set forth in Sections 1 through 5, above, the City Council hereby APPROVES the herein described Application, attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 7. Indemnification. The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 8. Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 9. Severability. If any section, sentence, clause or phrase of this Ordinance or the application thereof to any entity, person or circumstance is held for any reason to be invalid or unconstitutional, such invalidity or unconstitutionality shall not affect other provisions or applications of this Ordinance which can be given effect without the invalid provision or application, and to this end the provisions of this Ordinance are severable. The People of the City of Ontario hereby declare that they would have adopted this Ordinance and each section, sentence, clause or phrase thereof, irrespective of the fact that any one or more section, subsections, sentences, clauses or phrases be declared invalid or unconstitutional.

SECTION 10. Effective Date. This Ordinance shall become effective 30 days following its adoption.

SECTION 11. Publication and Posting. The Mayor shall sign this Ordinance and the City Clerk shall certify as to the adoption and shall cause a summary thereof to be published at least once, in a newspaper of general circulation in the City of Ontario, California within 15 days following the adoption. The City Clerk shall post a certified copy of this ordinance, including the vote for and against the same, in the Office of the City Clerk, in accordance with Government Code Section 36933.

PASSED, APPROVED, AND ADOPTED this ____ day of _____ 2020.

PAUL S. LEON, MAYOR

ATTEST:

SHEILA MAUTZ, CITY CLERK

APPROVED AS TO FORM:

COLE HUBER LLP
CITY ATTORNEY

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, SHEILA MAUTZ, City Clerk of the City of Ontario, DO HEREBY CERTIFY that foregoing Ordinance No. ____ was duly introduced at a regular meeting of the City Council of the City of Ontario held June 16, 2020, and adopted at the regular meeting held _____2020 by the following roll call vote, to wit:

AYES: COUNCIL MEMBERS:

NOES: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

SHEILA MAUTZ, CITY CLERK

(SEAL)

I hereby certify that the foregoing is the original of Ordinance No. ____ duly passed and adopted by the Ontario City Council at their regular meeting held _____2020 and that Summaries of the Ordinance were published on _____ and _____, in the Inland Valley Daily Bulletin newspaper.

SHEILA MAUTZ, CITY CLERK

(SEAL)

ATTACHMENT A:

File No. PDA05-002

Fourth Amendment to the Development Agreement

By and Between

**City of Ontario
a California municipal corporation**

and

**SC Ontario Development Company, LLC
a Delaware limited liability company**

(Document follows this page)

**RECORDING REQUESTED BY AND
WHEN RECORDED RETURN TO:**

CITY OF ONTARIO
CITY CLERK / RECORDS MANAGEMENT
303 EAST "B" STREET
ONTARIO, CA 91764-4196

Space above this line for Recorder's Use

Exempt from Fees Per Gov. Code § 6103

File No. PDA05-002

FOURTH AMENDMENT TO THE DEVELOPMENT AGREEMENT

By and Between

**City of Ontario
a California municipal corporation**

and

**SC Ontario Development Company, LLC
a Delaware limited liability company**

_____, 2020

San Bernardino County, California

**FOURTH AMENDMENT TO THE DEVELOPMENT AGREEMENT
BY AND BETWEEN THE CITY OF ONTARIO AND
SC ONTARIO DEVELOPMENT COMPANY, LLC
(File No. PDA05-002)**

This Fourth Amendment (hereinafter "**Fourth Amendment**") is entered into as of the _____ day of _____ 2020 by and among the CITY OF ONTARIO, a California municipal corporation (hereinafter "**CITY**"), and SC ONTARIO DEVELOPMENT COMPANY LLC, a Delaware limited liability company (hereinafter "**OWNER**").

RECITALS

WHEREAS, the CITY and OWNER's predecessor have previously entered into a (i) Development Agreement dated September 19, 2006 and recorded in San Bernardino County, California on November 14, 2006 as Instrument No. 2006-0774531 pursuant to Section 65864, et seq., of the Government Code, (hereinafter the "**Original Development Agreement**"), as amended by (ii) that First Amendment to Development Agreement dated June 16, 2009, and recorded on September 14, 2009 as Instrument No. 2009-0403692 (the "**First Amendment**"), (iii) that Second Amendment to the Development Agreement dated April 4, 2017, and recorded on May 15, 2017 as Instrument No. 2017-0199310 (the "**Second Amendment**"), and (iv) that Third Amendment to Development Agreement dated June 18, 2019, and recorded on June 27, 2019 as Instrument No. 2019-0211740 (the "**Third Amendment**") all with respect to the real property described in Exhibit "A-R" to this Agreement.

WHEREAS the CITY and OWNER now intend to amend the Original Development Agreement, as amended by the First, Second, and Third Amendments respectively (collectively the "**Existing Development Agreement**"), as set forth herein.

WHEREAS, Section 2.5 of the Existing Development Agreement specifies that the Development Agreement may be amended in whole or in part only in the manner provided for in Government Code Section 65868.1 and the procedure for adopting and entering into an amendment to the Existing Development Agreement shall be the same as the procedure for adopting and entering into the Original Development Agreement; and

WHEREAS, the CITY and OWNER agree to modify the commencement of certain specific infrastructure improvements to reflect the new Tentative Tract Map 20316 (File No. PMTT19-020), and proposed development standards in Ontario Ranch as it relates to the Parkside Specific Plan Amendment (PSPA19-007); and

WHEREAS, the CITY and OWNER agree that execution of this Fourth Amendment shall constitute Certification of Agreement Compliance under Section 6.4 of the Existing Development Agreement; and

WHEREAS, the term of the Original Development Agreement was for a ten (10) year term, expiring on September 19, 2016. The term of the Original Development Agreement was extended an additional five (5) years by the execution of the Second Amendment, expiring September 19, 2021. CITY and OWNER now agree to extend the term of the Existing Development Agreement for an additional five (5) year period, expiring on September 19, 2026.

AGREEMENTS

NOW, THEREFORE, in consideration of the above recitals and of the mutual agreements hereinafter contained, the parties agree as follows:

1. DEFINITIONS AND EXHIBITS.

1.1. Existing Definitions. The following terms when used in this Fourth Amendment shall be defined as in the Existing Development Agreement: CITY; Construction Agreement; Construction Agreement Amendment; Deferred Infrastructure; Development; Development Approvals; Development Exaction; Development Impact Fee; Development Plan; General Plan; Land Use Regulations; Model Units; OWNER; Project; Property; Production Units; Specific Plan; Storm Water Capacity Availability Equivalents; Subsequent Development Approvals; Subsequent Land Use Regulations; and Water Availability Equivalent (WAE). Any capitalized terms not expressly defined in Section 1.2 below or otherwise expressly defined or modified in this Fourth Amendment shall have the meaning given to those terms in the Existing Development Agreement.

1.2. Additional and Modified Definitions. The following terms used in this Fourth Amendment shall be defined as follows:

“Effective Date” means the date that the ordinance approving this Fourth Amendment becomes effective.

“Development Agreement” is revised to mean, collectively, the Original Development Agreement, First Amendment, Second Amendment, Third Amendment and this Fourth Amendment.

“Existing Development Approvals” is revised to mean all Development Approvals approved or issued prior to the Effective Date. Existing Development Approvals includes the Development Approvals incorporated herein as Exhibit “C-R” and all other Development Approvals that are a matter of public record on the Effective Date.

“Existing Land Use Regulations” is revised to mean all Land Use Regulations in effect on the Effective Date and all other Land Use Regulations that are in effect and a matter of public record on the Effective Date. Existing Land Use Regulations includes the Regulations incorporated herein as Exhibit “D-R”.

“Improvement”, “Improvements”, or “Infrastructure” is revised to mean those public improvements and public infrastructure required to support the development of the Project as described in the Tract Map conditions for the “B” Tract Map No. 20316 and any and all future “A” and “B” Tract Maps for the Property. Improvement, Improvements, or Infrastructure shall also mean those public improvements required to support the Phase 1 Units, Phase 2 Units, Phase 3 Units and Phase 4 Units as further described in the respective Exhibit “F” for each respective Phase (the **“Infrastructure Improvements Exhibits”**).

“Phase” means the four (4) development phases of the Project as shown on Exhibit “F”.

“Phase 1 Improvements” means the Improvements (as defined in Section 1.2, below) that shall be designed, constructed and completed by OWNER prior to, and as a condition precedent to, CITY’s issuance of the first building permit for any Production Unit and as shown in “Exhibit F - Phase 1.”

“Phase 1 Units” means the first five hundred forty (540) units for which the CITY issues building permits to OWNER and shall include up to thirty-six (36) Model Units.

“Phase 2 Improvements” means the Improvements that shall be designed, constructed and completed by OWNER prior to, and as a condition precedent to, CITY’s issuance of the first building permit for any Production Unit in the Phase 2 area and as shown in “Exhibit F – Phase 2”.

“Phase 2 Units” means the next two hundred ten (210) units for which the CITY issues building permits to OWNER after the issuance of building permits for the Phase 1 Units.

“Phase 3 Improvements” means the Improvements that shall be designed, constructed and completed by OWNER prior to, and as a condition precedent to, CITY’s issuance of the first building permit for any Production Unit in the Phase 3 area and as shown in “Exhibit F - Phase 3”.

“Phase 3 Units” means the next six hundred thirteen (613) units for which the CITY issues building permits to OWNER after the issuance of building permits for the Phase 2 Units.

“Phase 4 Improvements” means the Improvements that shall be designed, constructed and completed by OWNER prior to, and as a condition precedent to, CITY’s issuance of the first building permit for any Production Unit in the Phase 4 area and as shown in “Exhibit F- Phase 4”.

“Phase 4 Units” means the next six hundred fourteen (614) units for which the CITY issues building permits to OWNER after the issuance of the building permits for the Phase 3 Units.

1.3. Exhibits. The following documents are attached to, and by this reference made a part of, this Fourth Amendment and revise and replace Exhibits A, B, C-R, D-R, E-R, F, F-Phase 1, F-Phase 2, F-Phase 3 and F-Phase 4 in the Existing Development Agreement:

Exhibit “A-R” – Revised Legal Description of Property

Exhibit “B-R” – Revised Parkside Specific Plan

Exhibit “C-R” – Revised Existing Development Approvals.

Exhibit “D-R” – Revised Existing Land Use Regulations.

Exhibit “E-R” — Revised Conceptual Phasing Plan

Exhibit “F” — Infrastructure Improvements for all Phases

Exhibit “F-Phase 1” - Phase 1 Improvements

Exhibit “F-Phase 2” - Phase 2 Improvements

Exhibit “F-Phase 3” - Phase 3 Improvements

Exhibit “F-Phase 4” – Phase 4 Improvements

2. MODIFICATIONS TO DEVELOPMENT AGREEMENT TO CONFORM TO CONSTRUCTION AGREEMENT AMENDMENT.

A. The provisions of Section 2.3 of the Second Amendment shall be removed and replaced with the following:

2.3 Modifications of the Amounts of the CFD to Finance City Services. Prior to, and as a condition precedent to, the recordation of each final subdivision creating buildable lots, such map shall be included in a Community Facilities District (“CFD”) to finance CITY services through annual special taxes. The amounts contained in Section 5.1 in the Development Agreement shall be modified as follows:

- Single Family Detached Dwelling Unit from \$1,387.00 to \$1,687.00
- Multiple Family Dwelling Unit from \$1,202.00 to \$1,462.00

- Gated Apartment Community Dwelling Unit from \$1,008.00 to \$1,226.00.
- The amount for Non-Residential building shall be modified to \$0.32 per square foot.

These modified amounts shall be subject to an automatic increase, not to exceed four (4%) percent per year, beginning on January 1, 2021.

B. Section 2.4 of the Second Amendment shall be removed and replaced with the following:

2.4 Retention of Public Services Funding Fee Amounts and Payment Terms. CITY and OWNER acknowledge and agree that the modifications to the amount and payment terms included in Section 3.7.4 of the Construction Agreement Amendment shall not apply to OWNER's Public Services Funding Fee Amounts and payment terms as OWNER entered into the Original Development Agreement with the City prior to the effective date of the Construction Agreement Amendment. Additionally, CITY agrees that OWNER is in compliance with the requirements for payment of the first installment of the Public Services Funding Fees as specified in Section 4.6 in the Original Development Agreement. Furthermore, OWNER acknowledges and agrees to pay the CITY the Second and Third Installments as specified in Section 4.6 in the Original Development Agreement.

If OWNER receives approval for any non-residential uses for the Project, OWNER shall pay the Public Services Funding Fee in a Single Installment for non-residential uses, and such Single Installment shall be due and payable on a building-by-building basis prior to the issuance of a building permit for each non-residential building. The amount of the Single-Installment for non-residential uses shall be the fee in effect at the time of payment and such fee shall automatically increase (but no decrease) in the Consumer Price Index (Los Angeles-Anaheim-Riverside County), 1950-2001 (1982-84=100) over the preceding year on January 1st of each year, beginning on January 1, 2021. OWNER may exercise the option to pay any single installment amounts for the remainder of the non-residential square footage within the Project on or before December 31st, before the Single Installment amount is automatically increased.

3. MODIFICATIONS TO DEVELOPMENT AGREEMENT TO AMEND CERTAIN REQUIREMENTS FOR CONSTRUCTION OF SPECIFIC PUBLIC IMPROVEMENTS.

A. The following provisions shall be added to Section 3.7 of the Development Agreement:

3.7.3 Eucalyptus Avenue Bridge Improvements. To the extent the Eucalyptus Avenue Bridge Improvements have not been commenced by other

developers, OWNER shall be responsible to commence construction of the Eucalyptus Avenue Bridge Improvements prior to the first building for Phase 1 Production Units, and complete construction prior to requesting the last building permit for Phase 1 Production Units. If OWNER has not commenced construction of the Eucalyptus Avenue Bridge Improvements prior to requesting the first building permit for Phase 1 Production Units, OWNER shall provide proof to the satisfaction of the CITY that OWNER has exercised one (1) of the following two (2) options:

3.7.3.1 Enter into a cost sharing agreement with other developer(s) for the construction and completion of the Eucalyptus Avenue Bridge Improvements and OWNER has fully funded OWNER'S obligations under the cost sharing agreement; or

3.7.3.2 OWNER shall deposit their proportional share at thirty-three percent (33%) of the cost to construct and complete the Eucalyptus Avenue Bridge Improvements into an Escrow Account with the City.

3.7.4 Traffic Signal at Eucalyptus Avenue and Parkhouse Privado. OWNER shall design, construct and complete the Traffic Signal at Eucalyptus Avenue and Parkhouse Privado, prior to and as a condition precedent to OWNER'S request for the first occupancy permit for Phase 1 Production Units or prior to completion of the Eucalyptus Bridge Improvements, whichever occurs first.

3.7.5 Cucamonga Creek Channel Trail Improvements. CITY and OWNER mutually agree that the development of the Cucamonga Creek Channel Trail Improvements (the "**Trail Improvements**") shall be constructed and completed in phases in conjunction with the development of the respective Phase (Phases 1 through 4) adjacent to the Trail Improvements.

3.7.5.1 Phase 1 Trail Improvements. OWNER shall design and commence construction of the Trail Improvements adjacent to Phase 1 prior to and as a condition precedent to, the request of the 305th building permit for Phase 1 Production Units or, upon completion of the Eucalyptus Avenue Bridge Improvements, whichever occurs first. OWNER shall complete construction of the Trail Improvements adjacent to Phase 1, prior to the request of the last building permit for Phase 1 Production Units.

3.7.5.2 Phase 2 Trail Improvements. OWNER shall design and commence construction of the Trail Improvements adjacent to Phase 2 prior to and as a condition precedent to, the request of the first building permit for Phase 2 Production Units. OWNER shall complete construction of the Trail Improvements adjacent to Phase 2, prior to the request of the last building permit for Phase 2 Production Units.

3.7.5.3 Phase 3 Trail Improvements. OWNER shall design and commence construction of the Trail Improvements adjacent to Phase 3 prior to and as a condition precedent to, the request of the first building permit for Phase 3 Production Units. OWNER shall complete construction of the Trail Improvements adjacent to Phase 3, prior to the request of the last building permit for Phase 3 Production Units.

3.7.5.4 Phase 4 Trail Improvements. OWNER shall design and commence construction of the Trail Improvements adjacent to Phase 4 prior to and as a condition precedent to, the request of the first building permit for Phase 4 Production Units. OWNER shall complete construction of the Trail Improvements adjacent to Phase 4, prior to the request of the last building permit for Phase 4 Production Units.

- 3.7.6 OWNER shall provide periodic written progress reports to the City commencing thirty (30) days after the OWNER initiates construction of the Trail Improvements, and each sixty (60) days thereafter regarding the progress of construction of the Trail Improvements.
- 3.7.7 If OWNER is unable to complete the construction of the Trail Improvements adjacent to the respective Phase prior to requesting the last building permit for Production Units within the respective Phase of the Project, due to either the design, construction and completion of the Great Park or, the construction and completion of the Eucalyptus Avenue and Ontario Ranch Road (Edison) Bridges, the City Manager or designee shall have the administrative authority to establish alternative and/or additional requirements of the OWNER for the release of any remaining building permits for Production Units for the respective Phase within the Project, at his/her reasonable discretion.
- 3.7.8 Neighborhood Edge Improvements along Archibald Avenue, Eucalyptus Avenue, Ontario Ranch Road, and Hellman Avenue. OWNER shall design, construct, and complete all neighborhood edge improvements in conjunction with the development of the respective Phase (Phases 1 through 4), prior to requesting the first building permit for Production Units for the respective Phase. If OWNER has not completed the neighborhood edge improvements with the development of the respective Phase prior to the request for the first building permit for Production Units within that Phase, the City Manager or designee shall have the authority to reasonably consider and approve the release of Production Permits respective to each Phase before the completion of all of the neighborhood edge improvements within such Phase at his/her discretion.

4. CONSTRUCTION AND ACQUISITION OF GREAT PARK.

A. Section 4.2.4 of the Development Agreement shall be removed and replaced with the following:

4.2.4 OWNER agrees that the Great Park areas as identified in the Parkside Specific Plan are to be constructed and transferred to the CITY by OWNER in four (4) Phases and shall be developed in accordance with the City's park standards and open to the public. CITY and OWNER shall meet periodically and/or as needed from the Effective Date of this Amendment, to jointly develop a plan for the timing and phasing of the development, funding, completion and acquisition of each Phase of design and construction of the Great Park area. Subject to the successful development of the aforementioned plan and OWNER's and CITY's approval and execution of a separate DIF Credit and Reimbursement Agreement, the design and construction of the Great Park areas shall entitle OWNER to a credit towards its obligations under the Quimby Act (Gov. Code § 64477) and the CITY's implementing ordinance and/or resolution (collectively "**Quimby Act Obligations**"), and to the extent OWNER's Quimby Act Obligations are satisfied, OWNER shall be entitled to have the CITY acquire such developed and public available parks as Non-Program Interests as set forth in Section 3.6.2 of the Construction Agreement Amendment. CITY acknowledges that the development of the Great Park areas by OWNER exceeds OWNER's requirements for the development of parkland and open space and that OWNER's requirements for the development of parkland and open space and that OWNER may be entitled to further consideration for the development and dedication of the Great Park area in form of reimbursements to OWNER from Quimby Act Fees collected by CITY and paid by other development within the Ontario Ranch area of the New Model Colony. The specific terms of the issuance of DIF Credit and DIF Reimbursement to OWNER will be included in the separate DIF Credit and Reimbursement Agreement between CITY and OWNER as provided for in Section 4.2.6.

5. OTHER MODIFICATIONS. Section 4.2. of the Second Amendment shall be removed and replaced with the following:

4.2 Extension of Term of the Development Agreement. CITY and OWNER acknowledge that the Term of the Development Agreement will expire on September 19, 2021. CITY and OWNER also agree that the OWNER will not meet all requirements for the extension of the term of the Development Agreement as it is not anticipated that OWNER will obtain fifty percent (50%) of the building permits for Project prior to the end of the ten (10) year term of the Development Agreement. Notwithstanding this requirements, CITY and OWNER agrees that OWNER has met the remaining requirements for the extension of the term of the Development Agreement

and that OWNER is not in default of the Development Agreement. CITY and OWNER agree that the term of the Development Agreement shall be extended for an additional five (5) year period to September 19, 2026 for a total term of twenty (20) years following the Effective Date of the Development Agreement.

6. INTEGRATION.

6.1 Integration of Previous Understands and Clarifications. This Fourth Amendment reflects the complete understanding of the parties with respect to the subject matter hereof. To the extent this Fourth Amendment conflicts with the Original Development Agreement, First Amendment, Second Amendment, and Third Amendment, this Fourth Amendment supersedes such previous document(s). In all other aspects, the parties hereto re-affirm and ratify all other terms, conditions, provisions, and obligations under the Original Development Agreement, First Amendment, Second Amendment, and Third Amendment. The Property covered by this Fourth Amendment is as described in the legal description of the Property attached hereto as revised Exhibit "A-R". This Fourth Amendment shall be recorded against the Property.

[Signature Immediately on Following Page]

SIGNATURE PAGE
TO FOURTH AMENDMENT TO DEVELOPMENT AGREEMENT BY AND BETWEEN
THE CITY OF ONTARIO AND SC ONTARIO DEVELOPMENT COMPANY, LLC.
(File No. PDA05-002)

IN WITNESS WHEREOF, the parties hereto have executed this Fourth Amendment as of the Effective Date.

"OWNER"

SC ONTARIO DEVELOPMENT COMPANY, LLC,
a Delaware limited liability company

By: **LEWIS MANAGEMENT CORP.,**
a Delaware corporation - Its Sole Manager

By: _____
Name: _____
Its: Authorized Agent

"CITY"

CITY OF ONTARIO
a California municipal corporation

By: _____
Scott Ochoa, City Manager

Date: _____

ATTEST:

City Clerk, Ontario

APPROVED AS TO FORM:
COLE HUBER, LLP

City Attorney

ACKNOWLEDGEMENT

A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

STATE OF CALIFORNIA)
COUNTY OF _____)

On _____, 20____, before me, _____,
Date *Insert Name and Title of the Officer*

personally appeared _____
Name(s) of Signer(s)

who proved to me on the basis of satisfactory evidence to be the person whose name(s) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity, and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing paragraph is true and correct.

WITNESS my hand and official seal.

Signature _____
Signature of Notary Public

Place Notary Seal Above

ACKNOWLEDGEMENT

A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

STATE OF CALIFORNIA)
COUNTY OF _____)

On _____, 20____, before me, _____,
Date *Insert Name and Title of the Officer*

personally appeared _____
Name(s) of Signer(s)

who proved to me on the basis of satisfactory evidence to be the person whose name(s) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity, and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing paragraph is true and correct.

WITNESS my hand and official seal.

Signature _____
Signature of Notary Public

Place Notary Seal Above

EXHIBIT "A-R"

LEGAL DESCRIPTION

THE LAND HEREINAFTER DESCRIBED IS SITUATED IN THE CITY OF ONTARIO, STATE OF CALIFORNIA, COUNTY OF SAN BERNARDINO AND IS DESCRIBED AS FOLLOWS:

PARCEL 1 (SUNKIST)

GOVERNMENT LOTS 3 AND 4 AND THE SOUTHEAST ONE-QUARTER OF SECTION 15, TOWNSHIP 2 SOUTH, RANGE 7 WEST, SAN BERNARDINO MERIDIAN IN THE COUNTY OF SAN BERNARDINO, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL PLAT THEREOF.

EXCEPTING THEREFROM THAT PORTION LYING WITHIN THE LAND DESCRIBED IN THAT CERTAIN EASEMENT IN FAVOR OF THE SAN BERNARDINO COUNTY FLOOD CONTROL DISTRICT RECORDED APRIL 20, 1944, IN BOOK 1678 PAGE 154 OF OFFICIAL RECORDS.

ALSO EXCEPTING THEREFROM THAT PORTION CONVEYED TO THE SAN BERNARDINO COUNTY FLOOD CONTROL DISTRICT BY DEED RECORDED SEPTEMBER 29, 1977, IN BOOK 9273 PAGE 254 OF OFFICIAL RECORDS.

ALSO EXCEPTING THEREFROM THAT PORTION CONVEYED TO THE COUNTY OF SAN BERNARDINO BY DEED RECORDED AUGUST 29, 1990, AS INSTRUMENT NO. 90-344004 OF OFFICIAL RECORDS.

ALSO EXCEPT ALL COUNTY ROADS AND HIGHWAYS.

ALSO EXCEPTING THE FOLLOWING:

SHEET 1 OF 1

EXHIBIT "A"

GRANT DEED FOR LLA18-010
SC ONTARIO DEVELOPMENT TO ONTARIO LAND VENTURES

THAT PORTION OF GOVERNMENT LOT 4, SECTION 15, TOWNSHIP 2 SOUTH, RANGE 7 WEST, SAN BERNARDINO BASE AND MERIDIAN, IN THE CITY OF ONTARIO, COUNTY OF SAN BERNARDINO, STATE OF CALIFORNIA, ACCORDING TO THE UNITED STATES GOVERNMENT TOWNSHIP PLAT THEREOF, APPROVED BY THE SURVEYOR GENERAL ON AUGUST 30, 1873, DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF SAID GOVERNMENT LOT 1 AS SHOWN ON RECORD OF SURVEY 02-139, RECORDED IN BOOK 118, PAGES 82 THROUGH 95 INCLUSIVE OF RECORD OF SURVEYS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, SAID CORNER BEING ON THE CENTERLINE OF CARPENTER AVENUE AS SHOWN ON SAID RECORD OF SURVEY, SAID CORNER ALSO BEING ON THE EAST LINE OF RANCHO SANTA ANA DEL CHINO, AS PER MAP RECORDED IN BOOK 6, PAGE 15 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY;

THENCE ALONG SAID CENTERLINE AS SHOWN ON RECORD OF SURVEY, RECORDED IN BOOK 25, PAGE 15 OF RECORD OF SURVEYS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, NORTH 00' 02' 44" WEST, 187.76 FEET TO THE CENTERLINE OF EUCALYPTUS AVENUE, AS SHOWN ON LAST MENTIONED RECORD OF SURVEY;

THENCE ALONG THE EASTERLY PROLONGATION OF SAID CENTERLINE OF EUCALYPTUS AVENUE, NORTH 89' 19' 34" EAST, 144.43 FEET TO THE BEGINNING OF A TANGENT CURVE, CONCAVE SOUTHWESTERLY AND HAVING A RADIUS OF 905.00 FEET;

THENCE SOUTHEASTERLY ALONG SAID CURVE, THROUGH A CENTRAL ANGLE OF 27° 08' 53" AN ARC LENGTH OF 428.81 FEET TO THE BEGINNING OF A TANGENT REVERSE CURVE, CONCAVE NORTHEASTERLY AND HAVING A RADIUS OF 905.00 FEET, A RADIAL LINE TO SAID BEGINNING OF TANGENT REVERSE CURVE BEARS SOUTH 26° 28' 27" WEST;

THENCE SOUTHEASTERLY ALONG SAID CURVE, THROUGH A CENTRAL ANGLE OF 26° 27' 26" AN ARC LENGTH OF 417.90 FEET TO A POINT OF TANGENCY WITH THE NORTHERLY LINE OF SAID GOVERNMENT LOT 1, A RADIAL LINE TO SAID POINT OF TANGENCY BEARS SOUTH 00° 01' 02" WEST;

THENCE ALONG SAID NORTHERLY LINE OF GOVERNMENT LOT 1, NORTH 89° 58' 58" WEST, 961.54 FEET TO THE POINT OF BEGINNING.

CONTAINING: 105,724 SQUARE FEET OR 2.43 ACRES MORE OR LESS (GROSS)

EXHIBIT "B" ATTACHED HERETO AND BY THIS REFERENCE MADE A PART HEREOF.

SUBJECT TO: COVENANTS, CONDITIONS, RESTRICTIONS, RESERVATIONS, EASEMENTS AND RIGHTS-OF-WAY, IF ANY.

SURVEYOR:

PREPARED UNDER THE SUPERVISION OF:

Last Update: 12/04/18
O:\3290\LLA\GRANT DEED\3290_DEED.dwg

PREPARED BY:

Thienes Engineering, Inc.
CIVIL ENGINEERING • LAND SURVEYING
14349 FIRESTONE BOULEVARD
LA MIRADA, CALIFORNIA 90638
PH.(714)521-4811 FAX(714)521-4173

BRIAN L. THIENES
P.L.S. NO. 5750
REG. EXP. DEC. 31, 2019

DATE



EXHIBIT "B"

SHEET 1 OF 2

GRANT DEED FOR LLA18-010
SC ONTARIO DEVELOPMENT TO ONTARIO LAND VENTURES

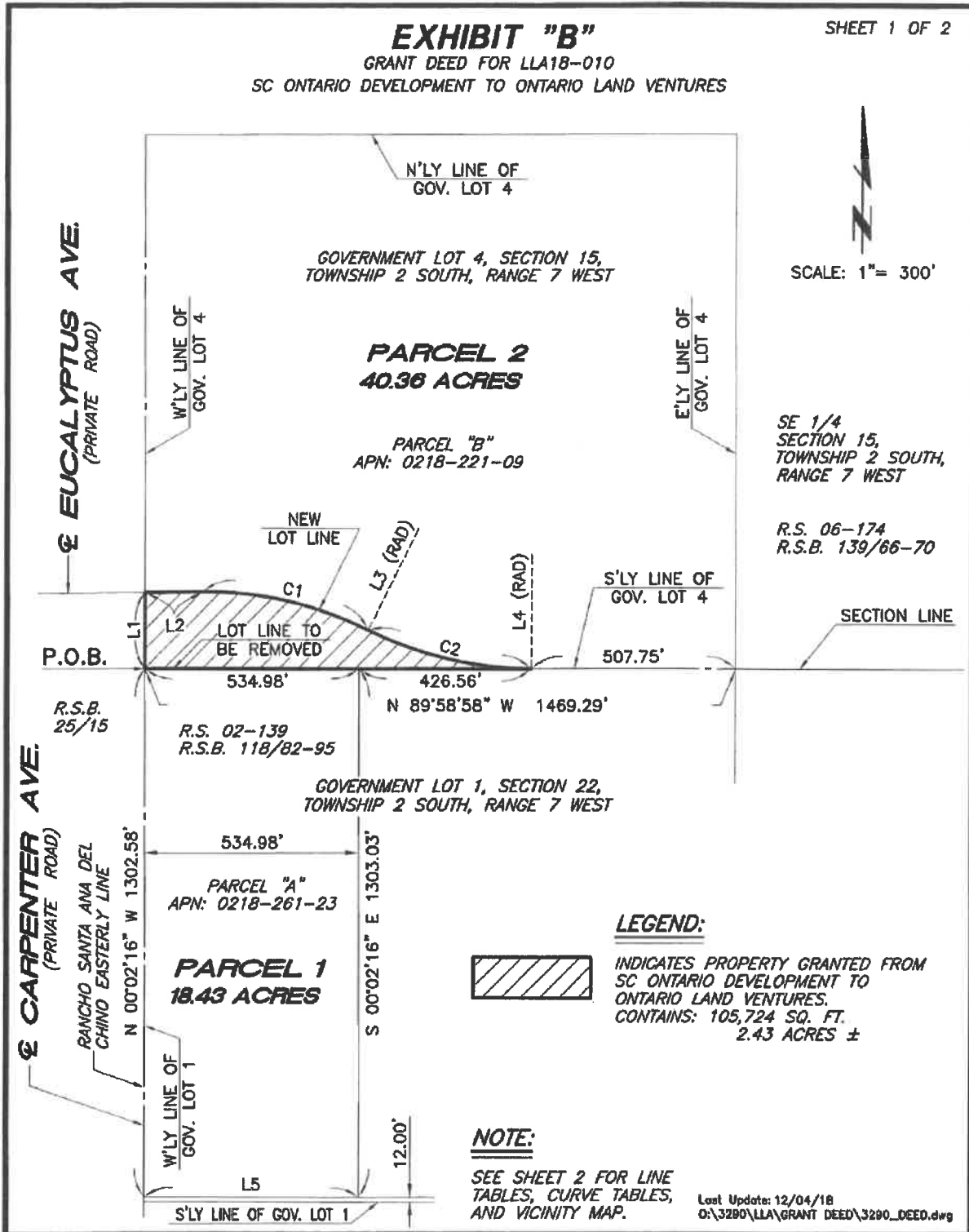


EXHIBIT "B-R"
PARKSIDE SPECIFIC PLAN

Parkside



Exhibit 1-3: Land Use Plan



EXHIBIT "C-R"

EXISTING DEVELOPMENT APPROVALS

On July 25, 2006, the Planning Commission:

- a) Issued Resolution No. PC06-060 Recommending City Council Adopt and certify the Parkside Specific Plan Environmental Impact Report.
- b) Issued Resolution No. PC06-061 Recommending City Council approval of the General Plan Amendment (File No. PGPA04-003).
- c) Issued Resolution No. PC06-062 Recommending City Council approval of the Parkside Specific Plan (File No. PSP03-002).
- d) Issued Resolution No. PC06-063 approving Tentative Tract Map 18048 (File No. PMTT06-039).

On August 15, 2006, the City Council:

- a) Adopted and certified the Parkside Specific Plan Environmental Impact Report and issued Resolution No. 2006-068.
- b) Approved General Plan Amendment (File No. PGPA04-003) and issued Resolution No. 2006-068A.
- c) Adopted Ordinance No. 2838 approving the Parkside Specific Plan (File No. PSP03-002).

On September 19, 2006, the City Council:

- a) Adopted Ordinance No. 2841, approving the original Development Agreement between the City of Ontario and SC Ontario Development Company, LLC, to provide the funding for additional City services required to support the Parkside Specific Plan (File No. PSP03-002) residential development of up to 1,947 residential units and infrastructure improvements required to support the related Tract Map 18048 (File No. PMTT06-039).

On July 7, 2009, the City Council:

- a) Adopted Ordinance No. 2909, approving the First Amendment to the Development Agreement (File No. PDA09-002) between the City of Ontario and SC Ontario Development Company, LLC, to allow Tract Map 18048 to be recorded for the purposes of financing.

EXHIBIT "C-R" Continued

EXISTING DEVELOPMENT APPROVALS

On December 15, 2014, the Planning Commission:

- a) Adopted Resolution No. PC14-116 recommending City Council approval of the SC Ontario Development Company, LLC Second Amendment to the Development Agreement (File No. PDA14-007); and
- b) Issued Resolution No. PC14-117 approving Tentative Tract Map No. 18999 (File No. PMTT14-021).

On February 3, 2015, the City Council:

- a) Adopted Ordinance No. 3012 approving the Second Amendment to the Development Agreement (File No. PDA14-007) between the City of Ontario and SC Ontario Development Company, LLC.

On April 23, 2019, the Planning Commission:

- a) Adopted Resolution No. PC19-025 recommending City Council approval of the Third Amendment to the Development Agreement (File No. PDA05-002) between the City of Ontario and SC Ontario Development Company, LLC.

On June 18, 2019, the City Council:

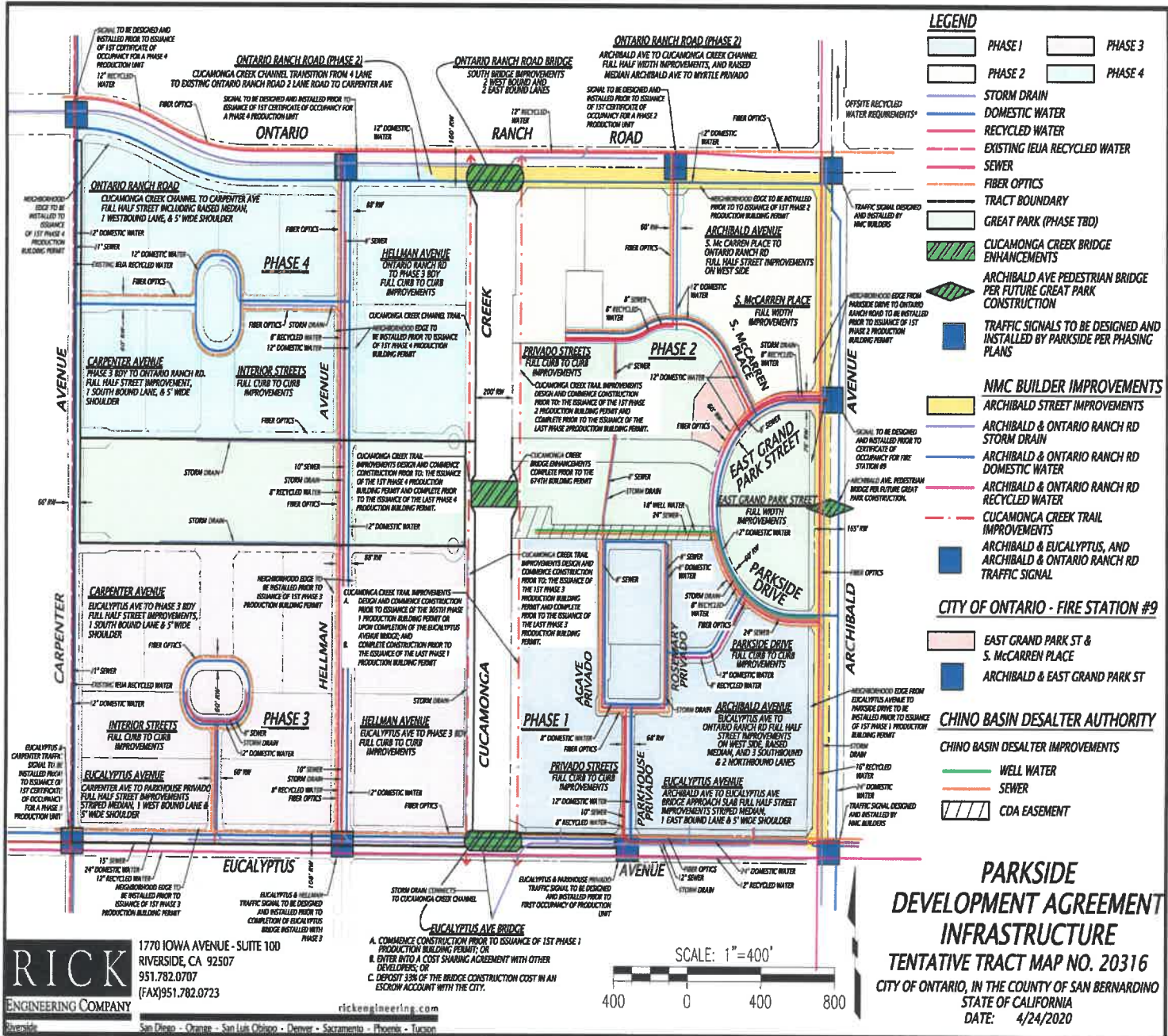
- a) Adopted Ordinance No. 3134 approving the Third Amendment to the Development Agreement (File No. PDA05-002) between the City of Ontario and SC Ontario Development Company, LLC.

EXHIBIT “D-R”

EXISTING LAND USE REGULATIONS

The Documents are listed for reference only:

1. Parkside Specific Plan Environmental Impact Report, Resolution No. 2006-068.
2. General Plan Amendment (File No. PGPA04-003), Resolution No. 2006-068A.
3. Parkside Specific Plan (File No. PSP03-002), Ordinance No. 2838.
4. City of Ontario Municipal Code:
 - a. Six – Sanitation & Health
 - b. Seven – Public Works
 - c. Eight – Building Regulations
 - d. Nine – Development Code
 - e. Ten – Parks and Recreation



CONCEPTUAL PHASING PLAN

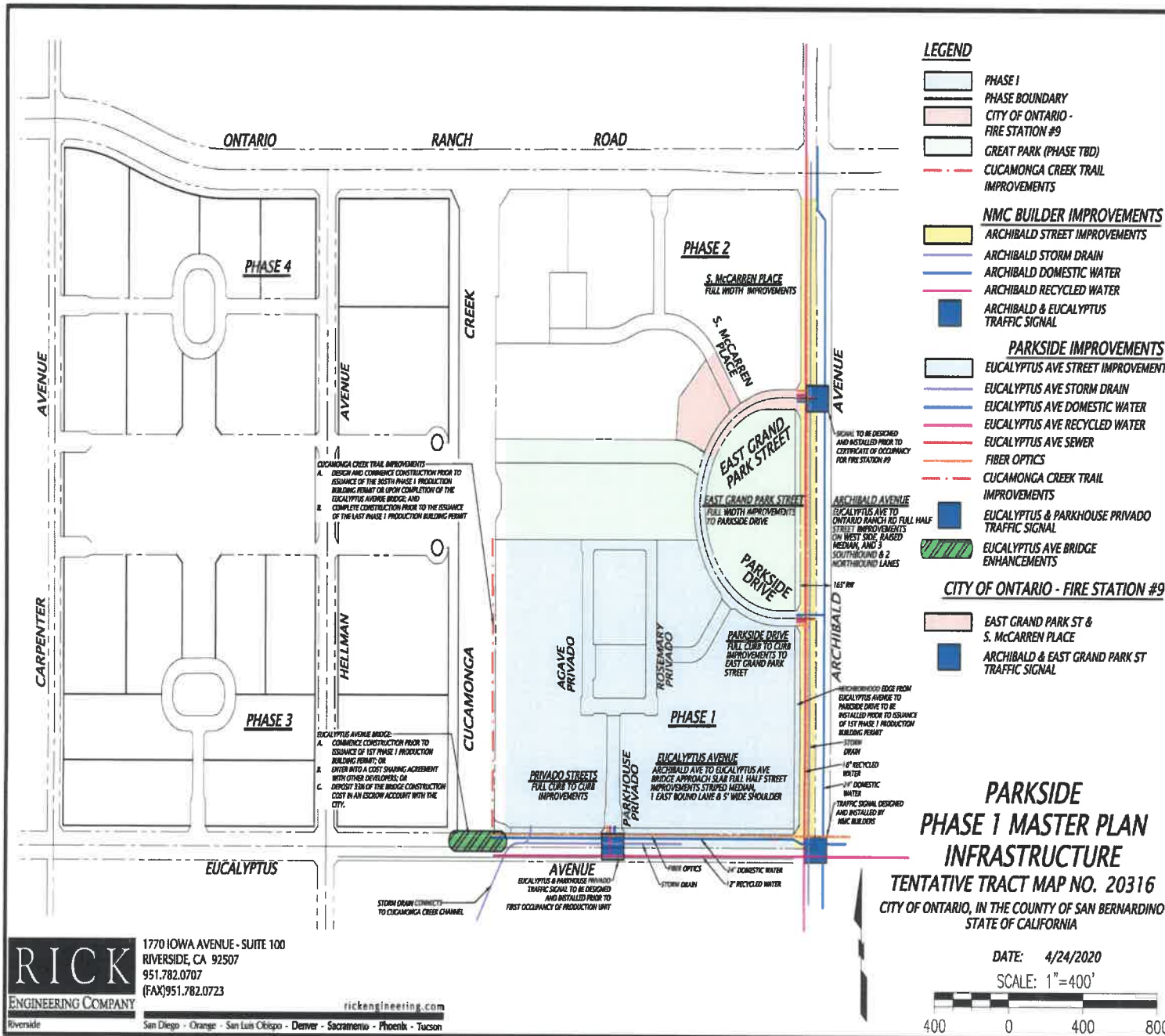
EXHIBIT "E-R"

EXHIBIT "F"

INFRASTRUCTURE IMPROVEMENTS EXHIBIT FOR ALL PHASES

[SEE ATTACHED]

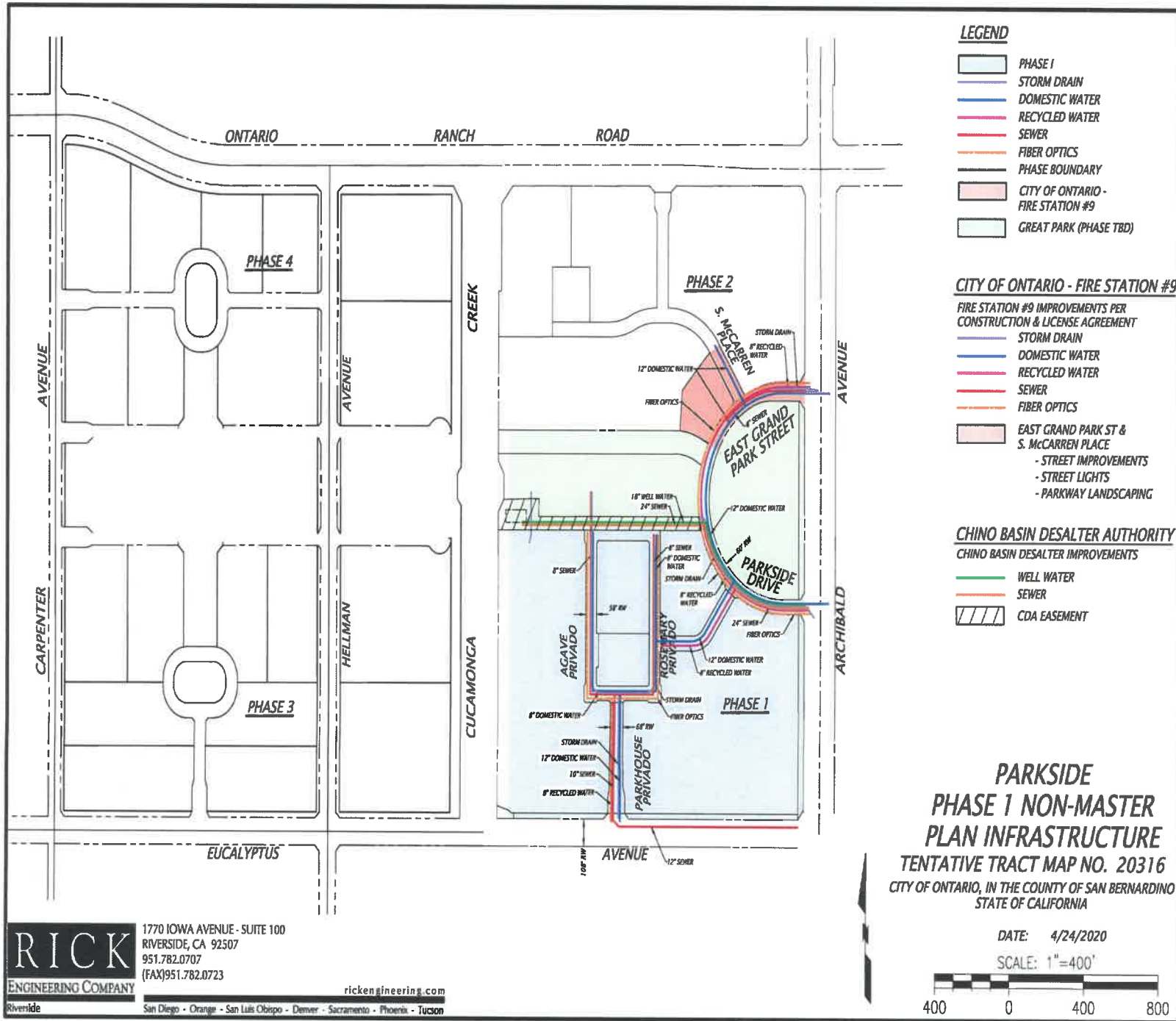
Exhibit "F-Phase 1" - Phase 1 Improvements



RICK
 ENGINEERING COMPANY
 1770 IOWA AVENUE - SUITE 100
 RIVERSIDE, CA 92507
 951.782.0707
 (FAX) 951.782.0723

rickengineering.com

Riverside San Diego - Orange - San Luis Obispo - Denver - Sacramento - Phoenix - Tucson



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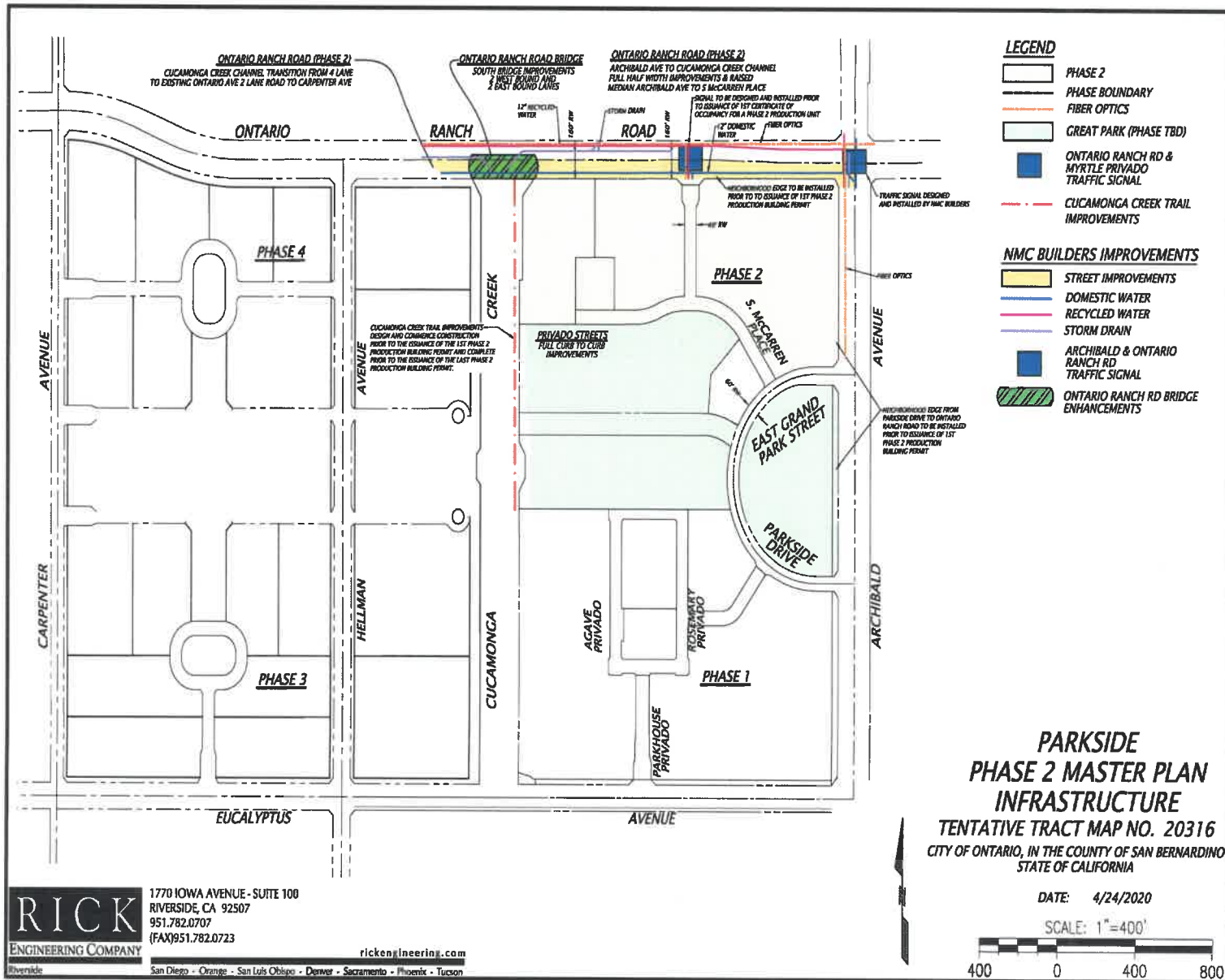
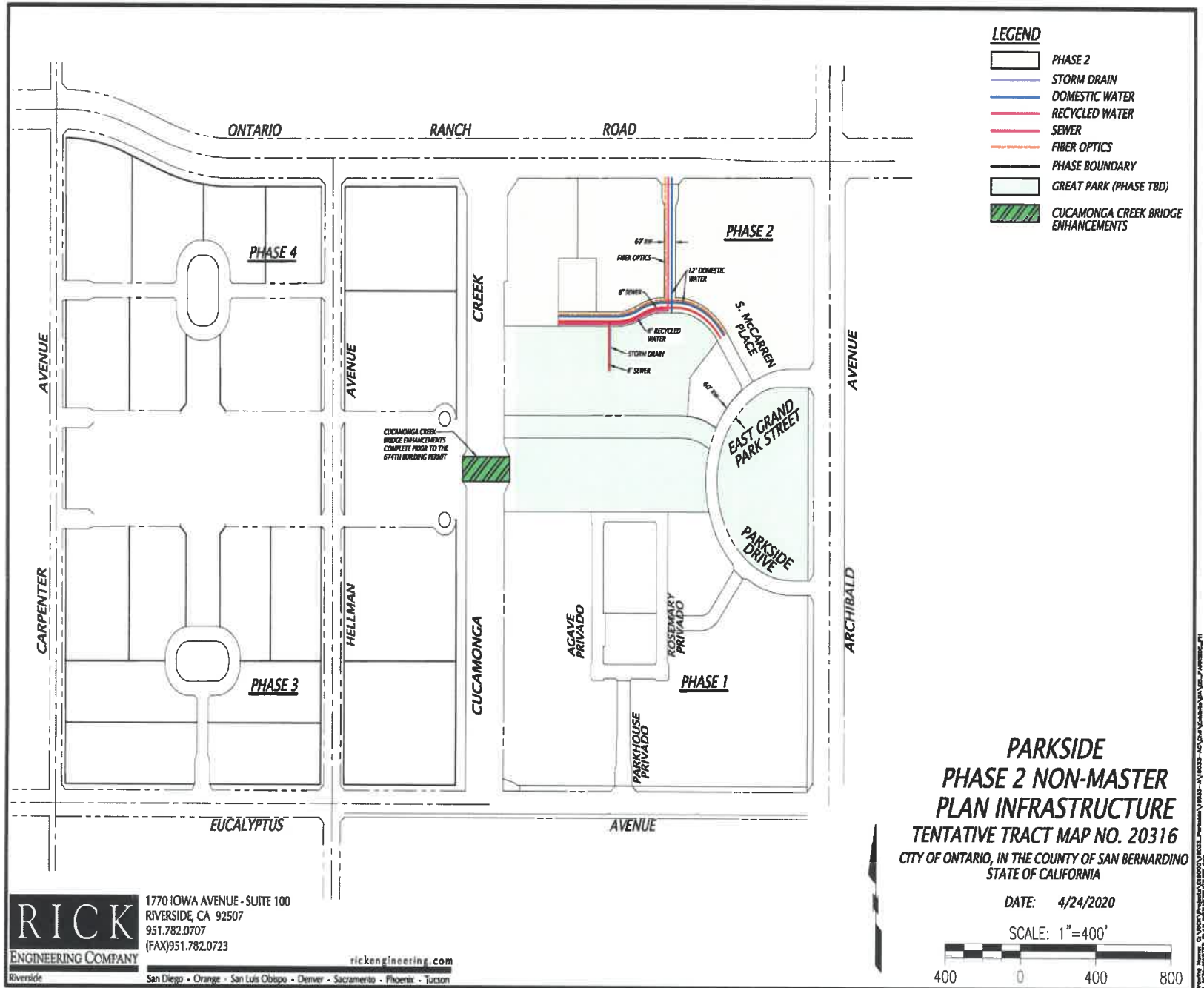


Exhibit "F-Phase 2" - Phase 2 Improvements



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**PARKSIDE
PHASE 2 NON-MASTER
PLAN INFRASTRUCTURE
TENTATIVE TRACT MAP NO. 20316
CITY OF ONTARIO, IN THE COUNTY OF SAN BERNARDINO
STATE OF CALIFORNIA**

DATE: 4/24/2020

SCALE: 1"=400'



Rick Engineering Company, 1770 Iowa Avenue, Suite 100, Riverside, CA 92507. Project No. 2020-01. Date: 4/24/2020. Scale: 1"=400'.

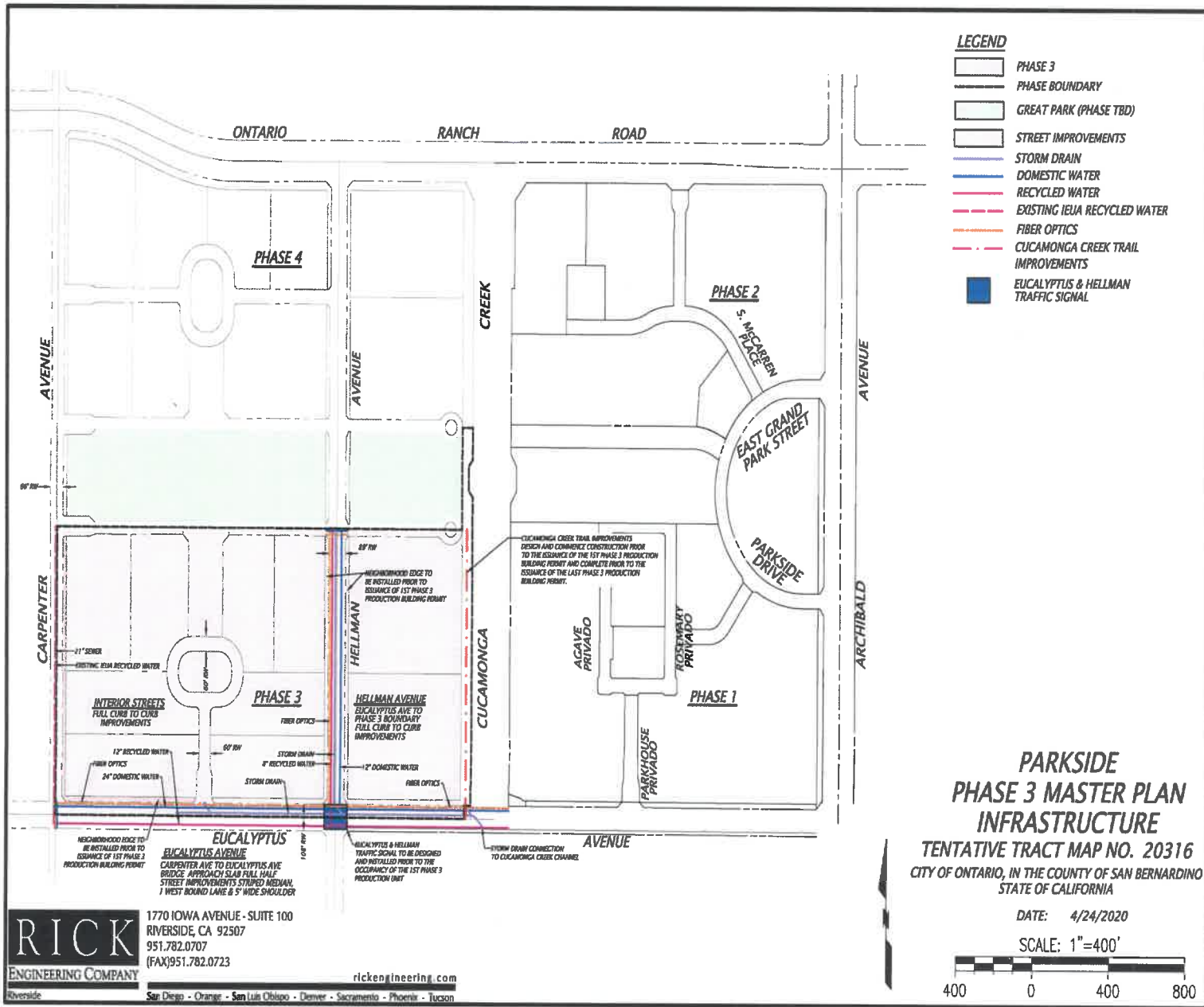
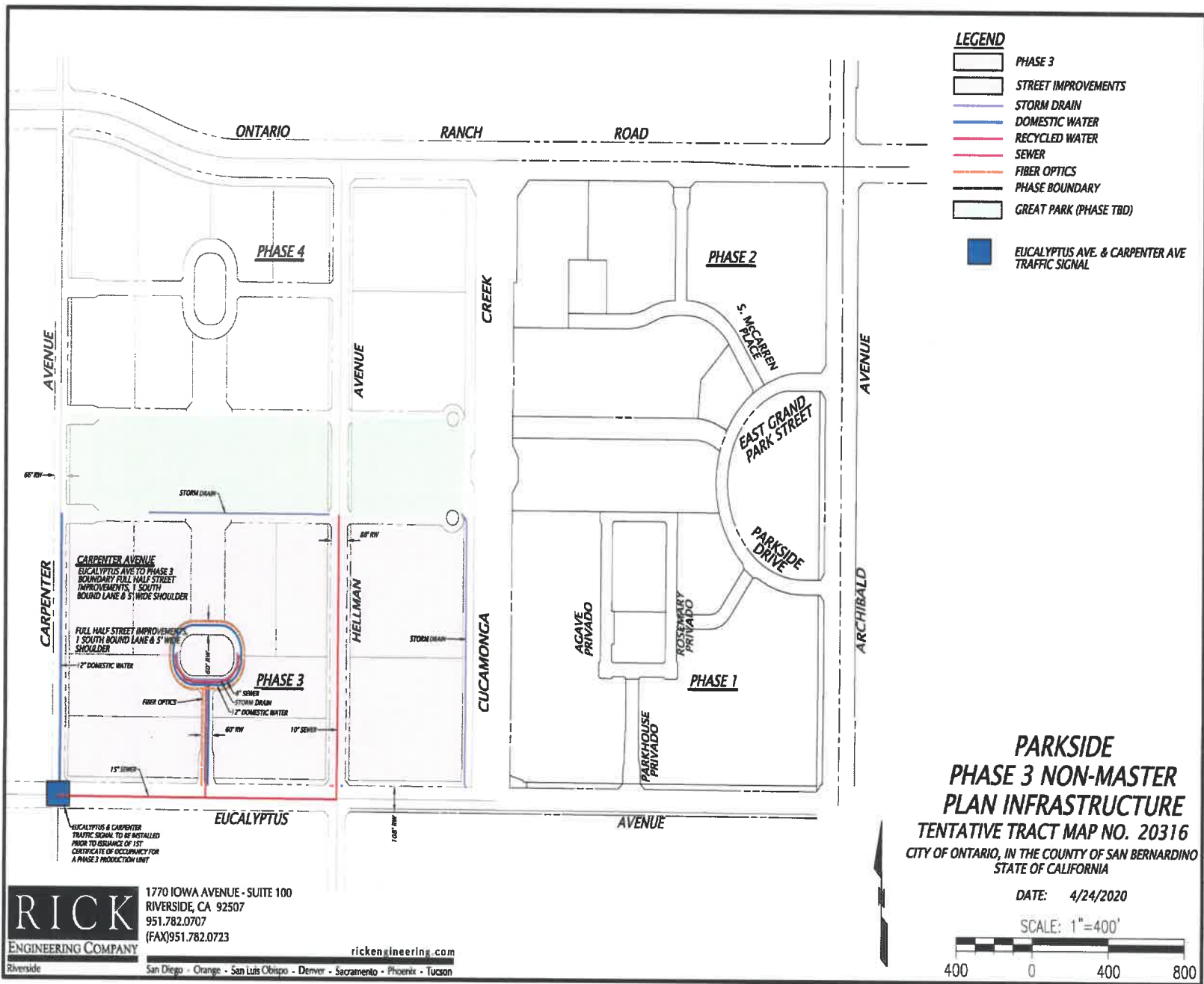


Exhibit "F-Phase 3" - Phase 3 Improvements



RICK
ENGINEERING COMPANY
1770 IOWA AVENUE - SUITE 100
RIVERSIDE, CA 92507
951.782.0707
(FAX)951.782.0723
rickengineering.com

**PARKSIDE
PHASE 3 NON-MASTER
PLAN INFRASTRUCTURE
TENTATIVE TRACT MAP NO. 20316**
CITY OF ONTARIO, IN THE COUNTY OF SAN BERNARDINO
STATE OF CALIFORNIA

DATE: 4/24/2020

SCALE: 1"=400'



DATE PLOTTED: 4/24/2020 10:54:33 AM
DRAWN BY: J. B. BROWN
CHECKED BY: J. B. BROWN
SCALE: 1"=400'
DATE: 4/24/2020

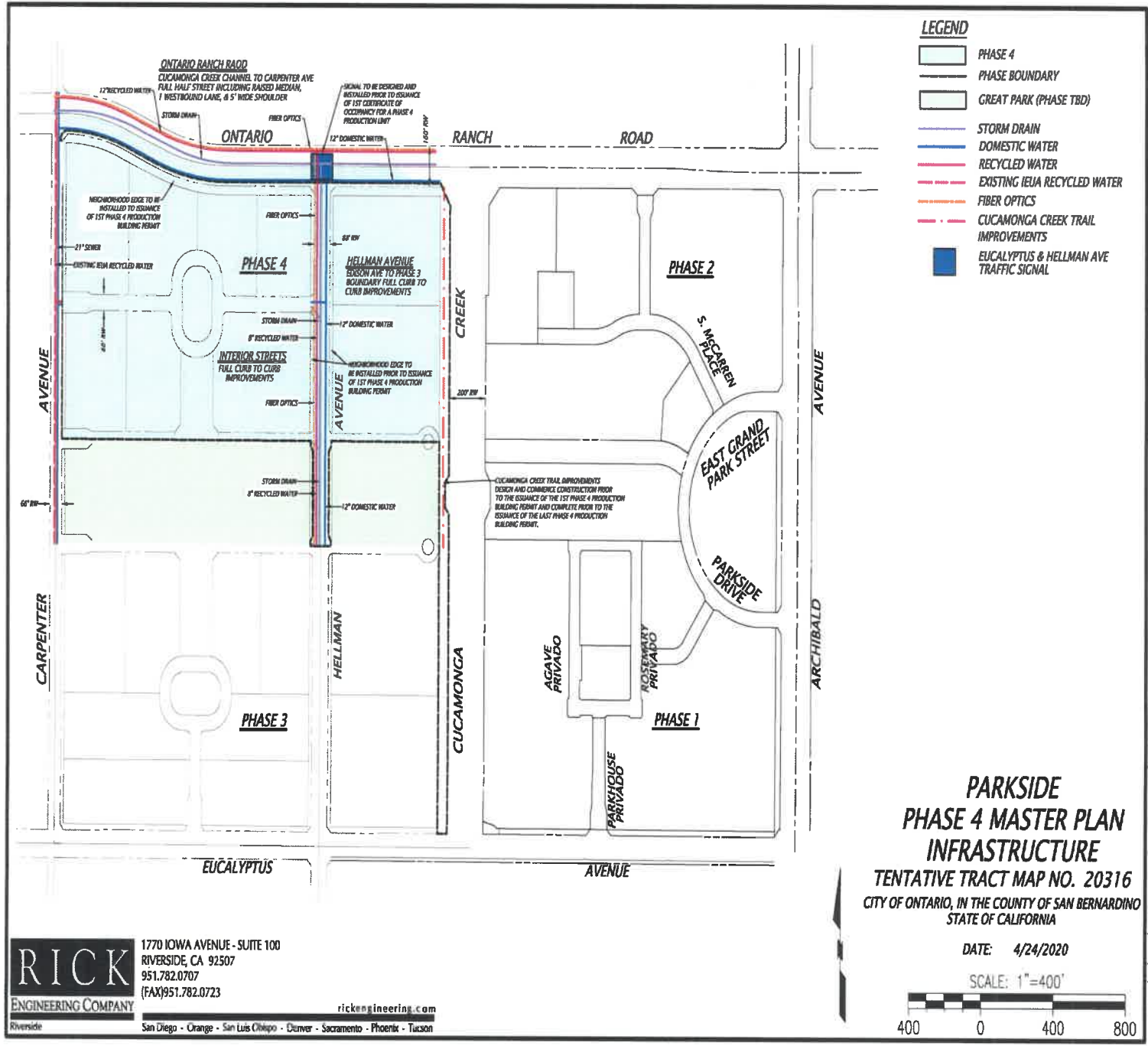
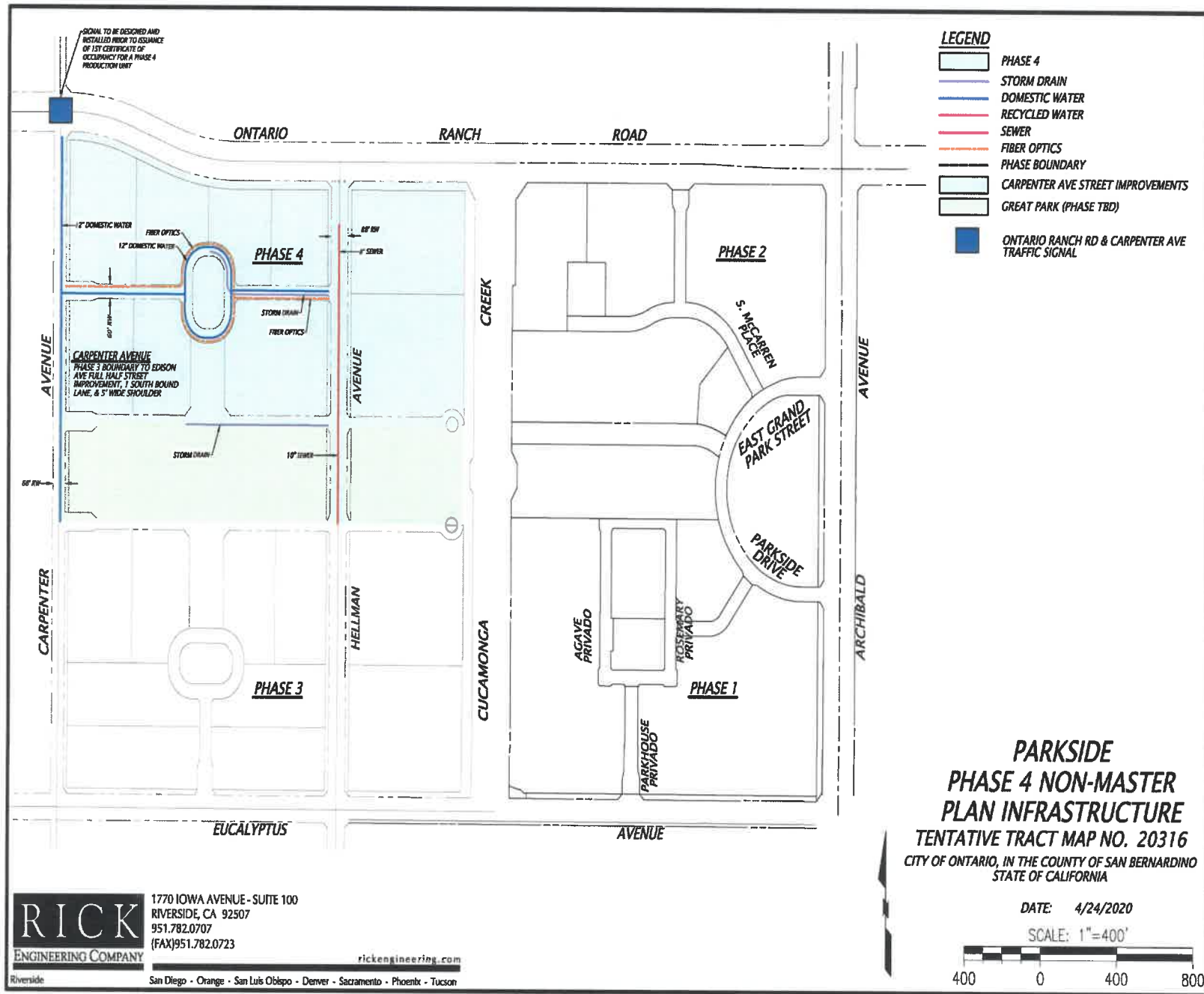


Exhibit "F-Phase 4" - Phase 4 Improvements



- LEGEND**
- PHASE 4
 - STORM DRAIN
 - DOMESTIC WATER
 - RECYCLED WATER
 - SEWER
 - FIBER OPTICS
 - PHASE BOUNDARY
 - CARPENTER AVE STREET IMPROVEMENTS
 - GREAT PARK (PHASE TBD)
 - ONTARIO RANCH RD & CARPENTER AVE TRAFFIC SIGNAL

**PARKSIDE
PHASE 4 NON-MASTER
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TENTATIVE TRACT MAP NO. 20316
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Riverside San Diego - Orange - San Luis Obispo - Denver - Sacramento - Phoenix - Tucson

Project Name: 05 Parkside Phase 4 Non-Master Plan Infrastructure, Tract Map No. 20316 - 11/2020 - 05/2021
 Date: 4/24/2020
 Scale: 1"=400'
 Author: Rick Engineering Company
 Designer: Rick Engineering Company
 Checker: Rick Engineering Company
 Title: 05/2021

CITY OF ONTARIO

Agenda Report
June 16, 2020

SECTION:
PUBLIC HEARINGS

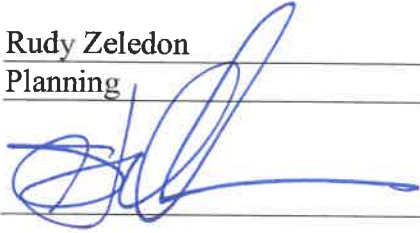
SUBJECT: A PUBLIC HEARING TO CONSIDER AN AMENDMENT TO THE PARKSIDE SPECIFIC PLAN (FILE NO. PSPA19-007) TO: [1] RECONFIGURE RESIDENTIAL PLANNING AREAS 1 THROUGH 4, AND 17 THROUGH 19; [2] RECONFIGURE THE GREAT PARK PLANNING AREA 22 (EAST OF THE CUCAMONGA CREEK CHANNEL); [3] REVISE INTERNAL CIRCULATION TO IMPROVE ACCESS INTO THE NEIGHBORHOOD COMMERCIAL PLANNING AREA 21; [4] UPDATE AND REVISE RESIDENTIAL DESIGN GUIDELINES (SECTIONS 7.1 THROUGH 7.6) TO INTRODUCE NEW HOUSING TYPES AND ARCHITECTURAL STYLES; AND [5] UPDATE AND REVISE LANDSCAPE STANDARDS (SECTION 7.7). THE PROJECT SITE IS BOUNDED BY ONTARIO RANCH ROAD TO THE NORTH, EUCALYPTUS AVENUE TO SOUTH, ARCHIBALD AVENUE TO THE EAST AND CARPENTER AVENUE TO THE WEST. (APNS): 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, AND 0218-221-10

RECOMMENDATION: That the City Council consider and adopt a resolution approving an Addendum to the Parkside Specific Plan Environmental Impact Report (SCH# 2004011008), analyzing the environmental effects of the Project, pursuant to State CEQA Guidelines section 15164; and adopt a resolution approving an Amendment to the Parkside Specific Plan (File No. PSPA19-007).

COUNCIL GOALS: Operate in a Businesslike Manner
Pursue City's Goals and Objectives by Working with Other Governmental Agencies
Focus Resources in Ontario's Commercial and Residential Neighborhoods

FISCAL IMPACT: Adoption of the Amendment to the Parkside Specific Plan does not propose an increase in residential units or commercial square feet allowed within the Specific Plan and therefore will not result in an increase of expenditures to the City. To offset the future increase in service expenditures,

STAFF MEMBER PRESENTING: Scott Murphy, AICP, Executive Director Development Agency

Prepared by: Rudy Zeledon
Department: Planning
City Manager Approval: 

Submitted to Council/O.H.A. 06/16/2020
Approved: _____
Continued to: _____
Denied: _____

20

an operations and maintenance Community Facilities District (CFD) will be established through the various tract map entitlements to cover the additional costs of police and fire services, landscape maintenance of medians, neighborhood edges, and street light operations and maintenance along the public streets.

BACKGROUND: The Parkside Specific Plan and the Environmental Impact Report (EIR) were approved by the City Council in September 2006. The Parkside Specific Plan established the land use designations, development standards, design guidelines and development capacity of 1,947 residential units and 115,000 square feet of commercial uses for the Specific Plan area. The Specific Plan is comprised of 26 land use districts, incorporating 19 distinctive neighborhoods and offering a variety of residential products.

On September 4, 2019, the Applicant submitted an Amendment to the Parkside Specific Plan (File No. PSPA19-007) that proposed the following:

- 1) *Reconfigure residential Planning Areas 1 through 4, and 17 through 19 and the Great Park Planning Area 22 (east of the Cucamonga Creek Channel).*

The land use plan for the Parkside Specific Plan offers a strong neighborhood character and identity for residents and visitors through a traditional approach to street design, architecture, and landscape design. To activate the streetscene and promote friendly interaction between neighbors, the Amendment to the Parkside Specific Plan proposes to reconfigure and expand Planning Areas 1 through 4. The reconfiguration and expansion of the Planning Areas 1 through 4 will allow for a more effective traditional residential single-family grid street design that enhances the pedestrian mobility, streetscene, and connectivity to the future Great Park. The attached Exhibit A illustrates the existing approved Parkside Specific Plan Land Use Plan and Exhibit B depicts the proposed Land Use Plan.

The reconfiguration and expansion of the Planning Areas 1 through 4 will also shift the Great Park (Planning Area 22) north, eliminating Planning Area 19 and reconfiguring Planning Areas 17 and 18. Shifting the Great Park north will create opportunities for an integrated mixed-use concept (village) with the Great Park, surrounding residential areas (Planning Areas 17 and 18) and the future commercial center (Planning Area 21), located at the southwest corner of Ontario Ranch Road and Archibald Avenue. This integration will also be accomplished through pedestrian walkways, bike network and trail linkages from the Great Park to the residential neighborhoods and future commercial center.

The reconfiguring of residential Planning Areas 1 through 4, 17 through 19 and Planning Area 22 (Great Park), will not increase or decrease the total amount residential units (1,947) or commercial square feet (115,000 SF) approved for the Specific Plan. Additionally, the shifting of the Great Park north, will not increase or decrease the park acreage (58.99 gross acres) approved originally by the Specific Plan and is consistent with the assumed TOP Policy Plan Land Use Plan park acreage for the Specific Plan area.

In addition to the reconfiguration of the planning areas as discussed above, the Specific Plan Land Use Plan has been simplified, recolored and the planning areas have been renumbered (see Exhibit B: Proposed Land Use Plan) and the product type descriptions on the Land Use Plan have been eliminated

and codified in the Specific Plan Land Use Summary Table (See Exhibit C: Table 1 – Parkside Land Use Summary Table). The Land Use Plan also includes the Chino Basin Desalter Authority’s well site (Planning Area 25) within the Great Park.

2) *Revise internal circulation to improve access into the neighborhood commercial Planning Area 21.*

To provide for a more efficient movement of vehicular traffic throughout the Specific Plan, as well as a pleasant environment for pedestrian and bicycle circulation, the internal circulation network within the Specific Plan has been revised (See Exhibit D: Proposed Circulation Master Plan). The revised internal circulation network eliminates the loop and u-shaped street network to a simplified street grind pattern design, which helps enforce and ensure traditional neighborhood design. To improve access to the neighborhood commercial center, a new street is proposed along the southwestern boundary of the commercial center. The street will provide access to the future commercial center from the residential Planning Areas 17 and 18 and to Archibald Avenue from Grand Park Drive.

3) *Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles.*

The Parkside Specific Plan provides for a mix of predominately multi-family products with some single-family detached green cluster products (6 and 8 pack). The Amendment proposes to introduce a new Conventionally Loaded Lane single family home product, and a new Detached Green Court Cluster (Up to 8 -10 Plex) single family home product. The characteristics of the two products are as follows:

- **Conventionally-Loaded Lanes Homes:** This product consists of small lot single family homes which are characterized by a private lane (Alley) that provides garage and primary entry access into each unit, with the exception of street facing units that have primary entry access from a street (See Exhibit E: Conventionally-Loaded Lanes Homes). The products feature private yards and patio areas. Parking is provided in garages and driveways.
- **Detached Green Court Cluster (Up to 8-10 Plex):** The Specific Plan currently allows for a Detached Green Court (Primary entry from street or paseo) and Motor Court (Primary entry from street or motor court alley) Cluster (6 and 8 Plex) single family homes products. The new Detached Green Court Cluster product will allow for a cluster of up to 8 to 10 single family homes, with smaller and more efficient home footprints. The single-family homes are clustered around a green paseo, with primary entry access from a paseo or street (See Exhibit F: Detached Green Court Cluster (Up to 8-10 Plex)). Parking for residents are provided in garages or uncovered parking spaces. The products feature private side yards and patio areas.

The introduction of the new home product types provides for additional single-family detached homes within the Specific Plan that are more attainable than traditional larger lot homes. While adding to the variety of housing products allowed within the Specific Plan, these new single-family detached products provide housing opportunities for every stage of life and price points to encourage the development of housing supportive of our efforts to attract business in growing sectors of the community (TOP Policy CE1-6).

The Parkside Specific Plan allows for a variety of traditional American styles found throughout California and Ontario. These styles include American Traditional, Craftsman, and Mediterranean. The Amendment proposes to introduce a new Millennium series architectural styles.

The Millennium Collection represents subtle differences in the architectural vocabulary using creative interpretation of traditional styles, utilizing embellished statements and details to express an iconic style in a new vocabulary (See Exhibit G: Millennium Collection Architectural Styles). Elevations can be abstracted architectural expressions of a recognizable style. These elevations are encouraged to push the artistic envelope of design to incorporate new, modern or alternative forms, details and materials in the modern context of architecture. Styles within this series include the following:

- Adaptive American Traditional
- Adaptive Farmhouse
- Adaptive Mediterranean
- Contemporary

4) *Update and revise Landscape Standards (Section 7.7).*

The Amendment to the Parkside Specific Plan proposes to redesign the community entry monumentation and perimeter wall theme. Monumentation occurs throughout the Parkside Specific Plan community and is designed to establish a hierarchy for entering each area of the community. Along the perimeter edges there are several entry points into the community. At key entries, a landscape and monumentation program will be utilized to help identify the community as well as convey a “welcoming” feeling for both vehicular and pedestrian traffic. Five entry and monument treatments are used to set this hierarchy: Community Gateway Monuments, Primary Community Gateway Entries, Major Community Gateway Entries, Secondary Community Entries, and Secondary Community Entry Monuments. (See Exhibit H: Community Gateway Monument Plan).

The current Specific Plan community monumentation design calls out for a more traditional design, with decorative brick, stone or split face block pilasters (with sign inlay) and face walls. The new proposed monumentation and wall design features a more iconic and modern design. The proposed modern monumentation design (See Exhibit I: Proposed Community Gateway Monument Concept) incorporates the following:

- 11-foot-high stone veneer column, with a pre-cast cap and steel eyebrow trellis
- 6-foot-high monumentation wall will feature an exterior “board form” tile finish, project sign logo location, community sign with raised letters on top of a horizontal mantel.

The perimeter community split face block wall design, that transitions from the community monumentation walls, has been revised to feature a “canyon bluff” tan exterior wall color. The “canyon bluff” tan color will replace the darker “sienna brown” exterior wall color required by the Specific Plan. The new “canyon bluff” tan exterior wall color will complement and provide a unified design with the new proposed monumentation design.

All changes and additions to the Specific Plan (exhibits, tables, development standards and design guidelines) are contained within the revised Specific Plan document accompanying resolution of this

report. All additions to the Specific Plan have been highlighted in blue and all deletion have been highlighted in red with a strikethrough.

On May 26, 2020, the Planning Commission conducted a public hearing and voted unanimously (6-0) to recommend City Council adopt an Addendum to the Parkside Specific Plan Environmental Impact Report (SCH# 2008101140) and a resolution approving an Amendment to the Parkside Specific Plan File No. PSPA19-007.

HOUSING ELEMENT COMPLIANCE: The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the proposed project is consistent with the 1,947 number of dwelling units Specified in the Available Land Inventory. Per the Available Land Inventory, the Parkside Specific Plan is required to provide 1,947 dwelling units with an overall density range of between 9 and 21 DU/AC.

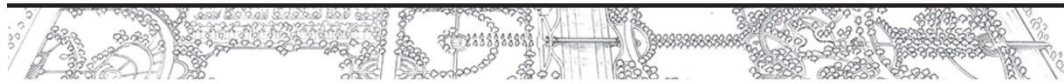
AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The project site is located within the Airport Influence Area of the Ontario International Airport and has been found to be consistent with the policies and criteria set forth within the Ontario International Airport Land Use Compatibility Plan.

ENVIRONMENTAL REVIEW: The environmental impacts of this project were analyzed in an Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures shall be a condition of project approval, as they are applicable, and are incorporated herein by this reference.

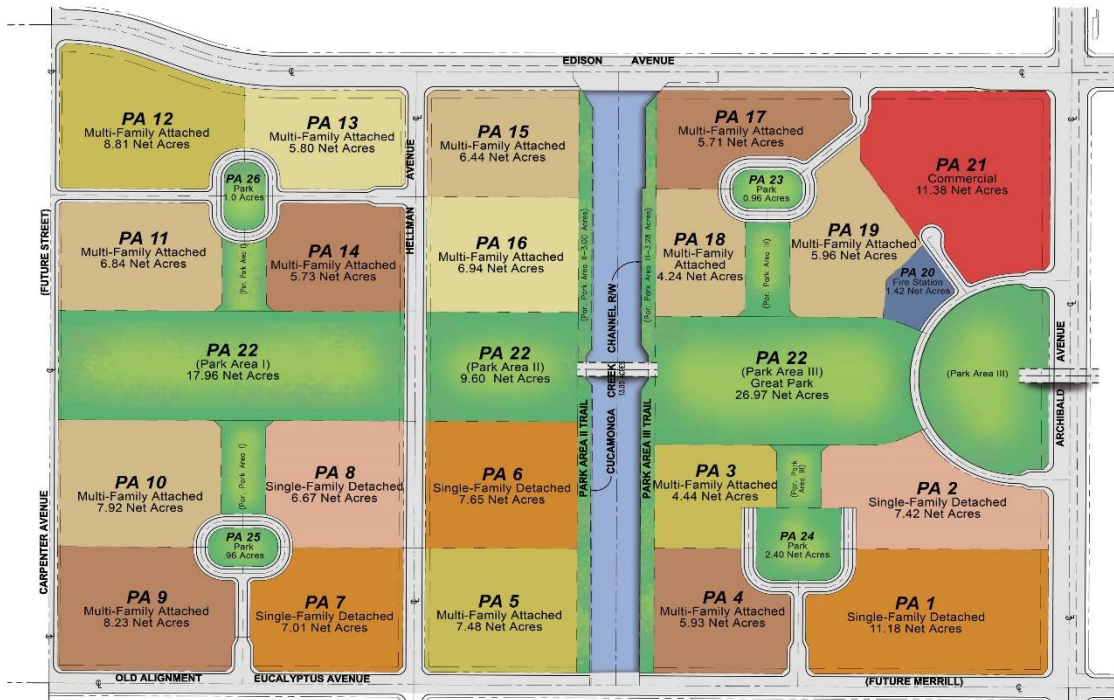
AGENDA REPORT EXHIBITS A - I

(Exhibits to follow this page)

Exhibit A – Existing Approved Parkside Specific Plan Land Use Plan



Parkside



Notes:

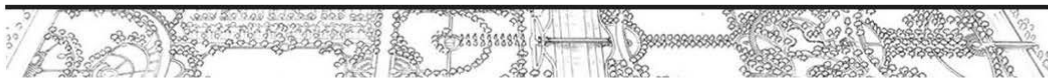
Net acres is Planning Area (PA) acreage, minus street R/W

© 2006 WILLIAM HEZMALHACH ARCHITECTS, INC



Exhibit 10—Land Use Plan

Exhibit B – Proposed Parkside Specific Plan Land Use Plan



Parkside



Exhibit 1-3: Land Use Plan

Legend	
	Medium Density Residential (11.1 - 25 du/ac)
	Neighborhood Commercial (0.4 FAR)
	Public Facility
	Open Space - Parkland
	Open Space - Private/Village Edges
	Roads - Master Planned
	Roads - Local
	Cucamonga Creek Channel

Exhibit C – Table 1 – Parkside Land Use Summary



Parkside

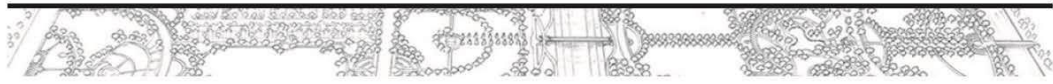
Table 1-1: Parkside Land Use Summary

Land Use	Lot Size / Use	Gross Acreage	Net Acreage	*Density Range / S.F.	Lots / D.U.
Residential					
PA-1 (Green Court/Lane Loaded Products 1, 2 & 4)	SFD	19.97	15.57	10-18	236
PA-2 (Lane Loaded Product 3)	SFD	3.57	2.66	10-18	41
PA-3 (Lane Loaded Product 3)	SFD	3.06	2.38	10-18	42
PA-4 (Green Court/Lane Loaded Products 1, 2, 3 & 4)	SFD	15.50	13.62	10-18	221
PA-5 (HD Townhomes)	MF	9.57	8.14	15-22	139
PA-6 (Attached Auto Court)	MF	7.34	6.89	8-14	75
PA-7 (Auto Court 6-8 Plex)	SFD	9.30	7.49	8-14	68
PA-8 (Green Court 8-10 Plex)	SFD	6.64	6.18	10-16	68
PA-9 (Triplex)	MF	10.49	8.70	10-16	120
PA-10 (Row Townhomes)	MF	7.56	7.22	17-25	143
PA-11 (HD Townhomes)	MF	8.11	7.22	15-22	123
PA-12 (Triplex)	MF	11.80	9.30	12-18	127
PA-13 (Attached Auto Court)	MF	8.53	6.34	8-14	64
PA-14 (Duplex)	MF	7.12	6.18	12-18	99
PA-15 (Duplex)	MF	8.62	6.97	12-18	111
PA-16 (Triplex)	MF	7.83	7.35	10-16	90
PA-17 (Row Townhomes)	MF	4.87	4.54	17-25	82
PA-18 (HD Townhomes)	MF	6.74	5.04	15-22	98
Residential Subtotal		156.62	131.79	14.77	1,947
Non-Residential					
PA-19 (Commercial)		15.66	11.15	115,000 SF	-
PA-20 (Fire Station)		1.72	1.39	-	-
PA-21 (Great Park)		58.86	54.40	-	-
PA-22 (Private Rec)		1.00	1.00	-	-
PA-23 (Private Rec)		1.90	1.34	-	-
PA-24 (Private Rec)		1.00	1.00	-	-
PA-25 (Private Rec)		1.00	1.00	-	-
PA-26 (CD Well Site)		0.13	0.13	-	-
Non-Residential Subtotal		81.27	71.41	115,000 SF	-
Channel ROW		13.00	13.00	-	-
Total Project		250.89	216.20		1,947

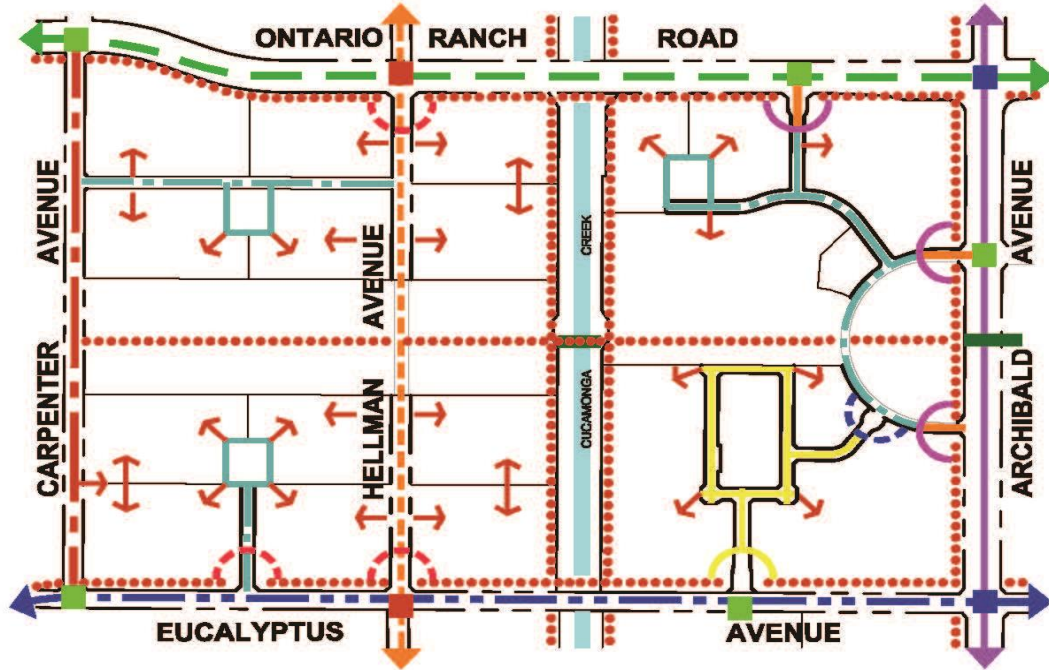
NOTES:

- 1) Gross acres taken to center line of streets.
- 2) Adjusted gross acres taken to street RDW dedicated to the City
- 3) Net acres taken to street right of way
- 4) A total of 34.69 acres of master planned roadway is included in the project
- * Density range is shown; it is based on net acres. The General Plan is based on adjusted gross acres
- 5) Proposed product types in each Planning Area are subject to change based on final development submittal

Exhibit D – Proposed Parkside Specific Plan Circulation Plan



Parkside



LEGEND

- | | | | | | |
|--|--|--|---|--|---------------------------------------|
| | SPECIFIC PLAN BOUNDARY | | SECONDARY COMMUNITY ENTRY STREETS (60/72/76' R/W) | | PRIMARY COMMUNITY GATEWAY ENTRY |
| | EXISTING TRAFFIC SIGNAL WITH MODIFICATIONS | | PRIMARY LOCAL STREETS (36/46/60/64' R/W WITH 36' PAVING) R/W DEPENDS ON ADJACENCY WITH PARK | | SECONDARY COMMUNITY ENTRY (GATED) |
| | MASTER PLAN TRAFFIC SIGNAL | | PRIMARY PRIVATE STREETS (40/56/60/68' R/W) | | MAJOR COMMUNITY GATEWAY ENTRY (GATED) |
| | TRAFFIC SIGNAL TO BE INSTALLED WHEN WARRANTED | | CLASS 1 BIKE PATH (OFF-STREET) | | SECONDARY COMMUNITY ENTRY |
| | INTERNAL CIRCULATION POINTS | | PEDESTRIAN BRIDGE | | |
| | ARCHIBALD: DIVIDED ARTERIAL PARKWAY 1A WITH CLASS 1 BIKE PATH (165' R/W) | | | | |
| | ONTARIO RANCH: DIVIDED ARTERIAL PARKWAY 1A (180' R/W) | | | | |
| | EUCALYPTUS: STANDARD ARTERIAL (108' R/W) | | | | |
| | HELLMAN: COLLECTOR STREET (88' R/W) | | | | |

Exhibit 5-1: Vehicular Circulation Master Plan

Exhibit E – Conventionally-Loaded Lanes Homes



Parkside

Conventionally Loaded Lane Homes

(PA 1-4)

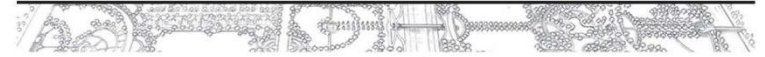


Characteristics

- Single-family small lot detached dwellings face street or lanes (private alleys);
- Automobile access via streets or lanes (private alleys);
- Primary entries and walks face streets or lanes (private alleys);
- Private yards/patios;
- Reciprocal use easements may extend private spaces on sides of buildings;
- Parking for residents provided in garages and driveways; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

Notes

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
- Refer to other areas of Section 7—Residential Design Guidelines, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
- Refer to other areas of Section 7—Residential Design Guidelines, for specific criteria regarding all landscape design and implementation.
- Typical Product Plotting Concepts are provided for representative purposes only and are not intended to be mandated layouts.



Parkside

Conventionally Loaded Lane Homes - Product 2

(PA 1 & 4)

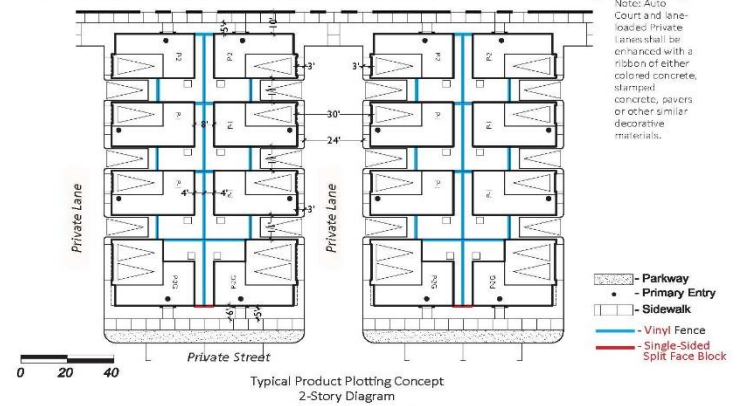
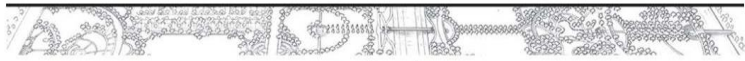


Table 7-1: Conventionally Loaded Lane Homes Development Standards

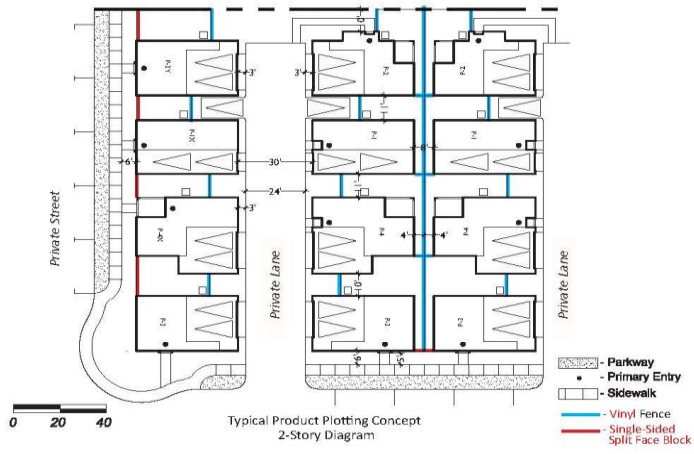
Building Separation	Garage Setbacks
Front to side living space	Garage face to garage face
Side to side living space	Garage face to lane (drive apron)
Side to side living space (with parking space in between)	Drive aprons shall be 5' or less in length or 18' in length or greater.
Rear living to rear living	
	Maximum Encroachments into Building Setbacks
Front Setbacks (Private Street Facing) - To Back of Sidewalk:	Second/third floor cantilever over garage
Living space	Fireplace, media niche (8' max. length), bay windows, furr outs, etc.
Porch, balcony	Potshelves, brackets, eaves, awnings, eyebrows, canopies, cantilever decks, etc.
Garden wall	
Front setbacks (Private Lane facing)	
	Private Open Space Per Unit
Side Setbacks - To Parcel Edge/Site Boundary	Minimum dimension
Living space to property line	Total per unit
Non-living space (garage-single story)	
Living space (adjacent to Cucamonga Creek Channel)	
	Parking
Rear Setbacks - To Parcel Edge/Site Boundary	2 spaces per homes, at least 1 is required to be covered
Living space (2-story)	
Living space (3-story)	
	Building Height
	Architectural projections may exceed the maximum building height of 42' by an additional 3'.

Exhibit E – Conventionally-Loaded Lanes Homes Continued



Parkside

Conventionally-Loaded Lane Homes (up to 12-Plex)- Product 3 (PA 2, 3 & 4)

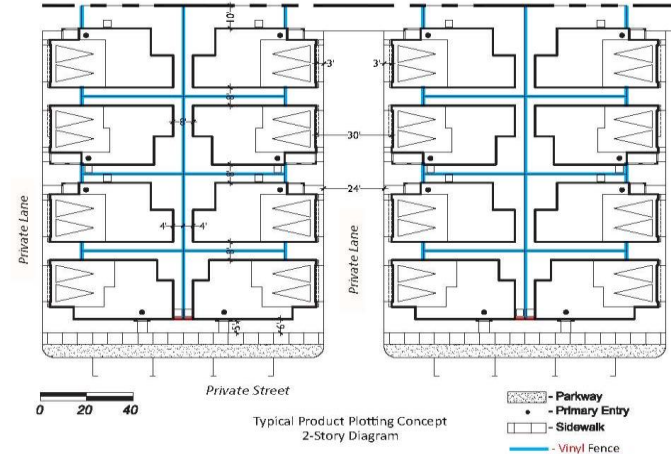


Note: Auto Court and lane-loaded Private Lanes shall be enhanced with a ribbon of either colored concrete, stamped concrete, pavers or other similar decorative materials.



Parkside

Conventionally-Loaded Lane Homes- Product 4 (PA 1 & 4)



Note: Auto Court and lane-loaded Private Lanes shall be enhanced with a ribbon of either colored concrete, stamped concrete, pavers or other similar decorative materials.

Exhibit E – Conventionally-Loaded Lanes Homes Continued



Parkside

Conventionally Loaded Lane Homes

(PA 1, 7 & 8)*

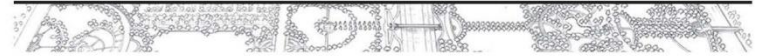


Characteristics

- Conventionally loaded lane homes are dwellings face street or lanes (private alleys);
- Automobile access via streets or lanes (private alleys);
- Primary entries and walks face streets or lanes (private alleys);
- Private yards/patios;
- Reciprocal use easements may extend private spaces on sides of buildings;
- Parking for residents provided in garages and driveways; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

Notes

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
- Refer to other areas of Section 7—Residential Design Guidelines, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
- Refer to other areas of Section 7—Residential Design Guidelines, for specific criteria regarding all landscape design and implementation.
- Typical Product Plotting Concepts are provided for representative purposes only and are not intended to be mandated layouts.



Parkside

Conventionally Loaded Lane Home

(PA 1, 7 & 8)*

* Proposed product types in each Planning Area are subject to change based on final development submittal.

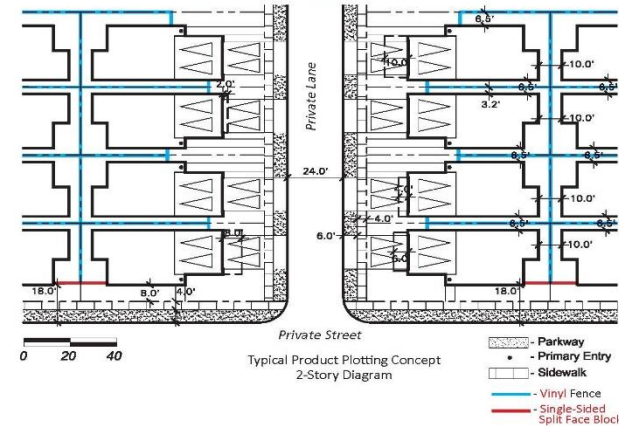


Table 7-3: Conventionally Loaded Lane Home Development Standards

Building Separation		Maximum Encroachments into Building Setbacks	
Front to side living space	N/A	Second floor cantilever over garage	10'
Side to side living space	6.5'		
Rear living to rear living	10'		
		Private Open Space Per Unit	
		Minimum dimension	10'
		Total per unit	200 s.f.
Front Setbacks (Private Street Facing) - To Back of Sidewalk		Parking	Per Ontario Development Code
Living space	8'		
Porch, balcony	5'		
Garden wall	3'		
Garage Setbacks		Building Height	
Garage face to lane (drive apron)	18'	Architectural projections may exceed the maximum building height of 35' by an additional 5'.	
Drive aprons shall be 5' or less in length or 18' in length or greater.			

Exhibit E – Conventionally-Loaded Lanes Homes Continued



Parkside

Conventionally Loaded Lane Home

(PA 1, 7 & 8)*

* Proposed product types in each Planning Area are subject to change based on final development submittal.



Characteristics

- Conventionally Loaded Lane Homes are detached dwellings face street or lanes (private alleys);
- Automobile access via streets or lanes (private alleys);
- Primary entries and walks face streets or lanes (private alleys);
- Private yards/patios;
- Reciprocal use easements may extend private spaces on sides of buildings;
- Parking for residents provided in garages and driveways; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

Notes

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
- Refer to other areas of Section 7—Residential Design Guidelines, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
- Refer to other areas of Section 7—Residential Design Guidelines, for specific criteria regarding all landscape design and implementation.
- Typical Product Plotting Concepts are provided for representative purposes only and are not intended to be mandated layouts.

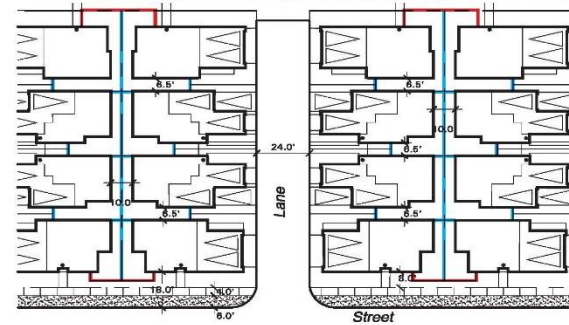


Parkside

Conventionally Loaded Lane Home

(PA 1, 7 & 8)*

* Proposed product types in each Planning Area are subject to change based on final development submittal.



Notes: Auto Court and lane-loaded Private Lanes shall be enhanced with a ribbon of either colored concrete, stamped concrete, pavers or other similar decorative materials.



Table 7-4: Conventionally Loaded Lane Home Development Standards

Building Separation		Maximum Encroachments into Building Setbacks	
Front to side living space	6.5'	Second floor cantilever over garage	2'
Side to side living space	6.5'		
Rear living to rear living	10'		
		Private Open Space Per Unit	
		Minimum dimension	10'
		Total per unit	100 s.f.
Front Setbacks (Private Street Facing) - To Back of Sidewalk		Parking	
Living space	8'	Per Ontario Development Code	
Porch, balcony	5'		
Garden wall	3'		
Garage Setbacks		Building Height	
Garage face to lane (drive apron)	8'	Architectural projections may exceed the maximum building height of 35' by an additional 5'	
Drive aprons shall be 5' or less in length or 18' in length or greater.			

Exhibit F – Detached Green Court Cluster



Parkside

Detached Green Court Cluster (8 to 10 Plex)

(PA 1, 7 & 8)*

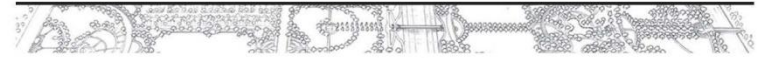


Characteristics

- Single-family detached dwellings clustered around a Auto Court;
- Automobile access via common Auto Court driveway;
- Primary entries and walks face Auto Court or street;
- Private side and rear yards;
- Parking for residents provided in garages;
- Guest parking provided on local streets, designated parking areas, or as approved by the City; and
- Reciprocal use easements extend private space.

Notes

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
- Refer to other areas of Section 7—Residential Design Guidelines, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
- Refer to other areas of Section 7—Residential Design Guidelines, for specific criteria regarding all landscape design and implementation.
- Typical Product Plotting Concepts are provided for representative purposes only and are not intended to be mandated layouts.



Parkside

Detached Green Court Cluster (8 to 10-Plex)

(PA 1, 7 & 8)*

* Proposed product types in each Planning Area are subject to change based on final development submittal.

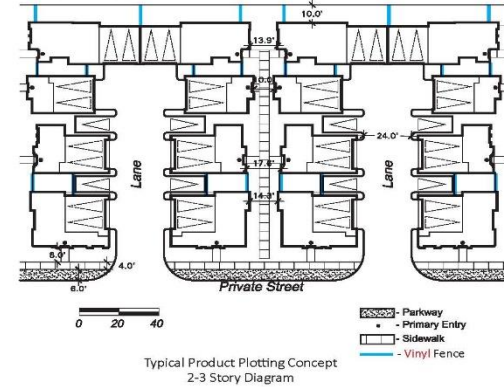


Table 7-6: Detached Green Court Cluster Development Standards

Building Separation	Maximum Encroachments into Building Setbacks
Front to front living space: 10'	Fireplace, media niche (8' max length), bay windows (cantilevered), potshelves, brackets, etc.: 2'
Side to side living space: 8' or 2" airspace	Second floor cantilever over garage: 2'
Front (Private Street-Facing) Setbacks - To Back of Sidewalk	Private Open Space Per Unit
Living space: 8'	Minimum dimension: 7'
Porch, balcony: 5'	Total per unit: 70 s.f.
Garden wall: 3'	
Side Setbacks - To Lot Edge	Parking
Living space to property line: 4'	Per Ontario Development Code
Rear Setbacks - To Lot Edge	Building Height
Living space: 10'	Architectural projections may exceed the maximum building height of 42' by an additional 3', * 3' min. from PL
Garage Setbacks	Note: Green Court pedestrian entries may have decorative arbor (or other similar decorative design feature to enhance pedestrian scale and street scene.
Garage face to garage face: 30'	
Garage face to driveway (apron): 3'-5'	
Drive aprons shall be 5' or less in length or 18" in length or greater.	
Note: Lane width will increase to 24' when the length exceeds 150'	

Exhibit F – Detached Green Court Cluster Continued



Parkside

Detached Auto Court Cluster (6 to 8-Plex)

(PA 1, 7 & 8)*



Characteristics

- Single-family detached dwellings clustered around Auto Court;
- Automobile access via common Auto Court driveway;
- Primary entries and walks face Auto Court or street
- Private side and rear yards;
- Parking for residents provided in garages; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.
- Reciprocal use easements extend private space

Notes

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
- Refer to other areas of Section 7—Residential Design Guidelines, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
- Refer to other areas of Section 7—Residential Design Guidelines, for specific criteria regarding all landscape design and implementation.
- Typical Product Plotting Concepts are provided for representative purposes only and are not intended to be mandated layouts.



Parkside

Detached Auto Court Cluster (6 to 8-Plex)

(PA 1, 7, & 8)*

* Proposed product types in each Planning Area are subject to change based on final development submittal.

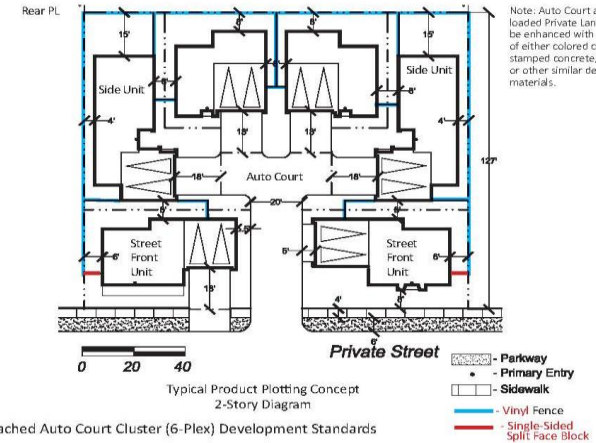


Table 7-7: Detached Auto Court Cluster (6-Plex) Development Standards

Building Separation			
Front to side living space	8' (4'+4')		
Side to side living space	8' (4'+4')		
Front Setbacks (Private Street Facing) - To Back of Sidewalk			
Living space	8'		
Porch, balcony	5'		
Garden wall	3'		
Side Setbacks - To Lot Edge			
Living space	4'		
Garage Setbacks			
Garage facing street	18'		
Garage face to garage face	30'		
Garage face to lane (drive apron)	5'		
Drive aprons shall be 5' or less in length or 18' in length or greater.			
Note: Lane width will increase to 24' when the length exceeds 150'			
		Maximum Encroachments into Building Setbacks	
		Fireplace, media niche (8' max length), bay windows (cantilevered), potshelves, brackets, etc.	2'*
		Second floor cantilever over garage	2'
		Private Open Space Per Unit	
		Minimum dimension	8'
		Total per unit	100' s.f.
		Parking	
		Per Ontario Development Code	
		Building Height	
		Architectural projections may exceed the maximum building height of 35' by an additional 5'.	
		*3' min. from PL	

Exhibit G – Millennium Collection Architectural Styles

Parkside

Adaptive American Traditional

American Traditional is a style evolved in the early 20th century originating from the New England states, spreading westward, and becoming established throughout the rest of the country.

The adaptive version of this style retains the massing and form that is reminiscent of early American heritage but with simpler lines and contemporary details. The massing breaks down the façade from 2 to 1-story forms providing a natural balance to the front elevation. Roofs are simple in form most often with accent gables. This contemporized American style has a fresh appeal while retaining its charming origins.



Parkside



Porch



Simplified Cornice Trim at Gable Ends

Element	Menu of Common Features
Form	<ul style="list-style-type: none"> Asymmetrical massing with a vertical and a horizontal break
Roof	<ul style="list-style-type: none"> 4:12 roof pitch 12" to 16" overhangs Concrete roof tile- flat or shake appearance Asphalt shingles Front to back gable or hip roof with intersecting hip or gable roofs
Walls	<ul style="list-style-type: none"> Stucco Horizontal siding- may be combined with stucco Wrapped material on sides and terminate at logical end (return block wall/fence) or inside corner
Windows	<ul style="list-style-type: none"> Vertical multi-paned window at front elevations Multi-paned window on side and rear elevations at visible edges Simplified cornice trim at gable ends Header window wood accent trim Simple 2 x 4/6 window and door trim- wood on siding, foam on stucco, 2 x 4 window and door trim may be used on specific product types, architectural styles subject to Planning Department approval.
Details	<ul style="list-style-type: none"> Surface mounted fixtures on front elevations must complement architectural style Porches and/or balconies with horizontal railing Front and garage door may have modern styling to complement style Square wood columns with trim Shutters, brick accents permitted
Colors	<ul style="list-style-type: none"> Field: Whites, off-white, dark or light colors Trim: White or contrasting with field color Accents: Light or dark colors in contrast or harmony with field colors

Exhibit G – Millennium Collection Architectural Styles Continued

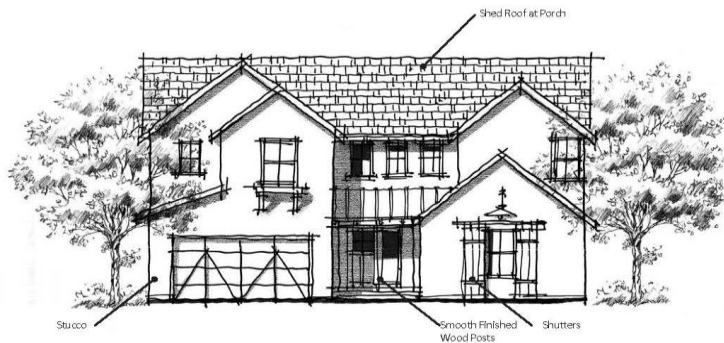


Parkside

Adaptive Farmhouse

Due to the simple and honest nature of this architectural form, the Farmhouse style can easily be adapted to a more abstract solution and is appropriate within many of the higher density settings. Since this style's roof shapes, variety of textures, colors and materials can easily be extrapolated to more contemporary applications, one can find many examples of this transformation throughout the West.

The Adaptive Farmhouse style blends the sleek clean lines of contemporary design with warm farmhouse charm to create a uniquely fresh take on this country living inspired style.



Parkside



Board and Batten Siding



Proper Terminate Points for Siding
and Color Accents

Element	Menu of Common Features
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Form	<ul style="list-style-type: none"> Symmetrical or asymmetrical massing
Roofs	<ul style="list-style-type: none"> Roof pitches 4:12 to 6:12, possibly lower pitch at main roof Shed roofs at porches, may use metal seam 0" - 12" overhang at eaves Asphalt shingle roof or tile shake pattern
Walls	<ul style="list-style-type: none"> Stucco Board and batten or horizontal siding Siding should return on sides and terminate at logical end (return block wall/fence) or inside corner
Windows	<ul style="list-style-type: none"> Vertically or horizontally proportioned Smooth finished posts (6" x 6" min.) with smooth finish cap and base trim Smooth finished beams Door and window surrounds shall consist of one of the following materials: <ul style="list-style-type: none"> Well-proportioned, stucco-wrapped, high density foam trim or Smooth textured wood
Details	<ul style="list-style-type: none"> Raised panel-style entry door and garage door Wood brackets below gables and/or rafter tails Awnings, may use metal seam Porches and/or balconies with horizontal railing Shutters Cementitious or stucco trim (with finish matching body of house) may be permitted at front doors/porch entries Front and garage doors may have modern styling to complement style
Colors	<ul style="list-style-type: none"> Field: Whites or light tinted colors Trim: Whites or light shades complementary to field color Accents: Light or dark shades in contrast with field color

Exhibit G – Millennium Collection Architectural Styles Continued

Parkside

Adaptive Mediterranean

The Adaptive Mediterranean style draws inspiration from Southern European countries along the Mediterranean Sea. Italian, Spanish, Portuguese and Greek architecture lend their influence to this style.

Simplified details borrowed from these Mediterranean styles include streamlined forms which move toward a modern and unpretentious aesthetic – including low-pitched, hipped or parapet roofs combined with a lighter color palette.



Parkside



Light or dark colors in contrast and in harmony with field colors



Limited ornamentation and no shutters

Element	Menu of Common Features
Form	<ul style="list-style-type: none"> Simple geometric forms
Roofs	<ul style="list-style-type: none"> Hip or gable roofs Roof pitches 4:12 to 6:12 where applicable, possibly lower pitch at main roof; may have a shed roof accent 0"-12" overhang S-shape, or flat tile, or asphalt shingle
Walls	<ul style="list-style-type: none"> Stucco Optional decorative ceramic tile
Windows	<ul style="list-style-type: none"> Vertically or horizontally proportioned windows or ganged windows Round top or arched primary window or architectural feature
Details	<ul style="list-style-type: none"> Front entry doors without a porch, recessed from front facade Limited ornamentation and no shutters Decorative metal elements or ceramic tile Cementitious or stucco trim (with finish matching body of house) may be permitted at front doors/porch entries Style appropriate light fixtures Horizontal railings at porches or balconies Built-up eave Color Blocking Front door and garage door may have modern styling to complement style
Colors	<ul style="list-style-type: none"> Field: Toned whites and light to medium light value warm colors Trim: Medium dark value browns reminiscent of stained wood Accents: Light or dark colors in contrast or harmony with field colors

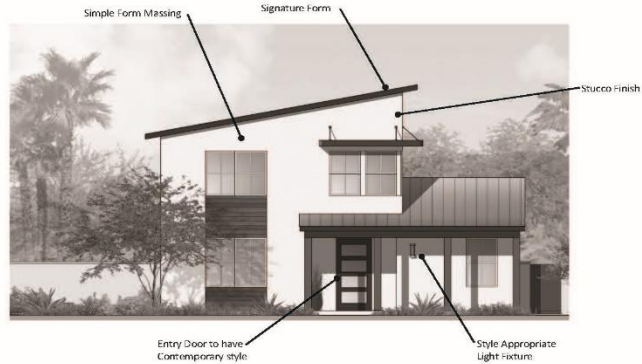
Exhibit G – Millennium Collection Architectural Styles Continued



Parkside

Contemporary

Clean lines and ordered appearance. An architectural expression that is forward facing and suggests a separation from the old world.



Parkside



Parapet Roof



Horizontal Grouped Windows

Element	Menu of Common Features
Form	<ul style="list-style-type: none"> Simple rectangular form massing Signature form, detail or feature
Roofs	<ul style="list-style-type: none"> Parapet, hip or shed roof Simple unadorned parapet walls Roof pitch range: flat or 4:12 to 6:12 0'- 12" overhang at eaves Shed element at entry with metal seam roof Asphalt shingles
Walls	<ul style="list-style-type: none"> Stucco finish Cementitious siding
Windows	<ul style="list-style-type: none"> Vertically or horizontally proportioned or grouped windows
Details	<ul style="list-style-type: none"> Entry accents such as awnings with metal seam roof or metal eyebrows Cementitious or stucco trim (with finish matching body of house) may be permitted at front doors/porch entries Style appropriate light fixtures Horizontal railings at porches or balconies Front door and garage door may have modern styling to complement style and may include clear or frosted glass window details Infill panels
Colors	<ul style="list-style-type: none"> Field: Whites or light shades Accents: Light or dark shades in contrast with field color

Exhibit H – Community Gateway Monument Plan



Parkside



Exhibit 7-2: Conceptual Streetscape Master Plan



Exhibit I – Community Gateway Monument Plan Concept



Parkside

Community Gateway Monuments

The community's identity will be defined at the four corners of the Specific Plan area. These four icons or "landmarks" will announce the Parkside community as somewhere truly special. Refer to Exhibit 7-12: Community Gateway Monument Concept for illustration.

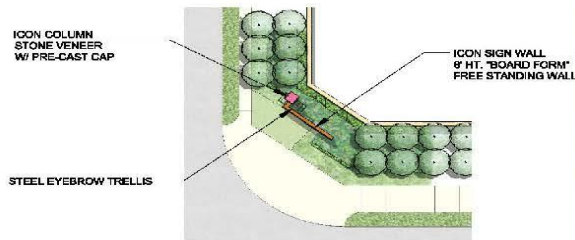
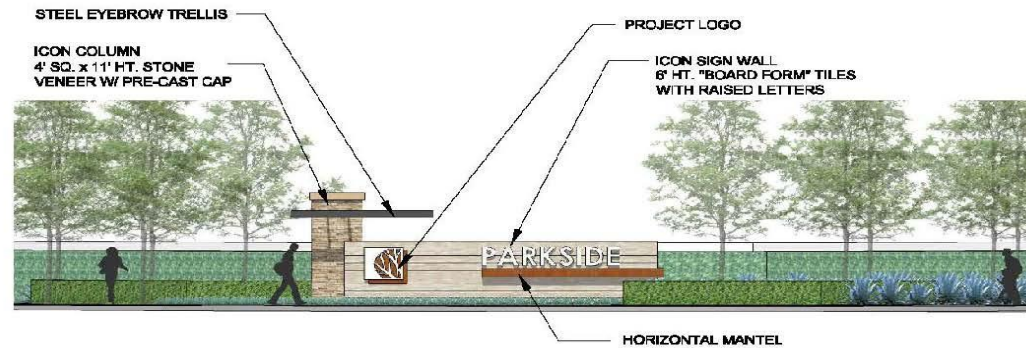
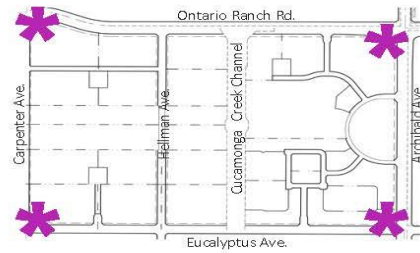


Exhibit 7-12: Community Gateway Monument Concept

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING AN ADDENDUM TO THE PARKSIDE SPECIFIC PLAN ENVIRONMENTAL IMPACT REPORT (SCH#2004011008), PURSUANT TO THE REQUIREMENTS OF THE CALIFORNIA ENVIRONMENTAL QUALITY ACT, AS AMENDED, FOR FILE NO. PSPA19-007, AND MAKING FINDINGS IN SUPPORT THEREOF — APNS: 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, AND 0218-221-10.

WHEREAS, SC Ontario Development Company, LLC, (hereinafter referred to as "Applicant") has filed an Application for the approval of a an Amendment to the Parkside Specific Plan, File No. PSPA19-007, which consists of: [1] Reconfigure residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 21; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7). The project site is bounded by Ontario Ranch Road to the north, Eucalyptus Avenue to south, Archibald Avenue to the east and Carpenter Avenue to the west, in the City of Ontario, California (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Parkside Specific Plan Environmental Impact Report (State Clearinghouse No. 2004011008) was certified on September 5, 2006 (hereinafter referred to as "Certified EIR"), in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario has prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as "EIR Addendum") in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as "CEQA"); and

WHEREAS, the EIR Addendum concluded that implementation of the Project could result in a number of significant effects on the environment that were previously analyzed in the Certified EIR, and that the Certified EIR identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, pursuant to State CEQA Guidelines Section 15164(a), a lead agency shall prepare an addendum to a previously certified EIR if some changes or additions are

necessary to a project, but the preparation of a subsequent or supplemental EIR is not required; and

WHEREAS, the City determined that none of the conditions requiring preparation of a subsequent or supplemental EIR would occur from the Project, and that preparation of an Addendum to the Certified EIR was appropriate; and

WHEREAS, the City of Ontario is the lead agency on the Project, and the City Council is the approving authority for the requested approval to construct and otherwise undertake the Project; and

WHEREAS, the City Council has reviewed and considered the Initial Study/Environmental Impact Report Addendum for the Project, has concluded that none of the conditions requiring preparation of a subsequent or supplemental EIR have occurred, and intends to take actions on the Project in compliance with CEQA and state and local guidelines implementing CEQA; and

WHEREAS, the EIR Addendum for the Project are on file in the Planning Department, located at 303 East B Street, Ontario, CA 91764, are available for inspection by any interested person at that location and are, by this reference, incorporated into this Resolution as if fully set forth herein; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the City Council of the City of Ontario, as follows:

SECTION 1. *Environmental Determination and Findings.* As the decision-making body for the Project, The City Council has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the City Council, the City Council finds as follows:

(1) The environmental impacts of this project were reviewed in conjunction with an Addendum to Parkside Specific Plan Environmental Impact Report (State Clearinghouse No. 2004011008), certified by the Ontario City Council on September 5, 2006, in conjunction with File No. PSP03-002; and

(2) The EIR Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts; and

(4) All previously adopted mitigation measures shall be a condition of project approval, as they are applicable to the Project, and are incorporated herein by this reference; and

(5) The EIR Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the City Council; and

(6) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts.

SECTION 2. Additional Environmental Review Not Required. Based on the Addendum, all related information presented to the City Council, and the specific findings set forth in Section 1, above, the City Council finds that the preparation of a subsequent or supplemental Environmental Impact Report is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

(d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3. City Council Action. Based upon the findings and conclusions set forth in Sections 1 and 2, above, the City Council hereby finds that based upon the entire record of proceedings before it, and all information received, that there is no

substantial evidence that the Project will constitute substantial changes to the Certified EIR, and does hereby approve the EIR Addendum and incorporated herein by this reference.

SECTION 4. *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 5. *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 6. *Certification to Adoption.* The City Clerk shall certify to the adoption of the Resolution.

PASSED, APPROVED, AND ADOPTED this 16th day of June 2020.

PAUL S. LEON, MAYOR

ATTEST:

SHEILA MAUTZ, CITY CLERK

APPROVED AS TO FORM:

COLE HUBER LLP
CITY ATTORNEY

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, SHEILA MAUTZ, City Clerk of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. 2020-_____ was duly passed and adopted by the City Council of the City of Ontario at their regular meeting held June 16, 2020, by the following roll call vote, to wit:

AYES: COUNCIL MEMBERS:

NOES: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

SHEILA MAUTZ, CITY CLERK

(SEAL)

The foregoing is the original of Resolution No. 2020-_____ duly passed and adopted by the Ontario City Council at their regular meeting held June 16, 2020.

SHEILA MAUTZ, CITY CLERK

(SEAL)

ATTACHMENT A:

**Addendum to Parkside Specific Plan
Environmental Impact Report**

(Addendum to follow this page)



City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

California Environmental Quality Act Addendum to The Ontario Plan Environmental Impact Report

Project Title/File No.: PSPA19-007

Lead Agency: City of Ontario, 303 East "B" Street, Ontario, California 91764, (909) 395-2036

Contact Person: Rudy Zeledon, Assistant Planning Director - 909-395-2422

Project Sponsor: City of Ontario, 303 East B Street, Ontario, CA 91764

Project Location: The project site is located in southwestern San Bernardino County, within the City of Ontario. The City of Ontario is located approximately 40 miles from downtown Los Angeles, 20 miles from downtown San Bernardino, and 30 miles from Orange County. As illustrated on Figures 1 through 3, below, the project site is located on Assessor Parcel Numbers (APN's): 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, and 0218-221-10) which is comprised of 250 gross acres bounded by Ontario Ranch Road to the north, Eucalyptus Avenue to south, Archibald Avenue to the east and Carpenter Avenue to the west.

Figure 1: REGIONAL LOCATION MAP



Figure 2: VICINITY MAP

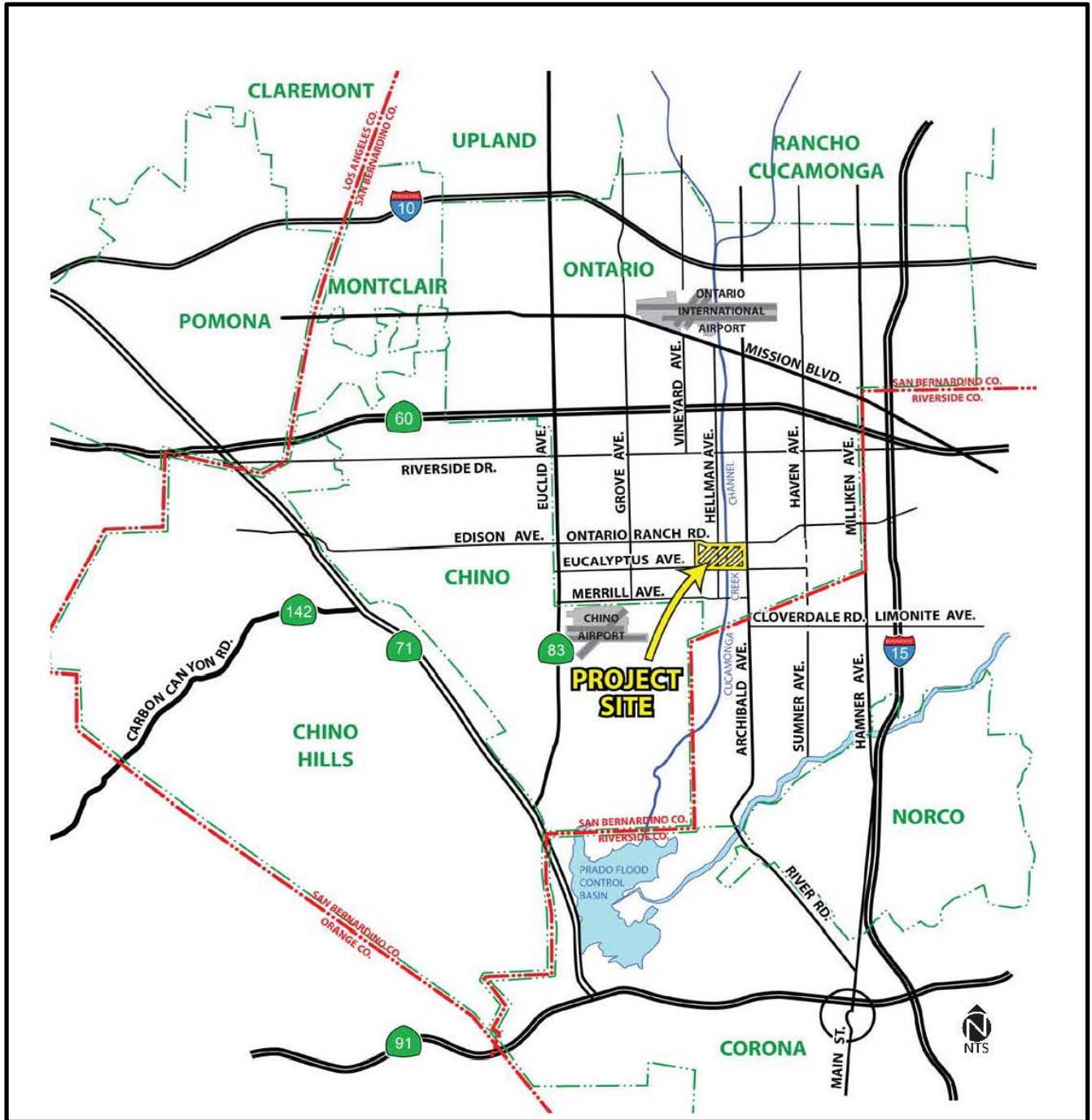


Figure 3: AERIAL PHOTOGRAPH



General Plan Designation: Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space Parkland.

Zoning: Specific Plan – Parkside Specific Plan

Description of Project: An Amendment to the Parkside Specific Plan (File No. PSPA19-007) to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7).

Project Setting: The project site, the Parkside Specific Plan area, is comprised of approximately 250.89 gross acres. SC Ontario owns approximately 237.89 gross acres of the project site. San Bernardino County Flood Control District owns the right-of-way for the Cucamonga Creek Channel that comprises approximately 13.00 gross acres, and effectively divides the eastern half of the project site from the western half, crossing the site from north to south approximately 1,650 feet west of Archibald Ave. The project site is bounded by Ontario Ranch Road to the north, Eucalyptus Avenue to south, Archibald Avenue to the east and Carpenter Avenue to the west. The Parkside Specific Plan area is generally undeveloped and general agricultural operations are scattered throughout the area, including an irrigation pond located within the northwestern portion of the project site. The approximate 250.89-gross acre project site has historically been used for general crop-related agricultural purposes (alfalfa, corn, milo, sorghum, etc.) and flood control.

Background: On January 27, 2010, the Ontario City Council adopted The Ontario Plan (“TOP”). TOP serves as the framework for the City’s business plan and provides a foundation for the City to operate as a municipal corporation that consists of six (6) distinct components: 1) Vision; 2) Governance Manual; 3) Policy Plan; 4) Council Priorities; 5) Implementation; and 6) Tracking and Feedback. The Policy Plan component of TOP meets the function. On January 27, 2010, the Ontario City Council adopted The Ontario Plan (TOP). TOP serves as the framework for the City’s business plan and provides a foundation for the City to operate as a municipal corporation that consists of six (6) distinct components: 1) Vision; 2) Governance Manual; 3) Policy Plan; 4) Council Priorities; 5) Implementation; and 6) Tracking and Feedback. The Policy Plan component of TOP meets the functional and legal mandate of a General Plan and contains nine elements: Land Use, Housing, Parks and Recreation, Environmental Resources, Community Economics, Safety, Mobility, Community Design and Social Resources.

An Environmental Impact Report (EIR) was prepared for TOP (SCH # 2008101140) and certified by the City Council on January 27, 2010 that included Mitigation Findings and a Statement of Overriding Considerations pursuant to CEQA. TOP EIR analyzed the direct and physical changes in the environment that would be caused by TOP; focusing on changes to land use associated with the buildout of the proposed land use plan, in the Policy Plan and impacts resultant of population and employment growth in the City. The significant unavoidable adverse impacts that were identified in the EIR included agriculture resources, air quality, cultural resources, greenhouse gas emissions, noise and transportation/traffic.

Prior to adoption of The Ontario Plan (TOP), an Environmental Impact Report (EIR) was prepared for the Parkside Specific Plan and certified (SCH# 2004011008) by the City Council on September 5, 2006, with a Statement of Overriding Considerations. The EIR analysis identified the potential environmental impacts associated with the Specific Plan. The significant unavoidable adverse impacts that were identified in the EIR included air quality, agriculture, traffic, water quality, solid waste and noise. In conjunction with the EIR, the City Council also approved the Parkside Specific Plan (File No. PSP03-002). The Specific Plan established the land use designations, development standards, and design guidelines for the existing Parkside Specific Plan project area boundary that presently encompasses approximately 250 acres of land for residential and commercial uses. The project was approved for a total of 1,947 single-family and multi-family residential units and 115,000 square feet of commercial. The proposed Amendment to the Parkside Specific Plan will not increase the total number of residential units (1,947) or commercial square (115,000 SF) feet and allow for additional single-family detached product types within the proposed Planning Areas 1 through 4.

Analysis: According to the California Environmental Quality Act Guidelines Section 15164, an Addendum to a previously Certified Parkside Specific Plan EIR may be used if some changes or additions are necessary, but none of the conditions described in Section 15162 requiring the preparation of a subsequent Negative Declaration or EIR have occurred. The CEQA Guidelines require that a brief explanation be provided to support the findings that no subsequent EIR or Negative Declaration are needed for further discretionary approval. These findings are described below:

1) *Required Finding: Substantial changes are not proposed for the project that will require major revisions of the previous EIR due to the involvement of new, significant environmental effects or a substantial increase in the severity of previously identified effects.*

Substantial changes are not proposed by the project and its implementation will not require revisions to the Certified Parkside EIR. The Parkside Specific Plan Certified EIR analyzed the direct and physical changes in the environment that would be caused by the development of 250.89-acre site; focusing on changes to land use associated with the buildout of the proposed land use plan of the Specific Plan. The Amendment to the Parkside Specific Plan (File No. PSPA19-007) proposes to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7). The project was approved for a total of 1,947 single-family and multi-family residential units and 115,000 square feet of commercial. The proposed Amendment to the Parkside Specific Plan will not increase the total number of residential units (1,947) or commercial square feet and allow for additional single-family detached units within the proposed Planning Areas 1 through 4; therefore, the proposed amendment will result in the same or less development than Parkside Specific EIR analyzed at buildout.

In addition, all previously adopted mitigation measures of the Certified Parkside Specific Plan EIR are conditions of project approval or mitigation measures and are incorporated herein by reference. The attached Initial Study provides an analysis of the Project and verification that the Project will not cause environmental impacts such that any of the circumstances identified in State CEQA Guidelines Section 15162 are present.

2) *Required Finding: Substantial changes have not occurred with respect to the circumstances under which the project is undertaken, that would require major revisions of the previous Environmental Impact Report due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects.*

Substantial changes have not occurred with respect to the circumstances under which the project was undertaken, that would not require major revisions to the Certified Parkside Specific Plan EIR in that the proposed changes would be in keeping with the surrounding area. The Certified Parkside Specific Plan EIR evaluated the site as residential, open space parkland, and commercial with a maximum number of 1,947 units 115,000 square feet of commercial. The proposed Amendment to the Parkside Specific Plan will not increase the total number of residential units (1,947) or commercial uses (115,000 SF) and allow for additional single-family detached units within the proposed Planning Areas 1 through 4. The proposed Specific Plan Amendment is implementing the Certified EIR's residential, commercial and parkland land use designations for the site and is consistent with The Ontario Plan (TOP) land use designation of Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space Parkland; therefore, no proposed changes or revisions to the Certified Parkside Specific Plan EIR are required. In addition, all previously adopted mitigation measures of the Certified Parkside Specific Plan EIR are incorporated herein by reference. The attached Initial Study provides an analysis of the Project and verification that the Project will not cause environmental impacts such that any of the circumstances identified in State CEQA Guidelines Section 15162 are present.

3) *Required Finding: No new information has been provided that would indicate that the proposed project would result in one or more significant effects not discussed in the previous EIR.*

No new information has been provided that would indicate the proposed project would result in any new significant effects not previously discussed in the Certified Parkside Specific Plan EIR. As stated above in Section 2, no substantial changes have occurred with respect to the circumstances under which the Project was undertaken. Parkside Specific Plan evaluated the site as Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space Parkland (see attached Exhibit "A"- Parkside Specific Plan Figure 1-3, Proposed Land Use Plan). In addition, the proposed Specific Plan Amendment is implementing the Certified EIR's residential, commercial and open space parkland land use designations for the site and is consistent with The Ontario Plan (TOP) land use designation of Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space-Parkland (see attached Exhibit "B"- TOP Policy Plan (Exhibit LU-01) Land Use Plan Map); therefore, no proposed changes or revisions to the Certified Parkside Specific Plan EIR are required. In addition, all previously adopted mitigation measures are incorporated herein by reference. The attached Initial Study provides an analysis of the Project and verification that the Project will not cause environmental impacts such that any of the circumstances identified in State CEQA Guidelines Section 15162 are present.

CEQA Requirements for an Addendum: If changes to a project or its circumstances occur or new information becomes available after adoption of a negative declaration, the lead agency may: (1) prepare a subsequent EIR if the criteria of State CEQA Guidelines Section 15162(a) are met, (2) prepare a subsequent negative declaration, (3) prepare an addendum, or (4) prepare no further documentation. (State CEQA Guidelines Section 15162(b)). When only minor technical changes or additions to the negative declaration are necessary and none of the conditions described in section 15162 calling for the preparation of a subsequent EIR or negative declaration have occurred, CEQA allows the lead agency to prepare and adopt an addendum. (State CEQA Guidelines Section 15164(b).)

Under Section 15162, a subsequent EIR or negative declaration is required only when:

- 1) Substantial changes are proposed in the project which will require major revisions of the previous negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;
- 2) Substantial changes occur with respect to the circumstances under which the project is undertaken which will require major revisions of the negative declaration due to the involvement of any new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or
- 3) New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the negative declaration was adopted, shows any of the following:
 - a) The project will have one or more significant effects not discussed in the previous negative declaration;
 - b) Significant effects previously examined will be substantially more severe than shown in the previous EIR;
 - c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or
 - d) Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative.

Thus, if the Project does not result in any of the circumstances listed in Section 15162 (i.e., no new or substantially greater significant impacts), the City may properly adopt an addendum to the Certified EIR.

Conclusion: The Parkside Specific Plan EIR, certified by City Council on September 5, 2006, was prepared

as a Program EIR in accordance with CEQA, the State CEQA Guidelines, and the City's Rules for the Implementation of CEQA and in accordance with Section 15121(a) of the State CEQA Guidelines (California Code of Regulations, Title 14, Division 6, Chapter 3). The Parkside Specific Plan EIR, considered the direct physical changes and reasonably foreseeable indirect physical changes in the environment that would be caused by Specific Plan. Consequently, Parkside Specific Plan EIR focused on impacts from changes to land use associated with buildout of the Specific Plans Land Use Plan, with impacts resulting from population and employment growth in the City. The proposed Specific Plan Amendment coordinates with the existing uses of the properties and uses within the surrounding areas. As described on page 4 above, the proposed Amendment to the Parkside Specific Plan will not increase the total number of residential units (1,947) or commercial square (115,000 SF) feet of the approved Specific Plan and allow for additional single-family detached units within the proposed Planning Areas 1 through 4. Subsequent activities within TOP Program EIR have been evaluated to determine whether an additional CEQA document needs to be prepared.

Accordingly, and based on the findings and information contained in the Certified Parkside Specific Plan EIR, the analysis above, the attached Initial Study, and CEQA statute and State CEQA Guidelines, including Sections 15164 and 15162, the Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary, nor is there a need for any additional mitigation measures; therefore, pursuant to State CEQA Guidelines Section 15164, the Ontario City Council hereby adopts this Addendum to the Certified EIR.

Surrounding Land Uses:

	Existing Land Use	General Plan Designation	Zoning Designation	Specific Plan Land Use
Site:	Vacant	Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space-Parkland	Parkside Specific Plan	Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space-Parkland
North:	Agriculture\Diary Uses	Low Density Residential (2.1 – 5 du/ac)	The Avenue Specific Plan	Low Density Residential
South:	Industrial Development/ Vacant Land	Business Park (0.6 FAR) Low Density Residential (2. Commercial (0.40 FAR) 1 – 5 du/ac)	West Ontario Commerce Center Specific Plan Subarea 29 Specific Plan	Business Park Low Density Residential
East:	Vacant/Agriculture\Diary Uses	Medium Density Residential (11.1 – 25 du/ac) and Open Space-Parkland	Grand Park Specific Plan	High Density Residential Great Park
West:	Vacant/ Agriculture\Diary Uses	Mixed-Use (NMC West)	Agriculture Overlay/Specific Plan	N/A

Other public agencies whose approval is required (e.g., permits, financing approval or participation agreement): None

Tribal Consultation: Have California Native American tribes traditionally and culturally affiliated with the project area requested consultation pursuant to Public Resources Code section 21080.3.1? Yes No

If "yes," has consultation begun? Yes No Completed

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

- | | | |
|--|---|---|
| <input type="checkbox"/> Aesthetics | <input type="checkbox"/> Agriculture/Forestry Resources | <input type="checkbox"/> Air Quality |
| <input type="checkbox"/> Biological Resources | <input type="checkbox"/> Cultural Resources | <input type="checkbox"/> Geology / Soils |
| <input type="checkbox"/> Greenhouse Gas Emissions | <input type="checkbox"/> Hazards & Hazardous Materials | <input type="checkbox"/> Hydrology / Water Quality |
| <input type="checkbox"/> Land Use / Planning | <input type="checkbox"/> Mineral Resources | <input type="checkbox"/> Noise |
| <input type="checkbox"/> Population / Housing | <input type="checkbox"/> Public Services | <input type="checkbox"/> Recreation |
| <input type="checkbox"/> Transportation | <input type="checkbox"/> Utilities / Service Systems | <input type="checkbox"/> Mandatory Findings of Significance |
| <input type="checkbox"/> Tribal Cultural Resources | <input type="checkbox"/> Wildfire | <input type="checkbox"/> Energy |

DETERMINATION (To be completed by the Lead Agency)

On the basis of this initial evaluation:

- I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
- I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
- I find that the proposed project MAY have a "potentially significant" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.

 Signature

 Rudy Zeledon, Assistant Planning Director
 Printed Name and Title

 Date

 City of Ontario
 For

EVALUATION OF ENVIRONMENTAL IMPACTS

1) A brief explanation is required for all answers except "No Impact" answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g. the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general

standards (e.g. the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).

2) All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.

3) Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. "Potentially Significant Impact" is appropriate if there is substantial evidence that an effect is significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.

4) "Negative Declaration: Less Than Significant with Mitigation Incorporated" applies where the incorporation of mitigation measures has reduced an effect from "Potentially Significant Impact" to a "Less than Significant Impact." The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from the "Earlier Analyses" Section may be cross-referenced).

5) Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or Negative Declaration. Section 15063(c)(3)(D). In this case, a brief discussion should identify the following:

a) Earlier Analyses Used. Identify and state where they are available for review.

b) Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.

c) Mitigation Measures. For effects that are "Less than Significant with Mitigation Measures Incorporated," describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.

6) Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g. general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.

7) Supporting Information Sources. A source list should be attached, and other sources used, or individuals contacted should be cited in the discussion.

8) This is only a suggested form, and lead agencies are free to use different formats; however, lead agencies should normally address the questions from this checklist that are relevant to a project's environmental effects in whatever format is selected.

9) The explanation of each issue should identify:

a) The significance criteria or threshold, if any, used to evaluate each question; and

b) The mitigation measure identified, if any, to reduce the impact to less than significance.

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in the Parkside SP EIR & TOP EIR
1. AESTHETICS. Would the project:				
a. Have a substantial adverse effect on a scenic vista?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in the Parkside SP EIR & TOP EIR
<p>b. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>c. In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>d. Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>2. AGRICULTURE AND FOREST RESOURCES. In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest protocols adopted by the California Air Resources Board. Would the project:</p>				
<p>a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>b. Conflict with existing zoning for agricultural use, or a Williamson Act contract?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>c. Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>d. Result in the loss of forest land or conversion of forest land to non-forest use?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>e. Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in the Parkside SP EIR & TOP EIR
3. AIR QUALITY. Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:				
a. Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in other emissions (such as those leading to odors adversely affecting a substantial number of people?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4. BIOLOGICAL RESOURCES. Would the project:				
a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
5. CULTURAL RESOURCES. Would the project:				

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in the Parkside SP EIR & TOP EIR
a. Cause a substantial adverse change in the significance of a historical resource pursuant to Section 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Disturb any human remains, including those interred outside of dedicated cemeteries?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
6. ENERGY. Would the project:				
a. Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
7. GEOLOGY AND SOILS. Would the project:				
a. Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury or death involving:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii. Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii. Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv. Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Be located on expansive soil, as defined in Table 18 1 B of the Uniform Building Code, creating substantial direct or indirect risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<i>Issues</i>	<i>Potentially Significant Impact</i>	<i>Less Than Significant with Mitigation</i>	<i>Less Than Significant Impact</i>	<i>Impacts Previously Analyzed in the Parkside SP EIR & TOP EIR</i>
f. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
8. GREENHOUSE GAS EMISSIONS. Would the project:				
a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emission of greenhouse gases?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
9. HAZARDS AND HAZARDOUS MATERIALS. Would the project:				
a. Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard or excessive noise for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g. Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
10. HYDROLOGY AND WATER QUALITY. Would the project:				
a. Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in the Parkside SP EIR & TOP EIR
b. Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i. result in substantial erosion or siltation on- or off-site;	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii. substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or offsite;	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii. create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff; or	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv. impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
11. LAND USE AND PLANNING. Would the project:				
a. Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
12. MINERAL RESOURCES. Would the project:				
a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
13. NOISE. Would the project result in:				
a. Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<i>Issues</i>	<i>Potentially Significant Impact</i>	<i>Less Than Significant with Mitigation</i>	<i>Less Than Significant Impact</i>	<i>Impacts Previously Analyzed in the Parkside SP EIR & TOP EIR</i>
<p>c. For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
14. POPULATION AND HOUSING. Would the project:				
<p>a. Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of road or other infrastructure)?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>b. Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
15. PUBLIC SERVICES. Would the project:				
<p>a. Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:</p>				
<p>i. Fire protection?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>ii. Police protection?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>iii. Schools?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>iv. Parks?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>v. Other public facilities?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
16. RECREATION. Would the project:				
<p>a. Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>b. Does the project include recreational facilities or require the construction or expansion of recreational facilities which have an adverse physical effect on the environment?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
17. TRANSPORTATION. Would the project:				
<p>a. Conflict with a program plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>b. Would the project conflict or be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)?</p>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in the Parkside SP EIR & TOP EIR
c. Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
18. TRIBAL CULTURAL RESOURCES. Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is				
a. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
19. UTILITIES AND SERVICE SYSTEMS. Would the project:				
a. Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Generate solid waste in excess of State or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in the Parkside SP EIR & TOP EIR
20. WILDFIRES. If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:				
a. Substantially impair an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
21. MANDATORY FINDINGS OF SIGNIFICANCE.				
a. Does the project have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current project, and the effects of probable future projects.)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p><u>Note:</u> Authority cited: Public Resources Code sections 21083, 21083.05, 21083.09.</p> <p><u>Reference:</u> Gov. Code section 65088.4; Public Resources Code sections 21073, 21074, 21080(c), 21080.1, 21080.3, 21080.3.1, 21080.3.2, 21082.3, 21083, 21083.3, 21083.5, 21084.2, 21084.3, 21093, 21094, 21095 and 21151; <i>Sundstrom v. County of Mendocino</i> (1988) 202 Cal.App.3d 296; <i>Leonoff v. Monterey Board of Supervisors</i> (1990) 222 Cal.App.3d 1337; <i>Eureka Citizens for Responsible Govt. v. City of Eureka</i> (2007) 147 Cal.App.4th 357; <i>Protect the Historic Amador Waterways v. Amador Water Agency</i> (2004) 116 Cal.App.4th 1099, 1109; <i>San Franciscans Upholding the Downtown Plan v. City and County of San Francisco</i> (2002) 102 Cal.App.4th 656.</p>				

EXPLANATION OF ISSUES

1. AESTHETICS. Would the project:

a. Have a substantial adverse effect on a scenic vista?

Discussion of Effects: The Policy Plan (General Plan) does not identify scenic vistas within the City. However, TOP Policy Plan (Policy CD1-5) requires all major north-south streets be designed and redeveloped to feature views of the San Gabriel Mountains. The project site is located at the northwest corner of Archibald Avenue and Eucalyptus Avenue. Archibald Avenue is identified as Principal Arterial and Eucalyptus Avenue as a Collector Street in the Functional Roadway Classification Plan (Figure M-2) of the Mobility Element within the Policy Plan. The proposed Specific Plan Amendment will not result in adverse environmental impacts with regard to views of the San Gabriel Mountains. Therefore, no adverse impacts are anticipated in relation to the project.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR or TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings and historic buildings within a state scenic highway?

Discussion of Effects: The City of Ontario is served by three freeways: I-10, I-15, and SR-60. I-10 and SR-60 traverse the northern and central portion of the City, respectively, in an east–west direction. I-15 traverses the northeastern portion of the City in a north–south direction. These segments of I-10, I-15, and SR-60 have not been officially designated as scenic highways by the California Department of Transportation. In addition, there are no historic buildings, or any scenic resources identified on or in the vicinity of the project site. Therefore, it will not result in adverse environmental impacts.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

b. In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?

Discussion of Effects: The Project would not degrade the existing visual character or quality of the site or its surroundings. The project site is in an area that is characterized by agriculture uses and residential development and is surrounded by urban land uses.

Any development proposals that would subsequently occur from the proposed Project will be required to be in accordance with the policies of the Community Design Element of the Policy Plan (General Plan) and development regulations and design guidelines of the Parkside Specific Plan. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

c. Create a new source of substantial light or glare that would adversely affect day or nighttime views in the area?

Discussion of Effects: The proposed land use change itself will not cause lighting to be installed in the Project. New lighting will be introduced to the site with the development of the project. Pursuant to the requirements of the City's Development Code, on-site lighting will be shielded, diffused or indirect, to avoid glare to pedestrians or motorists. In addition, lighting fixtures will be selected and located to confine the area of illumination to within the project site and minimize light spillage.

Site lighting plans will be subject to review by the Planning Department and Police Department prior to issuance of building permits (pursuant to the City's Building Security Ordinance). Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

2. AGRICULTURE AND FOREST RESOURCES. In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board. Would the project:

a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?

Discussion of Effects: As discussed in the Certified Parkside Specific Plan EIR (2006), a considerable portion of the project site has been used for agricultural/dairy farming. The project will convert this land, which is considered to be Prime Farmland pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to nonagricultural use. The conversion of farmland to urban uses was determined to be a potentially significant impact that is unavoidable. The changes to the Project do not change this conclusion and there is no additional mitigation presently available that could potentially reduce this impact. The impact will remain as a significant unavoidable impact.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

b. Conflict with existing zoning for agricultural use, or a Williamson Act contract?

Discussion of Effects: The 250-acre Parkside Specific Plan is currently used for irrigated crop production to be converted to urban use. The Parkside Specific Plan EIR determined that the conversion of agricultural uses to urban uses was a potentially significant impact that is unavoidable. Furthermore, there are no Williamson Act contract in effect within the 250-acre Parkside Specific Plan area. Therefore, no impacts to agricultural uses are anticipated, nor will there be any conflict with existing or Williamson Act contracts. The proposed Amendment to the Parkside Specific Plan do not change this conclusion and there is no additional mitigation presently available that could potentially reduce this impact. The impact will remain as a significant unavoidable impact.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

c. Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?

Discussion of Effects: The project proposes an Amendment to the Parkside Specific Plan to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7). This would not result in the rezoning of forest land, timberland, or timberland zoned Timberland Production because such land use designations do not exist within the City of Ontario. Therefore, no impacts to forest or timberland are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

d. Result in the loss of forest land or conversion of forest land to non-forest use?

Discussion of Effects: There is currently no land in the City of Ontario that qualifies as forest land as defined in Public Resources Code section 12220(g). Neither TOP nor the City's Zoning Code provide designations for forest land. Consequently, the proposed project would not result in the loss or conversion of forest land.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

e. Involve other changes in the existing environment, which, due to their location or nature, could individually or cumulatively result in loss of Farmland to non-agricultural use or conversion of forest land to non-forest use?

Discussion of Effects: The 250-acre Parkside Specific Plan area is currently used for irrigated crop production to be converted to urban use. The project will convert this land, which is considered to be Prime Farmland pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to nonagricultural use. The conversion of farmland to urban uses was determined to be a potentially significant impact that is unavoidable. The changes to the Project do not change this conclusion and there is no additional mitigation presently available that could potentially reduce this impact. The impact will remain as a significant unavoidable impact.

Additionally, there is currently no land in the City of Ontario that qualifies as forest land as defined in Public Resources Code Section 12220(g). Neither TOP nor the City's Zoning Code provide designations for forest land. Consequently, to the extent that the proposed project would result in changes to the existing environment, those changes would not impact forest land.

Mitigation Required: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

3. AIR QUALITY. Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:

a. Conflict with or obstruct implementation of the applicable air quality plan?

Discussion of Effects The project will not conflict with or obstruct implementation of any air quality plan. As noted in The Ontario Plan FEIR (Section 5.3), pollutant levels in the Ontario area already exceed Federal and State standards. To reduce pollutant levels, the City of Ontario is actively participating in efforts to enhance air quality by implementing Control Measures in the Air Quality Management Plan for local jurisdictions within the South Coast Air Basin.

The proposed Specific Plan Amendment is implementing the Certified Parkside Specific Plan EIR's residential, commercial and parkland land use designations for the site and which is consistent with The Ontario Plan (TOP) land use designation of Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space-Parkland. The project will not conflict with or obstruct implementation of any air quality plan.

The proposed project is consistent with The Ontario Plan, for which the EIR was prepared and impacts evaluated. Furthermore, the project is consistent with the City's participation in the Air Quality Management Plan and will not conflict with or obstruct implementation of the plan. Mitigation (Mitigation Measure 5.3-2) has been adopted by the City that requires fugitive dust control measures pursuant to SCAQMD's Rule 403, use of Tier 3 construction equipment, proper service and maintenance of construction equipment, limiting nonessential idling of construction equipment, and use of Super-Compliant VOC paints for coating and architectural surfaces. Any future development proposals on the project site will be required,

as a condition of approval, to comply with Mitigation Measure 5.3-2. No new impacts beyond those identified in the Certified Parkside Specific Plan EIR or TOP EIR that would result from Project implementation.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

b. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?

Discussion of Effects: The project will not result in a cumulatively considerable net increase of any criteria pollutant for which the region is in non-attainment under an applicable federal or state ambient air quality. The proposed Amendment to the Parkside Specific Plan will not increase the total number of residential units (1,947) or commercial uses (115,000 SF) and will allow for additional single-family detached units within the proposed Planning Areas 1 through 4. The proposed Specific Plan Amendment is implementing the Certified EIR's residential, commercial and parkland land use designations for the site, which is consistent with The Ontario Plan (TOP) land use designation of Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space Parkland. Mitigation (Mitigation Measure 5.3-1) has already been adopted by the City that would facilitate continued City cooperation with the SCAQMD and SCAG to achieve regional air quality improvement goals, promote energy conservation design and development techniques, encourage alternative modes of transportation, and implement transportation demand strategies. The project will comply with the air quality standards of the Certified Parkside Specific Plan EIR, TOP EIR and the SCAQMD resulting in impacts that are less than significant [please refer to Sections 3(a) and 3(b)].

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

c. Expose sensitive receptors to substantial pollutant concentrations?

Discussion of Effects: The subject site was previously analyzed by TOP EIR as Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space Parkland is surrounded on the north, east, west and south by future residential and commercial uses. As discussed in Section 5.3 of TOP EIR, the proposed Project is within a non-attainment region of the South Coast Air Basin (SCAB). The proposed Specific Plan Amendment is implementing the Certified Parkside Specific Plan EIR's residential, commercial and parkland land use designations for the site and is consistent with The Ontario Plan (TOP) land use designation of Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space Parkland. Adequate mitigation (TOP EIR Mitigation Measure 5.3-5) has already been adopted by the City that would require new developments to be consistent with recommended buffer distances of the California Air Resources Board's Air Quality and Land Use Handbook: A Community Health Perspective (April 2005). Under this mitigation, new development that is inconsistent with the recommended buffer distances shall only be approved if all feasible mitigation measures, such as high efficiency Minimum Efficiency Reporting Value filters, have been incorporated into the project design to protect future sensitive receptors from harmful concentrations of air pollutants as a result of proximity to existing air pollution sources. No new impacts beyond those identified in the Certified Parkside Specific Plan EIR or TOP EIR would result from Project implementation.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

d. Result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?

Discussion of Effects: The subject site was previously analyzed by TOP EIR as Medium Density Residential (11.1 – 25 du/ac), Neighborhood Commercial (0.40 FAR) and Open Space Parkland is surrounded on the north, east, west and south by future residential and commercial uses. The uses proposed on the subject site, as well as those permitted within the residential and commercial district

Parkside Specific Plan, do not create objectionable odors. Further, the project shall comply with the policies of the Ontario Municipal Code and the Policy Plan (General Plan). Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

4. BIOLOGICAL RESOURCES. Would the project:

a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?

Discussion of Effects: The Ontario Plan FEIR (Section 5.4) and the Parkside Specific Plan EIR (2007) identified the potential habitat for the federally listed Raptor and Burrowing Owl. According to the Certified Parkside Specific Plan EIR, the project site is located within an area that has been identified as containing species identified as a candidate, sensitive, or special status species in local or regional plans, policies or regulations or by the California Department of Fish and Game or the U.S. Fish and Wildlife Service. These species include: Burrowing Owl, and various species of nesting birds protected under the Migratory Bird Treaty Act. However, the proposed Amendment to the Parkside Specific Plan would not have an impact on these species because no development project is proposed at this time that would have the ability to disturb the site. Any future development within the Specific Plan area would be required to conduct pre-construction surveys for the Burrowing Owls, and focused surveys for migratory bird nests. If the surveys identify any of these species on the Project site, the proper measures must be undertaken by the future project applicant to ensure that these impacts are reduced to less than significant. Therefore, because the proposed Project is an Amendment to the Parkside Specific Plan and no development of the site is proposed at this time, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?

Discussion of Effects: The subject site was previously analyzed by the Certified Parkside Specific Plan EIR as residential, open space parkland and commercial uses. The site does not contain any riparian habitat or other sensitive natural community identified by the Department of Fish & Game or Fish & Wildlife Service. Therefore, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

c. Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?

Discussion of Effects: No wetland habitat is present on site. Therefore, project implementation would have no impact on these resources.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?

Discussion of Effects: The site is a vacant and used for crop production and that is bounded on all four sides by fragmented residential development. As a result, there are no wildlife corridors connecting this site to other areas. Therefore, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?

Discussion of Effects: The City of Ontario does not have any specific policies or ordinances protecting biological resources. Further, the Parkside Specific Plan area does not contain any mature trees necessitating the need for preservation. As a result, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

f. Conflict with the provisions of an adopted Habitat Conservation Plan (HCP), Natural Community Conservation Plan (NCCP), or other approved local, regional, or state habitat conservation plan?

Discussion of Effects: The site is not part of an adopted HCP, NCCP or other approved habitat conservation plan. As a result, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

5. CULTURAL RESOURCES. Would the project:

a. Cause a substantial adverse change in the significance of a historical resource pursuant to Section 15064.5?

Discussion of Effects: The subject site was previously analyzed by Parkside Specific Plan EIR for residential, open space parkland, and commercial uses. The subject site is vacant and does not contain any buildings or structures constructed more than 50 years ago and cannot be considered for eligibility for listing in the California Register of Historic Resources. A Cultural Resources Records Search was conducted on July 16, 2019 at the California Historic Resource Inventory System at the South Central Coastal Information Center (CHRIS-SCCIC). The records search indicated that there are no cultural resources (prehistoric, historic, or built environments) recorded within the Project boundaries. There was one (1) historic resource (CA-SBR-008857H) located within a one-half mile radius of the Project Site. The historic resource is a section of the Southern California Edison Company's Lugo-Mira No. 1 500kv Transmission Line. The transmission line was determined eligible for listing in the National Register of Historic Place (NRHP) under Criteria A and C, and therefore, is eligible for listing in the California Register of Historic Resources. This historic resource will not be impacted (directly or indirectly) by the proposed Project. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

b. Cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5?

Discussion of Effects: The Certified Parkside Specific Plan EIR (Section 4 Culture Resources, page III-4-6 &7) indicates no archeological sites or resources have been recorded in the City with the Archeological Information Center at San Bernardino County Museum. The NAHC Sacred Lands File search also failed to indicate archaeological resources or artifacts associated with Tribal Cultural Resources (TCRs) within the Project site. The Project site has been highly disturbed by modern human activities to with agricultural production since the 1950's. Therefore, the proposed Project will not impact cultural (prehistoric, historic, or historic built environments) resources and no mitigation measures are recommended. While no adverse impacts to archeological resources are anticipated at this site due to its urbanized nature, standard conditions have been imposed on the project that in the event of unanticipated archeological discoveries, construction activities will not continue or will moved to other parts of the project site and a qualified archaeologist shall be contacted to determine significance of these resources. If the find is discovered to be historical or unique archaeological resources, as defined in Section 15064.5 of the CEQA Guidelines, avoidance or other appropriate measures shall be implemented.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

c. Disturb any human remains, including those interred outside of formal cemeteries?

Discussion of Effects: The proposed project is in an area that has been previously disturbed by human activity. No known religious or sacred sites exist within the Project area. Thus, human remains are not expected to be encountered during any construction activities. However, in the unlikely event that human remains are discovered, existing regulations, including the California Public Resources Code Section 5097.98, would afford protection for human remains discovered during development activities. Furthermore, standard conditions have been imposed on the project that in the event of unanticipated discoveries of human remains are identified during excavation, construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and/or Native American consultation has been completed, if deemed applicable.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

6. ENERGY Would the project:

a. Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?

Discussion of Effects: Energy was not analyzed in the Certified Parkside Specific Plan EIR but has been included as part of the 2019 revisions to the State CEQA Guidelines. Implementation of the Project would increase the demand for electricity and natural gas at the Project site and gasoline consumption in the region during construction and operation. A detailed discussion is provided below.

Electricity

Construction. Temporary electric power would be required for lighting and electronic equipment (e.g., computers) located in trailers used by the construction crew. However, the electricity used for such activities would be temporary and would have a negligible contribution to the project's overall energy consumption.

Operational. The project proposes an Amendment to the Parkside Specific Plan proposes to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7). The Project does not include construction of an actual development. However, during hypothetical operation of the Project, a residential use or commercial, use would require electricity for multiple purposes, such as: building heating and cooling, lighting, appliances, and electronics. Any future development on the site would be required to comply with the

CALGreen Building Code requirements in effect at the time of development, which are more efficient than the 2016 standards. Moreover, the Project includes a sample Greenhouse Gas Reduction Measure Screening Table for Residential and Commercial Development. The Screening Table includes measures energy efficient development, indoor space efficiency measures, building efficiency measures, renewable energy measures, and water conservation measures. Measures that would reduce electricity consumption include, but are not limited to: greatly enhanced window insulation, an enhanced cool-roof, an improved efficiency heating, ventilation, and air conditioning (“HVAC”) system, blower doors HERS verified Envelope leakage or equivalent, enhanced duct insulation, Energy Star commercial appliances, water efficient landscaping and irrigation systems, and water-efficient toilets and faucets. Although electricity consumption would increase at the site under implementation of the Project, the building envelope, HVAC, lighting, and other systems, would be designed to maximize energy performance. The project would also be subject to statewide mandatory energy requirements as outlined in the CALGreen Code. In addition, the project would implement additional measures, as detailed in the GHG reduction measures screening table, that would further reduce electricity consumption. Electricity that would be consumed by the Project would also be subject to the cap-and-trade regulation. For these reasons, the electricity that would be consumed by the Project is not considered to be inefficient or wasteful, and impacts would be less than significant.

Natural Gas

Construction. Natural gas consumption is not anticipated during construction of the Project. Fuels used for construction would generally consists of diesel and gasoline, which are discussed in the next subsection. Any amounts of natural gas that may be consumed during project construction would be nominal and would have a negligible contribution to the project's overall energy consumption.

Operational. The project proposes an Amendment to the Parkside Specific Plan proposes to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7). The Project does not include construction of an actual development. However, during hypothetical operation of the Project, a residential and commercial use would require natural gas consumption for various purposes, such as building heating and cooling. Any future structure developed on the site would be built to the 2016 Title 24 CALGreen efficiency requirements or the code in effect at the time of development. In addition, measures will be applied based on the information contained in the County's GHG DRP checklist. These measures include, but are not limited to: enhanced wall, attic, and window insulation; high efficiency water heater, and optimized building orientation. Although natural gas consumption would increase at the site under implementation of the Project, the building envelope, HVAC, lighting, and other systems, would be designed to maximize energy performance. The project would be subject to statewide mandatory energy requirements as outlined in the CALGreen Code. In addition, the project would implement additional measures, as detailed in the GHG reduction measures screening table, that would further reduce natural gas consumption. Natural gas that would be consumed by the Project would also be subject to the cap-and-trade regulation. For these reasons, the natural gas that would be consumed by the project is not considered to be inefficient or wasteful, and impacts would be less than significant.

Diesel and Gasoline Fuel

Construction. Diesel and gasoline fuels, also referred to as petroleum in this subsection, would be consumed throughout construction of the Project. Fuel consumed by construction equipment would be the primary energy resource consumed over the course of construction, and vehicle miles traveled (“VMT”) associated with the transportation of construction materials (e.g., deliveries to the site) and worker trips to and from the site would also result in petroleum consumption. Whereas on-site, heavy-duty construction equipment and delivery trucks would predominantly use diesel fuel, construction workers would generally rely on gasoline-powered vehicles. Any future development would be required to comply with CARB's Airborne Toxic Control Measures, which restricts heavy-duty diesel vehicle idling to five minutes. Since petroleum use during construction would be temporary and required to conduct development activities, it would not be wasteful or inefficient, and impacts would be less than significant.

Operational. Fuel consumption associated with development pursuant to the Project's operational phase would primarily be attributable to workers commuting to and from the Project and the operation of

large, diesel-powered trucks (e.g., semi-trucks) needed to transport goods. Over the lifetime of the Project, the fuel efficiency of the vehicles being used by the employees is expected to increase. As such, the amount of petroleum consumed as a result of vehicular trips to and from the Project site during operation is anticipated to decrease over time. There are numerous regulations in place that require and encourage fuel efficiency. For example, CARB has adopted an approach to passenger vehicles by combining the control of smog-causing pollutants and GHG emissions into a single, coordinated package of standards. The approach also includes efforts to support an accelerate the number of plug-in hybrids and ZEVs in California. In addition, per the requirements identified in SB 375, CARB adopted a regional goal for the SCAG region of reducing per-capita GHG emissions from 2005 levels by 8 percent by 2020 and 19 percent by 2035 for light-duty passenger vehicles. Accordingly, operation of the Project is expected to decrease the amount of petroleum it consumes in the future due to advances in fuel economy. Although the Project would increase petroleum use in the region during construction and operation, the use would be a small fraction of the statewide use and, due to efficiency increases, would diminish over time. As such, petroleum consumption associated with the Project would not be considered inefficient or wasteful and would result in a less-than-significant impact.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

b. Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?

Discussion of Effects: The Project would not conflict with or obstruct a state or local plan adopted for the purposes of increasing the amount of renewable energy or energy efficiency. The California Title 24 Building Code contains energy efficiency standards for residential and commercial buildings. These standards address electricity and natural gas efficiency in lighting, water, heating, and air conditioning, as well as the effects of the building envelope (e.g., windows, doors, walls and roofs, etc.) on energy consumption. As described above, the Project would be required to comply with the 2019 Title 24 CALGreen standards and would implement additional measures as identified in the County's GHG DRP checklist. Since the Project would comply with applicable State standards and adhere to the County's energy reductions measures identified in the GHG Emissions Reduction Plan, the Project would not conflict with nor obstruct a state or local plan for renewable energy or energy efficiency. This impact would be less than significant.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

7. GEOLOGY & SOILS. Would the project:

a. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury or death involving:

i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.

Discussion of Effects: There are no active faults known on the site and the project site is located outside the Fault Rapture Hazard Zone (formerly Alquist-Priolo Zone). The Certified TOP EIR (Section 5.7/Figure 5.7-2) identifies eight active or potentially active fault zones near the City. Given that the closest fault zone is located more than ten miles from the project site, fault rupture within the project area is not likely. All development will comply with the Uniform Building Code seismic design standards to reduce geologic hazard susceptibility. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

ii. Strong seismic ground shaking?

Discussion of Effects: There are no active faults known on the site and the project site is located outside the Fault Rapture Hazard Zone (formerly Alquist-Priolo Zone). The Certified TOP EIR (Section 5.7/Figure 5.7-2) identifies eight active or potentially active fault zones near the City. The closest fault zone is located more than ten miles from the project site. The proximity of the site to the active faults will result in ground shaking during moderate to severe seismic events. All construction will comply with the California Building Code, the Ontario Municipal Code, The Ontario Plan and all other ordinances adopted by the City related to construction and safety. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

iii. Seismic-related ground failure, including liquefaction?

Discussion of Effects: As identified in the Certified TOP EIR (Section 5.7), groundwater saturation of sediments is required for earthquake induced liquefaction. In general, groundwater depths shallower than 10 feet to the surface can cause the highest liquefaction susceptibility. Depth to ground water at the project site during the winter months is estimated to be between 250 to 450 feet below ground surface. Therefore, the liquefaction potential within the project area is minimal. Implementation of The Ontario Plan strategies, Uniform Building Code and Ontario Municipal code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

iv. Landslides?

Discussion of Effects: The project would not expose people or structures to potential adverse effects, including the risk of loss, injury, or death involving landslides because the relatively flat topography of the project site (less than 2 percent slope across the City) makes the chance of landslides remote. The allowed residential and commercial uses within the Parkside Specific Plan will not create greater landslide potential impacts than were identified in the Certified Parkside Specific Plan EIR or TOP EIR. Implementation of TOP EIR strategies, Uniform Building Code and Ontario Municipal Code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR or TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

b. Result in substantial soil erosion or the loss of topsoil?

Discussion of Effects: Amendment to the Parkside Specific Plan proposes to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles, will not create greater erosion impacts than were identified in the Certified Parkside Specific Plan EIR and TOP EIR. Impacts will be less than significant with mitigation.

The project will not result in significant soil erosion or loss of topsoil because of the previously disturbed nature of the Project site and the limited size and scope of the Project. Grading increases the potential for erosion by removing protective vegetation, changing natural drainage patterns, and constructing slopes. However, compliance with the California Building Code and review of grading plans by the City Engineer will ensure no significant impacts will occur. In addition, the City requires an erosion/dust control plan for projects located within this area. Implementation of a NPDES program, the Environmental Resource Element of the Policy Plan (General Plan) strategies, Uniform Building Code and Ontario Municipal code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?

Discussion of Effects: Amendment to the Parkside Specific Plan proposes to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles, will not create greater landslide potential impacts than were identified in the Certified Parkside Specific Plan EIR and TOP EIR. In addition, the associated projects would not result in the location of development on a geologic unit or soil that is unstable, or that would become unstable because as previously discussed, the potential for liquefaction and landslides associated with the project is less than significant. TOP EIR (Section 5.7) indicates that subsidence is generally associated with large decreases or withdrawals of water from the aquifer. The project would not withdraw water from the existing aquifer. Further, implementation of The Ontario Plan strategies, Uniform Building Code and Ontario Municipal code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

d. Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?

Discussion of Effects: The majority of Ontario, including the project site, is located on alluvial and eolian soil deposits. These types of soils are not considered to be expansive. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

e. Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?

Discussion of Effects: The area is served by the local sewer system and the use of alternative systems is not necessary. There will be no impact to the sewage system.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

f. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?

Discussion of Effects: The City of Ontario is underlain by deposits of Quaternary and Upper-Pleistocene sediments deposited during the Pliocene and early Pleistocene time, Quaternary Older Alluvial sediments may contain significant, nonrenewable, paleontological resources and are, therefore, considered to have high sensitivity at depths of 10 feet or more below ground surface. In addition, the Certified TOP EIR (Section 5.5) indicates that one paleontological resource has been discovered in the City. Moreover, Results of the paleontological resources records search through the Natural History Museum of Los Angeles County (NHMLAC) indicate that there are no known vertebrate fossil localities or unique geological features that have been previously identified within the Project area or within a one-mile radius. The results of the literature review and the search at the NHMLAC indicate that the Project site has surficial sediments

composed of younger Quaternary Alluvium, derived as alluvial fan deposits from the San Gabriel Mountains to the north or as dune sands. These deposits typically do not contain significant vertebrate fossils, at least in the uppermost layers, but they may be underlain by older sedimentary materials at estimated depths greater than 9 feet (McLeod 2019). Therefore, it is concluded that the proposed Project will not impact paleontological resources or unique geological features and as such no mitigation measures are recommended. While no adverse impacts are anticipated, standard conditions have been imposed on the Project that in the event of unanticipated paleontological resources are identified during excavation, construction activities will not continue or will be moved to other parts of the Project site and a qualified paleontologist shall be contacted to determine significance of these resources. If the find is determined to be significant, avoidance or other appropriate measures shall be implemented.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

8. GREENHOUSE GAS EMISSIONS. Would the project:

a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?

Discussion of Effects: The subject site was previously analyzed by the Certified TOP EIR as residential, open space parkland and commercial uses. Additionally, the impact of buildout of The Ontario Plan on the environment due to the emission of greenhouse gases ("GHGs") was analyzed in the Certified Parkside Specific Plan EIR. According to the EIR, this impact would be significant and unavoidable (Recirculated Portions of the Ontario Plan Draft Environmental Impact Report, p. 2-118.) This EIR was certified by the City on January 27, 2010, at which time a statement of overriding considerations was also adopted for The Ontario Plan's significant and unavoidable impacts, including that concerning the emission of greenhouse gases. Amending the Specific Plan Planning Areas will not create significantly greater impacts than were identified in the Certified TOP EIR or Parkside Specific Plan EIR. All development projects proposed within the Specific Plan will be required to complete a sample GHG Reduction Measures Screening Threshold Table, which provides guidance in measuring the reduction of greenhouse gas ("GHG") emissions attributable to certain design and construction measures incorporated into development projects. The analysis, methodology, and significance determination (thresholds) are based upon the City's Climate Action Plan ("CAP"), which includes GHG emission inventories (2008 and 2020 forecasts), a year 2020 emission reduction target, the goals and policies to reach the target, together with the Addendum prepared for the CAP. The Screening Table assigns points for each option incorporated into a project as mitigation or a project design feature (collectively referred to as "feature"). The point values correspond to the minimum emissions reduction expected from each feature. The menu of features allows maximum flexibility and options for how development projects can implement the GHG reduction measures. The point levels are based upon improvements compared to 2008 emission levels of efficiency. Projects that garner at least 100 points will be consistent with the reduction quantities anticipated in the City's CAP. As such, those projects that garner a total of 100 points or greater would not require quantification of project specific GHG emissions. Consistent with CEQA Guidelines, such projects would be determined to have a less than significant individual and cumulative impact for GHG emissions. As shown in the Project GHG Reduction Measures Screening Table, the Project garners a total of 103 points, and is therefore consistent with the reduction quantities anticipated in the City's CAP. Therefore, quantification of Project-specific GHG emissions is not required.

Additionally, pursuant to Public Resources Code Section 21083.3, this impact need not be analyzed further, because (1) the proposed project would result in an impact that was previously analyzed in the Certified TOP EIR, which was certified by the City; (2) the proposed project would not result in any greenhouse gas impacts that were not addressed in the Certified TOP EIR; (3) the proposed project is consistent with The Ontario Plan. The proposed impacts of the project were already analyzed in the Certified TOP EIR and the project will be built to current energy efficient standards. Potential impacts of project implementation will be less than significant with mitigation already required under the Certified TOP EIR and current energy efficiency standards. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

Mitigation Required: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to Parkside Specific Plan EIR and TOP EIR analyses are necessary. The mitigation measures adopted as part of TOP EIR adequately address any potential significant impacts and there is no need for any additional mitigation measures. The City has reviewed the emission reduction measures and concepts in The Ontario Plan EIR's MM 6-2 and 6-3, and has determined that the following actions apply and shall be undertaken by the applicant in connection with the project: energy efficient design, efficient irrigation systems, electric vehicle charging stations, and compliance with Title 24 of the California Code of Regulations.

b. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?

Discussion of Effects: The subject site was previously analyzed by the Certified TOP EIR for residential, parkland and commercial uses. The proposed Project is consistent with The Ontario Plan Goal ER 4 of improving air quality by, among other things, implementation of Policy ER4-3, regarding the reduction of greenhouse gas emissions in accordance with regional, State, and federal regulations. In addition, the proposed Project is consistent with the policies outlined in Section 5.6.4 of the Environmental Impact Report for The Ontario Plan, which aims to reduce the City's contribution of greenhouse gas emissions at build-out by fifteen (15 percent), because the project is upholding the applicable City's adopted mitigation measures as represented in 6-1 through 6-6 and energy efficient design, efficient irrigation systems, electric vehicle charging stations, and compliance with Title 24 of the California Code of Regulations. The Project is consistent with the City's Climate Action Plan. Therefore, the proposed Project does not conflict with an applicable plan, policy or regulation adopted for the purpose of reducing emissions of greenhouse gases.

Mitigation Required: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

9. HAZARDS & HAZARDOUS MATERIALS. Would the project:

a. Create a significant hazard to the public or the environment through the routine transport, use or disposal of hazardous materials?

Discussion of Effects: The subject site was previously analyzed by the Parkside Specific Plan EIR for residential, open space parkland and commercial uses. The project is not anticipated to involve the transport, use or disposal of hazardous materials during either construction or project implementation. Therefore, no adverse impacts are anticipated. However, in the unlikely event of an accident, implementation of the strategies included in The Ontario Plan will decrease the potential for health and safety risks from hazardous materials to a less than significant impact.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?

Discussion of Effects: The subject site was previously analyzed by the Certified Parkside Specific Plan EIR and TOP EIR for residential, open space parkland and commercial uses. The proposed project does not include the use of hazardous materials or volatile fuels. In addition, there are no known stationary commercial or industrial land uses within close proximity to the subject site, which use/store hazardous materials to the extent that they would pose a significant hazard to visitors/occupants to the subject site, in the event of an upset condition resulting in the release of a hazardous material.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances or waste within one-quarter mile of an existing or proposed school?

Discussion of Effects: The proposed project does not include the use, emissions or handling of hazardous or acutely hazardous materials, substances, or waste. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?

Discussion of Effects: The proposed project site is not listed on the hazardous materials site compiled pursuant to Government Code Section 65962.5. Therefore, the project would not create a hazard to the public or the environment and no impact is anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

e. For a project located within the safety zone of the airport land use compatibility plan for ONT or Chino Airports, would the project result in a safety hazard for people residing or working in the project area?

Discussion of Effects: The proposed project was reviewed and found to be located within the Airport Influence Area of Ontario International Airport ("ONT") and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan ("ALUCP") for ONT. The subject site is required to file and record an Avigation Easement with the Ontario International Airport Authority prior to obtaining a Certificate of Occupancy. The site is located within the airport influence area but outside the airport safety zones. The proposed Specific Plan Amendment will continue to allow residential uses within Planning Areas 1-4, and 17-19 is compatible with the ALUCP for ONT. Therefore, any impacts would be reduced to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

f. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?

Discussion of Effects: The City's Safety Element, as contained within The Ontario Plan, includes policies and procedures to be administered in the event of a disaster. The Ontario Plan seeks interdepartmental and inter-jurisdictional coordination and collaboration to be prepared for, respond to and recover from every day and disaster emergencies. In addition, the project will comply with the requirements of the Ontario Fire Department and all City requirements for fire and other emergency access. Because the project is required to comply with all applicable City codes, any impacts would be reduced to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

g. Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?

Discussion of Effects: The project site is not located in or near wildlands. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

10. HYDROLOGY & WATER QUALITY. Would the project:

a. Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality?

Discussion of Effects: The grading and drainage of the Parkside Specific Plan area shall be designed to detain, filter, and treat surface runoff, in a manner and combination which is practical, to comply with the most recent requirements of the San Bernardino County NPDES Stormwater Program's Water Quality Management (WQMP) for significant new development projects. The objective of the WQMP for this project is to minimize the detrimental effects of urbanization on the beneficial uses of receiving waters, including effects caused by increased pollutants and changes in hydrology. These effects may be minimized through the implementation of site designs that reduce runoff and pollutant transport by minimizing impervious surfaces and maximizing on-site infiltration, Source Control Best Management Practices (BMPs) and/or either on-site structural Treatment Control BMP's, or participation in regional or watershed-based Treatment Control BMPs. Prior to the issuance of a grading or construction permit, a Storm Water Pollution Prevention Plan (SWPPP) will also be prepared. The SWPPP will be prepared to comply with the California State Water Resources Control Board's (State Water Board) current, "General Permit to Discharge Storm Water Associated with Construction Activity" and the current Areawide Urban Storm Water Runoff (Regional NPDES Permit). The SWPPP will identify and detail all appropriate Best Management Practices (BMPs) to be implemented or installed during construction of the project. In addition to the preparation of a SWPPP for construction related activities, and as part of the approval of any grading plans within the Specific Plan Area, the applicant will be required to submit a Water Quality Management Plan (WQMP) on the regional model form provided by the City. The WQMP shall identify and detail all Site Design, Source Control, and Treatment Control BMPs to be implemented or installed at this site in order to reduce storm water pollutants and site runoff. All Priority Land Use (PLU) areas within the Specific Plan Area shall comply with the statewide Trash Provisions adopted by the State Water Resources Control Board (SWRCB) and trash requirements in the most current San Bernardino County Area-Wide MS4 Permit. Drainage from such as a Debris Separating Baffle Box (DSBB), Continuous Deflective System (CDS), or an equivalent hydrodynamic separator which has been approved by the SWRCB. The Specific Plan area is located within the sub-watershed area of a regional storm water runoff treatment facility. This regional treatment facility serves as an alternative to complete on-site treatment of all pollutants of concern. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

b. Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?

Discussion of Effects: The subject site was previously analyzed by the Certified Parkside Specific Plan EIR for residential, open space parkland and commercial uses. No increases in the current amount of water flow to the project site are anticipated, and the proposed project will not deplete groundwater supplies, nor will it interfere with recharge. The water use associated with the proposed use of the property was included in the Certified Parkside Specific Plan EIR analysis. The development of the site will require the grading of the site and excavation is expected to be less than three feet and would not affect the existing aquifer, estimated to be about 230 to 250 feet below the ground surface. No adverse impacts are anticipated.

Mitigation: No additional mitigation required.

c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:

i. Result in substantial erosion or siltation on- or off-site?

Discussion of Effects: It is not anticipated that the Project would alter the drainage pattern of the site or area, in a manner that would result in erosion, siltation or flooding on-or-off site, nor will the proposed Project increase the erosion of the subject site or surrounding areas. The existing drainage pattern of the site will not be altered, and it will have no significant impact on downstream hydrology. Stormwater generated by the project will be discharged in compliance with the statewide NPDES General Construction Activities Stormwater Permit and San Bernardino County MS4 permit requirements. With the full implementation of a Storm Water Pollution Prevention Plan developed in compliance with the General Construction Activities Permit requirements, the Best Management Practices included in the SWPPP, and a stormwater monitoring program would reduce any impacts to below a level of significance. No streams or streambeds are present on the site. No changes in erosion off-site are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

ii. Substantially increase the rate or amount of surface runoff water in a manner which would result in flooding on- or off-site?

Discussion of Effects: The proposed Project is not anticipated to increase the flow velocity or volume of storm water runoff to cause environmental harm from the site and will not create a burden on existing infrastructure. Furthermore, with the implementation of an approved Water Quality Management Plan developed for the site, in compliance with the San Bernardino County MS4 Permit requirements, stormwater runoff volume shall be reduced to below a level of significance.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

iii. Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?

Discussion of Effects: It is not anticipated that the project would create or contribute runoff water that would exceed the capacity of existing or planned stormwater drainage systems or create or contribute stormwater runoff pollutants during construction and/or post-construction activity. The stormwater flows will enter an existing storm pipe in Jurupa Street. Pursuant to the requirements of The Ontario Plan, the City's Development Code, and the San Bernardino County MS4 Permit's "Water Quality Management Plan" ("WQMP"), individual developments must provide site drainage and WQMP plans according to guidelines established by the City's Engineering Department. If master drainage facilities are not in place at the time of project development, then standard engineering practices for controlling post-development runoff may be required, which could include the construction of on-site storm water detention and/or retention/infiltration facilities. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

iv. Impede or redirect flood flows?

Discussion of Effects: Urbanization in the areas surrounding the project site have resulted in increased responsiveness of the basin to rainfall. The increase in impervious surfaces such as roofs, roads, and parking lots has resulted in a decrease in groundwater infiltration and larger storm surges. The Project site currently slopes southeast, and the existing drainage pattern is characterized by sheet flows that follow the slope to the northwest. The project site is not impacted by offsite flows. The project site is not located in a FEMA Firm Panel designated Flood Zone Risk, and according to the United States Fish and Wildlife Service National Wetlands Inventory ("NWI") no wetlands exist on the property. The Project could lead to the conversion of permeable surfaces to impermeable surfaces such as parking areas and building foundation areas. Any future development on the Project site would discharge onsite flows into an existing storm drain facility. As such, the proposed project would not impede or redirect flood flows. With adherence to existing federal, state, and local regulation no changes to the existing flood flows would occur.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

d. In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?

Discussion of Effects: Impacts associated with flooding are primarily related to the construction or placement of structures in areas prone to flooding including within an unprotected 100-year flood zone, and in areas susceptible to high tides, tsunamis, seiches, mudflows or sea level rise. Specifically, structures placed in flood prone areas, if flooded, would be damaged, and could subject people to injury or death. The National Flood Insurance Act of 1968 requires the identification of floodplain areas and establishment of flood-risk zones within those areas. FEMA administers the programs and coordinates with communities to establish effective floodplain management standards. According to FEMA, the Project is not located in a known floodplain. Furthermore, this area is not known to flood and is not typically subjected to flooding. The Project site is not located in a floodplain as shown in Figure S-2 of TOP. The Project site is dominated by Agricultural fallow fields and does not contain any vegetation associated with riparian features. No wetlands have been mapped on the project site according to the NWI. According to the FEMA, the Project is not located in an area that is subject to flood hazard, tsunami, or seiche zones. The project site is located over 60 miles east of the Pacific Ocean and is not located in a mapped tsunami zone. Therefore, the project would not have a significant risk of flood hazard, tsunami, seiche zones, release of pollutants due to project inundation.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

e. Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?

Discussion of Effects: The Regional Water Quality Control Board's Basin Plan is designed to preserve and enhance water quality and protect the beneficial uses of all regional waters. Specifically, the Basin Plan (i) designates beneficial uses for surface and ground waters, (ii) sets narrative and numerical objectives that must be attained or maintained to protect the designated beneficial uses and conform to the state's anti-degradation policy, and (iii) describes implementation programs to protect all waters in the region. Development allowed by the Project would be required to adhere to requirements of the water quality control plan, including all existing regulation and permitting requirements. This would include the incorporation of best management practices ("BMPs") to protect water quality during construction and operational periods. Development of the Project would be subject to all existing water quality regulations and programs, as described in the regulatory section above, including all applicable construction permits. Existing General Plan policies related to water quality would also be applicable to the Project. Implementation of these policies, in conjunction with compliance with existing regulatory programs, would ensure that water quality impacts related to the Project would be less than significant.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

11. LAND USE & PLANNING. Would the project:

a. Physically divide an established community?

Discussion of Effects: The project site is in an area that is currently being developed with residential and future commercial land uses. This project will be of similar design and size to surrounding development. No adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified

Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

b. Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?

Discussion of Effects: The subject site was previously analyzed by the Certified Parkside Specific Plan EIR residential, parkland and commercial uses. The Amendment to the Parkside Specific Plan (File No. PSPA19-007) proposes to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7) and will not create greater impacts than were identified in the Certified Parkside Specific Plan EIR. The proposed project does not interfere with any policies for environmental protection. As such, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

12. MINERAL RESOURCES. Would the project:

a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?

Discussion of Effects: The Project site is located within a mostly developed area surrounded by residential and future commercial uses. There are no known mineral resources in the area. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

b. Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?

Discussion of Effects: There are no known mineral resources in the area. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

13. NOISE. Would the project result in:

a. Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?

Discussion of Effects: The subject site was previously analyzed by the Certified Parkside Specific Plan EIR for residential, parkland and commercial uses. In addition, the Project will not expose people to or generate noise levels in excess of standards as established in the Certified TOP EIR (Section 5.12). The subject site is surrounded on the north, east, and south sides by residential new and future residential uses, to the east of the subject site is the Cucamonga Flood Control channel, and the subject site is not within the landing approach of the Ontario International Airport or Chino Airport. No additional analysis will be required at the time of site development review.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

b. Generation of excessive groundborne vibration or groundborne noise levels?

Discussion of Effects: The subject site was previously analyzed by the Certified Parkside Specific Plan EIR for residential, parkland and commercial uses and the uses associated with this project normally do not induce groundborne vibrations. As such, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

c. For a project located within the vicinity of a private airstrip or the noise impact zones of the airport land use compatibility plan for ONT and Chino Airports, would the project expose people residing or working in the project area to excessive noise levels?

Discussion of Effects: The proposed Amendment was reviewed and found to be located within the Airport Influence Area of Ontario International Airport ("ONT") and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan ("ALUCP") for ONT. The project site is located outside of the Safety, Noise Impact and Airspace Protection Zones. A portion of the project site is located within the 65-70 dB CNEL Noise Impact Zones; however, the proposed zone change is a compatible land use. In addition, the project site lies outside the boundaries of the Chino Airport Influence Area. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

14. POPULATION & HOUSING. Would the project:

a. Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of road or other infrastructure)?

Discussion of Effects: The subject site was previously analyzed by the Certified Parkside Specific Plan EIR for residential, open space parkland and commercial uses and the Amendment to the Parkside Specific Plan is consistent with General Plan land use designations and would not induce significant population growth. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

b. Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?

Discussion of Effects: The project site does not contain existing housing. Changing the permitted zoning on the site will not create existing housing impacts.

Mitigation: No additional mitigation required. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

PUBLIC SERVICES. Would the project:

c. Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:

i. Fire protection?

Discussion of Effects: The site is in a developed area currently served by the Ontario Fire Department. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. In addition, permits have issued for the construction of the City of Ontario Fire Station 9, which is in Planning Area 20 of the Specific Plan. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

ii. Police protection?

Discussion of Effects: The site is in a developed area, currently served by the Ontario Police Department. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

iii. Schools?

Discussion of Effects: Upon development, the Project will be required to pay school fees as prescribed by state law prior to the issuance of building permits. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

iv. Parks?

Discussion of Effects: The site is in a developed area, currently served by the City of Ontario. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. In addition, TOP Policy Plan (Policy PR1-5) has established a standard of 5 acres of parkland (public and private) per 1,000 residents, with a minimum of 2 acres of developed private park space per 1,000 residents (Policy PR1-6). Private parks are required to be approximately a quarter mile walking/biking distance from each residence. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

v. Other public facilities?

Discussion of Effects: The site is in a developed area, currently served by the City of Ontario. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

RECREATION. Would the project:

d. Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?

Discussion of Effects: This project is not proposing any new housing or large employment generator that would cause an increase in the use of neighborhood parks or other recreational facilities. In addition, TOP Policy Plan (Policy PR1-5) has established a standard of 5 acres of parkland (public and private) per 1,000 residents, with a minimum of 2 acres of developed private park space per 1,000 residents (Policy PR1-6). Private parks are required to be approximately a quarter mile walking/biking distance from each residence. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

e. Does the project include recreational facilities or require the construction or expansion of recreational facilities that have an adverse physical effect on the environment?

Discussion of Effects: This project is not proposing any new housing or large employment generator that would require the construction of neighborhood parks or other recreational facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

15. TRANSPORTATION. Would the project:

a. Conflict with a program plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?

Discussion of Effects: The proposed Amendment to the Parkside Specific Plan EIR will not create greater impacts than were identified in the certified the Parkside Specific Plan EIR. The proposed Amendment will not increase the residential units (Total 1,1947) and commercial square footage (115,000 SF) that was analyzed in the Parkside Specific Plan EIR. In addition, the number of vehicle trips per day will not increase above the established thresholds within Table III-11-E; *Project Trip Generation* of the Parkside Specific Plan EIR. Therefore, the traffic impacts will be consistent with and less than the traffic impacts projected and analyzed under the Certified Parkside Specific Plan EIR. The project will not create a substantial increase in the number of vehicle trips, traffic volume or congestion at intersections. Less than significant impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

b. Conflict or be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)?

Discussion of Effects: CEQA Guidelines Section 15064.3 subdivision (b) has been included in the 2018 CEQA Guidelines as part of the implementation of SB 743 which requires local jurisdictions to use Vehicle Miles Travelled (VMT) instead of Level of Service (LOS) methodologies for the purpose of determining the significance of traffic impacts under CEQA. Also, as part of the implementation of SB 743 local jurisdiction are given until July 1, 2020 to develop and implement thresholds of significance criteria and methodologies for evaluating VMT under the new SB 743 requirements. The City of Ontario has not yet established a VMT analysis threshold or analysis methodology. Therefore, impacts with respect to CEQA Guidelines Section 15064.3(b) are less than significant.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

c. Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?

Discussion of Effects: The Project is in an area that is being developed. Street improvements are along the Archibald Avenue and Eucalyptus complete, and no alterations are proposed for adjacent intersections or arterials. Each proposed development within the Specific Plan will be required to construct certain improvements to the streets and intersection per the circulation component of the Specific Plan. The Project will, therefore, not create a substantial increase in hazards due to a design feature. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

d. Result in inadequate emergency access?

Discussion of Effects: Development of the Project will be designed to provide access for all emergency vehicles and will therefore not create an inadequate emergency access. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

e. Result in inadequate parking capacity?

Discussion of Effects: The Amendment to the Parkside Specific Plan will not affect the parking capacity in the Specific Plan. Future development of the site will be required to meet parking standards established by the Ontario Development Code and will therefore not create an inadequate parking capacity. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

16. TRIBAL CULTURAL RESOURCES. Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:

a. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k)?

Discussion of Effects: The Certified Parkside Specific Plan EIR (Section 4 Culture Resources, page III-4-6 &7) indicates no archeological sites or resources have been recorded in the City with the Archeological Information Center at San Bernardino County Museum. The NAHC Sacred Lands File search also failed to indicate archaeological resources or artifacts associated with Tribal Cultural Resources (TCRs) within the Project site. The Project site has been highly disturbed by modern human activities to with agricultural production since the 1950's. However, as part of TOP EIR Mitigation Measure 5-4, prior to the issuance of grading permits for a Specific Plan or project that requires a General Plan amendment which the CEQA document defines cultural resource mitigation for potential tribal resources, the project applicant shall contact the designated tribe(s) to notify them of the grading, excavation, and monitoring program. The applicant shall coordinate with the City of Ontario and the tribal representative(s) to develop mitigation measures that address the designation, responsibilities, and participation of tribal monitors during grading, excavation, and ground-disturbing activities; scheduling; terms of compensation; and treatment and final disposition of any cultural resources, sacred sites, and human remains discovered on the site. The City of Ontario shall be the final arbiter of the conditions for projects within the City's jurisdiction. Therefore, it is concluded that the proposed Project will not impact Tribal Cultural Resources or Native America artifacts relating to TCRs and as such, no mitigation measures are recommended.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified

Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

b. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.

Discussion of Effects: The subject site is not listed in the California Register of Historic Resources. It is anticipated that during the application process the Lead Agency will notify the tribes of the proposed Specific Plan Amendment and will commence AB 52 Consultations as specified in the regulations. In addition, the results of the records research compiled from the CHRIS-SCCIC, the Scared Lands File Search (commissioned through the NAHC) failed to indicate known Tribal Cultural Resources (“TCR”) within the Project boundaries or within a one-mile radius of the Project area as specified in Public Resources Code (“PRC”): 210741, 5020.1(k), or 5024. Moreover, there was no indication of known TCRs within the Project site or within a one-mile radius of the Project Area. No impacts are anticipated through Project implementation.

Mitigation: No new mitigation measures are required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

17. UTILITIES AND SERVICE SYSTEMS. Would the project:

a. Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?

Discussion of Effects: The proposed Project is served by City of Ontario water system and has a 12-inch water line available for connection in Archibald Avenue adequate water supply for the Project. The proposed Project is served by the City of Ontario sewer system, which has a 36-inch trunk sewer line available for Archibald Avenue, and which has waste treated by the Inland Empire Utilities Agency at the RP-5 treatment plant. RP-5 is not at capacity and future development of this Project site will not cause RP-5 to exceed capacity. The Project will therefore not require the construction of new water or wastewater treatment facilities, or the expansion of existing facilities. No impacts are anticipated.

As discussed in the energy section above, the Project will have less than significant impacts with regard to electric power and natural gas. In addition, the Project will not have an impact on telecommunications facilities.

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Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

b. Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years? In making this determination, the City shall consider whether the project is subject to the water supply assessment requirements of Water Code Section 10910, et seq. (SB 610), and the requirements of Government Code Section 664737 (SB 221).

Discussion of Effects: The Project site is served by the City of Ontario water system. There is currently sufficient water supply available to the City of Ontario to serve this Project as per the findings of TOP EIR. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

c. Result in a determination by the wastewater treatment provider that serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?

Discussion of Effects: The Project site is served by the City of Ontario sewer system, which has waste treated by the Inland Empire Utilities Agency at the RP-5 treatment plant. RP-5 is not at capacity and future development of this project site will not cause RP-5 to exceed capacity. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

d. Generate solid waste in excess of state or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?

Discussion of Effects: City of Ontario serves the Project site. Currently, the City of Ontario contracts with a waste disposal company that transports trash to a landfill with sufficient capacity to handle the City's solid waste disposal needs. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to analyses are necessary.

e. Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?

Discussion of Effects: This Project complies with federal, state, and local statutes and regulations regarding solid waste. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

18. WILDFIRE. If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:

a. Substantially impair an adopted emergency response plan or emergency evacuation plan?

Discussion of Effects: Wildfire impacts were not analyzed in the Certified Parkside Specific Plan EIR. A discussion of potential wildfire impacts is provided herein.

The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

b. Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?

Discussion of Effects: The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

c. Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?

Discussion of Effects: The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

d. Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?

Discussion of Effects: The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

19. MANDATORY FINDINGS OF SIGNIFICANCE.

a. Does the project have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?

Discussion of Effects: The proposed Project does not have the potential to reduce wildlife habitat and threaten a wildlife species; therefore, no environmental impacts resulting from the Project are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

b. Does the project have the potential to achieve short-term environmental goals to the disadvantage of long-term environmental goals?

Discussion of Effects: The Project does not have the potential to achieve short-term environmental goals to the disadvantage of long-term environmental goals.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

c. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current project, and the effects of probable future projects.)

Discussion of Effects: The Project does not have impacts that are cumulatively considerable.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR and TOP EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

d. Does the project have environmental effects that will cause substantial adverse effects on human beings, either directly or indirectly?

Discussion of Effects: The Project does not have environmental effects that will cause substantial adverse effects on human beings, either directly or indirectly.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified Parkside Specific Plan EIR. No changes or additions to the Certified Parkside Specific Plan EIR analyses are necessary.

EARLIER ANALYSES

(Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, one or more effects have been adequately analyzed in an earlier EIR or Negative Declaration. Section 15063(c)(3)(D)):

- 1) Earlier Analyses Used. Identify earlier analyses used and state where they are available for review.
 - a) The Parkside Specific Plan EIR
 - b) The Ontario Plan Final EIR
 - c) The Parkside Specific Plan
 - d) The Ontario Plan (TOP)
 - e) City of Ontario Official Zoning Map
 - f) City of Ontario Development Code
 - g) Ontario International Airport Land Use Compatibility Plan
 - h) Ontario International Airport Land Use Compatibility Plan Negative Declaration (SCH 2011011081)

All documents listed above are on file with the City of Ontario Planning Department, 303 East "B" Street, Ontario, California 91764, (909) 395-2036.

- 2) Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of, and adequately analyzed in, an earlier document pursuant to applicable legal standards.

MITIGATION MEASURES

(For effects that are "Less than Significant with Mitigation Incorporated," describe the mitigation measures, which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.)

The Mitigation Measures contained in the Certified Parkside Specific Plan EIR adequately mitigate the impacts of the proposed Project. These mitigation measures are contained in the attached Mitigation Monitoring Program.

No additional mitigation beyond that previously imposed is required.

Exhibit A— Certified Parkside Specific Plan EIR Figure 1-3, Proposed Land Use Plan

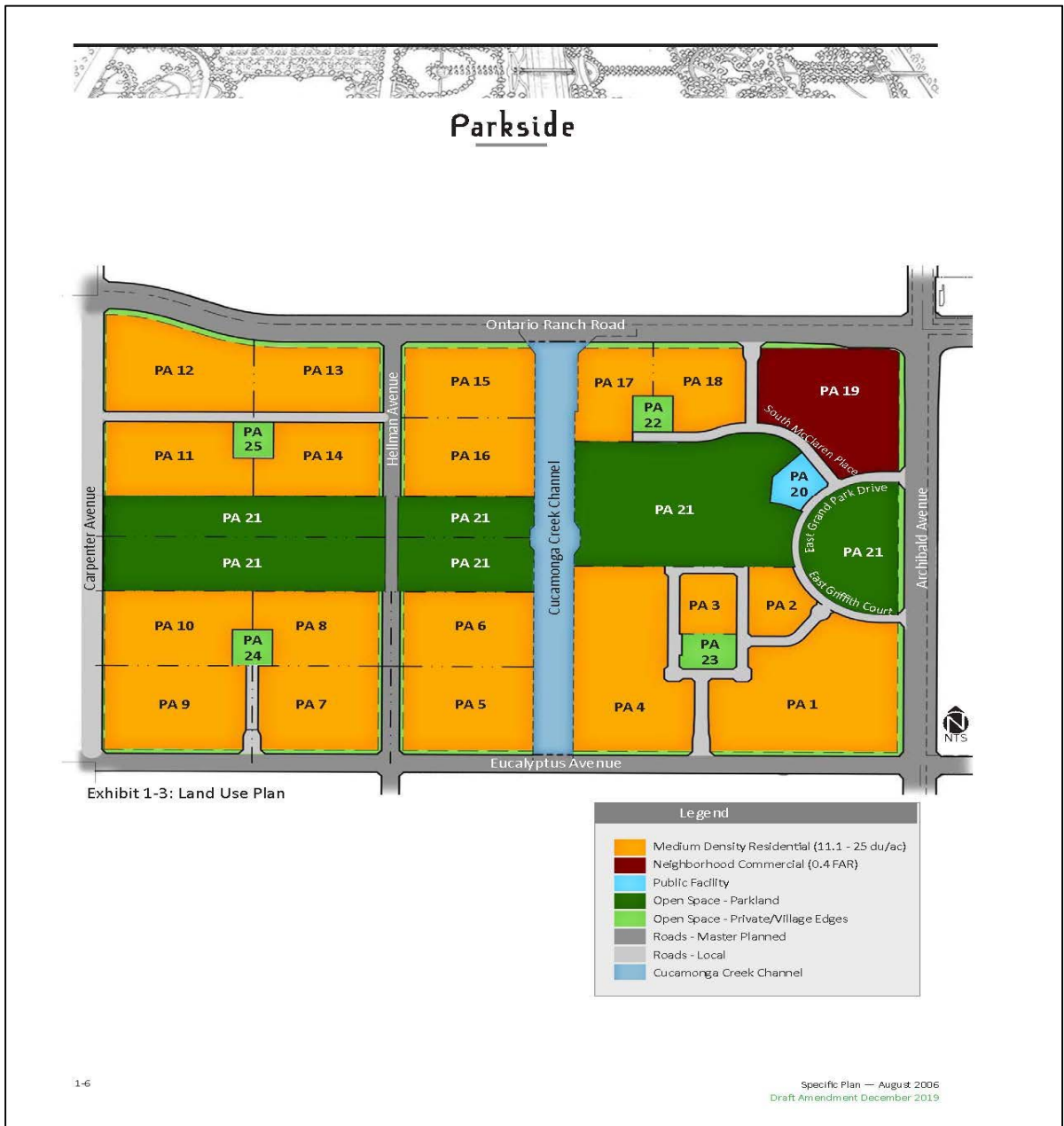
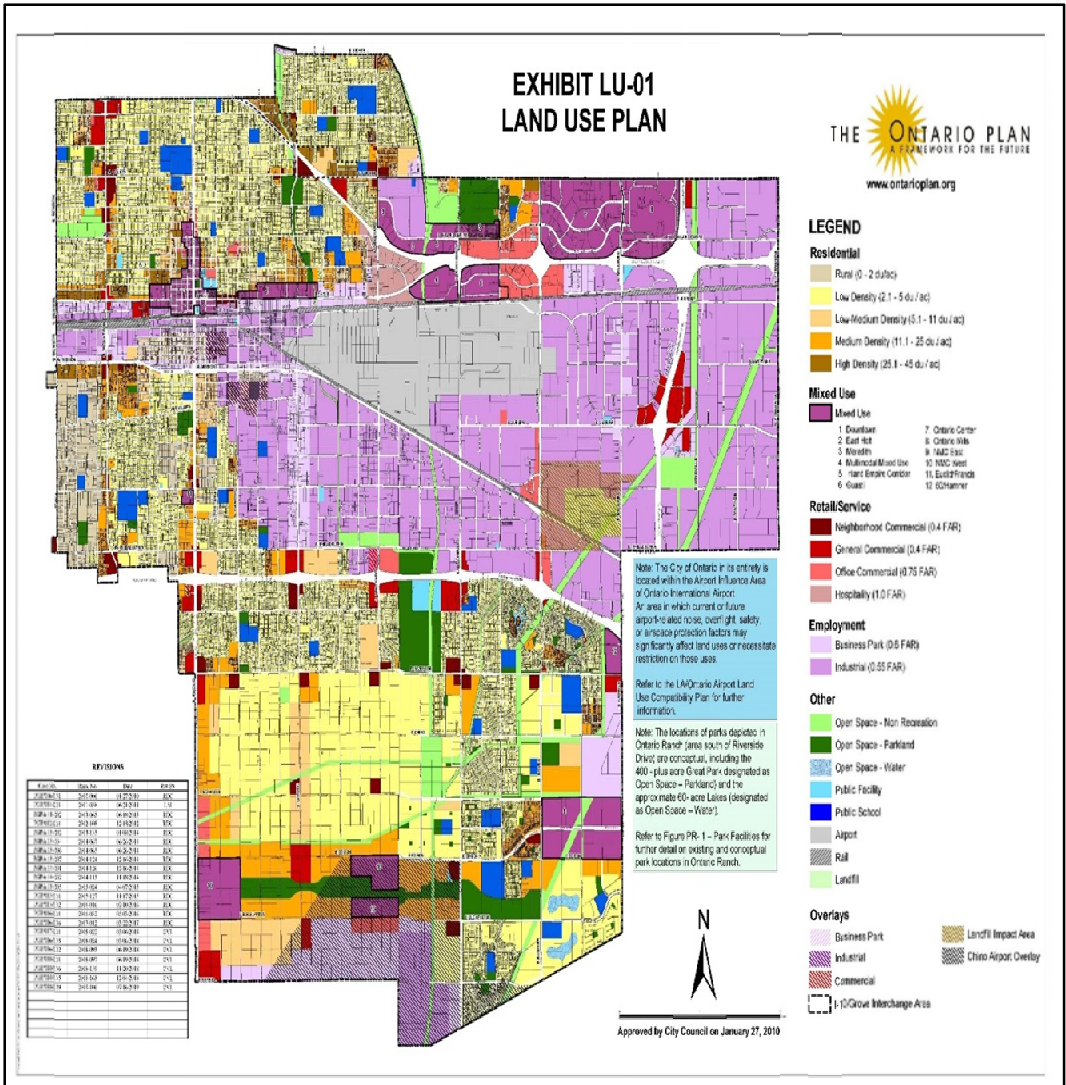


Exhibit B—2010 TOP Policy Plan (Exhibit LU-01) Land Use Plan



5.0 MITIGATION MONITORING PROGRAM

CITY OF ONTARIO, CALIFORNIA

**Parkside Specific Plan
SCH Number 2004011008**

July 2006

Prepared for:

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This statement is prepared in
compliance with the California
Environmental Quality Act

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INTRODUCTION

CEQA Requirements

The California Environmental Quality Act (CEQA) requires that when a public agency completes an environmental document that includes measures to mitigate or avoid significant environmental effects, the public agency must adopt a reporting or monitoring program for the changes to the project which it has adopted or made a condition of project approval in order to mitigate or avoid significant effects on the environment. The reporting or monitoring program must be designed to ensure compliance during project implementation (Public Resources Code Section 21081.6, CEQA Guidelines Section 15097).

The City of Ontario will coordinate monitoring of the implementation of all mitigation measures for the Parkside Specific Plan project. Monitoring will include: 1) verification that each mitigation measure has been implemented; 2) recordation of the actions taken to implement each mitigation measure; and 3) retention of records in the project file.

Program Objectives

The objectives of the mitigation monitoring program for the Parkside Specific Plan project are:

- To provide assurance and documentation that mitigation measures are implemented as planned;
- To collect analytical data to assist the City in its determination of the effectiveness of the adopted mitigation measures;
- To make available to the public, upon request, the City 's record of compliance with project mitigation measures.

By including both monitoring and reporting provisions, the City of Ontario has voluntarily exceeded the minimum requirements of Public Resource Code Section 21081.6, which allows selection of monitoring or reporting, but does not require both.

Overview of the Project

The Specific Plan consists of approximately 250 acres located within the 8,200-acre New Model Colony, and is bounded by Edison Avenue to the north, Archibald Avenue to the east, and Eucalyptus Avenue to the south. Cucamonga Creek flows in a southerly direction approximately through the center of the project area. The Specific Plan is an application for 430 single-family residential dwelling units, 1,517 multi-family residential units, 11.5 acres of commercial use, a 50-acre Great Park, and 6 acres of recreational trails.

More detailed information regarding the project is provided in the March 2006 Draft Environmental Impact Report related to this project.

Organization of the Mitigation Monitoring Program

Introduction: Provides an overview of CEQA's monitoring and reporting requirements, program objectives, the project for which the program has been prepared, and the manner in which the mitigation monitoring program has been organized.

Description of Program: Describes the City of Ontario entities responsible for implementation of the mitigation monitoring program, the program scope, procedures for monitoring and reporting, public availability of documents, the process for making changes to the program, types of mitigation measures and the manner in which monitoring will be coordinated to ensure implementation of mitigation measures.

Mitigation Monitoring and Reporting Summary: Outlines the impacts and mitigation measures, responsible entities, and the timing for monitoring and reporting for each mitigation measure included in the program.

Report Preparation: Lists the individuals involved in development of this mitigation monitoring program.

DESCRIPTION OF PROGRAM

Mitigation Monitoring Procedures

This mitigation monitoring program delineates responsibilities for monitoring the project, but also allows responsible parties flexibility and discretion in determining the best manner of monitoring implementation. Monitoring procedures will vary according to the type of mitigation measure. The timing for monitoring and reporting is described in the monitoring and reporting summary table included as part of this program. Adequate monitoring consists of demonstrating that monitoring procedures took place and that mitigation measures were implemented.

In order to enhance the effectiveness of the monitoring program, the city will utilize existing systems where appropriate. For instance, with any major construction project, the city generally has at least one inspector assigned to monitor project construction. These inspectors are familiar with a broad range of regulatory issues and will provide first line oversight for much of the monitoring program.

Reporting Procedures

A plan check review and construction inspection process will be utilized as the first line for much of the monitoring program, and will also serve to provide the background documentation for the reporting program.

Reporting consists of establishing a record that a mitigation measure is being implemented, and generally involves the following steps:

- Reporting forms are distributed to the appropriate responsible entity or its representative (as indicated in the summary form) or existing reporting processes are used for verification of compliance.
- Responsible entities or their representatives verify compliance by signing the monitoring and reporting form and/or documenting compliance using their own internal procedures when monitoring is triggered.
- Responsible entities or their representatives provide the city with verification that monitoring has been conducted and ensure, as applicable, that mitigation measures have been implemented.
- Construction inspectors prepare construction activities reports during the construction phase and provide project reports, as appropriate, to the city.

The city will also be responsible for assisting responsible entities and/or their representatives with reporting responsibilities to ensure that they understand their charge and complete their reporting procedures accurately and on schedule.

Public Availability

All monitoring reporting forms, summaries, data sheets, and correction instructions related to the mitigation monitoring program for the Parkside Specific Plan project will be available for public review upon request at the City of Ontario Planning Department.

Program Changes

Minor changes to the mitigation monitoring program, if required, will be made in accordance with CEQA and would be permitted after further review and approval by the city. Such changes could include reassignment of monitoring and reporting responsibilities and/or program redesign to make any appropriate improvements. No change will be permitted unless the mitigation monitoring and reporting program continues to satisfy the requirements of Public Resources Code Section 21081.6.

Implementation of Mitigation Measures Being Monitored

In general, implementation of the mitigation monitoring program will require the following actions:

- Responsible entities or their representatives with reporting responsibilities will review the EIR, which provides general background information on the reasons for including specified mitigation measures.
- Problems or exceptions to compliance will be addressed by the City, as appropriate.
- Periodic meetings may be held during project implementation to report on compliance with mitigation measures.

Mitigation Monitoring Program Matrix

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments
Agricultural Resources	The proposed project would conflict with existing agricultural uses.	MM Ag 1: In order to minimize conflicts between urban and agricultural land uses, each Specific Plan developed for properties within the NMC must comply with the Agricultural Overlay District requirements for urban development in proximity to existing agricultural operations. The proposed project shall establish a minimum 100-foot separation between active agricultural operations and new, non-agricultural development, or an equivalent easement that is approved by the City of Ontario.	Prior to construction.	Planning Department	Less than Significant	
Agricultural Resources	The proposed project would conflict with existing agricultural uses.	MM Ag 2: In order to minimize conflicts between urban and agricultural land uses, all residential units in the Parkside Specific Plan shall be provided with a deed disclosure, or similar notice, approved by the City Attorney regarding the proximity and nature, including odors, of neighboring agricultural uses.	Prior to opening of model homes	City Attorney	Less than Significant	
Agricultural Resources	The proposed project would result in the cancellation of Williamson Act contracts, loss of prime Farmland, loss of existing agricultural use, and provide	No feasible mitigation measures were found. See Section III-1 for complete analysis.	NA	NA	Significant	

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification		
						Signature	Date	Comments
	infrastructure which might cause other ag. lands to convert.							
Air Quality	Emissions from project construction equipment.	MM Air 1: During construction, mobile construction equipment will be properly maintained at an offsite location, which includes proper tuning and timing of engines. Equipment maintenance records and equipment design specification data sheets shall be kept on-site during construction.	During construction.	Contractor	Significant			
Air Quality	Emissions from project construction equipment.	MM Air 2: During construction of the proposed improvements, all contractors will be advised not to idle construction equipment on site for more than ten minutes.	During construction.	Contractor	Significant			
Air Quality	Emissions from project construction equipment.	MM Air 3: Configure construction parking to minimize traffic interference.	During construction.	Contractor	Significant			
Air Quality	Emissions from project operation.	MM Air 4: Local transit agencies shall be contacted to determine bus routing in the project area that can accommodate bus stops at the project access points and the project shall provide bus passenger benches and shelters at these project access points.	Prior to approval of street improvement plans.	Specific Plan Developer and Engineering Department	Significant			
Biological Resources	Adversely affect any endangered or threatened species, or any species identified as a candidate, sensitive or	MM Bio 1: There may be a probability of owl colonization within the project site considering the presence of foraging habitat and previous records of presence. To ensure that no direct loss of individuals occurs, mitigation shall be completed prior to initiation of on-site grading activities for each development phase. A pre-construction survey for	Prior to grading permit	Planning Department	Less than significant			

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments
	<p>special status.</p> <p>According to the Habitat Evaluation conducted for the project site, there may be a probability of owl colonization prior to site construction due to their presence in the vicinity of the site.</p>	<p>resident burrowing owls will be conducted by a qualified biologist. The survey will be conducted 30 days prior to construction activities. If ground-disturbing activities are delayed or suspended for more than 30 days after the preconstruction survey, the site should be resurveyed for owls.</p> <p>If owls are determined to be present within the construction footprint, they will be captured and relocated. If non-breeding owls must be moved away from the disturbance area, passive relocation techniques will be used. The pre-construction survey and any relocation activity will be conducted in accordance with the CDFG Report on Burrowing Owl Mitigation, 1995. According to CDFG guidelines, mitigation actions will be conducted from September 1 to January 31, which is prior to the nesting season. However, burrowing owl nesting activity is variable, and as such the time frame will be adjusted accordingly. Should eggs or fledglings be discovered in any owl burrow, the burrow cannot be disturbed (pursuant to CDFG guidelines) until the young have hatched and fledged (matured to a stage that they can leave the nest on their own).</p> <p>Occupied burrows will not be disturbed during the nesting season (February 1 through August 31) unless a qualified biologist approved by the Department of Fish and Game verifies through non-</p>				

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification		
						Signature	Date	Comments
		<p>invasive methods that either: a) the adult birds have not begun egg-laying and incubation; or b) the juveniles from the occupied burrows are foraging independently and are capable of independent survival. If a biologist is unable to verify one of the above conditions, then no disturbance shall occur within 300 feet of the burrowing owls nest during the breeding season to avoid abandonment of the young.</p> <p>Passive relocation can be used to exclude owls from their burrows (outside the breeding season or once the young are able to leave the nest and fly) by installing one-way doors in burrow entrances. These one-way doors allow the owl to exit the burrow, but not enter it. These doors should be left in place 48 hours to ensure owls have left the burrow. Artificial burrows should be provided nearby. The project area should be monitored daily for one week to confirm owl use of burrows before excavating burrows in the impact area. Burrows should be excavated using hand tools and refilled to prevent reoccupation. Sections of flexible pipe should be inserted into the tunnels during excavation to maintain an escape route for any animals inside the burrow.</p>						
Biological Resources	The proposed project will affect open foraging habitat.	MM Bio 2: The project proponent shall be required to pay City of Ontario open space mitigation fees. Fees collected will be used “to acquire and restore mitigation lands to offset impacts to species now living in the New Model Colony and impacts to	Prior to grading permit	Planning Department	Less than Significant			

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification	
						Signature	Date Comments
		existing open space,” according to the City of Ontario Development Impacts Fee Calculation Report and the Settlement and general Release Agreement. Development is currently required to pay \$4,320 per acre. Therefore, the proposed project will pay approximately \$2,298,240 for open space acquisition based upon the current fee.					
Biological Resources	The proposed project will affect open foraging habitat.	MM Bio 3: While project impacts to individual raptor species were considered to be not significant, the following mitigation measure will also be incorporated in order to eliminate or reduce any potential impacts to raptors and/or migratory birds. Construction and/or removal of windrow trees will occur outside of the nesting season (February 1 through August 31). If tree removal activities must occur during the breeding season, the mitigation measure in MM Bio 4 shall be implemented.	Prior to grading permit	Planning Department	Less than Significant		
Biological Resources	Adversely affect any endangered or threatened species and any species identified as candidate, sensitive or special status through the loss of habitat.	MM Bio 4: If project construction activities involving heavy equipment and/or windrow tree removal are to occur during the nesting/breeding season (between February 1 st and August 31 st) of potentially occurring sensitive bird species, a pre-construction field survey shall be conducted by a qualified biologist to determine if active nests of species protected by MBTA or CDFG are present in the construction zone or within a buffer of 500 feet. Pre-construction nesting/breeding surveys shall be conducted in all CDFG jurisdictional areas and within windrow trees. If no active nests are found during	Prior to issuance of grading permits	Planning Department	Less than significant		

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification	
						Signature	Date Comments
		<p>the survey, construction activities may proceed.</p> <p>If active nests are located during the pre-construction surveys, no grading, heavy equipment or tree removal activities shall take place within at least 500 feet of an active listed species or raptor nest, 300 feet of other sensitive bird nests (non-listed), and 100 feet of most common songbird nests.</p>					
Cultural Resources	The proposed project could affect unknown buried cultural resources.	MM Cultural 1: Should any cultural and/or archaeological resources be accidentally discovered during construction, construction activities shall be moved to other parts of the project site and a qualified archaeologist shall be contacted to determine the significance of these resources. If the find is determined to be an historical or unique archaeological resource, as defined in Section 15064.5 of the CEQA Guidelines, avoidance or other appropriate measures shall be implemented.	During construction	Planning Department	Less than significant		
Cultural Resources	The proposed project could affect unknown buried cultural resources.	MM Cultural 2: If human remains are uncovered at any time, all activities in the area of the find shall be halted by the developer or its contractor and the County Coroner shall be notified immediately pursuant to CA Health & Safety Code Section 7050.5 and CA PRC Section 5097.98. If the Coroner determines that the remains are of Native American origin, the Coroner shall proceed as directed in Section 15064.5(e) of the CEQA Guidelines.	During construction	Planning Department	Less than significant		
Cultural	The proposed	MM Cultural 3: Prior to the issuance	Prior to grading	Planning	Less than		

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments
Resources	project has the potential to affect unknown buried paleontological resources	<p>of grading permits, a qualified paleontologist shall be retained to prepare a Paleontological Resources Survey of the project site, for approval by the City, to determine the site specific potential of finding paleontological resources within the project site. If the approved Paleontological Resources Survey determines that it is unlikely that paleontological resources will be uncovered by earth-moving activities, grading and construction activities may proceed, subject to compliance with all other mitigation measures. However, if the approved Paleontological Resources Survey determines that it is likely that paleontological resources will be uncovered during earth-moving activities, a qualified paleontologist shall be retained to develop a Paleontological Resources Monitoring and Treatment Plan (PRMTP) for approval by the City. Following City approval of the PRMTP, grading and construction activities may proceed in compliance with the provisions of the approved PRMTP. The PRMTP shall include the following measures:</p> <ul style="list-style-type: none"> a. Identification of those locations within the project site where paleontological resources are likely to be uncovered during grading. b. A monitoring program specifying the procedures for the monitoring of grading activities by a qualified paleontologist or qualified designee. 	permits	Department	significant	

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments
		<p>c. If fossil remains large enough to be seen are uncovered by earth-moving activities, a qualified paleontologist or qualified designee shall temporarily divert earth-moving activities around the fossil site until the remains have been evaluated for significance and, if appropriate, have been recovered; and the paleontologist or qualified designee allows earth-moving activities to proceed through the site. If potentially significant resources are encountered, a letter of notification shall be provided in a timely manner to the City, in addition to the report (described below) that is filed at completion of grading.</p> <p>d. If a qualified paleontologist or qualified designee is not present when fossil remains are uncovered by earth-moving activities, these activities shall be stopped and a qualified paleontologist or qualified designee shall be called to the site immediately to evaluate the significance of the fossil remains.</p> <p>e. At a qualified paleontologist or qualified designee's discretion and to reduce any construction delay, a construction worker shall assist in removing fossiliferous rock samples to an adjacent location for temporary stockpiling pending eventual transport to a laboratory facility for processing.</p>				

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments
		<p>f. A qualified paleontologist or qualified designee shall collect all significant identifiable fossil remains. All fossil sites shall be plotted on a topographic map of the project site.</p> <p>g. If the qualified paleontologist or qualified designee determines that insufficient fossil remains have been found after fifty percent of earthmoving activities have been completed, monitoring can be reduced or discontinued.</p> <p>h. Any significant fossil remains recovered in the field as a result of monitoring or by processing rock samples shall be prepared, identified, catalogued, curated, and accessioned into the fossil collections of the San Bernardino County Museum, or another museum repository complying with the Society of Vertebrate Paleontology standard guidelines. Accompanying specimen and site data, notes, maps, and photographs also shall be archived at the repository.</p> <p>i. Within 6 months following completion of the above tasks, a qualified paleontologist or qualified designee shall prepare a final report summarizing the results of the mitigation program and presenting an inventory and describing the scientific significance of any fossil remains accessioned into the museum repository. The report shall be</p>				

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments
		submitted to the City Planning Department and the museum repository. The report shall comply with the Society of Vertebrate Paleontology standard guidelines for assessing and mitigating impacts on paleontological resources.				
Geology/Soils	The project has the potential increase erosion of topsoil by wind.	MM Geo 1: To reduce impacts associated with erosion due to high winds, prior to construction, all tentative tracts and other construction activities will apply for and adhere to the permit given by the City of Ontario and enforced by the Building Official found in Title 6, Chapter 12, sections 6-12.01 – 6-12.07. The permit lasts for one (1) year, therefore, all construction lasting for a period of more than one calendar year from the date of issue will reapply for the permit and pay appropriate annual fees. At a minimum, the permit prohibits the disturbance of the surface or subsurface of more than one (1) acre of land without meeting permit requirements which can include such things as the application of soil stabilizers and limitations on grading activities during wind events.	Prior to grading permits	Building Department	Less than significant	

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification	
						Signature	Date Comments
Geology/Soils	The project has the potential to include/affect soils which are unsuitable for construction.	MM Geo 2: To properly assess and address the suitability of on-site soils to be used as fill, a geotechnical evaluation shall be performed by a qualified professional prior to the approval of the Tentative Tract map or site plan for a given phase of development. This evaluation will include an analysis of the organic matter content of soils on the site. If the organic matter content of the soils is greater than 2 percent when mixed with subsurface soils and/or imported fill, then manure will be removed from the site prior to grading operations.	Prior to tentative map approval report shall be submitted. Removal of unsuitable soils prior to grading.	Planning and Building Departments	Less than significant		
Geology/Soils	The project has the potential to have soils that are/could become unstable due to high organic content.	MM Geo 3: Site materials should be continuously tested and excavated to a minimum of 4 feet where soils generally become denser. Actual removal depths will be determined during grading when subsurface conditions are exposed. Input of crop residues and application of organic fertilizers at this site could have resulted in high soil organic matter contents. The mitigation proposed in Section III-6, Hazards/Hazardous Materials, will also mitigate for the management of organic matter in the soil.	Prior to grading permits	Building Department	Less than significant		
Hazards/Hazardous Materials	The proposed project could be located on a site that has been impacted by hazardous materials.	MM Haz 1: During development of the Specific Plan, if soils are found to be contaminated with petroleum products or other hazardous materials, they will be excavated and properly disposed of. After removal of contaminated soils, confirmation samples will be collected from the excavation to confirm adequate removal of petroleum-impacted soils.	During grading	Planning Department	Less than significant		

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification		
						Signature	Date	Comments
Hazards/ Hazardous Materials	The proposed project could be located on a site that has been impacted by hazardous materials.	MM Haz 2: All septic tanks encountered on the project site will be properly removed and disposed of, per City and State procedures, prior to site development. All water wells on the project site which are proposed to be abandoned will be properly destroyed prior to site development in accordance with City requirements. These activities will be subject to the City of Ontario Building Safety requirements.	Prior to grading permits	Building Department	Less than significant			
Hazards/ Hazardous Materials	The proposed project could be located on a site that has been impacted by hazardous materials.	MM Haz 3: If, while performing any excavation as part of project construction, material that is believed to be hazardous waste is discovered, as defined in Section 25117 of the California Health & Safety Code, the developer shall contact the City of Ontario Fire Department and the County of San Bernardino Fire Department Hazardous Materials Division. Excavation shall be stopped until the material has been tested and the presence of hazardous waste has been confirmed. If no hazardous waste is present, excavation may continue. If hazardous waste is determined to be present, the California Department of Toxic Substances Control shall be contacted and the material shall be removed and disposed of pursuant to applicable provisions of California law.	Prior to grading permits	Planning Department	Less than significant			
Hazards/ Hazardous Materials	The proposed project will create a significant hazard to the public or the	MM Haz 4: Prior to demolition of all onsite buildings and remaining foundations that were built before 1978 shall be evaluated for the presence of asbestos, mercury and lead-based paint and those materials shall be removed	Prior to grading permits	Planning Department	Less than significant			

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification		
						Signature	Date	Comments
	environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment.	according to the applicable regulations and guidelines established by the South Coast Management District, Department of Toxic Substances Control, and the United States Environmental Protection Agency. As per HM-2 in the GPA for the NMC Final EIR, page 5.10-6, the developer shall submit documentation to the City Building Department that asbestos, mercury and lead-based paint are not present on their site, or that the above removal process has occurred.						
Hazards/ Hazardous Materials	The proposed project would create a significant hazard to the public or the environment through ground cracking or the presence or release of methane gas.	MM Haz 5: To properly assess and address the suitability of on-site soils to be used as fill, a geotechnical evaluation shall be performed by a qualified professional prior to the approval of the Tentative Tract map or site plan for a given phase of development. Fill material imported from other areas shall be tested prior to placement on-site to assess that is suitable to be used as fill, including testing for unsafe levels of hazardous materials. This evaluation, on both on- and off-site soils, will include an analysis of the organic matter content of the soils. If the organic matter content of the soils is greater than 2 percent when mixed with subsurface soils and/or imported fill, then manure will be removed from the site prior to grading operations.	Prior to grading permits	Planning Department	Less than significant			

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification	
						Signature	Date Comments
Hazards/ Hazardous Materials	The proposed project would create a significant hazard to the public or the environment through ground cracking or the presence or release of methane gas.	MM Haz 6: To reduce the risk of ground cracking, manure shall be removed from the site, such that the organic matter content of on-site soils shall not exceed 2 percent (a 2 percent total organic content is allowed, of which no more than 1 percent can be manure) in the building foundation areas when mixed with underlying clean soils and imported fill.	Prior to building permits	Planning Department	Less than significant		
Hazards/ Hazardous Materials	The proposed project would expose people or property to risk associated with proximity to an airport.	MM Haz 7: To mitigate for any potential impacts related to proximity to the Chino Airport, all development within the Specific Plan will comply with the building height constraints identified in the GPA for the NMC (1998).	Prior to building permits	Planning Department	Less than significant		
Hazards/ Hazardous Materials		MM Haz 8: To disclose to the buyer or lessee of subdivided lands within the Parkside Specific Plan project of the proximity of this site to the Chino Airport as required by AB 2776, the City shall disclose, and ensure that the developer makes such disclosures, as required by law to all future buyers.	Prior to building permits	Planning Department	Less than significant		
Hydrology/ Water Quality	During project construction, the project could create or contribute runoff water that would violate any water quality	MM Hydro 1: In order to ensure that construction activities associated with the Parkside Specific Plan will not cause a violation of any water quality standard or waste discharge requirements and to assure no substantial degradation of water quality occurs, and to implement the intent of mitigation measures included in the Final EIR for the GPA	Prior to and during construction	Engineering Department	Less than significant		

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification	
						Signature	Date Comments
	standards or waste discharge requirements, including the terms of the City’s municipal separate stormwater sewer system permit.	for the NMC, developments within the project area shall comply with all applicable provisions of the state’s General Permit for Construction Activities (Order No. 99-08-DWQ, or most recent version) during all phases of construction. A copy of evidence of the receipt of a Waste Discharge Identification Number from the State Regional Water Quality Control Board shall be filed with the City Engineer along with a copy of the Storm Water Pollution Prevention Plan (SWPPP) maps and BMPs. The City Engineer shall review and approve the provisions of the SWPPP prior to implementation of any SWPPP provision or starting any construction activity.					
Hydrology/ Water Quality	During project construction, the project could create or contribute runoff water that would violate any water quality standards or waste discharge requirements, including the terms of the City’s municipal separate stormwater sewer system	MM Hydro 2: In order to ensure that development within the Specific Plan will not cause or contribute to violations of any water quality standard or waste discharge requirements, and to assure no substantial degradation of water quality occurs, the project will complete a Water Quality Management Plan (WQMP) pursuant to the MS4 permit (Order No. 2002-0012) adopted by the City of Ontario. The project shall incorporate Site Design BMPs and Source Control BMPs, and potentially Treatment Control BMPs. The following tables (Table III-7-F and G) provide guidelines and BMPs that shall be incorporated as appropriate into project design (on construction drawings) and/or project specifications and implemented in the field to reduce the expected pollutants	Prior to and during construction	Engineering Department	Less than significant		

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification	
						Signature	Date Comments
	permit.	from various types of development. Table III-7-G correlates each BMP to the pollutants of concern which it removes/reduces and/or meets the design objectives for the BMP.					
Hydrology/ Water Quality	During project construction, the project could create or contribute runoff water that would violate any water quality standards or waste discharge requirements, including the terms of the City's municipal separate stormwater sewer system permit.	MM Hydro 3: To assure that development within the Specific Plan will not cause a violation of any water quality standard or waste discharge requirements, including San Bernardino County's MS4 permit issued by the SARWQCB, and to assure that no substantial degradation to water quality occurs after construction, any loading docks present within the academic or retail areas designated in the Specific Plan will be designed with devices to trap oil and grease, such that these pollutants are not discharged from the site in storm water or non-storm water discharges.	Prior to, during and after construction	Engineering Department	Less than significant		
Hydrology/ Water Quality	Significantly alter the flow velocity or volume of stormwater runoff in a manner that results in environmental harm.	MM Hydro 4: In order to reduce the risk of flooding and to implement mitigation measures included in the GPA for the NMC Final EIR, prior to issuance of grading permits, the City of Ontario shall coordinate with the San Bernardino County Flood Control District to ensure that the project meets County flood control requirements.	Prior to grading permits	Engineering Department	Less than significant		
Hydrology/	Substantially	MM Hydro 5: In order to conserve	Post construction	Planning	Less than		

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification		
						Signature	Date	Comments
Water Quality	deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted).	water and to mitigate for any potential unforeseen adverse impacts to a reduction in ground water recharge, the following measure has been recommended by the Chino Basin Water Conservation District. Landscaping within individual development projects and the 52-acre Great Park will retain and percolate both applied irrigation water and storm water in vegetated areas of parking lots and other areas, where appropriate; “depressed” planted areas bordered by shrubbery screens will be implemented rather than “mounded” grass and shrubbery planted screens. Neighborhood Edges and parks will be irrigated via reclaimed water.		Department	significant			
Hydrology/ Water Quality	After the project is completed, create or contribute runoff water that would violate any	MM Hydro 6: In order to reduce pollutants in post construction run-off and to implement mitigation measures included in the Final Environmental Impact Report for the NMC, the individual project owners and operators (e.g., homeowner associations, retail center owners, school district, parks	Post construction	Engineering Department	Less than significant			

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification		
						Signature	Date	Comments
	water quality standards or waste discharge requirements, including the terms of the City’s municipal separate stormwater sewer system permit.	department, etc.) shall ensure that all pest control, herbicide, insecticide and other similar substances used as part of maintenance of project features are handled, stored, applied and disposed of by those conducting facility maintenance in a manner consistent with all applicable federal, state and local regulations. The city Engineer shall monitor and enforce this provision.						
Hydrology/ Water Quality	After the project is completed, create or contribute runoff water that would violate any water quality standards or waste discharge requirements, including the terms of the City’s municipal separate stormwater sewer system permit.	MM Hydro 7: To mitigate possible temporary run-off from undeveloped properties located north (up-gradient) of the project site, drainage from properties north of the project site shall be conveyed to appropriate drainage facilities, as approved by the City Engineer.	Post construction	Engineering Department	Less than significant			
Noise	The project will result in a substantial temporary or	MM Noi 1: The construction activities of the proposed project shall comply with the City of Ontario noise ordinance that prohibits construction activities on	During construction	Planning Department	Less than significant			

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification		
						Signature	Date	Comments
	periodic increase in ambient noise levels in the project vicinity above levels existing without the project.	Sundays, federal holidays, and other days between the hours of 7:00 p.m. and 7:00 a.m.						
Noise	The project will result in a substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project.	MM Noi 2: Construction staging areas shall not be located within 150 feet of existing sensitive receptors and construction equipment shall be fitted with properly operating and maintained mufflers.	During construction	Planning Department	Less than significant			
<p><i>To reduce or eliminate impacts related to exterior and interior noise levels within the project exceeding City of Ontario standards, the following mitigation measures shall be implemented. However, the wall heights recommended in MM Noi 3 through 67 only apply to lots which have backyards directly adjacent to the roadways. For lots with front yards adjacent to the roadways, the windows and/or doors would need to have upgraded sound rated glazing products in order to comply with the City of Ontario's interior noise standards.</i></p>								
Noise	The project will expose people to, or generate, noise levels in excess of standards established in the local general plan or noise	MM Noi 3: A sound wall at least 7 feet high (relative to pad elevation) shall be constructed along the project site boundary for all perimeter	Prior to occupancy	Planning Department	Less than significant			

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments	
	ordinance or applicable standards.	lots adjacent to Archibald Avenue. If any residential structures are two-stories high, then windows facing Archibald Avenue would need to have upgraded sound rated glazing products and the rooms would need to have supplemental ventilation. A final acoustical report shall be submitted to address wall heights based on final grading plans. The report shall be reviewed and approved by the Planning Department prior to issuance of building permits.					

Impact Category	Impact	Mitigation Measure		Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments	
Noise	The project will expose people to, or generate, noise levels in excess of standards established in the local general plan or noise ordinance or applicable standards.	MM Noi 4: A sound wall at least 6 feet high (relative to pad elevation) shall be constructed along the project site boundary for all perimeter lots adjacent to Edison Avenue. If any residential structures are two-stories high, then windows facing Edison Avenue would need to have upgraded sound rated glazing products and the rooms would need to have supplemental ventilation. A final acoustical report shall be submitted to address wall heights based on final grading plans.	Prior to occupancy	Planning Department	Less than significant			

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments	
		The report shall be reviewed and approved by the Planning Department prior to issuance of building permits.					
Noise	The project will expose people to, or generate, noise levels in excess of standards established in the local general plan or noise ordinance or applicable standards.	MM Noi 5: A sound wall at least 7 feet high (relative to pad elevation) shall be constructed along the project site boundary for all perimeter lots adjacent to Eucalyptus Avenue. If any residential structures are two-stories high, then windows facing Eucalyptus Avenue would need to have upgraded sound rated glazing products and the rooms	Prior to occupancy	Planning Department	Less than significant		

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments	
		would need to have supplemental ventilation. A final acoustical report shall be submitted to address wall heights based on final grading plans. The report shall be reviewed and approved by the Planning Department prior to issuance of building permits.					
Noise	The project will expose people to, or generate, noise levels in excess of standards established in the local general plan or noise ordinance or applicable standards.	MM Noi 6: Architectural plans shall be submitted to the City of Ontario for an acoustical plan check prior to the issuance of building permits to assure the proper window and/or doors are upgraded for sound reduction and	Prior to occupancy	Planning Department	Less than significant		

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification	
						Signature	Date Comments
		proper ventilation systems are incorporated in order to meet the interior noise level requirement.					
Public Services	The project could result in impacts to fire services.	MMServ 1: To reduce fire hazards, wood-shingled and shake-shingled roofs are prohibited.	Prior to occupancy	Fire Department	Less than significant		
Public Services	The project could result in impacts to fire services.	MMServ 2: To reduce fire hazards, fire hydrant locations and water main sizes shall meet standards established by the City Fire Department and reviewed and implemented by the Engineering Department.	Prior to occupancy	Fire Department	Less than significant		
Public Services	The project could result in impacts to fire services.	MMServ 3: To reduce fire hazards when water is	Prior to occupancy	Fire Department	Less than significant		

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification	
						Signature	Date Comments
		provided to the site, adequate fire flow pressure shall be provided for residential areas and non-residential projects in accordance with currently adopted standards (2001 California Fire Code Appendix III-A).					
Public Services	The project could result in impacts to fire services.	MMServ 4: To reduce fire hazards, adequate water supply shall be provided by the Fire Department prior to the framing stages of construction.	Prior to construction	Engineering and Fire Departments	Less than significant		
Public Services	The project could result in impacts to fire services.	MMServ 5: To reduce fire hazards, houses located on cul-de-sacs longer than 300 feet shall	Prior to occupancy	Planning Department	Less than significant		

Impact Category	Impact	Mitigation Measure		Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification	
							Signature	Date
		be constructed with residential fire sprinklers.						
Public Services	The project could result in impacts to fire services.	MMServ 6: To reduce fire hazards, access roadways designed in accordance with Fire Department standards to within 150' of all structures, shall be provided prior to the framing stages of construction. This access is to be maintained in an unobstructed manner throughout construction.	Prior to occupancy	Planning Department	Less than significant			

Impact Category	Impact	Mitigation Measure		Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification	
							Signature	Date Comments
Public Services	The project could result in impacts to fire services.	MM Serv 7: A fire station located within the Specific Plan must be operational prior to the issuance of any certificates of occupancy in the Specific Plan.	Prior to occupancy	Planning Department		Less than significant		
Public Services	The project could impact public services.	MMServ 8: The developer shall pay library, police, and fire service development impact fees.	Prior to permits	Planning Department		Less than significant		
Public Services	The project could impact school services.	MMServ 9: The developer shall pay school fees or otherwise meet project obligations to schools, as required by Mountain View Unified and Chaffey Joint Union High School Districts.	Prior to permits	Planning Department		Less than significant		
Public Services	The project could impact parks.	MMServ 10: The portions of the Great	Prior to occupancy, as described.	Planning Department		Less than significant		

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments	
		Park (PA 22) located east of Cucamonga Creek shall be constructed no later than the issuance of the Certificate of Occupancy for the last housing unit in PAs 1 - 4 and PAs 17 - 19. The portion of the Great Park located west of Cucamonga Creek in PA 22 east of Hellman Avenue shall be constructed no later than the issuance of the Certificate of Occupancy for the last housing unit in PA 6 and PA 16. The remainder of PA 22 located west of Hellman Avenue shall be constructed no later than the issuance of					

Impact Category	Impact	Mitigation Measure		Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation	Verification Signature Date Comments		
		the Certificate of Occupancy for the last housing unit in PAs 7 – 10 and 11 – 14.							

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
Transportation/Traffic	The project will exceed, either individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<p>*MM Trans 1: Modify the intersection of Archibald Avenue/Edison Avenue to include the following geometrics:</p> <p>Northbound: Two left-turn lanes. Four through lanes. One right-turn lane.</p> <p>Southbound: Two left-turn lanes. Four through lanes. One right-turn lane.</p> <p>Eastbound: Two left-turn lanes. Three through lanes. Two right-turn lanes.</p> <p>Westbound: Two left-turn lanes. Three through lanes. One right-turn lane.</p>	Prior to occupancy	Engineering Department	Less than significant
Transportation/Traffic	The project will exceed, either individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<p>*MM Trans 2: Modify the intersection of future Carpenter Street/Edison Avenue to include the following geometrics:</p> <p>Northbound: One shared left-turn, through and right-turn lane.</p> <p>Southbound: N/A</p> <p>Eastbound: Two through lanes. One shared through and right-turn lane.</p> <p>Westbound: One left-turn lane. Three through lanes.</p> <p>Intersection: Control: Install Signal.</p>	Prior to occupancy of adjacent planning areas	Engineering Department	Less than significant

*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
Transportation/Traffic	The project will exceed, either individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<p>*MM Trans 3: Modify the intersection of future Carpenter Street/Merrill / Avenue to include the following geometrics: Northbound: N/A Southbound: One shared left-turn, through and right-turn lane. Eastbound: One shared left-turn and through lane. One through lane. Westbound: One through lane. One shared through and right-turn lane. Intersection: Control: TWSC.</p>	Prior to occupancy of adjacent planning areas	Engineering Department	Less than significant
Transportation/Traffic	The project will exceed, either individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	<p>MM Trans 4: Construction of full width of internal roadways and part width of the following roadways shall comply with City of Ontario Standards:</p> <ul style="list-style-type: none"> • Construct partial width improvements on the westerly side of Archibald Avenue at its ultimate cross-section as a divided arterial parkway 1A with bikeway (165’ right-of-way) adjacent to project boundary line. • Construct partial width improvements on the southerly side of Edison Avenue at its ultimate cross-section as a divided arterial parkway 1A (160’ right-of-way) adjacent to project boundary line. • Construct partial width improvements on the northerly side of Merrill Avenue at its ultimate cross-section as a standard arterial (108’ right-of-way) adjacent to project boundary line. 	Prior to occupancy of adjacent planning areas	Engineering Department	Less than significant

*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
Transportation/Traffic	The project will substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment).	MM Trans 4a: Intersection, median opening, and traffic signal spacing shall be in accordance with the City of Ontario New Model Colony Access Guidelines.	To be shown on tract maps. Prior to map approval.	Engineering Department	Less than significant
Transportation/Traffic	The project will substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment).	MM Trans 5: Sight distance at the project entrance roadways should be reviewed with respect to standard City of Ontario sight distance standards at the time of preparation of final grading, landscape and street improvement plans.	During plan check, prior to grading permit	Engineering Department	Less than significant
Transportation/Traffic	The project will substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment).	MM Trans 6: Signing/stripping should be implemented in conjunction with detailed construction plans for the project site.	During plan check, prior to approval of final road construction plans	Engineering Department	Less than significant
Transportation/Traffic	The project will exceed, either individually or cumulatively, the level of service standard established	MM Trans 7: The City should work with Omnitrans to develop additional routes and service for both local and regional service to the project area.	Prior to building permits	Engineering Department	Less than significant

*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
	by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.				
Transportation/Traffic	The project will conflict with adopted policies, plans or programs supporting alternative transportation (e.g., bus turnouts, bicycle racks).	MM Trans 8: The City should establish a Transportation System Management (TSM) Program with the goal of reducing vehicle trips to and from land uses within the City, and particularly focusing on the reduction of drive-alone vehicle use in work commuting. The program should set the overall policy and goals for trip reduction measures within the City, and require new developments to implement programs and measures to ensure compliance with those goals, such as preferential parking for carpools and vanpools, flex-time work hours, compressed work week, and distribution of information about ridesharing and transit services.	Ongoing	Planning Department	Less than significant
Transportation/Traffic	The project will conflict with adopted policies, plans or programs supporting alternative transportation (e.g., bus turnouts, bicycle racks).	MM Trans 9: The project will participate in the cost of off-site improvements through fair-share payment of the Development Impact fee as established by the City of Ontario. These fees should be collected and utilized as needed by the City to construct the improvements necessary to maintain the required level of service.	Ongoing	Planning Department	Less than significant
<i>The following Mitigation Measures (MM Trans 10 through MM Trans 31) have been identified to reduce the cumulative traffic impacts to a less than significant level and are required to attain the required LOS of intersections in the project area. The project will either install these improvements or pay their fair share mitigation fee, as determined by the City Engineer.</i>					

*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
Transportation/Traffic	The project will exceed, either individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	*MM Trans 10: Modify the intersection of Euclid Avenue/ Riverside Drive to include the following geometrics: Northbound: Two left-turn lanes. Four through lanes. One shared right-turn/ through lane. Southbound: One left-turn lane. Four through lanes. One right-turn lane. Eastbound: Two left-turn lanes. Three through lanes. One right-turn lane. Westbound: One left-turn lane. Three through lanes. One shared right-turn/through lane.	Prior to occupancy	Engineering Department	Less than significant
Transportation/Traffic	The project will exceed, either individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	*MM Trans 11: Modify the intersection of Euclid Avenue/ Chino Avenue to include the following geometrics: Northbound: Two left-turn lanes. Four through lanes. One share right-turn/through lane. Southbound: One left-turn lane. Four through lanes. One right-turn lane. Eastbound: Two left-turn lanes. Three through lanes. One right-turn lane. Westbound: Two left-turn lanes. One through lane. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant

*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
Transportation/Traffic	The project will exceed, either individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	*MM Trans 12: Modify the intersection of Euclid Avenue/ Schaefer Avenue to include the following geometrics: Northbound: Two left-turn lanes. Four through lanes. One right-turn lane. Southbound: One left-turn lane. Four through lanes. One shared right-turn/ through lane. Eastbound: One left-turn lane. Two through lanes. One right-turn lane. Westbound: One left-turn lane. Two through lanes. One shared right-turn/ through lane.	As determined by the City Engineer	Engineering Department	Less than significant
Transportation/Traffic	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	*MM Trans 13: Modify the intersection of Euclid Avenue/ Edison Avenue to include the following geometrics: Northbound: Two left-turn lanes. Four through lanes. One right-turn lane. Southbound: Two left-turn lanes. Four through lanes. One right-turn lane. Eastbound: One left-turn lane. Three through lanes. Two right-turn lanes. Westbound: Two left-turn lanes. Three through lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant

*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
Transportation/ Traffic	The project will exceed, either individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	*MM Trans 14: Modify the intersection of Euclid Avenue/ Merrill Avenue to include the following geometrics: Northbound: One left-turn lane. Four through lanes. Two right-turn lanes. Southbound: Two left-turn lanes. Four through lanes. Eastbound: N/A Westbound: Two left-turn lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant
Transportation/ Traffic	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	*MM Trans 15: Modify the intersection of Grove Avenue/ Riverside Drive to include the following geometrics: Northbound: One left-turn lane. Three through lanes. One shared right-turn/ through lane. Southbound: One left-turn lane. Three through lanes. One right-turn lane. Eastbound: One left-turn lane. Two through lanes. One shared right-turn/ through lane. Westbound: One left-turn lane. Two through lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant

*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
Transportation/Traffic	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	*MM Trans 16: Add traffic signal and modify the intersection of Grove Avenue/ Chino Avenue to include the following geometrics: Northbound: One left-turn lane. Three through lanes. One right-turn lane. Southbound: One left-turn lane. Three through lanes. One right-turn lane. Eastbound: One left-turn lane. Two through lanes. One right-turn lane. Westbound: One left-turn lane. Two through lanes. One shared right-turn/ through lane.	As determined by the City Engineer	Engineering Department	Less than significant
Transportation/Traffic	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	*MM Trans 17: Add traffic signal and modify the intersection of Grove Avenue/ Edison Avenue to include the following geometrics: Northbound: Two left-turn lanes. Two through lanes. One right-turn lane. Southbound: Two left-turn lanes. Three through lanes. One right-turn lane. Eastbound: Two left-turn lanes. Two through lanes. One right-turn lane. Westbound: Two left-turn lanes. Two through lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant

*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
Transportation/ Traffic	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets	*MM Trans 18: Add traffic signal and modify the intersection of Grove Avenue/ Merrill Avenue to include the following geometrics: Northbound: N/A Southbound: One shared left-turn and right-turn lane. One right-turn lane. Eastbound: One left-turn lane. Two through lanes. Westbound: Two through lanes. One shared right-turn/ through lane.	As determined by the City Engineer	Engineering Department	Less than significant
Transportation/ Traffic	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	*MM Trans 19: Modify the intersection of Vineyard Avenue/ Riverside Drive to include the following geometrics: Northbound: Two left-turn lanes. Three through lanes. One right-turn lane. Southbound: Two left-turn lanes. Three through lanes. One right-turn lane. Eastbound: One left-turn lane. Two through lanes. One right-turn lane. Westbound: One left-turn lane. Two through lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant

*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
Transportation/ Traffic	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	*MM Trans 20: Modify the intersection of Archibald Avenue/ SR-60 WB Ramps to include the following geometrics: Northbound: One left-turn lane. Three through lanes. Southbound: Three through lanes. One right-turn lane. Eastbound: N/A Westbound: One left-turn lane. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant
Transportation/ Traffic	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	*MM Trans 21: Modify the intersection of Archibald Avenue/ SR-60 EB Ramps to include the following geometrics: Northbound: Three through lanes. One right-turn lane. Southbound: One left-turn lane. Three through lanes. Eastbound: One left-turn lane. One right-turn lane. Westbound: N/A	As determined by the City Engineer	Engineering Department	Less than significant

*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
Transportation/ Traffic	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	*MM Trans 22: Modify the intersection of Archibald Avenue/ Riverside Drive to include the following geometrics: Northbound: One left-turn lane. Three through lanes. One shared right-turn/ through lane. Southbound: One left-turn lane. Three through lanes. One right-turn lane. Eastbound: One left-turn lane. Three through lanes. One shared right-turn/ through lane. Westbound: One left-turn lane. Three through lanes. One shared right-turn/ through lane.	As determined by the City Engineer	Engineering Department	Less than significant
Transportation/ Traffic	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	*MM Trans 23: Modify the intersection of Archibald Avenue/ Chino Avenue to include the following geometrics: Northbound: One left-turn lane. Three through lanes. One right-turn lane. Southbound: One left-turn lane. Three through lanes. One right-turn lane. Eastbound: One left-turn lane. Three through lanes. One right-turn lane. Westbound: Two left-turn lanes. Two through lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant

*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
Transportation/ Traffic	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	*MM Trans 24: Add traffic signal and modify the intersection of Archibald Avenue/ Schaefer Avenue to include the following geometrics: Northbound: Two left-turn lanes. Three through lanes. One shared right-turn/ through lane. Southbound: One left-turn lane. Three through lanes. One right-turn lane. Eastbound: Two left-turn lanes. One through lane. Two right-turn lanes. Westbound: One left-turn lane. One through lane. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant
Transportation/ Traffic	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	*MM Trans 25: Modify the intersection of Archibald Avenue/ Edison Avenue to include the following geometrics: Northbound: Two left-turn lanes. Four through lanes. One right-turn lane. Southbound: Two left-turn lanes. Four through lanes. One right-turn lane. Eastbound: Two left-turn lanes. Three through lanes. Two shared right-turn/ through lanes. Westbound: Two left-turn lanes. Three through lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant

*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
Transportation/ Traffic	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	*MM Trans 26: Add traffic signal and modify the intersection of Archibald Avenue/ Merrill Avenue to include the following geometrics: Northbound: Two left-turn lanes. Four through lanes. One right-turn lane. Southbound: Two left-turn lanes. Four through lanes. One right-turn lane. Eastbound: Two left-turn lanes. Three through lanes. One right-turn lane. Westbound: Two left-turn lanes. Three through lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant
Transportation/ Traffic	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	*MM Trans 27: Modify the intersection of Archibald Avenue/ Cloverdale Road to include the following geometrics: Northbound: Four through lanes. One right-turn lane. Southbound: Two left-turn lanes. Four through lanes. Eastbound: N/A Westbound: Two left-turn lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant

*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
Transportation/Traffic	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	*MM Trans 28: Modify the intersection of Haven Avenue/ Riverside Drive to include the following geometrics: Northbound: One left-turn lane. Two through lanes. Two right-turn lanes. Southbound: One left-turn lane. Two through lanes. One right-turn lane. Eastbound: One left-turn lane. Three through lanes. One right-turn lane. Westbound: One left-turn lane. Two through lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant
Transportation/Traffic	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	*MM Trans 29: Add traffic signal and modify the intersection of Haven Avenue/ Edison Avenue to include the following geometrics: Northbound: One left-turn lane. Two through lanes. One shared right-turn/ through lane. Southbound: One left-turn lane. Two through lanes. One right-turn lane. Eastbound: Two left-turn lanes. One through lane. One shared right-turn/ through lane. Westbound: One left-turn lane. One through lane. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant

*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
Transportation/Traffic	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	*MM Trans 30: Add traffic signal and modify the intersection of Hamner Avenue/ Eucalyptus Avenue to include the following geometrics: Northbound: Two left-turn lanes. Three through lanes. Southbound: Three through lanes. Two right-turn lanes. Eastbound: Two left-turn lanes. One right-turn lane. Westbound: N/A	As determined by the City Engineer	Engineering Department	Less than significant
Transportation/Traffic	The project will exceed, individually or cumulatively, the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.	*MM Trans 31: Modify the intersection of Hamner Avenue/ Bellegrave Avenue to include the following geometrics: Northbound: One left-turn lane. Two through lanes. One right-turn lane. Southbound: Two left-turn lanes. Three through lanes. One right-turn lane. Eastbound: One left-turn lane. Two through lanes. One right-turn lane. Westbound: Two left-turn lanes. Three through lanes. One right-turn lane.	As determined by the City Engineer	Engineering Department	Less than significant

*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
<p>Transportation/Traffic</p>	<p>Due to the unknown construction timing of area-wide improvements, the project will temporarily exceed the level of service standard established by the county congestion management agency for designated roads or highways – LOS D or better for intersections during peak hours for collector and arterial roadways and LOS C or better for residential streets.</p>	<p>See MM Trans 7 through 31</p>	<p>Unknown, as determined by the City Engineer</p>	<p>Engineering Department</p>	<p>Temporary significant cumulative impacts</p>

*The Applicant shall pay their proportionate share (prior to building permit issuance) for or install (prior to occupancy of any structure) the above transportation improvements needed to serve the project. The determination of whether the payment of proportionate share or installation of the improvements is required shall be made by the City Engineer at the time of Tentative Tract Map approval. The method for determining proportionate share is identified in the Traffic Impact Analysis.

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
Utilities	Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects.	MM Util 1: All water and sewer pipelines within and adjacent to the project boundaries shall be constructed and/or funded for construction on a fair-share basis based on the NMC Infrastructure Master Plans and/or the interim sewer plan herein, and to the satisfaction of the City.	Prior to occupancy	Engineering Department	Less than Significant
Utilities	Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects.	MM Util 2: The Archibald trunk sewer line off-site connection to the IEUA Kimbal Avenue interceptor shall be complete and operational prior to issuance of first certificate of occupancy for development located east of the Cucamonga Creek Channel. The applicant shall participate on a fair share basis in the development of the necessary sewer facilities.	Prior to issuance of building permits for the development located east of Cucamonga Creek	Engineering Department and IEUA	Less than Significant
Utilities	Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects.	MM Util 3: The planning areas located west of Cucamonga Creek Channel shall have sewer lines in place to connect, via master planned lines with the western area trunk sewer system in Euclid Avenue, or via the Carpenter Street interim connection to the eastern area trunk sewer system. The interim condition that may exist for the Specific Plan where the wastewater generated may be tied into the Eastern Trunk System (ETS) will be via a 36-inch line located in Vineyard Avenue, north of Merrill Avenue, a 15-inch line in Merrill Avenue from Vineyard Avenue to Carpenter Avenue, and a 15-inch line in Carpenter Avenue from Merrill Avenue to the ETS. This interim connection shall be constructed per the approved sewer master plan. Thus, should the approved master plan require larger size pipelines, the developer will be required to construct them. Installation of one of these connections shall be in place and operable prior to issuance of building permits	Prior to issuance of building permits for the development located west of Cucamonga Creek	Engineering Department and IEUA	Less than Significant

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
		for the development located west of Cucamonga Creek to the satisfaction of the City and IEUA.			
Utilities	Result in adverse impacts to natural gas or other dry utility systems.	MM Util 4: Off-site water lines, tanks, interconnectors and other facilities required in the Water Master Plan to provide water to the site shall be in place and operational prior to issuance of the first certificate of occupancy. The applicant shall participate on a fair share basis in the development of these off-site facilities.	Prior to first certificate of occupancy	Engineering Department	Less than Significant
Utilities	Result in adverse impacts to natural gas or other dry utility systems.	MM Util 5: Prior to obtaining grading permit(s), the project proponent shall coordinate with the applicable natural gas, electrical, and telephone utility providers for the project site to ensure that all existing underground and overhead lines are not damaged during project construction.	Prior to grading permits	Engineering Department	Less than significant
Utilities	Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects.	MM Util 6: To reduce the quantity of energy used and to conserve water resources, the project developer and City of Ontario should work to include sustainable systems for use of water and energy within the project design. One source of assistance in this regard is Southern California Gas Company Commercial/Industrial Support Center at 1-800-GAS-2000, which should be contacted at the time of development of the commercial center located within the project.	Ongoing	Engineering Department	Less than significant
Utilities	Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects.	MM Util 7: The project applicant shall plan and construct a dual pipe system to supply reclaimed water when available in the future (GP Policy 5.1.4). An Engineer's Report approved by the City and the Department of Health Services is required prior to the use of recycled water.	Prior to occupancy	Engineering Department	Less than significant
Utilities	Disruption of adequate temporary water supply.	MM Util 8: All existing agricultural wells on the project site will be destroyed and abandoned per the California Department of Health Services guidelines. A	Prior to demolition permit.	Engineering Department	Less than significant

Impact Category	Impact	Mitigation Measure	Implementation Timing	Responsible Party	Project-Specific Impact After Mitigation
		well use/destruction plan and schedule for all existing agricultural wells on the project site shall be prepared and submitted for approval, prior to the issuance of grading permits. This plan shall also include a temporary water supply plan, as applicable, in order to avoid potential significant temporary impacts resulting from the disruption of current water supply through the abandonment of on-site wells, the developer of any parcel located within the Specific Plan which contains a well that services one or more adjacent parcels that are not proposed to be developed in the current phase, shall provide the City Engineer with a temporary water supply plan for approval. Construction of any temporary pipes or facilities needed to provide water to the existing uses which are to temporarily remain shall be installed per City requirements at the developer's expense.			
Utilities	Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects.	MM Util 9: Prior to approval of the Specific Plan and EIR, a hydraulic analysis of the area served by the interim sewer main to be located in Carpenter Avenue shall be submitted to the City Engineer.	Prior to approval of the Specific Plan and EIR	Engineering Department	Less than significant
Utilities	Be served by a landfill with insufficient permitted capacity to accommodate the project's solid waste disposal needs.	No feasible mitigation measures exist that would eliminate or substantially lessen the cumulative impacts to solid waste facilities.	Not applicable	Not applicable	Significant cumulative impacts

RESOLUTION NO. _____

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF ONTARIO, CALIFORNIA, APPROVAL OF FILE NO. PSPA19-007, AN AMENDMENT TO THE PARKSIDE SPECIFIC PLAN TO: [1] RECONFIGURE THE RESIDENTIAL PLANNING AREAS 1 THROUGH 4, AND 17 THROUGH 19; [2] RECONFIGURE THE GREAT PARK PLANNING AREA 22 (EAST OF THE CUCAMONGA CREEK CHANNEL); [3] REVISE INTERNAL CIRCULATION TO IMPROVE ACCESS INTO THE NEIGHBORHOOD COMMERCIAL PLANNING AREA 19; [4] UPDATE AND REVISE RESIDENTIAL DESIGN GUIDELINES (SECTIONS 7.1 THROUGH 7.6) TO INTRODUCE NEW HOUSING TYPES AND ARCHITECTURAL STYLES; AND [5] UPDATE AND REVISE LANDSCAPE STANDARDS (SECTION 7.7). THE PROJECT SITE IS BOUNDED BY ONTARIO RANCH ROAD TO THE NORTH, EUCALYPTUS AVENUE TO THE SOUTH, ARCHIBALD AVENUE TO THE EAST AND CARPENTER AVENUE TO THE WEST, AND MAKING FINDINGS IN SUPPORT THEREOF—APNS: 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, AND 0218-221-10.

WHEREAS, SC Ontario Development Company, LLC (hereinafter referred to as "Applicant") has filed an Application for the approval of an Amendment to the Parkside Specific Plan, File No. PSPA19-007, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 250.89 gross acres of land bounded by Ontario Ranch Road to the north, Eucalyptus Avenue to the south, Archibald Avenue to the east and Carpenter Avenue to the west, within the Parkside Specific Plan, and is presently improved with general crop-related agricultural purposes (alfalfa, corn, milo, sorghum, etc.) and flood control; and

WHEREAS, the properties to the north of the Project site are within the Low-Density Residential zoning district of the Avenue Specific Plan and are vacant with some agriculture/dairy uses. The properties to the east are within the High Density Residential/Great Park zoning district of the Grand Park Specific Plan and are vacant with some agriculture/dairy uses. The properties to the south are within the Business Park zoning district of the West Ontario Commerce Specific Plan and developed with industrial uses and within the Low-Density Residential zoning district of the Subarea 29 Specific Plan and vacant. The properties to the west are within the Agriculture Overlay/ Specific Plan zoning district, and are vacant with some agriculture/dairy uses; and

WHEREAS, on September 4, 2019; SC Ontario Development Company LLC, submitted an Amendment to the Parkside Specific Plan to: 1] Reconfigure residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning

Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 21; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7); and

WHEREAS, the Parkside Specific Plan Land Use Plan Exhibit 1-3, the Land Use Summary Table 1.1, Vehicular Circulation Master Plan Exhibit 5.1, Residential Design Guidelines Sections 7.1 – 7.6, and Landscape Standards Section 7.7 have been updated to reflect the Amendment to Parkside Specific Plan. In addition, the Amendment includes minor updates to the development standards, exhibits and text changes. All changes and additions to the Specific Plan (exhibits, tables, development standards and design guidelines) are contained within the revised Parkside Specific Plan document accompanying this Resolution (Attachment "A"). All additions to the Specific Plan have been highlighted in blue; and

WHEREAS, revisions were made to the Specific Plan for compliance with the Development Code and TOP Compliance; and

WHEREAS, the Parkside Specific Plan Environmental Impact Report (State Clearinghouse No. 2004011008) was certified on September 5, 2006 (hereinafter referred to as "Certified EIR"), in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as "EIR Addendum") in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as "CEQA"); and

WHEREAS, the environmental impacts of this project were thoroughly analyzed in the EIR Addendum, which concluded that implementation of the Project could result in a number of significant effects on the environment that were previously analyzed in the Certified EIR, and that the Certified EIR identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an EIR Addendum has been prepared to determine possible environmental impacts; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the City Council the responsibility and authority to review and approve the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 26, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting (6 to 0) to issue Resolution No. PC20-028, recommending the City Council approve the Application; and

WHEREAS, as the first action on the Project, on June 16, 2020, the City Council issued a Resolution to adopt the EIR Addendum, finding that the proposed Project introduces no new significant environmental impacts and applying all previously adopted mitigation measures to the Project, which were incorporated by reference; and

WHEREAS, on June 16, 2020, the City Council of the City of Ontario conducted a hearing to consider the Initial Study/Addendum and the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the City Council of the City of Ontario, as follows:

SECTION 1. ***Housing Element Compliance.*** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the City Council finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy

Plan (General Plan) component of The Ontario Plan. The project site is one of the properties listed in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix, and the proposed project is consistent with the 1,947 number of dwelling Specified in the Available Land Inventory. Per the Available Land Inventory, the Parkside Specific Plan is required to provide 1,947 dwelling units with an overall density range of between 9 and 21 DU/AC.

SECTION 2. *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan (“ALUCP”), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

SECTION 3. *Concluding Facts and Reasons.* Based upon the substantial evidence presented to the City Council during the above-referenced hearing, and upon the specific findings set forth in Sections 1 and 2, above, the City Council hereby concludes as follows:

(1) ***The proposed Specific Plan, or amendment thereto, is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Amendment to the Parkside Specific is consistent with Vision, Policy Plan (General Plan), and City Council Priorities components of TOP. Section 9.0, General Plan Consistency, of the Parkside Specific Plan describes the manner in which Parkside Specific Plan complies with the Policy Plan goals and policies.

(2) ***The proposed Specific Plan, or amendment thereto, would not be detrimental to the public interest, health, safety, convenience, or general welfare of the City.*** The TOP EIR and Parkside Specific Plan EIR include safeguards and imposed certain mitigation measures to ensure that development within the Parkside Specific Plan boundary area would not be detrimental to public interest, health, safety, or general welfare of City.

(3) ***In the case of an application affecting specific property(ies), the proposed Specific Plan, or amendment thereto, will not adversely affect the harmonious relationship with adjacent properties and land uses.*** The project site is in an area that is characterized by agriculture uses and residential development and is

surrounded by urban land uses. Any development proposals that would subsequently occur from the proposed Project will be required to be in accordance with the policies of the Community Design Element of the Policy Plan (General Plan) and development regulations and design guidelines of the Parkside Specific Plan and therefore, will not adversely affect the harmonious relationship with adjacent properties and land uses.

(4) ***In the case of an application affecting specific property(ies), the subject site is physically suitable, including, but not limited to, parcel size, shape, access, and availability of utilities, for the request and anticipated development.*** The Amendment the Parkside Specific Plan will maintain the appropriate balance of land uses within the City consistent with TOP Policy Plan. In addition, development within the Parkside Specific Plan will be required to construct the necessary infrastructure and public services that will support future residential and commercial/office uses.

SECTION 4. City Council Action. Based upon the findings and conclusions set forth in Sections 1 through 3, above, the City Council hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment "B," and incorporated herein by this reference.

SECTION 5. Indemnification. The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 6. Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 7. Certification to Adoption. The City Clerk shall certify to the adoption of the Resolution.

PASSED, APPROVED, AND ADOPTED this 16th day of June 2020.

PAUL S. LEON, MAYOR

ATTEST:

SHEILA MAUTZ, CITY CLERK

APPROVED AS TO FORM:

COLE HUBER LLP
CITY ATTORNEY

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, SHEILA MAUTZ, City Clerk of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. 2020- was duly passed and adopted by the City Council of the City of Ontario at their regular meeting held June 16, 2020, by the following roll call vote, to wit:

AYES: COUNCIL MEMBERS:

NOES: COUNCIL MEMBERS:

ABSENT: COUNCIL MEMBERS:

SHEILA MAUTZ, CITY CLERK

(SEAL)

The foregoing is the original of Resolution No. 2020- duly passed and adopted by the Ontario City Council at their regular meeting held June 16, 2020.

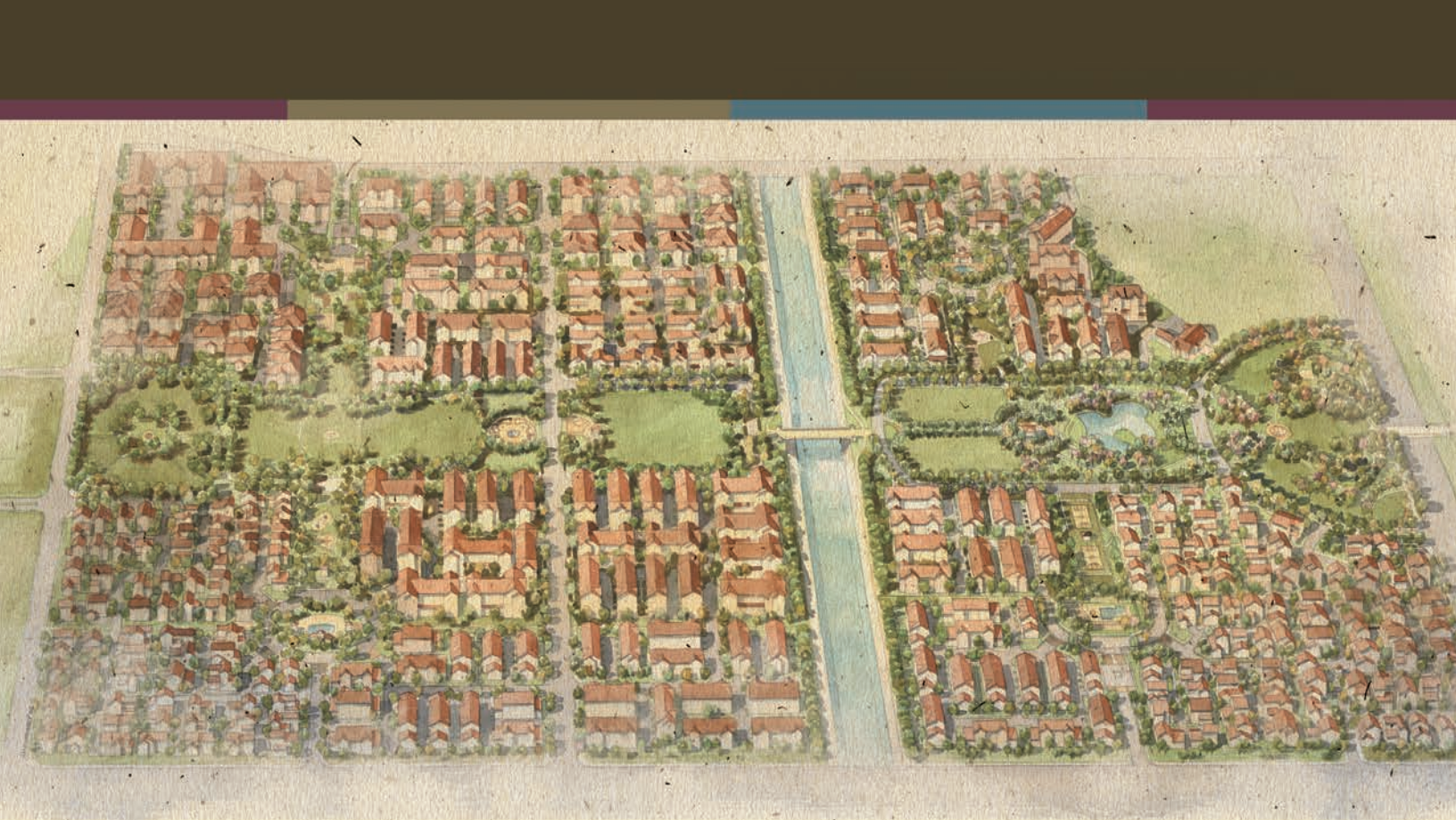
SHEILA MAUTZ, CITY CLERK

(SEAL)

ATTACHMENT A:

**File No. PSPA19-007
Parkside Specific Plan**

(Document to follow this page)



Parkside

Specific Plan

Adopted August 2006
May 2020 Amendment Draft

2001368



Parkside

SPECIFIC PLAN

Portions of Subareas 22 and 23
Parkside at New Model Colony
PSP03-002

Approved by City of Ontario City Council on 08/15/2006
Amended XX 2020

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Parkside

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Appendix: Legal Description

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Chapter 1: Specific Plan Overview



New York City, New York: Park Row and City Hall Park



Parkside

1.0 Specific Plan Overview

Parkside is ~~proposed as a new~~ comprised of approximately 250.899 gross acre ~~planned community located in the south/central portion of the New Model Colony (NMC) acres.~~ The Specific Plan includes the western portion of Subarea 23, between Cucamonga Creek and Archibald Ave., and the eastern portion of Subarea 22, between Cucamonga Creek and future Carpenter Ave. The entire Specific Plan area amounts to 237.899 acres in ~~26~~ 25 planning areas (~~exclusive~~ of Cucamonga Creek). Net project acreage amounts to 203.2 acres. Refer to Table 1-1: ~~Parkside~~ Land Use Summary.

The regional context and local setting of the Specific Plan area are illustrated in ~~the Exhibit 1-1: Regional Location Map Exhibit 1~~ and ~~Exhibit 1-2: Vicinity Map Exhibit 2:~~

The ~~project~~ Specific Plan is consistent with and implements the ~~gross residential density~~ Medium Density Residential allocation (11.1 to 25 dwelling units permitted per gross acre) and broad “community design” policy requirements for the ~~New Model Colony Ontario Ranch~~, including a general neo-traditional community design. These include:

- Integration with and connectivity to the Great Park;
- Creation of a “sense of place;”
- Diversity in housing product and architecture;
- Pedestrian connectivity;
- Smaller “neighborhoods;” and
- Elimination of the “subdivision” feel.

~~The New Model Colony General Plan Amendment adopted by the City of Ontario designates the subject~~

~~portions of Planning Subareas 22 and 23 for development of low and high density residential and park land uses. Residential land uses are delineated with various areas assigned for development with a total of 1,947 residential units permitted. As part of the project, a General Plan Amendment will be adopted concurrently with the Parkside Specific Plan, modifying only the General Plan Land Use Policy Map to 1) reassign the locations of the high density residential uses, eliminate low density designations, and create new medium density designations; 2) add a Neighborhood Center commercial designation at the intersection of Archibald Ave. and Edison Ave., and 3) add a community facility designation (fire station), west of the Neighborhood Center.~~

On January 26th, 2010, the City adopted The Ontario Plan (TOP) which serves as the City’s new business plan and includes a long-term Vision and a principle based Policy Plan (General Plan). The City’s Policy Plan, which acts as the City’s General Plan, designates (Policy Plan Exhibit LU-1- Land Use Plan) the project site for development of Medium Density Residential (11.1 to 25 dwelling units per acre), Neighborhood Commercial, Public Facility, and Open Space-Parkland.

Consistent with the adopted General Plan, Parkside is allowed a maximum of 1,947 residential units, Neighborhood Commercial area, a fire station and a Chino Desalter Authority (CDA) production well site; see Table 1-1: Parkside Land Use Summary.

Parkside

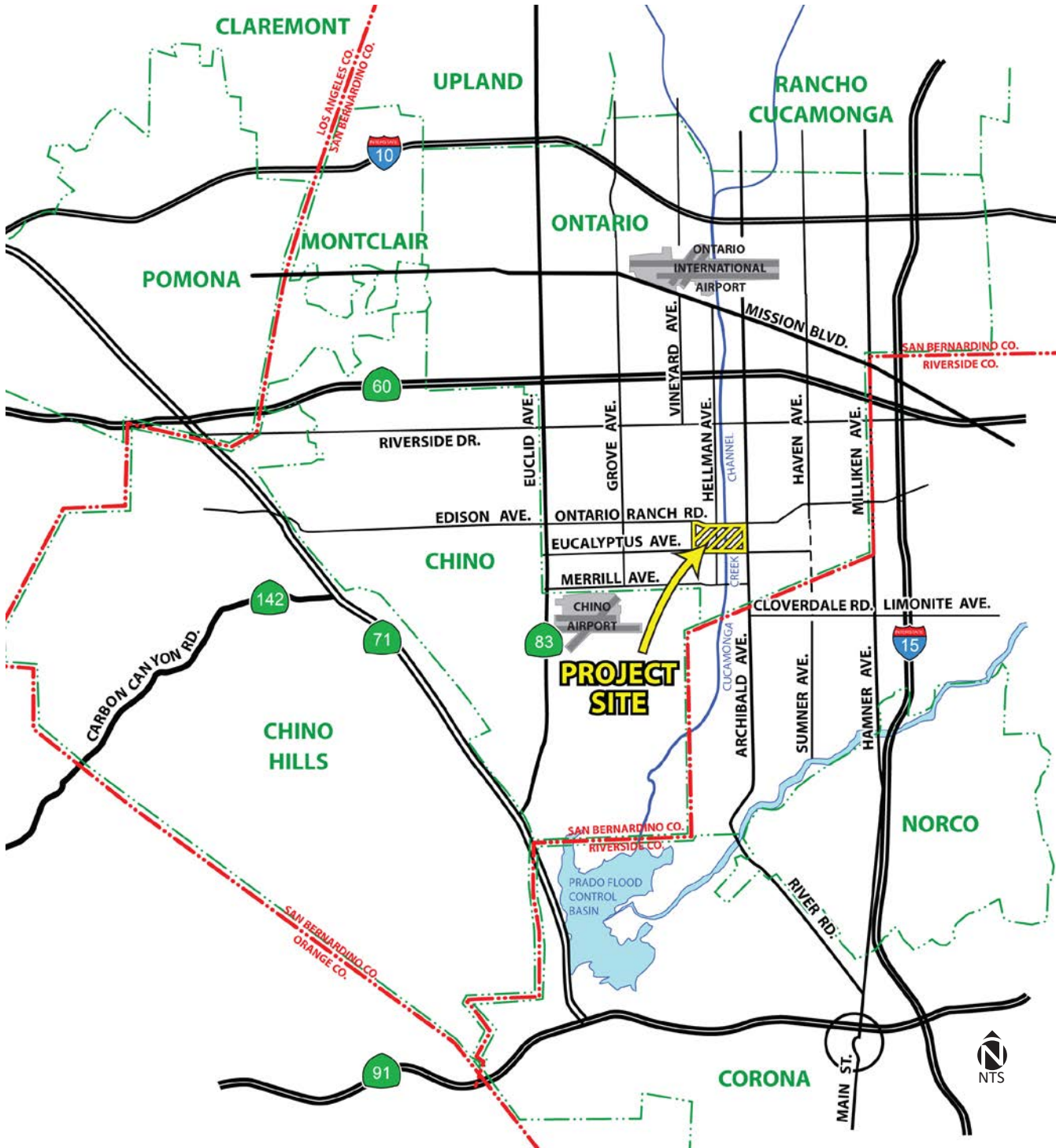


Exhibit 1-1: Regional Location Map



Parkside

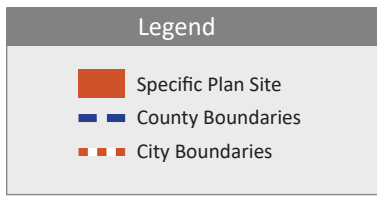
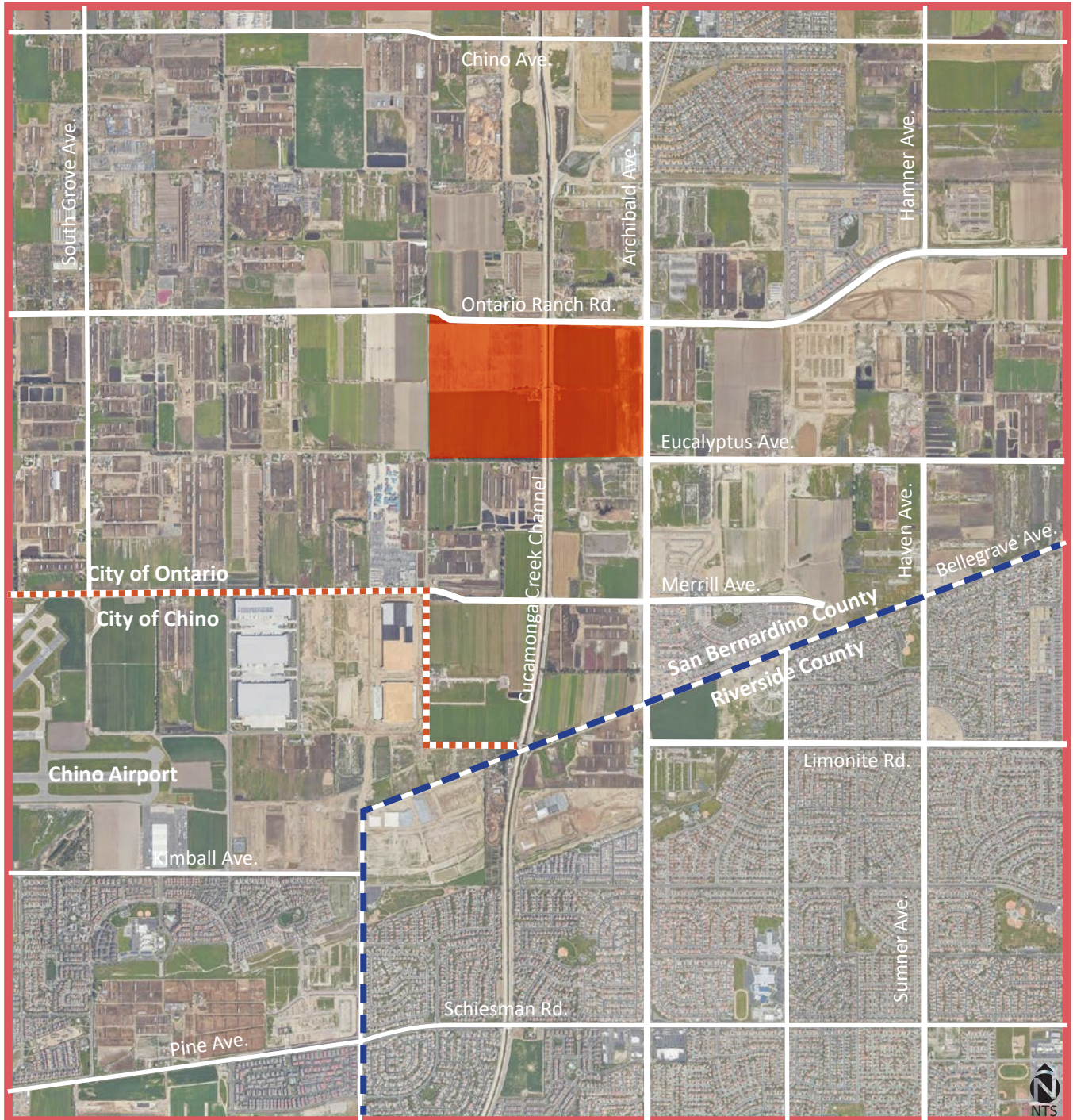


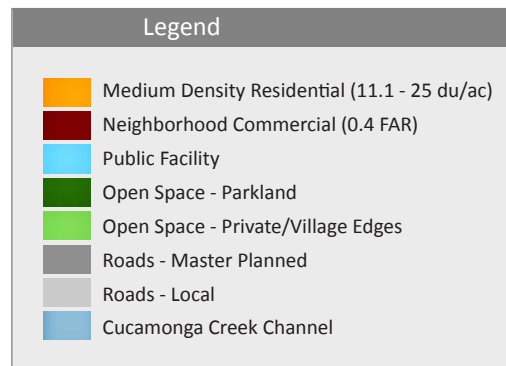
Exhibit 1-2: Vicinity Map



Parkside



Exhibit 1-3: Land Use Plan





Parkside

A variety of residential neighborhoods are planned within Parkside, with varying product design and architectural character. Residential products envisioned for Parkside include, but are not limited to, [conventional/compact single family detached homes](#), green/motorcourt clusters, triplexes, townhomes, condominiums, and apartments. A maximum total of 1,947 dwelling units are proposed for Parkside.

It should be noted that use of the term “Parkside” throughout this document applies to the entire Specific Plan area, held under a single ownership, by SC Ontario Development [Corporation Company, LLC](#) (“SC Ontario”) except for the Cucamonga Creek Channel.

The Parkside Specific Plan area occupies a strategic location [in the overall New Model Colony General Plan Land Use Plan](#), providing a crucial link between major active recreation uses planned east of Archibald Ave., and the future Town Center planned to the west of Parkside.

Table 1-1: Parkside Land Use Summary

Land Use	Lot Size / Use	Gross Acreage	Net Acreage	*Density Range / S.F.	Lots / D.U.
Residential					
PA-1 (Green Court/Lane Loaded Products 1, 2 & 4)	SFD	19.97	15.57	10-18	236
PA-2 (Lane Loaded Product 3)	SFD	3.57	2.66	10-18	41
PA-3 (Lane Loaded Product 3)	SFD	3.06	2.38	10-18	42
PA-4 (Green Court/Lane Loaded Products 1, 2, 3 & 4)	SFD	15.50	13.62	10-18	221
PA-5 (HD Townhomes)	MF	9.57	8.14	15-22	139
PA-6 (Attached Auto Court)	MF	7.34	6.89	8-14	75
PA-7 (Auto Court 6-8 Plex)	SFD	9.30	7.49	8-14	68
PA-8 (Green Court 8-10 Plex)	SFD	6.64	6.18	10-16	68
PA-9 (Triplex)	MF	10.49	8.70	10-16	120
PA-10 (Row Townhomes)	MF	7.56	7.22	17-25	143
PA-11 (HD Townhomes)	MF	8.11	7.22	15-22	123
PA-12 (Triplex)	MF	11.80	9.30	12-18	127
PA-13 (Attached Auto Court)	MF	8.53	6.34	8-14	64
PA-14 (Duplex)	MF	7.12	6.18	12-18	99
PA-15 (Duplex)	MF	8.62	6.97	12-18	111
PA-16 (Triplex)	MF	7.83	7.35	10-16	90
PA-17 (Row Townhomes)	MF	4.87	4.54	17-25	82
PA-18 (HD Townhomes)	MF	6.74	5.04	15-22	98
Residential Subtotal		156.62	131.79	14.77	1,947
Non-Residential					
PA-19 (Commercial)		15.66	11.15	115,000 SF	-
PA-20 (Fire Station)		1.72	1.39	-	-
PA-21 (Great Park)		58.86	54.40	-	-
		1.00	1.00	-	-



Parkside

PA-23 (Private Rec)	1.90	1.34	-	-
PA-24 (Private Rec)	1.00	1.00	-	-
PA-25 (Private Rec)	1.00	1.00	-	-
PA-26 (CD Well Site)	0.13	0.13	-	-
Non-Residential Subtotal	81.27	71.41	115,000 SF	-
Channel ROW	13.00	13.00	-	-
Total Project	250.89	216.20		1,947

NOTES:

- 1) Gross acres taken to center line of streets.
- 2) Adjusted gross acres taken to street ROW dedicated to the City
- 3) Net acres taken to street right of way
- 4) A total of 34.69 acres of master planned roadway is included in the project
- * Density range is shown is based on net acres. The General Plan is based on adjusted gross acres
- 5) Proposed product types in each Planning Area are subject to change based on final development submittal.

Homes will frame the Great Park community recreation area, defining public space, activities, and visually establishing a community “sense of place.”

Non-residential uses proposed in the Specific Plan include a ~~15.49-gross-acre~~ commercial ~~parcel~~ lot on the southwest corner of Ontario Ranch Rd. and Archibald Ave. A public fire station ~~is included~~ directly west of the commercial site ~~and that~~ fronts on the Great Park, ~~and a CDA well site.~~

The Great Park ~~consisting of 58.47 gross acres,~~ extends east to west through the center of the Specific Plan area ~~with segments extending north and south into the residential planning areas,~~ and provides a unique community sense of place. All of the Parkside community is within a 1/4 mile walk of the Great Park, establishing Parkside as a walkable community.

A system of pedestrian trails ~~and sidewalks links~~ all residential neighborhoods with the planned park and internal private recreation amenities. Additionally, connections to the ~~paseos along~~ Cucamonga Creek Channel ~~trail~~ are planned.

The internal street system includes a modified grid and landscaped parkways in all local residential streets. Refer to Exhibit 1-3: Land Use Plan.

1.0.1 Purpose for Amendment

This Specific Plan Amendment (2019) modifies the land use plan to improve the interface between the residential neighborhoods and the Great Park, provide better internal circulation and access to the private recreation centers, incorporate a CDA production well site, introduce new home types to support the goals of the Specific Plan, add additional architectural styles and reallocate density between planning areas. The range of home types provides for a varied and dynamic neighborhood composition serving a wide range of family and income types. New home types with development standards have been introduced to increase the home type variation in the neighborhoods and increase opportunities for homes with architecture-front designs. The new home types include high density detached homes, cluster configurations, condominiums and apartments.



Parkside

The modifications to the Specific Plan have been incorporated into the body of the text, standards, and exhibits of this document. Revisions to the Design Guidelines, ownership name change and minor text revisions have also been completed. The amendment shall not be referenced for the remainder of this document as the standards and guidelines of the Specific Plan are designed to create a cohesive community.

1.0.2 Governing Documents

Development of Parkside will be regulated by the following documents:

- The [City of Ontario Sphere of Influence Plan \(TOP\) Policy Plan](#) (General Plan [Amendment \(January 1998\)](#); [as amended](#)) which establishes policies governing land use, circulation, housing, conservation and open space, noise, safety, and public facilities within the [Parkside](#) Specific Plan area;
- The Parkside Specific Plan, [to include](#) a Land Use Plan, Infrastructure Plan, Design Guidelines, and Development Regulations;
- The [City of Ontario](#) Development Code, as applicable to the project [site](#), where the [Parkside](#) Specific Plan is silent on development standards and regulations;
- The [Airport Land Use Compatibility Plans](#) of [Chino Airport](#) and [Ontario Subdivision Ordinance, regulating the subdivision of land within the International Airport](#);
- [The approved](#) Parkside [project area](#) [Environmental Impact Report](#); and

- Covenants, Conditions, and Restrictions (CC&R's) to be established by the [developer of Parkside](#), and [other](#) developers of Parkside as a means of ensuring and enforcing quality design and development of the master planned community.

1.0.3 Specific Plan Components

The Parkside Specific Plan is organized into the following sections in addition to Section 1, Specific Plan Overview.

Section 2: Introduction

The Introduction provides:

- The project setting;
- A general description of the project proposal;
- The goals and policies of the Specific Plan;
- The entitlements to accompany the Specific Plan;
- The relationship of the Specific Plan to [The Ontario New Model Colony Plan \(TOP\) Policy Plan](#) (General Plan [Amendment, as amended](#)) and the Ontario Development Code.

Section 3: Existing Conditions

This section describes the setting for the project, outlining the existing physical conditions on and around the site [at the time of the original approval](#).

Section 4: Land Use

The Land Use section describes the various residential planning areas and residential types, commercial planning area, public use planning area, and the parks and [multi-purpose](#) trails within the planned community.



Parkside

Section 5: Infrastructure and Public Improvements

This section provides information on proposed circulation improvements, the planned backbone water, sewer, and storm drain systems, the grading concept for the project [site](#), and a discussion of public utilities and services to serve the Specific Plan area.

Section 6: Development Regulations

The Development Regulations identify permitted uses and standards regulating the development of various residential product types, commercial use, fire station, [well site](#) and parks. The relationship of the Parkside Specific Plan development regulations to the Ontario Development [Ordinance Code](#) is also provided. The policies and procedures for the City's review and approval of future development proposals within the Specific Plan are included in this section. This section provides the methods and procedures for interpreting and amending the Parkside Specific Plan, as necessary [in the future](#).

Section 7: Design Guidelines

The Specific Plan Design Guidelines are intended to direct the site planning, landscaping, and architectural quality of the development. Streetscapes, entries, edge treatments, walls and fencing, lighting, signage, and architectural design are addressed in the Design Guidelines.

Section 8: Commercial Design Guidelines

This section will direct site planning, architectural design, and landscaping for the commercial [parcel lot](#).

Section 9: General Plan Consistency

This section includes the [City of Ontario](#) General Plan consistency matrix describing the relationship of the Specific Plan to each policy of [the New Model Colony The Ontario Plan \(TOP\) Policy Plan](#) (General Plan [Amendment](#)).

Chapter 2: Introduction



New York City, New York: Central Park



Parkside

2.0 Introduction

The Specific Plan is comprised of 250.899 gross acres included in Planning Subareas 22 and 23 of the **New Model Colony Ontario Ranch** within the City of Ontario. The Specific Plan portions of Planning Subareas 22 and 23 are comprised of 133.76 gross acres and 104.13 gross acres respectively, exclusive of 13 acres included in the Cucamonga Creek Channel. The project site illustrated in Exhibit 1-2: Vicinity Map is bounded by **Edison Ave. Ontario Ranch Rd.** on the north, Archibald Ave. on the east, **Merrill Eucalyptus Ave.** on the south, and Carpenter Street on the west. The Parkside Specific Plan is a comprehensive plan proposed by SC Ontario Development **Corp Company, LLC** for the development of residential neighborhoods designed to reflect the small town character of **older** established communities. The Specific Plan also includes a commercial site, parks (public and private), **and** a fire station, **and a well site.**

2.1 Purpose and Objectives

2.1.1 Purpose

~~The Specific Plan comprehensively describes residential, recreational, commercial, and public (fire station) land uses planned for the site. The resolution adopting the Specific Plan establishes the zoning for the site and defines the development regulations, requirements, and design guidelines governing development of the project, as well as the procedures and requirements to approve development within the Specific Plan, to ensure that the City of Ontario's Sphere of Influence General Plan Amendment, as amended, is implemented.~~

The Parkside Specific Plan serves to implement the City's Policy Plan (General Plan) for the project site (refer to Exhibit 2-1: Policy Plan Land Use Plan and provides

zoning regulations for development of the project site by establishing permitted land use, development standards, infrastructure requirements, and implementation requirements for development. A comprehensive set of design guidelines and development regulations are included to guide and regulate site planning, landscape, and architectural character within the community ensuring that excellence in community design is achieved during project development. The Parkside Specific Plan establishes the procedures and requirements to approve new development within the project site.

The Specific Plan is designed to address the following guiding planning principles:

- Diversity and choice of single-family and multi-family housing types and opportunities to address a variety of lifestyles, home sizes, and economic segments of the marketplace;
- Connectivity among land uses within the Specific Plan area and to surrounding public facilities and the existing Ontario community, including the Great Park; [Refer to Exhibit 2-2: Policy Plan Functional Roadway Classification Plan,](#)
- Use of traditional development patterns found in older established neighborhoods in Southern California;
- Recreational amenities within walking distance of all residential neighborhoods;
- Bicycle and pedestrian accessibility and mobility to encourage alternative modes of travel, within the project site, and along the Cucamonga Channel Regional Trail;
- Diversity in architectural design; and



Parkside

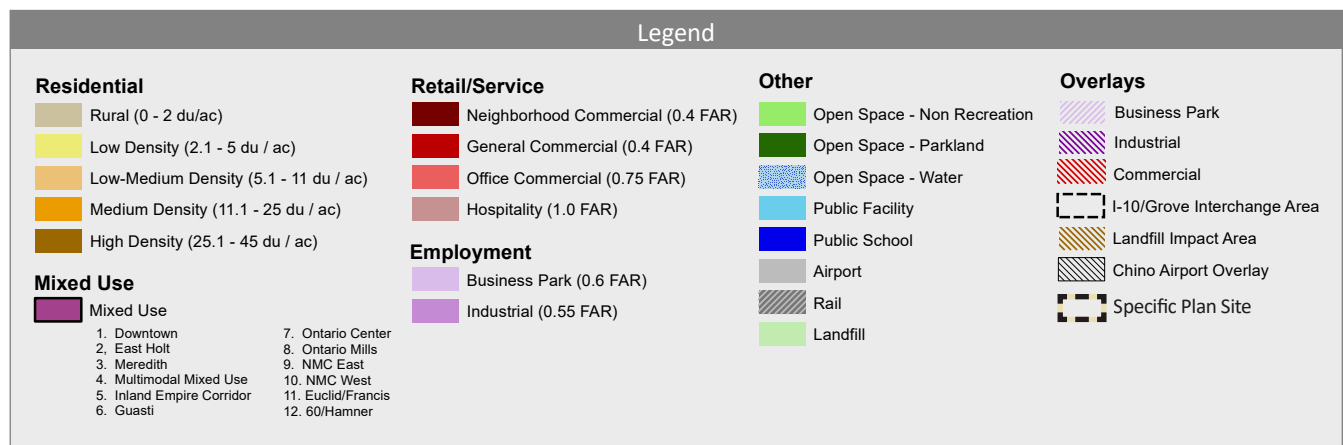
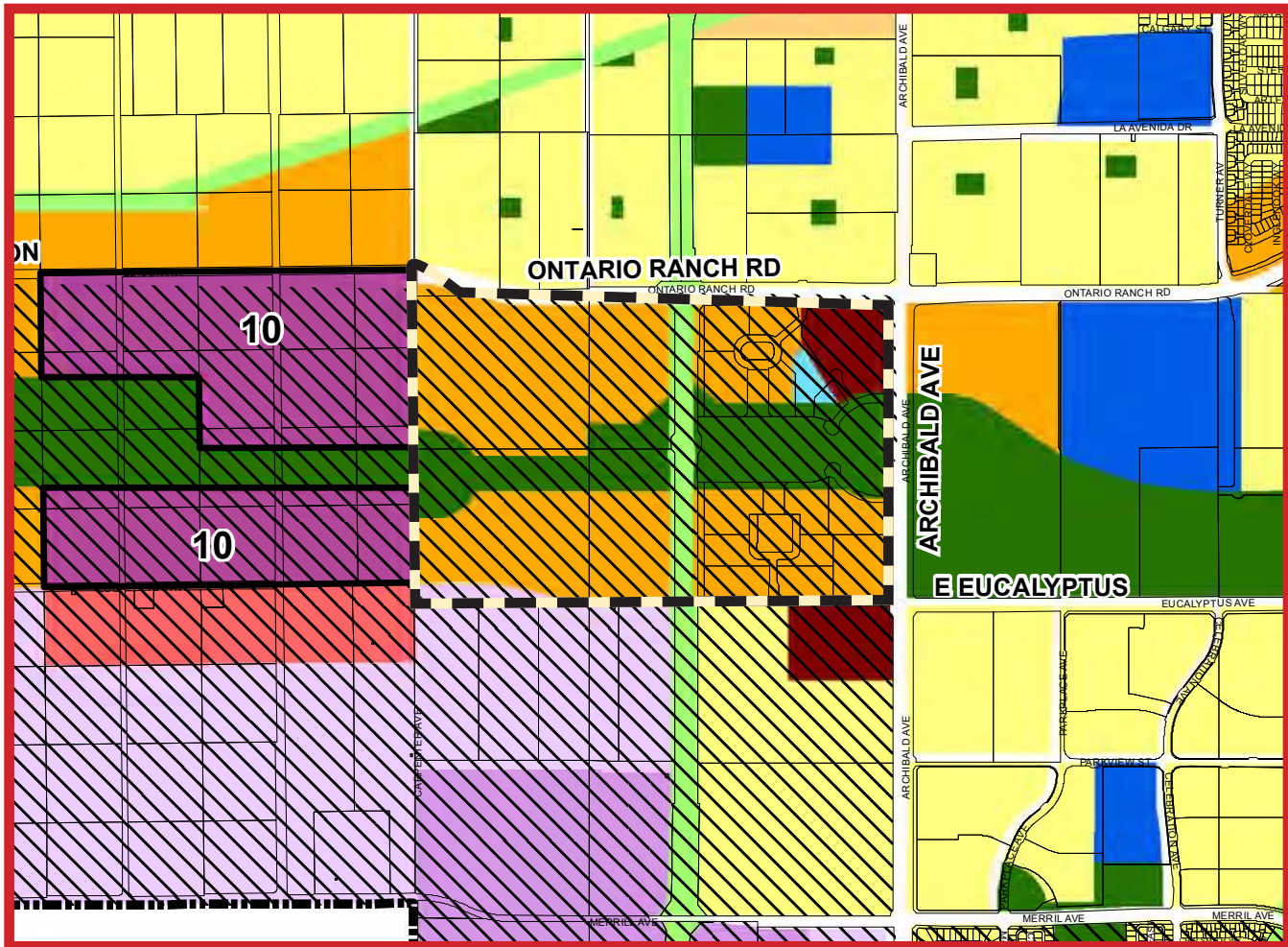


Exhibit 2-1: Policy Plan Land Use Plan



Parkside

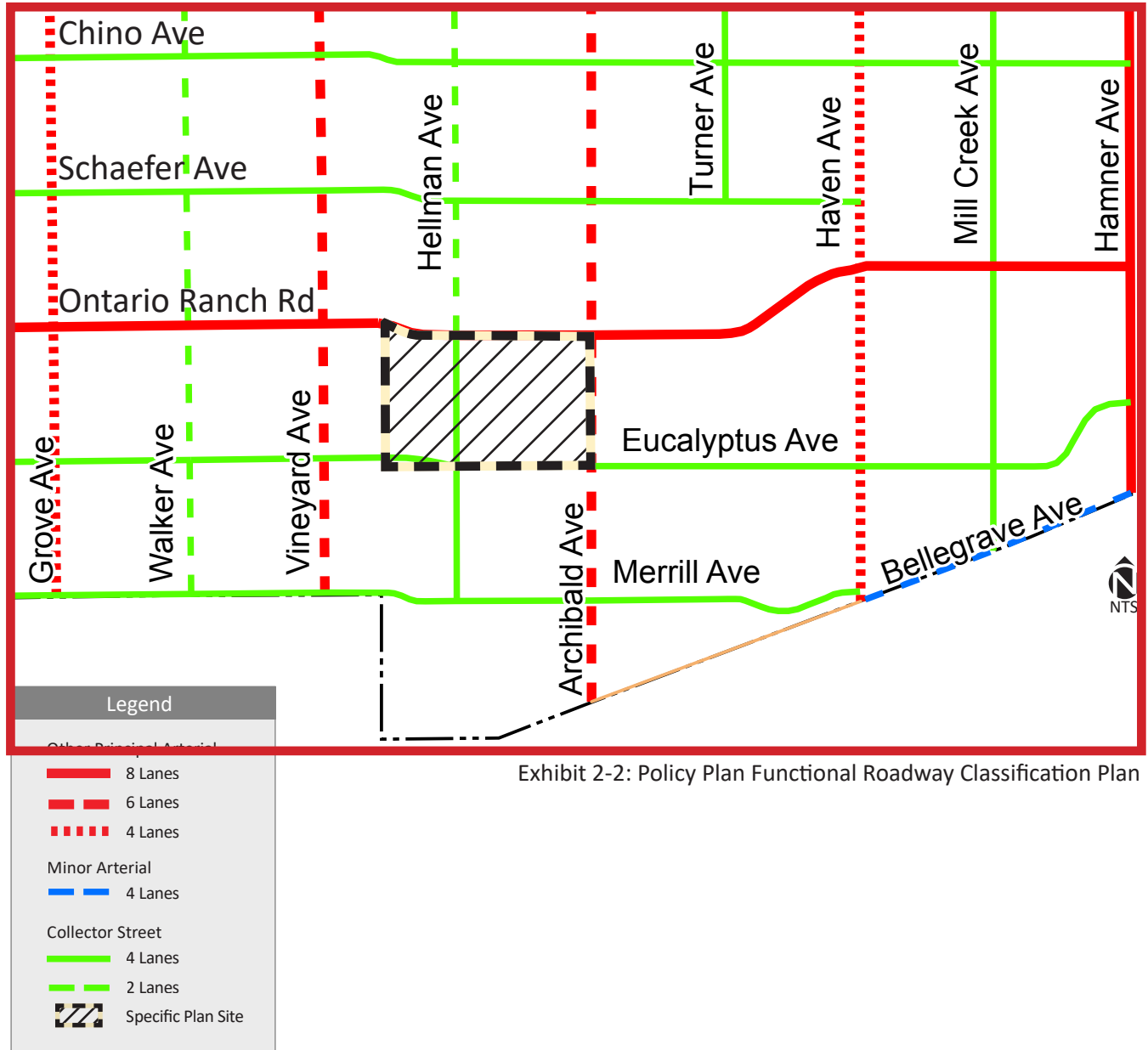


Exhibit 2-2: Policy Plan Functional Roadway Classification Plan



Parkside

- Sustainable development practices addressing energy efficiency.

2.1.2 Objectives

The following objectives are established for the Specific Plan.

Residential Areas

- Residential neighborhoods designed at a “human scale” and oriented to pedestrian activity;
- Connectivity provided among residential neighborhoods and recreational areas through a network of pedestrian sidewalks ~~and on-street bicycle trails~~;
- Connectivity provided between residential neighborhoods and the adjacent commercial land use, as well as to the central Great Park, by ~~means of pedestrian and bicycle~~ multi-purpose trail linkages, paseos and sidewalks;
- A variety of housing types incorporated into the land use plan addressing lifestyle considerations of singles, families, and empty nesters;
- Residential neighborhoods designed around a Great Park green core, framing green spaces and promoting passive recreational activity and casual social interaction among neighbors; and
- Residential neighborhoods with diverse architectural styles ~~and with both traditional and modern~~ design elements ~~reflecting the characteristics of older established Ontario neighborhoods~~.

Commercial Area

- Neighborhood commercial uses to meet the needs of the residential community within the project site as well as the larger surrounding market area;
- Provision for ~~paseos, trails, and sidewalks~~ connecting the residential community with the commercial center and adjacent uses;
- The development of plaza areas and other amenities within the commercial center providing space for social interaction; and
- Orientation of commercial buildings to the street and Great Park wherever possible, to create an urban edge and sense of arrival.

Streets and Pedestrian/Bicycle Mobility

- Streets and lanes (private alleys) designed in a modified grid pattern, reminiscent of traditional neighborhood streets;
- Streets with landscaped parkways and pedestrian walkways separated from the street, to create a pleasant pedestrian environment, promoting friendly interaction among neighbors;
- Several traffic calming techniques within the proposed street design, including enhanced parkway landscaping, median landscaping, tapered street intersections, and alley entrances to influence a driver’s peripheral vision and encourage motorists to drive more slowly; and
- Inside turning radii at corners reduced to slow traffic at corners.



Parkside

Recreation/Multi-purpose Trails

- New recreational opportunities for residents through the development of the Great Park to include areas for passive recreation;
- Private recreation amenities;
- A ~~portion~~ Extension of the City's ~~Master Plan of planned multipurpose~~ trails ~~provided~~ through ~~development of pedestrian trails~~ the Specific Plan including the multi-purpose trail within the Cucamonga Creek Trail Corridor connecting ~~Edison Ave.~~ Ontario Ranch Rd. with ~~Merrill Eucalyptus Ave.~~, and the surrounding area; and
- A system of ~~on-street bikeways~~ multipurpose trails integrated into the ~~project~~ Specific Plan area providing pedestrian and bicycle access from the residential community to surrounding areas.

Sustainable Development

- Native plant materials and non-invasive ornamental landscape materials will be incorporated into the landscaping plan.
- Residential development will be connected to the commercial land use area located within the site through pedestrian and bicycle trails promoting walking and bicycling as an alternative means of ~~transportation~~ automobile travel to and from these areas;
- Residential development will be equipped with the latest technology for internet access allowing residents to shop and work on-line, thereby reducing vehicle trips to employment centers and shopping;

- ~~Homes will be sited on east/west street alignments wherever feasible, to allow for a southerly orientation of residential development to promote energy efficiency;~~
- Canopy trees will be utilized in public areas such as parkways, medians, and the Great Park ~~to achieve natural ventilation and cooling~~ to provide shade to counteract the heat island effect;
- Roadway widths will be reduced as much as possible to minimize heat generating asphalt surfaces; and
- Recycled water will be utilized for the Great Park, neighborhood edges and other common landscape areas.

2.2 Specific Plan Proposal

2.2.1 Project Summary

The Specific Plan Land Use Plan is described below and in the Table 1-1: Parkside Land Use Summary.

A. Residential Uses

The Specific Plan will provide for development of a variety of residential housing types, many oriented toward park and open space amenities and designed to promote walkability and interaction among residents. Residential development within ~~156.83 gross acres of~~ the Specific Plan area will contain up to 1,947 dwelling units (per Table 1-1: Parkside Land Use Summary), providing a ~~variety~~ varied mix of single-family detached, single-family attached, and multi-family attached housing types as described below.



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1. Residential Detached

Examples of single-family detached residential products that may be used in the Specific Plan include:

2- and 3-Story Detached Homes

Single-family detached homes may consist of residential units with vehicular access provided from lanes (private alleys) or motor courts, via interior streets, with direct access garages. These residential areas will be developed at an approximate density of 8-14 dwelling units per acre.

~~Green Court and Motor Court (PA1, PA2, PA6, PA7, PA8):~~

~~Green Court/Cluster or Motor Court/Cluster single-family detached residential development may consist of residential units with vehicular access provided from lanes (private alleys) or motor courts, via interior streets, with direct access garages. These residential areas will be developed at an approximate density of 10-16 dwelling units per acre.~~

Larger Green Court/Cluster single-family detached residential development consists of lane-served residential units designed with an orientation to the street or green court spaces. Residential areas will be developed at an approximate density of 8-14 dwelling units per acre.

2. Residential Attached

Examples of multi-family attached residential products that may be used in the Specific Plan include:

~~Triplex (PA13, PA16):~~

~~Residential development in PA13, PA16 may consist of triplex-Triplex residential homes will be designed with~~

vehicular access provided from lanes. This residential area will be developed at an approximate density of 10-16 dwelling units per acre, with some buildings fronting the Great Park. Triplex buildings ~~would~~ could include townhomes, flat or carriage units in 3 unit per ~~building structure~~ configurations.

~~Townhomes (PA3, PA4, PA5, PA9, PA10, PA11, PA12, PA14, PA15, PA17, PA18, PA19):~~

~~Townhomes:~~

~~Multi-family attached units with orientation to the Great Park and local streets with direct access garages, in varying orientations. Residential neighborhoods will be developed at an approximate density of 12-25 dwelling units per acre, in townhome and condominium designs.~~

~~Multi-family Attached Homes~~

~~High density multi-family attached homes accessed from lanes (private alleys). Densities average approximately 18-25 dwelling units per acre, and include such residential uses as apartments, condominiums, and townhomes. Multi-family buildings in these four (4) planning areas will front on the "Great Park."~~

2.3 Authority and Requirements

2.3.1 Authority

State ~~of California~~ Government Code, Title 7, Division 1, Chapter 3, Article 8, Section 65450-57 grants authority to cities to adopt Specific Plans for purposes of implementing the goals and policies of their General Plans. The Government Code specifies that specific plans may be adopted either by resolution or by ordinance and that the Specific Plan is required to be consistent with the General Plan. The City ~~of Ontario will adopt the Subareas~~



Parkside

~~22 and 23~~ Specific Plan adopts specific plans by resolution ordinance, thereby establishing the zoning regulations for development of the project site. The requirements of the Specific Plan shall take precedence over the Ontario Development Code. In instances where the Specific Plan is silent, the City of Ontario Development Code shall prevail.

2.3.2 Requirements of the Specific Plan

California Government Code Section 65451 sets forth the minimum requirements and review procedures for Specific Plans as follows:

A Specific Plan shall include a text and a diagram or diagrams, which specify all of the following in detail:

- The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan;
- The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the project area covered by the plan and needed to support the land uses described in the plan;
- Standards and criteria by which improvements will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable;
- A program of implementation measures including regulations, programs, public works projects and the financing measures necessary to carry out the paragraphs ~~1, 2, and 3~~ above; and

- The Specific Plan shall include a statement of the relationship of the Specific Plan to the General Plan.

The Parkside Specific Plan meets the requirements of the State of California Government Code.

2.3.3 Development Agreement Required

Unless done in a coordinated manner and with adequate fiscal planning, development projects within the New Model Colony Ontario Ranch are likely to present a challenge in their implementation because of the lack of existing public facilities, including, streets, sewerage, transportation, drinking water, school, and utility facilities. California law has established a mechanism for ensuring the adequate provision of such facilities, while at the same time providing assurances to applicants that, upon approval of the project, the applicants can proceed with their projects. Approval of this Specific Plan without a development agreement may result in a waste of resources, escalate the cost of housing to the consumer, and discourage investment in and commitment to comprehensive planning, as envisioned by the City, which seeks to make maximum efficient utilization of resources at the least economic cost to the public.

Therefore, a statutory development agreement, authorized pursuant to California Government Code sections 65864 et seq., shall be required as part of the approval of this Specific Plan. For the above-mentioned reasons, the development agreement for this Specific Plan shall include, among other things, methods for financing acquisition and construction of infrastructure, acquisition and development of adequate levels of park land, and schools, as well as the provision of adequate housing opportunities for various segments of the



Parkside

community consistent with the regional housing needs assessment. Such development agreement shall have been fully approved before the issuance of the first building permit for this project.

2.4 Development Approval Components

The components of the development approval process for the Specific Plan are as follows:

General Plan ~~Amendment~~ Policy Plan

The Ontario Plan (TOP) establishes the direction and vision for the City providing a single guidance system that will shape the Ontario community for the future. TOP provides for policies to accommodate change over a 30-year period commencing in 2010, the beginning of the planning period. TOP consists of a six part Component Framework: 1) Vision, 2) Governance Manual, 3) Policy Plan, 4) City Council Priorities, 5) Implementation, and 6) Tracking and Feedback.

Specific Plan

~~A General Plan Amendment will be adopted for the project prior to the approval of the Specific Plan. The General Plan Amendment, when adopted, will modify the General Plan Land Use Map to:~~

- ~~• Reassign the high density residential uses around the “Great Park” as the central organizational element of the community;~~
- ~~• Eliminate low density designations;~~
- ~~• Create new medium density designations;~~

The Policy Plan (General Plan) requires the approval of a Specific Plan for development of the project site to ensure that sufficient land area is included to achieve unified districts and neighborhoods. The City has zoned the project site as Specific Plan. This zoning designation requires that a Specific Plan be approved to guide development of the project site and to implement the goals and policies of the Policy Plan. The Specific Plan provides the zoning regulations to govern development of the project site. The requirements of the Specific Plan shall take precedence over the Ontario Development Code. In instances where the Specific Plan is silent, the Ontario Development Code shall prevail.

~~Center designation adjacent to the park and Archibald Avenue;~~

~~• Add a community facility (fire station) designation directly west of the Neighborhood Center Commercial site; and~~

~~• Create a revised 237.9-acre planning area.~~

~~No other changes to the General Plan are proposed. The Specific Plan is consistent with overall residential land use intensity established by the General Plan. Refer to Exhibits 4, 5, and 6, General Plan Land Use Plan, General Plan Amendment, and Specific Plan Subareas.~~

~~Table 2a, General Plan Acreages (Existing) summarizes the total Parkside land use acreages for Subarea 23 in the New Model Colony General Plan, as adopted in 1998:~~

~~Table 2a—General Plan Acreages (Existing)~~

~~Land Use Designation — Approx. Acres (gross)~~



Parkside

K-5/K-6 School—10

Neighborhood Center—10

Residential—192

Parks/Village Green—205

Total—417 (Subarea 23)

Table 2b, General Plan Acreages (Proposed) summarizes land use acreages consistent with the proposed General Plan Amendment.

Table 2b—General Plan Acreages (Proposed)

Land Use Designation—Approx. Acres (gross)

Residential Medium Density

(12 du average per gross acre)—63.53

Residential High Density

(18 du average per gross acre)—93.51

Neighborhood Center—15.49

Great Park, Paseos—63.79

Public Use (fire station)—1.57

Total—*237.89

(Subareas 22 and 23)

*Acreage does not include Cucamonga Creek Channel

Specific Plan

The Specific Plan provides the zoning for the entire site. It serves as “blueprint” for future development by establishing the distribution of land use and criteria for development as set forth herein. The Specific Plan also serves as the legal document to implement the City’s General Plan, as amended by this Specific Plan proposal.

Subdivision Maps

Tentative tract map(s) will be approved reviewed by the City of Ontario for the residential portion of the Specific Plan indicating the approximate location of lot lines, streets, and proposed grading. Following approval by the City of the tentative tract map(s), a final map(s) will be prepared. Parcel Lot maps will be approved by the City of Ontario for development proposed within the commercial portion of the Specific Plan.

2.5 CEQA Compliance

A project site Environmental Impact Report (EIR) prepared by the City for the Specific Plan, in accordance with the California Environmental Quality Act (CEQA), addressed impacts associated with the Specific Plan and subdivision map. The EIR recommended mitigation measures to reduce impacts of the project site to a less than significant level. The EIR was prepared as the basis for the environmental review for all subsequent discretionary and ministerial actions.



Parkside

2.6 Relationship to General Plan and Zoning

The ~~City of Ontario New Model Colony General City's Policy Plan Amendment~~, which acts as the City's General Plan, designates the ~~site as a portion of Planning Subareas 22 and 23 project site for development of the following land uses~~ as follows:

- Medium Density Residential (11.1-25 dwelling units per acre)
- Neighborhood Commercial
- Public Facilities
- Open Space-Parkland

The Policy Plan (General Plan) establishes a ~~maximum~~ development capacity of 1,947 ~~residential~~ dwelling units and ~~approximately~~ 115,000 square feet of commercial ~~use for the uses within Planning Subareas 22 and 23. Residential development capacity is single-family detached and multi-family attached.~~

~~The City of Ontario has pre-zoned the project site as "SP" (Specific Plan Ag Preserve). The zoning designation of "SP" requires use for the area to be developed with a Specific Plan that carries out objectives of the New Model Colony General Plan, with land uses consistent with the approved General Plan project site.~~

California Government Code (Title 7, Division 1, Chapter 3, Article 8, Section 65450-65457) permits the adoption and administration of specific plans as an implementation tool for elements contained in the local general plan. Specific plans must demonstrate consistency in regulations, guidelines, and programs with the goals and policies set forth in the general plan. The Parkside Specific Plan has been prepared in conformance with the goals and policies of the City Policy Plan (General Plan). The policy analysis is located in the Appendix of this document, describes the manner in which the Parkside Specific Plan complies with the Policy Plan policies applicable to the project.

2.7 Development Plan Review

Following the approval of the Specific Plan, all development proposals for individual residential neighborhoods or product areas within the Specific Plan will be subject to the Development Plan Review process pursuant to ~~Article 8: Development Plan Review Chapter 2.0 Administration and Procedures~~ of the ~~City of~~ Ontario Development Code.

2.8 Airport Land Use Compatibility Planning Consistency

All development proposals or Specific Plan Amendments are required to be consistent with the Airport Land Use Compatibility Plans of Chino Airport and Ontario International Airport.

Chapter 3: Existing Conditions/Utilities



Denver, Colorado: City Park, Robert Burns Monument



Parkside

3.0 Existing Conditions/**Utilities**

3.1 Property Ownership

The Specific Plan area is comprised of approximately 250.9 gross acres. SC Ontario owns approximately 237.9 gross acres of the project site. San Bernardino County Flood Control District owns the right-of-way for the Cucamonga Creek Channel that comprises approximately 13.0 gross acres, and effectively divides the eastern half of the project site from the western half, crossing the site from north to south approximately 1,650 feet west of Archibald Ave. Exhibit 3-1: Existing Property Ownership depicts the property ownership within the Parkside Specific Plan area.

3.2 Airport Land Use Compatibility Plan Consistency

The Project Site is located within the Airport Influence Areas of Chino Airport and Ontario International Airport (ONT).

3.3 Existing Site Conditions and Improvements

The Parkside Specific Plan area is generally undeveloped and general agricultural operations are scattered throughout the area, including an irrigation pond located within the northwestern portion of the project site. The approximate 250.89-gross acre project site has historically been used for general crop-related agricultural purposes (alfalfa, corn, milo, sorghum, etc.) and flood control.

3.4 Surrounding Land Uses

Uses surrounding the Parkside Specific Plan area include rural residential, dairy farming, agricultural, nursery, Southern California Edison (SCE) Substation, and roadways. The surrounding land uses are as follows:

North: Dairy farming, poultry farming, nursery, crops, rural residential, and SCE Substation

South: Dairy farming and rural residential

East: Dairy farming, crops, and rural residential

West: Dairy farming, crops, and rural residential

Refer to Exhibit 3-2: Aerial Photo for further detail relative to existing and surrounding land uses.

3.5 Topography

The existing ground within the project site generally slopes to the southwest at approximately 0.8% to 2.0%. Exhibit 5-16: Conceptual Grading Plan illustrates the topographic features of the Parkside Specific Plan area.

3.6 Hydrology

The Parkside Specific Plan area is presently in agricultural use; therefore, only a limited portion of the project site is now covered with impervious surfaces. Normal rainfall for the area is, therefore, able to percolate through on-site soils and does not result in high volumes of surface runoff, typically associated with urban areas. The entire project site drains generally to the south and southeast towards the Cucamonga Creek. The project site is located approximately one-half mile south of the Cucamonga Basin. The Cucamonga Basin provides regional storm water runoff and drainage capacity to portions of south Ontario. The Cucamonga Creek Channel, which bisects the Parkside Specific Plan, is an improved concrete trapezoidal channel.



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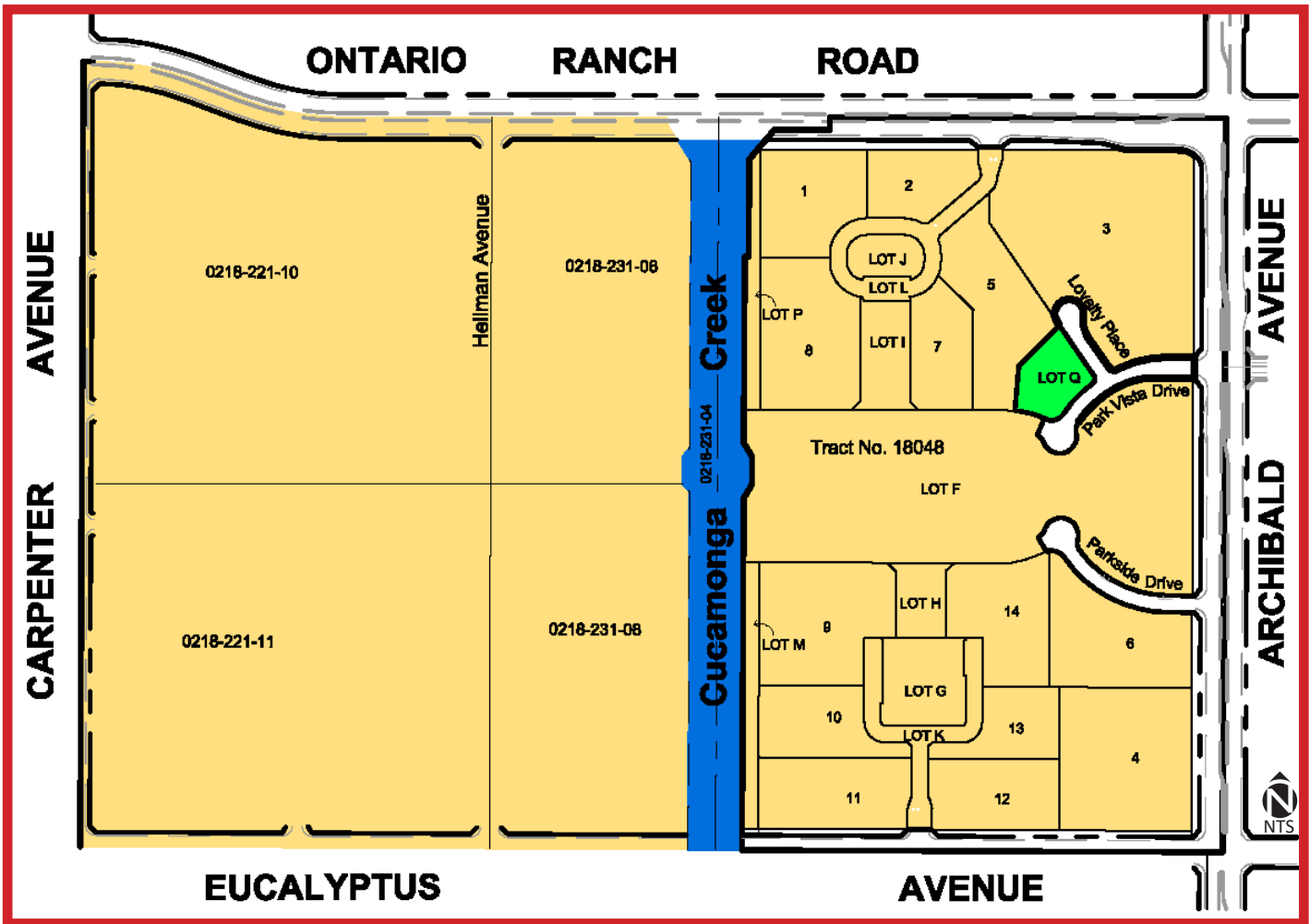


Exhibit 3-1: Existing Property Ownership

Tract. No 18048: SC Ontario Development Company, LLC

Lot 1: 0218-231-09	Lot 9: 0218-231-17	Lot H: 0218-231-30
Lot 2: 0218-231-10	Lot 10: 0218-231-18	Lot I: 0218-231-31
Lot 3: 0218-231-11	Lot 11: 0218-231-19	Lot J: 0218-231-32
Lot 4: 0218-231-12	Lot 12: 0218-231-20	Lot K: 0218-231-33
Lot 5: 0218-231-13	Lot 13: 0218-231-21	Lot L: 0218-231-34
Lot 6: 0218-231-14	Lot 14: 0218-231-22	Lot M: 0218-231-35
Lot 7: 0218-231-15	Lot F: 0218-231-28	Lot P: 0218-231-38
Lot 8: 0218-231-16	Lot G: 0218-231-29	Lot Q: 0218-231-39

Legend	
	SC Ontario Development Company, LLC
	City of Ontario
	San Bernardino County Flood Control



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Groundwater within and surrounding the project site, as a whole, contains certain high concentrations of salt, attributable to historic agricultural activities such as dairy farming. The high organic content of on-site soils has contributed incrementally to the degradation of surface and groundwater quality. Removal of the organic materials, which constitute by-products of those dairy operations, and compliance with National Pollution Discharge Elimination System (NPDES) and other storm water permit requirements will ~~beneficially impact regional water quality~~ be required as part of project site development.

~~The project level EIR, to be prepared for the Parkside Specific Plan, will provide additional drainage information:~~

3.7 Existing Circulation/Access

3.7.1 Regional Circulation

The Pomona Freeway, State Route (SR-60), is located 2.25 miles north of the project site. Currently, access to SR-60 is to the north, via Archibald Ave. In this region, SR-60 has full diamond-type interchanges with Euclid Ave., Grove Ave., Vineyard Ave., Archibald Ave., Haven Ave., and Milliken Ave. SR-60 carries approximately 160,000 vehicles per day in this area.

The Ontario Freeway, Interstate 15 (I-15), is located 2.4 miles east of the project site ~~The nearest existing~~ and carries approximately 90,000 vehicles per day in the vicinity of the Ontario Ranch area. The closest arterial ~~interchange to the Parkside Specific Plan area~~

is located at Limonite Ave. in Riverside County, which is 3.8 miles to the east, via Archibald Ave. to ~~Cloverdale Road (Cloverdale Road turns into~~ Limonite Ave. east of Hamner Ave). ~~The Cantu-Galleano or at Ontario Ranch Road is master-planned and will provide the Parkside Specific Plan access to I-15. The Cantu Rd./Cantu/Galleano Ranch Rd., located~~ 2.5 miles to the east via Edison Ave., from the project site.

State Highway SR-83 (Euclid Ave.) extends along the western boundary of the ~~New Model Colony~~ Ontario Ranch and is 2.5 miles west of the project site Specific Plan area via Ontario Ranch Road. Another major State Highway in the area is SR-71 (Chino Valley Freeway). This highway freeway is a major commuter route in southwestern San Bernardino County, connecting the SR-91 freeway with SR-60 and I-10 freeways. SR-71 is located 6.5 miles west of the Parkside Specific Plan area via Ontario Ranch Rd./Edison Ave.

3.7.2 Local Circulation

Access to the project site is provided from streets adjacent to the Parkside Specific Plan area. ~~Edison Ave.-Ontario Ranch Rd.~~ abuts the project site on the north and is currently improved with two travel lanes extending from west of the Archibald Ave. intersection, easterly to and beyond the western property boundary. Archibald Ave. abuts the project site on the east and



Parkside



Existing Groundwater Well Location

Exhibit 3-2: Aerial Photo



Parkside

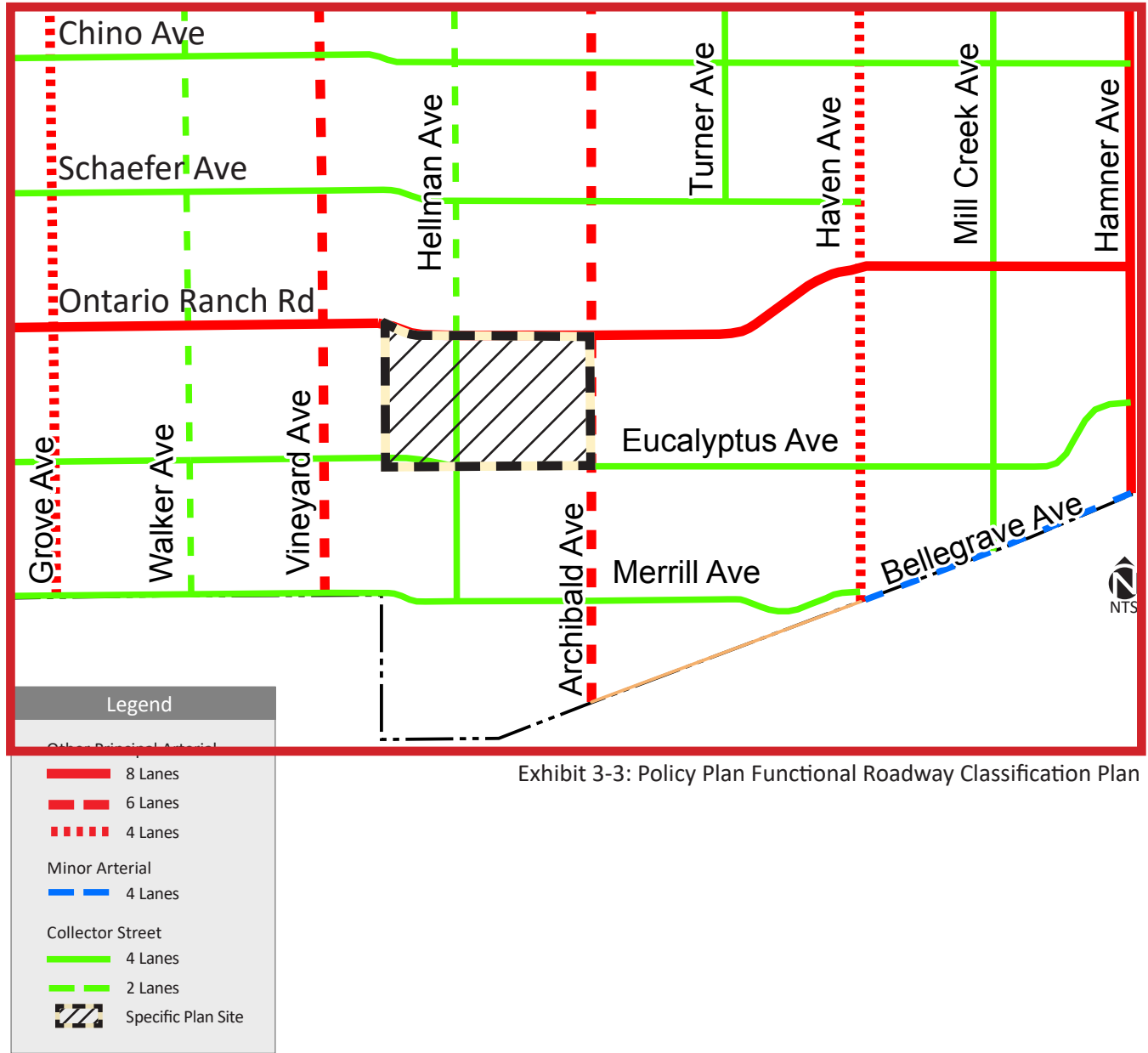


Exhibit 3-3: Policy Plan Functional Roadway Classification Plan



Parkside

is currently improved with four travel lanes, two travel lanes southbound and two travel lanes northbound and a landscape median. Eucalyptus Ave. (future Merrill Ave.) abuts the project site on the southwestern half of the Parkside Specific Plan area and is currently unimproved.

Exhibit 1-2, Regional Location Map illustrates the existing regional and local circulation surrounding the Parkside Specific Plan area. Exhibit 3-3: Policy Plan Functional Roadway Classification Plan illustrates the arterial streets in the area.

3.8. Existing Infrastructure/Utilities

3.8.1 Water

Currently there are no New water mains are located in either Edison Ave. or both Ontario Ranch Rd. and Archibald Ave. adjacent to the Parkside Specific Plan area.

3.8.2 Groundwater Wells

In compliance with the Chino Basin Water Master's Well Procedure for Developers, a well use/destruction plan and schedule for all existing private/agricultural wells shall be submitted to the City of Ontario for approval prior to the issuance of permits for any construction activity. If a private well is actively used for water supply, the Developer shall submit a plan to abandon such well and connect users to the City's water system (residential to the domestic water system and agricultural to the recycled water system) when available. Wells shall be destroyed/abandoned per the California Water Resource Guidelines and require permitting from County Health Department. The location of existing wells are shown

in Figure 2.#, "Existing Wells". On-site residential and agricultural uses within the project site are served by three private wells, as illustrated on Exhibit 3-2: Aerial Photo. Upon development of the well site areas, these wells, or any other wells found on the property, will be abandoned per California Department of Water Resources Health Guidelines and the City Guidelines. In addition, a well use/destruction plan, as approved by the City, and schedule for all existing wells will be required. If a private well is actively used for water supply, the Developer shall submit a plan to abandon such well and connect users to the City's water system (residential to the domestic water system and agricultural to the recycled water system) when available. Wells shall be destroyed/abandoned per the California Water Resource Guidelines and require permitting from County Health Department. The location of existing wells are shown in Figure 3-2: Aerial Photo. If a water connection is not available an alternate source may still be proposed by the Developer and approved by the City. The City's Water Master Plan provides significant new water facilities to serve the project site including a new production well to be constructed on-site by the Chino Basin Desalter Authority. If the Developer proposes temporary use of an existing agricultural well for purposes other than agriculture, such as grading, dust control, etc., the developer shall make a formal request to the City for such use prior to issuance of permits for any construction activity. Upon approval, the Developer shall enter into an agreement with the City and pay any applicable fees as set forth by the agreement.



Parkside

3.8.3 Sewer

~~The City of Ontario does not have sewer facilities within the vicinity of the project site. Wastewater Sewer mains are located in both Ontario Ranch Rd. and Archibald Ave. Currently, wastewater~~ disposal within the Specific Plan area is currently provided through septic tanks and subsurface disposal fields. Prior to grading operations, existing septic tanks and subsurface disposal fields will need to be abandoned in accordance with Department of Health Services requirements. ~~The City of Ontario's Sewer Master Plan provides significant new facilities to serve the project site.~~

3.8.4 Stormwater/Drainage Facilities

The Cucamonga Creek Channel bisects the Parkside Specific Plan area. The Cucamonga Creek Channel is an improved concrete lined trapezoidal channel. During periods of heavy rainfall, when ground surfaces are saturated, surface runoff is collected in the existing drainage ditches located within the Specific Plan area. The project site is located approximately one-half mile south of the Cucamonga Basin. The Cucamonga Basin acts as a regional flood control facility for portions of south Ontario. The City's Master Plan of Drainage includes storm drain lines in ~~Merrill Eucalyptus~~ Ave., with connections to the Cucamonga Creek Channel, to drain on-site flows from the Specific Plan area.

3.8.5 Solid Waste Disposal

~~The City of Ontario Public Works Agency currently provides~~ There is no solid waste ~~collection and~~ disposal in the ~~New Model Colony~~ vicinity of the project site.

3.8.6 Natural Gas

The Southern California Gas Company (SCG) provides natural gas service within the project site. Facilities within the area include an existing 8 inch line, reducing to 6 inches, along Archibald Ave., a 6-inch line and a 36-inch transmission line along Ontario Ranch Rd. (the 36-inch transmission line is located along the south side of ~~Edison Ave-Ontario Ranch Rd.~~).

3.8.7 Electrical Facilities

Southern California Edison Company (SCE) provides electricity to the project site. SCE has overhead facilities in the area servicing the existing farms and dairies. Existing facilities surrounding the Specific Plan area, outside of the Specific Plan boundary, include 66 KV and 12 KV lines extending along the north side of Ontario Ranch Rd. and 66 KV and 12 KV lines extending along the east side of Archibald Ave. Existing facilities within the project site include a 12 KV line and a 120 V line located west of the Cucamonga Creek Channel. The development of the Specific Plan area will increase electrical power requirements and SCE will develop the required distribution system. All existing lines, less than 34.2 KV, within the Specific Plan area shall be removed or undergrounded.

3.8.8 Communication Systems

~~Verizon Spectrum~~ provides telephone service within the project ~~site area~~. Currently, telephone service is provided to the residences, dairies, and farms in the area.



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3.9 Geology and Soils

~~The City of Ontario General Plan EIR identifies the project site as underlain by Pleistocene age (older than 12,000 years) and Holocene age (less than 12,000 years old) alluvial deposits. The youngest surficial deposit is eolian sands (Qhs), comprising wind-blown sands having fine-to-medium-sized grains. These loose sands form sheets and low-dune deposits that have been stabilized by vegetation. These deposits are exposed in the eastern portion of the New Model Colony area and extend westward to an area defined generally by a diagonal line extending from Harrison (Turner) Ave. on the south to Vineyard Ave. on the north. It is expected that most of these materials will be uncemented and subject to consolidation when saturated under structural loads. Erosion potential is considered high. Foundation and backfill suitability should be satisfactory with proper over-excavation, mixing with a finer-grained binder material, and compaction.~~

The City General Plan EIR identified the project site as containing Delhi series soils, as mapped by the United States Department of Agriculture, Soil Conservation Service in 1971 and 1980. Delhi series soils have been used for agriculture, primarily for grapes and citrus, since the 1800's.

~~As part of the EIR prepared for the Parkside Specific Plan, additional geologic and soils information for the project site will be provided.~~

3.10 Seismicity

The ~~City of Ontario Sphere of Influence~~ General Plan identified numerous earthquake faults within a 50-mile radius of the project site. Major mapped faults include, but are not limited to, the Chino, Whittier and North Elsinore, and Cucamonga Faults. For the “maximum probable earthquake” (MPE), defined as the 100-year event normally considered in the design of non-critical structures, the values range from about 0.13 to 0.20 g (i.e., the unit force of gravity). In the design of certain critical or important facilities such as hospitals and dams, the “maximum credible earthquake” (MCE) ~~event is considered. For the three (3) faults, the MCE~~ should yield an estimated peak horizontal acceleration in the range of 0.33 to 0.52 g.

A zone of concentrated, relatively low-magnitude seismicity extends to the southwest from the San Jacinto fault zone (Rialto-Colton branch) along ~~what is referred to as~~ an “inferred fault near Fontana.” Where the “inferred fault” (Fontana trend) stops, this zone of micro-seismicity continues in a southwesterly to westerly direction terminating in the ~~Sphere of Influence area~~ **Ontario Ranch**. It is expected that the MPE for this fault structure could produce horizontal accelerations in the range of 0.3 to 0.5 g. More distant faults are capable of larger earthquakes with a higher probability of occurrence. The San Andreas fault is expected to generate a MCE event every 150 to 200 years, yielding a peak horizontal ground acceleration of approximately 0.21 to 0.26 g.

~~In accordance with the~~ **The** Uniform Building Code (UBC), ~~the Specific Plan area is located within Seismic Zone No.~~



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4. UBC establishes procedures designed to ensure that all subsequent development occurs in a safe manner relative to those known hazards.

As part of the EIR prepared for the Parkside Specific Plan, additional seismicity analysis will be prepared.

3.11 Vegetation

The project site has historically been extensively used for general crop-related agricultural purposes (alfalfa, corn, milo, sorghum, etc.) and is currently utilized for operations, including both dairy use and the cultivation of row crops. Rural Those areas not in active agricultural production are occupied by rural residential housing, farm buildings, and other ancillary facilities occupy those areas not in active agricultural production. The natural vegetation and soils conditions that once occurred throughout the project site have been significantly altered through human uses.

3.12 Biological Resources

The project EIR prepared for the City's NMC General Plan Amendment states that the Ontario NMC General Plan area has been greatly altered from natural conditions, under the influence of intensive agriculture and dairy industry.

Agriculture comprises the vast majority of the total land use within Ontario Ranch area (GPA EIR for the NMC, 1998). Consequently, the project area has been exposed to widespread and extreme levels of human-related disturbances through agricultural uses. The 250-acre site contains existing structures associated with crop production (e.g., sheds), evidence of former residential structures, cultivated areas, ruderal areas, and an irrigation pond. The large pond was historically

filled with industrial process wastewater generated by Sunkist Growers, Inc. as well as extracted ground water. The irrigation pond no longer receives water from Sunkist Growers, Inc. or wells and has dried up. Cucamonga Creek Channel, a concrete lined flood control facility, traverses the center of the project site, but offers little habitat function or value. Windrows, located on the north, south, and east project boundaries, represent the tallest vegetation on the site and offer perching and nesting opportunities for birds. Agricultural land uses surround the project site; however, urban development exists to the northeast within the City of Ontario, and to the southeast in Riverside County.

3.12 — STUDIES AND ASSESSMENTS PERFORMED

The following are the conclusions and recommendations of the varying studies/assessments performed on the project site:

- Blasland, Bouck & Lee, Inc was retained by Lewis Homes Enterprises to conduct a Phase I Environmental Site Assessment for the project site (see Appendix B1, under separate cover, "Phase I Environmental Site Assessment Sunkist Property, Chino, California"). The report states; "Based on the findings of this investigation, the subject properties appear environmentally suitable for their intended uses as residential developments."

- LOR Geotechnical Group, Inc was retained by Lewis Investment Company, LLC to conduct a Phase I Environmental Site Assessment Update for the project site (see Appendix B2, under separate cover, "Phase I Environmental Site Assessment Update Sunkist Property 9343 Edison Ave. Ontario California"). The report states; "Based on the findings of the site investigation and



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with consideration given of recognized environmental conditions herein, (1) the subject property exhibits no evidence of recognized environmental conditions that would prohibit its intended use as residential/mixed use development, and (2) no further tests or investigations are recommended except as expressly stated in this report for such intended use.”

• Glenn Lukos Associates was retained by Lewis Operating Corp. to perform a Biological Constraints Analysis for the project site (see Appendix B3, under separate cover, “Results of Biological Constraints Analysis for 230-acre property located Southwest of the Archibald Ave. and Edison Ave. intersection, San Bernardino County, California”). The report states:

–“No sensitive habitats were identified on the site.”

–“Although Delhi sands have been identified for a majority of the property, habitat for the federally-endangered Delhi sands giant flower-loving fly is not present on site due to severe degradation of habitat associated with raising cattle and lack of the preferred DSFL host plants on site. Therefore, the site does not represent suitable habitat for this species and no further action regarding this species is required.”

• Larry Munsey International was retained by Lewis Investment Company LLC to perform a Focused Survey for Delhi Sands Flower-Loving Fly for the years 2002, 2003 and 2005 (see Appendix B4, under separate cover, “Report of the Year 2002 Focused Survey For Delhi Sands Flower-Loving Fly at Sunkist project site San Bernardino County, California” and Appendix B5, under separate cover, “Report of the Year 2003 Focused Survey For Delhi Sands Flower-Loving Fly at Sunkist project site

San Bernardino County, California”).

The 2002 survey states:

–“No DSF or DSF sign (i.e., discarded pupal cases) were observed on the Survey Site during the survey.”

The 2003 survey states:

–“No DSF or DSF sign (i.e., discarded pupal cases) were observed on the Survey Site during the current survey, nor was the DSF detected on the site during the prior year’s survey (LMI 2002).”

–“Based upon the following factors it may be concluded that the Survey Site is not occupied by the DSF nor is any suitable habitat for the species present:

- ◇ highly disturbed condition of entire site;
 - ◇ disturbed condition of Delhi Sands soils;
 - ◇ absence of California buckwheat, California croton, and telegraph weed;
 - ◇ low diversity of plants;
 - ◇ high proportion of non-native invasives in the site’s plant composition; and
 - ◇ type and condition of the habitat surrounding the site.”
- ◇ Petra Environmental Division was retained by Lewis Homes to perform a Preliminary Methane Investigation and Soil Testing on seven sites, one being the project site (i.e. Sunkist), (see Appendix C6, under separate cover, “Report of Preliminary Methane Investigation



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and Soil Testing”). The report states: “Methane was not identified in the probes located in the field portion of the Idsinga site nor was methane identified in any of the probes located at the Van Vliet, Durrington, Rohrs, Stueve, Vernola, and Sunkist sites.”



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Chapter 4: Land Use



San Francisco, California: Golden Gate Park



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4.0 Land Use

4.1 Introduction

The ~~250.89 gross acre Specific Plan for Parkside~~ is comprised of approximately 250.9 gross acres. The Specific Plan includes the western portion of Subarea 23, between Cucamonga Creek and Archibald Ave., and the eastern portion of Subarea 22, between Cucamonga Creek and future Carpenter Ave. The entire Specific Plan area amounts to 237.9 acres in 25 planning areas (exclusive of Cucamonga Creek). Net project acreage amounts to 203.2 acres.

The Specific Plan is consistent with and implements the Medium Density Residential allocation (11.1 to 25 dwelling units permitted per gross acre) and broad “community design” policy requirements for the Ontario Ranch, including a general neo-traditional community design. These include:

- Integration with and connectivity to the Great Park;
- Creation of a “sense of place;”
- Diversity in housing product and architecture;
- Pedestrian connectivity;
- Smaller “neighborhoods;” and
- Elimination of the “subdivision” feel.

Parkside is envisioned to provide a neighborhood lifestyle in a wide range of planning areas, featuring a variety of residential housing types, all designed around a major community amenity, the Great Park. All residential neighborhoods are designed within easy walking distance to internal recreational amenities, commercial retail uses and the Great Park. Connectivity within the Specific Plan is provided through a system of ~~pedestrian and bicycle sidewalks~~, multi-purpose trails and on-street bike lanes linking residential neighborhoods to one

another, parks, off-site schools, and to the commercial retail area. A component of the trail system ~~will be provided through the improvement of a portion of~~ is the Cucamonga Creek Trail within the Specific Plan area. These improvements represent a part of the City’s Master Planned multi-purpose trail system planned for the ~~New Model Colony~~ Ontario Ranch.

Residential development is designed to address a variety of income levels and lifestyles, such as singles, families, executives, and “empty nesters.” Housing types will include such types as clustered “green court” or “motor court” single-family detached homes, and a wide variety of multi-family attached products including, but not limited to, triplexes, row townhomes, condominiums, and apartments.

A commercial land use is provided for in the Specific Plan, in Planning Area ~~21~~ 24. The Specific Plan allows for the development of approximately 115,000 square feet of neighborhood commercial, retail and service uses conveniently located adjacent to Archibald Ave. and ~~Edison Ave~~ Ontario Ranch Rd.

The Land Use diagram illustrated in Exhibit ~~10~~ 4-1: Land Use Plan illustrates the overall land use pattern within ~~portions of Subareas 22 and 23~~ the Specific Plan area. Table 4-1: Parkside Land Use Summary documents statistical totals for the Specific Plan.

4.2 Residential Use

Residential land uses within the Specific Plan comprise approximately ~~157.04~~ 156.62 gross acres. The Specific Plan will permit the development of up to 1,947 single-



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distinctive neighborhoods, linked by a network of sidewalks, paseos, and on-street bicycle paths connecting all the neighborhoods to the central Great Park.

4.2.1 Variety of Housing Types

Parkside provides a unique mix of housing types to address the needs of a variety of lifestyle choices and economic segments. A variety of detached and attached residential products, with a variety of architectural styles and features will be offered. (See Chapter 7.2 for further details).

4.2.2 Traditional Neighborhood Design

The community plan for Parkside offers a strong neighborhood character and identity for residents and visitors through a traditional approach to street design, architecture, and landscape design elements to reflect a similar character to that of older traditional Southern California neighborhoods, including Ontario with a modern interpretation of traditional styles. The design features described below “activate” the streetscene and promote friendly interaction between neighbors.



Exhibit 4-1: Land Use Plan



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Table 4-1: Parkside Land Use Summary

Land Use	Lot Size / Use	Gross Acreage	Net Acreage	*Density Range / S.F.	Lots / D.U.
Residential					
PA-1 (Green Court/Lane Loaded Products 1, 2 & 4)	SFD	19.97	15.57	10-18	236
PA-2 (Lane Loaded Product 3)	SFD	3.57	2.66	10-18	41
PA-3 (Lane Loaded Product 3)	SFD	3.06	2.38	10-18	42
PA-4 (Green Court/Lane Loaded Products 1, 2, 3 & 4)	SFD	15.50	13.62	10-18	221
PA-5 (HD Townhomes)	MF	9.57	8.14	15-22	139
PA-6 (Attached Auto Court)	MF	7.34	6.89	8-14	75
PA-7 (Auto Court 6-8 Plex)	SFD	9.30	7.49	8-14	68
PA-8 (Green Court 8-10 Plex)	SFD	6.64	6.18	10-16	68
PA-9 (Triplex)	MF	10.49	8.70	10-16	120
PA-10 (Row Townhomes)	MF	7.56	7.22	17-25	143
PA-11 (HD Townhomes)	MF	8.11	7.22	15-22	123
PA-12 (Triplex)	MF	11.80	9.30	12-18	127
PA-13 (Attached Auto Court)	MF	8.53	6.34	8-14	64
PA-14 (Duplex)	MF	7.12	6.18	12-18	99
PA-15 (Duplex)	MF	8.62	6.97	12-18	111
PA-16 (Triplex)	MF	7.83	7.35	10-16	90
PA-17 (Row Townhomes)	MF	4.87	4.54	17-25	82
PA-18 (HD Townhomes)	MF	6.74	5.04	15-22	98
Residential Subtotal		156.62	131.79	14.77	1,947
Non-Residential					
PA-19 (Commercial)		15.66	11.15	115,000 SF	-
PA-20 (Fire Station)		1.72	1.39	-	-
PA-21 (Great Park)		58.86	54.40	-	-
PA-22 (Private Rec)		1.00	1.00	-	-
PA-23 (Private Rec)		1.90	1.34	-	-
PA-24 (Private Rec)		1.00	1.00	-	-
PA-25 (Private Rec)		1.00	1.00	-	-
PA-26 (CD Well Site)		0.13	0.13	-	-
Non-Residential Subtotal		81.27	71.41	115,000 SF	-
Channel ROW		13.00	13.00	-	-
Total Project		250.89	216.20		1,947

NOTES:

- 1) Gross acres taken to center line of streets.
- 2) Adjusted gross acres taken to street ROW dedicated to the City
- 3) Net acres taken to street right of way
- 4) A total of 34.69 acres of master planned roadway is included in the project
- * Density range is shown is based on net acres. The General Plan is based on adjusted gross acres
- 5) Proposed product types in each Planning Area are subject to change based on final development submittal.



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- A traditional grid and geometric street design in residential neighborhoods, with sidewalks separated by landscaped parkways, which provide visual interest, slower traffic, lower traffic volumes, and enhance a pedestrian orientation for neighborhoods. Sidewalks separated by a landscaped parkway promote pedestrian mobility, beautify the streetscene and lend a pedestrian scale;
- Streets adjacent to the Great Park may include on-street parking for park and recreation uses to the north and south, and a “linear” landscape treatment on both sides of the street. Parkways adjacent to both sides of the street are planned to be heavily landscaped;
- The architecture for residences within Parkside will be designed to focus on human scale details which will enhance the pedestrian friendly character of the community. These features may include the use of front porches, railings, enhanced entries, a mix of materials and textures, and authentic detailing on elements such as windows and doors, columns, balconies, and lighting; and
- Innovative garage designs will be utilized where appropriate in order to de-emphasize the visual impact of garage doors on the streetscene. ~~Design techniques may include varied garage setback requirements, split garages, turn-in garages, garages located on rear lanes (private alleys), or other similar techniques that de-emphasize the view of garage doors from the street.~~

4.3 Parks, Open Space and Recreational Facilities

Parkside has been designed as a planned community “in the park.” The Great Park is the key organizational element for the entire community. ~~A conceptual plan for the “Great Park” is illustrated in Exhibit 33, Great Park Concept Plan.~~ Further, refined recreation planning is currently underway by the City on this segment of the Great Park and adjacent portions of the entire facility. Other open space and recreation opportunities for Parkside are included in the private recreation facilities which are integral to the neighborhoods in the project.

4.3.1 Parks

An integral part of the development of the residential program within the development boundary of the 250.9-acre Specific Plan is approximately 63.9 gross acres of park and recreational areas, including the Great Park, which provides primarily passive recreational opportunities for residents of the project and the surrounding community. The City’s ~~Quimby Act~~ park dedication requirements ~~will~~ may be met through a combination of improved park land and payment of fees. Refer to Chapter 7.7, Landscape, included in the overall Residential Design Guidelines for the project, for park plans for Parkside, including the Great Park Plan and blow-ups of the pocket parks and private community recreation acres.

~~The Quimby Act, provides for the dedication of 3 acres of parkland per 1,000 residents. However, pursuant to the General Plan Amendment Goal 12.0, Objective 12.1, Policy 12.1.3, each specific plan is required to~~



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~~define a park acquisition and improvement financing plan to ensure the achievement of a park standard of 5 acres of parkland per 1,000 residents. Pursuant to the foregoing policy, the total parkland requirement for the project is 34.21 gross acres. The Parkside Specific Plan; however, will provide 63.99 gross acres of parkland and recreational areas. Such areas shall either be dedicated to the City or transferred to a homeowners association:~~

The Policy Plan (Policy PR1-5) has established a standard of 5 acres of parkland (public and private) per 1,000 residents, with a minimum of 2 acres of developed private park space per 1,000 residents (Policy PR1-6). Private parks are required to be approximately a quarter mile walking/biking distance from each residence. This private park requirement may be met within any residential development, or by satisfying the in-lieu park development impact fee as approved by the City. Fees will be paid to fulfill the balance of the City's park requirement (the remaining 3 acres per 1,000 residents).

4.3.2 Multi-Purpose Trails

~~Bicycle~~ Multi-purpose trails are an integral element in creating accessibility and mobility within the Specific Plan area. An off-street ~~Class I bicycle~~ multi-purpose trail system will be provided within the right-of-way on the west side of Archibald Ave. adjacent to the ~~development~~ Specific Plan area between ~~Edison and Merrill Ave~~ Ontario Ranch Rd. and Eucalyptus Ave. Adequate right-of-way will be provided for on-street bicycle routes lanes with the improvement of Merrill Eucalyptus Ave. This bicycle trail system will link residential neighborhoods to the planned parks and the commercial site located within the development area, as well as providing connectivity to future development areas within the project Specific Plan area

4.4 Cucamonga Creek Channel Trail

A link within of the City's Master Plan of Trails includes a portion of the multi-purpose trail which will be developed in the Specific Plan area, extending a multi-purpose trail from Edison Ave Ontario Ranch Rd. southerly to future Merrill Eucalyptus Ave. in the Cucamonga Creek Channel. Access to this trail will be provided at key points within the Great Park to provide for pedestrian and bicycle accessibility. ~~The bridge over the Cucamonga Creek Channel shall be strong enough to support a police vehicle for needed access.~~ A conceptual plan for the Cucamonga Creek Trail is illustrated in Exhibit 12, Pedestrian and Bicycle Circulation Plan, and Exhibits 37-37a, Channel Regional Trail Easement Plan, in Chapter 7, Design Guidelines.

4.5 Commercial

The Specific Plan includes approximately 15.7 gross acres designated for development of neighborhood commercial uses.

4.5.1 Neighborhood Center

The Specific Plan provides for the development of approximately 115,000 square feet of Neighborhood Center Commercial land uses adjacent to Archibald Ave. and Edison Ave Ontario Ranch Rd. Commercial development at this location is conveniently located to serve the residential community of Parkside as well as the surrounding community. Pedestrian and bicycle connectivity between residential land use areas within Parkside and the future commercial center will be provided through an extensive network of trails. Commercial uses which could be developed within this land use district include, but are not limited to,



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local servicing commercial uses, such as grocery, drug store, small restaurants, small retail shops, and personal services.

4.6 Public Use (Fire Station and Well Sites)

~~A site consisting of 1.57~~ 1.72 gross acres proposed directly west of the Neighborhood Center and fronting on the Great Park ~~devoted~~, is allocated to future use as a City Fire Station ~~as illustrated~~. Additionally, a CDA well site consisting of approximately 0.13 acres is proposed in the Great Park. Refer to Exhibit 4-1: Land Use Plan. ~~The final site design and definition of uses will be provided by the City.~~

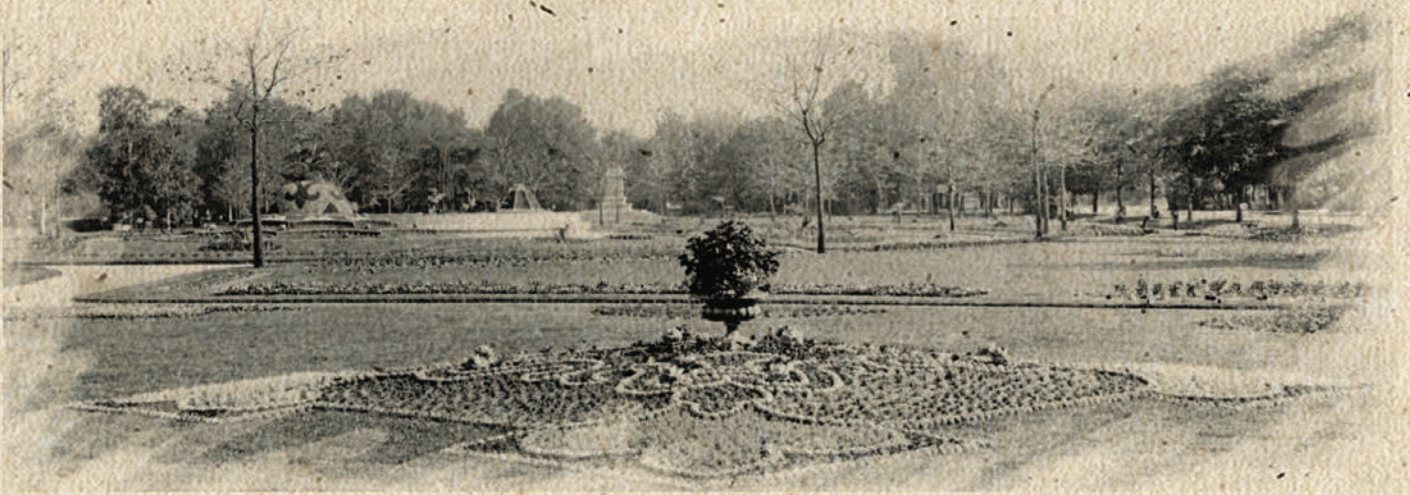
4.7 Dairy Farming

~~If the Specific Plan Area~~ The Ontario Ranch area contains existing dairies and feed lots. A minimum 100-foot separation shall be required between a new residential, commercial, or industrial development, or structures used for public assembly and an existing animal feed trough, corral/pen, or an existing dairy/feed lot, including manure stockpiles and related wastewater detention basins. The 100-foot separation requirement may be satisfied by an off-site easement acceptable to the Planning Director with adjacent properties, submitted with the initial final map and recorded prior to or concurrent with the final map.

4.8 Airport Land Use Compatibility Planning (ALUCP)

The Specific Plan area is located within the Airport Influence Area of Chino Airport and the Ontario International Airport (ONT). The area is considered to be an “Existing Land Use” as defined by the Riverside County ALUCP for Chino Airport and ONT ALUCP. There is an existing Development Agreement in place for the property and is therefore not subject to the policies and criteria of the ALUCP for either airport. However it is recommended that the standard real estate transaction disclosures be applied any development.

Chapter 5: Infrastructure & Services



Chicago, Illinois: Lincoln Park



Parkside

5.0 Infrastructure and Services

The infrastructure, utilities, and public services to be provided to support development of the Parkside Specific Plan are discussed in this section.

5.1 Circulation

The circulation plan for the Parkside Specific Plan area will provide efficient movement of vehicular traffic throughout the project site community as well as a pleasant environment for pedestrian movement and bicycle traffic thereby circulation, reducing the reliance on the automobile as a means for travel. The plan reinforces the concept of traditional neighborhood design with a grid street pattern. Exhibit 5-1: Vehicular Circulation Master Plan illustrates the roadway system planned for the Specific Plan area.

The project site will be served by a system of new and varied parkway, arterial and collector roadway classifications to be improved per the requirements of the City's adopted roadway classifications as illustrated in the New Model Colony General Plan Amendment (Figure 4-15 "Roadway Classifications—Parkway 1 and 1A Options," Figure 4-17, "Roadway Classifications") City Council Approved 2010 The Ontario Plan (TOP), General Plan, the 2012 Master Plan of Streets and Highways, and through proposed primary local streets to be constructed as part of the project site development. Improvements will be constructed for Archibald Ave., Edison Ave, Merrill Ontario Ranch Rd., Eucalyptus Ave., and Carpenter Ave., all of which border the project site. Proposed Hellman Ave. will be fully developed to serve the project site. Within the Parkside Specific Plan area, local collector streets and local interior streets will provide for the efficient movement of vehicles and pedestrians roadways

through the project. The developer will be designed to meet current and approved design standards. The traffic study will verify the need for additional rights-of-way at critical intersections required to accommodate additional left and right turn lanes. On street submit a parking and landscaping adjacent analysis to streets may be restricted within 100-150 feet of primary, secondary, and local street intersections the the City to ensure the proposal is in order to accommodate adequate sight distances conformance with the City's Parking Code for residential developments.

5.1.1 Streets

The developer(s) of the Parkside Specific Plan area will be responsible for the off-site improvements along the entire project frontage or as otherwise indicated. Phasing of the improvements will be implemented as required by the City Engineer and pursuant to the mitigation measures identified in the EIR and/or the Conditions of Approval established on the approved tentative maps for the project. Additionally, bus turnouts within the project site Specific Plan area may be required to be constructed to the reasonable satisfaction of the City and Omnitrans.

The following is a description of the streets to be improved by the Parkside Specific Plan and associated improvements:

The minimum design speeds to be used for centerline curve radii, superelevation, corner sight distance, vertical and horizontal alignment, and sight distance, etc. are listed below:



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Ontario Ranch Rd. 50 m.p.h.

Archibald Ave. 50 m.p.h.

Eucalyptus Ave. 45 m.p.h.

Hellman Ave. 40 m.p.h.

The traffic study prepared for the Parkside Specific Plan will determine the need for additional rights-of-way at critical intersections in order to accommodate additional left and right turn lanes.

On-street parking is prohibited on all master planned roadways.

Ontario Ranch Rd.

Archibald Ave. is designated as a Divided Arterial Parkway 1A street, with bikeway.

~~Archibald Ave. will carry regionally oriented traffic along the eastern boundary of the Specific Plan site and provide north/south access to and from the project site. Access to this street via driveways from residential units and on-street parking will be prohibited. Improvements to Archibald Ave. are illustrated on Exhibit 11a, Arterial Street Sections.~~

~~Edison Ave. is designated as a Divided Arterial Parkway 1A street. Edison Ave. Ontario Ranch Rd. is designated as an eight-lane Principal Arterial. Ontario Ranch Rd. provides east/west access to and from the project site along the northern boundary of the Specific Plan area. Access to this street via driveways from residential units and on-street parking will be prohibited. Exhibit 5-2: Principal Arterial Street Sections—Ontario Ranch Rd. illustrates the ultimate improvements to Edison Ave Ontario Ranch Rd.~~

Archibald Ave.

Per the City's 2012 Master Plan of Streets and Highways, Archibald Ave. is designated as a Principal Arterial. The street section for Archibald Ave. is per the approved Archibald Ave. Street Improvement Plans. Per this plan set, Archibald Ave. has a 165-foot right-of-way between Eucalyptus Ave. to Ontario Ranch Road. Archibald Ave. will provide north/south access along the eastern portion of the Specific Plan. Per The Ontario Plan (TOP) Figure M-3: Multipurpose Trails and Bikeway Corridor Plan, Archibald Ave. is to include an off-street multi-purpose trail. Access to this street via driveways from residential units and on-street parking will be prohibited. Improvements to Archibald Ave. are illustrated on Exhibit 5-2: Collector Street Sections—Archibald Ave.

Eucalyptus Ave.

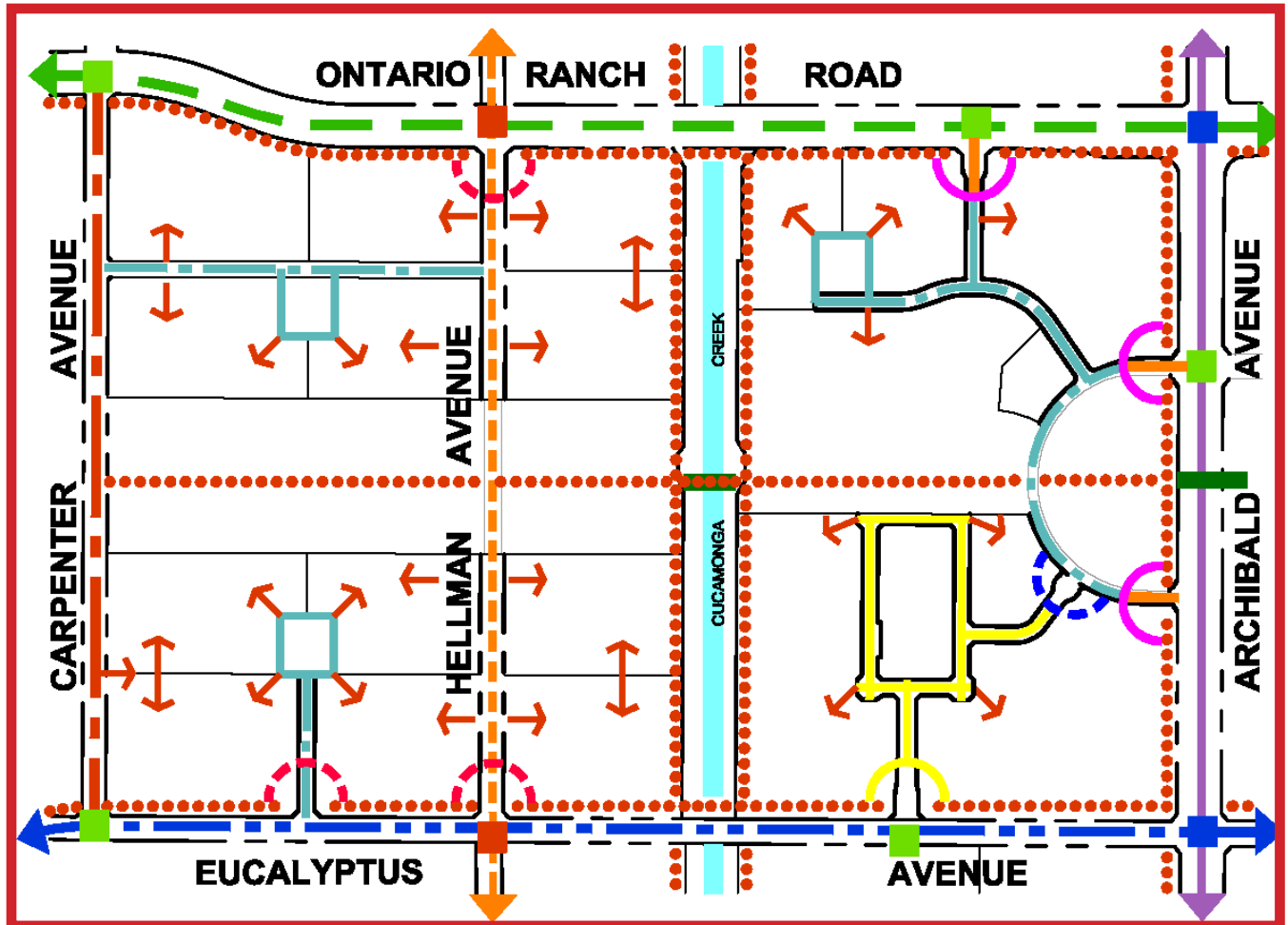
Eucalyptus Ave. is designated as a Collector. Eucalyptus Ave. will provide east/west traffic to and from the project site along the southern boundary of the Specific Plan area. Exhibit ~~11a, Arterial~~ 5-3: Collector Street Sections— ~~Merrill~~— Eucalyptus Ave. illustrates the ultimate improvements to ~~Merrill~~ Eucalyptus Ave. Access to ~~Merrill~~ Eucalyptus Ave. via driveways from residential units and on-street parking will be prohibited.

Hellman Ave.

~~The~~ As a Collector, ~~Street~~ Hellman Ave., is designed to carry less traffic than the ~~standard arterial and the Parkway 1A streets~~ Principal Arterials. Hellman Ave., located within the western half of the project site, will be improved as depicted on Exhibit 5-2: Collector Street Sections—Hellman Ave. Access to this street via driveways from residential units and on-street parking will be prohibited.



Parkside



LEGEND

- | | | | | | |
|---|--|---|--|---|---------------------------------------|
|  | SPECIFIC PLAN BOUNDARY |  | SECONDARY COMMUNITY ENTRY STREETS (60'/72'/76' R/W) |  | PRIMARY COMMUNITY GATEWAY ENTRY |
|  | EXISTING TRAFFIC SIGNAL WITH MODIFICATIONS |  | PRIMARY LOCAL STREETS (36'/48'/60'/64' R/W WITH 36' PAVING) R/W DEPENDS ON ADJACENCY WITH PARK |  | SECONDARY COMMUNITY ENTRY (GATED) |
|  | MASTER PLAN TRAFFIC SIGNAL |  | PRIMARY PRIVATE STREETS (40'/58'/60'/68' R/W) |  | MAJOR COMMUNITY GATEWAY ENTRY (GATED) |
|  | TRAFFIC SIGNAL TO BE INSTALLED WHEN WARRANTED |  | CLASS 1 BIKE PATH (OFF-STREET) |  | SECONDARY COMMUNITY ENTRY |
|  | INTERNAL CIRCULATION POINTS |  | PEDESTRIAN BRIDGE | | |
|  | ARCHIBALD: DIVIDED ARTERIAL PARKWAY 1A WITH CLASS 1 BIKE PATH (165' R/W) | | | | |
|  | ONTARIO RANCH: DIVIDED ARTERIAL PARKWAY 1A (180' R/W) | | | | |
|  | EUCALYPTUS: STANDARD ARTERIAL (108' R/W) | | | | |
|  | HELLMAN: COLLECTOR STREET (88' R/W) | | | | |

Exhibit 5-1: Vehicular Circulation Master Plan



Parkside

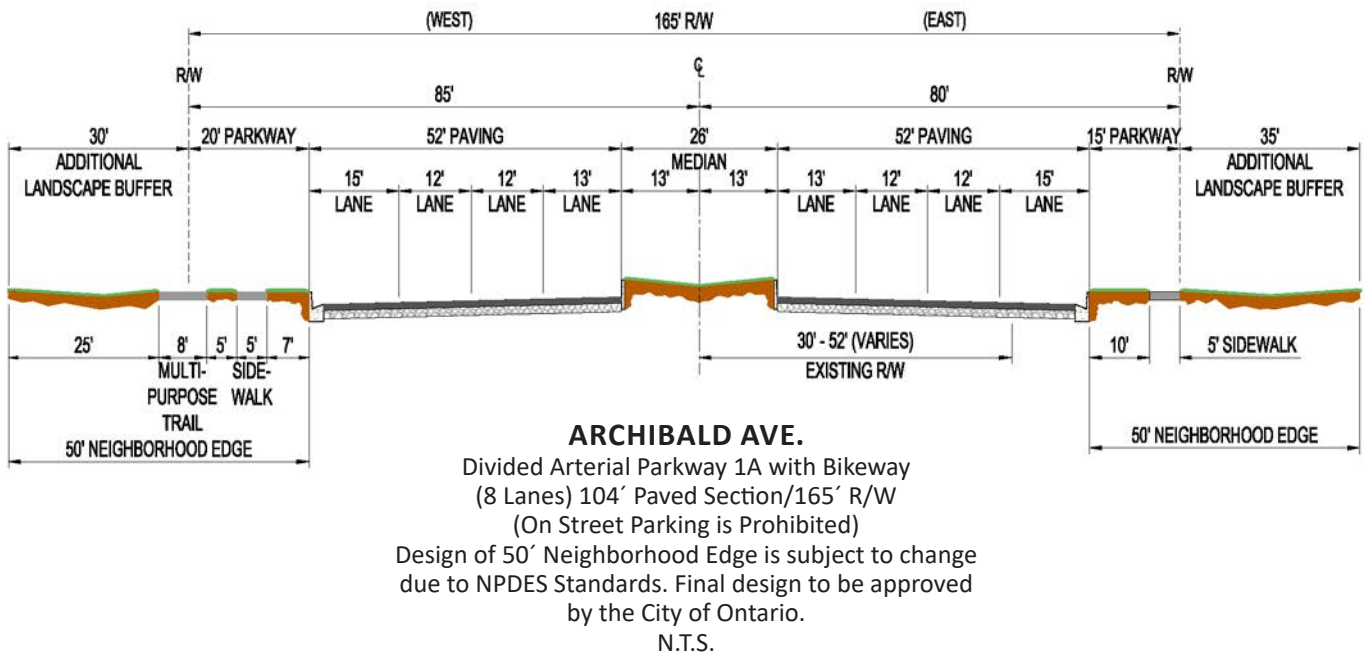
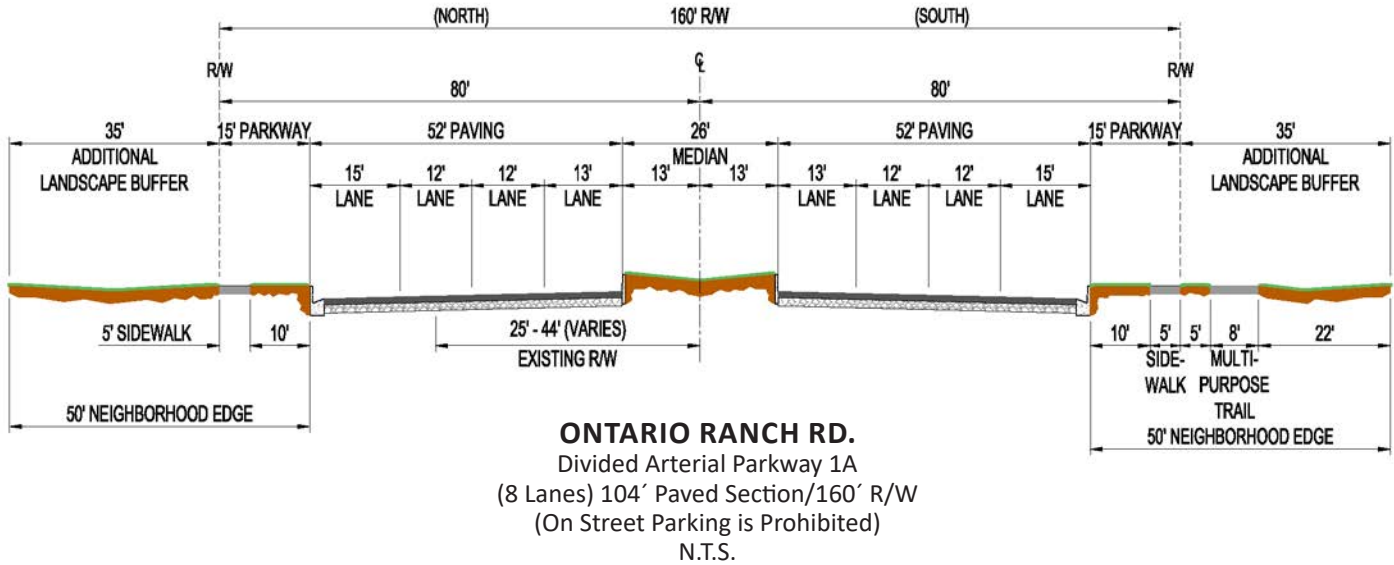
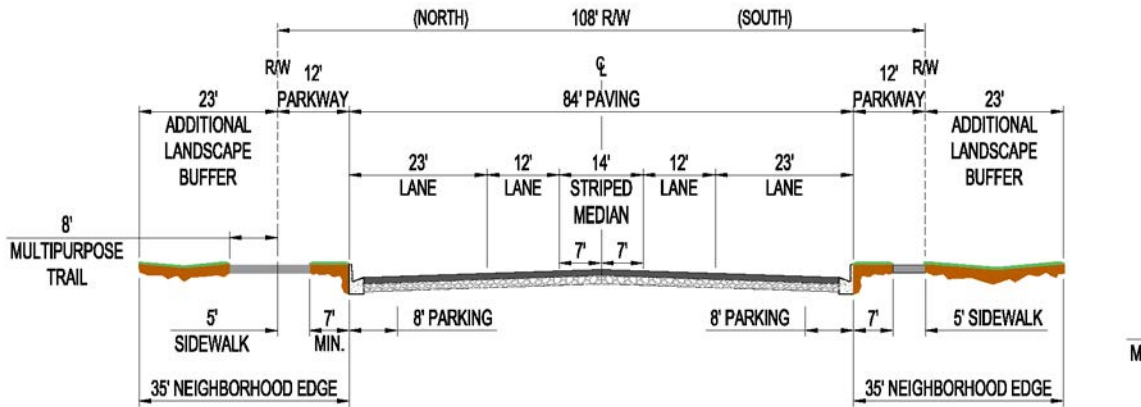


Exhibit 5-2: Principal Arterial Street Sections



Parkside



EUCALYPTUS AVE.

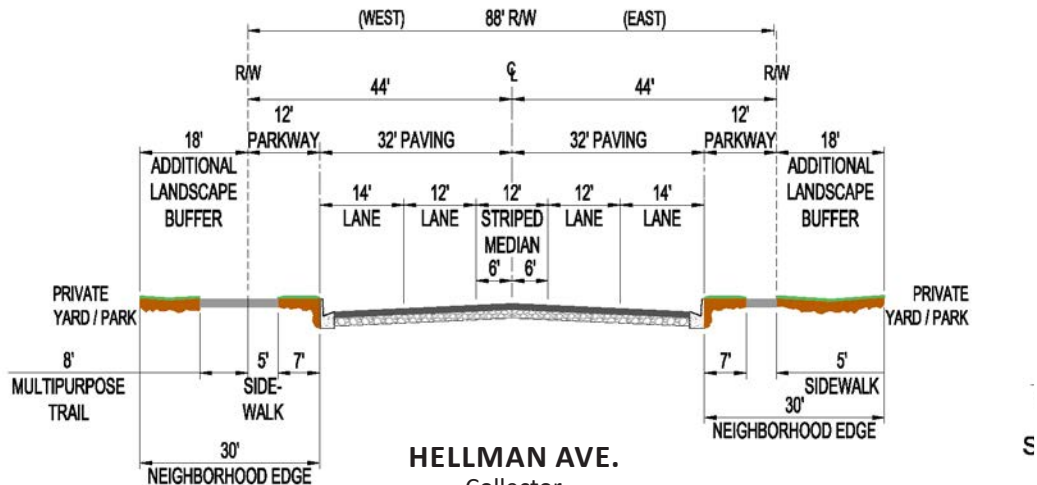
Collector

(4 Lanes) 84' Paved Section/108' R/W

(On Street Parking is Allowed)

(Sidewalk and Bike Path Configuration is Subject to City Approval)

N.T.S.



HELLMAN AVE.

Collector

(2 Lanes) 64' Paved Section/88' R/W

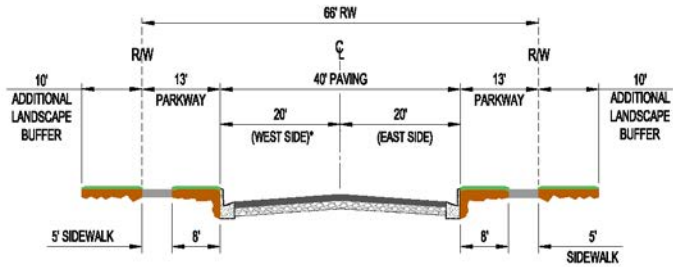
(On Street Parking is Prohibited)

N.T.S.

Exhibit 5-3: Collector Street Sections



Parkside



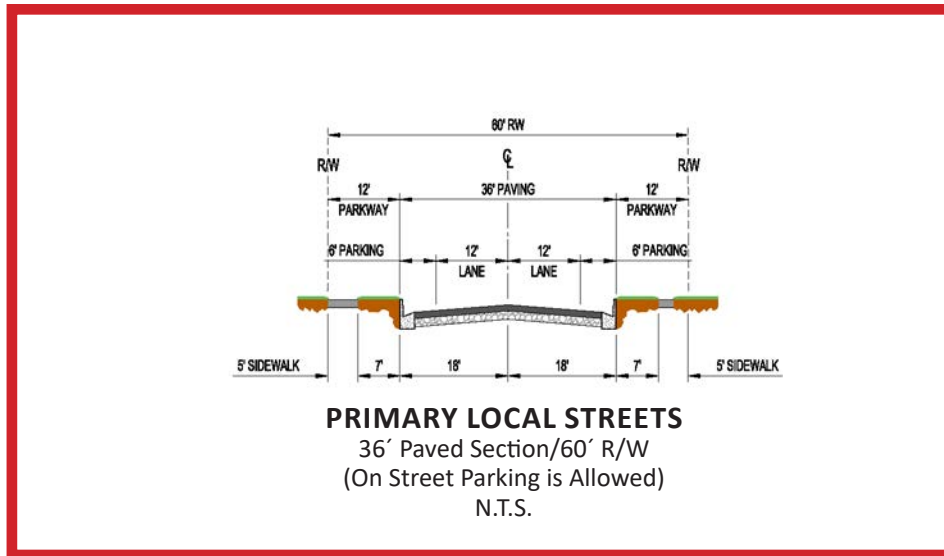
CARPENTER AVE.

40' Paved Section/66' R/W

(On Street Parking is Allowed)

*Finished Parkway Improvements West of Curb and Gutter by Others

N.T.S.



PRIMARY LOCAL STREETS

36' Paved Section/60' R/W

(On Street Parking is Allowed)

N.T.S.

Exhibit 5-4: Local Street Sections



Parkside

Carpenter Ave.

Carpenter Ave., a **Primary Local Street**, bounds the project site to the west and will provide north/south access to and from the Parkside Specific Plan area. Exhibit 11c, **Collector, Primary Residential and Park Entry 5-4: Local Street Sections—Carpenter Ave.** illustrates the ultimate improvements to Carpenter Ave. Easterly improvements will be constructed on Carpenter Ave. by this project.

Primary Residential and Park Entry Local Streets

Each **Primary Residential and Park Entry Street** will include 52 feet of paving with a 12-foot wide raised median. A 12-foot landscaped parkway (both sides) will include a 5-foot sidewalk. Exhibit 11c, **Collector, Primary Residential and Park Entry Street Sections** illustrates the proposed **Primary Residential and Park Entry Streets**.

Local Streets

The **Primary Local Streets** are designated as collector streets for local traffic. The 60-foot right-of-way including is composed of a total paved width of 36 feet and a 12-foot landscaped parkway, which also includes a 5-foot sidewalk (buffered from the street by 7 feet of landscaping) is proposed, as illustrated on). Exhibit 11d **5-4: Local Street Sections** illustrates these improvements.

Secondary Community Entry Streets

Secondary Community Entry Streets have 36, 48 or 52 feet of paving. An optional landscaped median is acceptable subject to City approval. 12-foot landscaped parkways are provided on both sides and may include a 5-foot sidewalk on one or both sides. Exhibit 5-5: **Entry Street Sections** illustrates the proposed **Park Entry Streets**.

The **Local Streets** within the project site will be constructed with a 60-foot right-of-way. The **Local Street Section** is designed to achieve a small neighborhood scale street, internal to the project, which will carry neighborhood related traffic only. The **Local Street section** will have a paving width of 36 feet, and 12-foot landscaped parkways, containing 5-foot sidewalks (buffered from the street by seven (7) feet of landscaping) and one (1) travel lane in each direction, as illustrated on Exhibit 11d, **Local Street Sections**.

One Way Streets at the Recreation Area

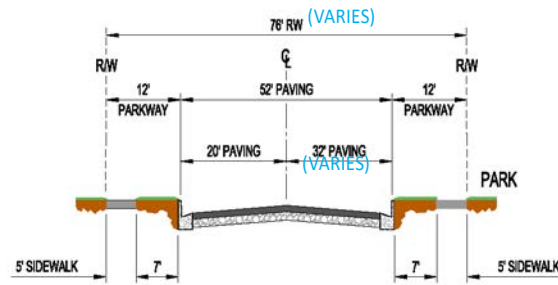
Two (2) of the proposed private recreation areas leading into the “Great Park” will include a one-way street encircling those areas. This street will provide access to the private recreation areas, to residential neighborhoods, and provides a point of pedestrian connection to the “Great Park.” The approaches and departures to and from the loops shall also be private, and designed with flares and splitter islands as per the City’s Roundabout Guidelines to properly direct motorists from the two-way streets into the corresponding one-way portion and vice versa. A plan view and typical section is illustrated on Exhibit 11e, **1-Way Street to Recreation Areas for PA17, 18, 19 and for PA 1, 8, 9, 10**.

Primary Private Streets

Primary Private streets within the Specific Plan will include right of way widths of 40 feet, 58 feet, 60 feet and 68 feet. The 40-foot right of way has a 24-foot paved section, a 5-foot parkway on one side and a 6-foot parkway with 5-foot sidewalk on the other side. Both the 58-foot and 60-foot right of way have 36 feet of pavement. The 58-foot right of way has 6-foot parkways



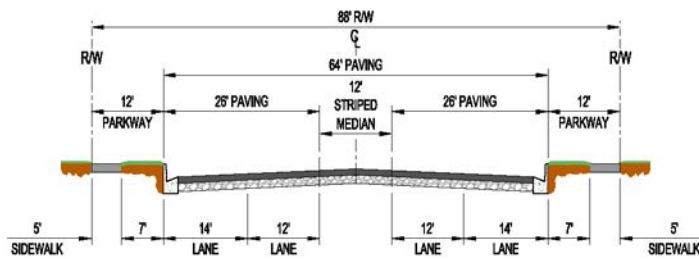
Parkside



SECONDARY COMMUNITY ENTRY STREETS

52' Paved Section/76' R/W
 (On Street Parking is Allowed)
 N.T.S.

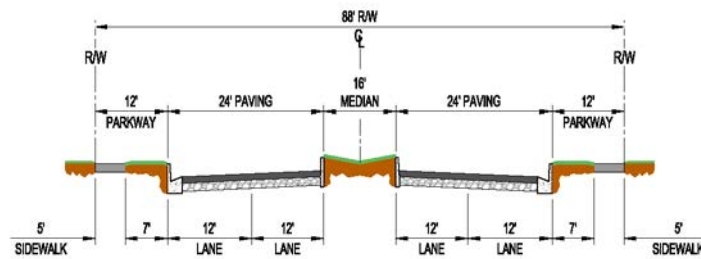
Note: An optional landscaped median is acceptable subject to City approval.



PRIMARY RESIDENTIAL ENTRY STREETS

52' Paved Section/88' R/W
 (On Street Parking is not Allowed)
 N.T.S.

Note: An optional landscaped median is acceptable subject to City approval.



PRIMARY RESIDENTIAL ENTRY STREETS "GATED"

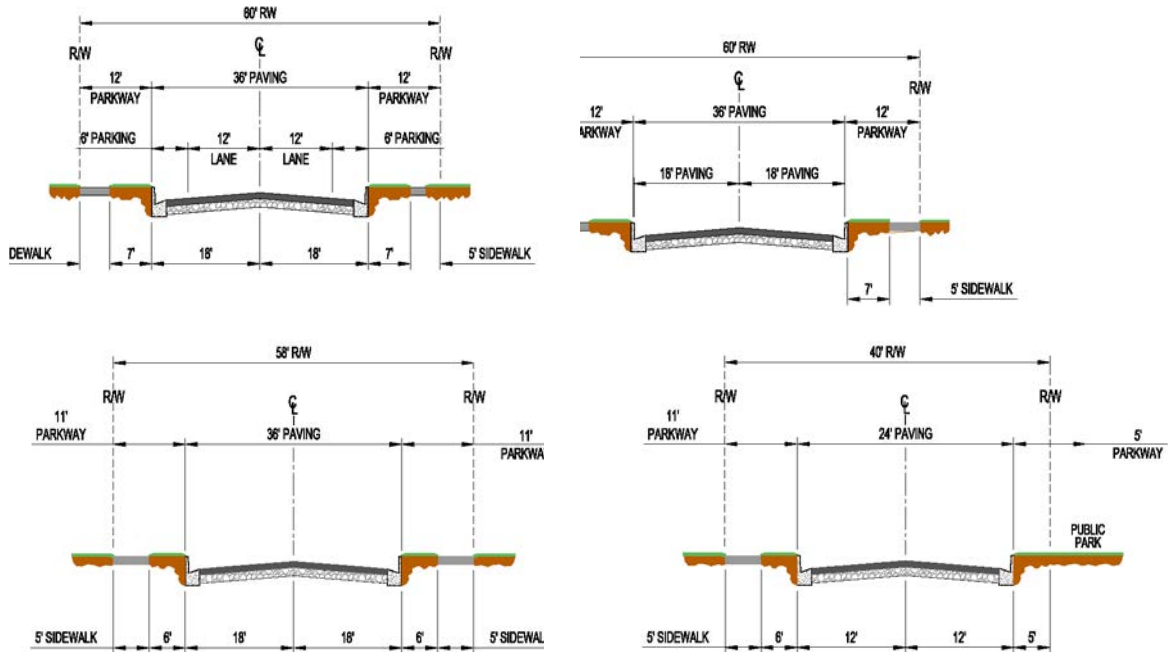
52' Paved Section/88' R/W
 (On Street Parking is not Allowed)
 N.T.S.

Note: An optional landscaped median is acceptable subject to City approval.

Exhibit 5-5: Entry Street Sections



Parkside

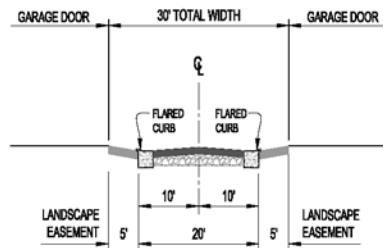


PRIMARY PRIVATE STREETS

24', 34' & 36' Paved Section/40', 58', 60' & 68' R/W
 (On Street Parking is Allowed)
 N.T.S.

Note: All private street design is subject to City Approval.

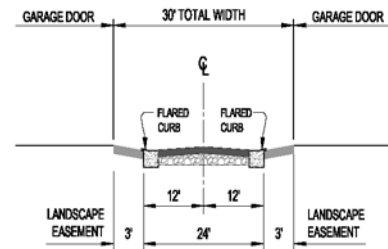
Exhibit 5-6: Primary Private Street Sections



LANES (PRIVATE ALLEYS)

20' Paved Section/30' Width to Garage
 (On Street Parking is Prohibited)
 N.T.S.

Note: Where emergency access alleys are to be used within the residential development, a 24-foot paved section with 3-foot minimum landscaping will be utilized.



LANES (PRIVATE ALLEYS)

24' Paved Section/30' Width to Garage
 (On Street Parking is Prohibited)
 N.T.S.

Exhibit 5-7: Private Lane/Alley Sections



Parkside

and 5-foot sidewalks on each side. The 60-foot right of way has 7-foot parkways and 5-foot sidewalks. Lastly, the 68-foot right of way has a 34-foot paved section, a 10-foot median and 7-foot parkways with 5-foot sidewalks on both sides. On-street parking is allowed on the 58-foot and 60-foot right of ways. Exhibit 5-6 illustrates the Primary Private Street Sections.

In conditions where Private Streets have a gated entry, all entry drives will be designed to City Engineering and Ontario Fire Department standards. Final design of the gated entries will be reviewed during the Development Plan process.

Private Lanes/Alleys

Lanes within the Specific Plan will primarily have 20 feet of paved area with 5 feet of landscaping on each side. Planning Areas 1-4 will have lanes with 24 feet of paved area and 3 feet of landscaping on each side. Exhibit 5-7: Private Lane/Alley Section illustrates the proposed lane sections. Lanes with “dead end” conditions will have a maximum length of 150 feet (subject to Fire Department approval). Parking is prohibited along lanes. Tapers will be incorporated at the point where private lanes intersect public streets. Hammer heads may be incorporated into the design of the lanes if the City determines they are required.

5.1.2 Non-NMC Ontario Ranch Planned Street Condition Along Park/Paseo Uses

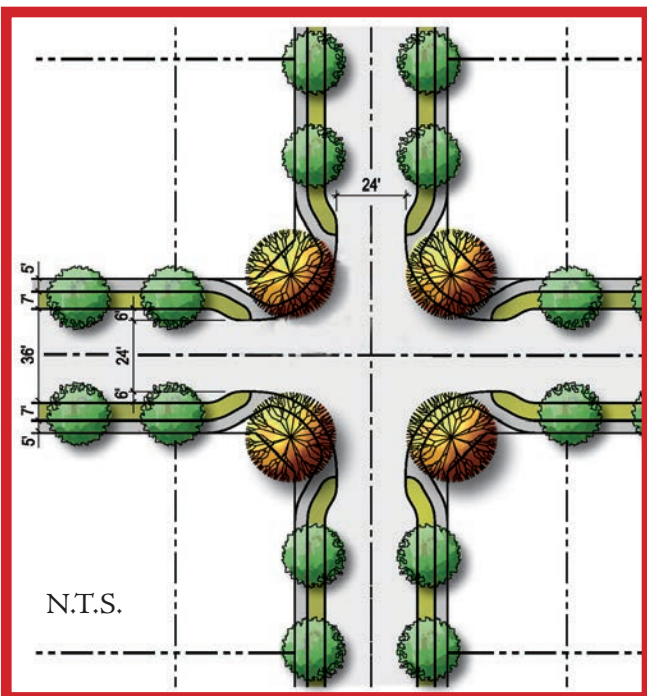
Where any non-New Model Colony Ontario Ranch General Plan Master Planned Street is adjacent to a park, paseo area, or paseo link, the street parkway will be included as park/paseo area/acreage.

5.1.3 Traffic Calming

Parkside provides for traffic calming within residential neighborhoods to slow traffic and reduce traffic noise on streets, contributing to livable neighborhoods in which to walk, bike, and drive.

Traffic calming within the Parkside Specific Plan area is designed to address the following:

- Reduction in traffic speeds;
- Reduction in traffic related noise;
- Reduction in cut-through traffic within residential neighborhoods;
- A secure pedestrian friendly circulation system,

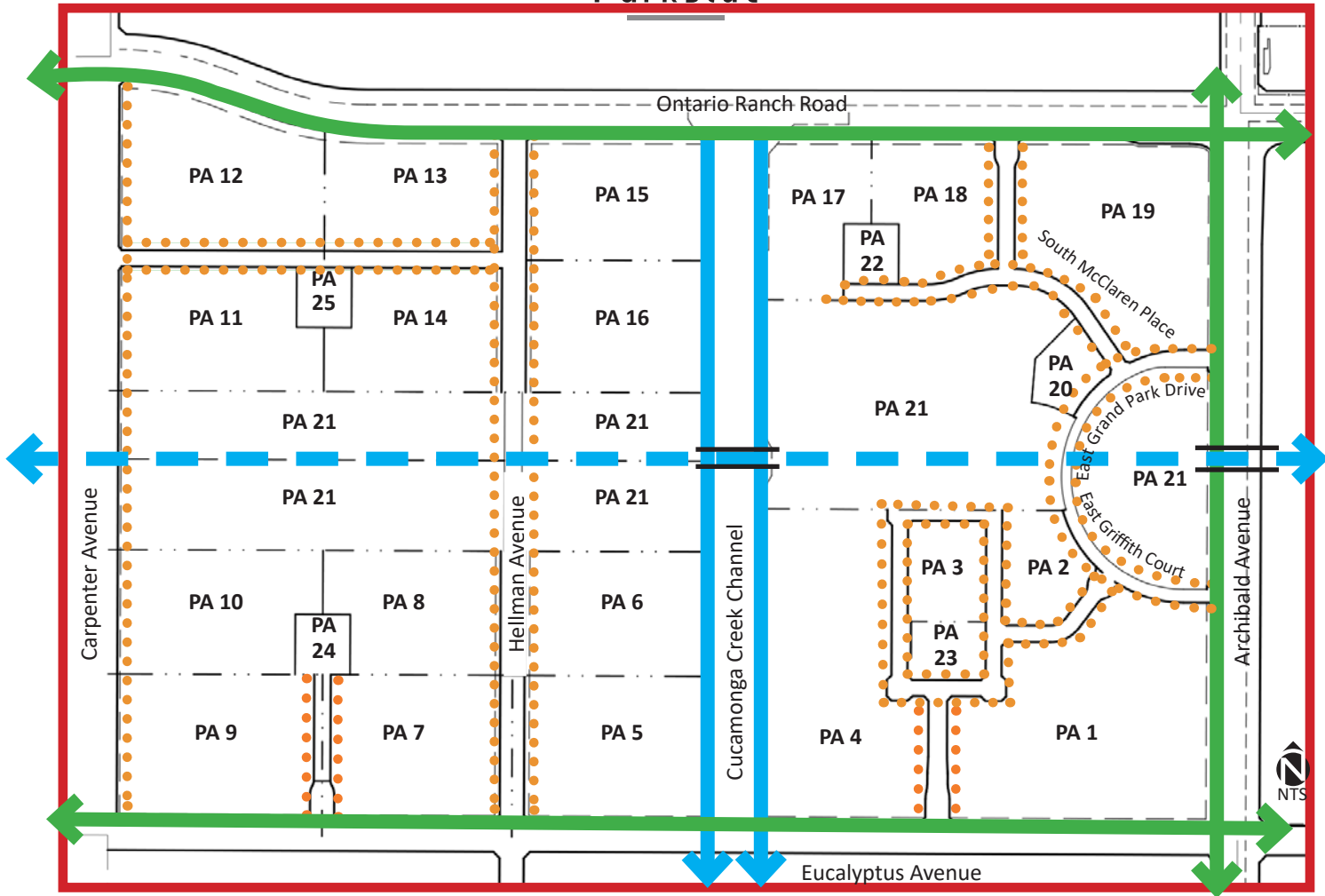






Note: Refer to the City Standard Drawing for Local Street Chokers.

Exhibit 5-8: Neighborhood Street Tapers



Parkside



Legend	
	Multi-purpose Trail (8' wide)
	Cucamonga Creek Multi-purpose Trail (8' wide)
	Grand Park Multi-purpose Trail
	Pedestrian Sidewalks

Notes:

Future Pedestrian Bridges across Archibald Avenue and Cucamonga Creek Channel are proposed

Exhibit 5-9: Pedestrian and Bicycle Circulation Master Plan



Parkside

which encourages walking;

- Allow for non-restricted access for emergency service vehicles such as police, fire, and ambulances; and
- Reduction in radii of streets and elimination of knuckles.

Factors affecting traffic speeds are those that influence the driver's perception of the roadway, such as:

- Type of adjacent development and the distance of development from the roadway;
- Frequency of access points onto the roadway;
- Roadway alignment and curvature;
- Type and massing of landscaping adjacent to the roadway;
- Frequency of traffic control devices along the roadway;
- **Width** of travel lanes; and
- **Roundabout locations.**

The following traffic calming techniques **may** be implemented in the design of the roadways within the Parkside Specific Plan area.

Local **Neighborhood Street Design**

Local Neighborhood streets within Parkside are designed in a grid with ~~landscaped parkways~~ landscaping on either side **within parkways** to add interest in the street, encouraging drivers to slow their travel speed and observe their surroundings. **Incorporating Traffic speeds on local residential streets may be reduced by incorporating** tapered intersections for local streets, as illustrated in Exhibit 5-8: Neighborhood Street Tapers, ~~will reduce traffic speeds on local residential streets.~~ **Taper may not be appropriate at all intersections.**

Lane (Private Alley) Design

Lanes (private alleys) within Parkside ~~will consist of 20 feet of paved travel area with five (5) feet of landscaping on each side, as illustrated in Exhibit 11h, Lane (Private Alley) Tapers.~~ **Incorporating may incorporate** tapers at the entrance to these lanes ~~can reduce speeds to slow traffic in these areas and to provide a visual element to discourage drivers from using lanes as a through street.~~ **Speeds** for traffic entering the lanes ~~as well as~~ and cut-through traffic ~~can be reduced by incorporating tapers at the entrance to these lanes as illustrated on Exhibit 5-8: Neighborhood Street Tapers.~~

Landscaping

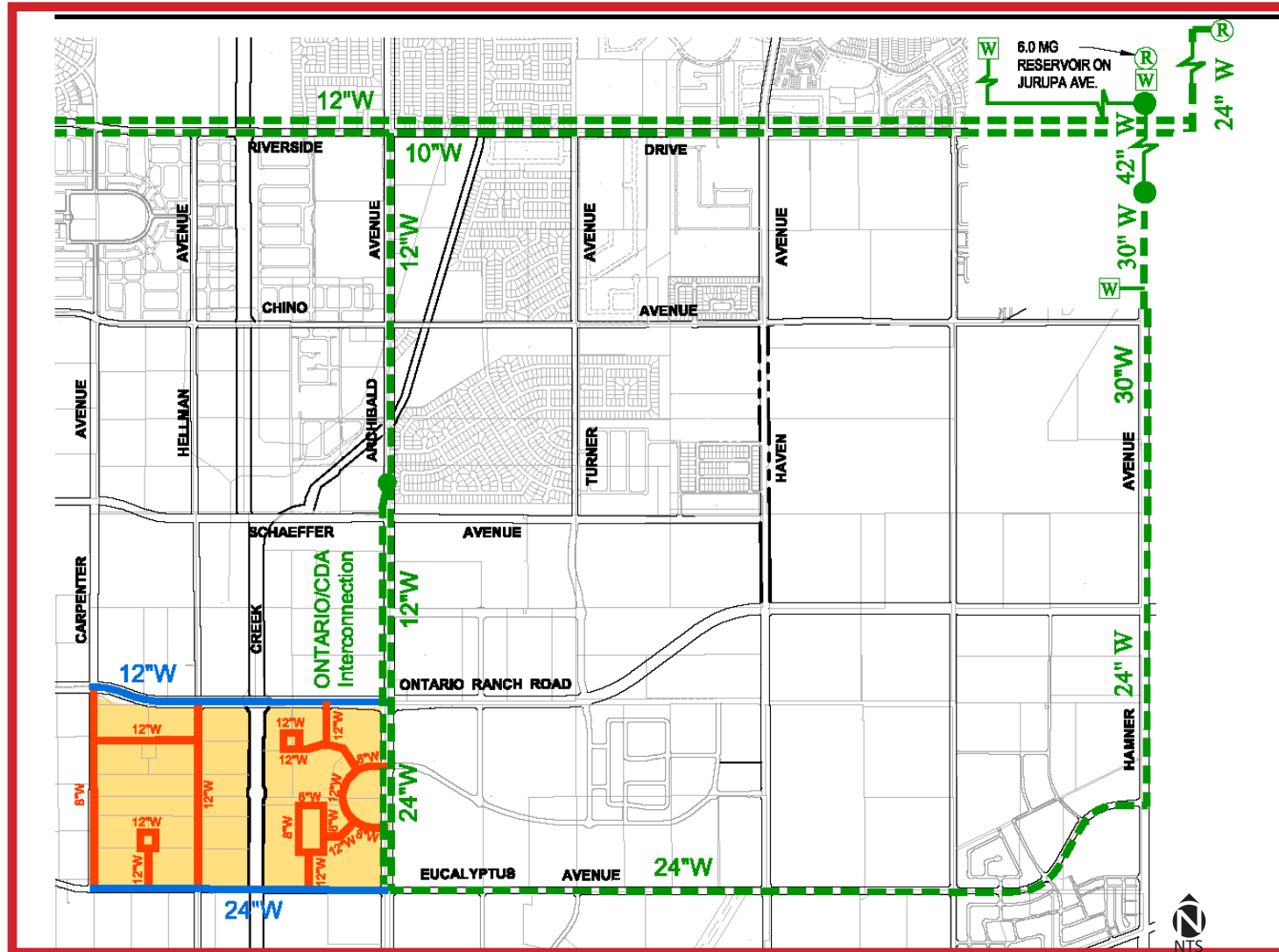
Landscaping adjacent to streets within the **Specific Plan area** will combine the use of shade trees, shrubs, and groundcover adjacent to sidewalks to create a more intimate streetscape, encouraging drivers to reduce driving speeds. The landscape concept for Parkside is designed to contribute a sense that the street system is a pedestrian "protected area" to promote slower traffic speeds. ~~Refer to the Landscape Design Guidelines, Chapter 7 Exhibit 5-8: Neighborhood Street Tapers illustrates these design features.~~

5.1.4 Pedestrian Circulation

Off-street pedestrian circulation will be available throughout the Parkside Specific Plan area ~~by means of with~~ an interconnected paved sidewalk system within the roadway right-of-way, separated from vehicular travel lanes by a landscaped parkway. Pedestrian access points to the Cucamonga Creek Channel will be provided at 2 points in the Great Park, east and west of the channel, and potentially in other locations within the project.



Parkside



Legend	
	Specific Plan Boundary
	NMC Master Plan Water
	NMC Water Supply Phase 1
	Proposed On-Site Water
	Pressure Reducing Station
	Reservoir
	Well

Exhibit 5-10: Conceptual Domestic Water Master Plan



Parkside

5.1.5 Bicycle Plan

~~Bicycle Multi-purpose~~ trails are an integral element in creating accessibility and mobility within the Specific Plan. ~~An eight-foot wide~~ multi-purpose pedestrian and bicycle trail will be provided within ~~the~~ Cucamonga Channel ~~(Area 3 Trail and Area 4 Trail)~~. ~~Class 1 Paths will be provided and~~ within the west parkway of Archibald Ave., within the ~~south~~ parkway of Ontario Ranch Rd. within the north parkway of Eucalyptus Avenue, ~~and within the Great Park. (east to west) and along the west side of the Cucamonga Creek Channel.. These bike paths will provide a link to~~ These ~~multi-purpose trails are identified~~ in the City's ~~master-planned bike paths system. Exhibit 11, Master Plan of Circulation, illustrates the locations of proposed bike paths~~ Policy Plan Multipurpose Trails and Bikeway Corridor Plan. Refer to Exhibit ~~12 5-8: Pedestrian and Bicycle Circulation~~ Master Plan, for locations of these paths and trails.

5.2 Water Master Plans

~~Water and recycled water may be designated as “public utilities” if located within the public or private streets. All public utilities within private streets shall be designed per City standards and contained within acceptable easements. The CC&R’s shall contain language that requires all proposed work within said easements to be plan checked and inspected by the City, including applicable fees. Generally, utilities will not be accepted as public within private alleys, parking areas, or driveways. Utilities within commercial and industrial parking lots and loading areas will be designated as private. The extent to which said utilities will be accepted as public utilities shall be determined, at the full discretion of the City, during final design plan review.~~

The ultimate sizing and alignment of all waterline improvements shall follow the most currently approved Master Plan. A Conceptual Technical Study and Final Technical Study shall be prepared and approved by the City prior to approval of Construction Drawings and Permits.

5.2.1 Domestic and Recycled Water

Master Plan

Domestic water for the project site will be provided by the City. The ~~New Model Colony~~ Ontario Ranch Water Master Plan Phase 1 (Phases 1a–1d) identifies new water facilities to include one reservoir, 3 groundwater wells and potable and recycled transmission water lines. Construction of the on-site and off-site Master Plan water service improvements shall be the responsibility of the developer(s) and is required prior to issuance of building permits (excluding models and private recreation facilities) for Parkside. One of the waterlines included in the Phase I improvements is a 24-inch water main in Archibald Ave., which borders Parkside on the east. The City is in the process of updating the Water Master Plan. Any changes resulting from the update will be incorporated into the Parkside Domestic Water and Recycled Water Master Plan. ~~A hydraulic modeling analysis report is required to demonstrate that the proposed water system will meet peak demands including maximum day plus fire demand and peak hour demand.~~

Master planned domestic and recycled water main lines serving and surrounding the Specific Plan, as identified in the adopted Water Master Plan, shall be constructed prior to issuance of first occupancy ~~On-site public system sizing is subject to the recommendations and approval of the Hydraulic Analysis. of each phase.~~ The sizing and alignment of all in-tract and looping



Parkside

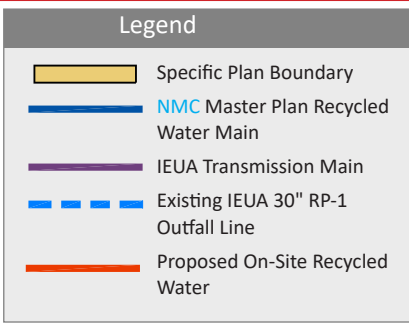
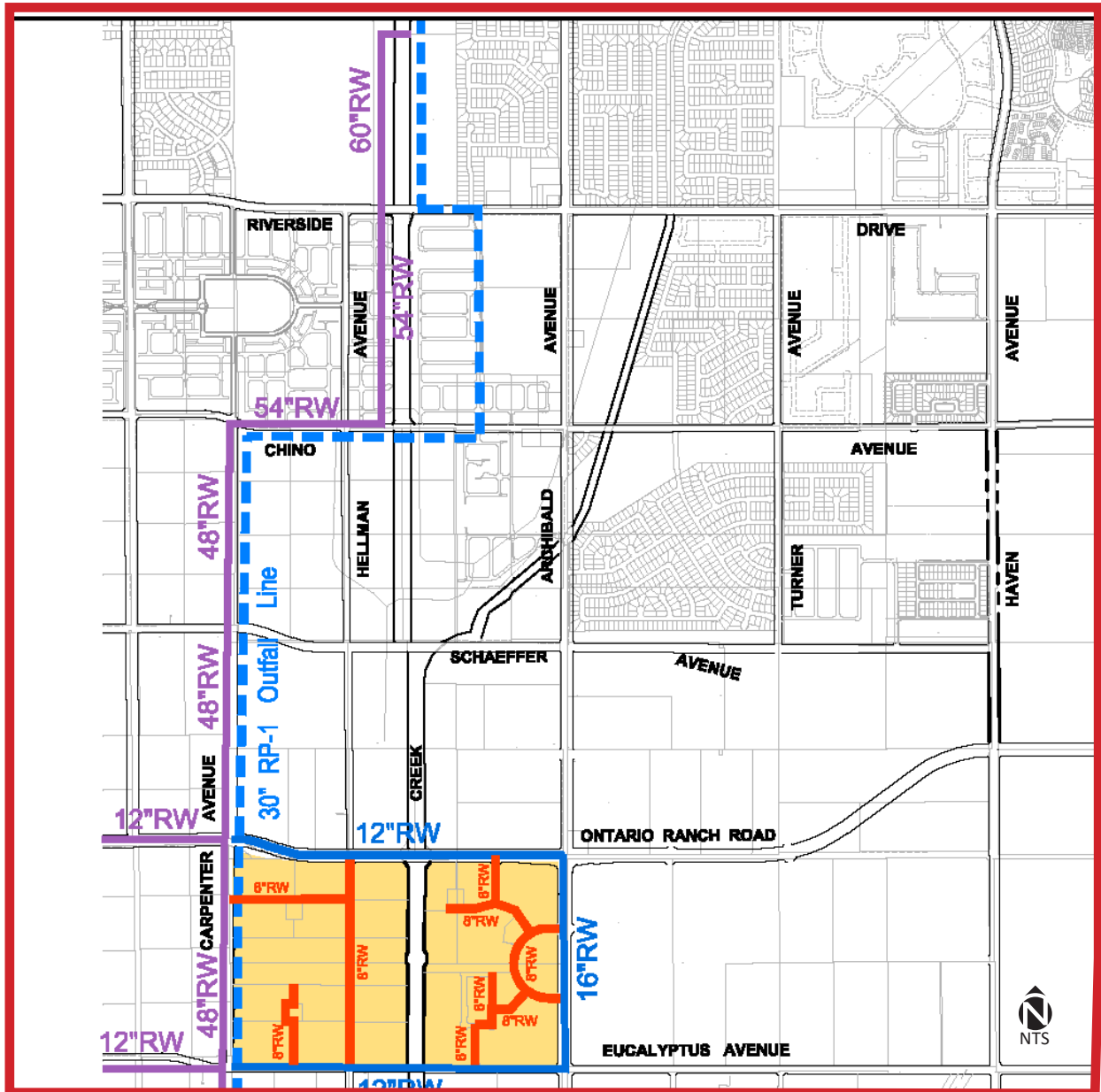


Exhibit 5-11: Conceptual Recycled Water Plan



Parkside

waterline improvements shall follow the results of the approved water hydraulic analysis. The public system location shall be approved during final design review.

5.2.2 Domestic Water

The Parkside Specific Plan area lies within the 925 Pressure Zone. New domestic water mains to be constructed as part of the development of Parkside will include a 24-inch Master Plan water main in Edison Eucalyptus Ave., from Archibald Ave. to Carpenter Ave. Also included is a 12-inch Master Plan water main in Ontario Ranch Rd. from Archibald Ave to Carpenter Ave. ~~Merrill Ave., and in Merrill Ave., from Carpenter Ave. to Archibald Ave. An estimated 8-inch line in Carpenter Ave., from Edison Ave. to Merrill Ave., will be constructed as part~~ The sizing and alignment of all in-tract and looping waterline improvements shall follow the project to complete the loop system. Within the project site, a network results of eight (8) inch water lines will be installed the approved hydraulic analysis.

The Master Plan for domestic water for Parkside is illustrated in Exhibit 13 5-10: Conceptual Domestic Water Master Plan.

5.2.3 Master Planned Recycled

Water System

~~New recycled water lines in conformance with the City's Recycled Water Master Plan (not yet adopted) will be constructed as part of the development of Parkside. The~~ ultimate sizing and alignment of all recycled waterline improvements shall follow the most currently approved Master Plan. In order to assure conformance with Master Plan Design Criteria during the Implementation process: a Conceptual Technical Study and a Final Technical Study shall be prepared and approved by the City prior to approval of Construction Drawings and Permits. The source of recycled water is the Inland Empire Utility Agency (IEUA) 30-inch RP-1 Outfall Line in Carpenter Ave. There is an existing 16-inch recycled water line in Archibald Ave., from Ontario Ranch Rd. to Eucalyptus Ave. There is a proposed IEUA 48-inch recycled transmission main in Carpenter Ave. New recycled master planned water mains to be constructed as part of the Parkside development will include a 12-inch line in Edison Ontario Ranch Rd., from Carpenter Ave. to Archibald Ave., a 12-inch line in Merrill Eucalyptus Ave., from Carpenter Ave. to Archibald Ave., ~~a 16-inch line in Archibald Ave., from Edison Ave. to Merrill Ave.,~~ and an 8-inch line in Hellman Ave., ~~from Edison Ave. to Merrill Ave.~~ On-site from Ontario Ranch Rd. to Eucalyptus Ave. The sizing and alignment of all in-tract and looping recycled waterline improvements shall follow the results of the approved recycled water lines will be installed as required by the City Engineer and/or by the Recycled Water Master Plan, ~~yet to be adopted by the City of Ontario, to provide irrigation for public landscape areas, such as parkways, buffer areas and parks~~ hydraulic analysis. The Parkside Specific Plan shall comply with ~~City Ordinance 2689~~ Ontario Municipal Code Chapter 8C (Recycled



Parkside

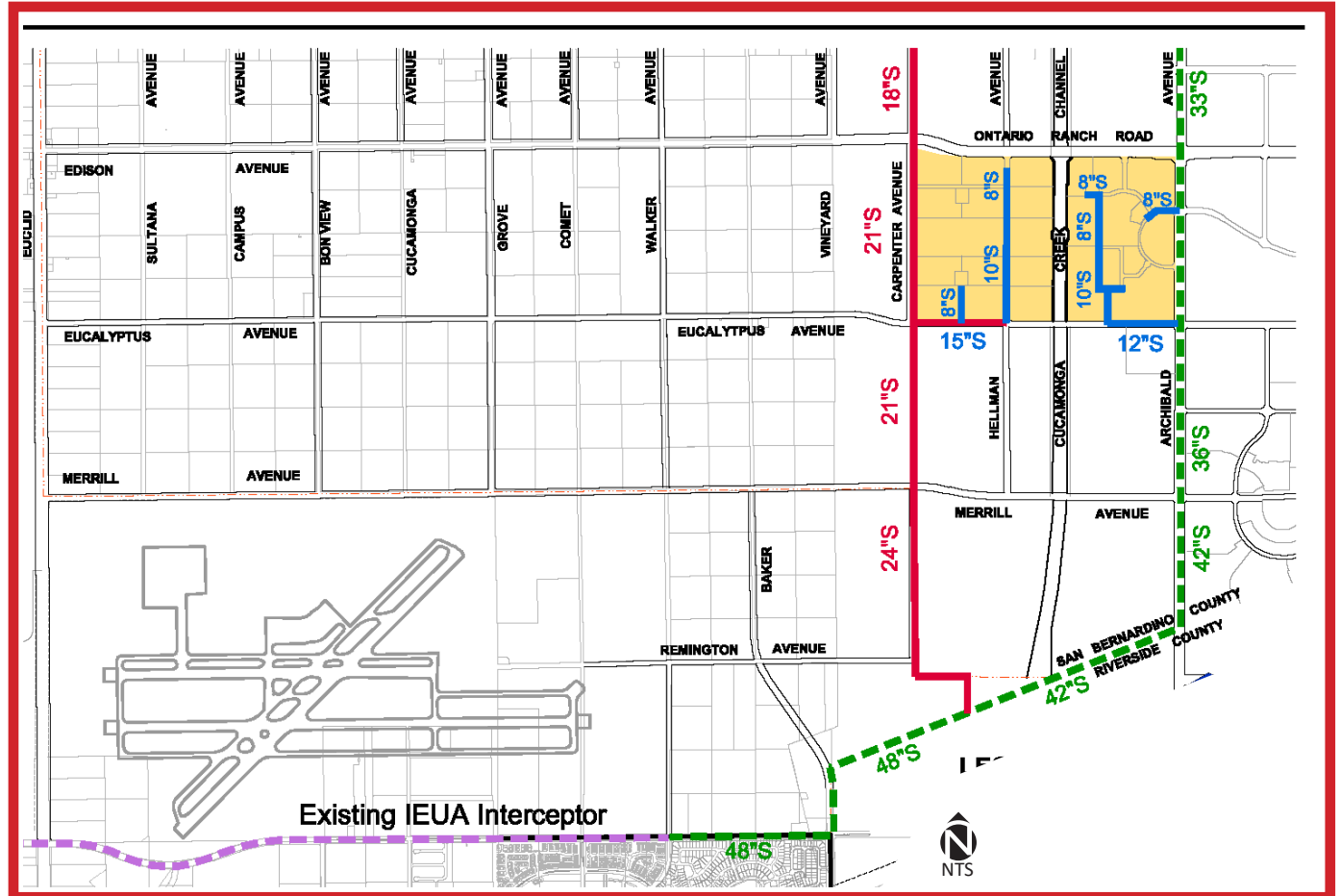







Exhibit 5-12: Conceptual Sewer Master Plan

Legend	
	Specific Plan Boundary
	NMC Master Planned Sewer Main
	Eastern Trunk Sewer
	Proposed On Site Sewer
	Existing IEUA Interceptor



Parkside

Water Use) and provide recycled water for all approved uses, including but not limited to irrigation of parks, street landscaping, recreational trails, HOA-maintained on-site common areas and commercial landscaping. An Engineer's Report approved by the City and the Department of Health Services is required prior to the use of recycled water. ~~The on-site recycled water system shall be subject to the approval of the City of Ontario.~~

~~There may be an interim period where recycled water supply does not have adequate pressure depending on the timing of development and construction of a regional booster station. In this case, the recycled water system will connect to the potable system initially, until such point that recycled water service is available.~~

The Master Plan for recycled water for Parkside is illustrated on Exhibit 5-11: Conceptual Recycled Water Plan. ~~On-site recycled water usage is illustrated on Exhibit 14a, Conceptual Recycled Water Usage.~~

5.3 Sewer Master Plan

~~Sewer utilities may be designated as "public utilities" if located within public or private streets. Reference Section 5.2, Water Master Plans.~~

The ultimate sizing and alignment of all sewer improvements shall follow the most currently approved Master Plan. In order to assure conformance with Master Plan Design Criteria during the Implementation process: a Conceptual Technical Study and a Final Technical Study shall be prepared and approved by the City prior to approval of Construction Drawings and Permits. Sewer service for the ~~project site~~ Specific Plan area will be provided by the City. The City's Sewer Master Plan identifies ultimate sewer facilities from the project site

to the Eastern Trunk Sewer. Master Planned facilities within the project area include the Eastern trunk Sewer Main in Archibald Ave. ~~will that serves~~ that portion of the project site lying easterly of the Cucamonga Creek Channel. The portion of Specific Plan area lying westerly of the Cucamonga Creek Channel will be served by the Eastern Trunk Sewer via the Master Planned sewer main connecting to the Eastern Trunk Sewer near the intersection of Carpenter Avenue and the San Bernardino County Line. ~~These Master Planned facilities include a 36-inch main in Vineyard Ave., from realigned Merrill Ave. (old Eucalyptus Ave.) to existing Merrill Ave.; a 12-inch main in Merrill Ave. to Carpenter Ave.; and an 18-inch main in Carpenter Ave, from Merrill Ave. to the Eastern Trunk Sewer. Completion of these Master Plan improvements is required to provide the ultimate sewer service to the Parkside Specific Plan area. Within the project site a network of 8-inch, 10-inch, and 12-inch sewer lines will be installed. Master planned sewer main lines serving and surrounding~~ This line connects to the existing IEUA Intercept line at Walker and Kimball Ave in the city of Chino.

The sizing and alignment of all in-tract sewer improvements shall follow the results of the approved Sewer Sub-Area Master Plan (SSAMP) analysis. The master planned sewer main line serving the western portion of the Specific Plan, as identified in adopted updated Sewer Master Plan, shall be constructed prior to issuance of first occupancy of the phases west of the Cucamonga Creek Channel. On-site public system sizing is subject to the recommendations and approval of the ~~Hydraulic Analysis. Location of public system shall be approved during final design review. The City is currently~~



Parkside

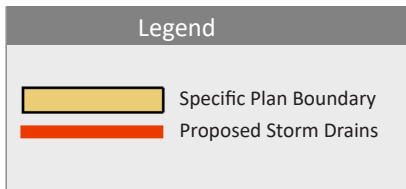
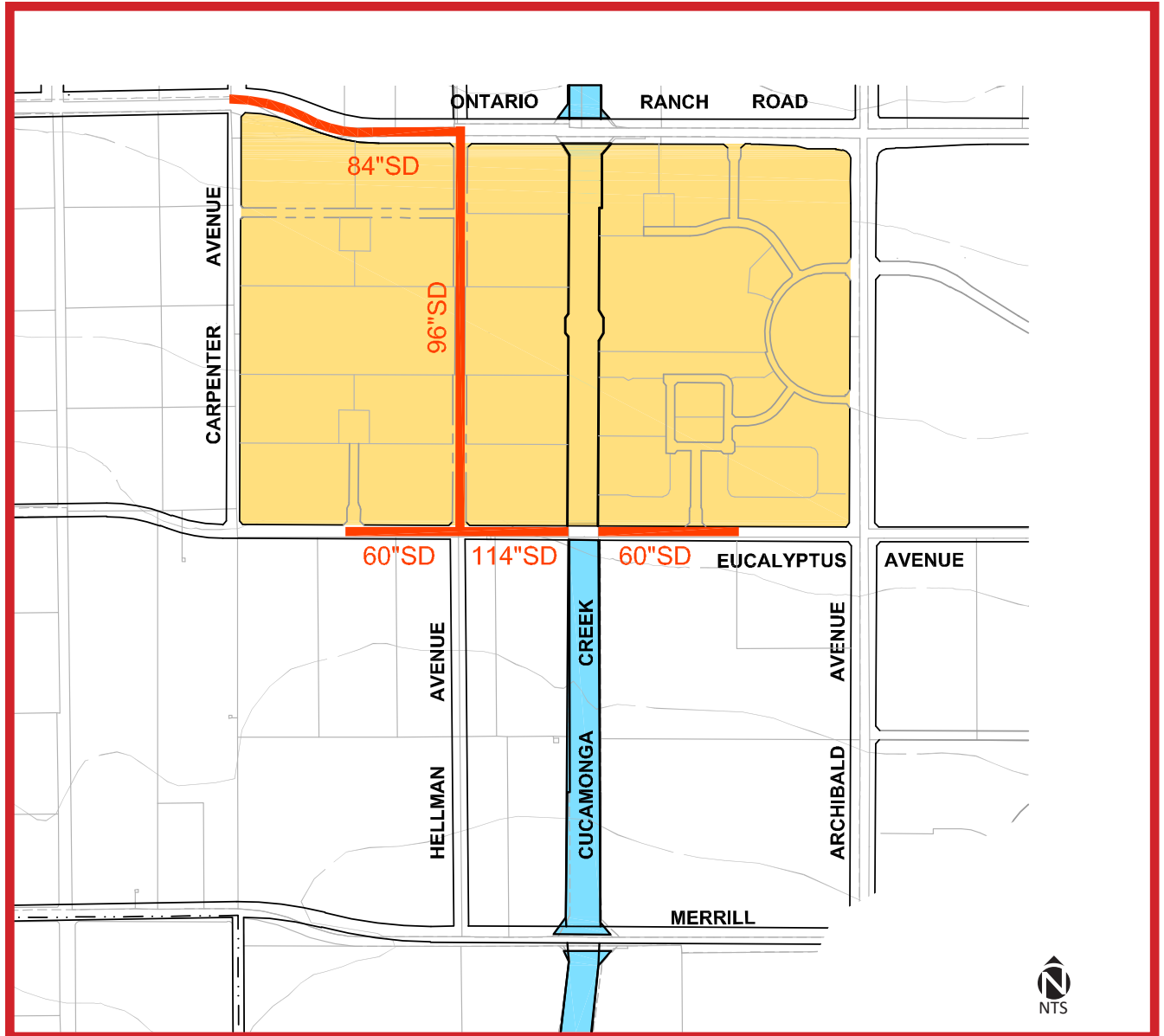


Exhibit 5-13: Conceptual Storm Drain Master Plan



Parkside

updating the Sewer Master Plan. Any changes resulting from the review will be incorporated into the Parkside Sewer Master Plan required sewer analysis.

The Sewer Master Plan for Parkside is illustrated on Exhibit 15 5-12: Conceptual Sewer Master Plan.

5.4 Drainage

All pipe, improvements, sizing, and alignment shall follow the most current approved version of the Storm Drain Master Plan (currently 2012), in case there are discrepancies between pipe improvements, sizing, and/or alignments as show on the utility exhibits versus the Master Plan. The City's Storm Drain Master Plan identifies new storm drain facilities to serve the project site extending northerly from towards the Cucamonga Creek Channel. These Master Plan improvements include a 84-inch storm drain in Ontario Ranch Road from Carpenter to Hellman Ave., a 96-inch storm drain in Hellman Ave., a 60-inch storm drain in Eucalyptus Ave., east of the Cucamonga Creek Channel and a 42-inch storm drain in Merrill Ave. west of Hellman Ave., a 114-inch storm drain in Eucalyptus Ave., between Hellman Ave. and the Cucamonga Creek Channel. Completion of these Master Plan improvements will provide permanent storm drain service to Parkside.

On-site storm drains will be constructed to convey the on-site flows to the proposed Master Planned storm drain lines in Merrill Eucalyptus Ave. Size and location of proposed storm drain may change based on the final design.

The Drainage Master Plan for Parkside is illustrated in Exhibit 5-12: Conceptual Storm Drain Master Plan.

5.4.1 NPDES Compliance

The grading and drainage of the Parkside Specific Plan area shall be designed to detain, filter, and treat surface runoff, in a manner and combination which is practical, to comply with the most recent requirements of the San Bernardino County NPDES Stormwater Program's Water Quality Management (WQMP) for significant new development projects. The objective of the WQMP for this project is to minimize the detrimental effects of urbanization on the beneficial uses of receiving waters, including effects caused by increased pollutants and changes in hydrology. These effects may be minimized through the implementation of site designs that reduce runoff and pollutant transport by minimizing impervious surfaces and maximizing on-site infiltration, Source Control Best Management Practices (BMPs) and/or either on-site structural Treatment Control BMP's, or participation in regional or watershed-based Treatment Control BMPs.

Prior to the issuance of a grading or construction permit, a Storm Water Pollution Prevention Plan (SWPPP) will also be prepared. The SWPPP will be prepared to comply with the California State Water Resources Control Board's (State Water Board) current, "General Permit to Discharge Storm Water Associated With Construction Activity" and the current Areawide Urban Storm Water Runoff (Regional NPDES Permit). The SWPPP will identify and detail all appropriate Best Management Practices (BMPs) to be implemented or installed during construction of the project.

In addition to the preparation of a SWPPP for construction-related activities, and as part of the approval of any



Parkside

grading plans within the Specific Plan Area, the applicant will be required to submit a Water Quality Management Plan (WQMP) on the regional model form provided by the City. The WQMP shall identify and detail all Site Design, Source Control, and Treatment Control BMPs to be implemented or installed at this site in order to reduce storm water pollutants and site runoff. All Priority Land Use (PLU) areas within the Specific Plan Area shall comply with the statewide Trash Provisions adopted by the State Water Resources Control Board (SWRCB) and trash requirements in the most current San Bernardino County Area-Wide MS4 Permit. Drainage from such as a Debris Separating Baffle Box (DSBB), Continuous Deflective System (CDS), or an equivalent hydrodynamic separator which as been approved by the SWRCB.

~~A proposed~~The Specific Plan area is located within the sub-watershed area of a regional storm water runoff treatment facility ~~for the sub-watershed area that this project lies within is being considered for construction.~~ This regional treatment facility ~~would be part of an overall solution for storm water treatment. If an approved regional storm water treatment facility is constructed, it may~~ serves as an alternative to complete on-site treatment of all pollutants of concern. ~~If the regional storm water treatment facility is not completed and operational prior to construction of this project, all necessary on-site treatment control BMPs and/or temporary water quality devices will be installed pursuant to the requirements of the current regional NPDES Permit and the approved Water Quality Management Plan for this project.~~

5.5 Public Services

5.5.1 Schools

The project site is located within the Mountain View School District, which serves the needs for kindergarten through eighth grade, and the Chaffey Joint Union High School District, serving the needs for grades 9 through 12. Several elementary, middle, and high schools are master planned throughout the ~~New Model Colony Ontario Ranch~~ and will ultimately serve the Parkside Specific Plan area educational demands. Development of the Parkside Specific Plan area will generate an estimated student population as follows (based on student generation numbers supplied by the City of ~~Ontario~~, "School Generation for New Model Colony Subareas," July 2, 2004):

Grades K-5 Generation Factor .38/D.U.0.38 x 1,947 = 740

Grades 6-8 Generation Factor .22/D.U.0.22 x 1,947 = 429

Grades 9-12 Generation Factor .20/D.U.0.20 x 1,947 = 390

5.5.2 Fire Department Services

The City of ~~Ontario~~ provides fire protection for the project site. The nearest fire station (2931 East Philadelphia Street) is approximately 2.5 miles northeasterly of the project site. Response time varies due to availability of units, according to Fire Department statements. A new fire station has been identified within the Parkside Specific Plan area, directly west of the commercial site, in Planning Area 20, which will service the Specific Plan area as well as the surrounding community. Refer to Exhibit ~~10~~ 4-1, Land Use Plan.



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5.5.3 Police Department Services

The City of Ontario will provide police protection for the Specific Plan area. The police station (2500 Archibald Ave.) was relocated to be more centrally located to the New Model Colony Ontario Ranch and is approximately 2.2 miles north of the project site. Response time varies based on location of police units and the type of calls, according to Police Department statements.

5.5.4 Libraries

The City of Ontario provides library service to the City. There are two existing library locations within the City of Ontario. The main library is located within the vicinity of the Civic Center (215 East “D” Street). The Colony Branch Library is located northeasterly of the project site, approximately 2.75 miles away, and can be reached via Archibald Ave. and Riverside Drive, within Colony High School (3859 East Riverside Drive). Both libraries will serve the project site. There will be a need for additional facilities over time. Currently, the City of Ontario has not planned for a library within the Parkside Specific Plan area.

5.5.5 Public Transportation

Omnitrans currently provides fixed-route bus and “Access” services within the vicinity of the project site. Omnitrans’ Fixed Bus Route 70 currently operates along Riverside Drive, between Vineyard and Milliken Ave., and is approximately 1.5 miles north of the project site via Archibald Ave. The project site is also served by the “Access” demand-response service for qualified persons with disabilities. The City has indicated that there is currently a study underway to determine the additional public transportation needs of the Ontario Ranch.

5.6 Public Utilities

Public utilities including telecommunications telephone, gas, and electrical service and solid waste collection will be available to the project site Specific Plan area and will be extended into the planned community as part of the development of the Parkside Specific Plan. All existing overhead utilities shall be placed underground, where feasible underground, including all on-site utilities and those within the public right-of-way along streets fronting the project site. Within the Specific Plan area, all new utilities will be placed underground.

5.6.1 Telecommunications

The City of Ontario will be providing OntarioNet, fiber-to-the-home. OntarioNet is a fiber-optic telecommunications system capable of providing advanced Internet/data services to all homes and businesses within the New Model Colony. OntarioNet will provide community related services including: traffic management; on-line civic services; meter reading; educational services; and a variety of other community services. Based on the demographics of the New Model Colony and the traffic issues related to traffic flows going into Orange and Los Angeles Counties, the demand for telecommuting in the New Model Colony is anticipated to be significant. OntarioNet and the high-speed data services it provides will allow residents of the Parkside Specific Plan area to effectively telecommute to their jobs and, in general, provide a significant economic benefit to the City of Ontario. Verizon currently provides telephone service within the project site and can provide telecommunication service if they choose to do so.

Proposed Fiber Optics (conduits and fiber) will be placed underground within a duct and structure system to be



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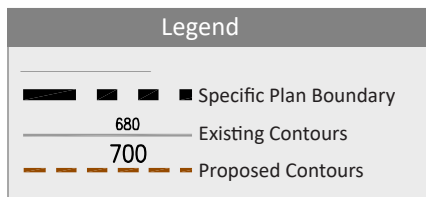
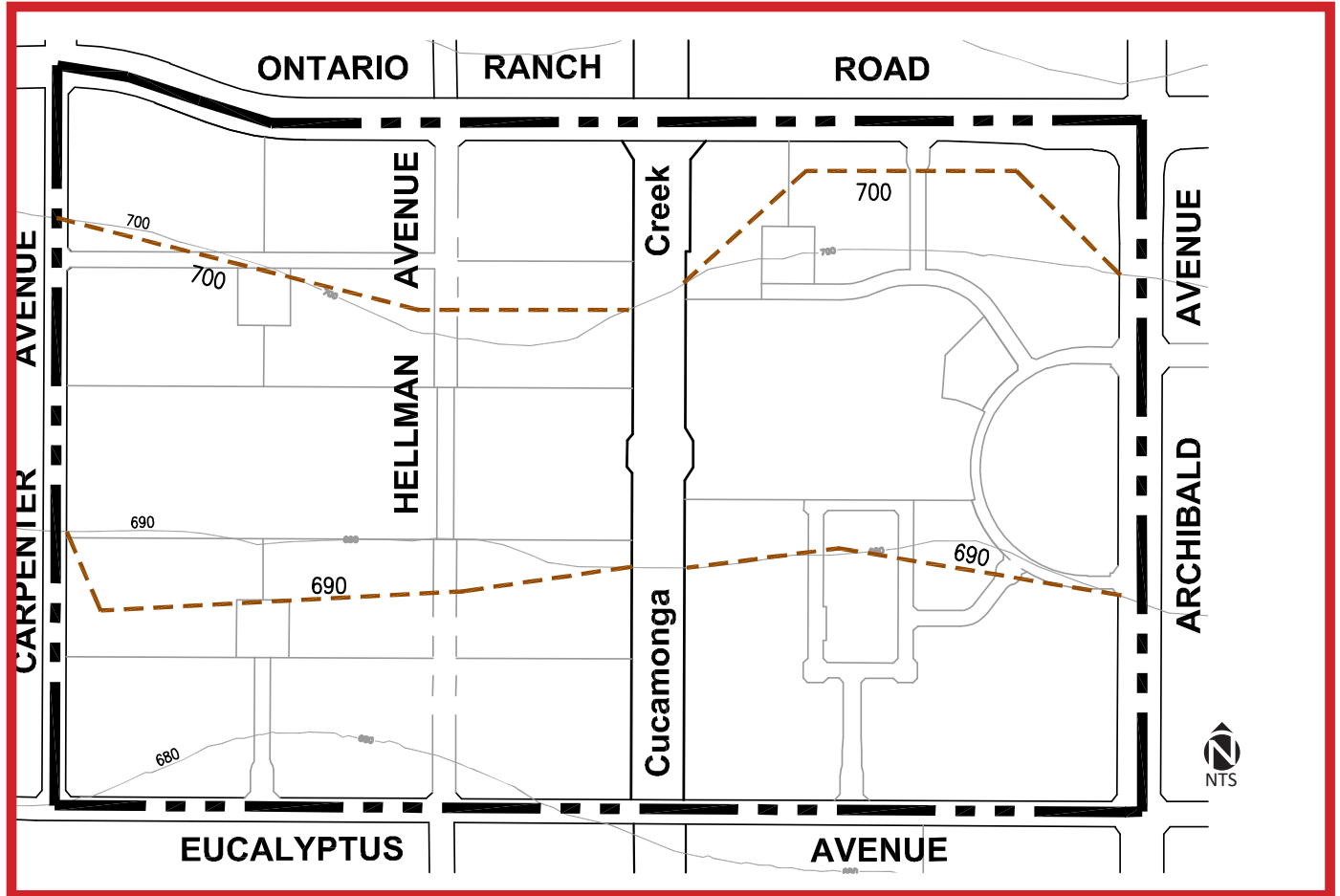


Exhibit 5-14: Conceptual Grading Plan



Parkside

installed by the Developer. The fiber optic conduits along the backbone streets shall be installed in a joint trench by each Developer as the last lane improvements are completed. In-tract fiber optic conduits shall be installed by the Developer in joint trenches with City Street Light conduits where possible. Maintenance of the installed system will be the responsibility of the City/Special District. Development of the Project requires the installation by the Developer of all fiber optic conduits and pullboxes as well as payment of Development Impact Fees to cover the installation of the fiber optic cable.

5.6.2 Natural Gas

The Southern California Gas Company (SCG) provides natural gas service within the project site. Facilities within the area include an existing 8-inch line, reducing to 6 inches, along Archibald Ave., a 6-inch line and a 36-inch transmission line (located on the south side of ~~Edison Ave~~ Ontario Ranch Rd.) along ~~Edison Ave~~ Ontario Ranch Rd.

The Gas Company will provide natural gas to the project site. SCG indicates that gas service could be provided to the Parkside Specific Plan area, based upon conditions of gas supply, regulatory agencies and in accordance with Southern California Gas Company's policies and extension rules on file with the California Public Utilities Commission at the time contractual arrangements are made.

5.6.3 Electricity

Southern California Edison Company (SCE) currently provides electrical service in the area. All new lines installed within the Parkside Specific Plan area, less than 66 KV, shall be installed underground. Additionally, existing lines within the Specific Plan area, less than 34.5

KV, shall also be placed underground by the developer and according to the City of Ontario Adopted Ordinance per OMC Chapter 5 (Underground Public Utility Districts).

5.7 Solid Integrated Waste

~~The City of~~ For all residential development, refuse and recyclable materials shall be stored as provided in Ontario Ontario provides solid waste collection services for the City and will service the Parkside Specific Plan area.

~~a. Commercial — Developer shall comply with Municipal Sec. 6-3.314 Commercial Sec. 6-3.314 Commercial Section 6.01.010F.4. (Refuse Storage) Standards, and Sec. 6-3.601 Business Recycling Plan.~~

~~b. Apartment — Areas). For apartments using all commercial bin service developer development, refuse and recyclable materials shall comply with Municipal be stored as provided in Ontario Development Code Sec. 6-3.314 Commercial Section 6.01.015E.17. (Refuse Storage Standards and Sec. 6-3.601 Business Recycling Plan. Areas (Trash Enclosures)).~~

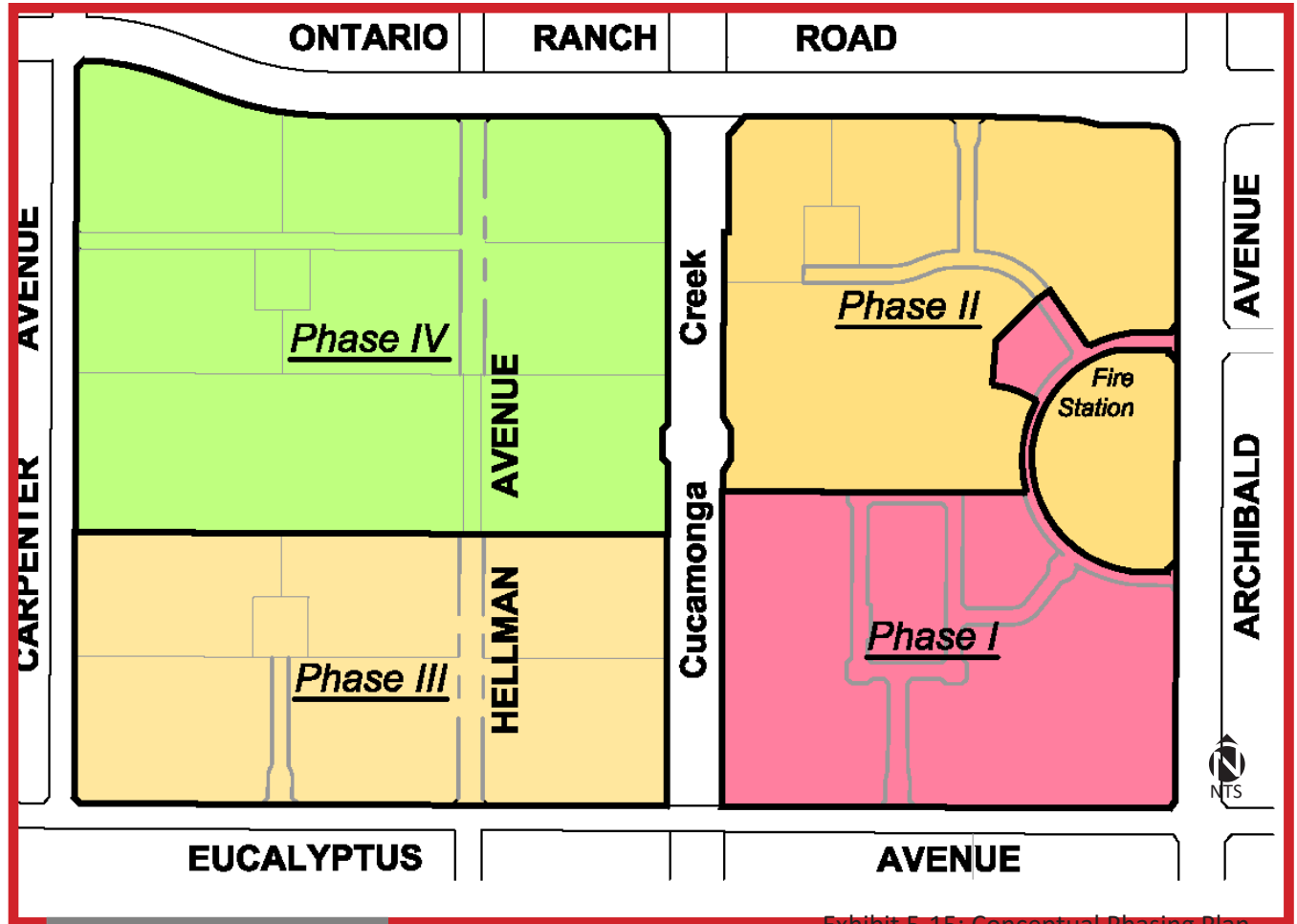
~~c. Residential — For curbside automated container service developer shall comply with Municipal Code Sec. 6-3.308.9(a) and (d), Residential Receptacles, Placement.~~

~~d. Recycling Requirements — Developer shall comply with Municipal Code Article 6. Recycling Requirements for Specified Business Activity, Sec. 6-3.601 Business Recycling Plan, and Sec. 6-3.602 Construction and Demolition Recycling Plan.~~

~~e. Site Improvement Plans shall follow the City of Ontario refuse collections standards~~



Parkside



Legend	
■	Phase I
■	Phase II
■	Phase III
■	Phase IV

Exhibit 5-15: Conceptual Phasing Plan



Parkside

~~Development Code Section 6.01.010F.4. (Refuse Storage Areas). For all commercial development, refuse and recyclable materials shall be stored as provided in Ontario Development Code Section 6.01.015E.17. (Refuse Storage Areas (Trash Enclosures)).~~

Project sites shall be designed to meet all the Integrated Waste Department's requirements, including the requirements for Sizing of Storage, Location of Collection Area, Accessibility for Collection Vehicles, and Collection of Sorted/Diverted Waste Types.

5.8 Grading Concept

The existing ground of the Specific Plan area generally slopes to the southwest at approximately 0.8% to 2.0%. The grading concept for Parkside is to work with the existing topography to maintain natural grade and elevations wherever possible. The grading operation for the Specific Plan area will generally consist of the removal of organic material remaining from farming operations, clearing and grubbing, demolition of existing structures, and moving of surface soils to construct building pads and streets. Additionally, where slope conditions are present, the property line shall be located at the top of a slope. In areas adjacent to a ridgeline or in moderate slope areas, dwelling units and buildings should be sited to:

- Use the natural ridgeline as a backdrop for structure buildings;
- Use landscape plant materials as a backdrop; and
- Use structure-buildings to maximize concealment of cut slope.

If retaining walls are required, the following criteria

shall be used:

- Exposed retaining walls facing roadways shall be no greater than 3 feet in height, except as necessary for acoustical purposes to satisfy the intent of the noise ordinance; and
- Where retaining walls face roadways, they shall be built of decorative materials consistent with the wall theme of the neighborhood.

The conceptual grading plan for the residential portion of Parkside, as illustrated in Exhibit 5-14: Conceptual Grading Plan will attempt to balance of cut/fills for the site.

Grading plans for each tract in the Specific Plan area will be reviewed and approved by the by the City of Ontario Building, Planning, and Engineering Departments prior to the issuance of grading permits. All grading plans and activities will conform to the City grading ordinance and dust and erosion control requirements.

5.9 Phasing

Development of the Specific Plan area will be phased to meet the following objectives:

- Orderly build-out of the Specific Plan area, based upon market and economic conditions;
- Provision for adequate infrastructure and public facilities as determined and deemed necessary by the City Engineer, and concurrent with development of each phase; and
- Protection of public health and welfare.

Development phasing of the project site will be determined by the landowner and/or developer based



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upon real estate market conditions. Phasing will occur as appropriate levels of infrastructure, community facilities, and open space dedications are provided. Phasing sequencing is subject to change over time to respond to these various factors and individual phases may overlap or develop concurrently. As the Specific Plan is Phased, the following minimum criteria shall be met for each Subdivision and Development of each Phase:

- For Domestic Water, all the Master Plan, Regional and Local Infrastructure identified in Section 5 and the Conceptual Domestic Water Plan for the Water Pressure Zone that the Phase is within shall be completed as part of the Phase's infrastructure requirements or as described in the Project's Development Agreement. Also, any domestic water infrastructure necessary to provide primary looping, secondary looping, and meet all hydraulic criteria and fire flow shall be completed as part of the Phase's infrastructure requirements or as described in the Project's Development Agreement.
- For Recycled Water, all the Master Plan, Regional and Local Infrastructure identified in Section 5 and the Conceptual Recycled Water Plan for the Recycled Water Pressure Zone that the Phase is within shall be completed as part of the Phase's infrastructure requirements or as described in the Project's Development Agreement. Also, any recycled water infrastructure necessary to provide primary looping, secondary looping, and meet all hydraulic criteria shall be completed as part of the Phase's infrastructure requirements or as described in the Project's Development Agreement.
- For Sewer, all the Master Plan, Regional and Local Infrastructure identified in Section 5 and the Conceptual Sewer Plan for the Master Plan Sewer Tributary Area that the Phase is within and upstream of shall be completed as part of the Phase's infrastructure requirements or as described in the Project's Development Agreement. Also, any sewer infrastructure necessary to meet all hydraulic criteria shall be completed as part of the Phase's infrastructure requirements or as described in the Project's Development Agreement.

Exhibit ~~18~~ 5-14: Conceptual Phasing Plan illustrates Phases ~~A and C~~ I-IV for the project. Phases ~~A and C~~ I-IV are dependent on area-wide infrastructure and master plan improvements, probably beginning in ~~2006-2007~~. ~~Phase B is also dependent on master plan infrastructure, particularly sewer service.-2020-2021.~~

Backbone infrastructure to serve all areas of Parkside shall be installed by the developer(s) in accordance with the City's adopted Master Plan for the areas or any approved amendments to it. Infill service mains will be installed/constructed in phases as development proceeds and conditioned by the City [Engineering Department](#) to support individual phases of development, which may require installation of off-site infrastructure improvements beyond a given phase boundary.

Mass grading and installation of infrastructure to serve Parkside is anticipated to be completed in [multiple](#) phases. ~~Phase A is that portion of Parkside lying westerly of the Cucamonga Creek Channel, Phase B is the residential~~



Parkside

~~portion lying easterly of the Cucamonga Creek Channel and Phase C is the commercial area located on the southwest corner of Archibald Ave. and Edison Ave.~~

The determination of which phase will be constructed first, either **Phase A I** or **Phase B-II** will be decided by the master developer and will be based on the availability of the sewer trunk main which will serve the phase. ~~Phase C~~ **The retail center development** will be constructed when there are a sufficient number of homes constructed to support the **neighborhood shopping center development**. It is anticipated that the fire station will be constructed **along with before** the first phase or at a subsequent phase determined by complying with the City requirement of a 5-minute emergency response time to the Parkside community. The mass grading for each phase will include that portion of the Great Park included within the phase boundaries. Refer to Exhibit ~~18~~ **5-15: Conceptual Phasing Plan.**

Following completion of the mass grading and infrastructure installation for the phase, the merchant builders will construct models for the product types being sold and the recreation area and pocket parks planned within that phase. The timing for installation of infrastructure and utilities and the provision of public services for the residential development within Parkside will be determined as part of the City's approval of tentative maps or development plans. Facilities will be constructed and services available in a timely manner as development progresses.

~~A "Model Home Permit" will be required for all model homes. The "Model Home Permit" will be reviewed through the "Temporary Use Permit" process.~~

"Model Homes" shall meet the requirements of Section 5.03.395B (Model Homes) of the Ontario Development Code.

Chapter 6: Development Regulations



Denver, Colorado: City Park, Indian Monument



Parkside

6.0 Development Regulations

6.1 Introduction

The provisions contained herein will regulate design and development within the Parkside Specific Plan area. The regulations contained herein provide for the development of all land use categories with development regulations established for residential uses, commercial uses, public use (fire station), parks and recreational facilities, and landscaping.

6.2 Definition of Terms

The meaning and construction of words, phrases, titles, and terms shall be the same as provided in the [City of Ontario Development Code Article 2, "Definitions,"](#) unless otherwise specifically provided herein.

6.3 Applicability

The development regulations contained herein provide specific standards for land use development for the Parkside Specific Plan ~~within the subject portions of Subareas 22 and 23~~. Regulations address residential, commercial, public, and parks and recreation land uses. General landscaping regulations are also provided. Application of the following regulations is intended to encourage the most appropriate use of the land, ensure the highest quality of development, and protect the public health, safety, and general welfare. The adoption of this Specific Plan by [resolution ordinance](#) supersedes otherwise applicable [City of Ontario zoning Development Code](#) regulations unless stated herein to the contrary. Whenever the provisions and development standards contained herein conflict with those contained in the

Ontario Development Code, the provisions of this Specific Plan shall take precedence. Where the Specific Plan is silent, City codes shall apply. These regulations shall reinforce specific site planning, architectural design, and landscape design guidelines contained in Section 7, Residential Design Guidelines, and Section 8, Commercial Design Guidelines, of this Specific Plan.

6.4 Administration

The Specific Plan is adopted by ordinance and will serve as the implementation tool for the [Policy Plan \(General Plan\) component of The Ontario Plan](#) as well as the zoning district for the site. The Specific Plan Development Regulations address general provisions, permitted uses, and [some](#) development standards for the [project Specific Plan area](#).

6.5 Methods and Interpretation

Development within the Specific Plan shall be implemented through the approval [by the City](#) of parcel maps, tract maps, and development permits. The [administrative](#) process described herein provides the mechanisms for review and approval of development projects within the project, consistent with the Specific Plan objectives.

6.6 General Site Development Criteria

The following general site development criteria shall apply to all land development proposed in the Specific Plan.



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Gross Acres—Except as otherwise indicated, gross acres for all development areas are measured to the centerline of streets;

Grading—Development within the site shall utilize grading techniques as approved by the City of Ontario. Grading concepts shall respond to the grading design included in this Specific Plan which guide the development of land use toward the goal of providing for a livable community with streets and entries designed for walking and resident interaction;

Structure Modification—Additions ~~and/or projections into setback areas permitted by the Specific Plan~~ shall match the architectural style of the primary unit and shall be constructed of the same materials and colors as the primary unit;

Utilities— All new and existing public utility distribution lines of ~~less than~~ 66 kV or less shall be subsurface throughout the planned community;

Technology—All homes and businesses shall accommodate the most modern technology for ~~computer~~ internet access, phone, fax, and television. Broadband fiber optics cable will be installed on all peripheral streets ~~and will be available to provide service to each home~~ per the approved Broadband Master Plan;

Density—The Specific Plan Land Use Plan allocates a total number of units to each residential neighborhood ~~as indicated in the Land Use Summary, Table 1, included in Section 4, Land Use, of this Specific Plan. As a master-planned community developer, the developer will review each guest builder's plans and design and strongly encourage the use of current architecture, design, and product type. Accordingly, variations in the number and type of dwelling units within each residential neighborhood may occur~~ [Table 4-1: Land Use](#)

[Summary, included in Section 4, Land Use](#). Variations in the number and type of dwelling units within each residential neighborhood may occur at the time of final design of the neighborhood, depending upon the residential product identified for development. [Water and Sewer Technical Studies shall be prepared for unit transfers that change the quantity and locations of sewer and water demands. If the net changes impact the overall projected water or sewer demands, or the sewer demands are transferred between different Master Plan Trunk Sewer Tributary Areas and Specific Plan Sewer Tributary Areas, then a Water or Sewer Master Plan Amendment may be required.](#) Increases or decreases in allocation of residential units up to a maximum of 15 percent are permitted among the planning areas within the Specific Plan area, provided the total number of units established for the Specific Plan area is not exceeded;

Maximum Number of Dwelling Units—The maximum number of residential dwelling units permitted within the Specific Plan ~~for portions of Subareas 22 and 23~~ shall be 1,947 dwelling units;

Measurement of Lot Widths—[The measurement of the width of the lots on curvilinear streets shall be taken from the front and rear setback lines. The front setback lines shall be displayed by dotted lines or by plotting the footprint of the dwelling unit; and](#)

Encroachments—Buildings or parts of buildings, in particular roof overhangs, shall not encroach into the 3-foot building setback from the property line.



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6.7 Implementation

Development proposals within the [Specific Plan area](#) shall be subject to the implementation procedures established herein. The provisions and development standards contained herein ~~conflict with~~ [supersede](#) those contained in the Ontario Development Code. [However, where the development standards are silent, the provisions of the Specific Plan Ontario Development Code shall take precedence apply;](#)

Severability—If any portion of these regulations is declared to be invalid or ineffective in whole or in part, such decision shall not affect the validity of the remaining portions thereof. The legislative body hereby declares that they would have enacted these regulations and each portion thereof irrespective of the fact that any one or more portions be declared invalid or ineffective;

Ambiguity—Unless otherwise provided, any ambiguity concerning the content or application of the Specific Plan shall be resolved by the Planning Director ~~or his/her designee, hereinafter referred to as Director~~, in a manner consistent with the goals, policies, purpose and intent established in this Specific Plan; and

Deviation—Any major deviation from the design guidelines within the Specific Plan shall require a Specific Plan Amendment. The [Planning](#) Director shall determine whether a proposed change constitutes a major deviation.

6.8 Development Permit

All development projects within the Specific Plan shall be subject to the Development Plan Review Process as established in on [Table 2.02-1 Review Matrix](#) of the Ontario Development Code. Adoption of ~~the project~~ [the Specific Plan](#) by the City includes adoption of the design guidelines contained within this Specific Plan. Where the Specific Plan design guidelines are silent, the design guidelines contained within the Ontario Development Code shall apply. The design guidelines are intended to be flexible in nature while establishing basic evaluation criteria for the review by the City of developer projects during design review.

Pursuant to these provisions, the Development Permit process constitutes a design review of project architecture, site plans, and landscape plans. All development project applications shall include a landscape and irrigation plan describing plant materials and their growth habits, plant size and spacing, methods of irrigation and landscaping maintenance, site plans, architectural elevations, floor plans, grading plans and other requirements as specified by the City. Development permits will be approved with conditions of approval.

6.9 Subdivision Maps

Approval of the Tentative Tract Map(s) and Parcel Maps will create legal lots for development. All Tentative Tract Maps and [Parcel Lot](#) Maps will be reviewed and approved pursuant to applicable provisions of Division 6.08 (Subdivisions) of the Ontario [Subdivision Ordinance Development Code](#) and consistent with the applicable provisions contained within this Specific Plan.



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6.10 Specific Plan Modifications and Amendments

6.10.1 Minor Modifications

The following constitute minor modifications to the Specific Plan, not requiring a Specific Plan Amendment, and are subject to review and approval by the Zoning Administrator. The Zoning Administrator shall have the discretion to refer any such request for modification to the Planning Commission or the City Council.

- Change in utility and/or public service provider;
- An increase or decrease of more than 15 percent to the number of units within a planning area an individual neighborhood, provided the total number of units for the entire project site do Specific Plan areas does not exceed that established in the approved Specific Plan;
- Adjustment of a neighborhood phase boundary, provided the total acreage of the affected area does not increase or decrease by more than 20% of the total acreage stated in the approved Specific Plan;
- Minor changes to the design guidelines, which are intended to be conceptual in nature only, and are intended to be flexible in implementation; and
- Other modifications of a similar nature to those listed above, which are deemed minor by the Planning Director, which are in keeping with the purpose and intent of the approved Specific Plan and which are in conformance with the General Plan.

6.10.2 Specific Plan Amendments

Amendments to the Specific Plan may be requested by the applicant or by the City at any time pursuant to Section 65433(a) of the Government Code. Amendments shall be processed pursuant to the provisions of the Government Code for Specific Plan Amendments. In the event the proposed amendment requires supplemental environmental analysis pursuant to the California Environmental Quality Act (CEQA), the applicant(s) will be responsible for preparing the necessary CEQA documentation.

~~A Specific Plan Amendment shall be required if adjustments are made to housing types or product lines in any neighborhood that do not meet the policy goals of the New Model Colony, Development Standards in Chapter 6, and Design Guidelines in Chapter 7.~~

6.11 Appeals

Appeals from any determination of the Zoning Administrator shall be made to the Planning Commission. The applicant(s) or any other entity shall have the right to appeal the decision of the Planning Commission on any determination. Appeals shall be submitted by filing an application on forms provided by the City within 10 days following the final date of action for which an appeal is made. Appeals shall be processed consistent with the provisions of Division 2.04—Appeals of the Ontario Development Code.



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6.12 Project Financing

Construction and maintenance of public improvements and facilities to serve the Specific Plan will be financed through a combination of financing mechanisms including the use of public and private funds. In order for the project to be fiscally self-sufficient the following options can be considered for implementation:

Maintenance

A ~~community facilities lighting and landscape~~ district ~~may can~~ be implemented in order to defray the costs associated with street maintenance and street light costs.

Facilities and Services

A variety of other financing options ~~may can~~ be considered by the City of Ontario for financing facilities and services to be provided within the Specific Plan area to include:

- Establishment of a Mello-Roos Community Facilities District, which enables governmental entities to finance public infrastructure on a tax-exempt basis;
- Special assessment districts;
- Benefit assessment financing for improvements such as storm drains and street lighting; and
- Integrated financing districts such as landowner assessments that are contingent upon development.

City Council approval is required for the establishment of all special district financing mechanisms. Establishment of a district would enable the City to levy a special tax and to issue bonds for the financing of improvements and/or services. Among the facilities that potentially could be financed are flood control, drainage, utilities, street improvements and other infrastructure, and

related facilities ~~and/or payment for such infrastructure.~~

The district provisions for those services would cover all phases, including planning, design, acquisition, construction, operations, and maintenance. These assessments would only affect the subject portions of ~~Subareas 22 and 23~~ this Specific Plan, and the debt service incurred by these assessments would be paid by landowners or homeowners within the Specific Plan area.

6.13 Maintenance Responsibilities

The public and private improvements constructed within the Specific Plan area will be maintained through a combination of public and private entities as described ~~below and~~ in Table 3: Maintenance Responsibilities.

During the course of maintenance of public utilities (including storm drain) within private ~~and public~~ streets, the City will pave the streets and restore landscaping per City standards. Restoration of any enhancements above and beyond City standards, including but not limited to architectural, hardscaping, and landscaping enhancements shall be the responsibility of the HOA or other entity maintaining those enhancements. This applies to all areas where public utilities are located including but not limited to public and private streets, gated communities, ~~lanes (private~~ alleys, etc.

6.13.1 Public Maintenance

Water, recycled water, and sewer utilities may be designated as “public utilities” if located within public or private streets. All public utilities within private streets shall be designed and constructed per City standards and contained within acceptable easements. The CC&RS shall contain language that requires all proposed work by the HOA within said easements to be plan checked and



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inspected by the City, including applicable fees. Generally, utilities will not be accepted as public within alleys, parking areas or driveways. Utilities within commercial and industrial parking lots and loading areas shall be designated as private. The extent to which said utilities would be accepted as public utilities shall be determined, at the full discretion of the City, during final design plan review. Refer to Table 6-1: Maintenance Responsibilities.

6.13.2 Homeowner Association

Refer to Table 6-1: Maintenance Responsibilities.

Table 6-1: Maintenance Responsibilities

	City CFD	Private Homeowners Association (HOA)	Private (Homeowner/ Property Owners)	Utility Entity
Master Plan Roadways <ul style="list-style-type: none"> (Archibald Avenue, Eucalyptus Avenue, Ontario Ranch Road, Hellman Avenue, Carpenter Avenue) Parkways and Neighborhood Edges (curb to perimeter walls including landscape, sidewalks, trees) Community Theme Wall and Entry Monuments (outside face for graffiti removal and paint) 	X			
Monument Signs (within Neighborhood edges)		X		
Traffic Signals/Street Lights	X			
Traffic Control Signs (Public)	X			
Public Roadways	X			
NPDES Facilities on Arterial Streets (Off-site)	X			
Entry Streets (curb to perimeter walls including landscape, sidewalks, trees)	X			
Interior Project Streets (curb to curb Primary Entry Street, Secondary Entry Streets, Local Streets)	X			
Regional Park	X			
Community Trail (Cucamonga Creek Channel Trail)	X			
Off-site and In-tract water, sewer, and storm drain improvements (only those facilities in public roads and those in private streets within public utilities easements that meet public improvement design criteria)	X			
Police	X			
Fire	X			



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On-site water, sewer, and storm drain improvements (improvements that are: private, laterals, and lines behind meters and/or DCDAs, improvements serving only one lot/parcel, not within public or private roads, not within public utility easements, or not meeting public improvement design criteria)		X	X	
Private Streets in Gated Multi-Family and SFD Complexes		X		
Parkways of Interior Project Streets (landscaping, sidewalks, trees and includes restoration work following public street repairs)		X		
Interior Tract Graffiti Removal		X		
Private Recreation Areas		X		
	City CFD	Private Homeowners Association (HOA)	Private (Homeowner/ Property Owners)	Utility Entity
Monument Signs within tract entry		X		
Traffic Control Signs (Private)		X		
Lanes (Private Alleys)		X		
Pocket Parks		X		
Paseos		X		
Landscaped Common Areas		X		
Community Theme Wall and Entry Monuments on Master Planned Roadways: Surface (interior) opposite street side (structural integrity and face repairs)		X		
Lane (Private Alley) Landscaping and Lighting		X		
Interim Detention Basin		X		
NPDES Facilities (On-site) in Public Right-of-Way		X		
Front Yard/Corner Street Side Landscaping Areas			X	
Private Interior Yard Walls			X	
Private Exclusive Use Landscaped Areas			X	
Driveways and Parking Areas Serving Commercial			X	
Communication Systems	X			X
Electricity				X
Natural Gas				X

6.14 Residential Development Standards



Parkside

6.14.1 Residential Detached

General

This category includes the development of all single-family detached dwelling units. The purpose of the residential standards for single-family detached housing is to establish the minimum criteria for the development of these product types in cluster configurations within the neighborhoods specified within the Specific Plan.

[Additional residential design guidelines can be found in Chapter 7.0: Design Guidelines.](#)

Permitted Uses

- Single-family detached dwellings, and their accessory uses;
- Public or private parks, recreational buildings, newsstands, coffee concessions and sales of grocery items, greenbelts, or open space;
- Accessory uses to include the following:
 - Garages ([minimum 2-car](#));
 - [Granny flats](#) (i.e. Second Dwellings, in accordance with the [Ontario Development Code](#));
 - Home occupations;
 - Swimming pools, spas, sports courts, and other similar outdoor recreational amenities;
 - Patios and patio covers;
 - Storage, garden structures, cabanas, and greenhouses.
 - Monument signage;
 - Temporary uses such as model home and subdivision sales trailers; temporary construction

parking, offices, and facilities; real estate signs, signage indicating future development, [marketing signage](#), and directional signage;

- Second story additions to existing single story dwelling units; and
- Child care facilities up to 7 children.

Conditionally Permitted Uses

- Places of worship; and
- Child care facilities/day care facilities (8-14 children).

Determination of Use

Uses not specifically listed may be deemed permitted subject to a “Determination of Use” as provided for in Section [1.02.010](#) ~~1.1310~~ of the Ontario Development Code.



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6.14.2 Residential Attached

General

The category includes the development of all multi-family attached dwelling units. The purpose of the residential standards for multi-family attached housing is to establish the minimum criteria for the development of these product types in street facing and cluster configurations within the neighborhoods specified within the Specific Plan.

Permitted Uses

- Multi-family attached dwellings, and their accessory uses;
- Public or private parks, recreational buildings, coffee concessions, greenbelts, or open space; and
- Accessory uses to include the following:
 - Garages;
 - Senior apartments, townhomes, and/or condominiums;
 - Home occupations;
 - Swimming pools, spas, sports courts, and other similar outdoor recreational amenities;
 - Patios and patio covers;
 - Storage, garden structures, cabanas, and greenhouses;
 - Monument signage;
 - Temporary uses such as model home and subdivision sales trailers; temporary construction parking, offices, and facilities; real estate signs, signage indicating future development,

marketing signage, and directional signage;

- Second story additions to existing single story dwelling units; and
- Child care facilities up to 7 children.

Conditionally Permitted Uses

- Religious Assembly; and
- Child care facilities/day care facilities (8-14 children).

Determination of Use

Uses not specifically listed may be deemed permitted subject to a “Determination of Use” as provided for in Section 1.02.010 of the Ontario Development Code.



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6.15 Neighborhood Center Commercial Land Use Development Standards

General

This section sets forth the development regulations for development of commercial land uses within the Specific Plan.

Permitted Uses

- Bakery (Retail);
- Barber Shop and Beauty/Nail Salon;
- Book Stores;
- Camera and Photographic Supply Stores;
- Clothing and Accessory Stores;
- Convenience Market;
- Daycare Facilities (Single-Family), 8 or less children;
- Delicatessen;
- Dry Cleaner;
- Electronics and Appliance Stores;
- Florist;
- Jewelry and Watches/Clocks;
- Laundry – Self-Service;
- Locksmith/Key Shop;
- Police Storefront/Sub-Station;
- Satellite dishes/Ham Radio Antennas (private use);
- Shoe Repair;
- Specialty Food Stores;
- Tailor; and
- Travel Agency

Conditionally Permitted Uses

- Alcoholic Beverage Sales;
- [Commercial Day Care Facility \(including Adult Day Care\)](#);
- Banks, Credit Unions and Other Depository Institutions;
- Church;
- Money Transmitting;
- Other Financial Services;
- Police/Fire Station;
- Recreation Center; and
- Senior Citizen Center

Table 6-2: Neighborhood Commercial Development Standards

Minimum Site Area	7 acres, unless developed as part of an integrated commercial center. Minimum lot size shall be large enough to accommodate the proposed use and meet all Development Standards as specified within the Specific Plan.
Maximum Development Floor Area Ratio (FAR)	.35
Minimum Building Setbacks ⁽¹⁾⁽²⁾	
• From Public Street Property Lines	20 ft.
• From Private and Local Street Property Lines	15 ft.
• From Interior Property Lines	0 ft.
• From Interior Property Line Adjacent to Residential District	25 ft.
• From Ontario Ranch Rd. Right-of-Way	35 ft.
• From Archibald Ave. Right-of-Way	19 ft.
Parking & Drive Aisle Setbacks⁽¹⁾	
• From Public Arterial Streets (R/W)	25 ft.
• From Private and Local Streets & Drive Aisles to Bldgs. (R/W)	10 ft.
• From Interior Property Line	5 ft.
• From Rear Property Line	5 ft.
• From Interior Property Line Adjacent to Residential District	5 ft.
• From Ontario Ranch Rd. Right-of-Way	35 ft.
• From Archibald Ave. Right-of-Way	19 ft.
Minimum Landscape Coverage	15%
Maximum Building Height	
• Main Building	35 ft.
Architectural Projections and Focal Elements such as Towers, Cupolas, and other⁽³⁾	55 ft.
Porte-Cocheres ⁽⁴⁾	1-story
Other Development Regulations	
• Walls, Fences & Hedges	When a non-residential use adjoins a residential district, a 6-foot high masonry wall shall be required at the interior side or rear property lines.
• Parking	Per Division 6.03 (Off-Street Parking and Loading), Ontario Development Code
• Screening	All loading areas shall be screened from adjacent public streets and residential and open space uses through the use of landscaping, earthen berms, decorative walls, or fencing. All storage, including cartons, containers, materials or trash shall be shielded from view within a building or area enclosed by a solid fence or wall not less than 6' in height.
	All roof-mounted equipment, including but not limited to, mechanical equipment, satellite dishes, tanks, ducts, towers, and all equipment appurtenant thereto, shall be screened on all sides from public view from the street adjoining properties, and neighboring residential units, by a parapet wall, decorative enclosure, or other architectural element. Equipment screening shall appear as an integral part of the structure architecture.
• Lighting	All interior and exterior lighting shall be directed away from residential and open space uses.

(1) All setbacks areas shall be landscaped.

(2) All setbacks are measured to habitable area, not architectural appurtenance or projection. An architectural projection is defined as an element that articulates the building elevation such as eaves, window and door pop-out surrounds, bay windows, pot shelves, chimneys, enhanced window sills, shutter detail, window trim, and balconies, and other similar elements. Such elements may project a max. of 3' into setback areas. Building setbacks shall be determined by the hierarchy of streets (major arterial and local streets) and measured from PL for local streets and RW for major arterials.

(3) Architectural elements are not to be used for signage, subject to Planning Director approval. Within commercial areas, towers and vertical projections are limited in size to 10% of the total building square footage.

(4) Porte-Cocheres shall be open on three sides.



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Determination of Use

Uses not specifically listed may be deemed permitted subject to a “Determination of Use” as provided for in Section 1.02.010 of the Ontario Development Code.

Design Guidelines

Prior to the approval of the first site development permit and/or lot map within the Community Commercial land use area, a detailed set of design guidelines for the area shall be submitted by the developer and approved by the City.

6.16 Public Use (Fire Station) Development Standards

General

This section sets forth the development regulations for development of the Fire Station land use in the Specific Plan.

Permitted and Conditionally Permitted Uses

Permitted uses are as defined in Table 5.02-1: Land Use Matrix under the CIV (Civic) Zoning District of the Ontario Development Code.

~~Permitted uses are as defined in Article 13, Sec. 9-1.1300; Permitted, Conditional and Ancillary Land Uses of the City of Ontario Development Code.~~

Determination of Use

Uses not specifically listed may be deemed permitted subject to a “Determination of Use” as provided for in Section 1.02.010 of the Ontario Development Code.

6.17 Landscape Standards

6.17.1 General Provisions

- All landscape plans, streetscape plans, and graphic designs which create neighborhood identity or enhance entry monuments shall conform to the design guidelines and regulations as set forth herein and shall be subject to review and approval by the City;
- The landscape/streetscape improvements for the Specific Plan area shall establish a landscape theme reminiscent of the regional landscape character of the surrounding area;
- Installation of landscaping within ~~PA1 through PA19 residential areas of Parkside~~ the front yards of single-family detached housing products will be provided by the home builder; ~~and: Front yard landscaping shall be installed prior to Certificate of Occupancy;~~
- Installation of landscaping in residential areas of Parkside may be provided by the home builder, based on future landscape plans submitted to the City during site plan review;
- Installation of landscaping within the commercial area of the Specific Plan will be provided by the developer/builder; ~~and~~
- All loading areas shall be screened from adjacent public streets, and residential and open space use through the use of landscaping, earthen berms, and/or decorative walls or fencing. All storage including cartons, containers, materials, or trash shall be shielded from view within a building or area enclosed by a solid fence or wall not less than 6 feet in height.



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6.17.2 Landscape Standards

- Landscaping within the Specific Plan shall be provided in accordance with the Design Guidelines for Streetscapes and Entries utilizing plant materials specified on the Plant Palette included in Section 7, Residential Design Guidelines, and
- Section 8, Commercial Design Guidelines, established for the Specific Plan.
- Boundary landscaping will be required adjacent to residential, commercial, park, and public (fire station) areas. Landscaping shall generally be placed along the entire property line; and
- Landscaping and irrigation systems within the public rights-of-way of the Specific Plan area shall be installed by the developer.
- ~~The developer will provide site inspection of all construction and installation of open space areas in accordance with City of Ontario requirements;~~
- ~~Parking lot landscaping within commercial areas shall be required in accordance with Section 9-1.3040 of the City's Development Code; and~~
- ~~Vegetation that is non-toxic, free of thorns or spines, and without any hazardous characteristics shall be utilized adjacent to all public open space areas.~~

6.17.3 Walls and Fences

- Freestanding perimeter walls and view fencing shall be provided within and at the perimeter of the project as specified in the Exhibit 40 Wall and

Fence Master Plan, Section 7, Residential Design Guidelines. Such walls and fences will be constructed concurrently with the construction of improvements required for development of the neighborhoods of the Specific Plan;

- Walls and Fencing—Perimeter walls shall be constructed in locations and of a design consistent with the Residential Design Guidelines and the Fence and Walls Plan, Exhibit ~~40~~ 7-26, and Fence and Wall Details, Exhibits ~~41 and 42~~ 7-27 through 7-30, provided in Section 7, Residential Design Guidelines, and shall not exceed 6 feet in height from finished grade. Perimeter walls may exceed 6 feet in height if required for sound attenuation. Perimeter walls shall be constructed of either decorative masonry (both sides) or other permanent, durable, low maintenance material subject to approval by the City;
- Residential—Side and rear yard walls and fencing shall not exceed 6 feet in height from the highest adjacent finished grade. Walls and fencing within the residential front yard setback area shall not exceed 3 feet in height. Walls shall be decorative masonry construction (both sides) or other permanent low maintenance materials. ~~Decorative all-weather vinyl fencing is not permitted~~ Front yard fences may be constructed of attractive, durable materials, including, but not permitted limited to, wrought iron with pilasters, textured concrete block, formed concrete with reveals, or other materials approved by the Planning Director. View fencing may be of a decorative wrought iron, glass panels, or other durable material subject to approval by the City;



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- All perimeter wall and fence materials throughout the Specific Plan ~~shall be constructed of decorative material and~~ area will be of uniform manufacture with colors specified for the overall design theme;
- ~~Decorative block (both sides) is required for all interior block walls;~~
- All interior walls (both sides) that are visible from the public street shall be constructed of attractive, durable materials, including, but not limited to, wrought iron with pilasters, textured concrete block, formed concrete with reveals, or other materials approved by the Planning Director. Colors shall be consistent with the colors specified for the overall design theme and shall be decoratively capped. Decorative all-weather vinyl fencing may be permitted for compact small lot product, subject to Planning Department review and approval;
- ~~Combo walls are also permitted which include decorative block~~The developer will provide site inspection of all construction ~~and wrought iron.~~ Combo walls shall not exceed the maximum height installation of open space areas in accordance with City requirements;
- Parking lot landscaping within commercial areas shall be required in accordance with Division 6.05 (Landscaping) of the Ontario Development Code;
- Non-toxic and non-invasive vegetation shall be utilized adjacent to all public open space areas; and
- All landscaping features shall comply with City requirements for sight distance.

6.18 Signage

A Master Sign Program shall be submitted by the developer(s) of each land use component for the Specific Plan and approved by the City pursuant to Division 8.0 (Sign Regulations) of the Ontario Development Code to address residential project entries, residential neighborhood identification signs, commercial center identification, tenant signage, and “way-finding” signs within the Specific Plan area. All other signs shall be subject to the approval of a sign permit pursuant to the Ontario Development Code. All traffic-related signs shall conform to City Traffic and Transportation Design Guidelines and California Manual of Uniform Traffic Control Devices (MUTCD), latest editions.



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6.18.1 Master Sign Program Contents

All sign programs shall address, at a minimum, the following:

- Permitted signs;
- Prohibited signs;
- The hierarchy of signage;
- Definition of types of signs;
- Locations and dimensions for monument signs, neighborhood identification signs, and public facilities signs;
- Locations and dimensions of directional signage;
- Provisions for size, location, and duration of display of temporary signs;
- Permitted sign types, styles, construction materials, colors, and lettering styles;
- Requirements for a sign permit application;
- Procedures for obtaining approval of a sign permit; and
- Procedures for amendments to the sign program.

6.19 Private Recreational Facilities

This category includes the development of all private recreation facilities and is subject to the Development Permit process contained herein.

As consistent with Sections 5.7 and 7.5.2, all project sites shall be designed to meet all the Integrated Waste Department's requirements, including the requirements for Sizing of Storage, Location of Collection Area, Accessibility for Collection Vehicles, and Collection of Sorted/Diverted Waste Types.



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Chapter 7: Residential Design Guidelines



New York City, New York: Central Park



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7.0 Residential Design Guidelines

7.1 Introduction

Parkside is a new master-planned community located in Ontario, California. The historical character of the Ontario area is partly traditional and agricultural, but being on the path of the advancing railroad, the later automobile Route 66 and eventually Ontario International Airport, Ontario has also typically been associated with transit and forward-moving progress.

The proposed community design integrates the traditional character of the past with a plan which provides a variety in housing choices that serve the needs and lifestyles of today.

Parkside has been designed as a planned community “in the park.” The Great Park is the key organizational element for the entire community. Connections to both neighborhood and regional trails will be provided from this central park area. With a focus on promoting a pedestrian friendly, healthy lifestyle, the Great Park area is within less than a ¼ mile walk from all neighborhoods in the community.

Community Overview

Community design elements include:

- The Great Park;
- Neighborhood Commercial;
- A wide variety of home sizes and plans to fulfill the needs of different life stages, in gated and non-gated neighborhoods;
- Tree-lined neighborhood streets with curb-separated sidewalks and generous plantings in parks and

open spaces;

- Private recreational facilities for each neighborhood;
- A network of sidewalks and multi-purpose trails that link all neighborhoods to parks, open spaces, schools and nearby retail; and
- Nearby connections to local, regional, and global transit.

The community character is inspired by:

- The charm and tradition of Ontario’s historic downtown neighborhoods and an eclectic mix of architectural styles;
- The simple, yet stately landscape along Euclid Ave. and the region’s agricultural heritage; and
- Environments that encourage wellness by providing opportunities for pedestrians and outdoor physical activity.

~~Neighborhood retail goods and services will be conveniently located in Planning Area 21 at the southwest corner of the intersection of Edison Ave. and Archibald Ave.~~

~~The Specific Plan~~ Community entries along Archibald Ave., Edison Ave Ontario Ranch Rd., Eucalyptus Ave., and Carpenter Ave. will be announced with community character monuments and entry statements. Signage, lighting and monuments within the project site will reflect an identifiable community character.

7.1.1 Guiding Principles

The following guiding principles have been created to help develop architecture that reflects quality in design, simplicity in form and plan, and contributes charm and



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appeal to the **proposed** single-family and multi-family neighborhoods of Parkside.

- The home, not the garage, is the primary focus of the front elevation. A variety of garage placements is encouraged to emphasize the pedestrian environment as opposed to the automobile;
- A variety of compatible architectural styles are provided to ensure a degree of individuality throughout **neighborhoods**;
- Appropriate massing and roof forms play a major role in defining architecture styles, roof framing related to each plan vs. individual elevations;
- Architectural elements, details, and appropriate color palettes are used to reinforce individual architectural styles;
- Rear elevations **visible to the public viewshed should provide** articulated walls, windows, and varied roof profiles, all **provide defining** a level of **quality in** detail and interest in neighborhood architecture; and
- Selected architectural styles are related to those historically represented in Ontario. These are: Farmhouse, Cottage, **Adobe Ranch**, American Colonial, American Traditional, **American Traditional Revival**, Craftsman, **English Revival**, **Italianate**, Andalusian, Spanish Colonial, Tuscan, **Prairie** and Provence. **However, modern variations of these styles are permitted.** Modern interpretation of traditional styles, such as Adaptive American Traditional, Adaptive Farmhouse, Adaptive Mediterranean and Contemporary are permitted.

7.2 Architectural Styles

7.2.1 Introduction

The style information on the following pages provides the builder and design consultants with the tools to create functional and attractive architectural designs without using gimmicks or sacrificing the integrity of the style.

Authenticity

The design criteria **in these guidelines is offered** encourage a high quality of architecture and reasonable level of authenticity of styles through the use of appropriate elements. Although detail elements may be used to further convey the character of a style, the overall massing and appropriate roof forms should be used to establish a recognizable style. Proper scale and proportion of architectural elements and appropriate choice of details are all factors in achieving authenticity.

7.2.2 Architectural Philosophy

Looking back to Ontario's older, established neighborhoods for inspiration, the **design intent for Parkside** is to recapture the charm and essence of these original home styles and express them in the simple, honest manner that they originated. These homes reflected function and tradition, simplicity in massing, plan and roof forms, and authentic, regional styles. **Parkside homes will provide the kind of function, quality, value, and appeal that were built into those original homes.** Additional more progressive styles that also reflect this charm are also allowed in Parkside. Neighborhood parks, sidewalks, parkways, and street trees will add to the traditional feeling in these neighborhoods **linking them with the overall community.**



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Design Character

The design character **of the neighborhoods** will be one of hometown appeal characterized by simple, yet charming homes, curb separated sidewalks, and parkways lined with street trees. Homes and streetscenes will reflect an overall feeling of quality and tradition. The architectural palette of styles has a regional historic background **and context in keeping** but includes more modern styles to reflect with the character of Ontario.

7.2.3 Architectural Styles

Architectural Style/Plan Mix - Planning Areas 5-18

An important goal in this community is to develop small, varied, and interesting streetscenes. Typically, the neighborhoods will consist of 80-100 dwelling units. In order to achieve dynamic streetscenes the following architectural style and plan requirements must be met according to the number of homes in a neighborhood:

~~In all~~ All single-family detached neighborhoods shall provide:

- A minimum of 4 plans and 3 elevations for neighborhoods of more than 100 homes;
- A minimum of 3 plans and 3 elevations for neighborhoods of ~~less than or equal to~~ 100 homes or less; and
- A minimum of 4 different color schemes per number of architectural styles used.

Planning Areas 1-4 (Integrated Neighborhood)

PA 1-4 is envisioned to provide home types that are more attainable than the traditional larger lot homes. Simple yet sophisticated building forms mixed with

smaller square footage that efficiently and effectively utilize less room will help to accomplish this goal. These features help in providing elegant yet cost effective dwellings for a greater market base. PA 1-4 is proposed to be an Integrated Neighborhood that shall provide a minimum of 3 product types.

PA 1-4 Integrated Neighborhood shall provide:

- A minimum of 3 plans and 3 elevations for the Neighborhood; and
- A minimum of 4 different color schemes per number of architectural styles used.

Cluster Homes Style/Plan Mix - Planning Areas 5-18

Cluster Homes plotting generally results in the same 1 or 2 plans being repeated along the street front. To support varied neighborhood streetscenes, each Cluster Home neighborhood shall provide:

- A minimum of 3 significantly different styles per plan. Although the styles shall be significantly different, they shall be coordinated to provide a harmonious and cohesive neighborhood preferably from a family of related architectural styles; and
- A minimum of 4 different color schemes per number of plans.

Number of plans and elevations shall not be regulated for interior units; the above standards shall apply to any unit plotted along (facing or siding on to) a street more than one time.



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Architectural Styles

Every neighborhood will be comprised of a variety of architectural styles selected from the following palette of styles. [There is a Menu of Common Features listed with each style that should act as a guide for design. Every feature does not have to be included in the architecture, but the features listed are characteristic to each style and may be used as appropriate to create authentic and identifiable elevations.](#) Additional styles may be proposed; however, they must follow the same principals and attention to detail as the specific styles here. These additional styles may be added with the review and approval of the Design [Advisory](#) Board.

American Heritage Style Series

This series represents traditional American styles found throughout the country. The architectural form and design elements of these styles descend from the first homes built in the New England colonies in the 17th century. This traditionally influenced American Heritage series became part of the early California vernacular as the new state experienced an influx of money and population from the East Coast during the gold rush era of the mid-19th-century. Styles within this series include the following:

- American Colonial
- American Traditional
- Farmhouse

California Heritage Style Series

California Heritage styles re-interpreted architectural lifestyle choices with the addition of Western materials and innovation while retaining the decorative detailing of exposed wood work, wrought iron hardware, and moldings. Craftsman style homes may use specialized wood working and siding materials to create beautifully simple homes suited to the desirable California landscape. Representing one of California's true vernacular styles, the adobe ranch style evolved using native materials and considerations of climate and lifestyle. Styles within this series include the following:

- Craftsman

Mediterranean Heritage Style Series

The Mediterranean heritage style series include transplanted styles developed in a climate zone similar to the climate found in California. The Mediterranean styles are not strictly European, but rather American stylization of European landmarks and residences that were popular in the late nineteenth century. Primarily stemming from Italian, French and Spanish influences, these styles are principally based on simpler and informal residential living styles of country settlements or old world villages. Their appeal is in their informal, rustic character expressed in warm colors, textures and materials. Although residential adaptations were less formal, sometimes traditional classical elements are included. Styles within this series include the following:

- Andalusian
- Provence
- Spanish Colonial
- Tuscan



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Revival Style Series

Southern California residential neighborhoods of **Each architectural style is defined by a series of “elements” and “common features,” which illustrate the distinguishing typical characteristics of that style:** the 1920s and 1930s captured this aesthetic particularly well, translating a vital mix of people and ideas into an architecture of variety and comfort. These communities can be said to have no inherent style, but instead are interpreted through a number of disciplines to form residences both singular and appropriate to the area. This method of architectural expression can be loosely referred to as “revival,” yet is perhaps more aptly described by its atmosphere, which is relaxed and casual. As a result, these areas manage to retain the romance of their stylistic roots yet remain relatively free of their basic cultural idioms. It is no coincidence then that we can find in these neighborhoods a true evocation of the Southern California aesthetic; one which represents creativity, diversity, imagination, and openness. Styles within this series include the following:

- Cottage

Millennium Style Series

The Millennium Collection represents subtle differences in the architectural vocabulary using creative interpretation of traditional styles utilizing exaggerated statements and details to express an iconic style in a new vocabulary. Elevations can be abstracted architectural expressions of a recognizable style. These elevations are encouraged to push the artistic envelope of design to incorporate new, modern or alternative forms, details and materials in the modern context of architecture. Styles within this series include the following:

- Adaptive American Traditional
- Adaptive Farmhouse
- Adaptive Spanish
- Adaptive Mediterranean
- European Heritage
- Contemporary

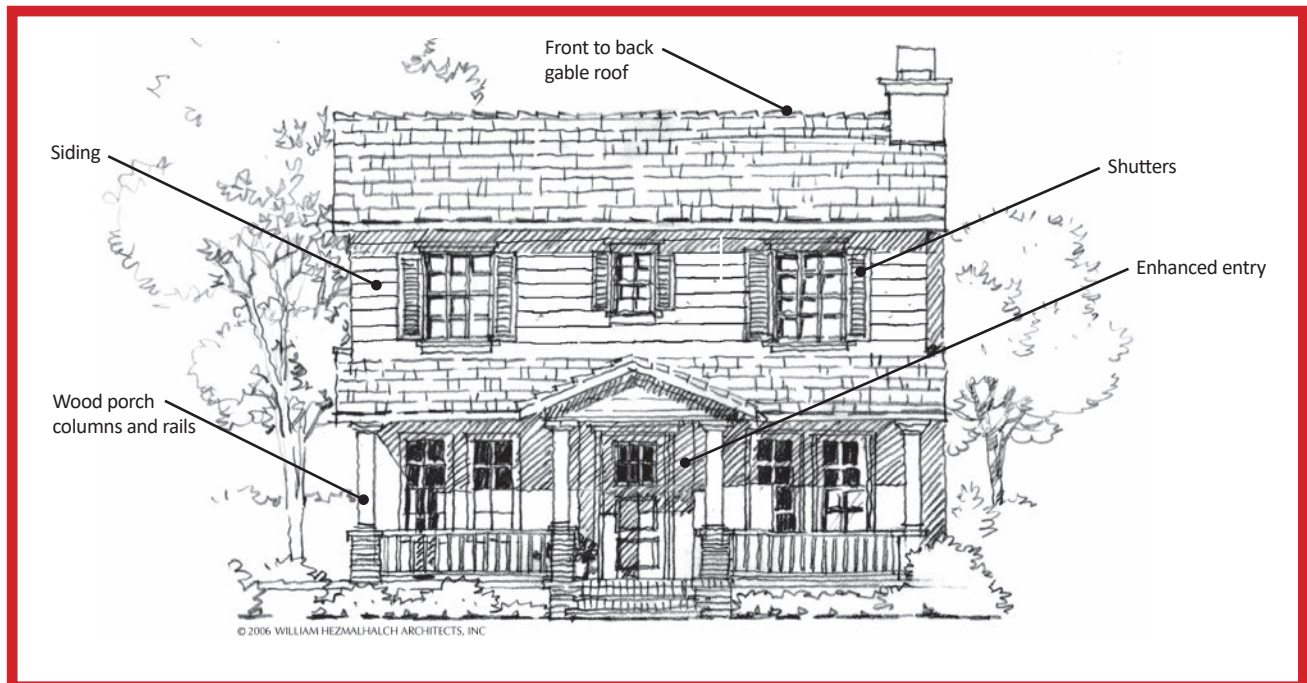


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American Colonial

This classic American style descended directly from the first homes built in the New England colonies in the 17th century. Their beginnings were as small and unpretentious as the one story saltbox, favoring the cultures and traditions of the settlements.

As living functions became more defined and prosperity increased, so did the need for additional space. Second stories with overhangs, dormers and gabled roof forms became favored solutions, later evolving into classic elements of this traditional American style. Wood shutters and an enhanced entry element and/or trim are the finishing details for an otherwise simple and functional form.





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Vertical Multi-Pane Windows with Shutters



Accented Entry



Simple massing with a Red Door

Element	Menu of Common Features
Form	<ul style="list-style-type: none"> Simple plan form and massing Added dormers Enhanced plan forms
Roof	<ul style="list-style-type: none"> 4:12 to 12:12 roof pitch 12" to 16"-18" overhang Simple front to back gable roof Architectural quality asphalt shingles or shingle texture flat concrete tiles 12" to 24" overhang Shed or pitched roof dormers
Walls	<ul style="list-style-type: none"> Horizontal siding may be combined with stucco. All material changes must occur at an inside corner or logical end (return block wall/fence). Wrapped horizontal siding elements on sides and/or rear elevations in high visibility public view areas Brick as a face material or accent material
Windows	<ul style="list-style-type: none"> Vertical multi-paned window inserts at front elevations Multi-paned window on side and rear elevations in high visibility public view areas Vinyl wrapped windows Bay windows Dormer window in roof
Details	<ul style="list-style-type: none"> Simplified cornice trim at gable ends Wood porch columns and rails 2 x 6 window and door trim - wood on siding, foam on stucco Enhanced entry element such as a covered porch Surface mounted fixtures on front elevations must complement architectural style Shutters, enhanced window trims, louvered attic vents Garage door patterns to complement style Neoclassic columns or stucco posts at entry
Colors	<ul style="list-style-type: none"> Field: Pastels, whites, light earth tones, off-whites, or dark earth tones Trim: Light or dark in contrast to field Accents: Whites, light tones, or medium to dark tones in contrast to field, grayed or dark greens, blues, reds or black



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American Traditional

The American Traditional style as manifested in Southern California was often realized as an East Coast derived expression with Cape Cod influences, and displayed the aspects of practicality and functional elegance. This expression evolved from early Colonial beginnings and truly began to proliferate as an American building style from the 1850s to the early 20th century. In Southern California, the style was employed from the foothill neighborhoods of Pasadena to Los Angeles communities such as Westwood and was occasionally used in combination with the Monterey and Ranch styles to create homes which opened to the exterior gracefully yet retained a sense of formality and reserve.

Features of this style include simple gable roof forms, louvered shutters, and articulated entry surrounds. Dormers and second-floor wood decks are also occasionally used.





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Element	Menu of Common Features
Form	<ul style="list-style-type: none"> Asymmetrical massing with a vertical and a horizontal break Recessed 2nd floor Vertical and horizontal breaks in massing
Roof	<ul style="list-style-type: none"> 4:12 to 12:12 roof pitch 16" to 24" overhangs Concrete roof tile - flat or shake appearance Front to back gable or hip roof with intersecting hip or gable roofs Multiple intersecting hip or gable roofs at first and second floors
Walls	<ul style="list-style-type: none"> Stucco with horizontal siding accents allowed All material changes must occur at an inside corner or logical end (return block wall/fence). Wrapped horizontal siding elements on sides and/or rear elevations in high visibility public view areas
Windows	<ul style="list-style-type: none"> Vertical multi-paned window inserts at front elevations Multi-paned window inserts on side and rear elevations in high visibility public view areas Vinyl wrapped windows Round top accent or bay windows
Details	<ul style="list-style-type: none"> Simplified cornice trim at gable ends Header window wood accent trim 2x6 window and door trim - wood on siding, foam on stucco Surface mounted fixtures on front elevations must complement architectural style Full wood porches and/or wood balconies Garage door patterns to complement style Square wood columns with trim Wood window pot shelves Brick veneer or stucco chimney wrap
Colors	<ul style="list-style-type: none"> Field: Whites, off-white, dark or light colors Trim: White or contrasting with field color Accents: White, light or dark colors in contrast or harmony with field color

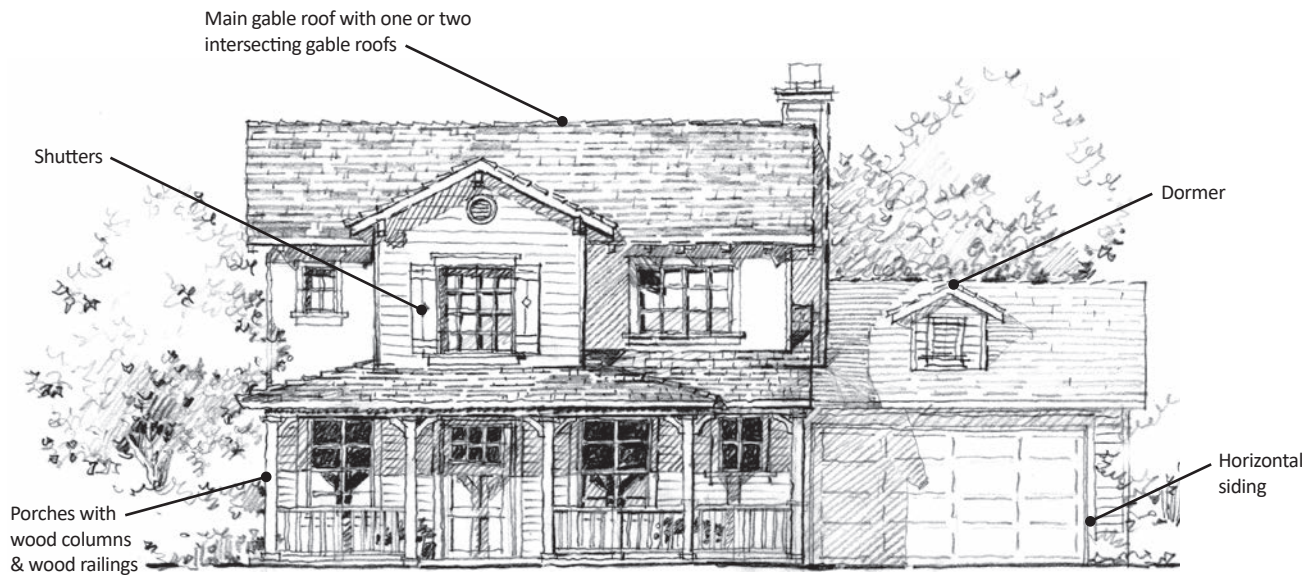


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Farmhouse

The American Farmhouse represents a practical and picturesque country house. Its beginnings are traced to both Colonial styles from New England and later the Midwest. As the American Frontier moved westward, the American Farmhouse style evolved according to availability of materials and technological advancements, such as balloon framing.

Predominant features of the style are large front and/or wrapping porches with a variety of wood columns and railings. Two-story massing, dormers and symmetrical elevations occur most often on the New England Farmhouse variations. The asymmetrical, casual cottage look, with a more decorated appearance, is typical of the Midwest American Farmhouse. Roof ornamentation is a characteristic detail consisting of cupolas, weather vanes and dovecotes.





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Full to Partial Width Front Porch



Shutters with Horizontal Siding Accents



Siding Accents

Element	Menu of Common Features
Form	<ul style="list-style-type: none"> • Simple plan form massing and simple roof design • Main gable roof with one or two intersecting gable roofs • 12" to 16" overhangs with open eaves
Roof	<ul style="list-style-type: none"> • 4:12 to 12:12 roof pitch • Front to back main gable roof • 12" to 16" overhangs with open eaves • Wrapped horizontal siding elements on sides and/or rear elevations • Tiles with smooth flat or shingled appearance
Walls	<ul style="list-style-type: none"> • Horizontal siding may be combined with stucco • All material changes must occur at an inside corner or logical end (return block wall/fence). • Wrapped horizontal siding elements on sides and/or rear elevations in high visibility public view areas
Windows	<ul style="list-style-type: none"> • Vertical multi-paned window inserts at front elevation • Multi-paned window inserts on side and rear elevations in high visibility public view areas • Vinyl wrapped windows • Bay windows • Header trim at front windows • Single hung windows at front
Details	<ul style="list-style-type: none"> • Porches with simple wood columns and wood railings • Shaped wood columns, wood windows and/or door trim • Min. 2x4 window and door trim - wood on siding, foam on stucco • Shutters & layered header trims at front elevation • Cupolas or dormers • Surface mounted fixtures on front elevations must complement architectural style • Garage door patterns to complement style • Stone/brick/stucco chimney wrap • Pot shelves, louvered attic vents
Colors	<ul style="list-style-type: none"> • Field: Whites or light shades of blue, yellow, green, beige or gray • Trim: Whites or light shades complementary to field color • Accents: Medium to dark shades in contrast with field color

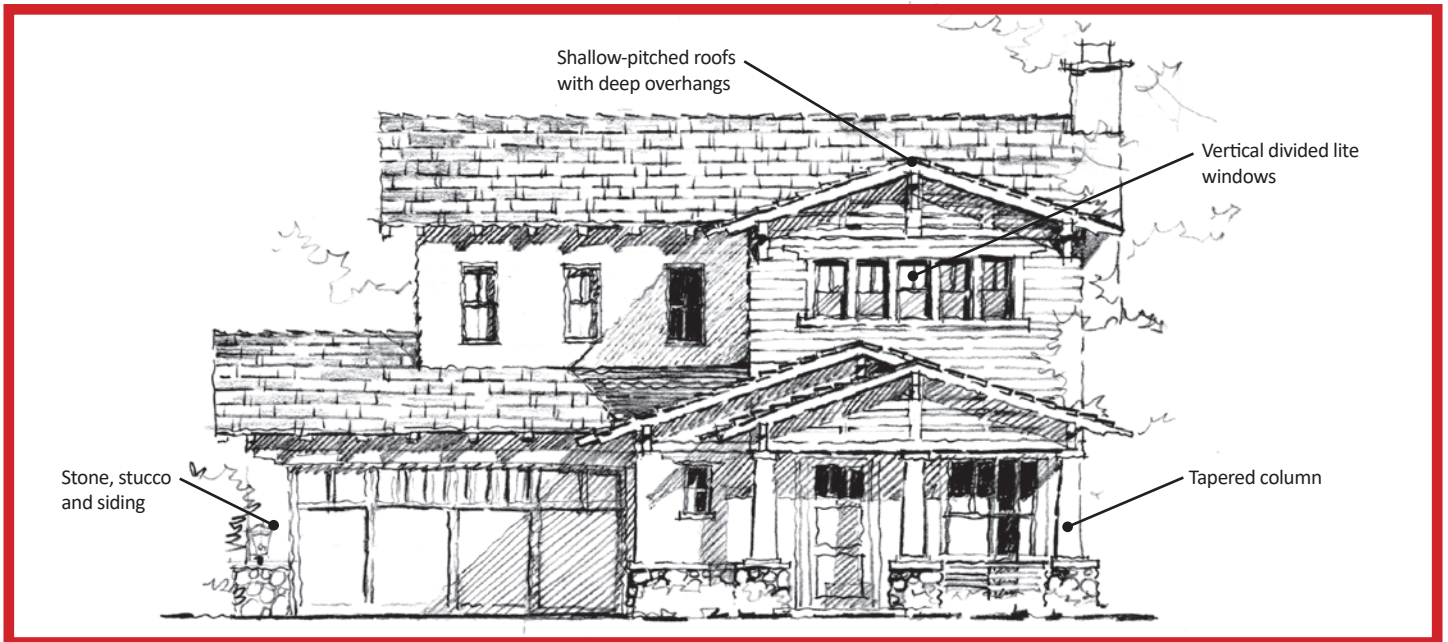


Parkside

Craftsman

The Craftsman style was inspired by the English Arts and Crafts Movement of the late 19th century. Of the utmost importance was that all exterior and interior elements received both tasteful and “artful” attention. The movement influenced numerous California architects such as Greene and Greene, and Bernard Maybeck.

The resulting Craftsman style responded with extensive built-in elements and by treating details such as windows or ceilings as if they were furniture. The style was further characterized by the rustic texture of building materials, broad overhangs with exposed rafter tails at the eaves and trellises over the porches. The overall affect was the creation of a natural, warm and livable home. In Southern California, the Craftsman style spun out of bungalows that were the production home of the time.





Parkside



Front Porch with Battered Columns



Gabled Roof with Deep Overhang



Vertical Divided lite Windows

Element	Menu of Common Features	
Form	<ul style="list-style-type: none"> Simple 2 story boxed massing with vertical and horizontal breaks 	<ul style="list-style-type: none"> Varied plan shapes
Roof	<ul style="list-style-type: none"> 4:12 to 6:12 roof pitch 16" to 24" overhangs Flat concrete tile - shingle appearance Basic gabled roof - side to side with cross gables 	<ul style="list-style-type: none"> Varied porch roofs - shed or gabled 16" to 24" overhangs
Walls	<ul style="list-style-type: none"> Horizontal siding - may be combined with stucco. All material changes must occur on inside corner Stone base accents on walls and/or porch Wrapped horizontal siding elements on sides and/or rear elevation 	<ul style="list-style-type: none"> Shingle siding used as an accent treatment Battered wall accents
Windows	<ul style="list-style-type: none"> Vertical multi-paned window inserts at front elevation Multi-paned window inserts on side and rear elevations in high visibility public view areas Vinyl wrapped windows Use windows individually or in groups 	<ul style="list-style-type: none"> Single hung windows at front elevation Feature ribbon windows 3 or more
Details	<ul style="list-style-type: none"> Entry porches with heavy square columns or posts on stone or brick piers Shaped wood header trim at windows and doors - wood on siding, foam on stucco Simple knee brace Surface mounted fixtures on front elevations must complement architectural style Garage door patterns to complement style Stone and brick base accents Open eave overhangs with shaped roof rafter tails 	<ul style="list-style-type: none"> Full porches with heavy square columns or posts on stone piers Classically styled columns or battered columns Blended stone and brick chimney Wood trim at doors and windows Decorative ridge beams and purlins Triangulated knee braces
Colors	<ul style="list-style-type: none"> Field: Light to dark earth tones Trim: Light to dark earth tones in harmony or contrasting with field Accents: Light to dark earth tones in contrast with field 	



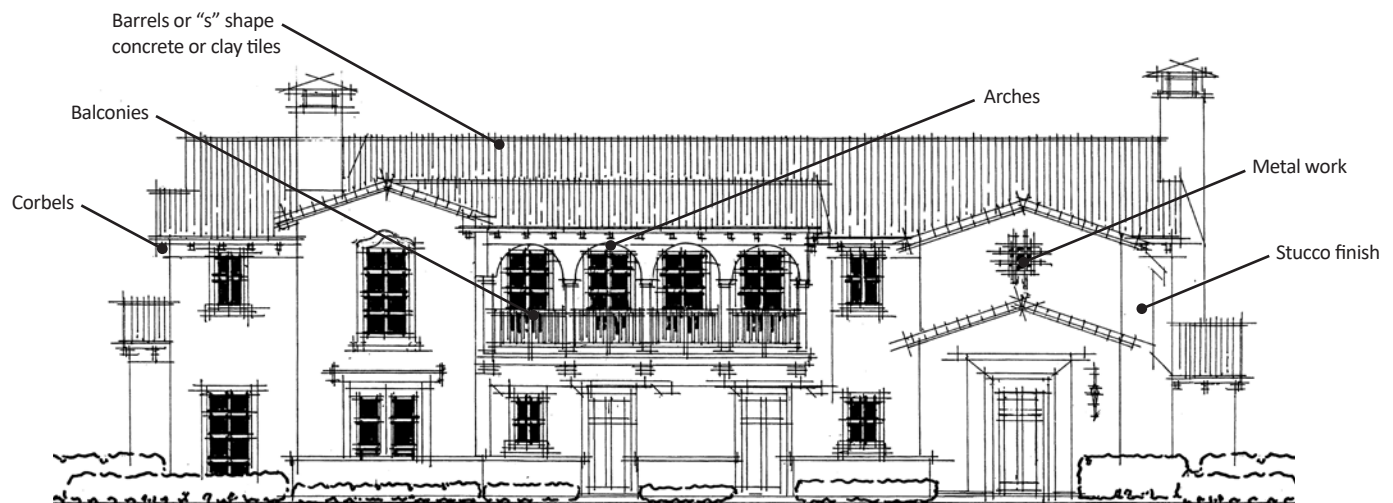
Parkside

Andalusian

The Andalusian style is an adaptation of the farmhouses and cottages found in the rural areas throughout Andalusia in **which is located in the South of** southern Spain. Enriched with details and elements derived from Moroccan, Moorish, and classical Roman architecture this style represents a simple, yet elegant blend of cultures.

The informality of rural life originally inspired a simple, rectangular form that evolved organically over time to meet the spatial needs of the owner. Subsequent massing was typically similar in form, but changed in dimension with new functions and arrangement of spaces. The resultant irregularity in massing and appearance adds to the charm and romance of this southern European style. Its informal rustic character is highlighted with sophisticated details, natural, indigenous materials, warm, sun-bleached colors and rich textures.

Architectural distinction is established through the use of details such as softly sculpted or curved wall and chimney articulation, colorful mosaics and tile work, rustic stone accents, balconies, perforated wall patterns and vents, cantilevered windows, classical elements, arches, and/or **wrought iron** metal grillwork accents.





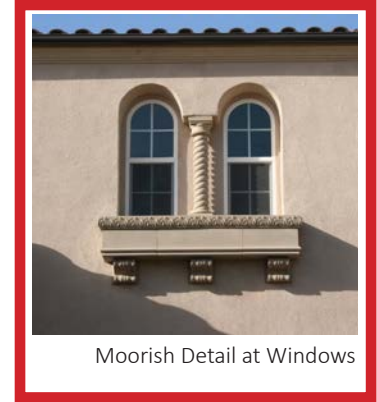
Parkside



Feature Windows and Simple Massing



Vertical Windows with Metal Detail



Moorish Detail at Windows

Element	Menu of Common Features	
Form	<ul style="list-style-type: none"> • 2 story massing with one story elements at courtyards/ entries 	<ul style="list-style-type: none"> • Courtyard plan form • Recessed upper story elements
Roof	<ul style="list-style-type: none"> • 4:12 to 6:12 roof pitch • 12" to 16" - 18" overhang • Simple hip or gable roof forms • Barrel or 'S' shape concrete or clay tiles 	<ul style="list-style-type: none"> • Intersecting gabled roofs
Walls	<ul style="list-style-type: none"> • Stucco finish 	<ul style="list-style-type: none"> • Stucco - sand finish with rustic brick accents
Windows	<ul style="list-style-type: none"> • Vertical multi-paned window inserts at front elevations • Multi-paned window inserts on side and rear elevations in high visibility public view areas • Vinyl wrapped windows • Simple 2x window and door trim - wood or stucco over foam 	<ul style="list-style-type: none"> • Feature recessed arched window • Accent beveled glass recessed window • Single or grouped round top windows
Details	<ul style="list-style-type: none"> • Arcades supported by classic columns at courtyards, balconies and porches • Wrought Iron/Metal or wood balconies and accent details • Enhanced articulation at entrances and feature windows • Decorative ceramic tile accents • Wood or wrought iron/metal railings at balconies or porches • Garage door patterns to complement style • Shaped rafter tails at feature arches 	<ul style="list-style-type: none"> • Balustrades at balconies or porches • Sculpted forms at facades, chimneys, feature windows, etc. • Tile vents in gable end
Colors	<ul style="list-style-type: none"> • Field: Sunwashed white tones or light tints • Trim: Dark brown, burnt red or ochre • Accents: Dark or medium shades of green, red, beige, blue using sparingly 	



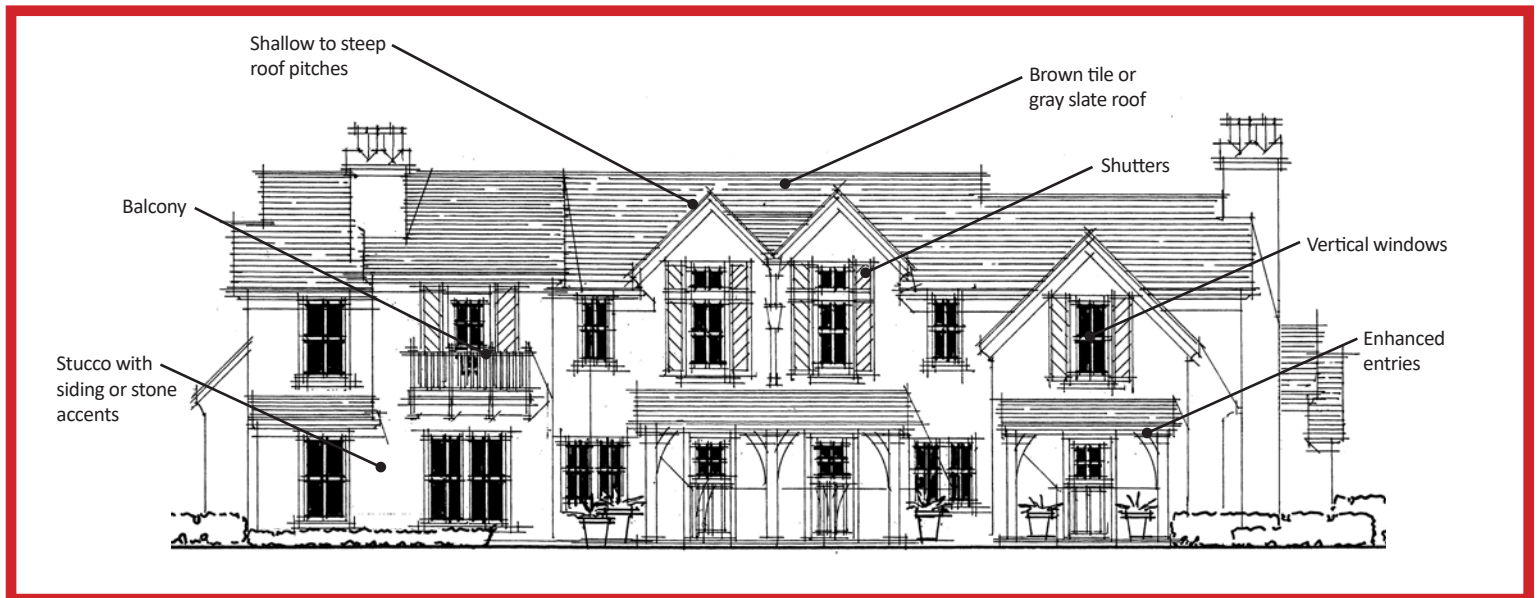
Parkside

Provence

The character of the Provence home could be classified as part farmhouse and part château. Its origins are in the simple rectangular farmhouses and cottages that exist in the rural hillsides and villages within the Provence region of France.

Looked upon as an enchanting style, graceful details such as stone lintels and surrounds at doorways and windows, curved or detailed shutters and asymmetrical accent elements capture the aging essence of the Provence style. The use of chiseled stones and enhanced entries are included in a palette of common features that are subtly placed on elevations.

Provence style roof pitches vary from shallow to steep. Rafter tail ends and wood or metal balconies are silhouetted against building facades, with an unassuming brown tile or gray slate roof.





Parkside



6:12 to 12:12 Roof Pitch



Stone and Wood Accents



Steep Roof Accent

Element	Menu of Common Features	
Form	<ul style="list-style-type: none"> Rectangular plan form massing with some recessed 2nd floor area 	<ul style="list-style-type: none"> Distinctive plan form massing with substantially recessed 2nd floor
Roof	<ul style="list-style-type: none"> Main roof hip or gable with intersecting gable roofs 4:12 to 6:12 roof pitch Shallow overhangs to 12" to 16" Architectural quality asphalt shingles or smooth flat tiles 	<ul style="list-style-type: none"> Main roof hip or gable with intersecting gable roof or steep 2nd story roof breaking over 1st story elements Curved slope at roofline 6:12 to 12:12 roof pitch
Walls	<ul style="list-style-type: none"> Stucco with siding or stone accents 	
Windows	<ul style="list-style-type: none"> Vertical multi-divisioned window inserts at front elevation Multi-divisioned window inserts on side and rear elevations in high visibility public view areas 	<ul style="list-style-type: none"> Curved or round top accent windows Bay windows Single hung windows at front
Details	<ul style="list-style-type: none"> Entry accents with faux stone Garage door patterns to complement style Wrought Iron/Metal or wood balconies and potshelves Shutters Simple 2x4 window and door trim - wood, stone, siding, or stucco over foam 	<ul style="list-style-type: none"> Decorative shutters Stone veneer chimney or accented feature elements
Colors	<ul style="list-style-type: none"> Field: Pale earth tones Trim: Whites or light shades complementary to field color Accents: Light or dark shades in contrast with field color 	



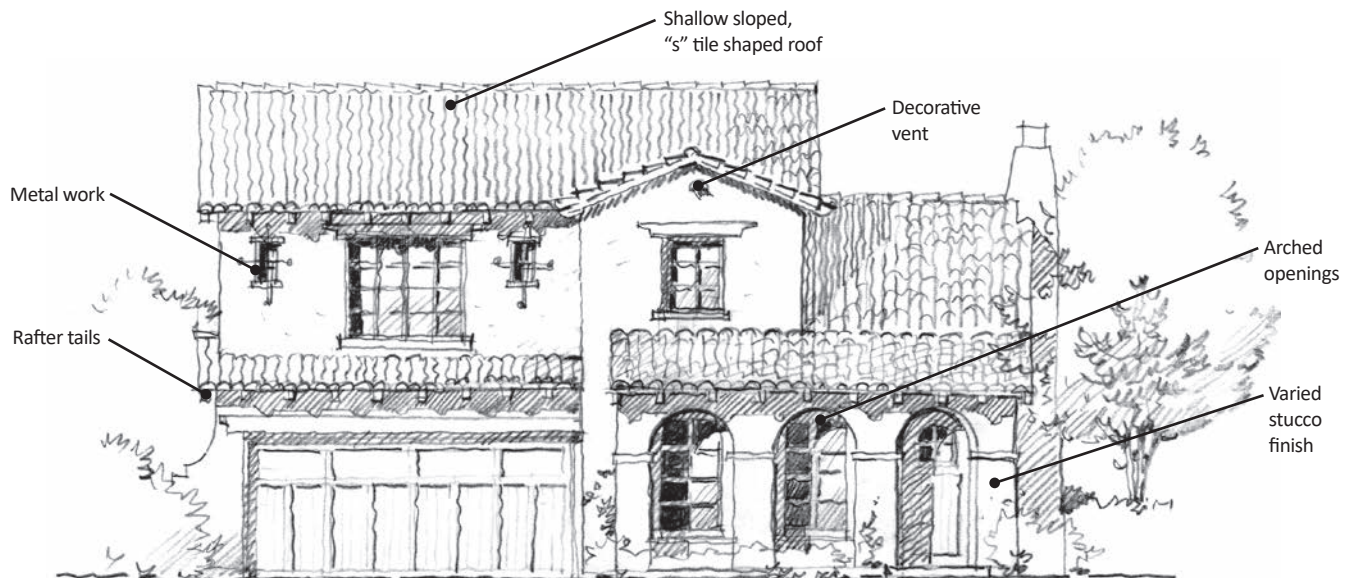
Parkside

Spanish Colonial

Spanish Colonial, also known as Spanish Eclectic is an adaptation of Mission Revival enriched with additional Latin American details and elements. The style attained widespread popularity after its use in the Panama-California Exposition of 1915-1916.

The simple courtyards of the Spanish Colonial heritage with hanging pots, a flowering garden and sprawling shade trees are hardly surpassed as foreground design elements. Further architectural distinction was established through the use of tile roofs, stucco walls, heavily textured wooden doors and highlighted ornamental ironwork.

Key features of this style were adapted to the Southern California locale. The plans were informally organized around a courtyard with the front elevation very simply articulated and detailed. The charm of this style lies in the directness, adaptability, and contrast of materials and textures.





Parkside



Feature Window



Tile Roof, Gable Vent & Metal Accents



Stucco Walls with Arched Openings

Element	Menu of Common Features
Form	<ul style="list-style-type: none"> • 2 story asymmetrical massing with strong one story element • Simply articulated 2 story boxed plan massing with not more than 50% one story element across front elevation
Roof	<ul style="list-style-type: none"> • 4:12 to 6:12 roof pitch • 12" to 16" overhang • Simple hip or gable roof with one intersecting gable roof • Barrel or 'S' shape concrete tiles • Shed roof over porch
Walls	<ul style="list-style-type: none"> • Stucco • Stucco - sand finish
Windows	<ul style="list-style-type: none"> • Vertical multi-paned window inserts at front elevations • Multi-paned window inserts on side and rear elevations in high visibility public view areas • Vinyl wrapped windows • Simple 2x4 window and door trim - wood or stucco over foam • Feature recessed arched window • Accent beveled glass recessed window • Single or grouped round top windows
Details	<ul style="list-style-type: none"> • Stucco over foam window and door trim • Arched stucco column porches • Surface mounted fixtures on front elevations must complement architectural style • Garage door patterns to complement style • Shaped rafter tails at feature areas • Wrought Iron/Metal balconies and accent details • Entry door design to compliment style
Colors	<ul style="list-style-type: none"> • Field: Whites, beige, or warm tints • Trim: Dark shades to contrast field color • Accents: Deep tones of green, blue, red (on shutters, door, balcony trim)

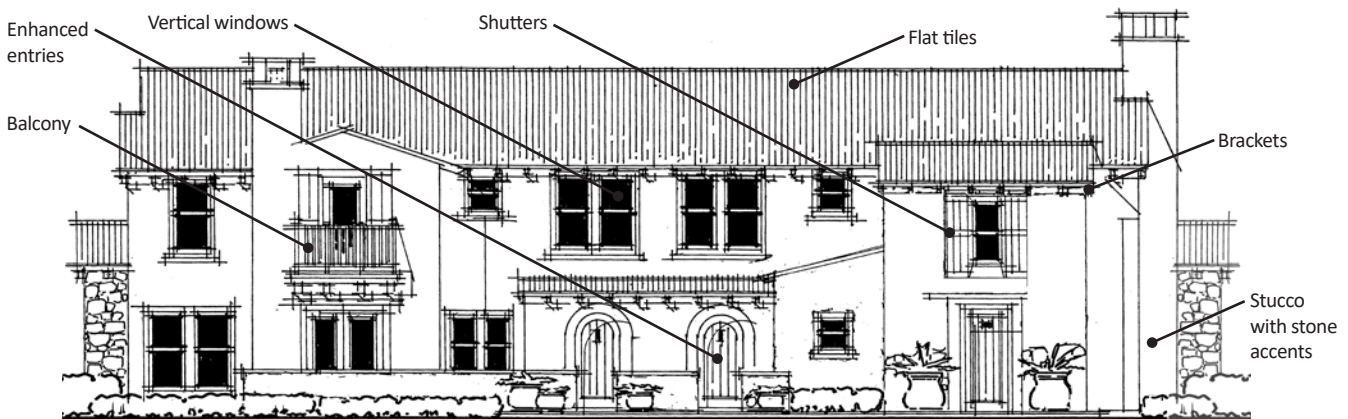


Parkside

Tuscan

During the 15th and 16th century large numbers of houses were built along roads and hillsides in rural Tuscany, many of which, although altered, are still in use today. As this region was and is primarily agricultural, these homes reflect the character of the farmhouse estate or 'podere'. Built by their owners with the indigenous materials and colors of the surroundings, these buildings blend naturally with the land.

The building form and massing is an example of simplicity; a plan that began as a simple rectangular form that evolved organically over time. Smaller components similar to the original form were typically added as necessary to meet the spatial needs of the owner. The resulting building, with the flexibility and variety apparent in this style, is what makes it so appealing. The informality of these rural farmhouse and settlement building types, including their traditional squared towers, eventually became the inspiration for Tuscan villas.





Parkside



Recessed Second Floors Earth Tones



Rectangular Form, Stucco with Stone Accents



Enhanced Entry Articulation Vertical Multi-divided Windows

Element	Menu of Common Features	
Form	<ul style="list-style-type: none"> • Rectangular plan form massing with some recessed 2nd floor area • Enhanced articulation and detail of entries, windows, doorways and balconies 	<ul style="list-style-type: none"> • Distinctive plan form massing with recessed 2nd floor • Squared tower elements at entry or upper story
Roof	<ul style="list-style-type: none"> • Main roof hip or gable with intersecting gable roofs • 4:12 to 6:12 roof pitch • 16" overhangs at eaves • Architectural quality asphalt shingles or smooth flat tiles 	<ul style="list-style-type: none"> • Main roof hip or gable with intersecting gable roof or 2nd story roof breaking over 1st story elements • Brackets under eaves • Deeper overhangs to 24"
Walls	<ul style="list-style-type: none"> • Stucco with stone veneer accents 	<ul style="list-style-type: none"> • Enhanced stone veneer siding
Windows	<ul style="list-style-type: none"> • Vertical multi-divisioned window inserts at front elevation • Multi-divisioned window inserts on side and rear elevations in high visibility public view areas 	<ul style="list-style-type: none"> • Curved or round top accent windows • Single hung windows at front
Details	<ul style="list-style-type: none"> • Entry accents with faux stone • Garage door patterns to complement style • Shutters • Balcony or veranda • Simple 2x4 window and door trim - wood on stone or siding, stucco on stucco 	<ul style="list-style-type: none"> • Decorative shutters • Stone veneer chimney and accented feature elements • Wrought Iron/Metal or wood balconies and potshelves • Rusticated hardware and details
Colors	<ul style="list-style-type: none"> • Field: Medium, saturated earth tones • Trim: Whites tones or light shades complementary to field color • Accents: Light or dark shades in contrast with field color 	



Parkside

Cottage

Cottage is a picturesque style derived from medieval Norman and Tudor domestic architecture. The resulting English and French inspired “cottage” became extremely popular nationwide after the adoption of stone and brick veneer techniques in the 1920s.

Although the cottage is looked upon as small and not costly, it has been recognized as one of the most popular styles in suburban America. Design of the homes typically reflected in the rural setting in which they evolved. Many established neighborhoods in Riverside, Orange, and Santa Ana contain homes with the charm and character that depict the alluring, yet unpretentious lifestyle of the Cottage home.





Parkside



Varying Roof Slopes & Forms



Feature Windows & Gable Accents



Stucco with Stone Accents

Element	Menu of Common Features	
Form	<ul style="list-style-type: none"> • Rectangular plan form massing with some recessed 2nd floor area 	<ul style="list-style-type: none"> • Irregular plan form massing with substantially recessed 2nd floor
Roof	<ul style="list-style-type: none"> • Main roof hip or gable with intersecting gable roofs • 4:12 to 12:12 roof pitch • Curved slope at roofline • 0" to 12" overhangs - (limit tight overhangs to distinguishing character elements) • Architectural quality asphalt shingles or smooth flat tiles 	<ul style="list-style-type: none"> • Main roof hip or gable with intersecting gable roof or steep 2nd story roof breaking over 1st story elements
Walls	<ul style="list-style-type: none"> • Stucco • Stucco with wood siding or stone accents 	
Windows	<ul style="list-style-type: none"> • Vertical multi-paned window inserts at front elevation • Multi-paned window inserts on side and rear elevations in high visibility public view areas • Bay windows 	<ul style="list-style-type: none"> • Vinyl wrapped windows • Curved or round top accent windows • Single hung windows at front
Details	<ul style="list-style-type: none"> • Entry accents with natural or manufactured stone • Shutters • Simple wood 2x4 window and door trim - wood on stone or siding, foam on stucco • Surface mounted fixtures on front elevations must complement architectural style • Garage door patterns to complement style 	<ul style="list-style-type: none"> • Decorative shutters • Stone veneer chimney • Wrought Iron/Metal or wood balconies and wood pot shelves
Colors	<ul style="list-style-type: none"> • Field: Soft, muted mid-tones of beige, tan, blue, yellow, grey or green • Trim: Light or similar mid-shades complementary to field color • Accents: Light or dark shades in contrast with field color 	

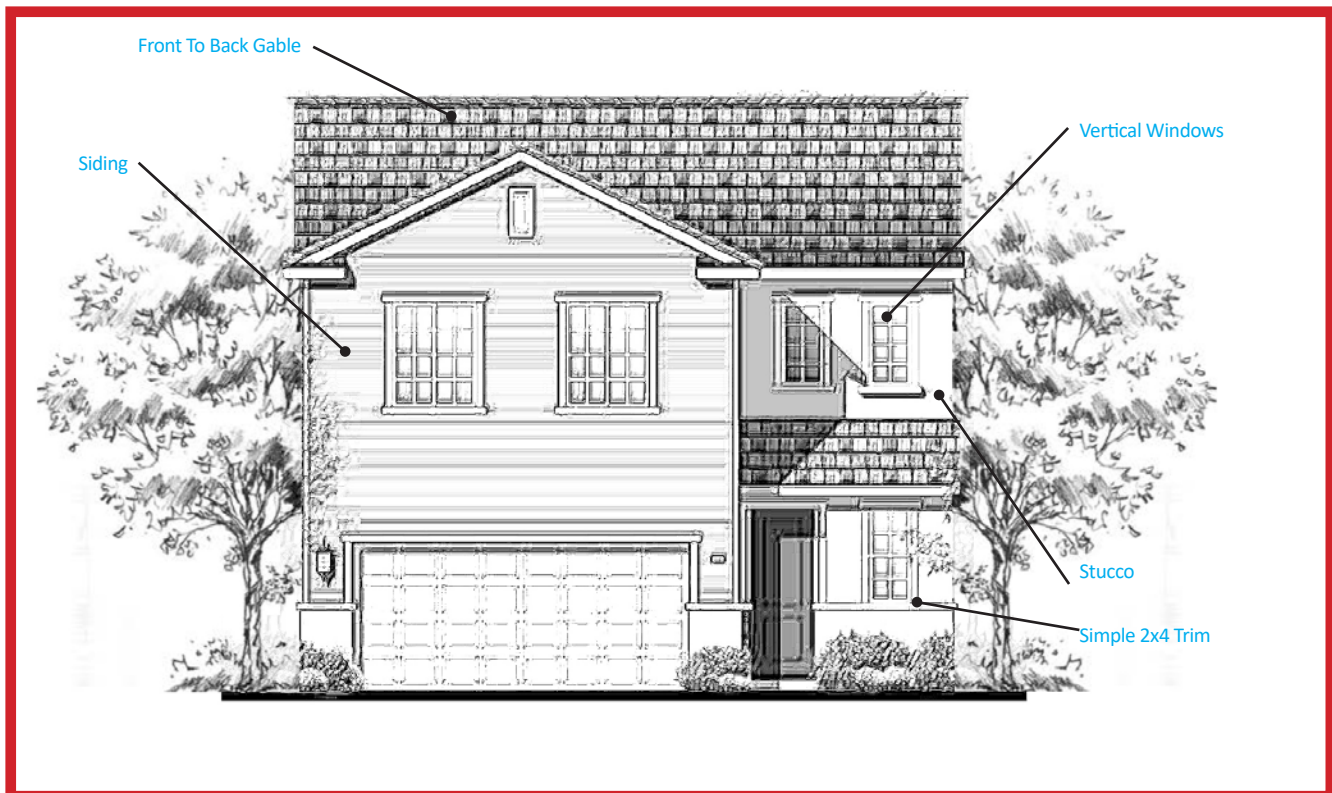


Parkside

Adaptive American Traditional

American Traditional is a style evolved in the early 20th century originating from the New England states, spreading westward, and becoming established throughout the rest of the country.

The adaptive version of this style retains the massing and form that is reminiscent of early American heritage but with simpler lines and contemporary details. The massing breaks down the façade from 2 to 1-story forms providing a natural balance to the front elevation. Roofs are simple in form most often with accent gables. This contemporized American style has a fresh appeal while retaining its charming origins.





Parkside



Porch



Simplified Cornice Trim at Gable Ends

Element	Menu of Common Features
Form	<ul style="list-style-type: none"> Asymmetrical massing with a vertical and a horizontal break
Roof	<ul style="list-style-type: none"> 4:12 roof pitch 12" to 16" overhangs Concrete roof tile- flat or shake appearance Asphalt shingles Front to back gable or hip roof with intersecting hip or gable roofs
Walls	<ul style="list-style-type: none"> Stucco Horizontal siding- may be combined with stucco Wrapped material on sides and terminate at logical end (return block wall/fence) or inside corner
Windows	<ul style="list-style-type: none"> Vertical multi-paned window inserts at front elevations Multi-paned window inserts on side and rear elevations at visible edges
Details	<ul style="list-style-type: none"> Simplified cornice trim at gable ends Header window wood accent trim Simple 2 x 4 window and door trim- wood on siding, foam on stucco, 2 x 4 window and door trim may be used on specific product types, architectural styles subject to Planning Department approval. Surface mounted fixtures on front elevations must complement architectural style Porches and/or balconies with horizontal railing Front and garage door may have modern styling to complement style Square wood columns with trim Shutters, brick accents permitted
Colors	<ul style="list-style-type: none"> Field: Whites, off-white, dark or light colors Trim: White or contrasting with field color Accents: Light or dark colors in contrast or harmony with field colors

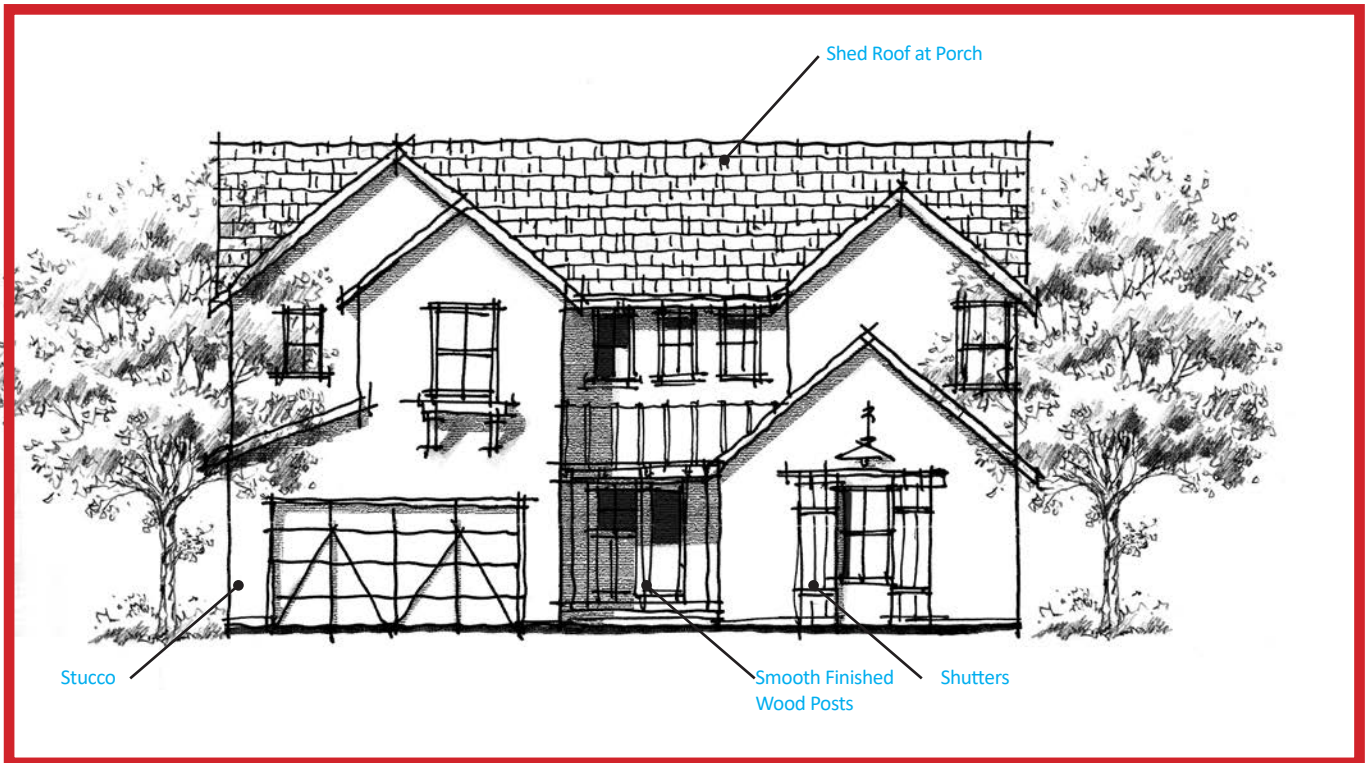


Parkside

Adaptive Farmhouse

Due to the simple and honest nature of this architectural form, the Farmhouse style can easily be adapted to a more abstract solution and is appropriate within many of the higher density settings. Since this style's roof shapes, variety of textures, colors and materials can easily be extrapolated to more contemporary applications, one can find many examples of this transformation throughout the West.

The Adaptive Farmhouse style blends the sleek clean lines of contemporary design with warm farmhouse charm to create a uniquely fresh take on this country living inspired style.





Parkside



Board and Batten Siding



Proper Terminate Points for Siding and Color Accents

Element	Menu of Common Features
Form	<ul style="list-style-type: none"> Symmetrical or asymmetrical massing
Roofs	<ul style="list-style-type: none"> Roof pitches 4:12 to 6:12, possibly lower pitch at main roof Shed roofs at porches, may use metal seam 0" - 12" overhang at eaves Asphalt shingle roof or tile shake pattern
Walls	<ul style="list-style-type: none"> Stucco Board and batten or horizontal siding Siding should return on sides and terminate at logical end (return block wall/fence) or inside corner
Windows	<ul style="list-style-type: none"> Vertically or horizontally proportioned
Details	<ul style="list-style-type: none"> Smooth finished posts (6" x 6" min.) with smooth finish cap and base trim Smooth finished beams Door and window surrounds shall consist of one of the following materials: <ul style="list-style-type: none"> Well-proportioned, stucco-wrapped, high density foam trim or Smooth textured wood Raised panel-style entry door and garage door Wood brackets below gables and/or rafter tails Awnings, may use metal seam Porches and/or balconies with horizontal railing Shutters Cementitious or stucco trim (with finish matching body of house) may be permitted at front doors/porch entries Front and garage doors may have modern styling to complement style
Colors	<ul style="list-style-type: none"> Field: Whites or light tinted colors Trim: Whites or light shades complementary to field color Accents: Light or dark shades in contrast with field color



Parkside

Adaptive Mediterranean

The Adaptive Mediterranean style draws inspiration from Southern European countries along the Mediterranean Sea. Italian, Spanish, Portuguese and Greek architecture lend their influence to this style.

Simplified details borrowed from these Mediterranean styles include streamlined forms which move toward a modern and unpretentious aesthetic – including low-pitched, hipped or parapet roofs combined with a lighter color palette.





Parkside



Light or dark colors in contrast and in harmony with field colors



Limited ornamentation and no shutters

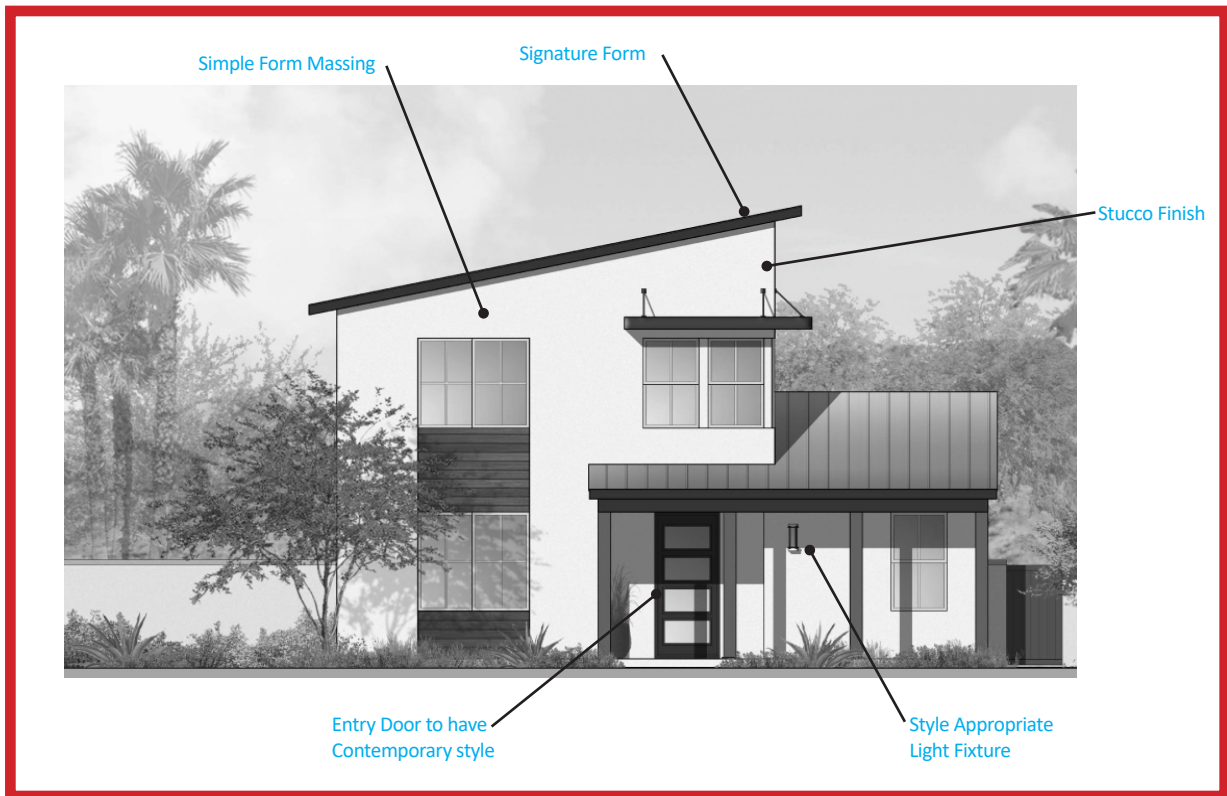
Element	Menu of Common Features
Form	<ul style="list-style-type: none"> Simple geometric forms
Roofs	<ul style="list-style-type: none"> Hip or gable roofs Roof pitches 4:12 to 6:12 where applicable, possibly lower pitch at main roof; may have a shed roof accent 0"-12" overhang S-shape, or flat tile, or asphalt shingle
Walls	<ul style="list-style-type: none"> Stucco Optional decorative ceramic tile
Windows	<ul style="list-style-type: none"> Vertically or horizontally proportioned windows or ganged windows Round top or arched primary window or architectural feature
Details	<ul style="list-style-type: none"> Front entry doors without a porch, recessed from front facade Limited ornamentation and no shutters Decorative metal elements or ceramic tile Cementitious or stucco trim (with finish matching body of house) may be permitted at front doors/porch entries Style appropriate light fixtures Horizontal railings at porches or balconies Built-up eave Color Blocking Front door and garage door may have modern styling to complement style
Colors	<ul style="list-style-type: none"> Field: Toned whites and light to medium light value warm colors Trim: Medium dark value browns reminiscent of stained wood Accents: Light or dark colors in contrast or harmony with field colors



Parkside

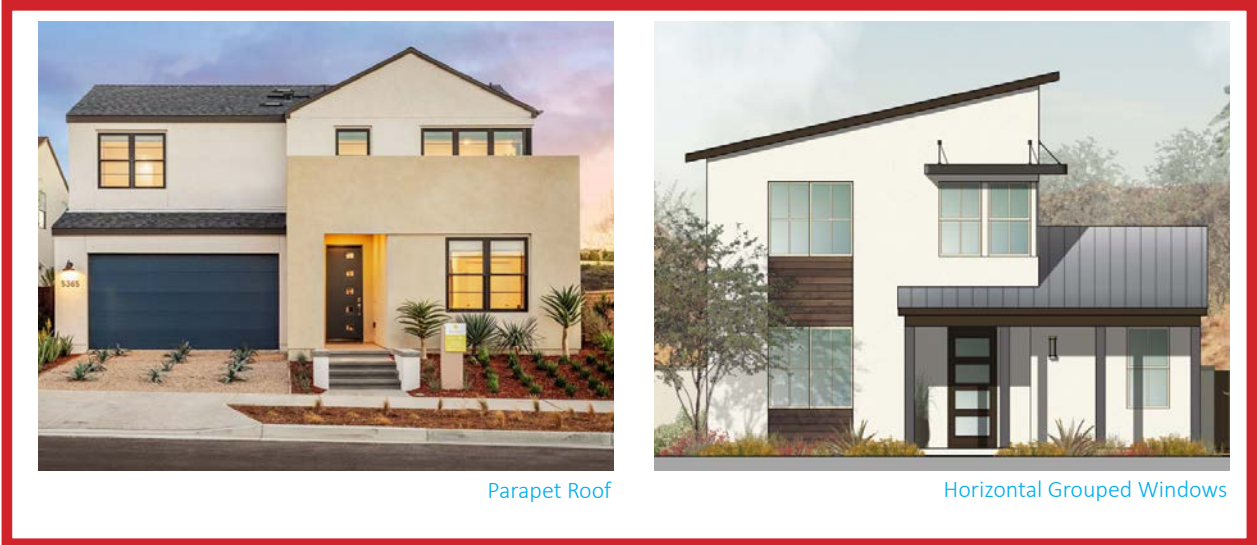
Contemporary

Clean lines and ordered appearance. An architectural expression that is forward facing and suggests a separation from the old world.





Parkside



Parapet Roof

Horizontal Grouped Windows

Element	Menu of Common Features
Form	<ul style="list-style-type: none"> • Simple rectangular form massing • Signature form, detail or feature
Roofs	<ul style="list-style-type: none"> • Parapet, hip or shed roof • Simple unadorned parapet walls • Roof pitch range: flat or 4:12 to 6:12 • 0" - 12" overhang at eaves • Shed element at entry with metal seam roof • Asphalt shingles
Walls	<ul style="list-style-type: none"> • Stucco finish • Cementitious siding
Windows	<ul style="list-style-type: none"> • Vertically or horizontally proportioned or grouped windows
Details	<ul style="list-style-type: none"> • Entry accents such as awnings with metal seam roof or metal eyebrows • Cementitious or stucco trim (with finish matching body of house) may be permitted at front doors/porch entries • Style appropriate light fixtures • Horizontal railings at porches or balconies • Front door and garage door may have modern styling to complement style and may include clear or frosted glass window details • Infill panels
Colors	<ul style="list-style-type: none"> • Field: Whites or light shades • Accents: Light or dark shades in contract with field color



Parkside

7.3 Massing (Single-Family)

7.3.1 Introduction

Creating streetscenes that function well and have visual interest is a primary community objective. The following basic elements and criteria **are intended to develop provide** variations in appearance and a sense of individuality for each home. Neighborhoods that have nearly identical homes and streets without variation in product placement and form will not be approved in design review. This section lists architectural massing and plotting techniques that will help to create a quality environment.

Objectives:

- Minimize visual impact of garages;
- Give attention to composition of building mass;
- **Incorporate single-story elements in two-story buildings;**
- Vary setbacks at porches, living and garage areas;
- Minimize 2- and 3-story dominance on **street (including lanes) scene, sidewalks** the streetscene **including lanes/alleys** and open spaces;

- Design with sensitivity to corner lot conditions;
- Use appropriate transition of scale; and
- Use four-sided elevation design.

In PAs that incorporate higher density single-family homes, high quality architecture is expected. Higher density homes call for simpler designs yet creative execution to reflect the Parkside vision. Higher density homes in correlation to the market demand, calls for innovative and more contemporary design. Therefore, the following objectives are encouraged:

- Minimize visual impact of garages;
- Give attention to composition of building mass;
- Where feasible, incorporate single-story elements;
- Design with sensitivity to prominent edge conditions and highly visible architecture;



Single-Story Elements



Parkside

- Concentrate enhanced design features to visible edges, while creating simple but quality elevations on the interior plans.

7.3.2 General Elements

The general elements of building massing include:

- Front articulation;
- Roof form;
- Garage placement;
- Balconies and projections;
- Rear articulation;
- Variable setbacks; and
- Corner lots/critical edge conditions

Neighborhood Massing and Plotting Concepts

To provide more interesting neighborhood streetscenes, varied massing on all elevations is encouraged.

Where possible when plotting, refrain from strict compliance to minimum garage setbacks to avoid contributing to repetitious streetscenes. Typically, plans are to be reversed and plotted so that garages and entries are adjacent to each other to create an undulating setback. Occasionally, this pattern should be broken to avoid monotony.

Simple Home Design

In an attempt to design aesthetically pleasing architecture that is economically feasible and functions well, the concept of “The Simple House” is to be implemented. This suggests that starting with simple structural forms and building masses and appropriate roof forms can

lead to the development of an “authentic” style while maintaining acceptable building costs. In order to achieve this the following basic criteria have been established:

- Design “simple,” attractive homes using simple plan forms and style-appropriate massing and roof pitches;
- Use style-appropriate elements and details to enhance good architecture;
- Provide a variety of single-story heights and profiles while stepping back second-story massing when possible;
- Acknowledge both street front facades on corner homes or buildings by ‘wrapping’ usable spaces, architectural elements, and materials around to face the side street frontage;
- Give added attention to side and rear articulation if facades are publicly exposed;
- Use covered and shaded spaces such as porches, landscaped courtyards or structural landscape elements to provide a sense of entry and respond to local climate conditions; and
- Respond to overall community character with appropriate choices of style, color, and materials.

Refer to Sections 7.2, 7.3 and 7.4 for specific guidelines concerning styles, massing, elements, and details of simple residential architectural design.

Single-Story Elements

Single-story elements [may be incorporated](#) on homes to help establish neighborhood pedestrian scale and add variety to the streetscene.



Parkside

If product allows, a minimum of 33% of Encourage homes in each neighborhood should to have at least one significant single-story element on an exposed front or side elevation.

Single-story elements include:

- Front or wraparound porch;
- Roofed porte-cochere;
- Single-story living space in conjunction with a second story recess of at least 5 feet; and
- Pop-out gable element (enclosed or open).

This requirement may be satisfied with the addition of a one-story porch or other elements listed above.

Where forward garages are used single-story massing is encouraged.

Two (2) enhanced elements are required on all homes without porches (see individual style matrices, on pages 7-4 to 7-21, for appropriate “enhanced elements”).

Planning Area 1-4

The goal of PA 1-4 is to provide single-family home-ownership opportunities to a greater span of the buyer population. Although single-story elements are desirable,

smaller home configurations may not lend themselves to this feature. However, where feasible, single-story elements are encouraged.

7.3.3 Front Articulation

The appearance of the streetscene and collection of homes within it is typically the first impression of any neighborhood. Most homes are seen primarily from the front and represent the transition from public to private space as well as the character of those who dwell within.

With this in mind, it is important that the front of the home, and any sides exposed to the street receive thoughtful design and articulation to bring character and quality to the individual home and to the neighborhood as a whole. The following guidelines will help to achieve that level of articulation:

- Design neighborhood plan packages with forms and front elevations in mind to work together in controlling the impact of buildings on the streetscene;
- Provide variety in form and massing to reflect interior uses and achieve attractive and interesting elevations;
- Design placement of windows to work with interior uses and provide “eyes on the street;”



Varied Massing on Elevation



Varied Roof Forms



Parkside

- Consider the entry as a focal point when designing the front elevation; and
- Provide an appropriate level of detail and trim at entries, doors, and windows to enhance good design.

7.3.4 Roof Forms

Rows of homes seen from a distance or along arterial roads are perceived by their contrast against the skyline or background. The dominant impact is the shape of the building and roof line. Articulate the building mass and roof lines to express a variety of conditions and minimize the visual impact of repetitious flat planes, similar building silhouettes and similar ridge heights. This can be achieved by designing a discernibly different roof for each home plan. Individual roof plans may be simple but between different plans should exhibit variety by using front to rear, side-to-side, gables, and hipped roofs and/or by the introduction of one-story elements.

Planning Areas 1-4

Roof designs that maximize space for photovoltaic panels and are orientation for optimal solar efficiency are encouraged. ~~have less roof variation due to increased roof clearances and solar accessibility needed to meet CA Energy Code requirements. Therefore, homes found in PA 1-4 will exhibit roof variation along streetscenes where possible.~~

7.3.5 Garage Placement & Treatment

Create attractive and comfortable streetscenes and pedestrian spaces by designing lane-accessed garages.

Lanes/Private Alley Facing Garages

Although lanes and private alleys are thought of as merely

a transitional vehicular space, they also provide garage and sometimes direct residence access, trash collection and other daily utility uses. They may be experienced by homeowners as often as the front entries to these homes.

Therefore, when designing lanes and motorcourts consider both functional and aesthetic issues. It is the intent to provide a pleasant experience for daily users by incorporating both in the elements of lane design.

~~Lanes (private alleys) lined with homes and garages that have similar or identical facades, setbacks, roof lines, color schemes, and landscape treatments are~~



Varied Garage Door Appearance



Parkside

monotonous and lack individual identity. This condition is not permitted.

Where feasible, in order to avoid the aforementioned condition minimize undesired streetscenes in lane/private alley conditions, the following requirements must be met-are suggestions:

- Avoid a multi-storied, “canyon-like” effect within the lane by providing varied massing, vertical and horizontal offsets of primary architectural elements and roof lines.
- Vary garage door appearance by using door patterns, colors, and windows appropriate to individual architectural styles;
- Where feasible, undulate the line of garage doors where space permits it. All drive apron and building separation setbacks shall be maintained (refer to Section 7.6, Home Types, for specific setback requirements) Where possible, vary ground floor building setbacks to provide undulation in the line of garage doors. All drive apron and building separation setbacks shall be maintained (refer to Section 7.6, Home Types, for specific setback requirements);
- Articulate building elevations with style-appropriate window trim, elements and details;
- Vary garage door appearance by using door patterns,

colors, and windows as appropriate to individual architectural styles; and

- For Conventional Compact Single Family Homes (access from local public/private street) shallow recessed garages shall be limited to 25% of the total umber of units in each builder’s plan package and shall be setback a minimum of 18 feet from property line.
- Design and locate rear privacy walls and pedestrian gate entries to provide ease of access to the rear yard, allow sufficient dimension for landscape requirements, and screen trash can storage from lane; and
- The number of homes with shallow recessed garages shall be limited to 25 percent of the total number of units in each builder’s plan package and shall be setback a minimum of 20 feet from the property line.

The impact of repetitive garages can be reduced by using the following techniques:

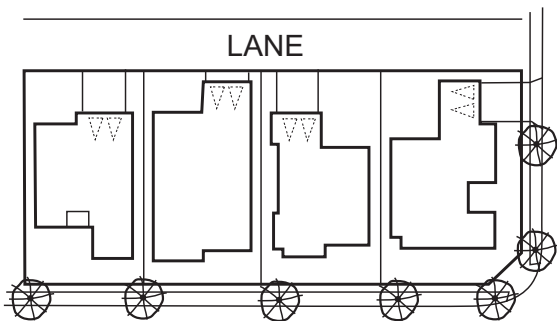
- Provide landscape planters (as feasible) within lanes to soften the appearance of the building elevations.

Vary garage door appearance

Vary garage door pattern, windows and/or color as appropriate to individual architectural styles. Additional treatments are encouraged to buffer direct or monotonous views of the garage doors.

Vary garage placement from plan to plan

Provide Where possible, provide varied garage placements in each lane-loaded neighborhood by reversing placement, varying horizontal massing, and offsetting or cantilevering upper-story massing.



Varied Garage Placement— Lane (Private Alley) Facing



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This garage placement allows the option of entering from the side street, thereby eliminating the garage and driveway from the front or lane (private alley) side of the house.

Optional Garage Screening Treatments

Garage screening treatments are encouraged to reduce or buffer the impact of garages and garage doors from the street or lane (private alley). **Install** These optional elements **such as** may include **vine pockets** or an attached trellis beneath a single-story garage roof fascia and/or above garage door header trims.

7.3.6 Balconies and Projections

The inclusion of balconies and projections **are encouraged** **may be used** for both aesthetic and practical purposes. They are useful in breaking up large wall planes, offsetting floors, providing shade, creating visual interest, and adding human scale to a building. Balconies also provide the practical advantage of extending living areas to the outdoors and providing elevated spaces and views.

- Balconies may be covered or open;
- They may be partially recessed into the mass of the building or serve as a projecting element ; **balconies and patios must adhere to the building to building setbacks;** and
- **Design** **Balconies should be designed** as integral elements of the building with details, eaves, supports, and railings **in keeping consistent** with the architectural style **and other elements of the building's design.**

Other architectural projections may include elements such as cantilevered massing, secondary roof changes, niches in plan, and bay windows.

7.3.7 ~~Articulation of All Elevations~~ Elevation Enhancement

The second-story portion of **all** elevations **of homes that are adjacent to facing public** roads, parks, multi-purpose trails, paseos, or public open space, must include a variety of window treatments, roof projections, **etc.** **Where these conditions of visibility exist, a** **to provide**



Cottage



Farmhouse



Parkside

an appearance similar in quality to front elevations. A variety of hipped and gabled roof forms must be used in each neighborhood. One-story elements also help to provide this variety.

7.3.8 Varied Massing

Provide varied massing and articulated elevations **keeping consistent** with the selected style and product type.

Floor Plan Form

Keep architectural styles in mind when designing the floor plan. For example, styles such as Cottage or Farmhouse that reflect asymmetrical massing and steeper roof pitches may be applied to a similar floor plan and elevation while both maintain an authentic appearance.

7.3.9 Corner Lots/Critical Edge Conditions

Neighborhood quality will be enhanced by adding a home plan designed specifically for the corner, or by enhancing an interior lot plan for use on the corner with additional architectural elements and/or details as found on the front elevation.

~~All corner homes (Conventional, Lane-Loaded, and Cluster) shall wrap materials (where applicable to the style/front elevation) and continue articulation around to the corner side facade.~~

All corner homes must include wrapping materials and continued articulation around to the side facade. All material changes must occur on an inside corner, on a logical end, or coincide with an architectural element that conceals the material change.

Corner Plans

~~At least two (2) plans per neighborhood~~ Conventional and Lane-Loaded residential product neighborhoods should be designed **for optional use on** with enhanced corner lot treatments. One of these plans ~~shall~~ may include an architectural element that encompasses an interior room or living space that ~~wraps around or~~ occupies the corner side yard.

- A second plan (primarily for conventional product) should include ~~a single-story~~ an element that helps to articulate the side facade such as:



Wrapping Materials



Corner Plan



Corner Plan with Porch



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- A wraparound porch;
- Pop-out, side gable;
- Significant wall plane offset; or
- Appropriately detailed second-story pop-out.

These elements will also provide logical stopping points/less conspicuous inside corners for wrapping materials. **Corner plans should also consider use of the following elements:**

- ~~Corner plans should also consider use of a~~ A corner-related front entry door placement; and
- A garage placement that allows entry from the side street or lane.

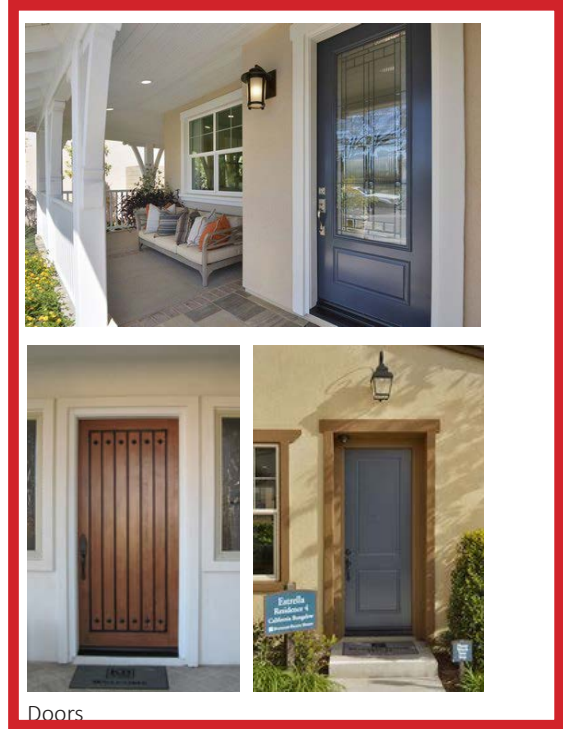
~~Where applicable, Cluster Home products should be designed appropriately to address the front and corner side exposure.~~

- A second plan should include an element that helps articulate the side facade such as:
 - A wraparound porch;
 - Pop-out, side gable;
 - Significant wall plane offset;
 - Appropriately detailed second-story pop-out; or
 - A single-story element.

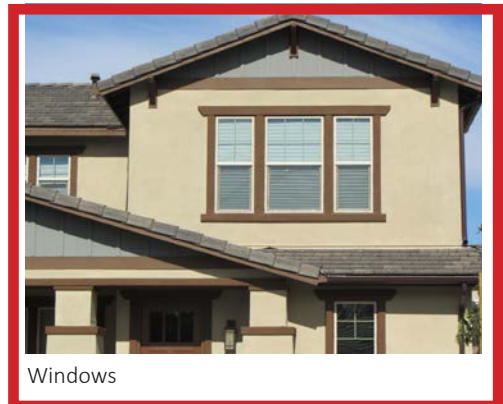
Planning Areas 1-4

PA 1-4 will offer progressive housing types geared toward an attainable price-point. Thus, building footprints will be more compact and efficient, leaving less room for considerable architectural pop-outs and features. However, corner enhancements are still encouraged with the following recommendations:

- Front elevation enhancements shall wrap onto the side elevation on all homes where the side elevation faces a loop road, are street side or a corner elevation.



Doors



Windows



Parkside



Rakes and Eaves

- Where feasible, an additional 5-feet of landscape setback shall be provided along the homes fronting the loop roads and the Great Park.

Community Recreation and Common Facilities

Private common recreation facilities may include a pool, spa, clubhouse, barbecue areas, or other facilities as appropriate to the community. Common facilities act as key character elements **in these neighborhoods** therefore the following should be considered when designing for such facilities.



- Clubhouse and other common area buildings should exhibit a high level of quality and attention to detail on all visible sides of the building;
- All architectural and community elements, such as street furnishings, benches, and lighting standards should be consistent with the selected overall architectural character for the community; and
- Colors, massing, roof pitch, and materials of surrounding residential buildings should also be considered in conjunction with the design of the common facility.



Lighting and Fixtures

7.4 Materials and Details

7.4.1 Introduction

Although the character and style of homes should be established primarily through the use of massing, roof forms and key architectural elements, the addition of appropriate secondary character elements, detailing, and accent materials provides an effective means of further reinforcing and enhancing character and style interpretations.

Key elements are those primary architectural features of the home that clearly discern one style from another. Secondary elements and details are identified as those that help to reinforce or enhance an already identifiable style. A list of appropriate elements, colors and materials



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for each individual style is found in the requirements matrices in the “Styles” section of this document.

The key or primary elements are:

- Doors and Entries;
- Windows;
- Rakes and Eaves;
- Exterior Lighting Fixtures;
- Roof Materials;
- Colors;
- Accent Materials; and
- Wall Finishes.

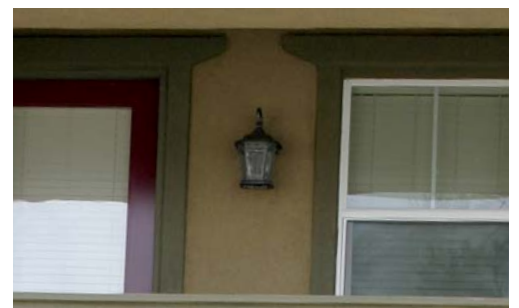
7.4.2 Doors and Entries

Building entries represent an important point of interface between public and private spaces typically creating the initial impression for the user and a focal point for the viewer. Front doors and associated entry spaces should ~~thereby~~ be treated with a sense of importance and detailed ~~so as~~ to emphasize their location and purpose. In designing the door and entry space of homes the following guidelines shall be observed:

- Design of door and entry spaces shall be consistent with the architectural style of the home;
- Doors shall be protected by a recess, porch or other covered element; and
- Door and entry spaces shall reflect a level of detail appropriate to their function and architectural style.
- ~~Surface mounted lights shall not be permitted in garage door soffits; and~~



Roof Materials



Accent Materials



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Finishes

7.4.3 Windows

The following guidelines shall be observed for windows:

- Proportions and alignment shall be appropriate to individual style;
- No highly reflective glazing shall be used;
- Windows may be vinyl-wrapped or wood;
- When using inset windows, provide minimum 2-inch depth;
- Window placement shall be designed to enhance privacy between neighbors;
- Full window trim is encouraged to be a minimum of twelve (12) inches in depth on street facing elevations;

- Shutters shall be sized to match window width and exhibit style-appropriate grates hardware; and
- Where appropriate accent features such as shutters and tile surrounds are encouraged. ;and
- Avoid direct alignment of windows between homes to ensure privacy.

7.4.4 Rakes and Eaves

As with other architectural elements, roof edge treatments vary with the character of each style. Rakes and eaves shall reflect that of each individual style and contribute to the overall look and feel of the community.

- Elements and proportions of rakes and eaves shall be appropriate to the individual style;
- Where appropriate to individual style, larger eave overhangs are encouraged to provide opportunities for shading and relief; and
- Rafter tails, when exposed, shall be a minimum of four (4) inches and proportionate to building painted or stained.



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7.4.5 Exterior Lighting and Fixtures

In the neighborhoods of the Specific Plan Emphasis shall be placed on lighting for safety, security, and aesthetics. Street poles and wall fixtures shall be used to provide sufficient light for traversal and visibility thereby providing the community with a comfortable level of safety and security at night. Every attempt should be made to reduce ambient overflowing light on adjacent homes and to increase the amount of light in less visible areas.

Opportunities to use lighting for directional and aesthetic reasons should also be considered. Where fixtures are not an important focal point, conceal light sources and concentrate on achieving the maximum effects of good lighting design. Where fixtures are part of architectural detailing they should reflect the selected style.

- Lighting used on walls and walkways shall focus light down and provide appropriate down-casting hardware to minimize glare;
- Ambient light shall be down-cast to reduce the impact on the neighborhood;
- Surface mounted lights shall not be permitted in garage door soffits; and
- Wall mounted lighting fixtures shall be selected according to the individual style of the home or building.

7.4.6 Roof Materials

Roof materials, colors, and treatments should correspond to the individual character or style of the home or building and be compatible with the overall look of the neighborhood. Style appropriate materials and colors are found in each appropriate matrix in the “Architectural

Styles” section of this document.

- Where feasible, a variety of roof plans, forms and ridge heights shall be provided using hips, gables, and intersecting elements to avoid a repetitious skyline;
- High quality composition, concrete, or clay tiles may be used in conjunction with the style of home; and
- Skylights are prohibited on sloped roofs facing public streets.

7.4.7 Colors

Varied palettes of regional and style-appropriate colors and materials have been selected for the homes in the Parkside Specific Plan area. This inherently rich and diverse palette will provide the variety required for visual interest while providing a sense of unity within the neighborhoods. Style-appropriate color selections will enhance the architectural character and impart individuality to each home.

- Color shall contribute to distinguishing the overall architectural character of the dwelling. Refer to “Architectural Styles” section of this document matrices for individual color palettes;
- Hue variation in adjacent homes shall be provided to create diversity within the neighborhood;
- Diversity of color is encouraged; and
- Roof tile colors will be selected according to architectural styles.



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To further this goal of diversity, the following criteria shall be met:

- Elevation/color schemes shall consider the composition of colors, appropriately combining colors for an aesthetic and varied streetscape. Consider field color, trim color, accent colors, and accent materials colors.
- Individual color schemes must be appropriate to the architectural styles with a harmonious selection of accent materials, roof profiles and colors;
- No adjacent or facing home shall have the same color scheme; and
- No more than 4 of the same floor plan shall be plotted adjacent to one another.
- Architectural style of homes may be identical within individual courts/lanes.

7.4.8 Accent Materials

Accent materials ~~should~~ can be used to enhance and reinforce the architectural style and composition of individual homes and provide variety in the streetscene. Selective use of appropriate materials, color, and placement can provide maximum impact while imparting a sense of unique character to each home.

- ~~Culmination of accent materials shall terminate at inside corners or coincide with an edge or architectural element to conceal changes in material. Where views are limited or edges concealed by an architectural element, accent materials may terminate at privacy wall conditions;~~

- All color and material changes shall occur at inside corners or other logical locations to provide natural transitions from one aesthetic to another;
- Natural stone, approved manufactured or cultured stone, painted or natural brick, precast concrete, ceramic tile, slump block, and horizontal or vertical siding or approved manufactured siding, (i.e. cementitious board) are encouraged;
- Accent materials shall be selected to complement the overall color and style of the home or building; and
- ~~Architectural~~ Where incorporated, architectural trim applied to all elevations is encouraged to shall be sized properly and consistent with front elevation and architectural style.

7.4.9 Wall Finishes

Style-appropriate wall finishes should visually enhance individual homes. The use and placement of extreme contrasts in color and/or materials should be carefully considered in order to maintain a compatible overall look throughout the neighborhood. Footings shall be exposed no higher than 6 inches above finished grade.

Permitted Finishes:

- Stucco-light to medium sand finish (~~“Spanish Lace” finish is prohibited~~);
- Exposed masonry walls (brick, slump block, split face, etc.);
- Stone, brick, adobe, brick veneers (accent materials);
- Horizontal plank siding; and
- Board and batten siding.



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7.5 Attached and Multi-Family Guidelines

It is the intent for all architecture in Parkside to achieve a high level of quality in building function and visual appearance, assure variety and compatibility in architectural character and to enhance the community's overall value. The goal is to promote these qualities in conjunction with the landscape and planning by using heritage styles combined with modern technology and architectural innovation to provide a pleasant, livable community.

By the very nature of the building types, attached and multi-family home neighborhoods are much like small villages, or communities. Each should be designed for compatibility within itself, using a blend of compatible architectural styles and a tastefully balanced palette of colors and materials.

The following general concepts should be considered when planning for and designing multi-family housing. See the following section for added criteria that is specific to each building type.

- Design and site buildings with a strong physical relationship to public areas of the community.
- Emphasize pedestrian access and connections to public sidewalks, paseos, and open space systems when preparing site plans.



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7.5.1 Architectural Inspiration - Themes and Styles

One key to ~~this community's~~ success is ~~of this community is an appropriate overall theme and the choice of supporting compatible~~ architectural styles. ~~Each multi-family planning area in Parkside is related to a private recreation area or the "Great Park."~~ Appropriate styles, in a simplified palette, include selected Mediterranean styles of Europe ~~including and Each of these community areas will reflect an architectural and landscape environment that expresses the specific character of that region with the entire community being linked by landscape elements and plantings common to all of these regions:~~

~~An important goal in Parkside is also to develop a strong community character and varied and interesting street scenes. In order to achieve this, an architectural palette has been selected that represents those styles found in each regional area of Mediterranean Europe styles that adapted to the Southern California climate.~~

The selected architectural ~~style regions for Parkside are~~ styles for high density attached units include:

Mediterranean Heritage Styles

- Andalusian
- Provence
- Spanish Colonial
- Tuscan

~~Each multi-family planning area is designated for a selected primary style that will be reflected in the recreation facilities and parks and most of the architecture for that area. A secondary, different style will also be selected for use in that area by proximity to other styles; using only the style which is in the adjacent or closest planning area.~~

Millennium Styles

- Adaptive American Traditional
- Adaptive Spanish
- Adaptive Farmhouse
- Adaptive Mediterranean
- European Heritage
- Contemporary

Plan and Style Requirements for Attached Homes:

- Provide at least 2 building plans per neighborhood (75-175 units), with the ability to reverse plot plans and vary placement or add elements to corner units;
- Provide a minimum of 2 individual unit plans per building. Individual unit plans may be repeated between building plans;
- Provide one or more styles per community as designated on the "Neighborhood Character" exhibit in this document; and
- Provide at least 3 different, yet compatible color schemes for each neighborhood.



Triplex

Townhomes

Exhibit 7-1: Plotting and Massing



Parkside

Materials and Colors

Individual elevation or building colors should be selected from the community color palettes selected for that style. See individual style matrices for style-appropriate color palettes.

7.5.2 Architectural Plotting & Massing Concepts

Form, Mass & Height

In the case of attached and multi-family buildings there is a tendency to “build out” to the maximum building envelope without articulated treatment on wall planes. This is to be avoided **as the intent is** to prevent the final product from becoming a multi-story stucco box, without vertical or horizontal relief.

- Minimize building “ends” and blank, singular planes oriented toward public views. Provide some architectural elements on all sides of buildings;
- Consider intended styles in conjunction with the development of building plans, massing forms, elements, details, and color;
- Carefully consider the building massing, details, and color, in conjunction with the architectural character for the neighborhood or community;
- Design buildings to define outdoor spaces, with floor plans that have logical and functional relationships between indoor and outdoor spaces; and
- Buildings shall not exceed height standards. (Refer to Section 7.6 for height restrictions).

Shade and Shadow - Building Relief

- Project a front door image and access to the street front. Articulation of end unit elevations is required to achieve four-sided articulation;
- Place and access garages at rear alleys to separate pedestrian from vehicular areas;
- Provide front porches where style-appropriate and possible for stepped massing and transition to public spaces;
- Vary setbacks on building elements/facades; and
- Vary roof pitches and directions.

Spaces—Single-Story Elements

Single story elements are encouraged on all buildings to establish pedestrian scale and add variety to the street scene. This is especially important on larger buildings where inadequate articulation of mass tends to produce monotonous, single planes.

On attached buildings, there are multiple opportunities for single-story elements such as interior living spaces, porches and entries. The addition of such elements helps to provide a transitional space between public and private space.

Entries

Entries should create an initial impression, locate and frame the doorway, act as an interface between public and private spaces, and further identify individual unit entries.



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- Wherever possible, site plans should orient the front door image and principal access toward the public street or entry courtyard;
- Incorporate appropriate roof elements, columns, feature windows and/or architectural forms in the entry statement to emphasize the building character and the location of individual doorways; and
- If front entry location is not immediately obvious due to building configuration, direct and draw the observer to it with added elements such as signs, lighting and landscape.



Windows

Typically the location of windows is determined by the practical considerations of room layout, furniture placement, views, and privacy. Design emphasis here should be of particular concern as windows play an important role in the exterior architectural character of multi-family buildings.

- Within the appropriate style requirements, group and coordinate windows with other design elements to create a sense of composition and order;
- Where appropriate to style and window form, the use of multiple window panes is encouraged; and
- Use appropriate scale and proportion in window design to enhance the elevation style, using shutters, trim, etc. to help convey character.



Balconies

The inclusion of balconies is encouraged for both aesthetic and practical purposes. They are useful in breaking up large wall planes, offsetting floors, creating visual interest, and adding human scale to the building. They provide the practical advantage of creating outdoor living areas and elevated open space.

- Balconies may be covered or open. They may be either recessed into the mass of the building or serve as a projecting element. Balconies and patios must adhere to required setbacks;



Townhome Product Lane Streetscape



Parkside

- Design balconies as an integral elements of the building with details, eaves, supports, and railings in keeping with the architectural style and other elements of the building’s design; and
- Avoid designing plans with repetitive balconies that occur side by side.

Parking

In the case of attached buildings adjacent to primary streets, the focus of these buildings should be their street front image and pedestrian access. Each project will incorporate interior oriented parking solutions and use the following design techniques to enhance the architecture of the streetscene:

- Distribute parking on-site to provide close proximity as possible to individual units;
- Group unassigned or guest parking in convenient locations;
- Refer to Division 6.03 (Off Street Parking and Loading) of the Ontario Development Code for [parking requirements](#). [Any parking standards that differ from the Ontario Development Code can be found in the development standards \(refer to the tables in Section 7.6: Home Types\)](#).
- [A parking analysis must be submitted to the City as part of the Development Permit process:](#)

Community Recreation and Common Facilities

Each neighborhood planning area is focused around (or is within a short walk from) the Great Park or common recreation facility. These may contain pools, spas, recreation [buildings structures](#), barbecues and/ or open space. Each is planned as appropriate to the demographic profiles identified for the community by the master developer.

- Recreation facilities are key character elements for the community;
- All architectural and community elements, such as street furnishings, benches, lighting standards, and trash receptacles, shall be consistent with the selected overall architectural character for the community; and
- Colors, massing, roof pitch, and materials shall be compatible with residential buildings or project theme.

Trash Enclosures

Enclosures shall be provided to accommodate the numbers and types of trash containers as required by the disposal company. These enclosures shall be positioned in a centrally convenient area for residents. Try to minimize impact on adjacent residences and neighborhood developments by keeping enclosures



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from the edges of the community.

- Trash enclosures shall be substantially constructed in a style and decorative wall finish that is consistent with the overall architectural character of the development.
- All trash enclosures shall be equipped with style complementary gates of durable construction, hinged to self-supporting steel posts.
- Lighted pedestrian access is required at each enclosure.
- Trash enclosures shall be placed in a location which does not interfere with on-site circulation.

As consistent with Section 5.7 of this Specific Plan, all project sites shall be designed to meet all the Integrated Waste Department's requirements, including the requirements for Sizing of Storage, Location of Collection Area, Accessibility for Collection Vehicles, and Collection of Sorted/Diverted Waste Types.

Garage Placement & Treatment/Lanes

Provide rear or side-accessed garages for lane-loaded, attached buildings. Tandem garages may be used to accommodate parking requirements. Vary plans where possible, placing garages at different locations relative to the lane. Roll-up garage doors with garage door openers are required on all homes.

Lanes (Private Alleys)

All lanes (private alleys) shall be designed to include landscaping to soften architectural massing. Architectural treatments such as varied massing, color variations, decorative paving, trellises, and window treatments

shall be designed to add interest to building facades.

~~Additionally, drainage swales shall not be constructed within the center of the lanes. The swales shall be constructed along the perimeter of the lanes.~~

7.6 Home Types

7.6.1 Introduction

A primary design objective for the Specific Plan is to provide a variety of home sizes and plans to suit the needs of different life stages and market segments. Parkside allows for individual home-ownership opportunities in a higher density more urban setting than typical neighborhoods. Some of the three-story detached homes may be located on fee simple lots or they may in a one-lot condominium subdivision. All homes will have fire separation distances as required by the California Residential Code that may be as minimal as a few inches.

The following section provides conceptual plotting examples for a variety of residential housing types, arrangements and lot size ranges. The plotting concepts are provided only to illustrate a typical layout of each building housing type. They are not intended to be mandated layouts.

7.6.2 Home Type Criteria

The following are design standards and minimum setbacks by typical residential product and housing type.



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Conventionally Loaded Lane Homes

(PA 1-4)



Characteristics

- Single-family small lot detached dwellings face street or lanes (private alleys);
- Automobile access via streets or lanes (private alleys);
- Primary entries and walks face streets or lanes (private alleys);
- Private yards/patios;
- Reciprocal use easements may extend private spaces on sides of buildings;
- Parking for residents provided in garages and driveways; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

Notes

- Actual acreages, lot sizes, unit counts and densities **by typical neighborhood building type** may vary based on final plans.
- Refer to other areas of Section 7—Residential Design Guidelines, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
- Refer to other areas of Section 7—Residential Design Guidelines, for specific criteria regarding all landscape design and implementation.
- Typical Product Plotting Concepts are provided for representative purposes only and are not intended to be mandated layouts.



Parkside

Conventionally Loaded Lane Homes - Product 2

(PA 1 & 4)

Note: Auto Court and lane-loaded Private Lanes shall be enhanced with a ribbon of either colored concrete, stamped concrete, pavers or other similar decorative materials.



Typical Product Plotting Concept
2-Story Diagram

Table 7-1: Conventionally Loaded Lane Homes Development Standards

Building Separation

Front to side living space	8'
Side to side living space	8'
Side to side living space (with parking space in between)	11'
Rear living to rear living	8'

Front Setbacks (Private Street Facing) - To Back of Sidewalk

Living space	5'
Porch, balcony	3'
Garden wall	3'
Front setbacks (Private Lane facing)	3'

Side Setbacks - To Parcel Edge/Site Boundary

Living space to property line	10'
Non-living space (garage-single story)	6'
Living space (adjacent to Cucumonga Creek Channel)	4'

Rear Setbacks - To Parcel Edge/Site Boundary

Living space (2-story)	10'
Living space (3-story)	10'

Garage Setbacks

Garage face to garage face	30'
Garage face to lane (drive apron)	3'
Drive aprons shall be 5' or less in length or 18' in length or greater.	

Maximum Encroachments into Building Setbacks

Second/third floor cantilever over garage	3'
Fireplace, media niche (8' max. length), bay windows, furr outs, etc.	2'
Potshelves, brackets, eaves, awnings, eyebrows, canopies, cantilever decks, etc.	Additional 36" over porch or furring setback

Private Open Space Per Unit

Minimum dimension	4'
Total per unit	70 s.f.

Parking

2 spaces per homes, at least 1 is required to be covered

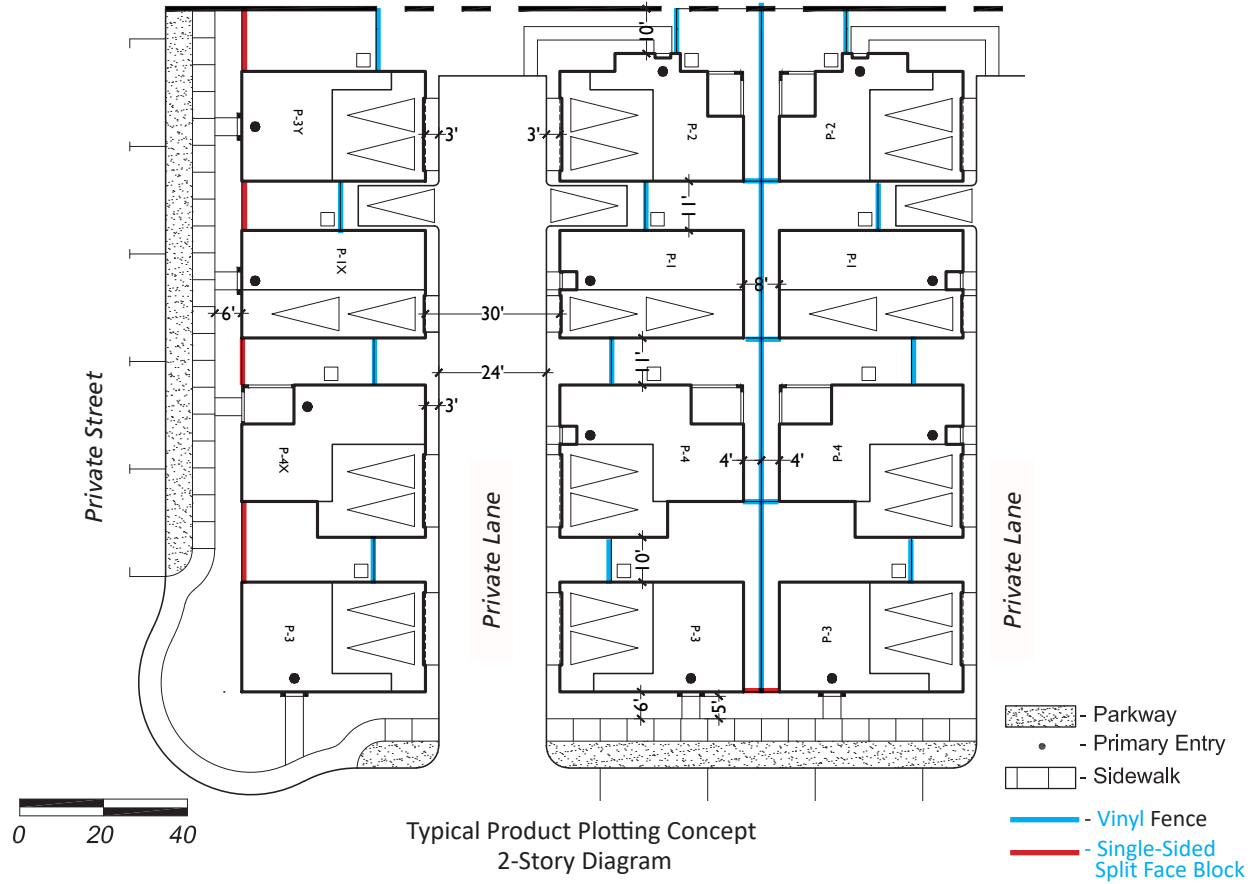
Building Height

Architectural projections may exceed the maximum building height of 42' by an additional 3'.



Parkside

Conventionally-Loaded Lane Homes (up to 12-Plex)- Product 3 (PA 2, 3 & 4)



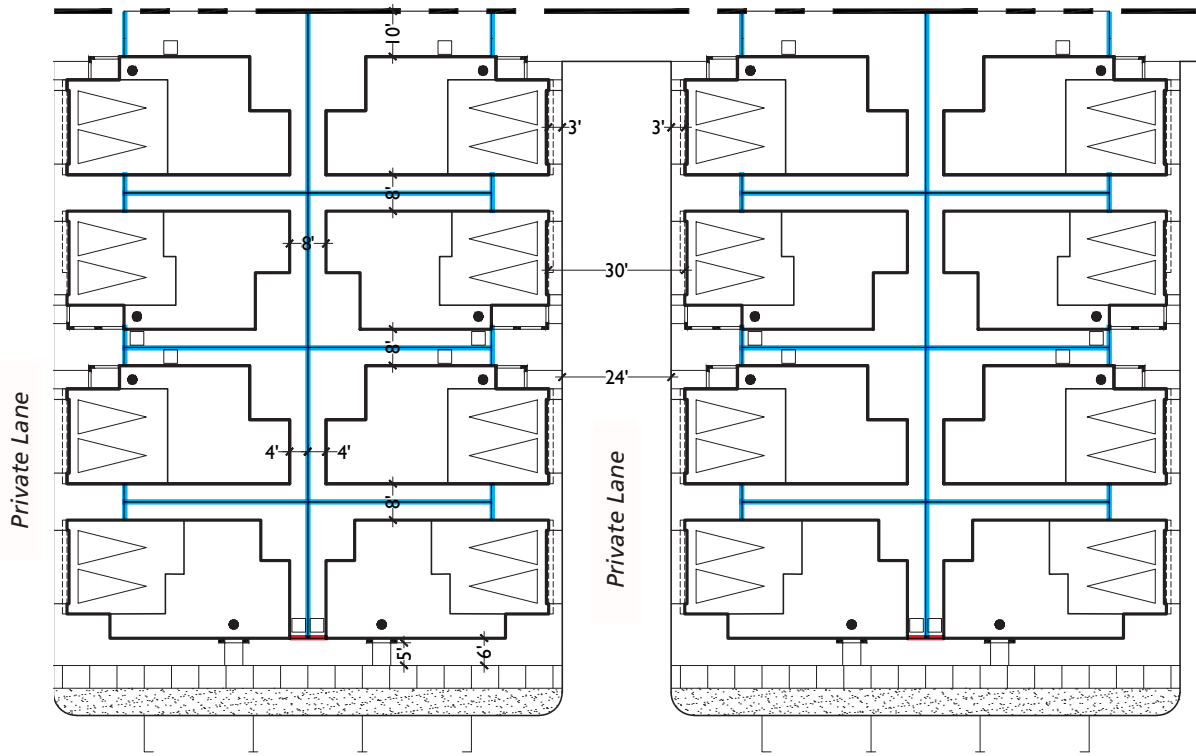
Note: Auto Court and lane-loaded Private Lanes shall be enhanced with a ribbon of either colored concrete, stamped concrete, pavers or other similar decorative materials.



Parkside

Conventionally-Loaded Lane Homes- Product 4

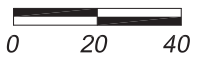
(PA 1 & 4)



Private Street

Private Lane

Private Lane



Typical Product Plotting Concept
2-Story Diagram

- Parkway
- Primary Entry
- Sidewalk
- Vinyl Fence
- Single-Sided Split Face Block

Note: Auto Court and lane-loaded Private Lanes shall be enhanced with a ribbon of either colored concrete, stamped concrete, pavers or other similar decorative materials.



Parkside

Detached Green Court Cluster (Up to 8 - 10-Plex)

(PA 1 & 4)



Characteristics

- Single-family detached dwellings clustered along paseo/court;
- Automobile access via lanes (private alleys);
- Primary entries and walks face paseo/court or street;
- Private side yards/patios;
- Parking for residents provided in garages or in uncovered resident parking spaces;
- Guest parking provided on local streets, designated parking areas, or as approved by the City; and
- Reciprocal use easements extend private space.

Notes

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Parkside

Detached Green Court Cluster (Up to 8 -10-Plex)- Product 1 (PA 1 & 4)

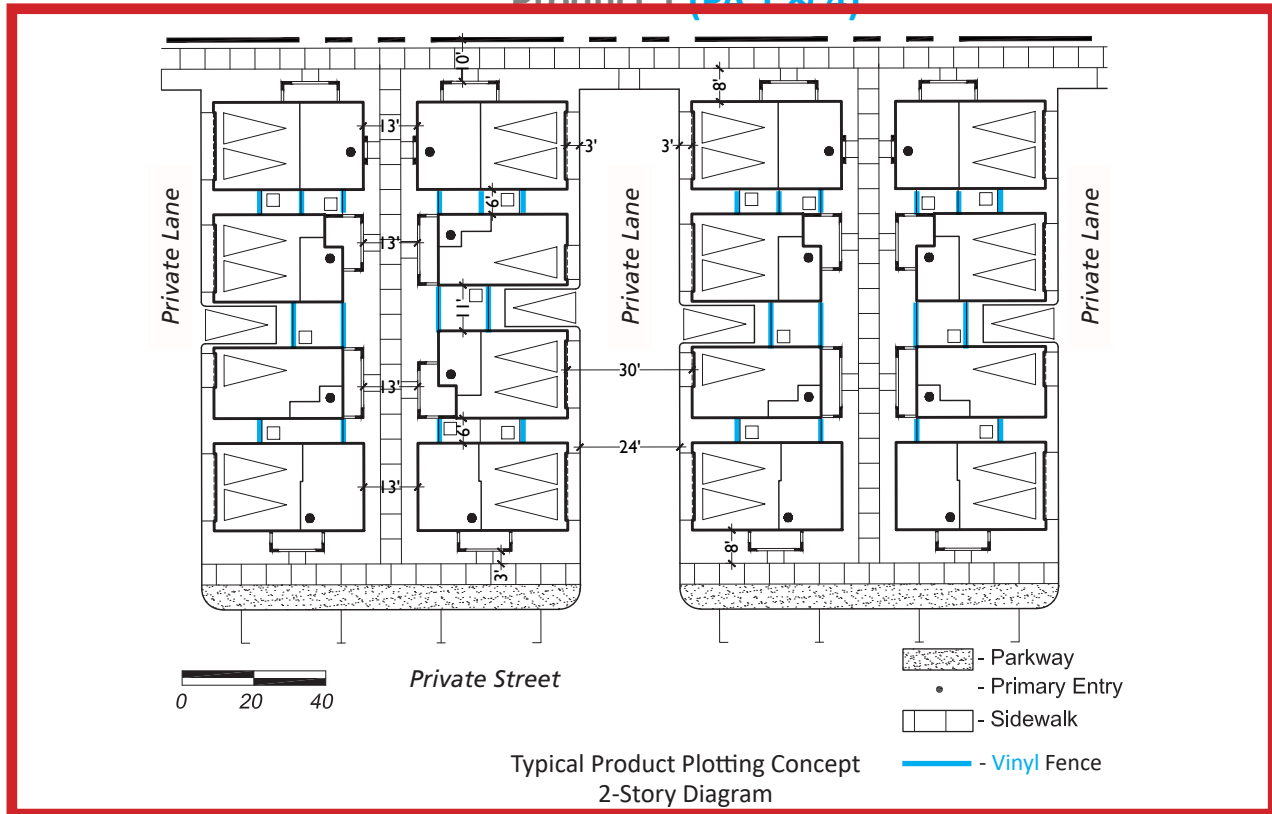


Table 7-2: Detached Green Court Cluster Development Standards

Building Separation

Front to front living space	13'
Side to side living space	6'

Front (Private Street-Facing) Setbacks - To Back of Sidewalk

Living space	7'
Porch, balcony	3'
Garden wall	3'

Side Setbacks - To Lot Edge

Living space to property line	10'
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Rear Setbacks - To Lot Edge

Living space	10'
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Garage Setbacks

Garage face to garage face	30'
Garage face to driveway (apron)	3'

Drive aprons shall be 5' or less in length or 18' in length or greater.

Maximum Encroachments into Building Setbacks

Second/third floor cantilever over garage	3'
Fireplace, media niche (8' max. length), bay windows furr outs, etc.	2'
Potshelves, brackets, eaves, awnings, eyebrows, canopies, cantilever decks, etc.	Additional 36" over porch or furring setback

Private Open Space Per Unit

Minimum dimension	5'
Total per unit	70 s.f.

Parking

2 spaces per unit, at least 1 space needs to be covered.

Building Height

Architectural projections may exceed the maximum building height of 42' by an additional 3'.

Note: Green Court pedestrian entries may have decorative arbor (or other similar decorative design feature to enhance pedestrian scale and street scene.



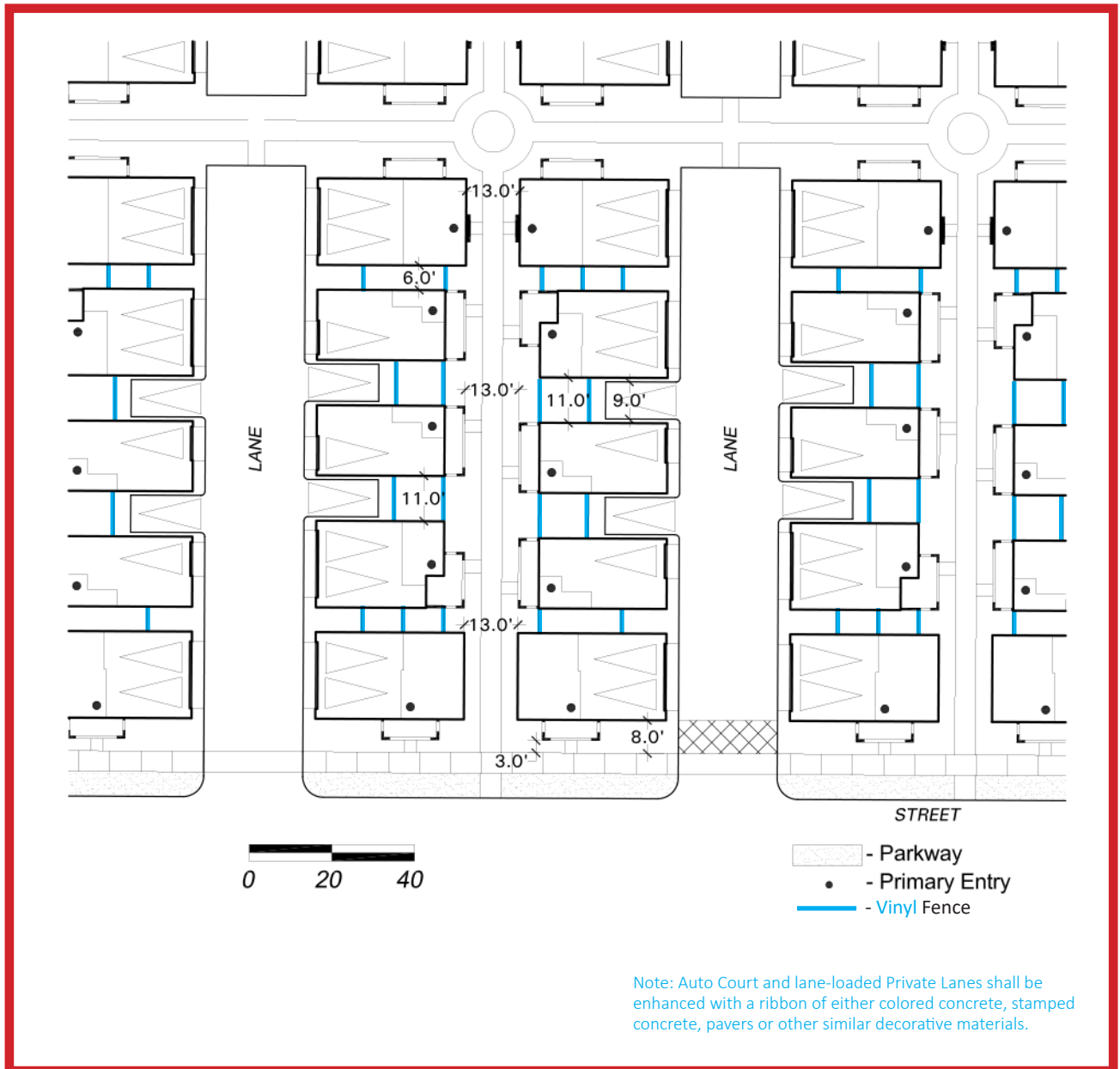
Parkside

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Parkside

Detached Green Court Cluster (Up to 8 -10-Plex)- Product 1 (PA 1 & 4)





Parkside

Conventionally Loaded Lane Homes

(PA 1, 7 & 8)*



Characteristics

- [Conventionally loaded lane homes](#) are dwellings face street or lanes (private alleys);
- Automobile access via streets or lanes (private alleys);
- Primary entries and walks face streets or lanes (private alleys);
- Private yards/patios;
- Reciprocal use easements may extend private spaces on sides of buildings;
- Parking for residents provided in garages and driveways; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

Notes

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Parkside

Conventionally Loaded Lane Home

(PA 1, 7 & 8)*

* Proposed product types in each Planning Area are subject to change based on final development submittal.

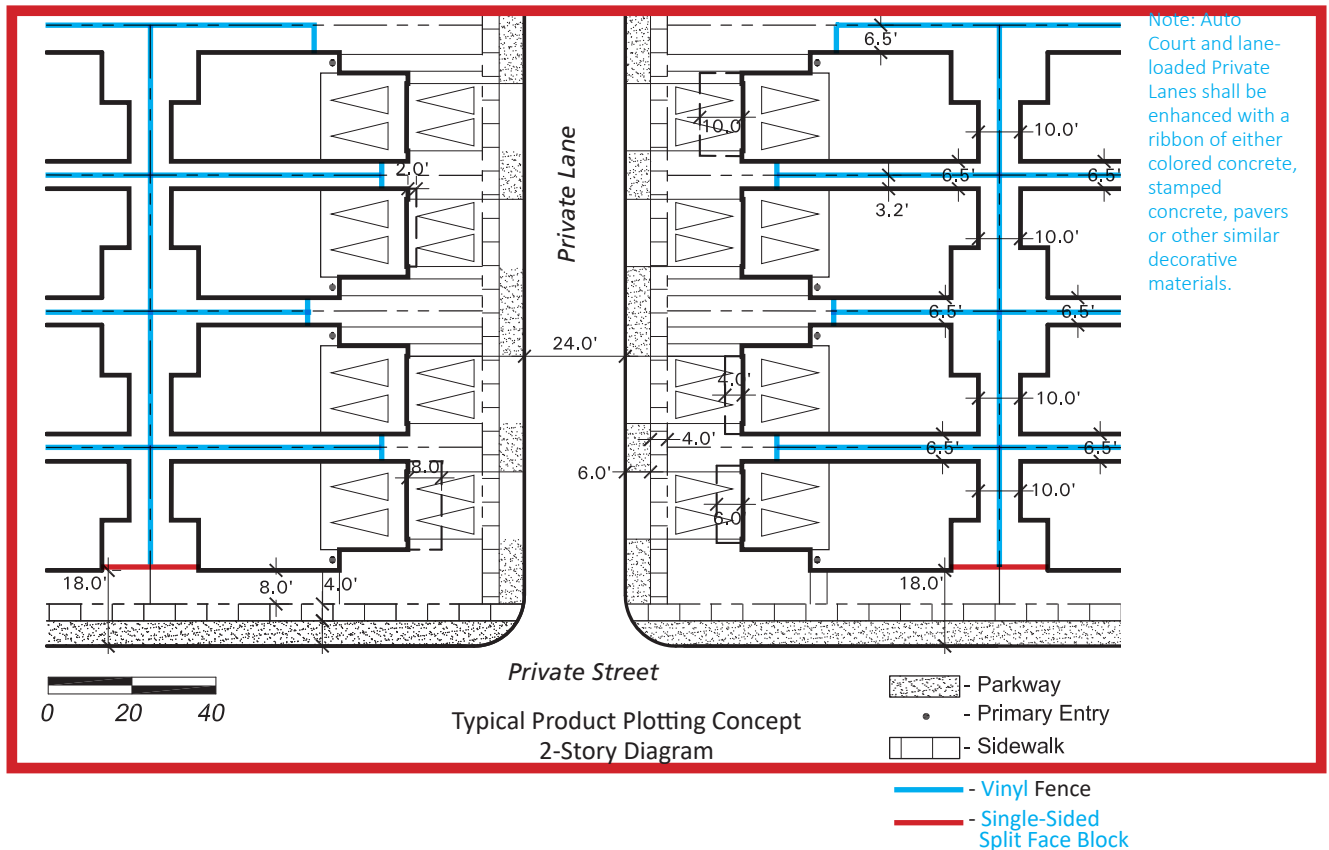


Table 7-3: Conventionally Loaded Lane Home Development Standards

Building Separation		Maximum Encroachments into Building Setbacks	
Front to side living space	N/A	Second floor cantilever over garage	10'
Side to side living space	6.5'	Private Open Space Per Unit	
Rear living to rear living	10'	Minimum dimension	10'
Front Setbacks (Private Street Facing) - To Back of Sidewalk		Total per unit	100 s.f.
Living space	8'	Parking	
Porch, balcony	5'	Per Ontario Development Code	
Garden wall	3'	Building Height	
Garage Setbacks		Architectural projections may exceed the maximum building height of 35' by an additional 5'.	
Garage face to lane (drive apron)	18'		
Drive aprons shall be 5' or less in length or 18' in length or greater. .			



Parkside

Conventionally Loaded Lane Home

(PA 1, 7 & 8)*

* Proposed product types in each Planning Area are subject to change based on final development submittal.



Characteristics

- **Conventionally Loaded Lane Homes** are detached dwellings face street or lanes (private alleys);
- Automobile access via streets or lanes (private alleys);
- Primary entries and walks face streets or lanes (private alleys);
- Private yards/patios;
- Reciprocal use easements may extend private spaces on sides of buildings;
- Parking for residents provided in garages and driveways; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

Notes

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
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Parkside

Conventionally Loaded Lane Home

(PA 1, 7 & 8)*

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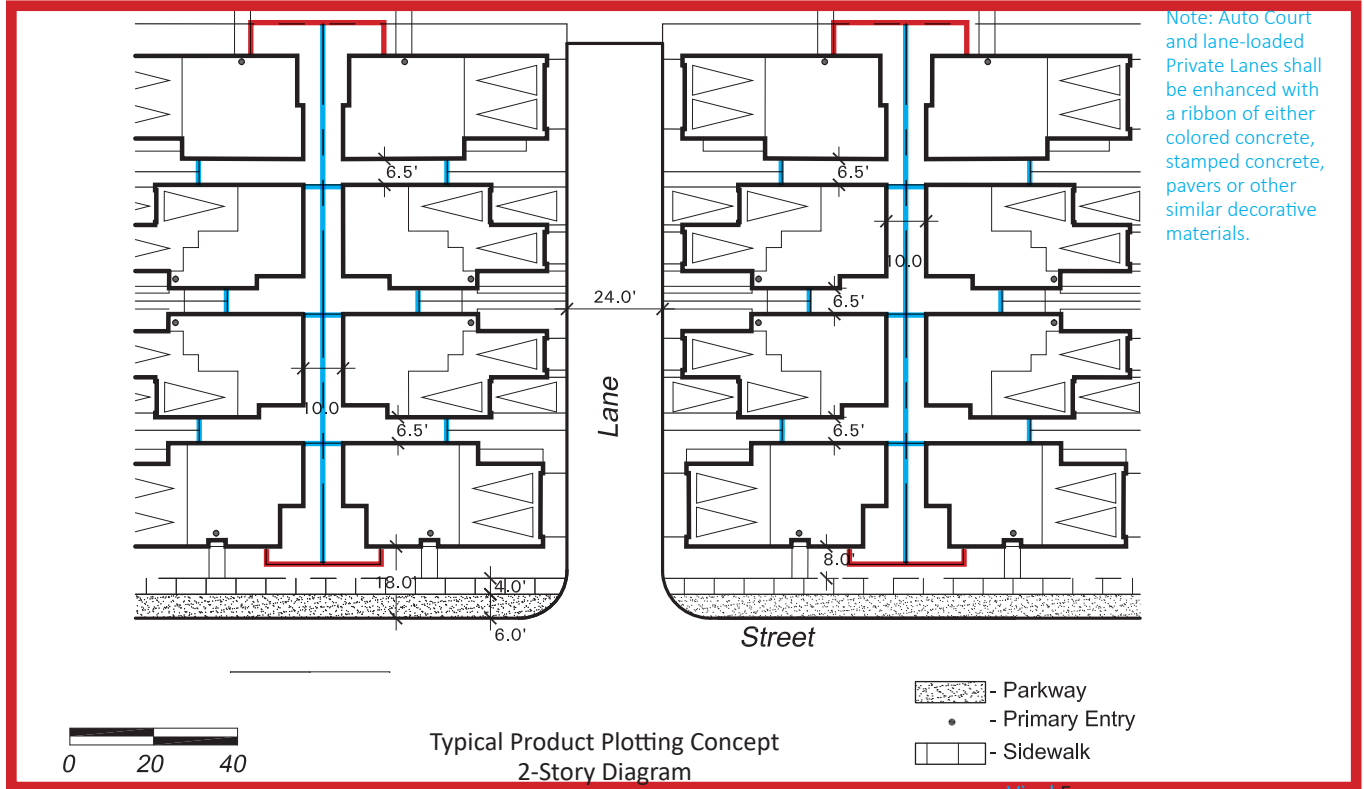


Table 7-4: Conventionally Loaded Lane Home Development Standards

Building Separation

Front to side living space	6.5'
Side to side living space	6.5'
Rear living to rear living	10'

Front Setbacks (Private Street Facing) - To Back of Sidewalk

Living space	8'
Porch, balcony	5'
Garden wall	3'

Garage Setbacks

Garage face to lane (drive apron)	8'
Drive aprons shall be 5' or less in length or 18' in length or greater.	

Maximum Encroachments into Building Setbacks

Second floor cantilever over garage	2'
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Private Open Space Per Unit

Minimum dimension	10'
Total per unit	100 s.f.

Parking

Per Ontario Development Code

Building Height

Architectural projections may exceed the maximum building height of 35' by an additional 5'



Parkside

Detached Green Court Paseo Cluster (6 or 8-Plex)

(PA 1, 7 & 8)*



Characteristics

- Single-family detached dwellings clustered along paseo/court;
- Automobile access via lanes (private alleys);
- Primary entries and walks face paseo/court or street;
- Private side yards/patios;
- Reciprocal use easements extend private space;
- Parking for residents provided in garages; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

Notes

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Parkside

Detached Green Court Paseo Cluster (6 or 8-Plex)

(PA 1, 7 & 8)*

* Proposed product types in each Planning Area are subject to change based on final development submittal.

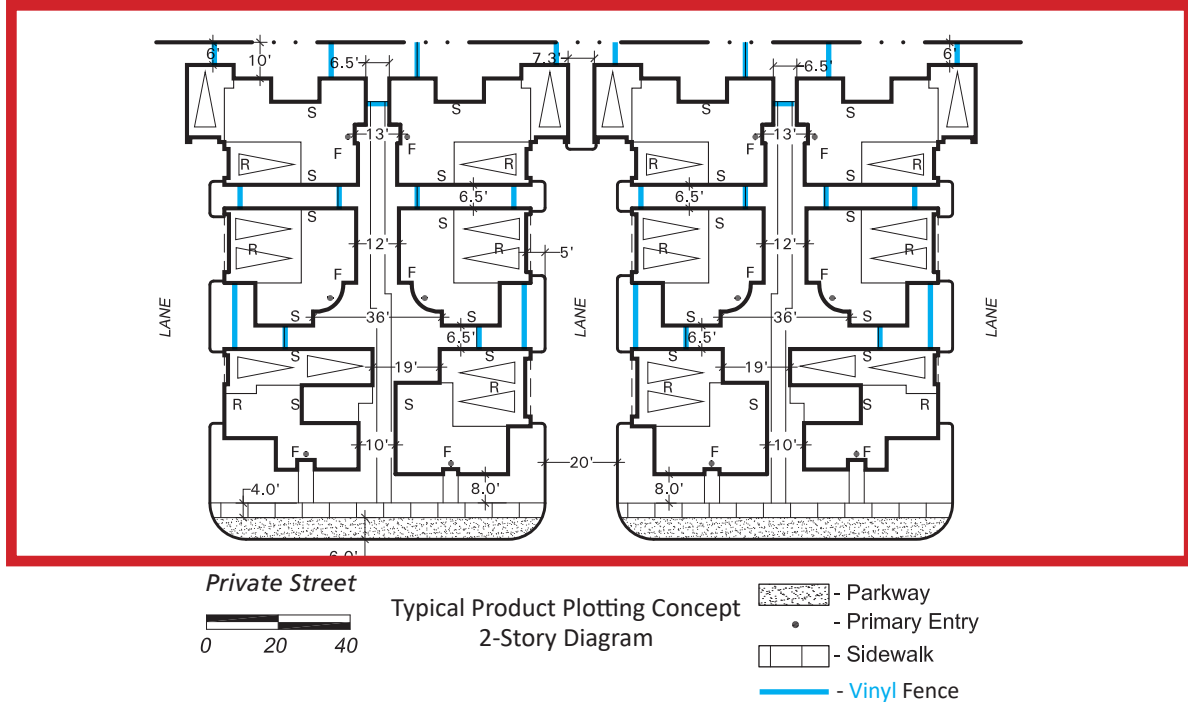


Table 7-5: Detached Green Court Paseo Cluster (6- or 8-Plex) Development Standards

Building Separation

Front to front living space-paseo/court	6.5' to 13'
Side to side living space	6.5'
Opposing entries—door to door	13'

Side Setbacks - To Back of Sidewalk - Private Street Facing

Living space	8'
Porch	5'
Garden wall	3'

Side Setbacks - To Lot Edge

Living space	10'
Non-living space (garage-single story)	6'

Garage Setbacks

Garage face to garage face	30'
Garage face to lane (drive apron)	3'-5'
Living space (to lane edge)	10'

Drive aprons shall be 5' or less in length or 18' in length or greater.

Note: Lane width will increase to 24' when the length exceeds 150'

Maximum Encroachments into Building Setbacks

Fireplace, media niche (8' max length), bay windows (cantilevered) potshelves, brackets, etc.	1.5'*
Second floor cantilever over garage	2'

Private Open Space Per Unit

Minimum dimension	7'
Total per unit	70 s.f.

Parking

Per Ontario Development Code

Building Height

Architectural projections may exceed the maximum building height of 35' by an additional 5'.

* 3' min. from PL

Note: Green Court pedestrian entries may have decorative arbor (or other similar decorative design feature to enhance pedestrian scale and street scene.



Parkside

Detached Green Court Cluster (8 to 10 Plex)

(PA 1, 7 & 8)*



Characteristics

- Single-family detached dwellings clustered around a Auto Court;
- Automobile access via common Auto Court driveway;
- Primary entries and walks face Auto Court or street;
- Private side and rear yards;
- Parking for residents provided in garages;
- Guest parking provided on local streets, designated parking areas, or as approved by the City; and
- Reciprocal use easements extend private space.

Notes

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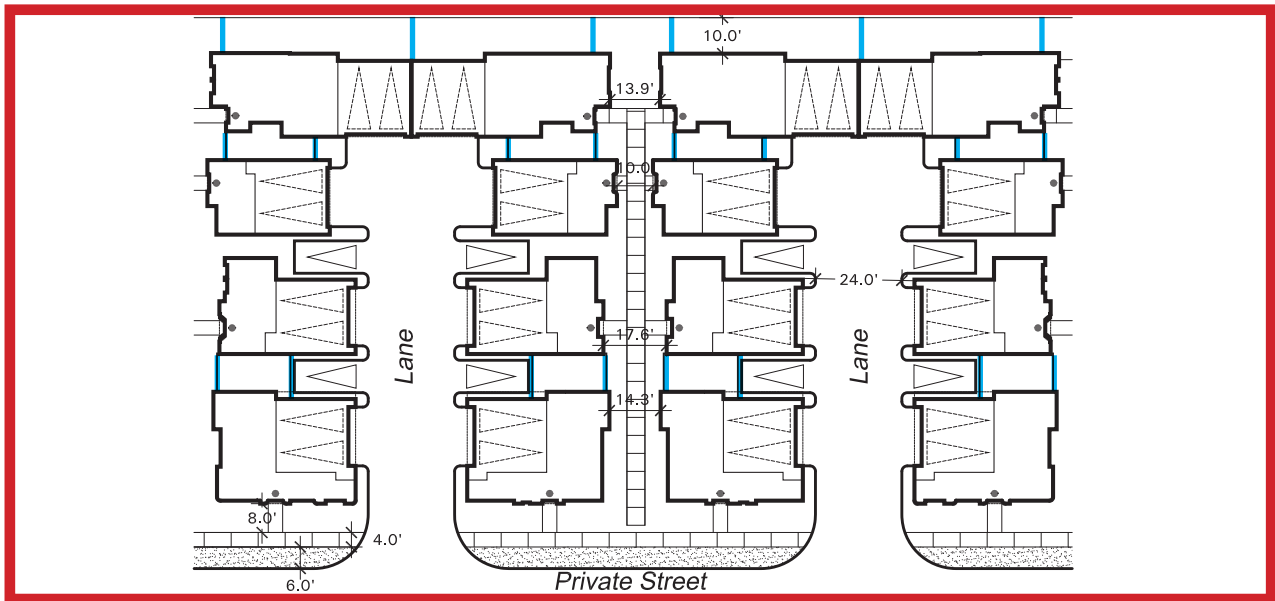


Parkside

Detached Green Court Cluster (8 to 10-Plex)

(PA 1, 7 & 8)*

* Proposed product types in each Planning Area are subject to change based on final development submittal.



- Parkway
- Primary Entry
- Sidewalk
- Vinyl Fence

Typical Product Plotting Concept
2-3 Story Diagram

Table 7-6: Detached Green Court Cluster Development Standards

Building Separation

Front to front living space	10'
Side to side living space	8' or 2" airspace

Maximum Encroachments into Building Setbacks

Fireplace, media niche (8' max length), bay windows (cantilevered), potshelves, brackets, etc.	2'*
Second floor cantilever over garage	2'

Front (Private Street-Facing) Setbacks - To Back of Sidewalk

Living space	8'
Porch, balcony	5'
Garden wall	3'

Private Open Space Per Unit

Minimum dimension	7'
Total per unit	70 s.f.

Side Setbacks - To Lot Edge

Living space to property line	4'
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Parking

Per Ontario Development Code

Rear Setbacks - To Lot Edge

Living space	10'
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Building Height

Architectural projections may exceed the maximum building height of 42' by an additional 3'.

Garage Setbacks

Garage face to garage face	30'
Garage face to driveway (apron)	3'-5'
Drive aprons shall be 5' or less in length or 18' in length or greater.	
Note: Lane width will increase to 24' when the length exceeds 150'	

* 3' min. from PL

Note: Green Court pedestrian entries may have decorative arbor (or other similar decorative design feature to enhance pedestrian scale and street scene.



Parkside

Detached Auto Court Cluster (6 to 8-Plex)

(PA 1. 7 & 8)*



Characteristics

- Single-family detached dwellings clustered around Auto Court;
- Automobile access via common Auto Court driveway;
- Primary entries and walks face Auto Court or street
- Private side and rear yards;
- Parking for residents provided in garages; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.
- Reciprocal use easements extend private space

Notes

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Parkside

Detached Auto Court Cluster (6 to 8-Plex)

(PA 1, 7, & 8)*

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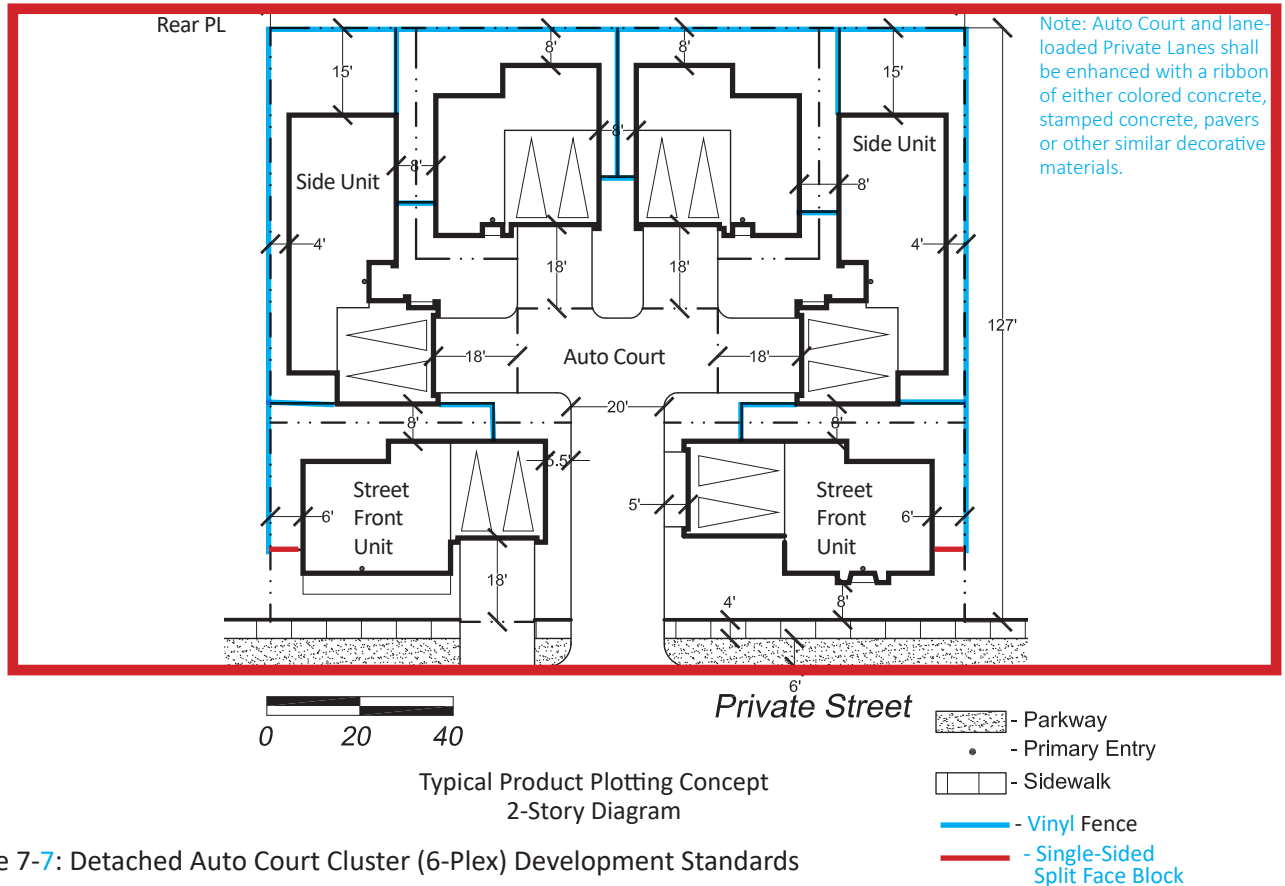


Table 7-7: Detached Auto Court Cluster (6-Plex) Development Standards

Building Separation

Front to side living space	8' (4'+4')
Side to side living space	8' (4'+4')

Front Setbacks (Private Street Facing) - To Back of Sidewalk

Living space	8'
Porch, balcony	5'
Garden wall	3'

Side Setbacks - To Lot Edge

Living space	4'
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Garage Setbacks

Garage facing street	18'
Garage face to garage face	30'
Garage face to lane (drive apron)	5'

Drive aprons shall be 5' or less in length or 18' in length or greater.

Note: Lane width will increase to 24' when the length exceeds 150'

Maximum Encroachments into Building Setbacks

Fireplace, media niche (8' max length), bay windows (cantilevered), potshelves, brackets, etc.	2'*
Second floor cantilever over garage	2'

Private Open Space Per Unit

Minimum dimension	8'
Total per unit	100' s.f.

Parking

Per Ontario Development Code

Building Height

Architectural projections may exceed the maximum building height of 35' by an additional 5'.

*3' min. from PL



Parkside

Detached Auto Court Cluster (6 or 8-Plex)

(PA 1, 7 & 8)*



Characteristics

- Single-family detached dwellings clustered around a Auto Court;
- Automobile access via common Auto Court driveway;
- Primary entries and walks face Auto Court or street;
- Private side and rear yards;
- Parking for residents provided in garages;
- Guest parking provided on local streets, designated parking areas, or as approved by the City; and
- Reciprocal use easements extend private space.

Notes

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Parkside

Detached Auto Court Cluster (6 or 8-Plex)*

(PA 1, 7 & 8)*

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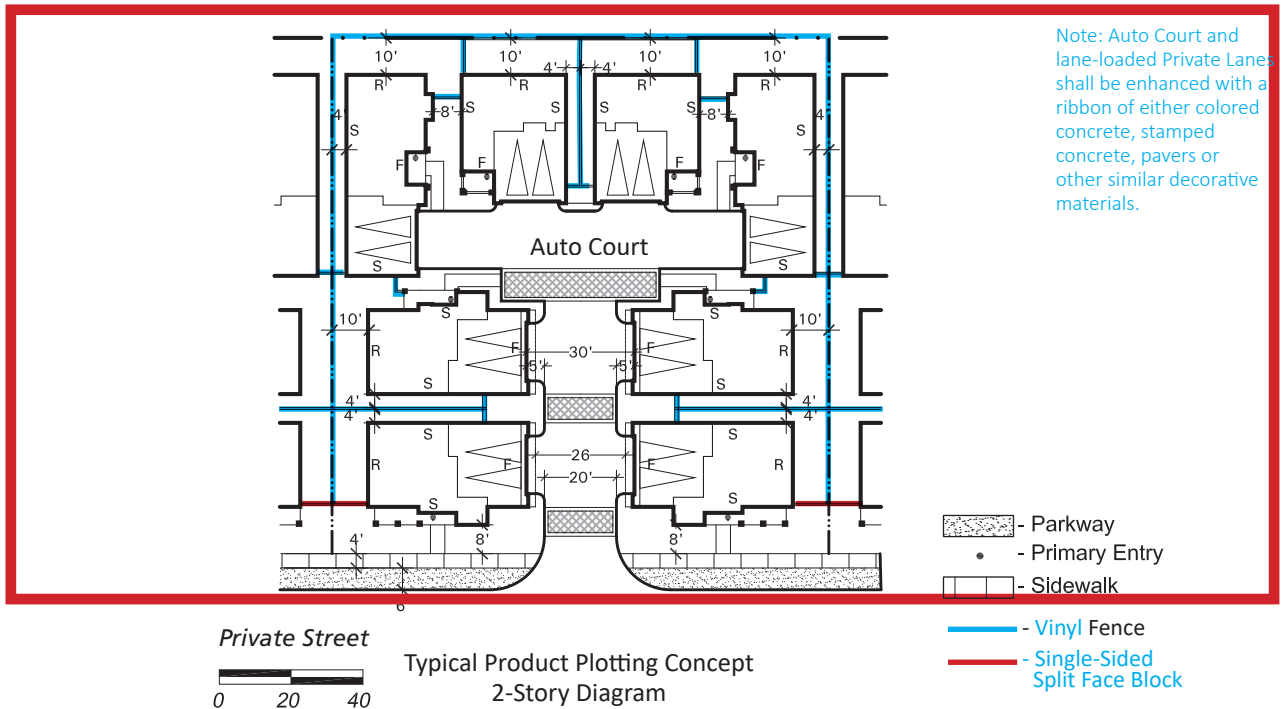


Table 7-8: Detached Auto Court Cluster (6- or 8-Plex) Development Standards

Building Separation

Front to side living space	8'
Side to side living space with entrance	10'
Side to side living space	8' (4'+4')

Maximum Encroachments into Building Setbacks

Fireplace, media niche (8' max length), bay windows (cantilevered), potselves, brackets, etc.	2'*
Second floor cantilever over garage	2'

Side Setbacks - To Back of Sidewalk - Private Street Facing

Living space	8'
Porch, balcony	5'
Garden wall	3'

Private Open Space Per Unit

Minimum dimension	10'
Total per unit	100 s.f.

Rear Setbacks - To Lot Edge

Living space	10'
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Parking

Per Ontario Development Code

Garage Setbacks

Garage face to garage face	30'
Garage face to driveway (apron)	3'-5'

Building Height

Architectural projections may exceed the maximum building height of 35' by an additional 5'.

Drive aprons shall be 5' or less in length or 18' in length or greater.

* 3' min. from PL

Note: Lane width will increase to 24' when the length exceeds 150'



Parkside

Attached Duplex

(PA 14 & 15)*



Not to Scale

Characteristics

- Single-family attached dwelling;
- Oriented toward street or paseos;
- Lane (private alley) loaded automobile access;
- Paired homes enter from front;
- Private rear yards;
- Parking for residents provided in garages; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

Notes

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Parkside

Attached Duplex

(PA 14 & 15)*

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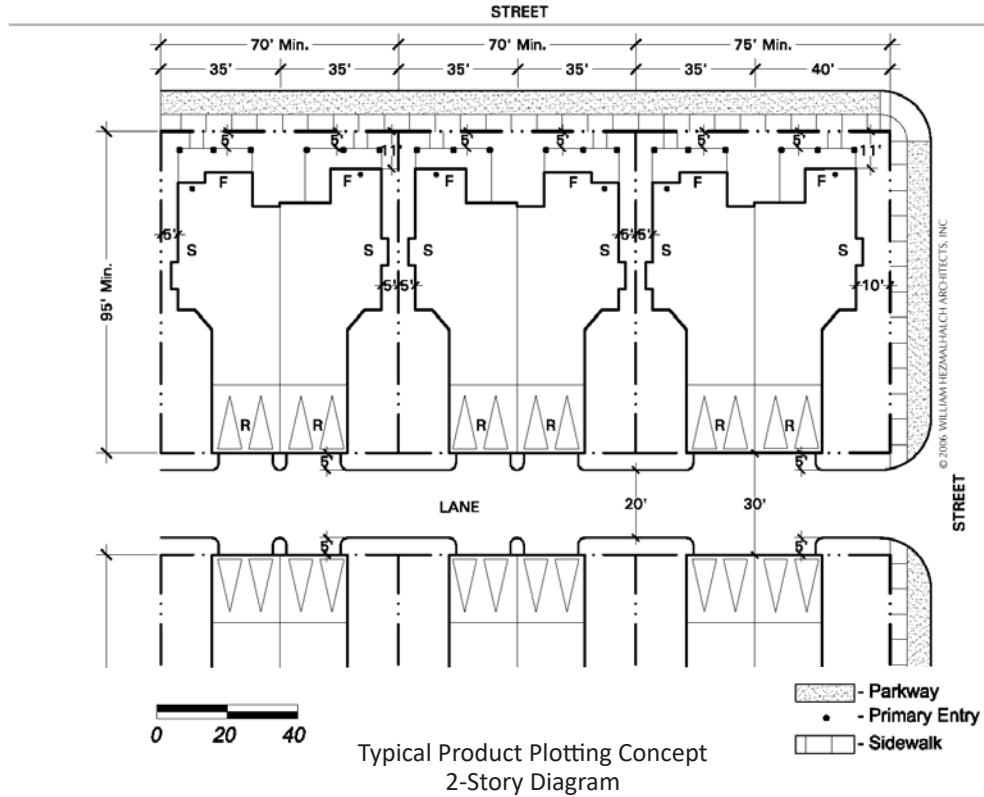


Table 7-9: Attached Duplex Development Standards

Side Setbacks

Side to side living space 10' (5' + 5')

Side Setbacks at Corners - To Back of Sidewalk

Living space 11'
 Living space (2-story) 10'
 Living space (3-story) 15'
 Porch or balcony 5'
 Garden wall 3'

Rear Setbacks

Living space (ground floor) to property line 25'

Note: Maximum lot coverage is 60%.

Garage Setbacks

Garage 5'
 (15' to center line of alley)

Garage face to garage face 30'

Garage face to lane 3'-5'*

Drive aprons shall be 5' or less in length or 18' in length or greater.

Maximum Encroachments into Building Setbacks

Fireplace, media niche (8' max length), bay windows (cantilevered), potshelves, brackets, etc. 2'

Private Open Space Per Unit

Minimum dimension 7'
 Total per unit 150 s.f.

Parking

Per Ontario Development Code

Building Height

Architectural projections may exceed the maximum building height of 35' by an additional 5'.

* Measurement is to be taken from the inside of the curb



Parkside

Attached Triplex

(PA 9, 12 & 16)*



Characteristics

- Attached multi-family dwellings face street or green court;
- Automobile access via lanes (private alleys);
- Primary entries and walks face green court or street;
- Private courtyards/patios and common open space;
- Parking for residents provided in garages; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City;

Notes

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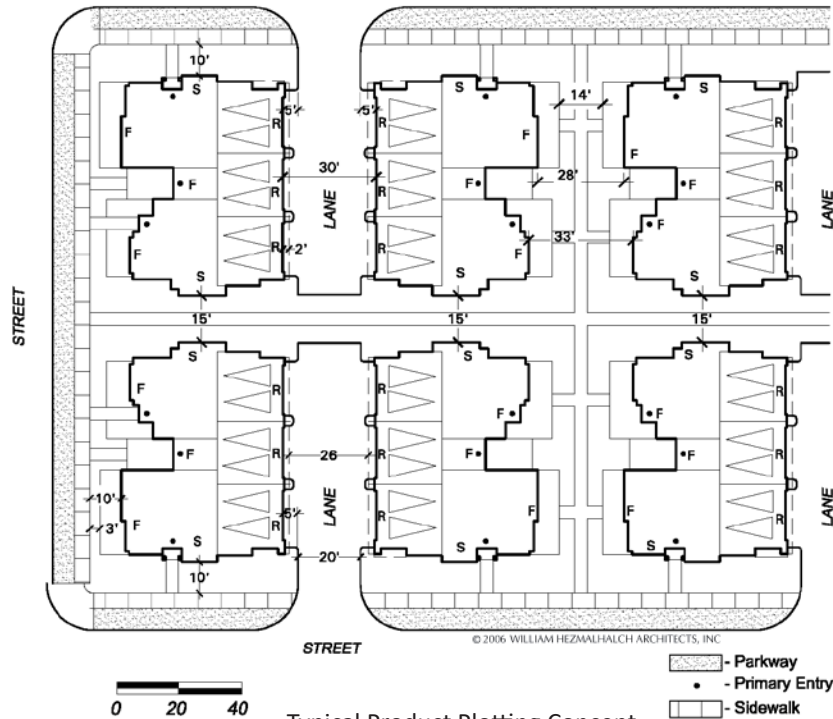


Parkside

Attached Triplex

(PA 9, 12 & 16)*

* Proposed product types in each Planning Area are subject to change based on final development submittal.



Typical Product Plotting Concept
2-Story Diagram

Table 7-10: Triplex Development Standards

Building Separation

Front to front living space	
28' minimum/35' average- up to 20% of building length	30'- central court
Front to side	25'
Porch/balcony to porch/balcony	14'
Porch/balcony to side	15'
Side to side	15'
Side to front	22'

Front Setbacks - To Back of Sidewalk- Private Street Facing

Living space	10'
Living space (2-story)	15'
Living space (3-story)	20'
Porch/balcony	5'
Garden wall	3'

Side Setbacks

Living space to back of sidewalk	10'
Porch/balcony to back of sidewalk	5'
Garden wall to back of sidewalk	3'

Garage Setbacks

Garage face to garage face	30'
Garage face to lane (drive apron)	3'-5'*
Drive aprons shall be 5' or less in length or 18' in length or greater.	
Living space over garage- min. offset or max. cantilever from face of garage	2'
Note: Lane width may be increased to 24' when the lane length exceeds 150'.	

Maximum Encroachments into Building Setbacks

Fireplace, media niche (8' max length), bay windows (cantilevered), potshelves, brackets, etc.	2'
--	----

Private Open Space Per Unit

Minimum dimension	7'
Total per unit	150 s.f.

Parking

Per City of Ontario Development Code

Building Height

Architectural projections may exceed the maximum building height of 35' by an additional 5'.

*Measurement is to be taken from the inside of the curb.



Parkside

Attached Higher Density Townhomes

(PA 5 & 18)*



Characteristics

- Attached multi-family dwellings face street or green court;
- Automobile access via lanes (private alleys);
- Primary entries and walks face green court or street;
- Private courtyards/patios;
- Parking for residents provided in garages; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

Notes

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
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Parkside

Attached Higher Density Townhomes

(PA 5 & 18)*

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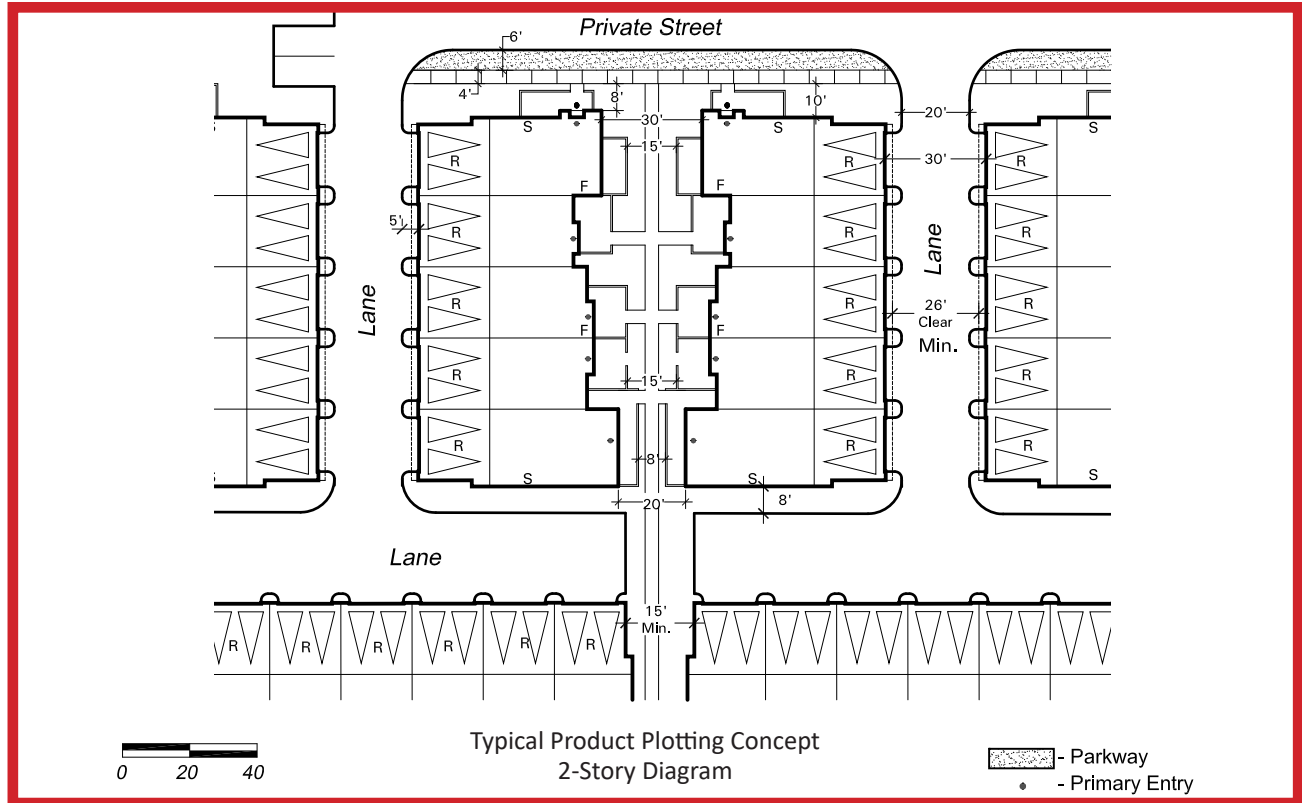


Table 7-11: Attached Higher Density Townhomes Development Standards

Building Separations

Front to front living space	20' minimum/25' average- up to 20% of building length
Front to side living space	25'
Porch/balcony to porch/balcony	14'
Porch/balcony to side	15'
Side to side living space	15'
Opposing entries- door to door	20'

Front Setbacks - To Back of Sidewalk - Private Street Facing

Living space	10'
Living space (2-story)	15'
Living space (3-story)	20'
Porch/balcony	5'
Garden wall	3'

Side Setbacks - To Back of Sidewalk - Street Facing

Living space to back of sidewalk	10'
Porch/balcony to back of sidewalk	5'
Garden wall to back of sidewalk	3'

Garage Setbacks

Garage face to garage face	30'
Garage face to lane (drive apron)	3'-5'*
<i>Drive aprons shall be 5' or less in length or 18' in length or greater.</i>	
Living space over garage- min. offset or max. cantilever from face of garage	2'
Note: Lane width will increase to 24' when the length exceeds 150'	

Maximum Encroachments into Building Setbacks

Fireplace, media niche (8' max length), bay windows (cantilevered), potshelves, brackets, etc.	2'
--	----

Private Open Space Per Unit

Minimum dimension	7'
Total per unit	150 s.f.

Parking

Per Ontario Development Code

Building Height

Architectural projections may exceed the maximum building height of 35' by an additional 5'.

*Measurement is to be taken from the inside of the curb.



Parkside

Attached Row Townhomes

(PA 10 & 17)*



Characteristics

- Attached multi-family dwellings face street or green court;
- Automobile access via lanes (private alleys);
- Primary entries face street or common open space;
- Private patios/courtyards;
- Parking for residents provided in garages; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

Notes

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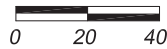
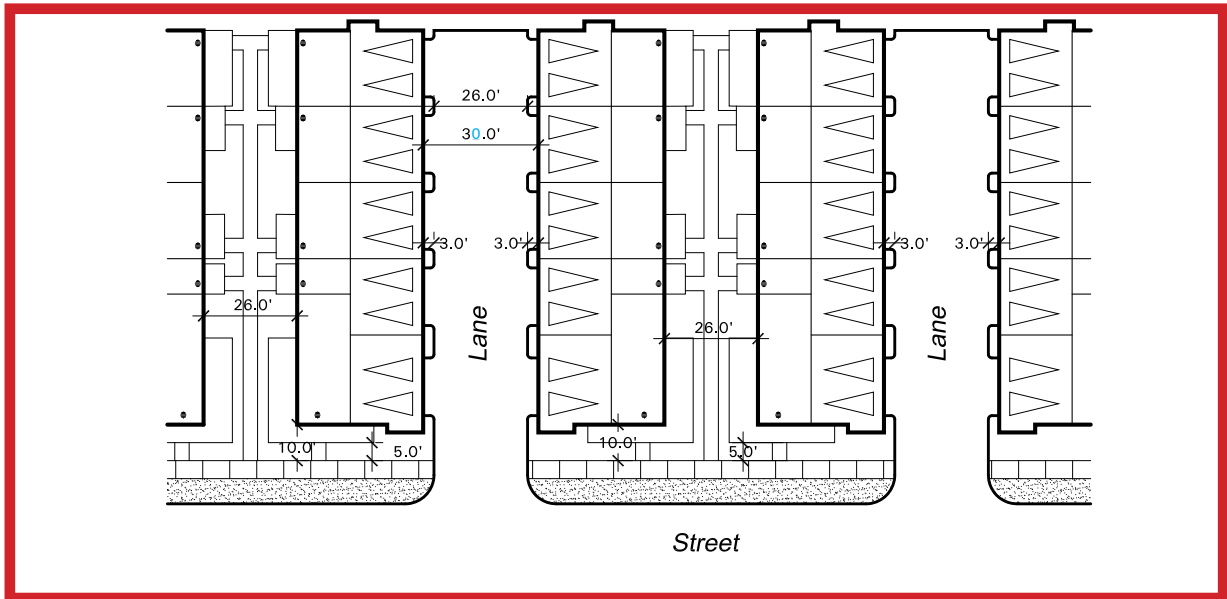


Parkside

Attached Townhomes

(PA 10 & 17)*

* Proposed product types in each Planning Area are subject to change based on final development submittal.



Typical Product Plotting Concept
3-Story Diagram with Tandem and Two-Car Garages

- Parkway
- Primary Entry
- Sidewalk

Table 7-12: Attached Townhomes Development Standards

Building Setbacks

Front to front living space	26' minimum
Side to side	10'

Maximum Encroachments into Building Setbacks

Fireplace, media niche (8' max length), bay windows (cantilevered), potshelves, brackets, etc.	2'*
Second floor cantilever over garage	2'

Front Setbacks (Private Street Facing) - To Back of Sidewalk

Living space	10'
Porch	5'
Garden wall	5'

Private Open Space Per Unit

Minimum dimension	5'
Total per unit	50 s.f.

Garage Setbacks

Garage face to garage face	30'
Garage face to lane (drive apron)	3'-5'
Drive aprons shall be 5' or less in length or 18' in length or greater.	
Note: Lane width will increase to 26' when the length exceeds 150'	

Parking

Per Ontario Development Code

Building Height

Architectural projections may exceed the maximum building height of 42' by an additional 3'.

*3' from PL



Parkside

Attached Auto Court Cluster (10- to 14-Plex)

(PA 6 & 13)*



Characteristics

- Living spaces oriented toward streets or paseos;
- Homes enter from front, side and lane;
- Lane to Auto Court vehicle access;
- Homes have courtyards, porches or balconies;
- Parking for residents provided in 1- or 2-car garages; and
- Guest parking provided on local streets, designated parking areas, or as approved by the City.

Notes:

- Actual acreages, lot sizes, unit counts and densities may vary based on final plans.
- Refer to other areas of Section 7—Residential Design Guidelines, for more specific criteria regarding building design, plotting, styles, materials, details and implementation.
- Refer to other areas of Section 7—Residential Design Guidelines, for specific criteria regarding all landscape design and implementation.
- Typical Product Plotting Concepts are provided for representative purposes only and are not intended to be mandated layouts.

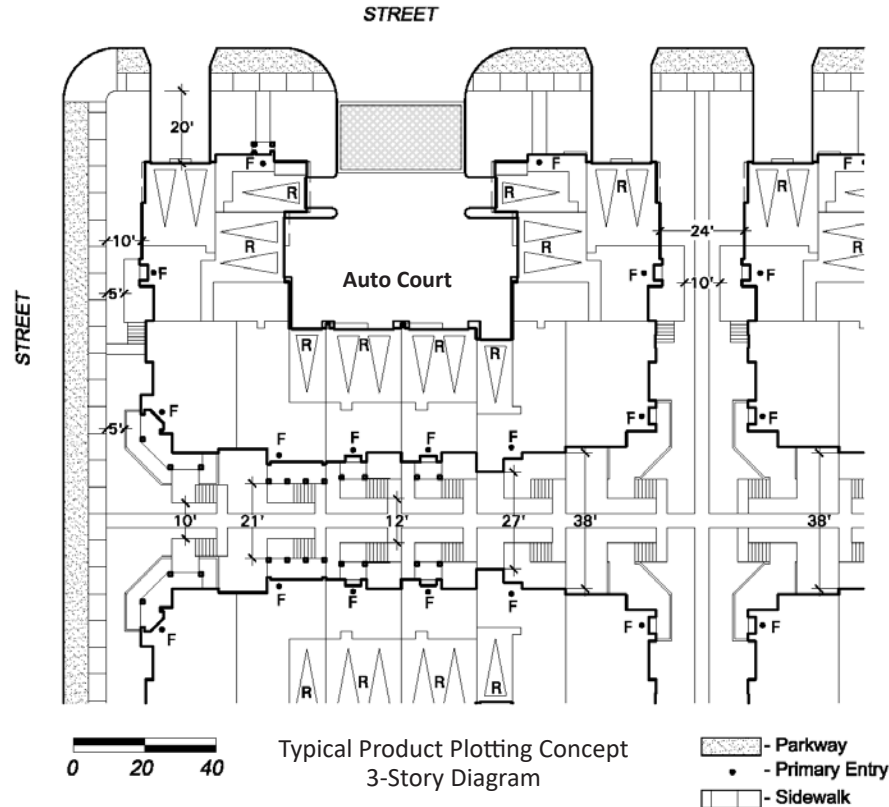


Parkside

Attached Auto Court Cluster (10- to 14-Plex)

(PA 6 & 13)*

* Proposed product types in each Planning Area are subject to change based on final development submittal.



Note: Auto Court and lane-loaded Private Lanes shall be enhanced with a ribbon of either colored concrete, stamped concrete, pavers or other similar decorative materials.

Table 7-13: Attached Auto Court Cluster (10- to 14-Plex) Development Standards

Building Separation

Front to front	32' minimum
Porch/balcony to porch/balcony	21'
Opposing entries- door to door	36'

Front Setbacks - To Back of Sidewalk

Living space	10'
Living space (2-story)	15'
Living space (3-story)	20'
Porch/balcony- street facing	5'
Garden wall	3'

Note: Decorative paving to be provided at the entrance of the motor court area.

Garage Setbacks

Living space over garage- min. offset or max. cantilever from face of garage	2'
--	----

Drive aprons shall be 5' or less in length or 18' in length or greater.

Maximum Encroachments into Building Setbacks

Fireplace, media niche (8' max length), bay windows (cantilevered), potshelves, brackets, etc.	2.5'
--	------

Private Open Space Per Unit

Minimum dimension	7'
Total per unit	150 s.f.

Parking

Per City of Ontario Development Code

Building Height

Architectural projections may exceed the maximum building height of 42' by an additional 3'.

Note: Lane width may be increased to 24' when the lane length exceeds 150'.



Parkside

7.7 Landscape

7.7.1 Introduction

Careful consideration has been given to the design of the community landscape **architectural** character for the Specific Plan area. The following design guidelines are organized to help define the **basic** landscape design principles for the Specific Plan. Observing these guidelines will help to assure the “design vision” and integrity of this planned community.

7.7.2 Overall Streetscapes and Entries

All landscape plans, streetscape plans, and graphic designs with regard to community identity, neighborhood identity, or entry monumentation shall conform to the guidelines as set forth herein, and shall be subject to review and approval by the City of Ontario. Conceptual design for all public and private landscaping, streetscape and entries shall maintain a cohesive design character in massing, form, and texture, and provide compatibility with future designs for the Great Park. Landscaped materials (trees, shrubs, vines, and groundcovers) shall be provided per the **NMC Ontario Ranch** Streetscape Master Plan (**Streetscape Master Plan**).

7.7.3 Perimeter Streetscape Design

Streetscape design guidelines establish a hierarchy for the landscape development along the surrounding roadways, as well as establishing a framework for consistency of design. All streetscape design **including the neighborhood edges** shall be in substantial conformance with the **Streetscape Master Plan**. The design are subject to change **dues to NPDES standards and must be approved by the City of Ontario’s streetscape master plan**. Four major **New Model Colony Master Planned roadways** arterials

surround the Specific Plan area: Archibald Ave. to the east, **Edison Ave Ontario Ranch Rd.** to the north, **Merrill Eucalyptus Ave.** to the south, and Carpenter Ave. to the west. Landscape easements associated with these roadways have **also** been defined **as noted in the City of Ontario New Model Colony General Plan Amendment**.

Landscape development surrounding this community will help to set the character, while maintaining consistency with the **City of Ontario’s City’s** pedestrian pathway system illustrated on the **Multipurpose Trails and Bikeways section of the New Model Colony Bikeway Corridor Plan of Policy Plan (General Plan Amendment)** component of The Ontario Plan.

Bus shelter design, if located on perimeter or interior streetscapes, shall incorporate architectural character/design which is consistent with the neighborhood or streetscape theme.

Archibald Ave.

Archibald Ave. streetscape along the residential areas shall include the following:

- **A landscaped median (26 feet wide) with a single row of street trees per the Streetscape Master Plan;**
- **Landscaping within the parkway (7-foot wide minimum (west side) - 10-foot wide minimum (east side) with a row of street trees per the City of Ontario Street Tree Streetscape Master Plan along both sides of the street;**
- **A shared 5-foot wide sidewalk /bikeway (13 feet wide) separated from the 8-foot wide multi-purpose trail with a 5-foot wide landscape area along the west side of the street;**



Parkside

- A lineal sidewalk (5 feet wide) along the other side of the street;
- A landscaped median (26 feet wide) with a single row of street trees per the City of Ontario Street Tree Master Plan;
- A landscaped neighborhood edge (NE) of 50 feet taken measured from face of curb to perimeter wall on the west side of the street with 25 feet of buffer landscape; and
- Background trees and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant grouping while combining interesting foliage textures and color. Tree species per City of Ontario Street Tree Streetscape Master Plan.
- ~~Monumentation as shown in the Conceptual Landscape Master Plan, Exhibit 20.~~
- Refer to the streetscape cross section Exhibit 7-3: Archibald Ave. Streetscape for illustration of Archibald.

Eucalyptus Ave. Merrill

Eucalyptus Ave. streetscape shall include the following:

- A landscaped parkway strip (7-foot wide minimum) with a single row of street trees per the City of Ontario Street Tree Streetscape Master Plan along the north side of the street;
- ~~A lineal sidewalk~~ (A shared 13-foot wide sidewalk/ multi-purpose trail with an additional 5 feet of landscape along the north side of the street;
- ~~A shared sidewalk/bikeway along one side of the street;~~
- A landscape easement of 35 feet taken measured from face of curb to perimeter wall on the north side of the street;
- Background trees and shrub masses planted in series of layers (foreground, midground, and background) to help define borders and plant groupings while combining interesting foliage textures and color. Tree species per the Streetscape Master Plan;
- Refer to streetscape cross section Exhibit 7-4: Eucalyptus Ave. Streetscape for illustration.

Ontario Ranch Rd.

Ontario Ranch Rd. streetscape along the residential areas shall include the following:

- A landscaped parkway (7-foot wide minimum) with a single row of street trees per the Streetscape Master Plan along both sides of the street;
- A 5-foot sidewalk separated from the 8-foot wide multi-purpose trail with 5 feet of landscape along the south side of the street;
- A Neighborhood Edge of 50 feet measured from face of curb to perimeter wall on the south side of the street with 25 feet of buffer landscape;
- Background trees and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant groupings while combining interesting foliage textures and color. Tree species per the Streetscape Master Plan;
- ~~Monumentation as shown in the Conceptual Landscape Master Plan, Exhibit 20; and~~
- Refer to the streetscape cross section Exhibit 7-5: Ontario Ranch Rd. Section (Merrill Ave.) Exhibit 22 Streetscape for illustration.















Parkside



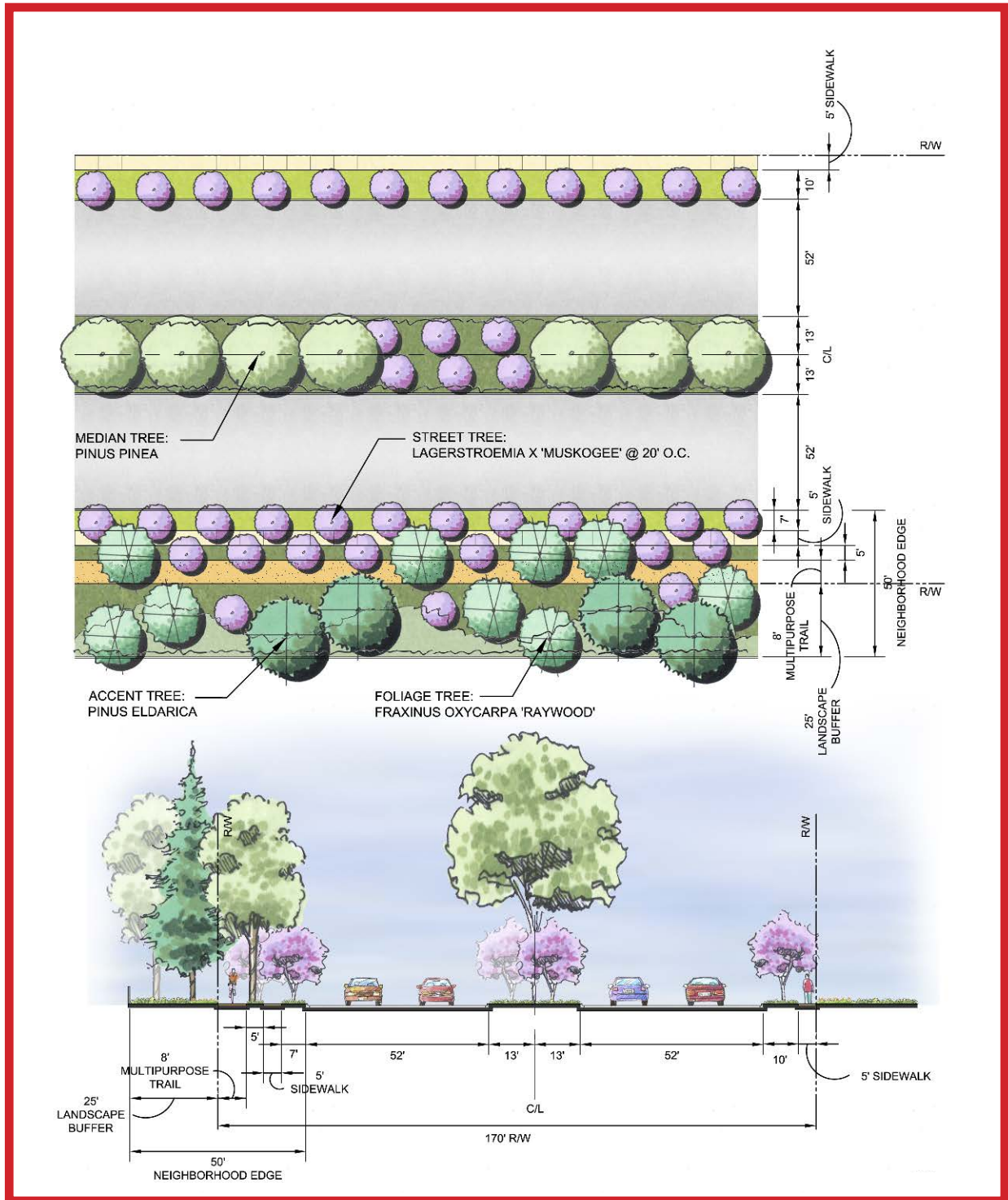
Exhibit 7-2: Conceptual Streetscape Master Plan

LEGEND

-  COMMUNITY GATEWAY MONUMENT
-  PRIMARY COMMUNITY GATEWAY ENTRY
-  MAJOR COMMUNITY GATEWAY ENTRY (GATED)
-  SECONDARY COMMUNITY ENTRY (GATED)
-  SECONDARY COMMUNITY ENTRY MONUMENT
-  ENHANCED LANDSCAPE SETBACK
-  MAJOR PEDESTRIAN CONNECTION
-  REGIONAL TRAIL
-  GREAT PARK
-  PRIVATE PARKS
-  REC. AREA
-  PARK AND TRAIL PEDESTRIAN CONNECTIONS



Parkside



Parkway and median trees shall match the Streetscape Master Plan.

Exhibit 7-3: Archibald Ave. Streetscape



Parkside

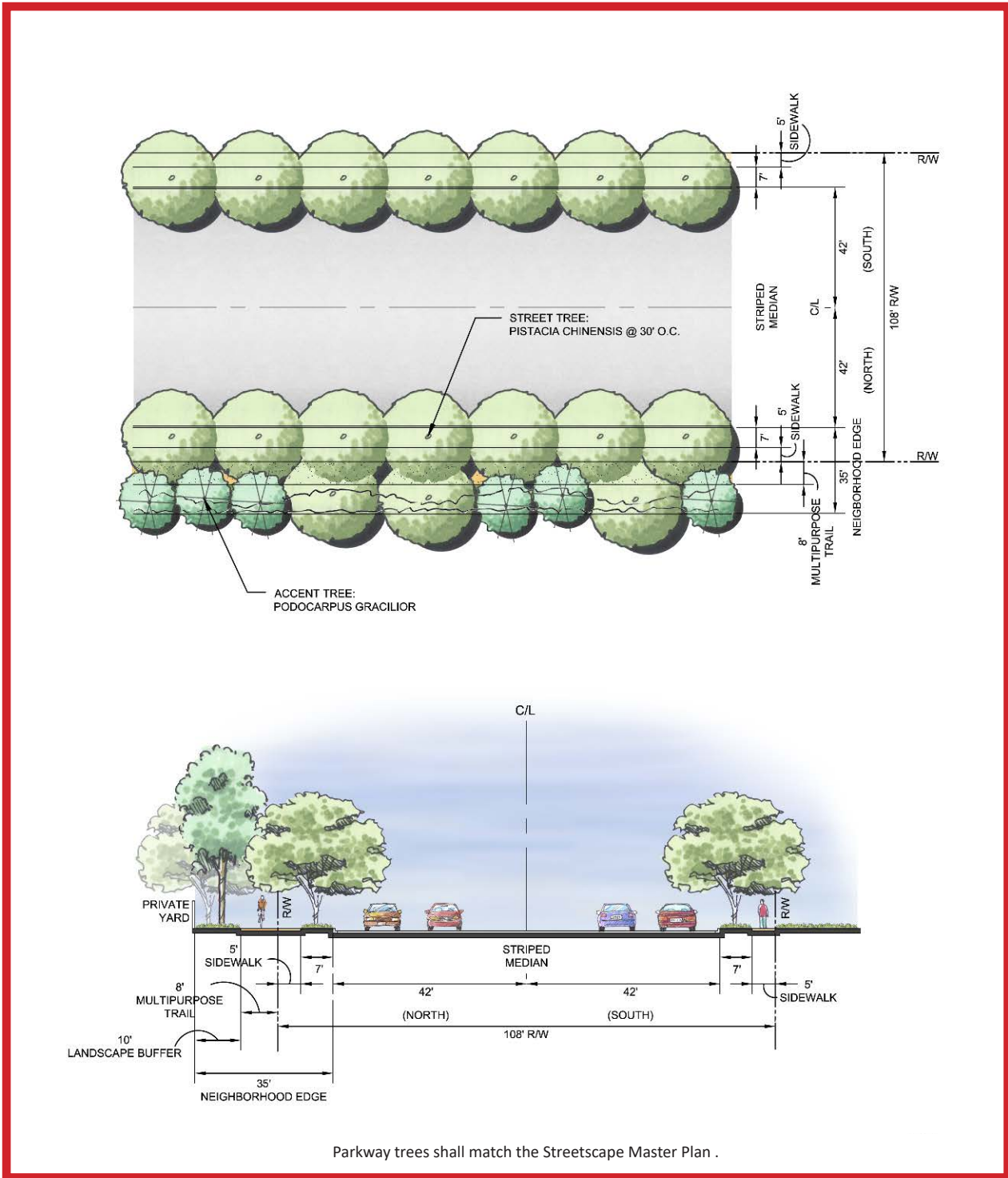


Exhibit 7-4: Eucalyptus Ave. Streetscape



Parkside

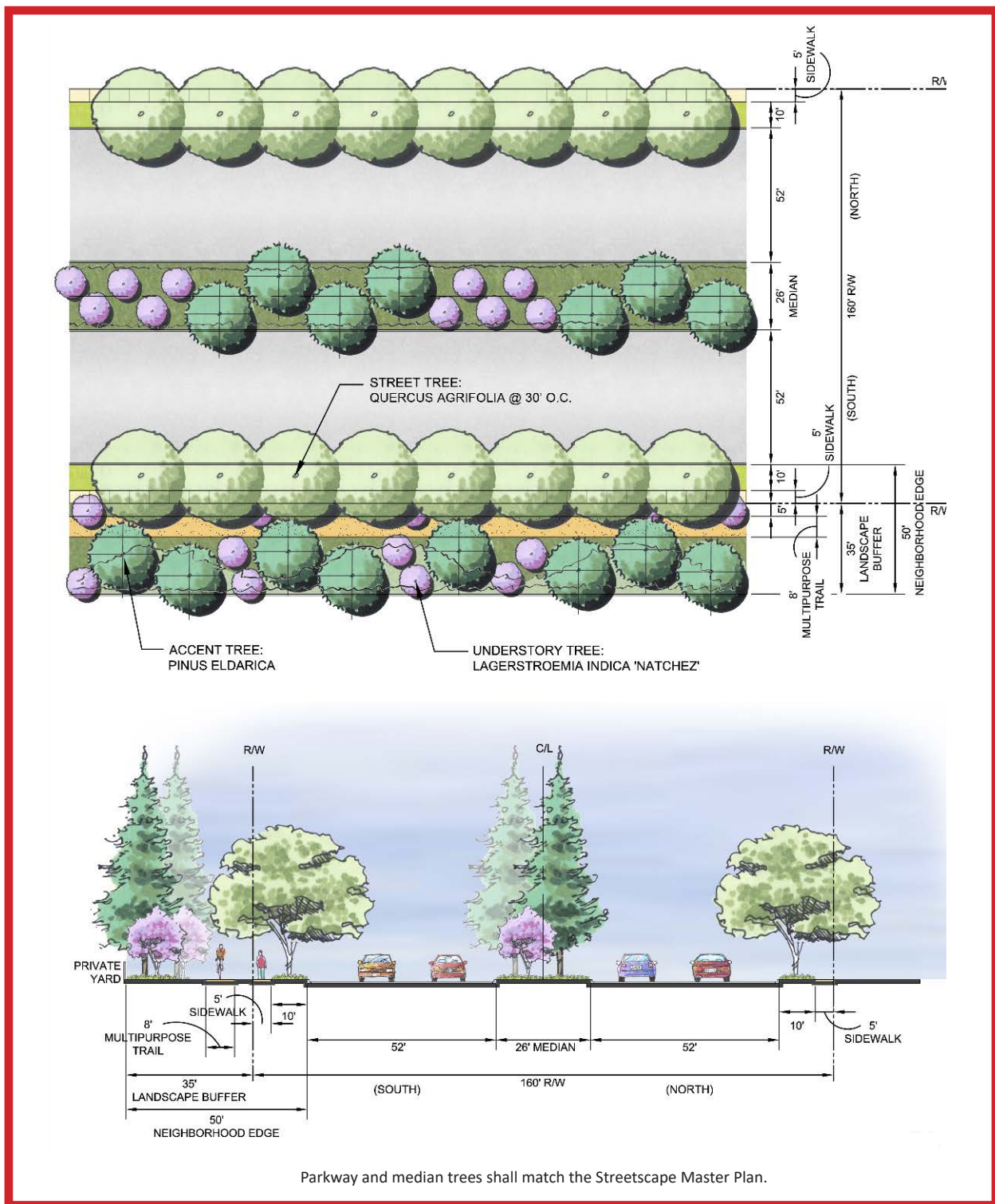


Exhibit 7-5: Ontario Ranch Rd. Streetscape



Parkside

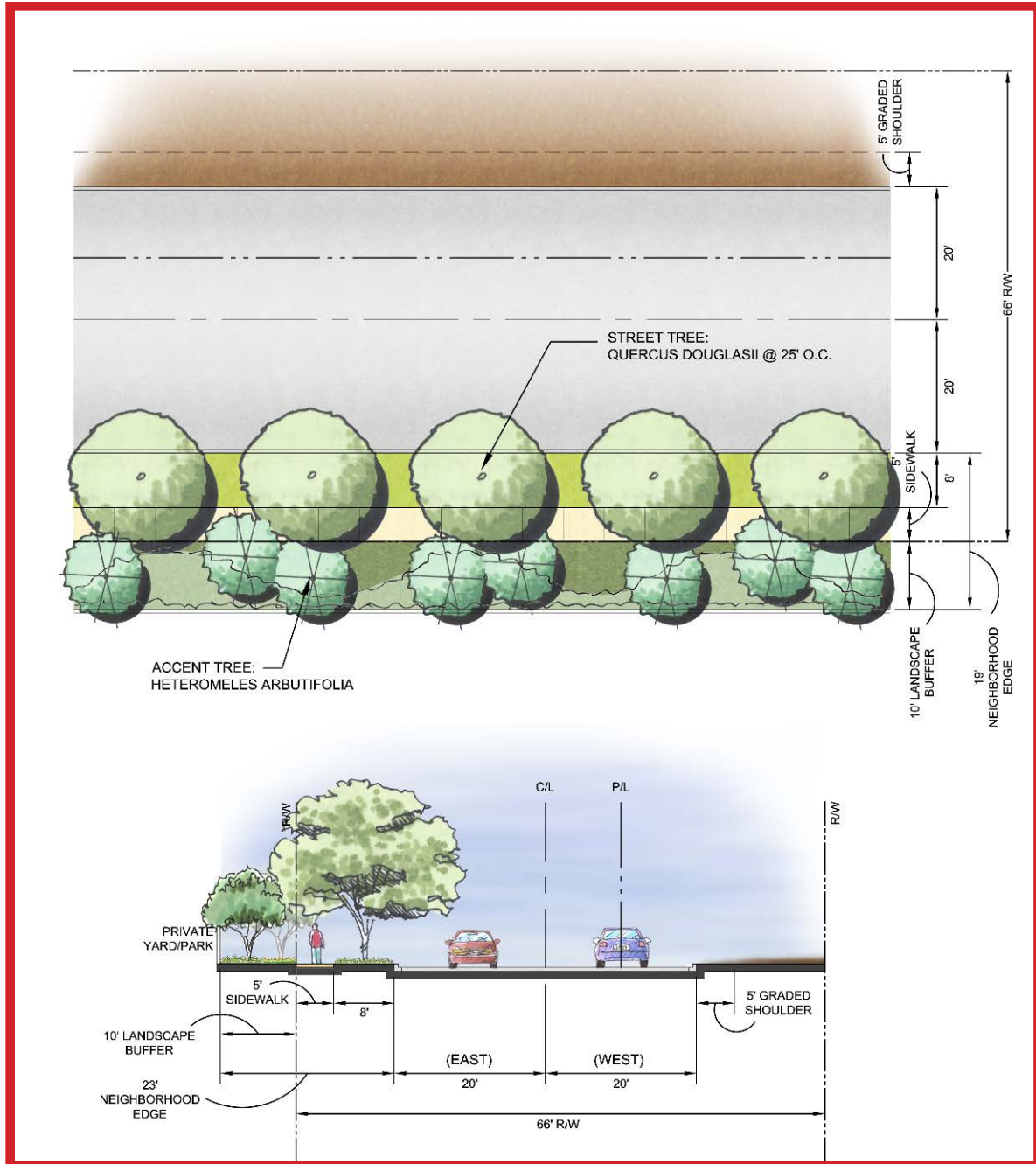


Exhibit 7-6: Carpenter Ave. Streetscape



Parkside

Carpenter Ave. Edison Ave.

Carpenter Ave. streetscape shall include the following:

- A landscaped parkway (10 feet 8 feet wide minimum) with a single row of street trees per the City of Ontario Street Tree Master Plan along both sides along east side of the street;
- A lineal 5-foot sidewalk (5 feet wide) along both sides of the street; on the east side of Carpenter Ave.;
- A Neighborhood Edge of 50 23 feet taken measured from face of curb to perimeter wall on the south east side of the street with 10 feet of buffer landscape;
- Background trees and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant groupings while combining interesting foliage textures and color Tree species per City of Ontario Street Tree Master Plan; and
- Monumentation as shown in the Conceptual Landscape Master Plan, Exhibit 20; and
- Refer to the streetscape cross section Exhibit 7-6: Carpenter Ave. Streetscape for illustration.

7.7.4 Interior Streetscape Design

Streetscape design within the interior of the Parkside Specific Plan community shall be consistent in character with the perimeter streetscapes and should help to promote pedestrian circulation into the Great Park.

Hellman Ave.

While Hellman Avenue is an interior street, the streetscape design is subject to the Streetscape Master Plan and shall include the following:

- A landscaped parkway (7-foot wide minimum) to accommodate shrubs and ground cover with a single row of street trees per the Streetscape Master Plan, along both sides of the street;
- A 5-foot wide sidewalk along both sides of the street that makes additional connections to park and paseo walkways;
- A Neighborhood Edge of 30 feet measured from face of curb to perimeter wall on both sides of the street with 18 feet of buffer landscape. Adjacent to the Park, there is no need for the 30-foot neighborhood edge. Tree species per the Streetscape Master Plan;
- Refer to streetscape section Exhibit 7-7: Hellman Ave. Streetscape for illustration.

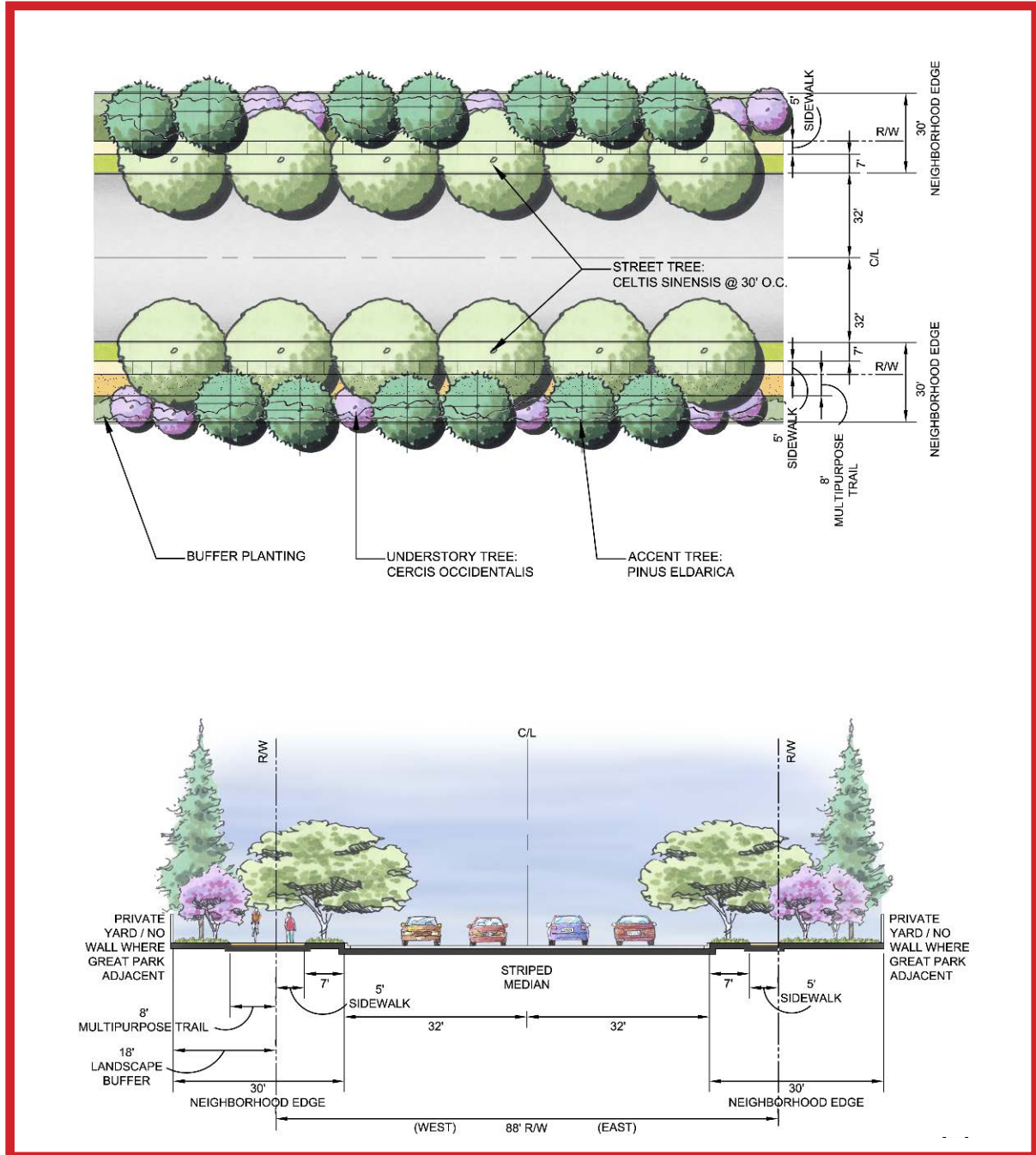
Primary Local Street

Where interior streetscapes interface with the Great Park and open space, special consideration should be taken to integrate pedestrian circulation into these areas via a street side pedestrian paseo system that links sidewalks to active walking trails and open space uses.

- A landscaped parkway (7-foot wide minimum) to accommodate shrubs and ground cover, along both sides of the street;
- A lineal sidewalk (5-foot wide) along both sides of the street that makes additional connections to park and paseo walkways;
- Refer to streetscape section Exhibit 7-8: Primary Local Street for illustration.



Parkside



Parkway trees shall match the Streetscape Master Plan.

Exhibit 7-7: Hellman Ave. Streetscape



Parkside

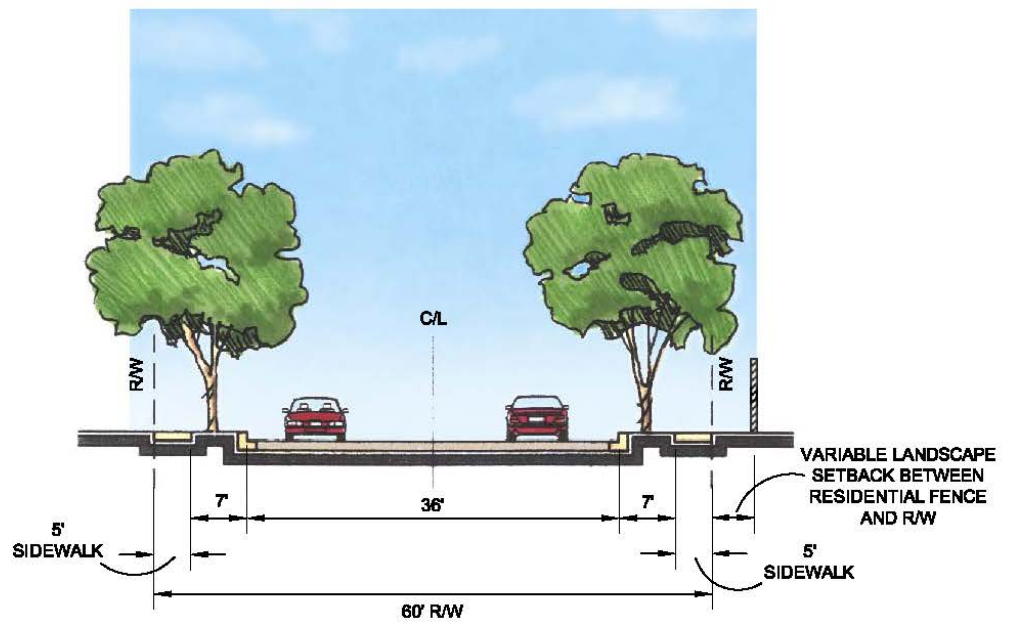
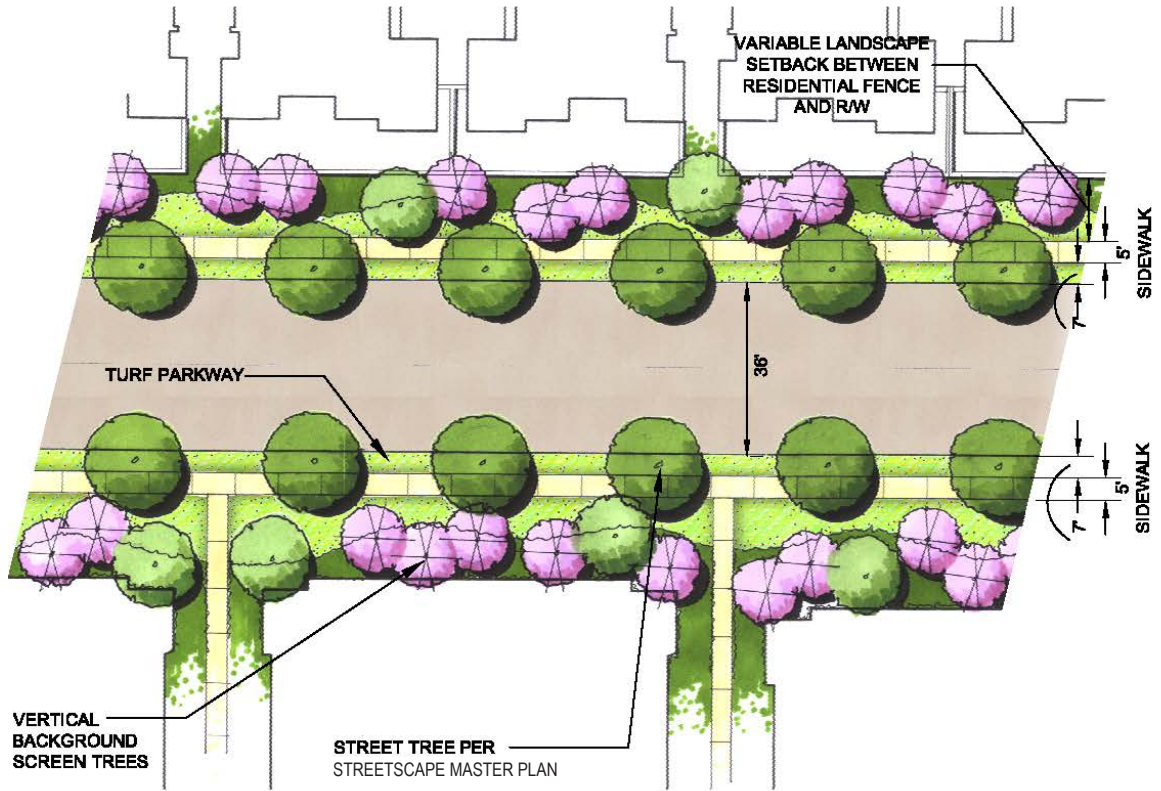
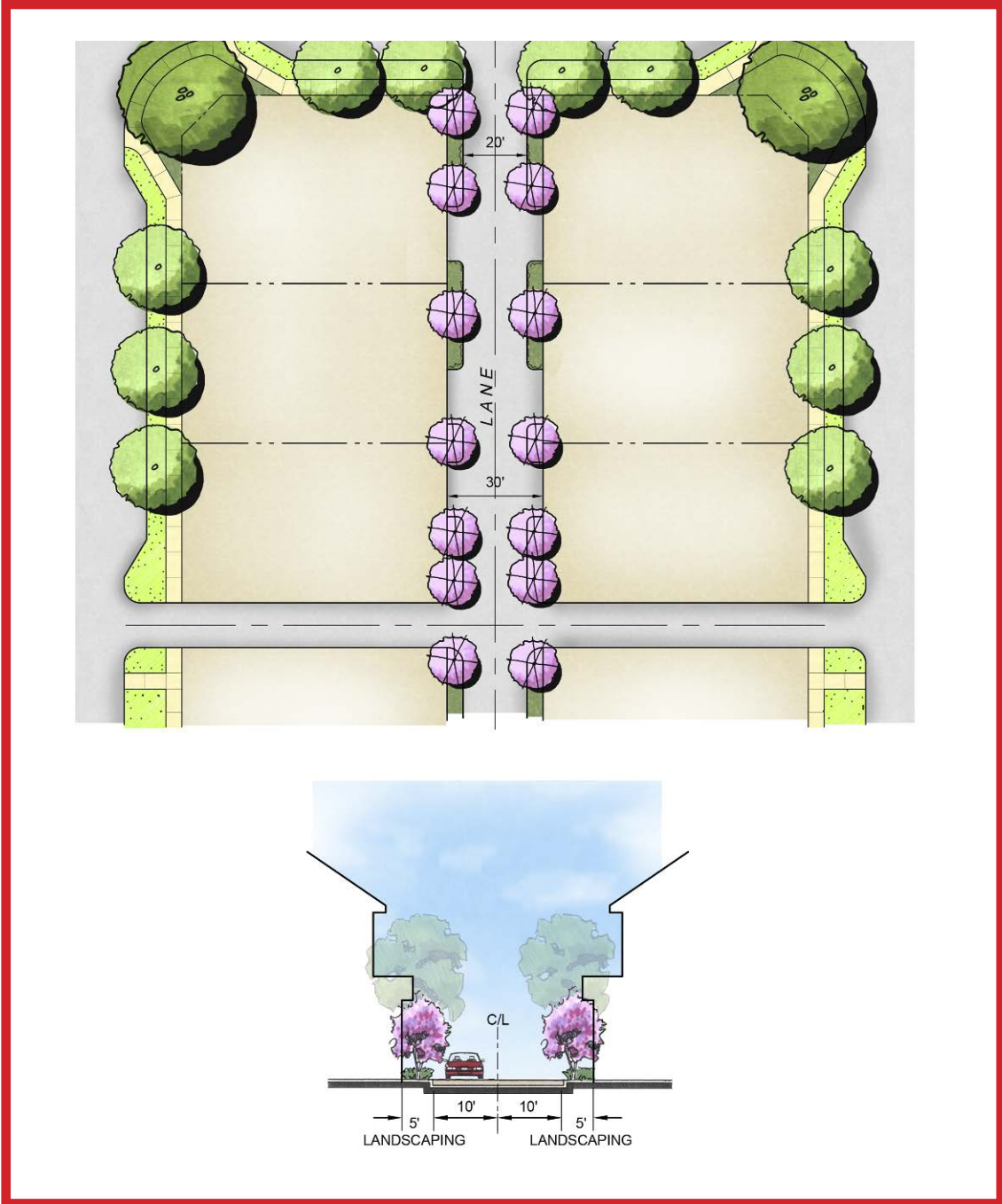


Exhibit 7-8: Primary Local Street



Parkside



Lanes (Private Alleys)

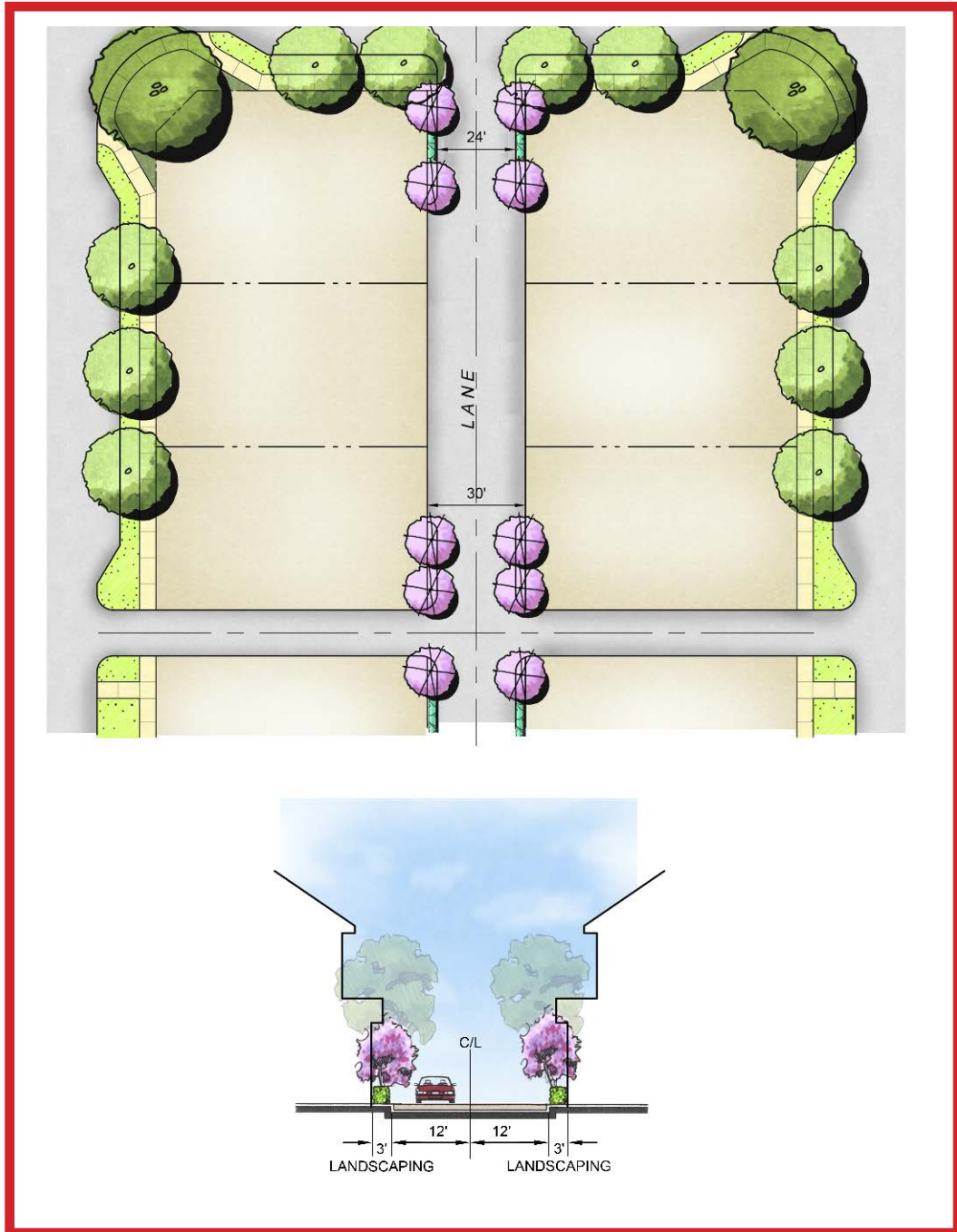
20' Paved Section / 30' Width, Garage to Garage

Note: Lane (private alley) paving may increase to 24' if required by the City Fire Department

Exhibit 7-9: Conceptual Lane (Private Alley) Streetscape



Parkside



Lanes (Private Alleys)

24' Paved Section / 30' Width, Garage to Garage

Exhibit 7-10: Conceptual Lane (Private Alley) Streetscape



Parkside

7.7.5 Lanes (Private Alleys)

Lanes (private alleys) proposed in **detached**, cluster, attached, and multi-family neighborhoods will be landscaped with smaller scale trees, shrubs, vines, and groundcover along pavement edges, to help add a pedestrian scale to the streetscene. Refer to Exhibits 7-9 & 7-10: Conceptual Lane (Private Alley) Streetscape.

7.7.6 Entries and Monumentation

Monumentation occurs throughout the Parkside Specific Plan community and is designed to establish a **basic** hierarchy for entering each area of the community. Along the perimeter edges there are several entry points into the community. At key entries, a landscape and monumentation program will be utilized to help identify the community as well as convey a “welcoming” feeling for both vehicular and pedestrian traffic. **Four basic Five entry** and monument treatments are used to set this hierarchy: Community Gateway Monuments, **Primary Community Gateway Entries**, **Major Community Gateway Entries**, **the Neighborhood Entry Secondary Community Entries**, and **Secondary Community Entry Monuments**. Refer to Exhibit 7-11: Community Gateway Monument Plan **for illustration of locations**.

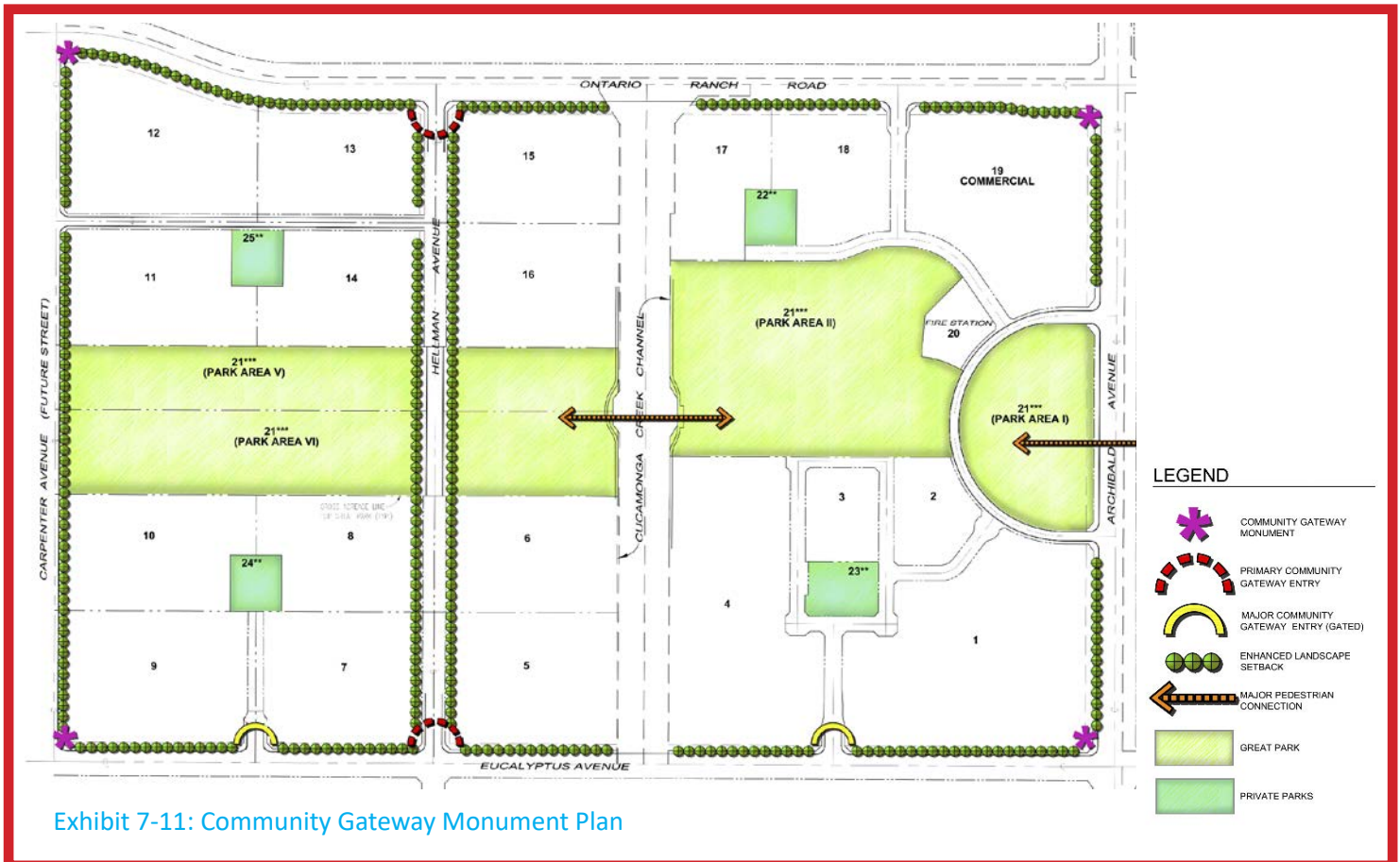


Exhibit 7-11: Community Gateway Monument Plan



Parkside

Community Gateway Monuments

The community of Parkside has a strong relationship with the "Great Park." The layout of the community planning areas completely embraces and encompasses the western section of the "Great Park."

As an example of this strong relationship between the community and the Park is the community's Gateway Monuments (Exhibit 20). Here the project's identity will

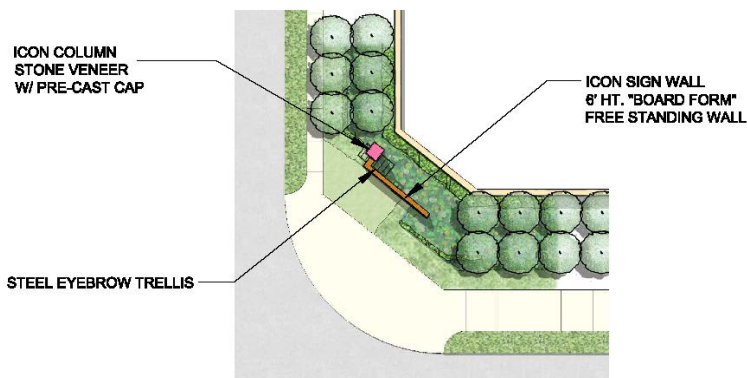
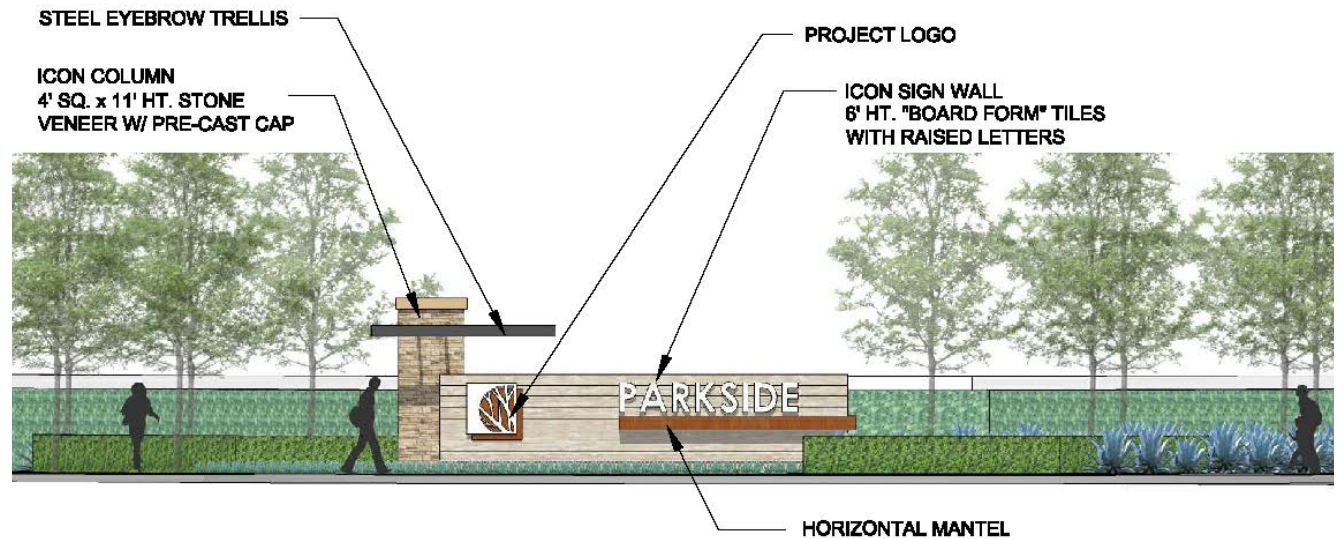
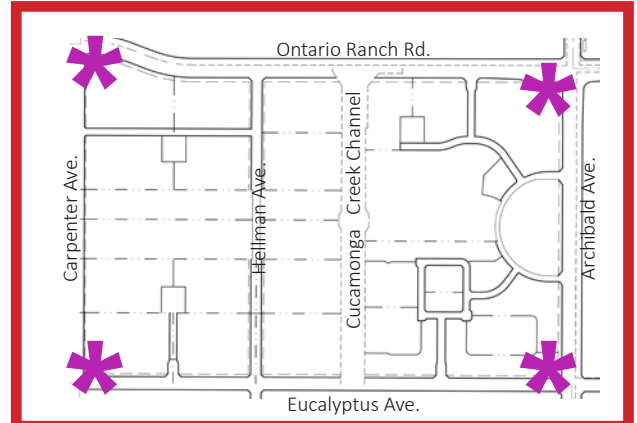


Exhibit 7-12: Community Gateway Monument Concept



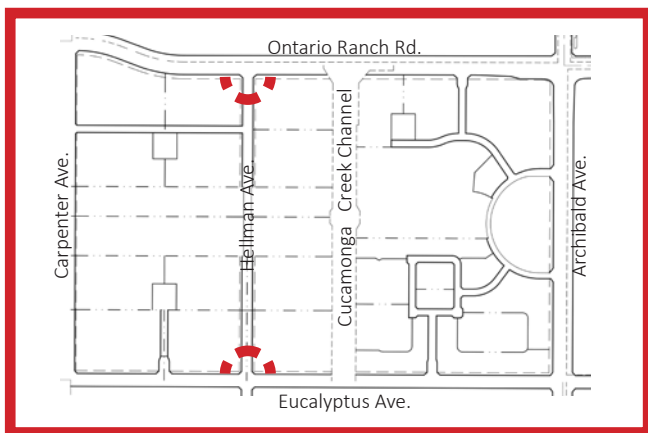
Parkside

be refined by adopting monument elements similarly found in the Great Park and bring it out to the identity will be refined by adopting monument elements similarly found in the Great Park and bring it out to the defined at the four corners of the community. Here these Specific Plan area. These four icons or “landmarks” will announce the Parkside community as somewhere truly special. Refer to Exhibit 7-12: Community Gateway Monument Concept for illustration.

Primary Community Gateway Entry

Parkside will have six three primary entries located at the major entry points into this community. Since two of these entries are shared points with the Great Park, the key design elements between the two should be continued and reinforced.

- A scaled-down version of the Community Gateway or park Monument shall be incorporated into the



primary entry statement and located to create a symmetrical formal design;

- The central primary statement shall be back dropped by an elegant 7-foot high curved stucco, brick, stone, or decorative split face block wall. The end of the wall shall be anchored by large, decorative pilasters an icon column;
- Community identification opportunities are located within the base of the primary statement or within a plaque inlaid into the anchoring pilasters columns;
- Architectural concrete caps, columns, trims, and bases help delineate each veneered material used;
- Use of “real” veneer material instead of faux concrete veneers;
- A linear row of vertical specimen screen trees shall be located behind the brick, stone, or decorative split face block wall, and in front of the community perimeter wall;
- Strong use of seasonal perennial and annuals providing for seasonal flowering and interest is encouraged;
- Enhanced pedestrian paving at street crossing and at monument location;
- Accent trees and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant groupings while combining interesting foliage textures and color;
- Accent and up-lighting of landscape/monumentation; and
- Refer to Exhibit 7-13: Primary Community Gateway Entry Concept for illustration.



Parkside

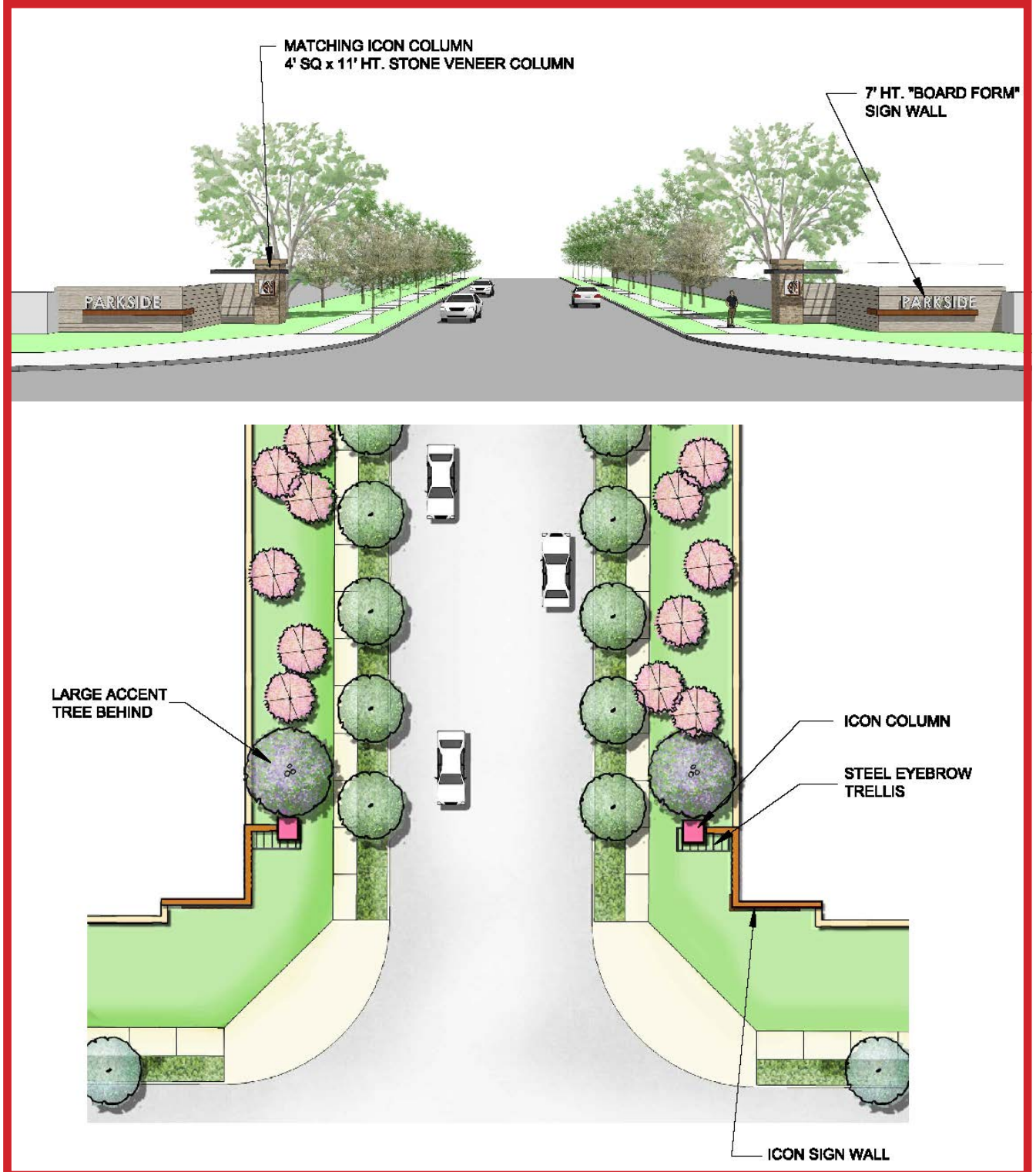


Exhibit 7-13: Primary Community Gateway Entry Concept



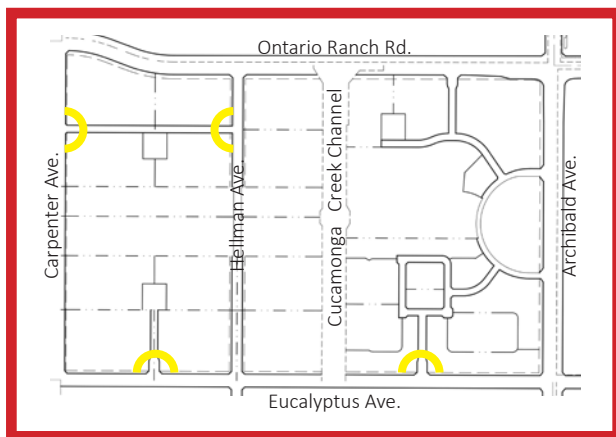
Parkside

Major Community Gateway (Gated)

Parkside will have 3 of these types of entry points. These Gateways will echo the same aesthetic as the Community Gateway Monuments located at the four corners of the Specific Plan boundary.

- A scaled-down version of the Community Gateway Monument shall be incorporated into this entry statement and located to create a symmetrical formal design;
- The central primary statement shall be back dropped by an elegant 7-foot high stucco, brick, stone, or decorative split face block wall ;
- Community identification opportunities are located on the monument wall;
- Architectural concrete caps, columns, trims, and bases may be used;

- Vehicular gates shall complement the style of the wall and be consistent with the other community monument characteristics. Gates shall be fully automatic with a directory system and set back a minimum of 100 feet from a public street intersection. Emergency access provided per local agency requirements;
- A linear row of canopy trees shall be located along the entry road leading up to the gate as well as in the median, if a median occurs;
- Strong use of seasonal perennial and annuals providing for seasonal flowering and interest is encouraged;
- Accent trees and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant groupings while combining interesting foliage textures and color;
- Accent and up-lighting of landscape/monumentation; and
- Refer to Exhibit 7-14: Major Community Gateway (Gated) Concept for illustration.



Secondary Community Entry and Monumentation

The Secondary Community Entry and Monumentation shall include the following:

- Similar in design as the Primary Entries but reduced in scale;
- Freestanding curved walls at each corner with anchoring entry pilasters. The large, elegant entry pilasters shall be brick, stone, or decorative split face block and located adjacent to right-of-way;



Parkside

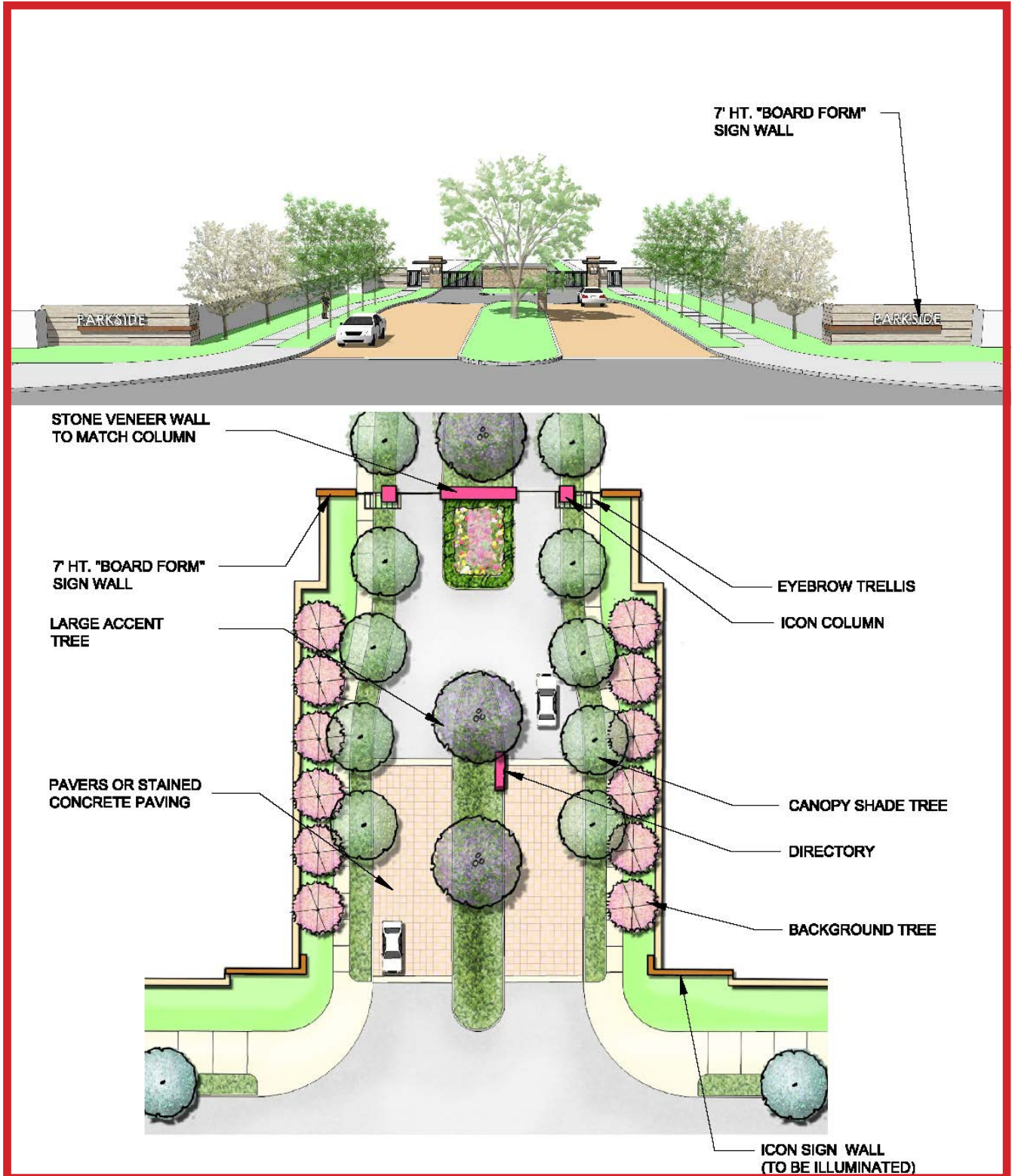


Exhibit 7-14: Major Community Gateway (Gated) Concept



Parkside

- Identification field for potential sign lettering placement shall be on a plaque inlayed into the two anchoring pilasters;
- Architectural concrete caps, trim, and bases to help delineate each veneered material used;
- Use of “real” veneer materials instead of faux concrete veneers;
- Use of large specimen native trees to flank each side of entry drive at site entry;
- Seasonal perennial flowering to allow for seasonal flowering interest throughout the year;
- Enhanced pedestrian paving at street crossing and at monument location;
- Accent trees and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant groupings while combining interesting foliage textures and color;
- Accent and up-lighting of landscape/monumentation; and

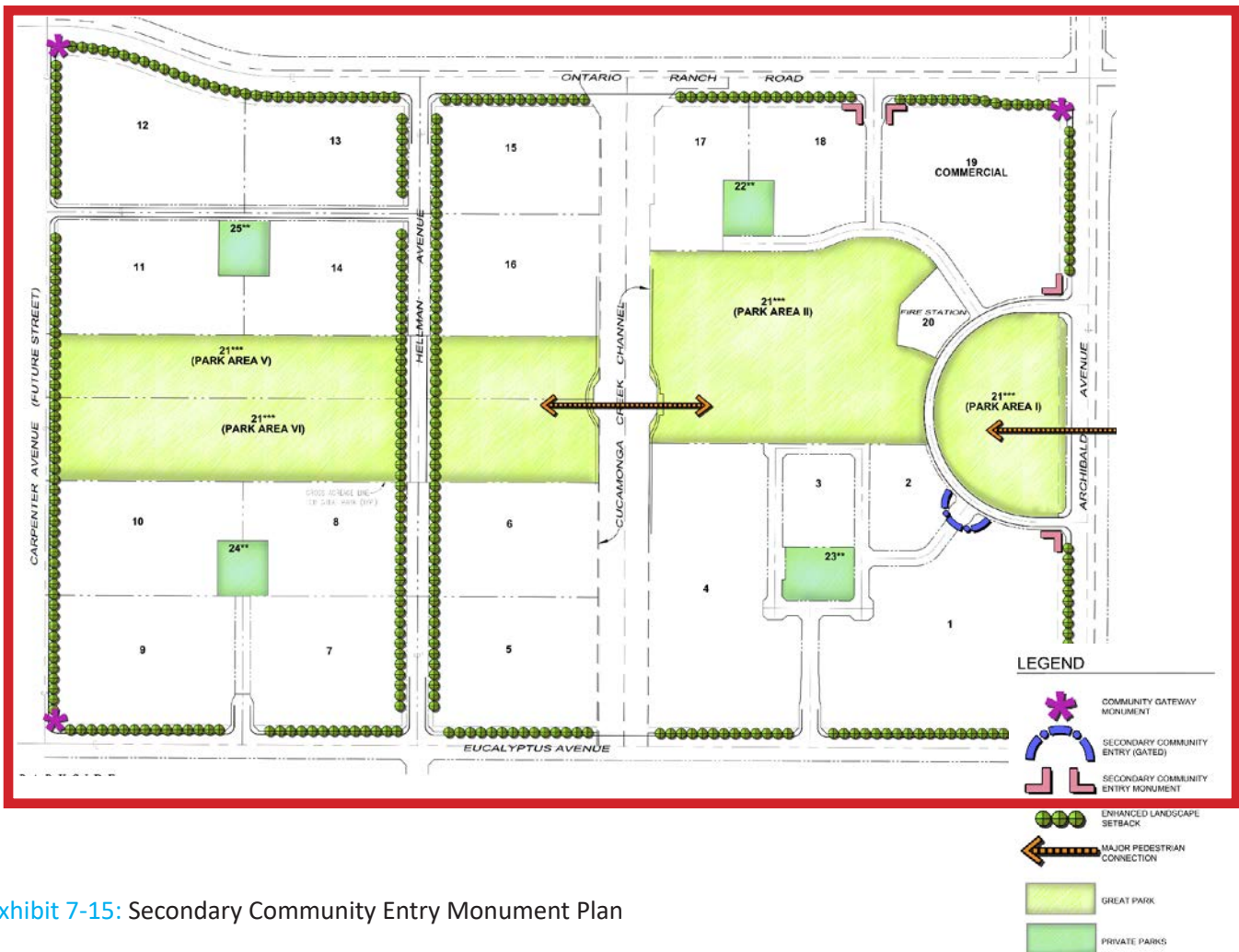


Exhibit 7-15: Secondary Community Entry Monument Plan



Parkside

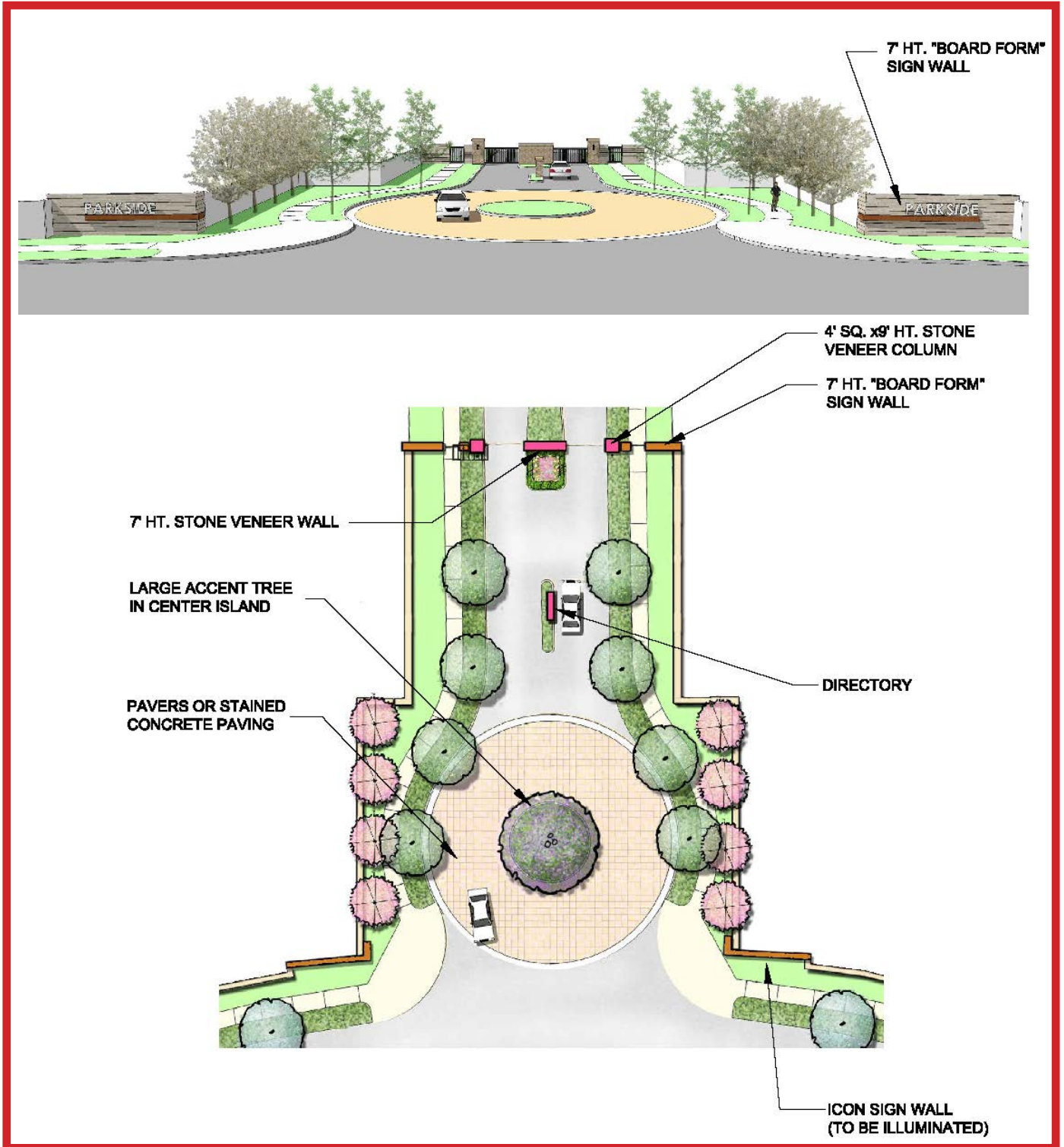


Exhibit 7-16: Secondary Community Entry (Gated) Concept



Parkside

• Refer to Exhibit 20, Conceptual Landscape Master Plan and Exhibit 30, Secondary Community Gateway and Gated Entry Monument Plan for detailed conceptual illustration, and Exhibit 30a, Secondary Community Gateway Entry.

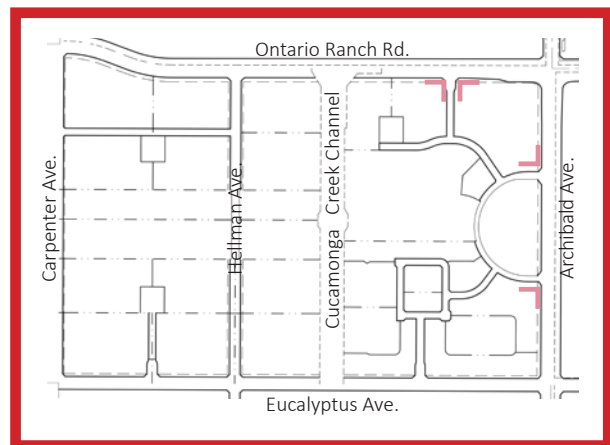
Secondary Community Entry (Gated)

Parkside will have one gated Secondary Community Entry. This entry will combine several neighborhoods together behind a grand gated entry statement. Beyond these gates each neighborhood would still have its own individual (non-gated) entry. The Secondary Community Gated-This Entry will have the same hierarchy and stature as the other Secondary Community Entry and Monumentation sharing most of the same similar elements and features. Location and final configuration of the Secondary Community Gated Entries shall be reviewed and approved by the City as the other listed entries and monumentation.

The Secondary Community Entry (Gated) shall include the following:

- A large spacious motor court A turnaround with decorative paving shall be provided and sized to accommodate required turn-around clearance and predicted volume of cars. At the center of the motor court, a large heritage tree shall be located in a raised island;
- Two (2) large, elegant entry pilasters Entry sign wall shall be located behind curb announcing entry into the widened motor court. These pilasters turnaround. This wall shall be constructed of stucco, brick, stone, or decorative split face block;

- Identification field for potential sign lettering placement shall may be on a plaque inlayed into the two entry pilasters sign wall;
- Walls enclosing the motor court turnaround and anchoring pilasters columns and wall at the vehicle gates shall be stucco, brick, stone, or decorative split face block with architectural concrete caps, trim and base to help delineate each veneered material used;
- A strong formal linear row of trees is encouraged for the entries entry drive and to frame to motor court;
- Vehicular gates shall be decorative and constructed complement the style of tubular steel the wall and be consistent with the other community monument characteristics. Gates shall be fully automatic with a directory system and set back a minimum of 100 feet from a public street intersection. Gates shall be fully automatic with a "state-of-the-art" directory system. Emergency access provided per local agency requirements;





Parkside

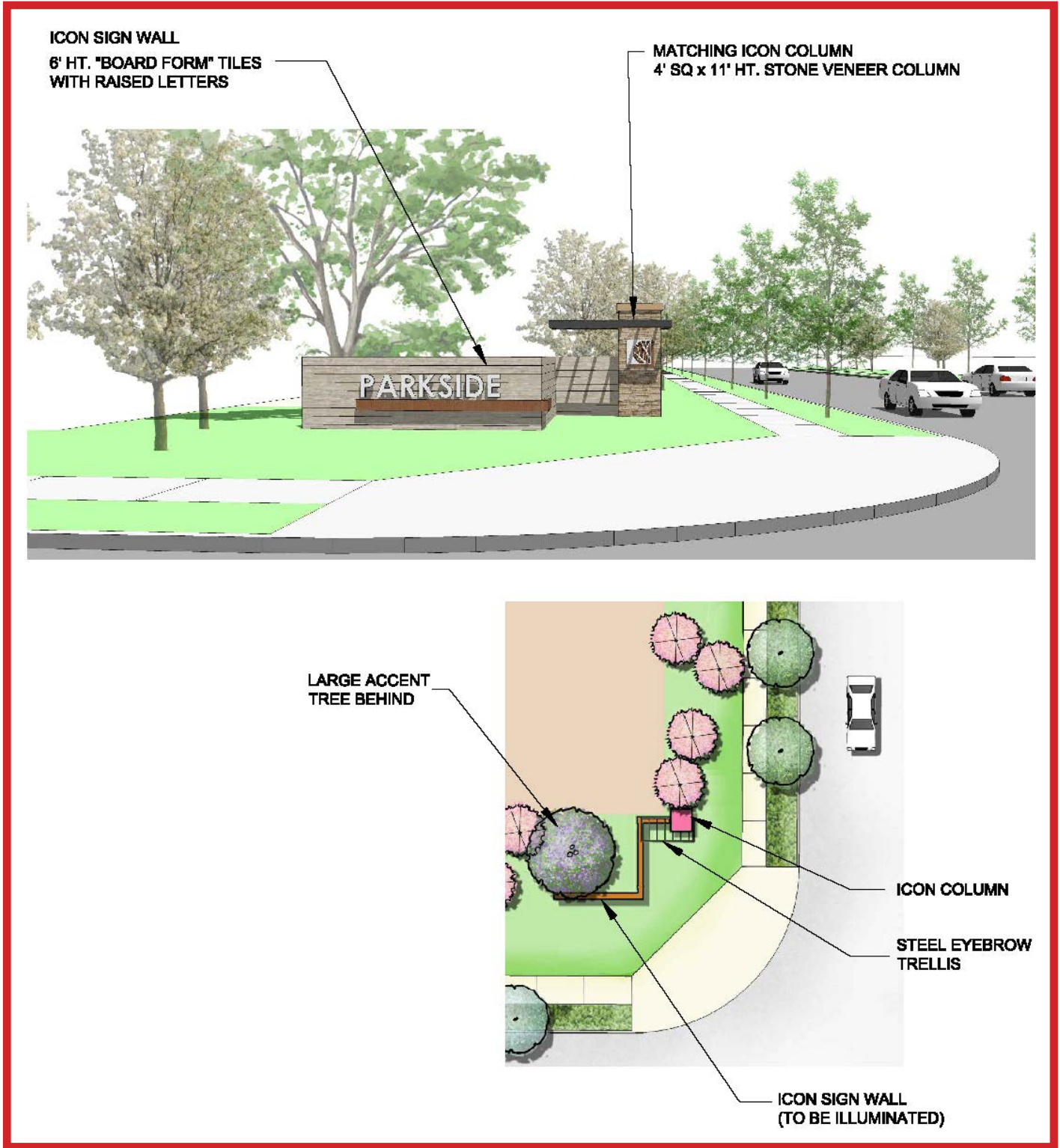


Exhibit 7-17: Secondary Community Entry Monument Concept



Parkside

- Use of “real” veneer materials instead of faux concrete veneers;
 - Seasonal perennial flowering to allow for seasonal flowering interest throughout the year;
 - Accent trees and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant groupings while combining interesting foliage textures and color;
 - Accent and uplighting of landscape/monumentation; and
 - Refer to Exhibit 7-16: Secondary Community Entry (Gated) Concept for illustration.
- Neighborhood Entry**
- Neighborhood entries shall occur) Concept for each planning area within the Parkside Specific Plan area. These entries shall be used to help continue the landscape character theme to the

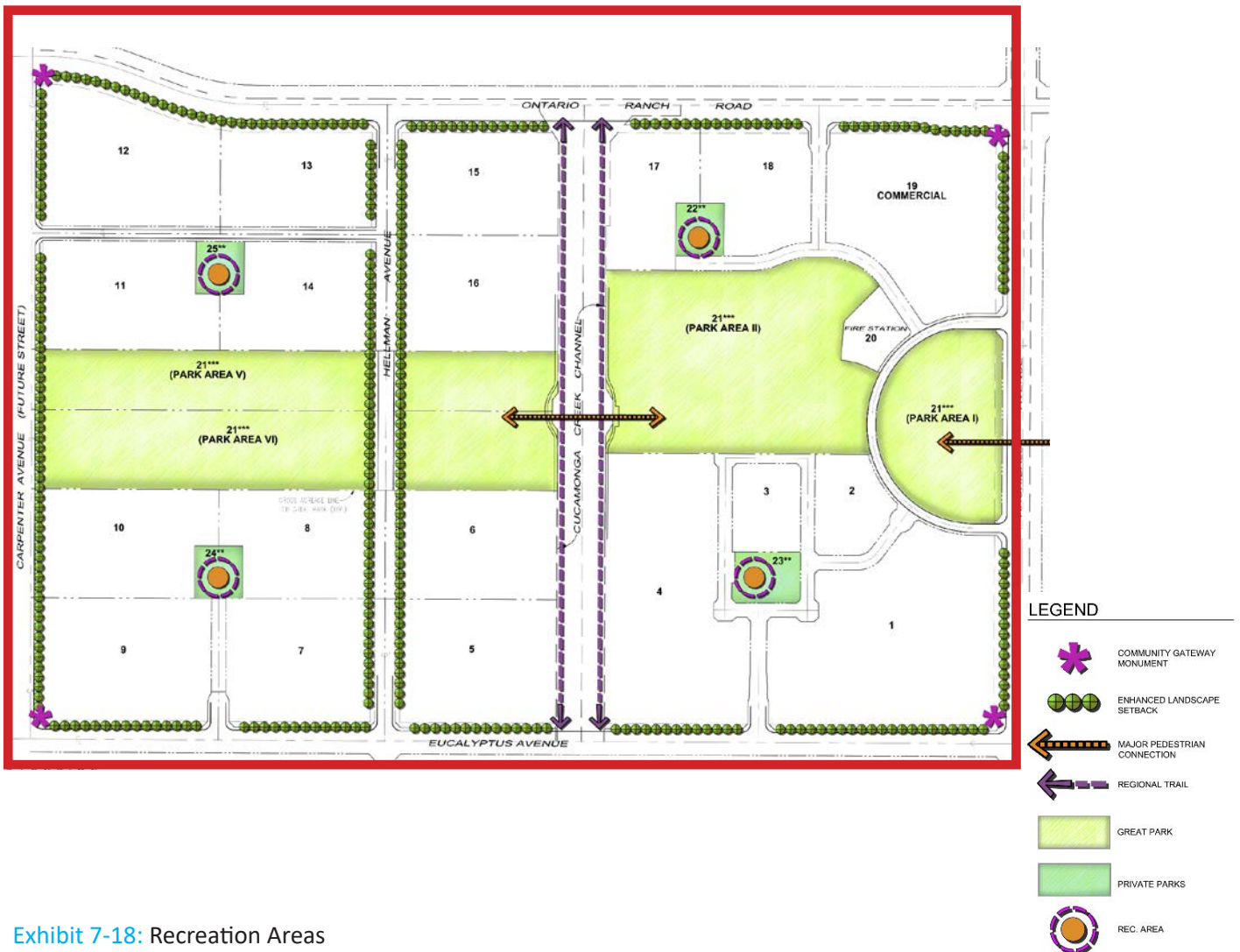


Exhibit 7-18: Recreation Areas



Parkside

“core” of the community. Each neighborhood built within the project site will have the opportunity to identify their individual project character while providing the basic design features of the other monuments illustration:

- There are two (2) types of neighborhood entries: Gated and Non-gated. Selection of entry type will depend upon space available, local street configuration and residential product type. Location and final configuration of gated entries shall be reviewed and approved by the City.
- The Neighborhood Entry (Gated) shall include the following:
 - Decorative freestanding walls embodying the same character as that of the secondary entries. At the end of the walls, located just behind curb of the entry road, are two large pilasters. These pilasters support two (2) sets of swing gates;
 - Identification for the gate community is located within a plaque inlayed into both pilasters;
 - Vehicular gates shall be decorative and constructed of tubular steel and set back a minimum of 100 feet from a public street intersection. Gates shall be fully automatic with a “state-of-the-art” directory system. Emergency access provided per local agency requirements;
 - A large spacious motor court with decorative paving shall be provided and sized to accommodate required turn-around clearance. At the center of the motor court, a large Heritage Tree shall be located in an island;
 - Use of “real” veneer materials instead of faux concrete veneers;
 - A strong formal linear row of trees is encouraged for the entries drive and along the sides of the motor court;
 - Seasonal perennial flowering to allow for seasonal interest throughout the year; and
 - Refer to Exhibit 20, Conceptual Landscape Master Plan and Exhibit 31, Neighborhood Entry Monument Plan for detailed conceptual illustrations, and Exhibit 31b, Neighborhood Gated Entry Monument.
- The Neighborhood Entry (Non-gated) shall include the following:
 - Freestanding large entry pilaster set within the landscaped parkway. This pilaster should embody the same character as that of the Secondary Community Entry but is slightly

Secondary Community Entry Monument

The Secondary Community Entry Monument shall include the following:

- Similar in design as the Primary Entries but reduced in scale to create a pedestrian “gateway” into each neighborhood;
- Freestanding walls at each corner with anchoring entry columns. The columns shall be stucco, brick, stone, or decorative split face block and located adjacent to right-of-way;
- Identification field for potential sign lettering placement within a plaque inlayed into pilasters shall be located on the sign wall;



Parkside

- Architectural concrete caps, trim, and bases to help delineate each veneered material used may be used as well as an eyebrow trellis connected to the icon column;
- Enhancement of corner cut wall and use of accent pilasters Use of large specimen native trees to anchor flank each side of entry drive at site entry;
- Use of “real” veneer materials instead of faux concrete veneers;
- Seasonal perennial flowering to allow for seasonal flowering interest throughout the year;
- Enhanced pedestrian paving at street crossing and at monument location;
- Accent trees and shrub masses planted in series of layers (foreground, midground, background) to help define borders and plant groupings while combining interesting foliage textures and color;
- Accent and up-lighting of landscape/monumentation; and
- Refer to Exhibit 7-17: Secondary Community Entry Monument Concept for illustration.



Exhibit 7-19: Pocket Park Concept A



Parkside



Exhibit 7-20: Pocket Park A Rendering

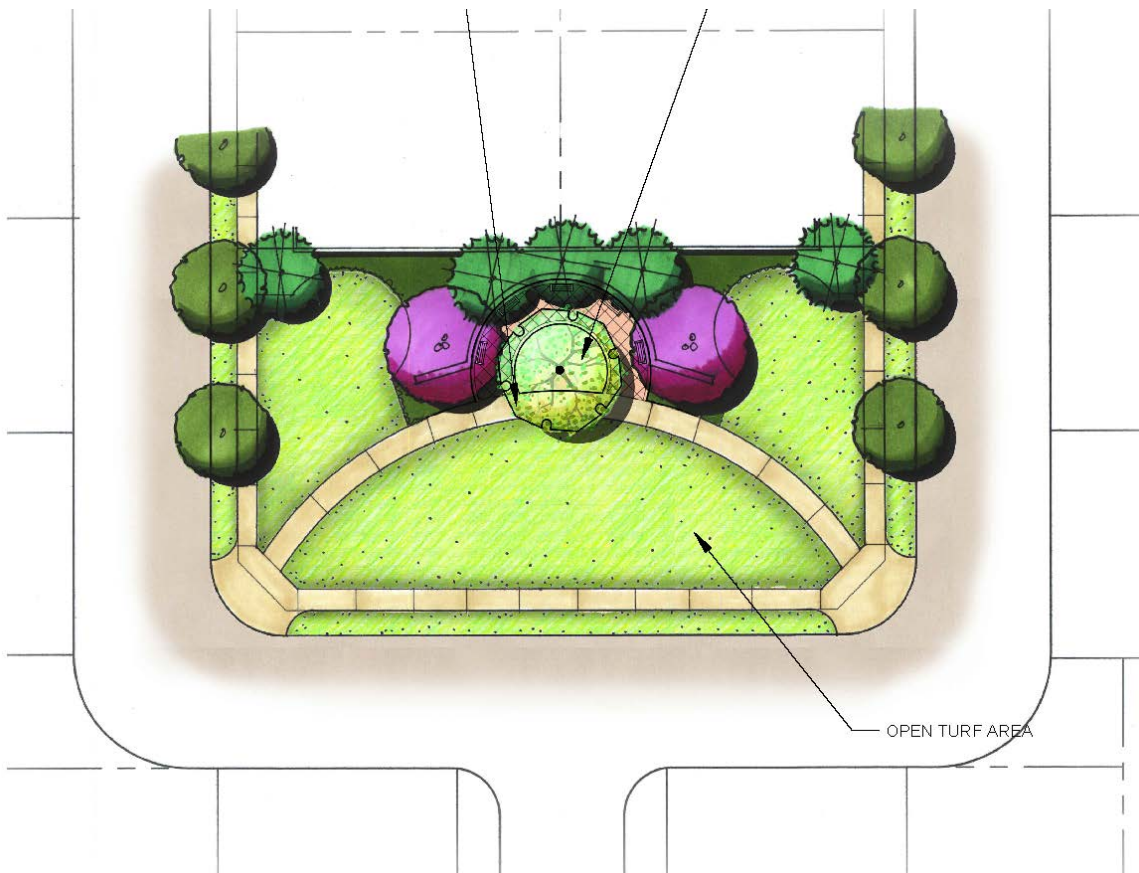


Exhibit 7-21: Pocket Park Concept B



Parkside

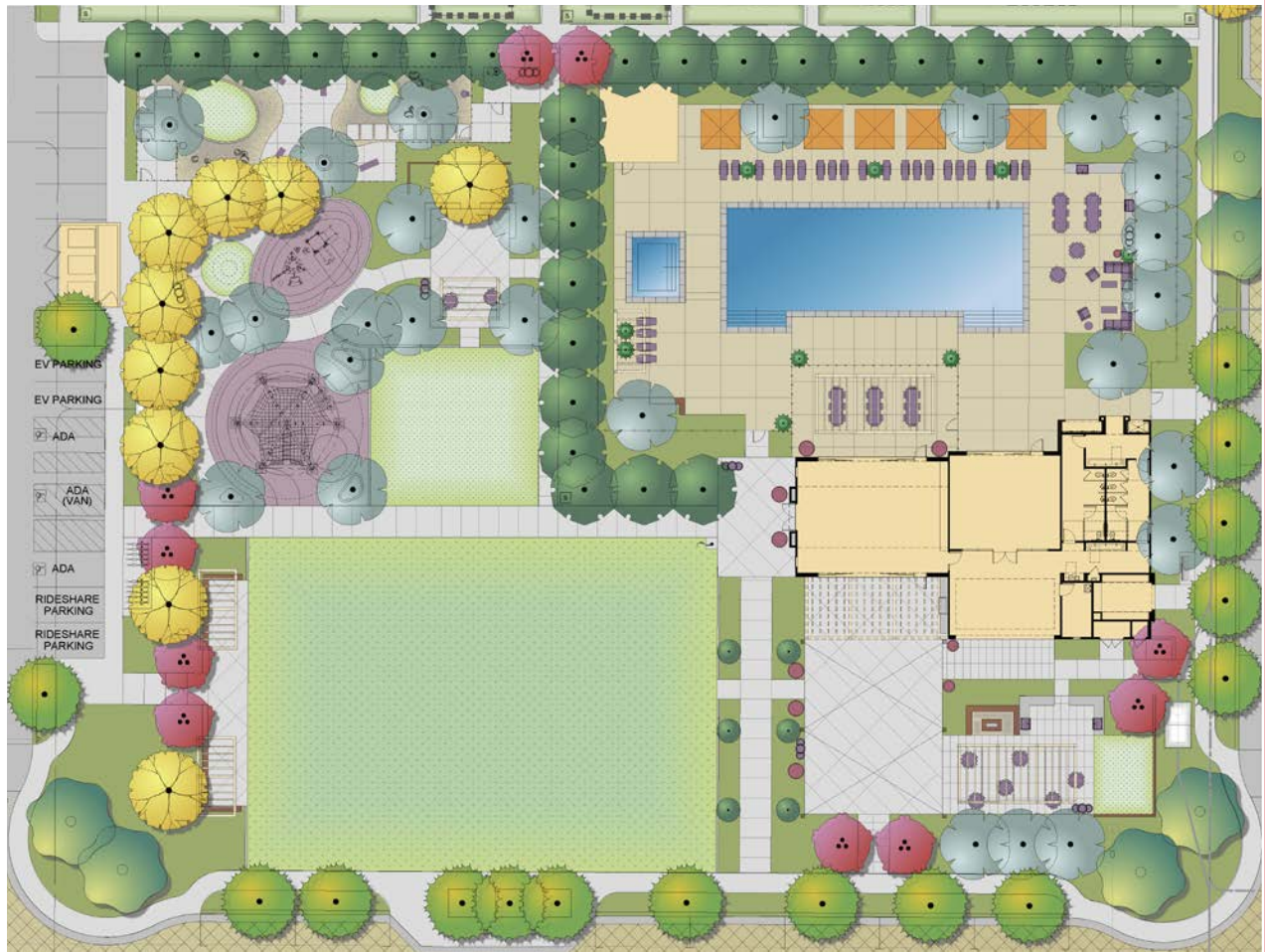
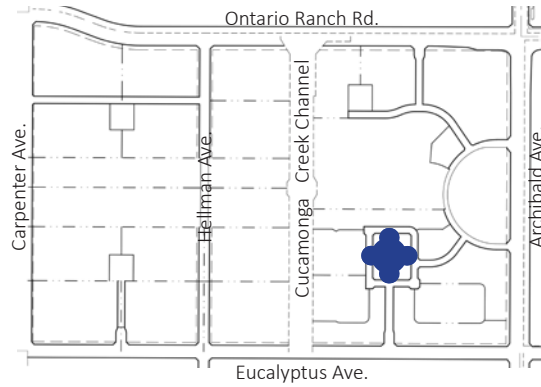


Exhibit 7-22: Recreation Area Concept A



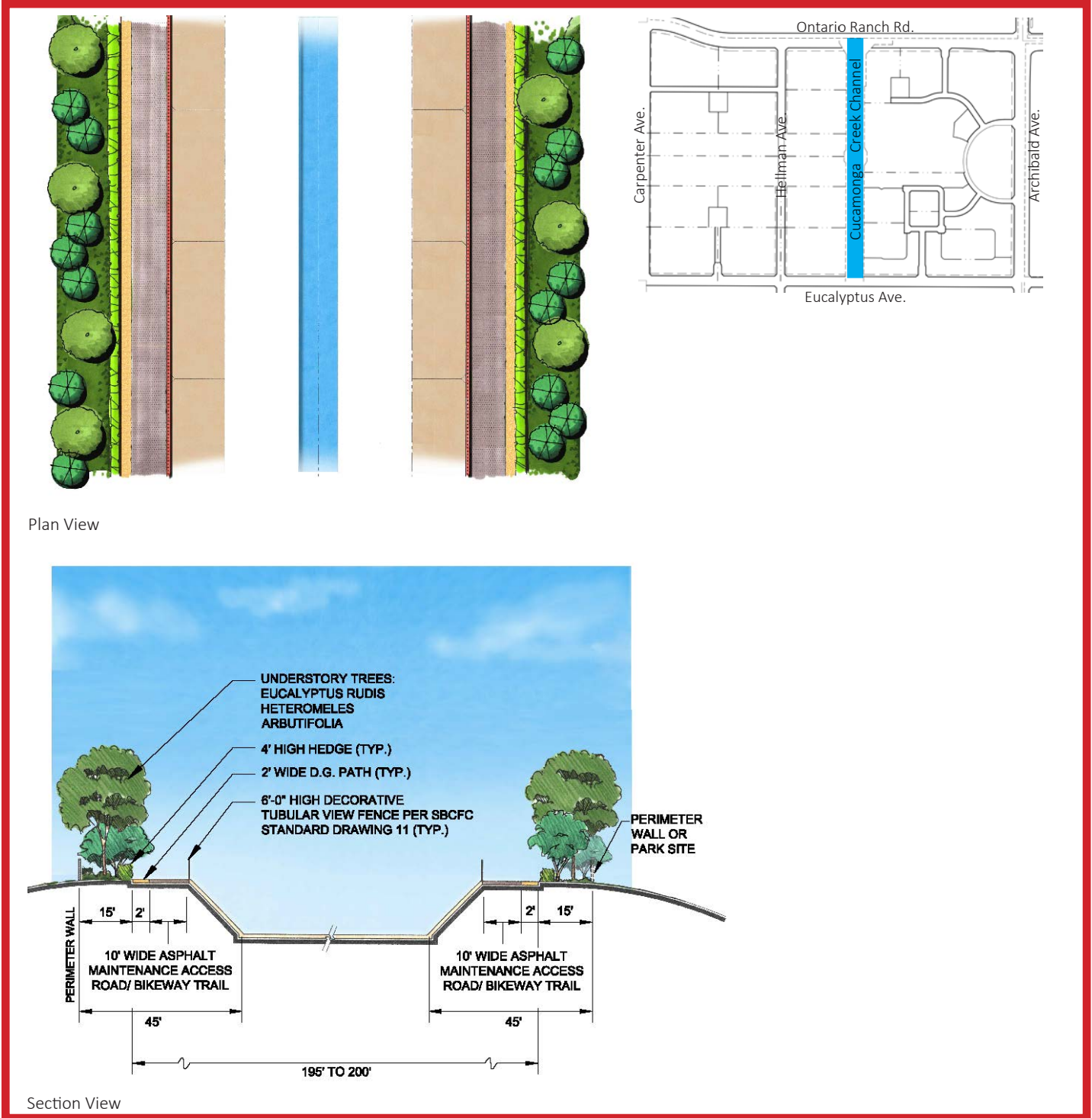
Parkside



Exhibit 7-23: Recreation Area Concept B



Parkside



Plan View

Section View

Exhibit 7-24: Cucamonga Creek Channel Regional Trail Easement



Parkside

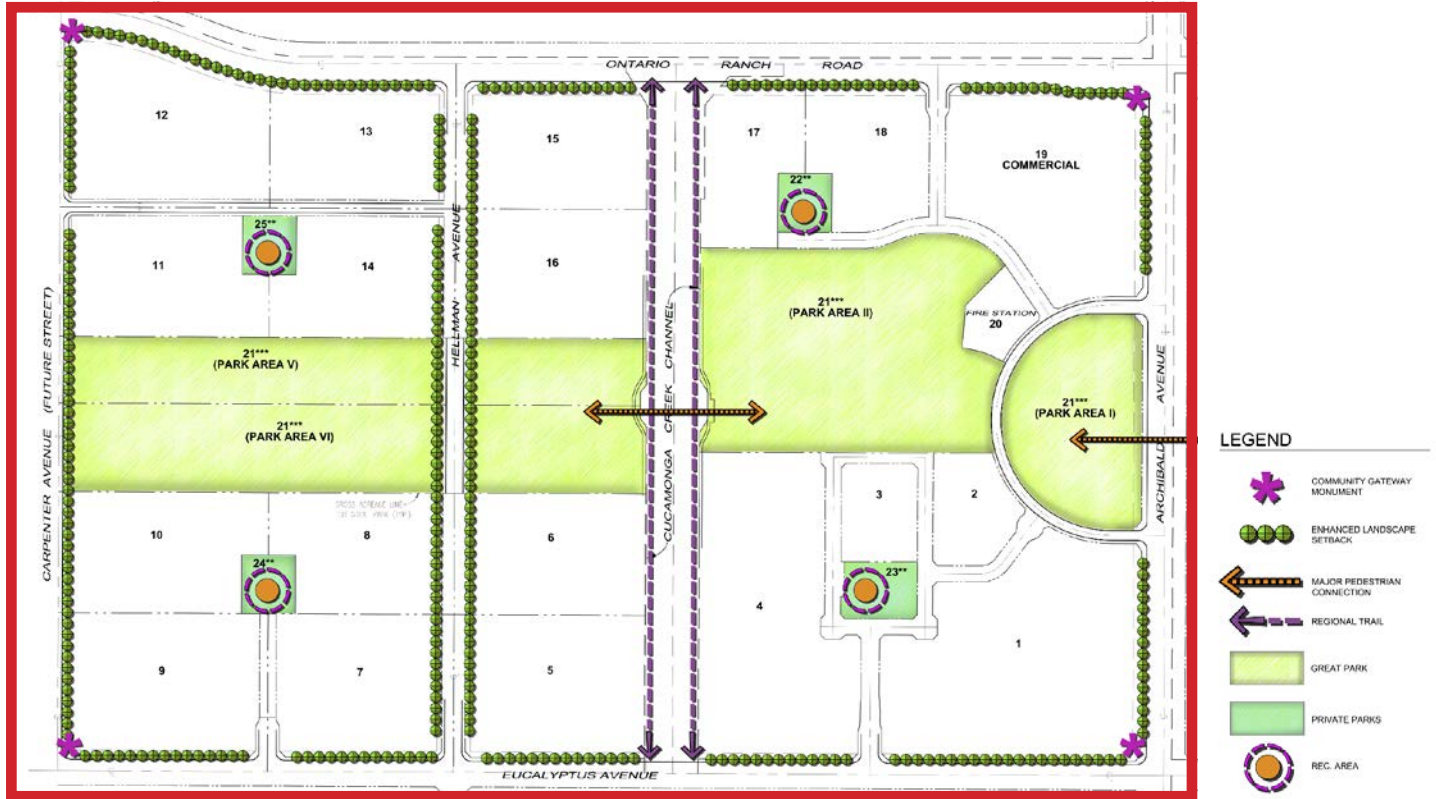


Exhibit 7-25: Channel Regional Trail Easement Plan

7.7.7 Parks and Open Space

The Parkside Specific Plan area will have a central Great Park green core that consists primarily of open space for recreation and passive use. Exterior walkways and paseos will lead to this centralized area. Refer to Exhibit 7-18: Recreation Areas for illustration.

The following areas make up the remainder of the Park and Open Space System for Parkside: private community recreation and the paseo trail system.

A. The Great Park

The Great Park consists of the following:

Parking—Parking shall be adequate to accommodate daily use of the park, and will be provided on adjacent streets or within the park area;

Landscaping—Landscaping within the Great Park shall harmonize with the surrounding streetscapes. Large specimen trees should be used within the open turf areas to help provide shade and screening of unwanted views. Accent trees should also be used at pedestrian entries for color and seasonal interest.

B. Pocket Parks

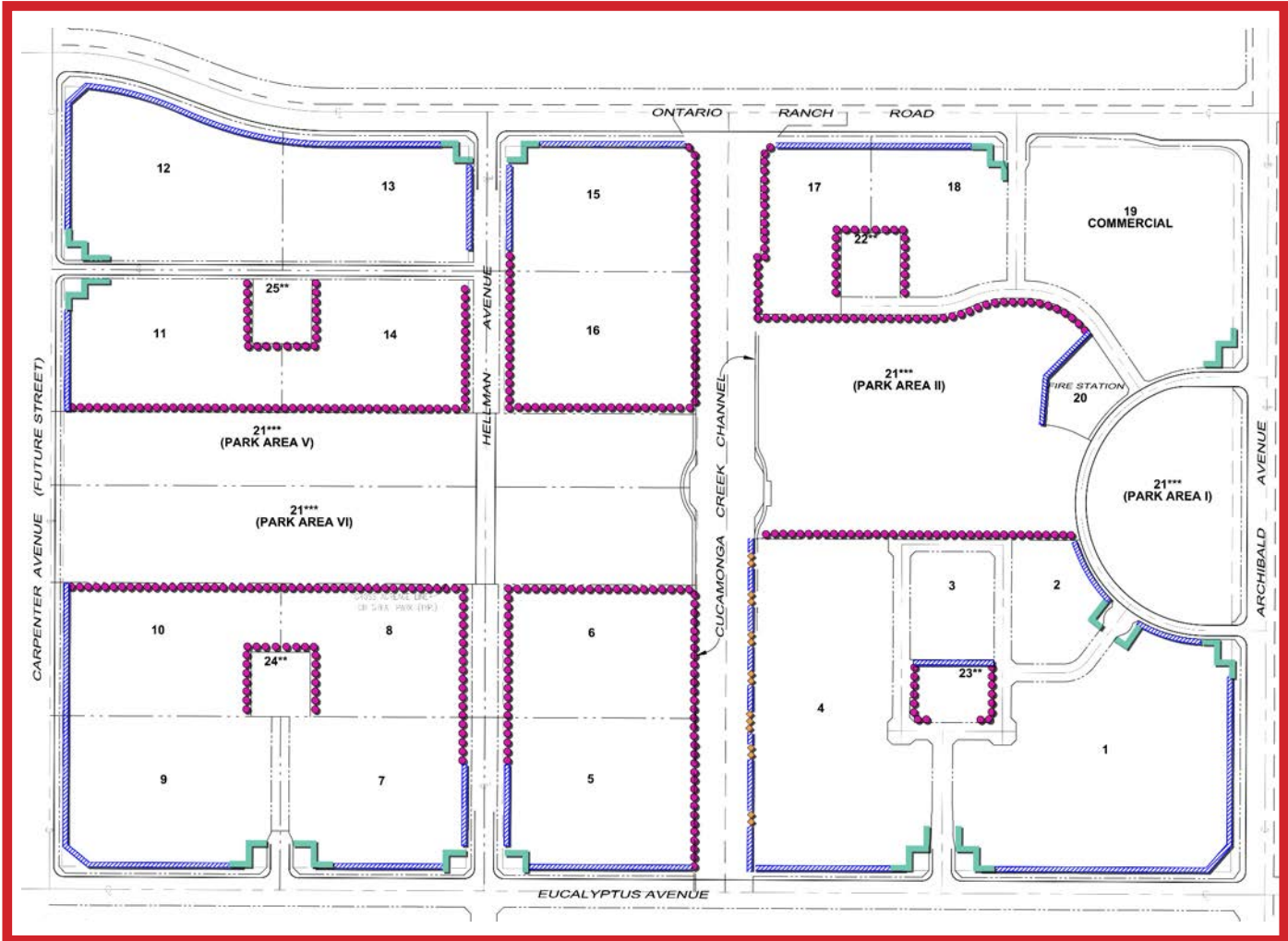
Pocket parks, if any, will consist of the following:

Parking—On-street parallel parking will be provided along local streets;

Open turf—Open turf, with bench seating, will be provided throughout the pocket parks. Lighting will be included;



Parkside



LEGEND





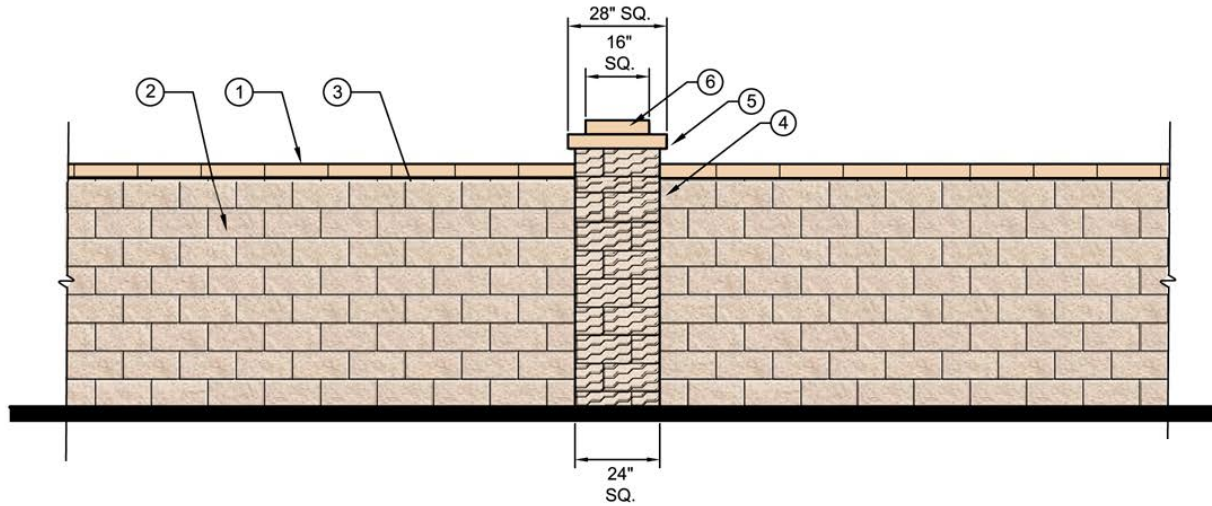
-  SPLIT FACE WALL
-  DECORATIVE ENTRY WALL
-  TUBULAR STEEL VIEW FENCE
-  TUBULAR STEEL VIEW FENCE OR COMBO BLOCK WALL W/ TUBULAR STEEL FENCE

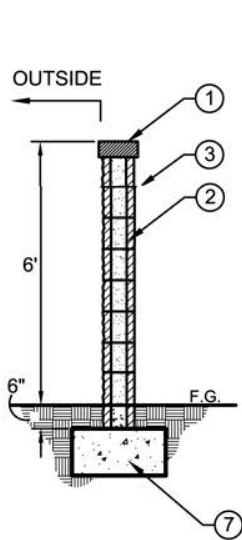
Exhibit 7-26: Fence/Wall Plan



Parkside



COMMUNITY WALL: 6x8x16 SPLIT FACE
 (OUTSIDE FACING ONLY)
 COLOR: "CANYON BLUFF"
 FROM: ANGELUS BLOCK OR EQUAL CAP: 4" HIGH
 PRECISION CAP BY ANGELUS BLOCK OR EQUAL.
 COLOR TO BE: "HARVEST"



- ① 8X4X16 CMU PRECISION CAP BY ANGELUS BLOCK
- ② 6x8x16 SPLIT FACED BLOCK (OUTSIDE FACE ONLY) BY ANGELUS BLOCK
- ③ 8X4X16 CMU PRECISION BLOCK BY ANGELUS BLOCK.
- ④ PILASTER TO BE CONSTRUCTED FROM SPLIT FACE BLOCK, RIVER ROCK, LEDGE STONE, FLAG STONE, BRICK OR OTHER DECORATIVE MATERIAL APPROVED BY THE CITY. LOCATE AT CORNERS & MAX 150' O.C.
- ⑤ 4" HIGH PRE-CAST CAP SMOOTH FINISH, CUT TO FORM 28" SQ.
- ⑥ 4" HIGH x 16" SQ. PRE-CAST CAP SMOOTH FINISH
- ⑦ CONCRETE FOOTING

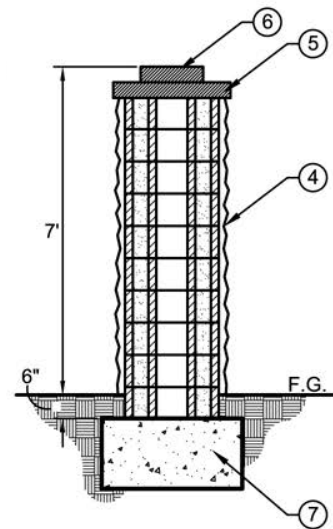
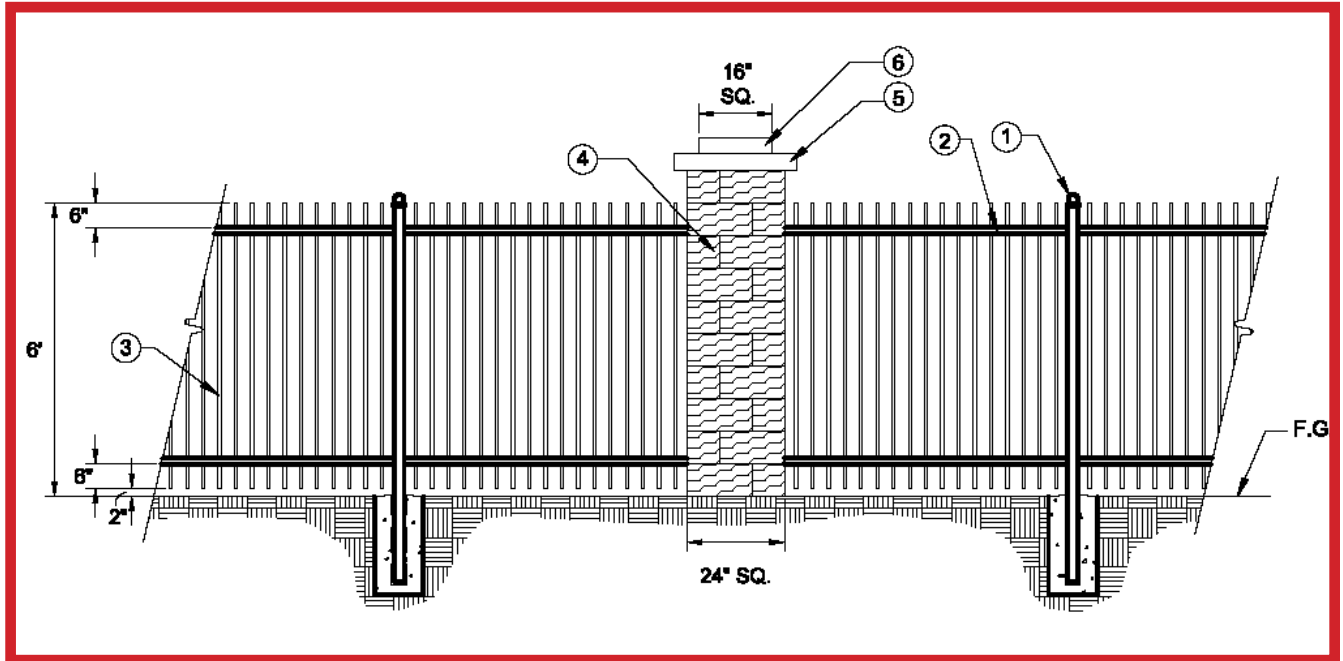


Exhibit 7-27: Wall Detail



Parkside



- | | |
|---|---|
| <ul style="list-style-type: none"> ① 2 1/2" SQ. TUBULAR STEEL POST W/DECORATIVE CAP ② 1 1/2" X 1" 12 GA. TUBULAR STEEL TOP & BOTTOM RAILS LAY FLAT ③ 1/2" SQ. X 16 GA. TUBULAR STEEL FENCE PICKETS @4" O.C. MAX. | <ul style="list-style-type: none"> ④ PILASTER TO BE CONSTRUCTED FROM 8X8X16 COLORED SPLIT FACE BLOCK. ⑤ 4" HIGH PRE-CAST CAP SMOOTH FINISH, CUT TO FORM 28" SQ. ⑥ 4" HIGH X 16" SQ. PRE-CAST CAP SMOOTH FINISH |
|---|---|

*FENCE TO BE "GENESIS" BY AMERISTAR OR EQUAL.

Exhibit 7-28: View Fence Detail



Parkside

① 8X4X16 CMU PRECISION CAP BY ANGELUS BLOCK

② 6x8x16 SPLIT FACED BLOCK (OUTSIDE FACE ONLY) BY ANGELUS BLOCK

③ PILASTER TO BE CONSTRUCTED FROM SPLIT FACE BLOCK, RIVER ROCK, LEDGE STONE, FLAG STONE, BRICK OR OTHER DECORATIVE MATERIAL APPROVED BY THE CITY. LOCATE AT CORNERS & MAX 150' O.C.

④ 4" HIGH PRE-CAST CAP SMOOTH FINISH, CUT TO FORM 28" SQ.

⑤ 4" HIGH x 16" SQ. PRE-CAST CAP SMOOTH FINISH

⑥ CONCRETE FOOTING

COMMUNITY WALL: 6x8x16 SPLIT FACE (OUTSIDE FACING ONLY)
 COLOR: "CANYON BLUFF"
 FROM: ANGELUS BLOCK OR EQUAL CAP: 4" HIGH PRECISION CAP BY ANGELUS BLOCK OR EQUAL.
 COLOR TO BE: "HARVEST"

① 2X7 TOP & BOTTOM RAIL

② POST CAP

③ 4X4 VINYL POST

④ 1X6 VINYL PANEL

NOTE:
 PRIVACY VINYL FENCE IS ONLY PERMITTED WITHIN THE INTERIOR WALLS/FENCES FOR PLANNING AREAS 1-4

Exhibit 7-29: Combo Wall Detail

Exhibit 7-30: Privacy Vinyl Wall Detail



Parkside

Focal Points—Pocket parks may include focal point elements, such as tot lots, fountains, or small gazebos, depending on the size of the tot lot and orientation in the neighborhood:

- Pocket Park tot lots shall contain ~~two~~ either a play ~~areas-constructed-one~~ area for younger children (2-5 years old) ~~and-one~~ or for older children (5-12 years old).

Refer to Exhibit 7-19: Pocket Park Concept A, Exhibit 7-21: Pocket Park Concept B and Exhibit 7-20: Pocket Park A Rendering for detailed conceptual illustrations of proposed Pocket Parks in Parkside.

7.7.8 Recreation Area Plan

The Recreation Area Plan for Parkside includes locations for private recreation facilities in local neighborhoods. These recreation areas may include the following:

Parking—Parking adequate to accommodate daily use of the recreation areas;

Recreation Building—A recreation building should adhere to the architectural design guidelines and should be designed to accommodate the community needs;

Pool and Spa Facility—Each neighborhood may have access to a pool (sized adequately to accommodate the needs of the neighborhood) as well as to serve as a summer leisure recreational amenity for adults and children of the community;

- The pool decking area should be sized to allow for lounge chairs and tables. A spa area separate from the pool may also be located within the recreation area. A group barbecue facility and shade structure

may be located within the pool deck area. The private recreational facilities shall be fenced and gated for resident entry only. Hours of operation will be enforced in an effort to avoid evening disturbance to adjacent neighborhoods;

Refer to Exhibit 7-22: Recreation Area Concept A and Exhibit 7-23: Recreation Area Concept B for detailed conceptual illustrations of proposed Recreation Areas in Parkside.

Landscaping —Landscaping within the Private recreation areas shall harmonize with the surrounding streetscapes and adjacent Great Park. Large specimen trees should be used within the open turf areas to help provide shade and screening of unwanted views. Accent trees should also be used at pedestrian entries and around the pool area for color and seasonal interest;

- A strong **visual** connection shall be created between the private recreation area and the ~~terminus of one of the four wings of the public Great Park. This connection will be in the form of sight lines, maintaining vistas surrounding neighborhoods; the mutual use of repetitive shade structures;~~ and

Paseo walkway System—A paseo walkway system will be designed to provide connections to adjacent neighborhoods, the Great Park, and commercial area.

Refer to Exhibit 7-19: Recreation Area Concept A, and Exhibit 7-21: Recreation Area Concept B.



Parkside

7.7.9 Paseo Trail System

Pedestrian circulation is highly encouraged within the Parkside Specific Plan area. Landscape easements are provided along major roadways and are encouraged within the neighborhood communities.

Paseo Trail System—A Paseo walkway system will be designed to provide connections to adjacent neighborhoods, the recreation areas, the Great Park and commercial area. The overall trail system will include:

Landscaping—Landscaping along the trails shall harmonize with the surrounding streetscapes;

Paving—Meandering sidewalks are encouraged with benches located approximately every 500 lineal feet and enhanced paving at paseo connections where pedestrian circulation crosses roadways;

Other Elements—Other elements include:

- Light poles shall be located at every 100 feet along the path;
- Entry points shall have adequate directional signage;
- Provide formal pickup/drop off areas at major entry points;
- ~~Integration of well defined pedestrian connections to the “Great Park.” Refer to Exhibit 38, Trail Connection to “Great Park”;~~
- Integration of the Channel Regional Trails Plan. Refer to the detailed conceptual illustrations Exhibit 7-25: Channel Regional Trail Easement Plan for illustration;
- Integration of well- defined pedestrian connections to the Great Park. Refer to Exhibit 7-24: Trail Connection To Great Park; and

- Connection to recreation areas. Refer to the detailed conceptual illustrations. Refer to Exhibit 7-25: Interior Paseo Concept.

7.7.10 Community Walls and Fencing

Walls are a major component in achieving an overall community theme within the Parkside Specific Plan. A strong, cohesive appearance is achieved through the use of “community walls” and general overall wall guidelines.

All walls that adjoin community streetscenes and are along the boundary edges of the Great Park and facing the Cucamonga Creek Channel shall be deemed “community walls.” Non-climbing fencing shall be used around the Cucamonga Creek Channel to prevent children from climbing the fence to gain access. All wall and fencing designs and layout shall be approved by the City of Ontario prior to construction.

Perimeter Walls and Fencing

Where common or private areas interface with perimeter streets, public open space and/or residential lots, a solid decorative theme wall, a tubular steel view fence with matching themed pilasters, and/or a combo wall shall be constructed. Brick Masonry walls or screening walls shall have climbing plants or anti-graffiti elements to prevent vandalism. Refer to Exhibit 7-26: Fence/Wall Plan and Exhibit 7-29: Combo Wall Detail.

For front yards facing the Great Park the same themed block or view fence shall be used between the structures buildings to secure the project behind. These wall/fences shall be located behind the building setback and arranged to create a project-wide unifying appearance.



Parkside

Interior Solid Walls and Fencing

Where privacy or protection of in-tract common and private area views dictates, walls shall either be tubular steel construction or decorative masonry block (~~block sides~~). Reverse frontage walls (both sides) and any wall return that is visible from the street shall be constructed of split-face block or precision block that is plastered or stuccoed ~~and of with~~ a color that blends with the Parkside color palette. Decorative caps and the use of decorative themed pilasters to help enhance the perimeter appeal of the walls are encouraged. Walls visible from the community streets ~~may shall~~ not be ~~made of~~ exposed or painted precision block ~~or slumped block~~. Refer to Exhibit 7-26: Fence/Wall Plan and Exhibit 7-27: Wall Detail.

View Fencing

These fences allow open views but not physical access; they shall be a ~~minimum of~~ 5 feet high and made of tubular steel construction. Areas where view fencing occurs will be subject to review by the City ~~of Ontario~~. Refer to Exhibit 7-26: Fence/Wall Plan and Exhibit 7-28: View Fence Detail.

Privacy Fencing ~~in Cul-De-Sacs~~

~~Where perimeter walls about a cul-de-sac at the perimeter of a tract, 40 to 60 feet of view fencing will be installed to allow emergency personnel to see within the cul-de-sac area from the street. These areas will also be enhanced with landscaping.~~

These fences provide privacy between homes within the neighborhoods. These fences will be 5 feet- 6 inches tall with a post and cap design. These fences will be vinyl and the posts will be a maximum of 8 feet on center.

This fencing is permitted for all housing types in non-publicly visible locations. Refer to Exhibit 7-30: Privacy Vinyl Wall Detail

7.7.11 Outdoor Lighting

Lighting standards within the ~~project site~~ Specific Plan ~~area~~ shall be consistent in style, color, and materials in order to maintain uniformity throughout. Lighting should be subtle, providing a soft wash of light over illuminated objects such as monumentation. A hierarchy shall be established by using a variety of lighting fixtures and illumination levels based off of lighting design intent. Lighting styles shall tie into architectural styles and provide sufficient illumination for the safety and well-being of the community.

Specimen trees may be uplighted with several fixtures into the canopy to avoid creating dark sides of the trees. Providing adequate lighting within the vehicular turn-around areas and lanes (private alleys) is essential. ~~Metal halide lighting to project true colors and clear visibility, and reflective street signs shall be used on all proposed streets~~. Lighting shall have anti-vandalism fixtures.

Each residential unit shall have illuminated addresses at the front door and the garage door, if the garage faces an alley.

7.7.12 Mailboxes

~~Mailbox units may be designed with features to complement the architectural style of the neighborhood with trellises, stone bases, and landscaping. The adjacent space shall be designed to allow frequent pedestrian use and to function as an important neighborhood gathering space. The developer(s)~~



Parkside

Entry Monuments

Avoid intensely bright or “hot” lighting of monuments; rather, each should be lit to provide a soft wash of light across the monument signage. Specimen trees should be uplighted with several fixtures into the canopy to avoid creating dark sides of the trees.

Recreation Center

Uplighting of specimen trees is very important and providing adequate lighting within the vehicular turn-around areas. Recreation center monumentation/kiosk should also be illuminated.

Community Park/Linear Park/Paseo Lighting

Lighting of the walkways, tot lot, and parking area within the Parks and Paseo Trail System should be considered for safety and security. Utilization of a traditional Globe/Acorn Post mounted light fixture should be considered for both the park open space areas as well as interior street lighting fixtures. Keeping with the character of traditional materials, this will help to create a better sense of scale to the pedestrian.

7.7.12 Mailboxes

The developers shall work with the United States Post Office to determine the best possible location for mailboxes within the residential subdivisions. The developer(s) will determine the specifications for the mailbox units.

7.8 Landscape Implementation

7.8.1 Improvement Responsibility Landscape Maintenance

Landscape Maintenance

- All public landscapes, including slopes, street trees, lighting, and irrigation systems, shall be maintained by the City of Ontario Landscape Maintenance District;
- Front yard landscapes shall be installed by the developer, however and maintained by the homeowner shall maintain the front yard landscape in a healthy condition at all times; and
- ~~All manufactured slopes which exceed three (3) feet in height shall be planted by the developer with an effective mixture of groundcover, shrubs, and trees. Such slopes shall also be irrigated as necessary to ensure germination and establishment in conformance with the fuel modification guidelines described later in this document.~~
- All manufactured and cut/fill slopes which exceed 3 feet in height shall be planted by the developer with an effective mixture of groundcover, shrubs, and trees. Such slopes shall also be irrigated as necessary to ensure germination and establishment in conformance with the fuel modification guidelines described later in this document.



Parkside

7.8.2 Homebuilder Landscape Requirements

Public Landscapes (HOA and City Maintained Areas)

- Landscape plantings in public areas should reflect a commitment to both developing a “sense of place” and maintaining harmony with [the New Model Colony Ontario Ranch](#);
- A landscape architect licensed in California shall be retained to prepare planting and irrigation plans for all public areas. Arrangement of plants should incorporate the concepts of mass planting; plants should be placed to allow them to grow to their natural sizes and forms, and sheared hedges should be kept to a minimum;
- Minimum sizes of trees shall conform to [Table 7-15: On-Site Tree Size](#); and
- All [public areas, including HOA designated areas pathways](#), shall be [installed by the developer designed to be barrier free and meet ADA guidelines](#).

The plant list at the end of this section offers a suggested plant palette for Parkside; while it is by no means all-inclusive, plantings in public areas should draw primarily from this palette for visual community continuity.

Table 7-14: On-Site Tree Size

Trees	Percentage on-site
15 Gallon	60%
24" Box	25%
36" Box	10%
48" Box	5%

Front Yard Landscapes

Plantings in front yards may vary substantially from the Specific Plan palette, but should retain some of the character and style of the public plantings. No more than [70-25](#) percent of the total square footage of any front yard shall be lawn; the balance shall be composed of shrubs and groundcovers, with an emphasis on drought tolerant plant species. [Each front yard shall be planted with one 24-gallon tree \(minimum\). Installation of landscaping within the front yards of single family detached housing products will be provided by the home builder. Front yard landscaping shall be installed prior to Certificate of Occupancy.](#)

Soil Testing

Soil samples shall be taken from several locations after the completion of rough grading operations, and a reputable soil-testing laboratory shall perform an agronomic soils test. The test shall assess soil fertility needs for water-wise California native and Mediterranean plant types. No planting shall take place until the soil has been properly prepared based on the recommendations of the soils testing laboratory.

Slope Landscaping (HOA)

All manufactured and cut/fill slopes which exceed 3 feet in height [\(if they occur\)](#) shall be planted with an effective mixture of groundcover, shrubs, and trees. [These slopes shall be installed before the Certificates of Occupancy.](#) Such slopes shall also be irrigated as necessary to ensure germination and establishment in conformance with the fuel modification guidelines.



Parkside

Private Interior Slopes: Residential Interior

- **Private interior** slopes may be more ornamental in character than exterior slopes. They may have a somewhat broader range of plant materials than exterior slopes, but should still be chosen primarily from the plant palette and are subject to the same fuel modification restrictions; **and**
- **All manufactured and cut/fill slopes which exceed 3 feet in height shall be planted by the developer with an effective mixture of groundcover, shrubs, and trees. Such slopes shall also be irrigated as necessary to ensure germination and establishment in conformance with the fuel modification guidelines described later in this document. ~~All manufactured slopes which exceed three~~ All slopes shall be landscaped in accordance with the City standards.**

Streetscape Landscaping

Streetscape Development Standards

- Turf grass shall not exceed 30 percent of streetscape planting and shall be located adjacent to the sidewalk or curb line;
- All new plantings within the planned community shall draw substantially from Table 7-16—Community Plant Palette;
- All streetscape landscaping within the Parkside planned community will be implemented by the Developer in accordance with this Specific Plan;

- **A balance of evergreen and deciduous trees shall be considered during the planning process for the selection of street trees and understory trees between them (if any). Street trees shall be sized per City standards;**
- The Developer shall install all primary and secondary improvements concurrently with the construction of the roadway on which they front. Neighborhood intersections shall be constructed as each neighborhood street is built;
- The Developer shall provide site inspection of all construction and installation of entries and intersections in accordance with City **of Ontario** requirements; **Self-attached vines are to be added against block walls;**
- **Along community walls, the use of 5-gallon plant material for hedgegrows is preferred; and**
- **Closely spaced plant materials (to horticultural practices) are expected on this project.**

Irrigation Design

Irrigation for both public and private landscapes should be designed to be as water-efficient as possible. All irrigation systems shall **have automatic controllers designed to properly water plant materials given the site's soil conditions, and irrigation systems for all public landscapes shall have automatic rain shut-off devices. Drip irrigation is encouraged. Spray systems shall have low-gallonage, matched-precipitation heads. Separate irrigation systems with bubblers or spray heads are required for trees and palms** comply with Subsection 6.05.035C Irrigation



Parkside

Requirements of the Ontario Development Code. All City maintained landscapes (CFD areas) are to be controlled with a central controlled system as approved by the City's Parks and Maintenance Department, and trees plantings are to consist of root barriers and a pop up stream bubbler system on a separate valve. Irrigation design shall meet the requirements of the Landscape Development Standards.

Addresses

Each single family home will be required to have the address numbers painted on the curb with black numerals on a white reflective paint background. Each homeowner will be responsible for the maintenance of these numbers. ~~Huminated addresses shall also be installed on the residential units as required by the City of Ontario Fire Department.~~

7.9 Design Guideline Implementation

7.9.1 Design Review Submittal Requirements

The master developer shall review all plans prior to submittal to the City for Plan Check. This design review process will include these steps: conceptual review, refined review and construction document review prior to plans being submitted for City processing. The design review process will ensure the vision of Parkside will be implemented.



Parkside

Table 7-15: Community Plant Palette

Note: The landscape materials (trees, shrubs, vines, and groundcovers) will be provided per the Streetscape Master Plan.v

BOTANICAL NAME	COMMON NAME												
		Archibald Avenue	Eucalyptus Avenue	Ontario Ranch Road	Hellman Avenue	Carpenter Avenue	Cucamonga Creek Channel	Primary Street / Park Loop Street	Interior Neighborhood Streets	Lanes (Private Alleys)	Community Gateway Monuments & Entries	Secondary Community Entries	Interior Paseo & Pocket Parks
Shrubs													
Acacia redolens	Creeping Acacia						•						
Agapanthus africanus	Lily-of-the-Nile				•					•			•
Agave attenuata	Foxtail Agave										•	•	•
Agave desmettiana 'Variegata'	Variegated Dwarf Century Plant												•
Agave vilmoriniana	Octopus Agave										•	•	
Agave weberi	Weber's Century Plant										•	•	
Aloe species	Aloe										•		•
Alyogene huegelii	Blue Hibiscus					•				•			
Arctostaphylos species	Manzanita						•						
Baccharis pilularis	Coyote Brush							•					
Buxus m. japonica	Japanese Boxwood										•	•	•
Callistemon viminalis 'LittleJohn'	Dwarf Bottlebrush							•	•				
Carex Specs	Sedge							•	•	•	•	•	•
Carissa grandiflora 'Green Carpet'	Natal Plum							•	•				
Ceanothus species	California Lilac							•					
Chondropetalum tectorum	Cape Rush								•				•
Cistus purpureus	Rockrose					•	•						
Cistus salvifolius 'Prostratus'	Sageleaf Rockrose				•								
Cotonesaster species	Cotoneaster							•			•		•
Dianella revoluta 'DR5000'	Little Rev Flax Lily							•	•	•	•	•	•
Dianella tasmanica 'Silver Streak'	Silver Streak Flax Lily							•	•	•	•	•	•
Dietes bicolor	Fortnight Lily							•		•	•	•	•
Echium fastuosum	Pride of Madiera												•
Eleagnus pungens	Silverberry							•					•
Eschscholzia californica	California Poppy							•					
Euryops pectinatus	Grey-Leafed Euryops												•
Fremontodendron californicum	Flannel Bush							•					



Parkside

Table 7-15: Community Plant Palette

Note: The landscape materials (trees, shrubs, vines, and groundcovers) will be provided per the Streetscape Master Plan.v

BOTANICAL NAME	COMMON NAME	Archibald Avenue	Eucahyptus Avenue	Ontario Ranch Road	Hellman Avenue	Carpenter Avenue	Cucamonga Creek Channel	Primary Street / Park Loop Street	Interior Neighborhood Streets	Lanes (Private Alleys)	Community Gateway Monuments & Entries	Secondary Community Entries	Interior Paseo & Pocket Parks	Commerical Area
Shrubs														
Gardenia jasminoides	Gardenia												•	•
Grevillea species	N.C.N				•									•
Hemerocallis species	Day Lily									•	•	•	•	
Heteromeles arbutifolia	Toyon						•							
Hibiscus rosa-sinensis	Golden Dust Hibiscus													
Ilex species	Holly										•			
Juniperus chinensis & CVS	Juniper									•				
Kniphofia uvaria	Red-Hot Poker												•	•
Lantana montevidensis	Lantana										•	•	•	
Lavandula species	Lavendar										•	•	•	•
Leptospermum scoparium 'Snow White'	New Zealand Tea Tree												•	
Leymus condensatus 'Canyon Prince'	Canyon Prince Wild Rye							•	•	•	•	•	•	•
Ligustrum japonicum 'Texanum'	Glossy Privet									•	•	•	•	•
Limonium perezii	Statice						•							
Liriope species	Lily Turf												•	
Muhlenbergia rigens	Deer Grass						•							•
Myroporum parvifolium	N.C.N.							•	•		•			•
Myrtus communis 'Compacta'	Dwarf Myrtle											•	•	
Pittosporum tobira & CVS	Mock Orange							•	•		•	•	•	•
Pittosporum tobira variegated	Tobira Variegated									•				•
Plumbago auriculata	Cape Plumbago												•	
Prunus caroliniana 'Bright & Tight'	Carolina Cherry	•												
Pyracantha species	Fire Thorn							•	•		•	•		
Rhaphiolepis indica	India Hawthorne		•					•	•		•	•	•	•
Rhus integrifolia	Lemonade Berry									•				•
Rosa species	Rose		•	•							•	•		
Rosmarinus officianalis	Rosemary							•	•		•	•	•	•



Parkside

Table 7-15: Community Plant Palette

Note: The landscape materials (trees, shrubs, vines, and groundcovers) will be provided per the Streetscape Master Plan.v

BOTANICAL NAME	COMMON NAME													
		Archibald Avenue	Eucalyptus Avenue	Ontario Ranch Road	Hellman Avenue	Carpenter Avenue	Cucamonga Creek Channel	Primary Street / Park Loop Street	Interior Neighborhood Streets	Lanes (Private Alleys)	Community Gateway Monuments & Entries	Secondary Community Entries	Interior Paseo & Pocket Parks	Commercial Area
Shrubs														
Salvia clevelandii	Cleveland Sage													•
Salvia species	Sage													•
Senecio mandraliscae	Blue Chalk Sticks							•						•
Strelizia reginae	Bird-of-Paradise													• •
Tecoma stans 'Sierra Apricot'	Sierra Apricot						•	•						•
Trachelospermum jasminoides	Star Jasmine									•				• •
Viburnum plicatum	Japanese Snowball			•										
Viburnum tinus	Laurustinus Viburnum			•										• •
Westringia fruticosa	Westringia													•
Xylosma congestum	Shiny Xylosma							•	•		•	•		
Yucca recurvifolia	Pendulous Yucca										•	•		

Note: Landscape slopes shall not be greater than 3:1.



Parkside

Table 7-15: Community Plant Palette

Note: The landscape materials (trees, shrubs, vines, and groundcovers) will be provided per the Streetscape Master Plan.v

BOTANICAL NAME	COMMON NAME													
		Archibald Avenue	Eucahyptus Avenue	Ontario Ranch Road	Hellman Avenue	Carpenter Avenue	Cucamonga Creek Channel	Primary Street / Park Loop Street	Interior Neighborhood Streets	Lanes (Private Alleys)	Community Gateway Monuments & Entries	Secondary Community Entries	Interior Paseo & Pocket Parks	Commercial Area
Trees														
<i>Albizia julibrissin</i> 'Rosa'	Silk Tree										•			
<i>Alnus rhombifolia</i>	White Alder							•						
<i>Bauhinia purpurea</i>	Orchid Tree										•	•	•	•
<i>Cedrus deodara</i>	Deodar Cedar											•		
<i>Celtis sinensis</i>	Chinese Hackberry				•									
<i>Cercidium hybrid</i> 'Desert Museum'	Desert Museum Palo Verde							•	•					
<i>Cercis occidentalis</i>	Western Redbud				•									
<i>Chitalpa tashkentensis</i>	Chitalpa Tree							•		•			•	
<i>Cinnamomum camphora</i>	Camphor Tree			•				•						
<i>Citrus sinensis</i>	Orange Tree										•		•	
<i>Cupaniopsis anacardioides</i>	Carrotwood									•				•
<i>Cupressus sempervirens</i>	Italian Cypress										•	•	•	
<i>Eucalyptus sideroxyylon</i> 'Rosea'	Pink Iron Bark							•						
<i>Fraxinus angustifolis</i>	Raywood Ash	•												
<i>Geijera parvifolia</i>	Australian Willow										•			
<i>Gleditsia t. inermis</i> 'Shademaster'	Honey Locust									•			•	
<i>Heteromeles arbutifolia</i>	Toyon							•	•				•	
<i>Jacaranda mimosifolia</i>	Jacaranda											•	•	
<i>Koelreuteria bipinnata</i>	Chinese Flame Tree								•				•	
<i>Lagerstroemia farnesia</i>	Crape Myrtle									•	•		•	•
<i>Lagerstroemia indica</i> 'Nanchez'	White Crape Myrtle			•										
<i>Lagerstroemia x</i> 'Muskogee'	Muskogee Crape Myrtle	•												
<i>Magnolia grandiflora</i> 'Little Gem'	Southern Magnolia											•	•	
<i>Magnolia grandiflora</i> 'Magestic Beauty'	Magesitc Beautify Magnolia		•											
<i>Magnolia grandiflora</i> 'Russet'	Southern Magnolia									•			•	•
<i>Melaleuca quinquenervia</i>	Cajeput Tree												•	
<i>Michelia champaca</i>	Michelia												•	



Parkside

Table 7-15: Community Plant Palette

Note: The landscape materials (trees, shrubs, vines, and groundcovers) will be provided per the Streetscape Master Plan.v

BOTANICAL NAME	COMMON NAME	Archibald Avenue	Eucalyptus Avenue	Ontario Ranch Road	Hellman Avenue	Carpenter Avenue	Cucamonga Creek Channel	Primary Street / Park Loop Street	Interior Neighborhood Streets	Lanes (Private Alleys)	Community Gateway Monuments & Entries	Secondary Community Entries	Interior Paseo & Pocket Parks	Commercial Area
		Trees												
<i>Olea europaea</i>	Olive										•	•		•
<i>Phoenix dactylifera</i>	Date Palm										•	•		•
<i>Pinus canariensis</i>	Canary Island Pine							•					•	•
<i>Pinus eldarica</i>	Afghan Pine	•		•	•						•	•	•	•
<i>Pinus pinea</i>	Italian Stone Pine	•									•			
<i>Pistacia Chinensis</i>	Chinese Pistache		•											
<i>Pittosporum undulatum</i>	Vicorian Box								•				•	
<i>Platanus acerfolia</i> 'Bloodgood'	London Plane Tree								•				•	
<i>Podocarpus gracilior</i>	Fern Podocarpus										•	•	•	•
<i>Prosopis glandulosa</i> 'Thornless'	Thornless Honey Mesquite								•				•	
<i>Pyrus calleryana</i> 'Chanticleer'	Chanticleer Pear								•	•			•	
<i>Pyrus kawakamii</i>	Evergreen Pear								•					•
<i>Quercus agrifolia</i>	Coast Live Oak							•			•			
<i>Quercus douglasii</i>	Blue Oak					•								
<i>Quercus ilex</i>	Holly Oak								•					•
<i>Raphiolepis</i> 'Majestic Beauty'	N.C.N.									•				
<i>Schinus molle</i>	California Pepper Tree							•					•	
<i>Syagrus romanzoffianum</i>	Queen Palm									•				
<i>Tabebuia impetiginosa</i>	Pink Trumpet Tree									•			•	
<i>Tipuana tipu</i>	Tipu Tree							•						
<i>Tristania conferta</i>	Brisbane Box								•				•	•
<i>Ulmus parvifolia</i> 'Ture Green'	Evergreen Elm							•	•					•



Parkside

Table 7-15: Community Plant Palette

Note: The landscape materials (trees, shrubs, vines, and groundcovers) will be provided per the Streetscape Master Plan.v

BOTANICAL NAME	COMMON NAME	Archibald Avenue	Eucalyptus Avenue	Edison Avenue	Hellman Avenue	Carpenter Avenue	Cucamonga Creek Channel	Primary Street / Park Loop Street	Interior Neighborhood Streets	Lanes (Private Alleys)	Community Gateway Monuments & Entries	Primary & Secondary Community Entries	Neighborhood Entries	Interior Paseo & Pocket Parks	Commercial Area
Vines															
<i>Clematis ligusticifolia</i>	Clematis	•													•
<i>Clytostoma callistegiodes</i>	Violet Trumpet Vine		•												
<i>Distictis buccinatoria</i>	Blood Red Trumpet Vine			•											
<i>Ficus repens</i>	Creeping Fig						•	•	•						
<i>Gelsemium sempervirens</i>	Carolina Jessamine												•	•	
<i>Jasminum polynthum</i>	Pink Jasmine													•	
<i>Macfadyana unguis-cati</i>	Cat's Claw						•								
<i>Pandorea jasminoides</i>	Bower Vine														
<i>Parthenocissus tricuspidata</i>	Boston Ivy	•	•	•	•	•		•	•	•					
<i>Passiflora species</i>	Passion Flower							•	•					•	
<i>Rosa banksiae</i>	Lady Banks Rose							•	•					•	
<i>Solanum jasminoides</i>	Potato Vine													•	
<i>Wisteria sinensis</i>	Chinese Wisteria												•	•	

Chapter 8: Commercial Design Guidelines



New Orleans, Louisiana: City Park



Parkside

8.0 Commercial Design Guidelines

8.1 Introduction

The Parkside Specific Plan area will include a commercial area in the northeastern portion of the project site, adjacent to Edison Ontario Ranch Rd. and Archibald Ave. These non-residential uses will provide the residents of Parkside with shopping conveniently located nearby. Residents will be able have the ability to access services by foot or ride their bicycles to shop via a pedestrian trail connecting the residential uses proposed in Parkside to at the commercial center.

The commercial site is included in Planning Area 21, amounting to 11.3 net acres.

Throughout the commercial development, appropriate landscaping, site planning, and architectural design should be considered to create friendly and welcoming places to shop. These non-residential uses will continue the vision established in the residential community, including integrating pedestrian-friendly design.

In order to maintain the character of Parkside's residential community, the commercial area is expected to be developed with a compatible and harmonious quality. The Parkside Commercial Design Guidelines will help guide the development of the area to enhance the pedestrian experience and innovative design that contributes to the character of the entire Parkside Specific Plan area. These guidelines are not intended to be strictly enforced rules, but rather as a tool to aid the design process. Additionally, these guidelines have been generalized for the Parkside Specific Plan. Potential developers will be required to submit their own supplemental design guidelines for the Commercial development area to the City of Ontario for approval.

The Commercial Design Guidelines Section is organized into the following components:

- Site Planning;
- Building Design;
- Architectural Details; and
- Open Space and Landscaping

8.1.1 Guiding Principles

Guiding Principles for commercial design for Parkside include:

Buildings should contribute to the qualitative nature of the overall neighborhood.

- Proper scale and proportions in massing and details should be used;
- Pedestrian open spaces should be implemented;
- Massing and articulation should be varied;
- Buildings should be designed at a human-scale; and
- Parking lots should be de-emphasized as much as possible.

Buildings should have an enduring quality.

- Design should draw inspiration from the rich and diverse Southern California architectural heritage;
- Materials and colors typical to Southern California should be used;
- Details and ornamentation should be authentic, as much as possible; and
- Design should create an inviting place to shop.



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8.1.2 Review Process

These Commercial Design Guidelines are to be used as a general tool to guide the development of the northeastern portion of Parkside. Final designs will be subject to ~~the City of Ontario's~~ approval. Additionally, more detailed Design Guidelines for each developer may be required by the City prior to final approval. Refer to the ~~City of Ontario~~ Development Code for further information regarding the review process.

8.2 Commercial Design Guidelines

8.2.1 Introduction

Commercial development within Parkside will continue the pedestrian-friendly character established in the residential community. By reducing the impact of the automobile on the appearance of the commercial development, appropriate site planning can de-emphasize the vehicle, creating a pleasant outdoor environment without ignoring the necessary needs of vehicular traffic. Through well executed planning, architecture, and landscape design, the commercial uses within Parkside will complement and, to some extent, integrate the residential community ~~rather than turn its back completely to the Parkside community residents.~~

8.2.2 Site Planning

Orientation

Buildings should be oriented for best visibility from the surrounding streets. Windows and entries should face the streets, avoiding blank walls which could dominate public views. Stores with entries not visible from the street may be oriented towards pedestrian open space.

All primary entries should face onto a street or a connecting internal walkway.

Driveways and Sidewalks

The ~~project~~ site shall be provided with a limited number of driveways to reduce the traffic impact along ~~Ontario Ranch Rd.~~ and Archibald Ave. Pedestrian crossing distances should be minimized at driveways. To increase safety for pedestrians, a raised median as a pedestrian island should be ~~provided~~ ~~considered~~ between travel lanes, especially those entries with more than one lane in any direction. Driveways should be paved with asphalt, concrete, or an enhanced paving material such as brick or concrete block.

Sidewalks should be located along natural travel paths for pedestrians. Sidewalks should be a minimum of 6 feet wide along pedestrian pathways and a minimum of 8 feet in width along more heavily traveled commercial areas.

Street Frontage and Buildings

~~Street side~~ buildings, or those with no parking separating them from the street, should use faux windows and entry features to soften the building's appearance from the street. These buildings should not be more than 100 feet long without a pedestrian plaza or walkway breaking the building mass and connecting the parking lot with the street. On corners, buildings should utilize massing elements to anchor the corner.

Street Side Setbacks and Buildings

The sides of buildings along street edges should be landscaped within the setback to soften the building's appearance to the street, as well as provide additional windows or design elements. Refer to the ~~City of Ontario~~ Development Code for specific setback requirements.



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Street Frontage and Parking Lots

Parking lots should generally be placed away from streets. No street frontages should consist of a continuous parking lot, but should be broken up with buildings, landscaping, plazas, and other pedestrian features. Any parking along street edges should be set back with a landscaped buffer to minimize the dominant feeling of the automobile along the street. Continuous parking stalls may be interrupted by landscaped islands, no more than 10 stalls apart.

Service and Storage Areas

Loading docks, garages, and storage areas are to be located behind or to the side of buildings, siding onto secondary access and not primary streets. These features must be screened with walls and landscaping as much as possible from public view of streets, residences, ~~pedestrians walkways, and connecting trails~~ and [sidewalks](#). Loading docks and storage areas should not conflict with pedestrian walkways.

Refuse Containers, Utility and Mechanical Equipment

Refuse containers and equipment should be easily accessed by service vehicles but screened from view of the streets, parking lots, and connecting walkways through roof forms, walls, and/or landscaping. Screening details should incorporate elements that are compatible with the architectural style of the building. Proper landscaping, including trellises, also may help to screen these elements. Equipment and enclosures shall not be located near pedestrian walkways.

8.2.3 Building Design

Massing

With larger buildings, appropriate massing becomes more important to maintain the human-scale of the development and create a visually pleasing environment. Bay windows, stepped buildings, height changes, and setback variations between stores helps to break up large buildings as well as indicate entries and store locations to customers. Tower elements or monumental features are encouraged at focal points, such as corners, plazas, major entrances, or where walkways meet streets. All elevations of a tower element or [monumental](#) feature shall be completely constructed. In no instance shall a feature of this nature be left unfinished and exposed to public view.

Roof Forms

Hipped and gable roofs are encouraged. Roof pitches should be compatible with the architectural style of the building. Flat roofs should vary in height and be accompanied by cornice, trim, or other accent features.

Entry Design

Building entries should be prominent and easily identified. Various elements can be used to enhance the entry features including massing variation, materials and color change, change in roof form, and awnings. For smaller retail buildings, as part of a cluster or strip, arcades, awnings, and simple signage may be acceptable as entries.

Arcades, Trellises, and Awnings

Outdoor arcades along store fronts are encouraged over pedestrian walkways. Arcades may be used to connect separate buildings, or to provide a more pleasing



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experience for pedestrians. Trellises or awnings may also be used to create a covered walkway to protect pedestrians from the sun and rain.

8.2.4 Architectural Details

Architectural Styles

A consistent architectural style should be used throughout the commercial development in Planning Area 21 to create a sense of continuity between the buildings. Related elements, such as trellises, planters, light-standards, windows, doors, etc. should also adopt detailing that is compatible with the selected architectural style. The selected style of the commercial development does not necessarily need to be of any one of those available for the residential community but should be compatible with them.

Architectural styles should derive or be adapted from Southern California and Ontario's history. ~~Modern and "high-tech" styles are not acceptable style choices.~~ Acceptable styles generally respond to the region's climate including such features as shade windows, outdoor circulation, and outdoor courtyards or plazas, recessed window frames, awnings, arcades, loggias, and trellises.

Building Wall Treatment

No wall should have a blank, uninterrupted length exceeding 20 feet without including one of the following:

- Change in texture;
- Change in material;
- Change in plane;
- Lattice; and
- A tree or equivalent element.

Facades that are visible from adjacent streets or walkways should display even greater visual interest by using architectural elements that break up the massing of large buildings, such as windows, arcades, awnings, porticos, and other architectural features.

Base and Top Treatments

Base and top treatments help to balance the "weight" of the building visually. Bases should appear to "ground" the building, while tops create a defined edge to the roofline. Possible treatment techniques are as follows:

- Base;
- Thicker walls;
- Natural materials;
- Different colored materials; and
- Enriched landscaping with a mature height of at least 18 inches.

Special materials, such as ceramic tile, granite, stone, and marble, are encouraged on the base of buildings that face streets or connecting walkways, especially adjacent to major entries.

- Tops;
- Cornice treatments;
- Roof overhangs with brackets;
- Stepped parapets;
- Textured materials; and
- Different colored materials.

Exterior Materials

Recommended materials include stucco, exterior plaster, brick, wood siding, tile, or stone. Exterior materials that appear pre-fabricated are not recommended. Selected materials and detailing should have an enduring appearance.



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Roof Materials

Roofing materials that are generally acceptable include metal standing seam, asphalt shingles, **concrete** tile, ceramic tile, and slate or slate-like materials.

Colors

Color selection should be consistent with the selected architecture style.

8.2.5 Open Space and Landscaping

Site Accessories

Site accessories, such as recycling bins, bike racks, litter cans, planters, benches, and transit shelters, should **employ** use materials and **provide have** an architectural character consistent with the overall **project development**. These accessories should be graffiti-resistant with materials that are easy to clean and paint.

Connecting Walkways

Walkways throughout **Parkside Specific Plan area** the commercial development should connect the various buildings to each other and to the sidewalks along **Edison Ave-Ontario Ranch Rd.** and Archibald **Ave**. Use of trellises, landscaping, sitting areas, and the site planning of buildings adjacent to one another will encourage pedestrian activity within the commercial area. A typical walkway should be a minimum of 6-feet wide. In large parking lots, pedestrian walkways should be provided within the median of at least one or two parking aisles depending on the size of the commercial center and/or design of the center.

Pedestrian walkways that may also be used for emergency access must allow a minimum clearance of 30-feet **wide**

and 14 **feet** in vertical clearance.

Off-site Connections

Off-site connections should be provided from the commercial area to Parkside's residential development. Access to the residential community will be available by special pedestrian walkways. **adjacent to the common entry areas**

Plazas

Plazas are encouraged as much as possible to emphasize a pedestrian-friendly environment by creating locations within the commercial development that allow for people to gather. Plazas create a more inviting feeling to pedestrians, allowing them to feel safe from the vehicular dominated parking lot. These outdoor places may include water features, landscaping, and other elements to create a comfortable place to sit. Plazas are encouraged where high levels of pedestrian activity are expected, such as adjacent to major entrances and nearby uses that allow for more interactivity with the plaza such as delis, cafes, restaurants, bakeries, and other food services.

Landscaping

Landscaping helps to soften the feel of the buildings and parking lots while enhancing the visual quality of the site. The following landscaping techniques may be used:

- Provide special landscaping treatment at all project site entries and building entries;
- Provide shade/canopy trees within parking areas;
- Use plants to define outdoor spaces such as edges, outdoor plazas, or movement paths between parking



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and building entrances; and

- Choose plantings that will provide a continuity of form across the entire project site.

Shrubs and tree canopies shall not be higher than 3 feet or lower than 7 feet respectively to allow police officers the ability to patrol the project site without visual obstructions. Plants near low-lying windows shall have thorns as a deterrent for suspects hiding and removing glass panes as a point of entry.

Where a parking lot contains 10 or more parking spaces and is visible from a street, not less than 5 percent of the total area, excluding any perimeter landscaping, shall be landscaped. Such landscaping shall be concentrated in any one location. A minimum of 50 percent of the plant material shall be canopy or shade trees.

All rows of parking spaces shall be provided with landscape islands at each row terminus, minimum 5-foot wide exclusive of curbs, to protect parked vehicles, ensure visibility, confine moving traffic to drive aisles and driveways, and provide within each row of parking spaces, landscaped islands, minimum 5-foot wide, exclusive of curbs, located so as to prevent more than 10 vehicles from being parked side-by-side in an abutting configuration.

Landscaped areas shall be delineated with a concrete curb, minimum 6-inch high by 6-inch wide; except where a landscape area is parallel and adjacent to a parking stall, the curb shall be a minimum 6-inches high by 12-inches wide to provide an area for persons to step when entering or exiting a motor vehicle.

Outdoor Storage Areas

Outdoor storage areas should be incorporated into the design of the project to avoid visual impacts on the project site.

Outdoor storage areas should be located away from the street, behind, or to the side of buildings. Walls and landscaping should be used to screen stored materials.

Transit Shelters

Transit shelters should be incorporated into the design of the commercial project and will be designed according to guidelines provided by the transit agency of Ontario. The commercial developer should explore, with the transit agency and the City, possibilities for a structure building that is integrated architecturally with the project through its color, materials, and architectural style.

Pedestrian walkways should be provided for easy accessibility by pedestrians moving to and from the transit stop to the commercial development.

Fence and Wall Design

Chain-link fencing, barbed wire, corrugated metal fencing, and “tennis windscreens” are not permitted. Fences and walls should be built with attractive, durable materials, including (but not limited to), wrought iron, textured concrete block, brick, or formed concrete with reveals. Fences or walls should be consistent with architectural style, materials, and designs used throughout the project site. Walls and fences should not exceed a height of 6 feet without being made of texture concrete block, interlocking “diamond” blocks, formed concrete with reveals, or similar materials to a maximum of 8 feet



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from grade.

Along streets, walls and fences should not exceed 42-inch in height. Avoid long expanses of uninterrupted fences and walls. Use of an opening, planter box, material change, pilaster, or post is acceptable. Openings shall be provided to connect walkways directly to the street and to allow pedestrians to avoid long, inconvenient walking paths. These pedestrian portals should be announced by pilasters, trellis, special landscaping, or other special features.

Lighting

Lighting standards under 32 feet high should provide adequate illumination throughout the project site during the nighttime hours. Lighting fixtures should be designed to be compatible with the architectural styles selected for the project site. At a minimum, all light standards should have an attractive base and top; overhead “cobra head” standards are not permitted. Buildings and landscaping can be illuminated indirectly to create a strong positive image. Along pedestrian walks, lighting should provide appropriate illumination at a human scale without glare. Lighting within larger parking lots should provide safe visibility. Service area lighting should be positioned to not be seen from public view.

8.2.6 Sign Program

Refer to Section 6.18, Signage.



Parkside



Exhibit 8-1: Retail Site Plan

Chapter 9: General Plan Consistency



Portland, Maine; Deering Oaks Memorial



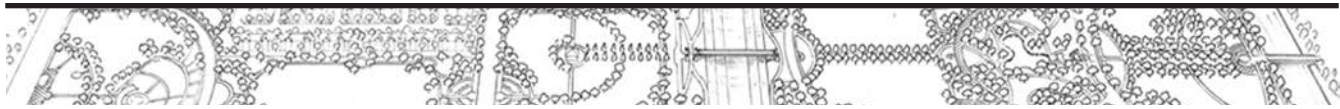
Parkside

9.0 General Plan Consistency

California Government Code (Title 7, Division 1, Chapter 3, Article 8, Section 65450-65457) permits the adoption and administration of specific plans as an implementation tool for elements contained in the local General Plan. Specific plans must demonstrate consistency in regulations, guidelines, and programs with the goals and policies set forth in the General Plan.

The [Subarea 29 Specific Plan \(SP\)](#) has been prepared in conformance with the goals and policies of The Ontario [Plan \(TOP\) Policy Plan](#) (General Plan). The [policy analysis in this Chapter](#) describes the manner in which the [SP](#) complies with [TOP Goals and Polices applicable to the project](#).

~~THIS CHAPTER HAS BEEN FULLY UPDATED FROM ORIGINAL 2006 APPROVED DOCUMENT . 2006 TEXT IS LOCATED AT THE END OF THIS CHAPTER, ALL IN RED TEXT.~~



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Policy Plan Goals and Policies	Specific Plan Consistency
Land Use Element	
GOAL LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.	
LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identify, maximizes available and planned infrastructure, and foster the development of transit.	The Specific Plan area is within the Ontario Ranch as identified in The Ontario Plan. The Specific Plan will adhere to current approved City Infrastructure Master Plans.
LU1-2 Sustainable Community Strategy. We integrate state, regional and local Sustainable Community/ Smart Growth principles into the development and entitlement process.	Sustainable Community/Smart Growth principles are incorporated into Land Use Plan. Pedestrian sidewalks and multipurpose trails provide connectivity.
LU1-3 Adequate Capacity. We require adequate infrastructure and services for all development.	The Specific Plan establishes an infrastructure and public facilities plan to ensure that adequate roadways and public utilities including sewer, water and drainage facilities along with parks and other public facilities are provided to serve the Specific Plan area.
LU1-4 Mobility. We require development and urban design, where appropriate, that reduces reliance on the automobile and capitalizes on multi-modal transportation opportunities. (Link to Mobility Element Policy M3-3)	Multipurpose trails are provided within the Specific Plan area as shown on the City’s Multipurpose Trails and Bikeway Corridor Plan.
LU1-6 Complete Community. We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Link to Complete Community Section of Community Economics Element)	The Specific Plan plans for a complete community with a mix of residential, commercial, and parks where residents can live, work, shop and recreate.
GOAL LU2: Compatibility between a wide range of uses.	
LU2-6 Infrastructure Compatibility. We require infrastructure to be aesthetically pleasing and in context with the community character.	The Specific Plan provides for an aesthetically pleasing streetscape concept.
LU2-9 Methane Gas Sites. We require sensitive land uses and new uses on former dairy farms or other methane-producing sites be designed to minimize health risks.	The project will comply with appropriate mitigation measures identified in the project EIR for soil remediation and proper venting to address the potential existence of methane gases within the project. Per development requirements, soil that is potentially methane producing will be removed from the site to minimize risks.
GOAL LU4: Development that provides short-term value only when the opportunity to achieve our Vision can be preserved.	
LU4-3 Infrastructure Timing. We require that the necessary infrastructure and services be in place prior to or concurrently with development.	The Specific Plan establishes an infrastructure and public facilities plan to ensure that adequate roadways and public utilities including sewer, water and drainage facilities along with parks and other public facilities are provided to serve the Specific Plan area.
GOAL LU5: Integrated airport facilities that minimize negative impacts to the community and maximize economic benefits.	



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Policy Plan Goals and Policies	Specific Plan Consistency
<p>LU5-7 ALUCP Consistency and Land Use Regulations. We comply with state law that requires general plans, specific plans and all new development to be consistent with the policies and criteria set forth within an Airport Land Use Compatibility Plan for any public use airport.</p>	<p>The Specific Plan is located within the Airport Influence Areas of Chino Airport and Ontario International Airport. The Specific Plan is considered to be an “existing land Use” as defined by the Riverside county ALUCP for the Chino Airport and the ONT ALUCP.</p>
<p>Housing Element</p>	
<p>GOAL H2: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.</p>	
<p>H2-4 New Model Colony. We support a premier lifestyle community in the New Model Colony distinguished by diverse housing, highest design quality, and cohesive and highly amenitized neighborhoods.</p>	<p>The Specific Plan features detached and attached single-family and multi-family housing types in a variety of plotting configurations distinguished by the highest design quality and cohesive and highly amenitized neighborhoods.</p>
<p>H2-5 Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices and other best practices.</p>	<p>The Specific Plan has its own design guidelines to achieve architectural excellence.</p>
<p>GOAL H4: Increased opportunities for low and moderate income households and families to afford and maintain quality ownership and rental housing opportunities, including move-up opportunities.</p>	
<p>H4-2 : Homeownership Opportunities. We increase and expand homeownership rates for lower and moderate income households by offering financial assistance, low-interest loans and educational resources, and by working in collaboration with partnerships.</p>	<p>The Housing Element has identified 1,510 dwelling units (at 10-24 du. ac) within the Specific Plan area to meet the Regional Housing Need Assessment (RHNA) for Moderate Income residents. Recently adopted Senate Bill 166, prohibits cities from allowing their inventory of available sites to be insufficient to meet their remaining unmet RHNA share for lower and moderate-income housing. California Government Code Section 65863 requires cities to make certain findings that the remaining housing element sites can accommodate the RHNA requirements by income level. The City has identified a surplus of 3,248 Moderate Income units with land availability to meet the requirements. Therefore, even if all the units are not constructed, the Specific Plan is still consistent with the Housing Element.</p>
<p>H4-4: Mixed-income Housing. We encourage the integration of affordable housing in the New Model Colony, Ontario Airport Metro Center Area, and existing neighborhoods.</p>	<p>The Specific Plan provides for a wide range of home ownership opportunities for many different lifestyles.</p>
<p>GOAL H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.</p>	
<p>H5-1 Senior Housing. We support the development of accessible and affordable senior housing and provide financial assistance for seniors to maintain and improve their homes.</p>	<p>All development will meet the required accessibility requirements of either the California Residential Code or the California Building Code.</p>



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Policy Plan Goals and Policies	Specific Plan Consistency
H5-2 Family Housing. We support the development of larger rental apartments that are appropriate for families with children, including, as feasible, the provision of services, recreation and other amenities.	See comments under H4-2 above.
H5-3 Disabled People. We increase the supply of permanent, affordable and accessible housing for people with disabilities, and provide assistance to allow them to maintain and improve their homes	All development will meet the required accessibility requirements of either the California Residential Code or the California Building Code.
Parks and Recreation Element	
Goal PR1: A system of safe and accessible parks that meets the needs of the community.	
PR1-1 Access to Parks. We strive to provide a park and/or recreational facility within walking distances (1/4 mile) of every residence.	The Specific Plan provides for parks, a multipurpose trails, and sidewalks that allow all homes to be within walking distance of a recreational facility.
PR1-5 Acreage Standard. We strive to provide 5 acres of parkland (public and private) per 1,000 residents.	The Specific Plan provides for park land, park improvements and/or park fees or a combination thereof in order to meet and comply with City park standards.
PR1-6 Private Parks. We expect development to provide a minimum of 2 acres of developed private park space per 1,000 residents.	The Specific Plan will provide a minimum of 2 acres of developed private and/or public park space per 1,000 residents.
PR1-9 Phased Development. We require parks be built in new communities before a significant proportion of residents move in.	Development within the Specific Plan is required to be reviewed and approved pursuant to the City’s development review process which requires the approval of tentative and final subdivision maps for the project. Conditions of approval associated with the City’s approval of tentative subdivision maps will provide for the timing of construction of parks as part of the development.
PR1-11 Environmental Function of Parks. We require new parks to meet environmental management objectives.	The Specific Plan requires all new development to be reviewed and approved pursuant to the provisions of the City’s development review process which provides for review by the City’s Parks and Maintenance Department which may require the development to incorporate environmental management objectives into the design of parks.
PR1-12 Trails. We promote connections between parks and local trails including those managed by other public agencies.	Multipurpose trails are provided within the Specific Plan area as shown on the City’s Multipurpose Trails and Bikeway Corridor Plan.
PR1-14 Multi-family Residential Developments. We require that new multi-family residential developments of five or more units provide recreational facilities or open space, in addition to paying adopted impact fees.	The Specific Plan requires that all condominium and multi-family developments within the project provide private recreational areas and/or pocket parks for residents of the development in addition to the payment by the developer of adopted impact fees.



Parkside

Policy Plan Goals and Policies	Specific Plan Consistency
Environmental Resources Element	
GOAL ER1: A reliable and cost effective system that permits the City to manage its diverse water resources and needs.	
ER1-3 Conservation. We require conservation strategies that reduce water usage.	All Specific Plan development will meet CALGreen low flow fixture requirements. All irrigation systems meet the City's water conservation requirements.
ER1-5 Groundwater Management. We protect groundwater quality by incorporating strategies that prevent pollution, require remediation where necessary, capture and treat urban run-off, and recharge the aquifer.	The Specific Plan development will comply with all applicable regulations in regards to protecting groundwater quality.
ER1-6 Urban Run-off Quantity. We encourage the use of low impact development strategies to intercept run-off, slow the discharge rate, increase infiltration and ultimately reduce discharge volumes to traditional storm drain systems.	Specific Plan development shall be designed to detain, filter and treat surface runoff, in a manner and combination which is practical, to comply with the most recent requirements of the San Bernardino County NPDES Stormwater Program's Water Quality Management (WQMP) for significant new development projects.
ER1-7 Urban Run-off Quality. We require the control and management of urban run-off, consistent with Regional Water Quality Control Board regulations.	The project is obligated to control urban run-off, consistent with Regional Water Quality Control Board regulations as part of the regional NPDES Permit.
ER1-8 Wastewater Management. We require the management of wastewater discharge and collection consistent with waste discharge requirements adopted by the Regional Water Quality Control Board.	The project is obligated to comply with wastewater discharge and collection requirements adopted by the Regional Water Quality Control Board.
GOAL ER3: Cost-effective and reliable energy system sustained through a combination of low impact building, site and neighborhood energy conservation and diverse sources of energy generation that collectively helps to minimize the region's carbon footprint.	
ER3-1 Conservation Strategy. We require conservation as the first strategy to be employed to meet applicable energy-saving standards.	All development within the Specific Plan will meet California's stringent CALGreen and Energy Codes.
ER3-2 Green Development- Communities. We require the use of best practices identified in green community rating systems to guide the planning and development of all new communities.	The Specific Plan contains a broad mix of land uses, density, multi-purpose trails and proximity to planned transit as identified in the best practices of several rating systems.
R3-3 Building and Site Design. We require new construction to incorporate energy efficient building and site design strategies, which could include appropriate solar orientation, maximum use of natural daylight, passive solar and natural ventilation.	All development within the Specific Plan will meet California's stringent CALGreen and Energy Codes.
GOAL ER4: Improved indoor and outdoor air quality and reduced locally generated pollutant emissions.	
ER4-4 Indoor Air Quality. We will comply with State Green Building Codes relative to indoor air quality.	Specific Plan development will be required to comply with all State laws in regards to indoor air quality.
GOAL ER5: Protected high value habitat and farming and mineral resource extraction activities that are compatible with adjacent development.	



Parkside

Policy Plan Goals and Policies	Specific Plan Consistency
ER5-2 Entitlement and Permitting Process. We comply with state and federal regulations regarding protected species.	The project will comply with all mitigation measures identified in the project EIR with regard to biological resources.
Community Economics Element	
GOAL CE1: A complete community that provides for all incomes and stages of life.	
CE1-7 Retail Goods and Services. We seek to ensure a mix of retail businesses that provide the full continuum of goods and services for the community.	The Specific Plan contains an approximately 11.3-acre neighborhood commercial site to provide goods and services to the community as well as surrounding areas.
CE1-12 Circulation. We continuously plan and improve public transit and non-vehicular circulation for the mobility of all, including those with limited or no access to private automobiles. (Link to Mobility Public Transit)	The Specific Plan multipurpose trails throughout the community. Bus turnouts are provided on Archibald Ave and Ontario Ranch Rd.
GOAL CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.	
CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community. (Link to Community Design Element)	The Specific Plan sets forth criteria for creating high-quality places.
CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.	The Specific Plan provides a framework for high quality architecture and urban design.
Safety Element	
Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.	
S1-1: Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.	All habitable buildings within the Specific Plan will be required to meet the California Building Code.
Goal S3: Reduced risk of death, injury, property damage and economic loss due to fires, accidents and normal everyday occurrences through prompt and capable emergency response.	
S3-8: Fire Prevention through Environmental Design. We require new development to incorporate fire prevention consideration in the design of streetscapes, sites, open space and buildings. (Link to Community Design Element)	All development within the Specific Plan incorporates fire prevention considerations in the design of streetscapes, sites, open spaces and buildings and will be verified for compliance during the development review process.
Goal S5: Reduced risk of injury, property damage and economic loss resulting from windstorms and wind- related hazards.	
S5-2 Dust Control Measures. We require the implementation of Best Management Practices for dust control at all excavation and grading project.	Construction within the Specific Plan will comply with all mitigation measures identified in the project EIR with regard to dust control.
Goal S6: Reduced potential for hazardous materials exposure and contamination.	



Parkside

Policy Plan Goals and Policies	Specific Plan Consistency
<p>S6-9 Remediation of Methane. We require development to assess and mitigate the presence of methane, per regulatory standards and guidelines.</p>	<p>The project will comply with all mitigation measures identified as part of the recommend methane mitigation requirements to address the potential existence of methane gases within the Specific Plan area.</p>
<p>Goal S7: Neighborhoods and commercial and industrial districts that are kept safe through a multi-faceted approach of prevention, suppression, community involvement and a system of continuous monitoring.</p>	
<p>S7-4 Crime Prevention through Environmental Design (CPTED). We require new development to incorporate CPTED in the design of streetscapes, sites, open spaces and buildings.</p>	<p>The Specific Plan requires all new development to be reviewed and approved pursuant to the provisions of the City’s development review process which provides for review by the Police Department which may require the development to incorporate CPTED in the design of streetscapes, sites, open spaces and buildings.</p>
Mobility Element	
<p>GOAL M1: A system of roadways that meets the mobility needs of a dynamic and prosperous Ontario.</p>	
<p>MI-1: Roadway Design and Maintenance. We require our roadways to:</p> <ul style="list-style-type: none"> * Comply with federal, state and local design and safety standards. * Meet the needs of multiple transportation modes and users. * Handle the capacity envisioned in the Functional Roadway Classification Plan. * Maintain a peak hour Level of Service (LOS) E or better at all intersections. * Be compatible with the streetscape and surrounding land uses. * Be maintained in accordance with best practices and our Right-of-Way Management Plan. 	<p>The Specific Plan is consistent with the requirements of the City’s Functional Roadway Classification Plan. The City will maintain arterial streets and curb-to-curb internal streets through the creation of an Community Facilities District. Parkway improvements within the ROW along interior streets and alleys shall be maintained by an HOA.</p>
<p>MI-2: Mitigation of Impacts. We require development to mitigate its traffic impacts.</p>	<p>Development within the Specific Plan will implement all traffic mitigation measures, standard conditions and project design features identified in the project EIR.</p>
<p>GOAL M2: A system of trails and corridors that facilitate and encourage bicycling and walking.</p>	
<p>M2-2 Bicycle System. We provide off-street multipurpose trails and Class II bikeways as our primary paths of travel and use the Class III for connectivity in constrained circumstances</p>	<p>Multi-purpose trails and connections are shown on Exhibit 5-10 promoting safe and convenient travel between destination points.</p>
<p>M2-3: Pedestrian Walkways. We require walkways that promote safe and convenient travel between residential areas, businesses, schools, parks, recreation areas, and other key destination points. (Link to Community Design CD3-1)</p>	<p>Pedestrian walkways, as shown in the roadway plans and sections, exist throughout the Specific Plan area promoting safe and convenient travel between destination points.</p>
<p>GOAL M3: A public transit system that is a viable alternative to automobile travel and meets the basic transportation needs of the transit dependent.</p>	
<p>M3-2 Transit Facilities at New Development. We require new development to provide transit facilities, such as bus shelters, transit bays and turnouts, as necessary.</p>	<p>Turnouts are location on Archibald Ave. and Ontario Ranch Road. Bus shelters are located on Eucalyptus Ave.</p>



Parkside

Policy Plan Goals and Policies	Specific Plan Consistency
M3-4 Bus Rapid Transit (BRT) Corridors. We work with regional transit agencies to implement BRT service to target destinations and along corridors, as shown in the Transit Plan.	Ontario Ranch Rd. is designated as a BRT Corridor. The design for the street meets the City’s requirements and includes an additional landscape buffer on the south side of the road.

GOAL M4: An efficient flow of goods through the City that maximizes economic benefits and minimizes negative impacts

M4-1 Truck Routes. We designate and maintain a network of City truck routes that provide for the effective transport of goods while minimizing negative impacts on local circulation and noise-sensitive land uses, as shown in the Truck Routes Plan.	Ontario Ranch Rd. and Archibald Ave. are designated as a Truck Route. The design for the street meets the City’s requirements and includes an additional landscape buffer on the south side of the road.
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Community Design Element

GOAL CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

CD1-2 Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.	The Design Guidelines set forth a clear and cohesive design theme by providing design criteria for site planning, architectural styles, landscaping, and walls and fences.
CD1-4 Transportation Corridors. We will enhance our major transportation corridors within the City through landscape, hardscape, signage and lighting.	The Design Guidelines set forth a clear and cohesive design theme by providing design criteria for landscape, walls and fences, signage and lighting.
CD1-5 View Corridors. We require all major north- south streets be designed and redeveloped to feature views of the San Gabriel Mountains, which are part of the City’s visual identity and a key to geographic orientation. Such views should be free of visual clutter, including billboards and may be enhanced by framing with trees.	The major north-south streets are designed to feature views of the San Gabriel Mountains and comply with City standards for these streets.

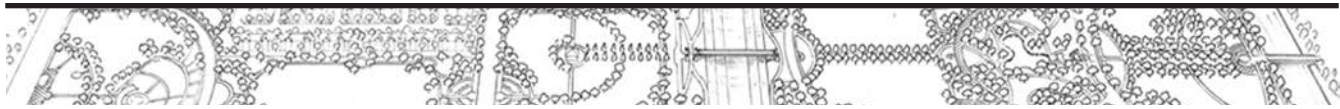
GOAL CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and district.

CD2-1: Quality Architecture. We encourage all development projects to convey visual interest and character through: * building volume, massing, and height to provide appropriate scale and proportion; * a true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and * exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.	The Design Guidelines set forth a clear and cohesive design theme by providing design criteria for architecture, landscaping, and walls and fences.
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Parkside

Policy Plan Goals and Policies	Specific Plan Consistency
<p>CD2-2 Neighborhood Design. We create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:</p> <ul style="list-style-type: none"> * a pattern of smaller, walkable blocks that promote access, activity and safety; * variable setbacks and parcel sizes to accommodate a diversity of housing types; * traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows; * floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the “outdoor living room”), as appropriate; and * landscaped parkways, with sidewalks separated from the curb. 	<p>The Design Guidelines provide for distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places.</p>
<p>CD2-3 Commercial Centers. We desire commercial centers to be distinctive, pedestrian friendly, functional and vibrant with a range of businesses, places to gather, and connectivity to the neighborhoods they serve.</p>	<p>The Design Guidelines set forth a clear and cohesive design theme by providing design criteria for architecture, landscaping, and walls and fences.</p>
<p>CD2-5: Streetscapes. We design new and, when necessary, retrofit existing streets to improve walkability, bicycling and transit integration, strengthen connectivity, and enhance community identity through improvements to the public right of way such as sidewalks, street trees, parkways, curbs, street lighting and street furniture.</p>	<p>The Circulation Plan is designed to accommodate pedestrians, bicycles and autos equally.</p>
<p>CD2-6: Connectivity. We promote development of local street patterns and pedestrian networks that create and unify neighborhoods, rather than divide them, and create cohesive and continuous corridors, rather than independent “islands” through the following means:</p> <p>local street patterns that provide access between subdivisions and within neighborhoods and discourage through traffic;</p> <p>a local street system that is logical and understandable for the user. A grid system is preferred to avoid circuitous and confusing travel paths between internal neighborhood areas and adjacent arterials;</p> <p>and neighborhoods, centers, public schools, and parks that are linked by pedestrian greenways open space networks. These may also be used to establish clear boundaries between distinct neighborhoods and/or centers.</p>	<p>The Circulation Plan is designed for connectivity that unifies neighborhoods.</p>



Parkside

Policy Plan Goals and Policies	Specific Plan Consistency
CD 2-7: Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.	All buildings will meet or exceed California’s stringent CALGreen and California Energy Codes reducing energy demand.
CD2-8: Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.	The Specific Plan encourages “eyes on the street” for neighborhood safety and security.
CD2-9: Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.	The Specific Plan landscape enhances the aesthetics of buildings, created and defines public and private spaces and provides shade.
CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.	The Specific Plan provides for landscape buffer and landscaped islands within commercial surface parking areas.
CD2-11: Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.	The Specific Plan contains guidelines for neighborhood entries and monumentation.
CD2-12: Site and Building Signage. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.	The Specific Plan contains guidelines for neighborhood entries and monumentation.

GOAL CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing, and safe during all hours.

CD3-1 Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics. (Link to Bicycle and Pedestrians Section of the Mobility Element and Policies M2-3 and M2-4)	The Specific Plan is designed for bicycle and pedestrian accessibility throughout the community through a network of off-street bike and pedestrian trails.
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Parkside

Policy Plan Goals and Policies	Specific Plan Consistency
CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.	Landscaping and paving are used to maintain a visual connectivity between streets, sidewalks, and walkways for pedestrians.
CD3-3 Building Entrances. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces. (Link to Safety Element Policy S7-4)	The Specific Plan guidelines for doors and entries promote the interface between public and private spaces.
CD3-5 Paving. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.	Sidewalks and road surfaces contribute to the appearance of streets and public spaces.
CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.	The Design Guidelines set forth a clear and cohesive design theme by providing design criteria for site planning, architectural styles, landscaping, and walls and fences.
GOAL CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages public and private investments.	
CD5-1 Maintenance of Buildings and Property. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.	A master Home Owner Association (HOA) will be established to maintain common areas and other areas as described in Table 6-1 Maintenance Responsibilities..
CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.	The HOA will maintain infrastructure elements as described in Table 6-1 Maintenance Responsibilities and in accordance with City rules and regulations.
Community Economics Element	
GOAL CE1: A complete community that provides for all incomes and stages of life.	
CE1-6 Diversity of Housing. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.	The Specific Plan provides a wide array of residential product types, including single family detached, single family attached and multi-family homes to provide housing opportunities for a variety of stages of life and price points to foster a socially balanced community.
GOAL CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.	
CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.	The Specific Plan provides for unique, high-quality places that add value to the community.
CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.	The Specific Plan requires all new development to be reviewed and approved pursuant to the provisions of the City's development review process.



Parkside

Policy Plan Goals and Policies	Specific Plan Consistency
CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.	The Specific Plan sets forth criteria for creating high-quality places.
CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.	Maintenance will be ensured by the HOA.
CE2-6 Public Maintenance. We require the establishment and operation of maintenance districts or other vehicles to fund the long-term operation and maintenance of the public realm whether on private land, in rights-of-way, or on publicly-owned property.	A Community Facilities District (CFD) will be established in conjunction with the Master Homeowners Association to maintain the public realm.



Parkside

9.0 General Plan Consistency

California Government Code (Title 7, Division 1, Chapter 3, Article 8, Section 65450-65457) permits the adoption and administration of Specific Plans as an implementation tool for elements contained in the local General Plan. Specific plans must demonstrate consistency in regulations, guidelines, and programs with the goals and policies set forth in the General Plan.

Prior to the approval of this Specific Plan, for portions of Subareas 22 and 23, the City will adopt a General Plan Amendment changing the General Plan Land Use Policy Map to reassign the locations of high density residential uses, eliminate all low density designations, create new medium density residential designations, add a Neighborhood Commercial Center designation at Archibald and Edison Avenues, and add a community facility designation (fire station), west of the Neighborhood Center. This Specific Plan has been prepared in conformance with the goals and policies of the City of Ontario General Plan, as amended. The City of Ontario New Model Colony Specific Plan Policy Matrix, which follows, describes the manner in which this Specific Plan complies with the General Plan.

Ontario New Model Colony Specific Plan Policy Matrix

Following are policies from the Ontario NMC General Plan; they are intended to achieve the “vision” for the New Model Colony planning area as outlined in Chapter 2 of the NMC General Plan. The policy matrix is divided into five (5) parts, consisting of the five (5) elements of the NMC General Plan Amendment: I. Community Development; II Infrastructure; III Aesthetic, Cultural, Open Space, and Recreational Resources; IV Natural Resources and V Hazards. The following matrix is intended to determine the adequacy of the proposed Specific Plan in meeting the policies of the NMC General Plan. The checklist shall be completed and submitted with the Specific Plan application. Check if the Specific Plan fully complies or partially complies with each respective policy, or if not applicable. A complete explanation is required in instances whereby the proposed plan does not fully comply with a policy or if not applicable.

Policy	Fully Complies	Partially Complies	Does Not	S.P Sec. No.	N/A
PART I: COMMUNITY DEVELOPMENT					
1.1 Role of New Model Colony General Plan					
1.1.1, 1.1.5	X			2.4, 2.6, 4.2	
1.1.4	X			2.2, 2.4, 4.2	
<p>The Proposed Specific encompasses sub area boundaries designated on Figure 3-6 of the New Model Colony General Plan and sufficient land to constitute a significant portion of a distinct and cohesive neighborhood or district, as depicted on Figure 3-7 of the New Model Colony General Plan.</p> <p>Sub area boundaries may be modified on the basis of the following conditions (check all that apply if sub area boundaries are not entirely reflected by proposed Specific Plan):-</p> <p><i>Note adjustments to sub area boundaries shall be accompanied by an equivalent adjustment of the development capacity, maintaining the overall coverage densities depicted on the NMC General Plan Land Use Plan.</i></p> <ul style="list-style-type: none"> • to achieve unified districts and neighborhoods; • to account for defined market demands regarding the types and scale/acreage of development that should be accommodated; • to reflect the location of major roads, utility corridors and easements, flood control improvements, natural resource areas, and public properties; • to reflect property subdivisions and ownerships; and • to consider appropriate infrastructure and public service phasing. 					

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
1.2 Structure and Organization of Land Use:					
1.2.1 The proposed plan provides for integrated development of distinct residential neighborhoods and major regional serving activity centers and corridors that are differentiated by their functional role, uses, and character. Each is conceived as a uniquely identifiable place, rather than a fragment of a continuous/undifferentiated urban area.	X			2.2 4.2	
1.2.2 The plan provides for a balance of land uses within each neighborhood and district in accordance with phasing policies of the NMC General Plan.	X			2.2 4.2	
1.2.3 The plan links neighborhoods, centers, and corridors by transportation, utility, and greenways/ open space networks. These may also be used to establish clear boundaries between distinct neighborhoods and/or centers.	X			5.1.4, 4.2, 5.1.3	
1.2.4 The plan includes major amenities that provide identity and enhances character including a large-scale park (“Village Green”), lake / streams, and golf courses. The Parkside Land Use Summary (Table 4-1) includes a portion of the Great Park, but it does not meet the criteria to be deemed a “Village Green”. There are no additional major amenities included in the plan.				4.3, 4.4	X
1.2.5 The plan includes an educational campus as the “centerpiece” of community identity and organization. The Parkside Specific Plan does not include an educational campus.					- X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>1.2.6 The plan includes elements that provide linkages and continuity with uses abutting the New Model Colony, including:</p> <ul style="list-style-type: none"> • continuity and linkage of transportation and utility infrastructure • continuity of street trees and landscaped median along Euclid Avenue The Parkside Specific Plan area does not border Euclid Avenue. • Pedestrian linkages to residential areas north of Riverside Drive The Parkside Specific Plan area does not border Riverside Drive. • continuity of commercial centers at the Riverside Drive/ Euclid Avenue and Riverside Drive/ Milliken Avenue intersections; and The Parkside Specific Plan area does not border Riverside Drive, Euclid Avenue, or Milliken Avenue. • relationship to existing and planned land uses on the west side of Euclid Avenue (City of Chino), north side of Riverside Drive (City of Ontario), and east side of Milliken Avenue (Riverside County)--by the use of complementary and/or transitional land uses and densities and streetscape improvements. <p>The surrounding land use is dominantly agricultural, which may not be considered to be a transitional land use for residential development.</p>	X			5.1, 5.2	
	X			5.3, 5.4	X
					X
					X
					X
<p>1.2.7 The Specific Plan establishes clearly defined “edges” for the New Model Colony, including landscape setbacks, transitional densities, and other suitable techniques that avoid the use of walls and creation of “walled” enclave.</p>	X			5.1.1, 7.0	
<p>1.2.8 The Specific Plan includes measures to minimize potential impacts of environmental hazards including flooding, and noise. The Plan provides for the use of electrical, transmission corridors, flood channels, and similar elements to form “edges” for residential neighborhoods and centers and/or accommodates public greenways / corridors.</p>	X			4.4	
<p>1.3 Residential Neighborhoods</p>					
<p>1.3.1 The Specific Plan accommodates single and multi-family housing, local serving commercial services (e.g., grocery stores, barber and beauty shops, small dining establishments, etc.), community and religious facilities, schools, parks, open spaces, and similar uses within “Residential Neighborhoods.”</p>	X			4.0	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>1.3.2 The Specific Plan includes priority areas for executive housing as provided for in the NMC General Plan (refer to Figure 3-8, NMC General Plan). Of the target 500 acres of executive housing provided in the General Plan, 20% or 100 acres shall be provided in the areas north of Schaefer Avenue. The following standards must be met in establishing an area large enough to achieve a distinctive identity and critical mass for executive housing (check all that apply):</p> <ul style="list-style-type: none"> • Development with lots of 10,000 square feet or greater: 80 contiguous acres minimum • Development adjacent to a golf course: 50 contiguous acres minimum • Developments adjacent to a large open space, lake, park, or Village Green: 20 contiguous acres minimum <p>The Parkside Specific Plan housing program does not include provision for executive housing.</p>					X
<p>1.3.3 The Plan accommodates a mix of land uses and a diversity of housing types and parcel sizes within each “Residential Neighborhood” with each designed to create a unified and distinct character. Each neighborhood may incorporate one or more planning sub areas depicted in Figure 3-7 of the NMC General Plan. Refer to Figures 3-9 and 3-10 for examples on the use of Residential Neighborhoods.</p>	X			4.2	
<p>1.3.4 The Plan incorporates a focal point of activity and identity within each “Residential Neighborhood” that consolidates local serving retail and services, public meeting facilities, schools, parks, common areas (for community events, farmers markets, and other celebrations), religious facilities, and other pertinent uses.</p>	X			4.2, 4.3	
<p>1.3.5 The plan incorporates supporting recreational, educational, retail, cultural, and institutional uses within each “Residential Neighborhood” based on appropriate service standards. To the extent feasible, these uses shall be integrated in “Neighborhood Centers”</p>	X			4.5	
<p>1.3.6 The Plan includes provisions to increase residential densities and reduce block sizes adjacent to “Neighborhood Centers” and other key activity centers, as appropriate.</p>		X			
<p>1.3.7 The Plan includes provisions for greenways for pedestrians and biking that links housing with parks, “Neighborhood Centers,” amenities, and adjacent neighborhood and activity centers</p>	X			4.4, 5.1.3, 5.1.4	
<p>1.3.8 The Plan establishes clearly defined “edges” and “entries” that contribute to a distinct neighborhood identity (refer to Figures 3-11, 3-12, and 3-13 of the NMC General Plan)</p>	X			7.0, 5.1	

Policy	Fully Complies	Partially Complies	Does Not	S.P Sec. No.	N/A
1.4 Housing Diversity/Quality					
1.4.1 The Plan accommodates single and multi-family housing, parks, and open spaces in areas designated as “Residential-Low Density” by the NMC General Plan.	X			4.2	
1.4.2 The Plan provides for a variety of housing types to serve the needs of residents, including large lot, estate, executive, affordable, live / work housing.		X		6.14, 4.2	
1.4.3 The Plan provides for an overall average density of 4.6 units per gross acre in areas designated as “Residential-Low Density” by the NMC General Plan Land Use Plan, within which a variety of parcel sizes and housing types area allowed. Densities may be varied throughout a planning sub area, as long as the average density is not exceeded.	X			4.2	
1.4.4 The plan provides design standards emphasizing neighborhood livability, and socialization, through such elements as (refer to Figures 3-7, 3-9 and 3-14, NMC General Plan):	X			4.1, 4.2, 5.1.1	
• Establishment of a pattern of blocks that promotes access and neighborhood activity, while maintaining safety;	X				
• use of variable setbacks and parcel sizes to accommodate a diversity of housing unit types and respond to changing market conditions and resident needs;	X				
• minimizing the width of streets to slow traffic and promote “intimacy,” while maintaining acceptable fire protection and traffic flows;	X				
• orientation of housing toward the street and reducing the visual and physical dominance of garages (introducing the street frontage as the “outdoor living room”); and	X			7.0	
• use of extensively landscaped parkways, with sidewalks separated from the curb.	X			7.0	
1.4.5 The Plan includes standards to achieve variable residential densities and housing types along a block where appropriate (e.g., single family attached units placed adjacent to duplexes, which could be adjacent to townhomes).	X			4.0	
1.4.6 The Plan includes provisions for small clusters of multi-family housing (generally, of a maximum of 2-5 acres) within areas designated as “Residential-Low Density” by the NMC General Plan Land Use Plan, that are integrated within the overall pattern of residential development, avoiding large scale, segregated concentrations of high density uses (refer to Figures 3-9 and 3-10 - NMC General Plan)	X			4.0	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>1.4.7 The plan includes standards for multi-family housing in areas designated as “Residential Low-Density” by the NMC General Plan Land Use Plan to convey a high level of visual quality and distinctive low density neighborhood including, but not limited to:</p> <ul style="list-style-type: none"> • modulation of building volumes, masses, and heights and articulation of elevations to convey the sense of individual units or smaller structures (precluding “big stucco boxes”); • orientation of the building elevation and use of clear visual and physical linkages to the primary street frontage; • inclusion of visually well-defined entries that promote individual identity, while maintaining adequate security; • integration of balconies and stairways as distinctive architectural elements, rather than “appendages”; • inclusion of common open space and recreational facilities that are accessible to all residents (with requirements for on-going maintenance of these by residents); • use of landscaped setbacks along primary street frontages that maintains the predominant single family neighborhood character; and • linkage of the development with adjacent sites (avoiding “walled enclaves”). 	X			4.0, 7.0	
<p>1.4.8 The proposed plan includes provisions to avoid the use of walls to separate residential areas from arterial and other high traffic volume streets by the use of expanded landscaped setbacks, frontage roads with landscaped medians, natural drainage swales, and other appropriate techniques (refer to Figure 3-13, NMC General Plan). Where used, walls shall be attractively designed and landscaped and setbacks used to promote a high level of visual quality.</p>	X			4.2, 7.0, 5.1	
<p>1.4.9 The Plan includes measures addressing all of the use and development restrictions as specified by Referral Areas A, B, and C, and Safety Zones I, II, and III, and of the adopted Chino Airport Comprehensive Land Use Plan. This includes prohibited land uses, interior noise levels, maximum persons per acre, aviation easements, and non-hazardous operations and materials. In addition, a noise impact notification area shall be implemented for all land uses within two miles of the Chino Airport. These measures are consistent with Federal Aviation Administration (FAA) rules, will help protect the long-term operation of the Chino Airport, and will prevent the kind of aircraft noise and other impacts created by flight patterns such as have been experienced by the City around the Ontario International Airport.</p> <p>The Parkside Specific Plan does not address that a portion of the plan area is within Referral Area C and Safety Zone III of the Chino Airport Comprehensive Land Use Plan.</p>			X		

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>1.4.10 The proposed plan includes provisions for the implementation of a Noise Impact Notification Area (NINA) and a requirement for aviation easements for all schools within two miles of the Chino Airport.</p> <p>_____ The Parkside Specific Plan does not include any schools.</p>					X
<p>1.4.11 The plan includes provisions requiring residential units to be set back from power transmission line easements in accordance with the California Department of Education's limits for locating school sites.</p> <p>_____ The Parkside Specific Plan does not include any schools, nor does it include a power transmission line easement.</p>					X
<p>1.5 Neighborhood Centers</p>					
<p>1.5.1 The proposed plan accommodates local serving commercial (food and beverage sales, small retail shops, personal services, etc.), small restaurants, community/public meeting facilities, libraries, government facilities, cultural facilities, parks, schools (refer to Policy 1.6.1 below), religious facilities, public plazas and performance spaces, and similar uses in areas designated as "Neighborhood Center."</p> <p>1.5.2 The plan accommodates multi-family housing and mixed use structures that incorporate housing units above ground level retail shops, offices, or community facilities, in areas designated as "Neighborhood Center" by the NMG General Plan Land Use Plan.</p> <p>_____ The Parkside Specific Plan does not include mixed use development.</p>	X			4.5	X
<p>Density/Intensity The plan provides for the following densities/intensities for Neighborhood Centers (Policies 1.5.1 to 1.5.6):-</p>					
<p>1.5.3 The Plan includes provisions insuring neighborhood centers contain 50,000 to 100,000 square feet of local serving commercial uses for each 3,500 to 4,000 residential units.</p>	X			4.5	
<p>1.5.4 The Plan includes provisions for the development of commercial and public uses at intensities that allow for the use of surface parking, generally at a maximum floor area ratio of 0.35:1.</p>	X			4.5	
<p>1.5.5 The Plan includes provisions for the development of multi-family housing at a maximum overall average density of 12 units per gross acre.</p> <p>_____ The Parkside Specific Plan includes a wide variety of multi-family housing product types with this density included at the low end of the density range.</p>					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>1.5.6 The Plan includes provisions for the development of mixed use structures at a maximum floor area ratio of 1.5:1.</p> <p>The Parkside Specific Plan does not include mixed-use development.</p>					X
<p><i>Design and Development Principles for Neighborhood Centers (policies 1.5.7 to 1.5.13):</i></p>					
<p>1.5.7 Provisions insuring “Neighborhood Centers” are designed and developed as pedestrian oriented “villages” including the following policies: (refer to Figures 3-10 and 3-16 – NMC General Plan)</p> <ul style="list-style-type: none"> • integration of multiple uses into a cohesive place; • use of multiple building volumes, masses, and heights and extensively articulated facades; • siting of buildings around common pedestrian areas, plazas, and public open spaces; • development of public spaces that support casual gatherings, outdoor dining and retail, entertainment, artistic exhibitions and performances, community events, and similar functions; • establishment of a continuous or semi-continuous business frontage along pedestrian spaces and design of ground elevations to be “pedestrian-friendly” (extensive windows, projecting signage, lighting, etc.); • siting of a portion of development along the primary street frontage, with parking partially or fully screened by the buildings; • development of clearly defined walkways from parking areas (paving materials, landscape, signage, etc.); and • design of buildings and sites to complement adjacent low density residential areas. 	<p>X</p> <p>X</p>	<p>X</p> <p>X</p>		<p>4.5</p> <p>6.15</p>	
<p>1.5.8 The Plan includes provisions insuring integration of public uses, such as community meeting rooms and auditoriums, with commercial uses in the designated “Neighborhood Centers”.</p> <p>The Parkside Specific Plan “Neighborhood Center” does not include detail for these types of uses.</p>					X
<p>1.5.9 The Plan includes provisions insuring linkages between multi-family residential development (single-family attached, townhomes, or apartments) to “Neighborhood Centers”. Include design standards insuring multi-family residential developments complement the “village” character in accordance with Policy 1.4.7.</p>	<p>X</p>			<p>4.2.2, 7.1</p>	
<p>1.5.10 The Plan includes provisions insuring “Neighborhood Centers” are developed as a low rise environment, 1 to 2 stories, except mixed use structures that may be developed to a maximum height of 3 stories.</p>	<p>X</p>			<p>6.15</p>	

Policy	Fully Complies	Partially Complies	Does Not	S.P Sec. No.	N/A
1.5.11 The Plan includes provisions to promote the consolidation of school campuses within or linked to “Neighborhood Centers” sharing common facilities such as play fields, gymnasiums, auditoriums, and other facilities, where feasible. ———— The Parkside Specific Plan does not include any schools.					X
1.5.12 The Plan includes provisions to Integrate public libraries and neighborhood parks with “Neighborhood Centers” and schools.	X			4.5.1	
1.5.13 The Plan includes provisions to link “Neighborhood Centers” with adjacent residential areas by the use of greenways, pedestrian walkways, and bicycle paths.	X			4.3.2	
1.6 — Schools					
1.1.1 The plan includes provisions to accommodate the development of school administration, classrooms, athletic facilities, and supporting uses in areas designated as “Elementary,” “Middle,” and “High School” in addition to those accommodated in designated “Neighborhood Centers.” <i>The number of schools depicted on the NMC General Plan Land Use Plan are based on the use of current (1997) school standards and may vary to reflect changes in these over the NMC’s development period. Locations are conceptualized and may be varied, provided that the intent of the NMC General Plan policies for consolidation with parks, other schools, and community facilities are attained.</i>	X			5.5	
1.6.2 The plan provides schools to meet resident needs in accordance with pertinent School District standards.	X			5.5	
1.6.3 The plan meets the policy to distribute schools throughout the New Model Colony to promote accessibility from all “Residential Neighborhoods.”	X			4.6	
1.6.4 The plan includes provisions to promote the consolidation of school campuses sharing common facilities such as play fields, gymnasiums, auditoriums, and other facilities, where feasible. ———— The Parkside Specific Plan does not include any schools.					X
1.6.5 The plan includes provisions to integrate public schools with other community-serving uses such as parks, libraries, public meeting rooms, day-care, and similar uses (see Figure 3-10 – NMC GPA).		X		5.5	
1.6.6 The plan includes provisions to link the public schools with adjacent housing, parks, and other schools by a pedestrian greenways network (see Figure 3-10 – NMC GPA).	X			5.1.3	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>1.6.7 The Plan includes provisions requiring educational facilities to be set back from power transmission-line easements in accordance with the California Department of Education’s limits for locating school sites.</p> <p>The Parkside Specific Plan does not include any schools.</p>					X
<p>1.7 Multi-Family Residential Corridors and Centers</p>					
<p>1.7.1 The plan accommodates multi-family residential units (small lot detached, single family attached, townhomes, and apartments) in areas designated as “Residential-Medium Density” and “Residential-High Density” by the NMC General Plan Land Use Plan.</p>	X			4.2	
<p>1.7.2 The plan requires the inclusion of sufficient on-site recreational amenities in higher density-developments to meet resident needs.</p>	X			4.3	
<p>1.7.3 The plan encourages the inclusion of community oriented uses such as public meeting rooms, plazas/courtyards, day-care facilities, and similar uses. These may be developed as single purpose-buildings or incorporated with residential uses.</p> <p>The Parkside Specific Plan does not include community amenities such as public meeting rooms, plazas, day-care facilities, etc.</p>	X				
<p>1.7.4 The Plan accommodates an overall average density of <u>12</u> units per gross acre in areas designated as “Residential Medium Density” and <u>18</u> units per gross acre in areas designated as “Residential-High Density” by the NMC General Plan, within which a variety of parcel sizes and housing types-would be allowed. Densities may be varied throughout a planning sub area, as long as the average density is not exceeded.</p> <p>The Parkside Specific Plan application includes a General Plan Amendment, which proposes a wide-variety of housing types, with an average of 12 du/gross acre of residential use.</p>		X			
<p>1.7.5 The Plan includes provisions to establish visual and physical linkages among individually developed-multi-family sites to create a cohesive and continuous corridor, rather than independent “islands.”—This may be achieved through the fronting of buildings adjacent or in immediate proximity to-the street, use of a consistent landscaped setback incorporating common street trees or other-vegetation, minimization of side property setbacks to achieve building continuity (a common-“building wall”) along the street frontage, location of all parking to the rear of street-facing buildings,—use of interconnecting pedestrian walkways, and similar techniques (see Figure 3-17 in the NMC-GPA).</p>	X			7.1, 7.5	
<p>1.7.6 The Plan includes standards and guidelines to promote visual interest in the design of building-elevations in accordance with Policy 1.4.7 (except for the use of extensive landscaped setbacks).</p>	X			7.0	

Policy	Fully Complies	Partially Complies	Does Not	S.P Sec. No.	N/A
1.7.7 The Plan includes provisions to link community serving facilities in multi-family corridors with adjacent residential neighborhoods.	X			7.7.9	
1.7.8 The Plan includes provisions insuring sidewalks are developed in multi-family corridors as per City Standards to facilitate and be attractive for pedestrian activity.	X			7.5, 7.7	
1.7.9 The Plan includes provisions requiring residential units be set back from power transmission line easements in accordance with the California Department of Education's limits for locating school sites. The Parkside Specific Plan does not include any power transmission lines.					X
1.8 Town Center					
1.8.1 The plan includes provisions for the development of retail, commercial, professional offices, entertainment, art galleries, dining establishments, hotels, and similar uses in areas designated as "Town Center" by the NMC General Plan Land Use Plan. The Parkside Specific Plan does not include a "Town Center"					X
1.8.2 The Plan includes provisions requiring integration of multi-family housing (townhomes and apartments) with retail, office, and other uses within the Town Center. The Parkside Specific Plan does not include a "Town Center"					X
1.8.3 The Plan includes provisions for mixed-use buildings that integrate housing with retail commercial, office, and similar uses and special housing types (e.g., artist live/work facilities and home occupations) within the Town Center. The Parkside Specific Plan does not include a "Town Center"					X
1.8.4 The Plan includes provisions for government office, cultural uses (libraries, museums, performance venues, etc.), religious facilities, schools, recreational facilities, multi-modal transportation hub, and similar public and quasi-public uses, within the Town Center area. The Parkside Specific Plan does not include a "Town Center"					X
1.8.5 The Plan includes provisions for community meeting rooms, day-care facilities, and public and private plazas, courtyards, and open spaces within the Town Center area. The Parkside Specific Plan does not include a "Town Center"					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>1.8.6 The Plan accommodates development within the Town Center at building heights, generally, of 1 to 3 stories and a maximum floor area ratio of 0.5, except for mixed use structures that integrate housing with retail, office, public, and other uses that may be developed up to four stories and a floor area ratio of 2.0:1.</p> <p>The Parkside Specific Plan does not include a “Town Center”</p>					✗
<p>1.8.7 The Plan accommodates free-standing residential uses at a maximum overall average density of 18 units per gross acre.</p> <p>The Parkside Specific Plan does not include a “Town Center”</p>					✗
<p>1.8.8 The Plan includes standards to integrate development of individual parcels to create a cohesive pedestrian oriented center. The following design considerations are included (as illustrated by Figures 3-18 and 3-19 of the NMC GPA):</p> <ul style="list-style-type: none"> • integration of multiple buildings and uses into a cohesive development pattern; • siting of buildings to create a common/semi-continuous “building wall” directly fronting sidewalks, plazas, and other open spaces—areas may be excepted to incorporate courtyards and corridors; • location of parking to the rear of street facing structures, subterranean, or in structures; • use of multiple building volumes, masses, heights, and highly articulated facades to promote visual interest and convey the sense of individual storefronts/businesses; • use of vertical setbacks above the second level to reduce visual impacts along sidewalks and street frontages; • design of the ground floor of structures along the street frontage of commercial and mixed use buildings to enhance pedestrian activity (extensive windows, clearly defined entries, modulation of facades, outdoor patios and seating, etc.) (as illustrated by Figures 3-20, 3-21, and 3-22 of the NMC GPA); • integration of consistent and well-designed building and public signage; • develop sidewalks and other public spaces to support casual gatherings, outdoor dining and retail, entertainment, arts exhibitions and performances, community events, and similar functions; and • incorporate streetscape amenities along the street frontages (benches, trees, planters, decorative paving, pedestrian oriented signage and lighting, trash receptacles, fountains, information kiosks, telephones, news racks, banners, public art, and similar elements). <p>The Parkside Specific Plan does not include a “Town Center”</p>					✗

Policy	Fully Complies	Partially Complies	Does Not	S.P Sec. No.	N/A
1.8.9 The plan incorporates at least one major public plaza/square as a centerpiece of community activity and identity of sufficient size to accommodate events and celebrations, outdoor performances, community meetings, picnics, farmers markets, and similar functions. The Parkside Specific Plan does not include a "Town Center"					X
1.8.10 The plan includes provisions to consolidate parking facilities, with retail uses located along the street frontage. The Parkside Specific Plan does not include a "Town Center"					X
1.8.11 The Plan includes standards to site and design multi-family housing to create a cohesive and continuous corridor, rather than independent "islands" in accordance with Policy 1.7.5; and promote a high quality of design in accordance with Policy 1.4.7 (except for the use of extensive landscaped setbacks). The Parkside Specific Plan does not include a "Town Center"					X
1.8.12 The plan includes provisions to integrate a diversity of housing types within any block rather than a singular type (e.g., duplexes, townhomes, and courtyard units). The Parkside Specific Plan does not include a "Town Center"					X
1.8.13 The plan includes provisions to integrate public meeting facilities and other community services with retail and other high activity uses. The Parkside Specific Plan does not include a "Town Center"					X
1.8.14 The design of internal local streets emphasizes pedestrian activity (15' plus width sidewalks) and reduces traffic speed using such techniques as reduced width, angled parking, landscaped "pullouts"/chokers, or traffic circles. The Parkside Specific Plan does not include a "Town Center"					X
1.8.15 The plan includes provisions to establish clearly defined "entries" to the Town Center, including the use of signage, art (banners, sculptures, etc.) landscape, fountains, lighting, building massing, and other techniques. The Parkside Specific Plan does not include a "Town Center"					X
1.8.16 The plan establishes pedestrian, bicycle, greenways, and other corridors that link the Town Center with adjacent "Residential Neighborhoods" and, if developed, educational campus. The Parkside Specific Plan does not include a "Town Center"					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<i>1.9 — Regional Centers</i>					
<p>1.9.1 — The plan accommodates regional-serving retail commercial, professional offices, entertainment, dining, hotel/motel, conference facilities, hospitals and supporting medical offices, research laboratories, and facilities, financial institutions, sports stadiums/arenas and facilities, and similar uses.</p> <p>— The Parkside Specific Plan does not include a “Regional Center”</p>					X
<p>1.9.2 — The Plan accommodates multi-family housing, mixed uses that incorporate housing and retail/offices, and live/work facilities that is integrated with other regional serving uses.</p> <p>— The Parkside Specific Plan does not include a “Regional Center”</p>					X
<p>1.9.3 — The Plan encourages the inclusion of community meeting rooms, day-care, and other public facilities.</p> <p>— The Parkside Specific Plan does not include a “Regional Center”</p>					X
<p>1.9.4 — The Plan accommodates buildings at a height of 1 to 3 stories, with 4 stories allowed for mixed use structures. This would accommodate a maximum floor area ratio of 0.5:1 for commercial/office uses and 2.0:1 for mixed use buildings. Sports stadium/arena, hotel/conference facilities, and other “specialty” uses may require additional height.</p> <p>— The Parkside Specific Plan does not include a “Regional Center”</p>					X
<p>1.9.5 — The Plan accommodates free-standing multi-family residential uses at an overall average density of 18 units per gross acre.</p> <p>— The Parkside Specific Plan does not include a “Regional Center”</p>					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>1.9.6 The Plan includes provisions to integrate development of individual parcels to create a cohesive center, which considers the (see Figure 3-16, NMC General Plan):</p> <ul style="list-style-type: none"> • development of sidewalks, plazas, and other public spaces around which individual buildings and businesses are grouped and incorporate pedestrian oriented amenities (benches, public art, fountains, pedestrian scaled lighting and signage, and similar); • inclusion of one or more public “square” to serve as a gathering place for public activity and events; • use of modulated building volumes, masses, and heights and articulated facades to create the sense of individual businesses, rather than undifferentiated “boxes”; • integration of consistent and well-designed building and public signage; • siting of a portion of buildings on the site’s peripheral streets to provide identity and connectivity with adjacent uses; • inclusion of sidewalks of sufficient width to accommodate pedestrian activity and outdoor restaurants, news stands, and other uses; • opening of the street and sidewalk/plaza facing building elevations to promote visual interest and pedestrian activity (extensive windows, clearly defined entries, and so on); • extensive use of landscape to enhance the environmental character; and • design of parking structures to be visually integrated with and continue the principal design character of commercial buildings. <p>The Parkside Specific Plan does not include a “Regional Center”</p>					X
<p>1.9.8 The Plan includes provisions to link Regional Centers with surrounding residential neighborhoods and centers through the street and highway network, distinctively paved pedestrian walkways and crosswalks, common “palette” of street trees and landscape and street furniture, well designed signage, and similar elements.</p> <p>The Parkside Specific Plan does not include a “Regional Center”</p>					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
1.10 Community Centers					
<p>1.10.1 The Plan accommodates large scale retail commercial (e.g., garden supply, furniture warehouses, discount retail, “big box,” and similar uses), professional offices, entertainment, dining, hotel and conference facilities, and similar uses in areas designated as “Community Commercial” by the NMG General Plan.</p> <p>———— The Parkside Specific Plan includes approximately 11.9 net acres designated for development of neighborhood commercial uses, but not Community Commercial uses.</p>					X
<p>1.10.2 The Plan accommodates multi-family housing and live/work facilities that is integrated with other region serving uses.</p> <p>———— The Parkside Specific Plan does not include live/work facilities.</p>					X
<p>1.10.3 The plan accommodates a community center of approximately 150,000 to 225,000 square feet of commercial uses for each 9,000 to 15,000 residents.</p> <p>———— The Parkside Specific Plan includes approximately 115,000 square feet designated for development of neighborhood commercial uses, but not a Community Center.</p>					X
<p>1.10.4 The plan includes provisions limiting buildings to 1 to 2 stories and a maximum floor area ratio of 0.35:1 with surface parking.</p> <p>———— The Parkside Specific Plan includes approximately 115,000 square feet designated for development of neighborhood commercial uses, but not a Community Center.</p>					X
<p>1.10.5 The plan includes provisions to accommodate free-standing residential uses at an overall average density of 18 units per gross acre.</p> <p>———— The Parkside Specific Plan does not include live/work facilities.</p>					X
<p>1.10.6 The plan includes provisions to integrate development of individual parcels to create a cohesive center considering the design and development principles defined by Policy 1.9.6.</p> <p>———— The Parkside Specific Plan does not include a Community Center.</p>					X
<p>1.10.7 The plan includes provisions to site and design development of multi-family housing to convey a high level of visual quality in accordance with Policy 1.4.7.</p> <p>———— The Parkside Specific Plan does not include live/work facilities.</p>					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>1.10.8 The plan includes provisions to link Community Centers with surrounding residential neighborhoods and centers through the street and highway network, distinctively paved pedestrian walkways and crosswalks, common “palette” of street trees and landscape and street furniture, well designed signage, and similar elements.</p> <p>_____ The Parkside Specific Plan does not include a Community Center.</p>					X
<i>1.11 Educational Campus</i>					
<p>1.11.1 The plan accommodates educational, administrative, academic, vocational, medical, research, athletic, student housing, services, cultural (library, museum, performance, etc.), and other supporting uses.</p> <p>_____ The Parkside Specific Plan does not include an educational campus.</p>					X
<p>1.11.2 The plan includes research and educational uses that reflect the agricultural history of the New Model Colony and promotes water fowl habitat restoration and management.</p> <p>_____ The Parkside Specific Plan does not include an educational campus.</p>					X
<p>1.11.3 The plan includes provisions for public meeting rooms, auditoriums, concert facilities, museums, libraries, and recreational facilities (e.g., sports fields) that are accessible to and serve adjacent residential neighborhoods.</p> <p>_____ The Parkside Specific Plan does not include an educational campus.</p>					X
<p>1.11.4 The Plan includes provisions to site and design development to convey a “campus” environment including the clustering of buildings on common plazas and courtyards, linkage of all areas by walkways, and extensive use of landscape.</p> <p>_____ The Parkside Specific Plan does not include an educational campus.</p>					X
<p>1.11.5 The plan includes provisions to site buildings and design the campus to promote continuity with the development fabric of the adjacent Town Center and business parks (e.g., continuity of a “Main Street” and overall street pattern, location of administrative offices as a visual terminus for the Town Center, continuity of greenways, etc.). Avoid the establishment of a isolated, “walled” environment.</p> <p>_____ The Parkside Specific Plan does not include an educational campus.</p>					X
<p>1.11.6 The plan includes provisions for walkways, bicycle paths, greenways, and other elements that link the campus with surrounding uses.</p> <p>_____ The Parkside Specific Plan does not include an educational campus.</p>					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<i>1.12 — Business and Industrial Parks</i>					
1.12.1 The plan accommodates light manufacturing, research and development, technology development, medical, entertainment facilities and production, innovative technology production, and similar primary uses. The Parkside Specific Plan does not include a business park or an industrial park.					✗
1.12.2 The plan accommodates limited supporting uses such as retail sales, product exhibition, galleries and arts exhibition, financial institutions, restaurants, health clubs, personal services, day-care, and professional offices. The Parkside Specific Plan does not include a business park or an industrial park.					✗
1.12.3 The plan accommodates “high impact” entertainment and commercial recreation uses that are not suitable in commercial districts due to their noise, traffic, or other characteristics that may conflict with residential or commercial uses. The Parkside Specific Plan does not include a business park or an industrial park.					✗
1.12.4 The plan accommodates live/work facilities for artists and professionals associated with other uses in the business park. The Parkside Specific Plan does not include a business park or an industrial park.					✗
1.12.5 The plan discourages the development of extensive warehouse development. The Parkside Specific Plan does not include a business park or an industrial park.					✗
1.12.6 The plan includes provisions limiting building heights at 1 to 3 stories and a maximum floor area ratio of 0.4:1. The Parkside Specific Plan does not include a business park or an industrial park.					✗

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>1.12.7 The plan includes site and design provisions to establish a cohesive and integrated district considering the following (see Figure 3-19 of the NMC GPA):</p> <ul style="list-style-type: none"> • development of a network of pedestrian walkways and plazas around buildings sited and concentrated to connect a site with abutting properties; • location of parking to the rear of buildings fronting pedestrian plazas or in structures; inclusion of restaurants, retail shops, services, and other “high activity” uses in the ground floor of structures facing pedestrian areas; • inclusion of restaurants, retail shops, services, and other “high activity” uses in the ground floor of structures facing pedestrian areas; • inclusion of design elements that visually tie individual buildings and sites together such as walkways, trellises, arcades, landscape, signage, and lighting; and • incorporation of monuments, signs, landscape, and other elements that clearly define entry points and activity locations. <p>The Parkside Specific Plan does not include a business park or an industrial park.</p>					X
<p>1.12.8 The plan includes site and design provisions requiring business and industrial park projects to convey visual interest and character considering:</p> <ul style="list-style-type: none"> • modulation of building volumes, masses, and heights to reduce bulk; • architectural treatment of all visible elevations; • use of consistent and well-designed building and site signage; and • incorporation of extensive landscape along street frontages, common open spaces, and parking lots. <p>The Parkside Specific Plan does not include a business park or an industrial park.</p>					X
1.13 Lake/Water Element					
<p>1.13.1 The plan includes provisions for a lake and/or streams for visual and recreational use that are abutted by commercial, residential, and other appropriate uses:</p> <p>The Parkside Specific Plan does not include a lake or water element. These elements may be included in the Great Park Master Plan by the City.</p>					X
<p>1.13.2 The plan includes provisions to site and design development to take advantage of lake views and access including the location of buildings to “open onto” the lake and/or streams—for example, the Town Center’s public plazas and outdoor restaurants should extend to the waterfront.</p> <p>The Parkside Specific Plan does not include a lake or water element.</p>					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
1.13.2 The plan incorporates pedestrian walkways/promenades and public parks along the lake frontage. The Parkside Specific Plan does not include a lake or water element.					X
1.13.3 The plan provides visual “windows” to the lake from surrounding streets and public places. The Parkside Specific Plan does not include a lake or water element.					X
1.13.4 The plan links residential neighborhoods adjacent to the lake by a network of pedestrian walkways, greenways, and bike paths. The Parkside Specific Plan does not include a lake or water element.					X
1.14.1 The plan includes provisions for one or more golf courses (minimum of 150 acres each) with supporting club houses, retail services, and overnight accommodations within “Residential Neighborhoods.” The Parkside Specific Plan does not include a golf course.					X
1.14 Golf Course(s)					
1.14.2 The plan includes provisions for neighborhood-oriented facilities, such as public meeting rooms, within the club house. The Parkside Specific Plan does not include a golf course.					X
1.14.3 The plan includes provisions for executive homes and other residential uses in concert with golf courses. The Parkside Specific Plan does not include a golf course.					X
1.14.4 The plan integrates golf courses with surrounding residential neighborhoods, orienting supporting facilities and housing to the course. The Parkside Specific Plan does not include a golf course.					X
1.14.5 The plan links adjacent residential neighborhoods to the golf course by a network of pedestrian walkways, greenways, and bike paths. The Parkside Specific Plan does not include a golf course.					X
1.14.6 The plan incorporate ponds and streams that assist in flood control and, as appropriate, are linked to the lake. The Parkside Specific Plan does not include a golf course.					X
1.14.7 The plan provides visual “windows” to the golf course(s) from surrounding streets and public places. The Parkside Specific Plan does not include a golf course.					X

Policy	Fully Complies	Partially Complies	Does Not	S.P Sec. No.	N/A
1.15 Major Community Park - "Village Green"					
1.15.1 Require that a minimum of 160 acres be designated by pertinent sub-area Specific Plans for the development of a "Village Green" that accommodates a diversity of active and passive recreational uses (athletic fields, picnic areas, performance venues, and botanical gardens), and that these lands be dedicated for this purpose through the development approval process. The Parkside Specific Plan includes a portion of the "Great Park" but does not meet the criteria of a "Village Green".					X
1.15.2 Require that the land acquisition and development of the "Village Green" be equitably funded by all development within the New Model Colony. The Parkside Specific Plan "Great Park" is 56.7 gross acres and does not meet the criteria for a "Village Green".					X
1.15.3 Allow the acreage allocated for the development of a "Village Green" to contribute to the total parks and recreational requirement of 5 acres per thousand residents.	X				
1.15.4 The plan uses the "village green" as an organizational element for the siting and design of development, such as the orientation and linkage of multi-family housing and commercial uses to the park (e.g., restaurants "opening" to the park) (see Figure 3-17 of the NMC GPA)	X				
1.15.5 The plan links residential neighborhoods adjacent to the "village green" by a network of pedestrian walkways, greenways, and bike paths.	X				
1.15.6 The plan incorporates ponds and streams as part of the "village green", that are linked to the lake, as appropriate. The Parkside Specific Plan does not include a lake or water element. These elements may be included in the Great Park Master Plan by the City.					X
1.16 Greenways and Open Spaces					
1.16.1 The plan includes provisions for greenway corridors containing pedestrian paths, bicycle trails, and other recreational/open space amenities.	X			4.4	
1.16.2 The plan includes an area-wide greenway network that links all residential neighborhoods, activity centers, and amenities and directly connect to and abut parks and schools as nodes along its length.	X			4.3, 5.1, 5.1.3	
1.16.3 The plan includes provisions for joint use of SCE easement-owned corridors, storm drainage, and other infrastructure rights-of-way for greenways, where feasible and compatible with the intended primary use.	X			7.7.9	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
1.16.4 The plan includes provisions for joint use of SCE fee-owned rights-of-way for greenways, where feasible and compatible with SCE's secondary land use licensing program. The Parkside Specific Plan does not include an S.C.E. corridor.					X
1.16.5 The plan includes provisions for drainage channels to be designed as an open space amenity, maintaining them as natural open landscaped swales where feasible. When concrete channels are used, consider the development of landscaped greenways along their length.	X			7.7.9	
1.16.6 The plan includes provisions for drainage detention basins to accommodate passive and active recreational uses during dry periods, locating these adjacent to and extending local park uses. The Parkside Specific Plan "Great Park" description does not call out a drainage detention basin.	X			5.0	
1.16.7 The plan includes provisions for principal roadways including Grove Avenue, Vineyard Avenue, Archibald Avenue, Haven Avenue, Euclid Avenue, Milliken Avenue, and Edison Avenue to be designed as "parkways" with expanded right-of-ways containing landscaped medians and frontages, pedestrian paths, and other elements.	X			5.1	
1.16.8 The plan incorporates extensive landscaping along street frontages in "urbanized" areas.(I-7 & I-10) The Parkside Specific Plan is not in an urbanized area.					X
1.16.9 The plan includes design provisions for public greenways and open spaces to ensure public safety through the avoidance of physically and visually isolated spaces, maintenance of visibility and accessibility, use of lighting, and other "defensible" space concepts.	X			4.3, 4.4, 5.1, 7.0	
1.17 Electrical Energy Transmission Corridors					
1.17.1 The plan includes pedestrian and bicycle trails in electrical energy transmission corridors to link neighborhoods and districts. The Parkside Specific Plan area does not include an electrical energy transmission corridor nor does is it adjacent to one.	X			4.4	X
1.17.2 The plan accommodates such uses as parking lots, storage, commercial recreation, nurseries, greenways/trails, and complementary and compatible uses in electrical energy transmission corridors in areas designated as "Neighborhood Centers," "Community Commercial," "Town Center," and "Industrial/Business Parks" by the NMC General Plan. The Parkside Specific Plan area does not include an electrical energy transmission corridor nor does is it adjacent to one.					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>1.17.3 The plan accommodates development in electrical energy transmission corridors in areas abutting the Village Green, parks, lake, and other major open space/recreational amenities of uses that promote continuity with the amenity such as extensive landscape, tree farms and nurseries, buffers, and similar uses.</p> <p>_____ The Parkside Specific Plan area does not include an electrical energy transmission corridor nor does is it adjacent to one.</p>					X
<p>1.17.4 The plan accommodates development in electrical energy transmission corridors in areas traversing “Residential Neighborhoods” of uses that serve as transitions among neighborhoods, pedestrian and biking trails, landscaped buffers, recreational facilities, and similar uses that are feasible and compatible with Southern California Edison’s secondary land use licensing program. At the corridor’s intersection with arterials and collectors, consider the accommodation of uses that support the primary residential activity (e.g. parking lots for churches), which shall be designated by the Specific Plan.</p> <p>_____ The Parkside Specific Plan area does not include an electrical energy transmission corridor nor does is it adjacent to one.</p>					X
<p>1.17.5 The plan includes provisions for development located in electrical energy transmission corridors be designed to complement, be compatible with, and promote continuity and cohesiveness with abutting land uses. They should not be designated to create “walls” or barriers among uses.</p> <p>_____ The Parkside Specific Plan area does not include an electrical energy transmission corridor nor does is it adjacent to one.</p>					X
<p>1.17.6 The Plan includes provisions encouraging Southern California Edison Company and private owners to visually improve, respectively, fee-owned and easement-owned rights-of-way that are retained for electrical transmission purposes to ensure their compatibility with abutting uses. This should emphasize the incorporation of landscape, parklands, and greenways.</p> <p>_____ The Parkside Specific Plan area does not include an electrical energy transmission corridor nor does is it adjacent to one.</p>					X
1.18 Phasing					
<p>1.18.1 The plan includes a phasing plan to ensure adequate supporting public services, retail, parks, schools, and other uses are in place to support residential uses. These should establish increments of residential development that “trigger” and cannot be exceeded until the construction of appropriate improvements has been initiated. Standards for schools, infrastructure, and other public services should be determined in collaboration with pertinent service agencies. Standards for local serving retail, parks, and other uses should be confirmed by the City and reflect other policies contained in this Plan.</p>	X			5.8	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
1.18.2 A fiscal impact analysis for the proposed Specific Plan has been prepared describing annual costs and revenues associated with development phasing for at least a ten-year period as input to the definition of land use balance thresholds and limits.	X				
1.18.3 Require that developers commit to the provision of supporting uses and services through Development Agreements, Conditions of Development, bonds, and other appropriate techniques.	X			5.0, 6.12	
2.1 — Agriculture					
2.1.1 The Plan includes provisions recognizing the right of operation including the choice of cultivation procedures, crops or livestock types, crop rotation and all other functions within the traditional scope of agricultural management decisions. These rights and choices shall be exercised in a manner which is consistent with applicable environmental and resource protection policies and regulations. The Parkside Specific Plan land use designation does not include agricultural uses.				Per proj. EIR	X
2.1.2 Adopt and enforce the provisions of the Right to Farm Ordinance and the State nuisance law (California Code Subsection 3482). Such an ordinance would require nonagricultural residents be made aware of the local agricultural operations, their practices, and the potential agriculturally related impacts (noise, odor, etc.). See Appendix A, NMC GPA for a “Right to Farm” Ordinance example. City Initiated, not required for SP’s The Parkside Specific Plan land use designation does not include agricultural uses.					X
2.2.1 The plan discourages the placement of new non-agricultural land uses in such a manner that agricultural fields or parcels become isolated or difficult to economically and conveniently farm. The Parkside Specific Plan land use designation does not include agricultural uses.					X
2.2.2 The Plan encourages the infilling of development in previously urbanized areas or immediately adjacent to existing urban development as an alternative to isolated, “leapfrog” projects. The Parkside Specific Plan land use designation does not include agricultural uses.					X
2.2.3 The Plan discourages the premature parcelization of large holdings which are not infill or urban perimeter properties. The Parkside Specific Plan land use designation does not include agricultural uses.					X
2.3.1 The plan includes provisions to limit premature introduction of urban uses into farming areas that may create the potential for conflict with continued agricultural use. The Parkside Specific Plan land use designation does not include agricultural uses.					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>2.3.2 The plan includes a Transitional Roadway Plan that minimizes farm product transport/farm equipment conflicts with urban use related transport. Said plan includes the following:</p> <ul style="list-style-type: none"> existing routes essential to the transportation of farm products through remaining agricultural areas and through non-agricultural areas as needed to access regional transportation routes; prioritize those roads that will be first to convert to primarily serving urban uses; and roadway signage and markings to inform drivers that farm transport vehicles and machinery may be using the roads. <p>The Parkside Specific Plan land use designation does not include agricultural uses.</p>					X
<p>2.3.3 The plan provides measures to prevent urban runoff flooding and silting from impacting agricultural operations.</p> <p>The Parkside Specific Plan land use designation does not include agricultural uses.</p>					X
<p>2.3.4 Inform new residents and property owners that existing agricultural uses may create nuisances such as flies, odors, dust, noise, night light, and chemical spraying.</p> <p>The Parkside Specific Plan land use designation does not include agricultural uses.</p>					X
<p>2.3.5 The Plan includes provisions to protect agricultural lands from trespass, theft, vandalism, roaming dogs, and comparable impacts from urban uses.</p> <p>The Parkside Specific Plan land use designation does not include agricultural uses.</p>					X
<p>2.4.1 Oppose regulations that impact agricultural operations unless they are justified on the basis of sound environmental concerns.</p> <p>The Parkside Specific Plan land use designation does not include agricultural uses.</p>					X
<p>2.4.2 Assist farmers and agricultural land owners in their efforts to understand and abide by regulations and to process applications for permits and licenses.</p> <p>The Parkside Specific Plan land use designation does not include agricultural uses.</p>					X
3.1 Housing					
<p>3.1.1 Provide for the development of an estimated 20,396 single family and 10,792 multi-family dwelling units to sustain sound economic development (applies to entire 8,069 acre New Model Colony).</p>	X			4.2	
<p>3.1.2 Ensure that the densities and characteristics of residential projects fulfill the intent of applicable land use designations.</p>	X			4.2	
<p>3.2.1 The plan includes provisions for infrastructure needed to support anticipated residential development and ensures the proper integration of all services.</p>	X			5.1, 5.8	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
3.2.2 — Maintain internal consistency among the General Plan’s elements to provide the necessary services and infrastructure for urban development.	X			5.1-5.8	
3.3.1 — The plan allows flexibility in the type of units developed on vacant, residentially designated properties in master-planned communities and other planned developments.	X				
3.3.2 — The plan includes a mix of housing types.	X			4.2	
3.3.3 — The plan encourages the development of residential uses in association with compatible nonresidential uses (i.e. mixed-use).	X			4.2, 4.3	
3.3.4 — The plan provides housing opportunities for upper-income residents by designating sites in selected areas for large-lot development or sites adjacent to significant amenities. — The Parkside Specific Plan promotes the development of high density housing. No land is allocated for large lot development in the Parkside housing program.					X
3.4.1 — The plan includes comprehensive design guidelines and development standards ensuring residential design that is functional, people and pedestrian-oriented, aesthetically pleasing, and contributes to a sense of community through the sensitive arrangement of buildings, open space (public and private), and circulation (vehicular and pedestrian).	X			7.0	
3.5.1 — The plan includes provisions to integrate development with neighborhoods in a way that minimizes adverse impacts on privacy, noise levels, and traffic.	X			5.1	
3.5.2 — The plan includes provisions for physical design elements of development (e.g., scale, street and circulation design, architectural elements) to complement and respect that of surrounding residential neighborhoods.	X			5.1, 6.41	
3.5.3 — Where the new development’s site design characteristics and standards will differ from those within an adjacent neighborhood, use design features (street trees, sidewalk siting and materials, etc.) that are common in both the neighborhoods as measures to transition from the surrounding neighborhood to the new neighborhood. — The Parkside Specific Plan area is not located adjacent to any other existing neighborhoods.					X
3.6 & 3.7 Affordable Housing					
3.6.1 — Designate sufficient vacant land with maximum densities high enough to facilitate the development of housing affordable to lower-income households. — A development agreement with the City of Ontario will be in order that will satisfy all requirements of affordable housing.	X				

Policy	Fully Complies	Partially Complies	Does Not	S.P Sec. No.	N/A
3.7.1 The Plan includes the provisions for affordable to very low, low, and moderate income households. A development agreement with the City of Ontario will is in order that will satisfy all requirements of affordable housing.	X				
3.7.2 The plan implements housing programs that comply with the State of California Housing and Community Development requirements, and ensures compliance and attainment of the regional housing need assessment "affordable" unit target. A development agreement with the City of Ontario will is in order that will satisfy all requirements of affordable housing.	X				
3.8 Housing for Special Needs					
3.8.1 The plan includes provisions for multi-family units specifically designed for the elderly located near Neighborhood Centers that provide access to transit and pedestrian access to amenities, goods, and services (i.e., recreational areas, commercial districts, and medical services). Age restricted housing product, potentially adjacent to the Neighborhood Center, will be selected by the Master Developer or builders at a future date.		X			
3.8.2 The plan includes provisions for rental units specifically designed for students to be located near the educational campus area. The Parkside Specific Plan includes provisions for residential units, but not adjacent to an educational campus.					X
PART II INFRASTRUCTURE AND PUBLIC SERVICE					
5.1 Water Sources					
5.1.3 Require Specific Plans and large development projects to prepare a water system planning study.	X			5.2	
5.1.4 Consider requiring the planning and construction of a dual pipe system to supply reclaimed water throughout the New Model Colony.	X			5.2	
5.2.1 Require new development to construct and dedicate water supply facilities.	X			5.2, 6.13.1	
5.2.2 Designate, preserve, and acquire land, as necessary, for water wells and storage facilities.	X			5.2	
5.3.1 The Plan requires improvements to the water supply facilities necessitated by new development be borne by the new development benefiting from the improvements, either through the payment of fees, or the actual construction of the improvements.	X			5.2	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
6.1 Wastewater System and Treatment					
6.1.1 Enable the planning for and construction of a wastewater system to support new development, as shown in Figure 4-11 of the NMC GPA.	X			5.3	
6.2.1 The Plan requires new developments to connect to the sewer system.	X			5.3	
6.2.2 The Plan includes provisions requiring that sewer capacity and facilities are available before building permits are issued for new development.	X			5.3	
6.3.1 The Plan includes provisions requiring the costs of improvements to the existing wastewater collection facilities necessitated by new development be borne by the new development benefiting from the improvements; either through the payment of fees, or by the actual construction of the improvements.	X			5.3	
7.1 Storm Drainage and Flood Control					
7.1.2 Require Specific Plan and development projects to prepare a storm drainage planning study for the affected drainage area.	X			5.4	
7.1.4 Permit the joint use of detention basins for non-intensive recreational purposes, in accordance with City policy.	X			5.4	
7.1.5 Require that the refined backbone infrastructure plan will be used in the development process to ensure that each project will construct adequate drainage facilities. A detailed drainage master plan must be in place and must have San Bernardino County Flood Control District concurrence prior to any major development approval.	X			5.4	
7.1.6 Strategically locate detention basins to mitigate downstream drainage concern as development occurs. Siting of these facilities will take into consideration potential impacts to flight paths of the crosswind runways of the Chino Airport. Detention basins shall be located as required to bring storm water flows to a safe and acceptable level downstream of Merrill Avenue. However, basins shall not be located within Referral Areas A and B and Safety Zones I and II around the Chino Airport so as to prevent potentially significant wildlife hazards to aircraft operations.	X			5.4	
7.1.7 The plan includes provisions requiring development plans to be consistent with and implement the Master Plan of drainage for the area as finally adopted.	X			5.4	
7.2.1 The Plan requires new development to control surface run-off through onsite measures.	X			5.4	
7.2.2 The Plan requires new development to construct and dedicate flood control and storm drainage facilities.	X			5.4, 6.6	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
7.2.3 Designate, preserve, and acquire land, as necessary, for storm drainage and flood control facilities.	X			5.4	
7.2.6 The Plan requires developers of each proposed project to submit a final drainage plan for the City Engineer's review and approval.	X			5.4, 6.9	
7.2.8 The plan requires developers to recommend measures which ensure that all structures located within the boundaries of the New Model Colony, subject to flooding from 100-year storm events, are constructed on a pad of earth elevated at least one foot above 100-year flood elevations. The recommended measures must be approved, monitored, and enforced by the City Engineer.	X			5.4, 6.6	
7.2.9 The Plan requires developers to provide evidence to the City Engineer that a National Pollutant Discharge Elimination System (NPDES) permit has been obtained from the State Water Resources Control Board (SWRCB) prior to moving construction equipment onto a New Model Colony site. Once obtained, the NPDES permit shall be retained on the construction site throughout the construction period, and a copy shall be filed with the City Engineer.	X			5.4, 6.6	
7.2.10 The plan includes provisions to ensure compliance with all the terms and conditions outlined in the National Pollutant Discharge Elimination System (NPDES) permit, including the implementation of Best Management Practices (BMPs).	X			5.4	
7.2.11 The Plan includes provisions requiring developers to prepare a Storm Water Pollution Prevention Plan (SWPPP) for individual proposed projects prior to the issuance of grading permits. These plans shall be submitted to the City Engineer for review and comment prior to implementing any SWPPP provisions or starting any construction activity. A copy of the SWPPP shall be held by the construction contractor(s) on the construction site throughout development of each project. The City Engineer will monitor and enforce the provisions of the SWPPP.	X			5.4.1	
7.2.12 The Plan includes provisions requiring the developer to prepare a Water Quality Management Plan in compliance with the Regional NPDES Permit requirements for New Development projects. The plan shall be submitted to the City Engineer for review and comment prior to issuance of any grading permits or any construction activity. A copy of the approved WQMP shall be held by the developer at the business office or at the construction site.					
8.1 Schools					
8.1.2 The Plan accommodates sufficient schools to meet School District criteria as shown in Table 4-10 of the NMC GPA. Based on these, the Land Use Plan's buildout will necessitate an estimated 20 elementary school sites, 5 middle school sites, and 3 high school sites within the total New Model Colony, with the number of schools in each planning sub area indicated in Table 3-4 of the NMC GPA. The required number of schools to be constructed may vary due to modifications of the school district's standards to account for demographic changes, the number and types of dwelling units, resident characteristics, and other pertinent criteria.	X			5.5	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>8.1.3 — Locate schools within a designated Neighborhood Center to the extent feasible, as illustrated in Figures 3-8 and 3-9 of the NMC GPA.</p> <p>— The elementary, middle, and high schools planned for in the New Model Colony master plan will serve the Parkside Specific Plan, but currently there are no schools planned within the Specific Plan area, or within the Neighborhood Center in the Specific Plan.</p>					✗
<p>8.1.4 — Encourage schools to be located as neighborhood focal points, with linkage to recreational uses, pedestrian paths, and bike ways, when locating schools in a Neighborhood Center is not feasible.</p> <p>— The elementary, middle, and high schools planned for in the New Model Colony master plan will serve the Parkside Specific Plan, but currently there are no schools planned within the Specific Plan area.</p>					✗
<p>8.1.5 — Encourage the consolidation or location of school campuses adjacent to one another, thereby enabling a joint use of recreational space, library, auditoriums, and other facilities.</p> <p>— The elementary, middle, and high schools planned for in the New Model Colony master plan will serve the Parkside Specific Plan, but currently there are no schools planned within the Specific Plan area.</p>					✗
<p>8.1.6 — Locate schools to avoid impacts attributable to Chino Airport. To ensure efficient accessibility of schools from residential neighborhoods, work with the California Department of Transportation, Division of Aeronautics, and Department of Education to permit schools to be developed at acceptable locations within the Airport's two-mile radius.</p> <p>— The elementary, middle, and high schools planned for in the New Model Colony master plan will serve the Parkside Specific Plan, but currently there are no schools planned within the Specific Plan area.</p>					✗
<p>8.1.8 — Encourage the school districts to design schools so that their periphery maintains an appropriate transition with adjoining residential neighborhoods; avoiding the use of walls, fences, and landscape which establish a visual barrier.</p> <p>— There are no schools within or adjacent to the Parkside Specific Plan area.</p>					✗
<p>8.1.11 — Locate preschool and day-care facilities in appropriate areas throughout the New Model Colony to meet the needs of a growing population. Require specific planned areas to designate sites for day-care facilities.</p>	✗			6.15	
<p>9.1 — Police Protection</p>					

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>9.1.3 The plan identifies appropriate sites for the construction of a police station within the neighborhood Centers, Town Center, or other activity nodes.</p> <p>Police Department services for the Parkside Specific Plan are provided by the City of Ontario. The nearest police station is approximately 2.2 miles northeast of the Specific Plan area.</p>	X			5.5.3	
<p>9.1.4 The plan encourages consolidation of public facilities, thereby creating mini civic centers comprised of police stations, libraries, fire stations, schools, parks, and administrative offices, etc.</p> <p>As apart of the Parkside Specific Plan, the nearest police station (2500 Archibald Ave.) was relocated to be more centrally located to the New Model Colony.</p>	X			5.5.3	
<p>9.2.1 The plan incorporates defensible space designs. These designs should help ensure maximum visibility and security for entrances, pathways, and corridors, as well as open space (both public and private) and parking lots/structures.</p>	X			4.2,6.1 46.15	
<p>9.2.2 The plan includes lighting provisions providing adequate exterior illumination to facilitate security surveillance around commercial, industrial, multi-family, and public structures.</p>	X			6.14, 6.15	
10.1 Fire Protection and Emergency Medical Service					
<p>10.1.2 Require that a total of three fire stations be incorporated within the Neighborhood centers, Town center, or other activity nodes.</p> <p>Fire Department services for the Parkside Specific Plan are provided by the City of Ontario. Currently, the nearest fire station is approximately 2.5 miles northeast of the Specific Plan area, and a new fire station will be built in Planning Area 11.</p>	X			5.5.2	
<p>10.1.3 The plan encourages the consolidation of public facilities thereby creating mini civic centers comprised of police stations, libraries, fire stations, schools, parks, and administrative offices, etc.</p> <p>The Parkside Specific Plan includes a new fire station in Planning area 11, which is adjacent to the commercial component of the plan, and the linear park.</p>	X			5.5.2	
11.1 Circulation					
<p>11.1.1 The plan promotes the timely development of the backbone circulation plan, as shown in Figure 4-14 – NMC GPA that is phased and based on projected needs generated by the implementation of the NMCGPA Land Use Plan and the surrounding uses.</p>	X			5.1	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
11.1.2 Develop a detailed master plan to phase the backbone transportation infrastructure in a logical manner. The master plan should address coordination with each of the Specific Plan's master planning efforts and funding mechanisms including cost sharing and future credits.	X			5.1, 6.12	
11.1.3 Implement hierarchy of roadways and roadway classifications, as shown in Figure 4-15, Figure 4-16, and Figure 4-17 of the NMC GPA , that provides for efficient movement of regional through traffic and also protects the residential neighborhoods from intrusion of through traffic.	X			5.1	
11.1.4 Reserve adequate rights-of-way for roadways to implement the hierarchy of local roads within the New Model Colony that is consistent with the planned land uses.	X			5.1	
11.1.5 The plan establishes safe and efficient truck routes that minimize exposure to noise sensitive land uses and reduce other adverse impacts to adjacent land uses, (see Figure 4-18 – NMC GPA .)	X			5.1	
11.1.6 The plan consider the development of high-capacity, regional traffic serving facilities in the east-west and north-south direction through the New Model Colony, such as Edison Avenue, Euclid Avenue, Archibald Avenue, and Milliken Avenue.	X			5.1	
11.1.7 The plan identifies appropriate intelligent transportation system (ITS) elements to be incorporated into the New Model Colony's transportation infrastructure. The ITS elements include such technologies as changeable message signs, closed circuit television, highway advisory radio, loop detectors, fiber optic communications connections, and links to regional traffic management centers.	X			Per proj.-EIR	
11.1.8 The plan incorporates a transitional Roadway Plan that minimizes agricultural vehicle-urban vehicle conflicts, minimizes agricultural vehicles impact on non-agricultural uses, and transitions the New Model Colony's roadways from agricultural oriented to urban oriented.	X			Per proj.-EIR	
11.1.9 The plan augments and implements the comprehensive Citywide Traffic Model within the New Model Colony area to reflect the New Model Colony land uses and the proposed circulation system.	X			Per proj.-EIR	
11.2.1 The plan maintains a level of service not to exceed LOS D for intersections during the peak hours.	X			Per proj.-EIR	
11.2.2 The plan maintains a peak period level of service not to exceed LOS D for collector and arterial roadways.	X			Per proj.-EIR	
11.2.3 The plan maintains a peak period level of service not to exceed LOS C for residential streets.	X			Per proj.-EIR	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
11.3.1 Coordinate with Riverside County and Caltrans to ensure the timely design and implementation of the Galena Interchange on the I-15 Freeway.	X			Per proj. EIR	
11.3.2 Coordinate with Caltrans and monitor traffic growth around freeway interchanges to determine the need, timing, and design for ramp improvements and additional right-of-way needs at the Route 60 Freeway interchanges.	X			Per proj. EIR	-
11.3.3 Actively support the County of San Bernardino's Congestion Management Plan.	X			Per proj. EIR	
11.3.4 Support and implement locally applicable portions of the Regional Mobility Plan and Air Quality Management Plan.	X			Per proj. EIR	
11.3.5 Coordinate the development of the New Model Colony's circulation plan with adjacent jurisdictions and regional agencies, including City of Chino to the west and Riverside County to the east and south, for proper future connectivity and service to regional trips.	X			Per proj. EIR	
11.3.6 Specific Plans for proposed developments within the NMC shall not be approved and adopted until such time as the San Bernardino County Congestion Management Program Traffic Impact Analysis (CMP TIA) for the New Model Colony General Plan Amendment has been completed and approved. This CMP TIA shall identify improvement measures to mitigate impacts of the traffic generated by the New Model Colony on transportation systems in the City of Ontario, the unincorporated San Bernardino County and all neighboring cities, which fall within the five-mile radius (designated impact area) of the New Model Colony boundaries, per CMP TIA guidelines.	X			Per proj. EIR	
11.3.7 The Plan includes a separate traffic and circulation studies to: <ul style="list-style-type: none"> • Assess internal circulation system need and to develop a traffic circulation plan for the Specific Plan area; • Identify regional transportation infrastructure connectivity requirements; and • Identify specific traffic impacts related to the buildout of the Specific Plan on the surrounding areas (outside the Specific Plan) and to identify traffic improvement measures to mitigate these impacts. 	X			Per proj. EIR	
11.3.8 Circulate all relevant traffic and circulation studies prepared for proposed Specific Plan developments within the New Model Colony area to all neighboring local jurisdictions that may be potentially impacted by the traffic generated from the buildout of the Specific Plan's land uses.	X			Per proj. EIR	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>11.3.9 For the following arterial segments outside the New Model Colony:</p> <ul style="list-style-type: none"> • Euclid Avenue – Riverside Drive to Philadelphia Street • Grove Avenue – Saint Andrews Street to 4th Street • Vineyard Avenue – Riverside Drive to Route 60 • Archibald Avenue – Riverside Drive to Philadelphia Street • Haven Avenue – Creekside to Route 60; Mission Boulevard to Airport Drive • Milliken Avenue – Riverside Drive to Mission Boulevard <p>Provide additional through-traffic lanes by:</p> <ul style="list-style-type: none"> • widening the roads to accommodate the ultimate number of lanes designated in the General Plan; • modifying functional classification for arterials to accommodate additional traffic lanes; • restriping; and • using parking restrictions and other measures, where feasible. 	X			Per proj. EIR	
<p>11.4.1 Require each major development phase or Specific Plan to develop a master plan of streets and conduct a comprehensive traffic impact study, as appropriate.</p>	X			5.1, Per proj. EIR	
<p>11.4.2 As condition of development for each planning area, require that proper connecting facilities and capacity be provided to the regional transportation system to minimize impacts to existing roadways.</p>	X			5.1, Per proj. EIR	
<p>11.4.3 Require that development within the New Model Colony be consistent with the provisions of the County-wide Congestion Management Program.</p>	X			4.2, per proj. EIR	
<p>11.4.4 The plan prohibits future development from providing backout driveways onto arterial roadways.</p>	X			4.2, 5.1	
<p>11.4.5 The plan discourages future development from providing backout driveways onto collector roadways.</p>	X			4.2, 5.1	
<p>11.4.6 The plan considers the use of alley ways (20 feet recommended width) to add flexibility to frontage designs and assist in the creation of more pedestrian oriented front areas.</p>	X			4.2, 5.1	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>11.4.7 The plan promotes development of local street patterns that create and unify neighborhoods, rather than divide them, through the following means:</p> <ul style="list-style-type: none"> • local street patterns should provide access between subdivisions within a neighborhood, with the exception of through traffic which should be directed onto major and secondary arterials; and • the local street system should be logical and understandable for the user. Creation of circuitous and confusing travel paths between internal neighborhood areas and adjacent arterials should be avoided. 	X			4.2, 5.1	
11.4.8 The plan includes measures requiring developers to provide all required onsite infrastructure and contribute their proportional share to offsite improvements. (I-30, I-31, and I-32)	X			5.1-5.8	
11.4.9 The plan includes an impact fee system in order to offset the cost of transportation required by new development.	X			Per City-DIF	
11.4.10 Consider whether it is appropriate to establish a transportation benefits district. The Parkside Specific Plan may not be a candidate for a transportation benefits district.	X			Per proj.-EIR	
11.5.1 The plan includes a comprehensive Transportation Mobility Plan to improve the movement of pedestrians, bicyclists, public transit, truck and automobile, freight, passenger rail, etc., within the New Model Colony.	X			4.2, 5.1	
11.5.2 The plan integrates the Transportation Mobility Plan with the Congestion Management Planning process to identify and develop necessary transportation services, and assist land use and urban design decision making.	X			Per proj.-EIR	
11.5.3 The plan promotes a variety of transportation modes.	X			4.2, 4.4, 5.1	
11.5.4 The plan explores the possibility of developing transit centers within the appropriate Specific Plan areas such as the Town Center, educational campus, and adjacent to the Regional Centers along Milliken Avenue and Euclid Avenue. The Parkside Specific Plan does not include a Town Center, and educational campus, or a Regional Center.					X
11.5.5 Analyze cost/benefit ratios in determining future publicly financed transportation improvements. A feasibility study has not been done for financing transportation improvements.					X
11.5.6 Consult and coordinate with the various transit agencies in order to bring about improved transit service to the New Model Colony.	X			Per proj.-EIR	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
11.5.7 Through coordination with transit agencies, the plan provide transit friendly design with the objective of maximizing the potential for transit use.	X			Per proj.-EIR	
11.5.8 The plan includes provisions requiring development to fund transit facilities, such as transit centers, bus shelters and turnouts, where feasible.	X			Per proj.-EIR	
11.5.9 The plan includes provisions requiring that development provide for transit-friendly features such as buildings located close to streets, bus amenities, etc.	X			4.2	
11.5.10 The plan encourages the development of connections with commuter rail stations in the vicinity of the New Model Colony. There are no commuter rail stations in the vicinity of the Parkside Specific Plan area.					X
11.6.1 The plan encourages nonresidential developments to provide employee incentives for using alternatives to the conventional single occupant automobile (i.e., carpools, vanpools, buses, bicycles, walking, telecommuting, etc.).	X			Per proj.-EIR	
11.6.2 The plan supports national, state, and regional legislation directed at encouraging the use of carpools and vanpools.	X			Per proj.-EIR	
11.6.3 The plan encourages the establishment of Transportation Management Associations (TMAs) within the commercial and industrial areas of the New Model Colony. The Parkside Community Plan commercial area is not large enough to merit a TMA.					X
11.6.4 The plan promotes the use of alternative transportation modes to the single occupancy vehicle within the New Model Colony, such as carpooling, bus and rail transit and explore the feasibility of linking such systems with other regional transportation systems.	X			4.2, 5.1	
11.6.5 The plan promotes ridesharing through publicity and provision of information to the public.	X			Per proj.-EIR	
11.6.6 The plan encourages the preservation and development of freight and passenger/commuter rail that occur or would occur in the region. The Parkside Specific Plan region does not include a freight or passenger commuter rail.					X
11.6.7 The plan encourages a telecommunications center within the planning area to reduce vehicle miles traveled.	X			5.6, 6.14	

Policy	Fully Complies	Partially Complies	Does Not	S.P Sec. No. Per proj. EIR	N/A
11.6.8 Develop a program for traffic signal coordination and synchronization for the arterial street system in coordination with and connecting to the City's signal system.	X				
11.7.1 The plan requires parking to be consolidated along parkways, arterials, and pertinent collectors, eliminating the number of ingress and egress points. Prime candidate locations for consolidated parking are the "Neighborhood Centers", "Town Center", "Business Parks", and "Regional Centers". (I-7 and I-10)	X			4.2, 5.1	
11.7.2 The plan requires that sufficient and secure bicycle parking be provided in all parking areas.	X				
11.7.3 The plan promotes sharing of off-street parking facilities.				6.15	
PART III AESTHETIC, CULTURAL, OPEN SPACE & RECREATIONAL RESOURCES					
<i>12.1 Parks & Open Space</i>					
12.1.1 Provide a minimum 509 acres of parkland consisting of a mix of mini, neighborhood, community and "village green" facilities to achieve a standard of 5 acres of parkland per 1,000 residents. The actual park acreage may vary as a result of future modifications of the City standards.	X			4.3	
12.1.2 Update the Parks and Bike Trail Master Plan to provide specific criteria and guidelines for the siting, design and programming of parks and recreational facilities. Consider the following when creating the criteria for acquiring or receiving dedicated parkland: <ul style="list-style-type: none"> • The usability of proposed parklands and the ease of accessibility for future users; and • Balance of passive and active recreational opportunities and facilities designed to meet the existing and future needs of all user groups. The Parkside Specific Plan does not include an update to the Parks and Bike Trail Master Plan.					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>12.1.3 The plan incorporate a comprehensive and unified parks and recreation plan that:</p> <ul style="list-style-type: none"> • identifies mini, neighborhood, and community park sites in accordance with the service standards and updated Parks and Bike Trail Master Plan criteria; • integrates neighborhood parks with Neighborhood Centers and schools (see Figure 5-2); • links parks by pedestrian greenway and bike trail networks (see Figure 5-3); • incorporates passive and active recreational uses as specified in the Parks and Bike Trail Master Plan; and • defines a park acquisition and improvement financing plan. (I-3 and I-34) 	X			4.2 4.3 4.4 5.1 5.1.3 5.1.4	
<p>12.1.4 The plan encourages:</p> <ul style="list-style-type: none"> • golf course site(s) be developed in the New Model Colony; The Parkside Specific Plan does not include a golf course. • school and park facilities be located in a manner that permits shared use of auditoriums, playing fields, and so on; • public facilities (auditoriums, amphitheaters, day-care centers, public meeting rooms) be integrated into park design; and The Parkside Specific Plan does not include public facilities. • drainage channels be developed as an open space amenity. (I-3, I-7, and I-47) 	X			4.1, 4.3 5.4	X X
<p>12.1.5 The plan requires Sub area 22 (Town Center) Specific Plan to locate park(s) and/or public open space along the lake, if the lake is developed. The Parkside Specific Plan does not incorporate park/public open space by a lake.</p>					X
<p>12.1.6 The Plan requires that Sub area 16 and Sub area 17 Specific Plans identify sites for the development of a linear “village green” park. The Village Green will act as a recreational focal point serving the entire New Model Colony. The Village Green shall incorporate a diversity of passive and active uses including athletic fields, picnic areas, botanical gardens, museums, and performance venues.</p>	X			4.3.2, 4.3.3	
<p>12.1.7 The plan requires that residential neighborhoods located next to a park provide pedestrian greenway and bike trails to the park.</p>	X				

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>12.1.8 The plan encourages storm drainage detention basins to be designed to accommodate passive and active recreational uses during the dry periods (see Figure 5-4 NMC GPA).</p>	X			5.4	
<p>12.1.9 The plan requires the use of extensive landscaping along street frontages. Landscaping elements should include tree and plant specimens currently found within the New Model Colony. (I-7 and I-10)</p>	X			5.1, 7.0	
<p>12.1.10 The plan requires the dedication and development of future community and neighborhood park sites before a significant proportion of the new population to be served by the park exists. (I-7 and I-10)</p> <p>———— The Parkside Specific Plan provides a guideline for the development of the park sites located within the plan area, and the remaining specific plan area is dedicated for other land uses and therefore could not be dedicated for future park sites.</p>					X
<p>12.1.11 The plan requires that new multi-family residential developments of five or more units provide recreational or open space facilities onsite and contribute appropriate fees that aid in the public development of other facilities to offset additional demands generated by their resident population. (I-7 and I-10)</p>	X			4.2, 4.3	
<p>12.1.12 The plan requires that large-scale commercial developments, such as the Town Center, Community Centers, Regional Centers, provide open space facilities within the development for passive or active recreation or contribute fees for the public development of such uses. (I-7 and I-10)</p> <p>———— The Parkside Specific Plan does not include a Town Center, a Community Center, or a Regional Center.</p>					X
<p>12.1.13 The plan encourages industrial and business parks to incorporate employee recreational facilities within their developments or contribute fees for the public development of such uses. (I-7 and I-10)</p> <p>———— The Parkside Specific Plan does not include an industrial park or a business park.</p>					X
<p>12.1.14 The plan encourages agricultural industry representatives and local university (ies) to provide educational and recreational programs that reflect the New Model Colony's agricultural history. (I-50)</p> <p>———— The Parkside Specific Plan does not specify any programs to reflect the New Model Colony's agricultural history.</p>					X

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
13.1.1 The plan incorporates a comprehensive network of greenways, pedestrian paths, and bike trails (see Figures 5-3 and 5-5 – NMC GPA), linking the Town Center, educational campus (if developed), Regional Centers, Community Commercial, Neighborhood Centers, “Village Green,” neighborhood parks, and schools. (I-7 and I-10)	X			4.3.2	
13.1.2 Require developments to link their internal trail system with the surrounding New Model Colony network.	X			4.4, 5.1.3, 5.1.4	
13.1.3 The plan encourages infrastructure rights-of-ways or easements to be designed and developed to accommodate trails and bikeways where feasible and where compatible with the intended primary use. (I-47)	X			4.4	
13.1.4 The plan includes provisions providing for joint use of SCE easement-owned rights-of-way for trails and bikeways where feasible and compatible with the intended primary use. (I-10 and I-47) The Parkside Specific Plan area does not provide for joint use of an SCE easement; none exists.					X
13.1.5 The plan includes provisions providing for joint use SCE’s fee-owned rights-of-way for trails and bikeways where feasible and compatible with SCE’s secondary land use licensing program. (I-10 and I-47)	X			4.4	
13.1.6 Work with the surrounding jurisdictions to ensure trail connections between the New Model Colony and other regional recreational destinations or amenities. (I-51)	X			4.3.2, 4.4	
14.1.1 The plan includes measures to extensively landscape Euclid Avenue, Grove Avenue, Vineyard Avenue, Archibald Avenue, Milliken Avenue, and Edison Avenue by the use of “enhanced parkways” with landscaped medians and rights-of-way (see Figure 5-3 – NMC GPA). (I-7 and I-10)	X			5.1	
14.1.2 The plan requires that comprehensive street tree and landscape plans be established to uniquely identify parkways, neighborhoods, centers, and districts. (I-7)	X			Per city Master Plan	
14.1.3 The plan requires that the landscaping within the public medians and rights-of-way as well as along the private developments’ street frontage is well designed and properly maintained, minimizes water usage, and maximizes visual continuity while permitting individual expression. (I-25)	X			6.13.1	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
14.1.4 The plan requires that the individual developments transition their landscaping with the neighboring properties' landscaping, thereby enhancing visual continuity along streets. (I-7)	X			7.0	
14.1.5 The plan requires that view corridors be provided from public places towards the San Gabriel Mountains, where possible. (I-7)		X		4.2	
15.1 Historic and Cultural Resources					
15.1.1 Validate the location of pending historical resources. (I-10)	X			Per proj. EIR	
15.1.2 The plan includes provisions requiring all development projects, including infrastructure construction, to conduct Phase I surveys and resource investigations in accordance with CEQA requirements to determine if additional cultural, historic, or archaeological resources are located within their area. If the development project involves the use of federal funds, conduct the study in accordance with federal requirements. File all site records, survey reports, etc. with the San Bernardino County Museum Archaeological Information Center. (I-10)	X			Per proj. EIR	
15.1.3 The plan includes provisions requiring all development projects to conduct Phase II evaluation studies (for archaeological resources and standing structures in accordance with CEQA requirements) if necessitated by the Phase I studies' findings. If the development project will involve the use of federal funds, conduct the study in accordance with federal requirements. File all site records, survey reports, etc. with the San Bernardino County Museum Archaeological Information Center. (I-10)	X			Per proj. EIR	
15.1.4 The plan includes provisions requiring development projects to propose avoidance or preservation plans for any archaeological or historic site, as necessary. (I-10)	X			Per proj. EIR	
15.1.5 The plan includes provisions require new development to be compatible with adjacent archaeological sites or historical structures in terms of buffering, scale, massing, and building materials, as necessary. (I-10)	X			Per proj. EIR	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
15.1.6 Conduct an intensive archaeological field survey under the supervision of a Society of Professional Archaeologists (S.O.P.A.) certified archaeologist for each proposed project not previously surveyed within the past ten years. A technical report following format and content guidelines proposed by the Office of Historic Preservation must be completed. (I-10)	X			Per proj. EIR	
15.1.7 The plan includes provisions requiring that each project resulting in an adverse impact on a known significant resource prepare an appropriate planning approach to reduce the impact to a level of insignificance. (I-10)	X			Per proj. EIR	
15.1.8 The plan includes provisions requiring that a qualified archaeologist be retained in order to monitor the grading, identify any cultural resources that may be exposed, complete a preliminary evaluation of the resource, and recommend appropriate resource management for the treatment of the resource. (I-10)	X			Per proj. EIR	
16.1 Libraries					
16.1.1 The plan includes provisions requiring that library site(s) be developed within the Neighborhood Centers and Town Centers, and near transit facilities, bike paths, and pedestrian paths. (I-7)	X			Per proj. DIF	
16.1.2 The plan encourages consolidation of public facilities thereby creating “mini civic centers” comprised of police station(s), library(ies), fire stations, schools, parks, and administrative offices, etc. (I-7) The Parkside Specific Plan does not include any public facilities (with the exception of the park and the fire station).					X
16.1.3 Construct a minimum of 61,107 square feet library facilities in order to provide the estimated 101,845 New Model Colony residents with adequate library facilities based on the City’s current standard of 0.6 square feet per resident, or as modified to reflect technology. (I-10 and I-30)	X			Per proj. DIF	
16.1.4 The plan encourages the installation of new technology including a fiber optic network for telecommunications. (I-22)	X			5.6	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>16.1.5 The plan encourages the joint use of school libraries and public library facilities. (I-47)The Parkside Specific Plan does not include a library or a school.</p>					X
<p>PART IV NATURAL RESOURCES</p>					
<p>17.1 Aggregate Resources</p>					
<p>17.1.1 Identify the locations of aggregate resources and coordinate their location with the appropriate land use designations. (I-19 and I-23)</p>	X			Per proj. EIR	
<p>18.1 Biological Resources</p>					
<p>18.1.1 Work with and encourage the Army Corps of Engineers, Chino Basin SAPA, and County Flood Control to retain a minimum of 160 acres of surface water in the El Prado Basin. This water shall serve as the habitat mitigation for waterfowl to offset the discontinued use of State mandated dairy manure water retention basins. (I-36 and I-47)</p>	X			Per proj. EIR	
<p>18.1.2 Ensure that development projects proposed for areas that support and retain waterfowl and raptor habitat:</p> <ul style="list-style-type: none"> • incorporate fences, walls, vegetative cover, or other measures to buffer waterfowl areas from the built environment; • locate and design roads and utilities in such a manner that they will not conflict or impact habitat areas; and • permit only the following uses: <ul style="list-style-type: none"> • educational and research facilities (excluding buildings); • passive (non-motorized, non-consumptive) recreation; • trails on public land(s) if located outside of a undeveloped buffer zone; • fish and wildlife management activities; and • flood control projects where no other viable methods are available to protect the public safety. (I-7 and I-10) 	X			Per proj. EIR	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>18.1.3 The plan includes measures to ensure that development projects that incorporate onsite mitigation for the loss of existing waterfowl and raptor habitat:</p> <ul style="list-style-type: none"> • are reviewed by a City-approved biologist who conducts an onsite assessment to determine whether the mitigation is sufficient based upon the quality, location, or size of the area to be lost; • include a Biological Assessment Report that addresses the proposed project's impact on state and federally listed and candidate plants and animals; California Department of Fish and Game Special Animals; waterfowl or raptor habitat and any other special interest species or communities identified in the General Plan Analyses of Existing Conditions and Trends Report, or those hereafter named by state or federal trustee agencies; • assess project conformance to specific habitat objectives described above; • identify mitigation measures necessary to eliminate significant adverse impacts to sensitive biological resources; and • define a program for monitoring and evaluating the effectiveness of the specified mitigation measures. (I-7 and I-10) 	X			Per proj. EIR	
<p>18.1.4 Periodically update the Biological Habitat Map (see Figure 6-5 – NMC GPA) and data base to maintain current information regarding waterfowl and raptor habitat. (I-6 and I-41)</p>	X			Per proj. EIR	
<p>18.1.5 The plan includes sufficient technical data to enable an adequate assessment of the potential for impacts on biological resources. Such technical data shall include species lists, habitat use, acreage of habitat, and descriptions of any vegetation. (I-7 and I-10)</p>	X			Per proj. EIR	
<p>18.1.6 The plan assesses impacts on local biological resources and recommend appropriate mitigation measures, if necessary to account for specific development characteristics or site conditions that are not adequately addressed by the New Model Colony General Plan Environmental Impact Report. (I-7 and I-10)</p>	X			Per proj. EIR	
<p>18.1.7 The plan includes provisions encouraging development plans take all reasonable measures to avoid creating significant impacts. (I-7 and I-10)</p>	X			Per proj. EIR	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
18.1.8 The plan includes provisions encouraging development plans take all reasonable measures to avoid altering the biological integrity. Reasonable measures may include but are not limited to vegetative restoration, control of alien plants and animals, and use of landscaping buffers. (I-7 and I-10)	X			Per proj. EIR	
18.1.9 The plan includes provisions for enhancing the ponded areas adjacent to the flood control channel (i.e., planting native trees to serve as buffers). (I-36)	X			Per proj. EIR	
18.1.10 The plan includes provisions requiring dedicated open space easements as a condition of approval for development projects proposed adjacent to waterfowl and raptor habitat areas that are voluntarily retained in the New Model Colony. These open space easements shall assure that the biological habitats are buffered and protected from degradation and shall be used only for passive recreational/ educational purposes. (I-7 and I-10)	X			Per proj. EIR	
18.1.11 Ensure that development projects located adjacent to waterfowl or raptor habitat areas that are voluntarily retained in the New Model Colony: <ul style="list-style-type: none"> Maintain a 100-foot buffer (minimum) from the edge of the habitat on the subject property; maintain connection to other preserved habitats, where they exist; and dedicate open space easements to the City, homeowner associations, or other responsible party. (I-7 and I-10) The Parkside Specific Plan area does not contain waterfowl or raptor habitat.	X			Per proj. EIR	
18.1.12 Establish an onsite "Wildlife Habitat" or Waterfowl and Raptor Conservation Area (WRCA) in the areas adjacent to the Chino Basin Flood Control Ponds located to the south of Chino Avenue, west of Archibald Avenue, north of Scafer and east of Whispering Lakes Lane consisting of approximately 145 acres adjacent to the 85 acres of existing county-owned detention basins. (I-6) Refer to p. 6-27, NMCGPA for complete discussion on the two approaches to establishing the WRCA.	X			Per proj. EIR	
18.2 Recovery of Delhi sands flower-loving fly					
18.2.1 Monitor the efforts of the Department of Fish and Wildlife to study the recovery of the Federally listed Delhi sands flower-loving fly. Assess the impacts of recovery recommendations for development in the New Model Colony. (I-5 and I-47)	X			Per proj. EIR	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>18.2.2 The City shall cooperate with the U.S. Fish and Wildlife Service in the following ways to mitigate potential impact to the DSFLF:</p> <ul style="list-style-type: none"> • cooperate with USFWS to ensure, through the Specific Plan Process, that potential recovery areas are appropriately mitigated; • establish a standard for buffers for protecting DSFLF restoration areas, in cooperation with USFWS; and • work with rights-of-way owners and USFWS to explore the possibility of creating DSFLF habitat within these undeveloped strips. (I-5 and I-47) 	X			Per proj. EIR	
<i>18.3 Increased public awareness of local biological resources</i>					
<p>18.3.1 Sponsor or co-sponsor “balanced” educational programs that:</p> <ul style="list-style-type: none"> • promote awareness of local biological resources; • inform about protection and preservation programs; • foster community attitudes and behaviors that help protect local plants and wildlife; and • encourage community involvement in protection programs. (I-50) 	X			Per proj. EIR	
PART IV HAZARDS					
<i>19.1 Geologic and Seismic Hazards</i>					
<p>19.1.1 The Plan includes requirements to validate the presence of unstable NMCIs, areas of liquefaction potential, unique geological features, and shallow water tables.</p>	X			Per proj. EIR	
<p>19.2.1 Identifies the location and type of utilities within the area and identifies emergency procedures in case of breaks, rupture, or other disruption.</p>	X			Per proj. EIR	
<p>19.2.2 Identifies the location and type of utilities within the area and identify emergency procedures in case of breaks, rupture, other disruption.</p>	X			Per proj. EIR	
<p>19.3.1 Determine portions of the New Model Colony are subject to subsidence.</p>	X			Per proj. EIR	
<p>19.3.2 Update planning (including setbacks requirements), building (including design considerations), grading, and engineering requirements, as necessary, to protect life and property located near potential subsidence and fissure zones.</p>	X			Per proj. EIR	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
20.1 — Protection from Fire Hazards					
20.1.1 — Requires two viable points of ingress and egress from development projects.	X			4.2, 5.1	
20.2.1 — The Plan includes procedures to ensure that manure storage practices are not conducive to the generation of fires due to spontaneous combustion.	X			Per proj. EIR	
21.1 — Dust, Wind and Blow Sand					
21.1.1 — The Plan requires that structures be sited and designed to prevent adverse funneling of wind onsite and on adjacent properties.	X			Per proj. EIR	
21.2.1 — The Plan requires provisions to construction for adherence to NMCI Erosion Control Area or City-mandated dust control programs.	X			Per proj. EIR	
21.2.2 — The Plan requires that technical information and mitigation methods examples, regarding wind blown sand, available to the general public.	X			Per proj. EIR	
21.2.3 — The Plan provides provisions to coordinate with other jurisdictions to maximize wind blown sand control planning.	X			Per proj. EIR	
22.1 — Soils					
22.1.1 — The Plan requires that geologic deposit technical information, erosion potential identification methods, and erosion control examples are made available to developers.	X			Per proj. EIR	
22.1.2 — The Plan requires that NMCI engineering problem information, identification methods, and mitigation method examples are made available to developers.	X			Per proj. EIR	
22.1.3 — The Plan includes provisions requiring proposed development projects to determine if the project would be located in or near areas with significant erosion potential or NMCI engineering problems. Require proposed project applications to include a detailed discussion regarding the types of NMCI and locations, erosion potential or NMCI engineering problems, and erosion control plans. Mitigation plans must address methods to be used during all phases of project development, implementation, and operation.	X			Per proj. EIR	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
22.1.4 The Plan requires establishment and implementation of a procedure to be used to evaluate the suitability of developer proposed erosion control methods.	X			Per proj. EIR	
22.1.5 The Plan includes provisions requiring development applicants to certify that all deleterious materials, particularly organic residue from dairy, farming, or agricultural activity, have been removed, properly disposed, and will not impact the development during the project's life.	X			Per proj. EIR	
22.1.6 The Plan requires creation (pulled together from existing materials) of a Grading and Geotechnical Investigation Standards manual that will be available to developers and consultants in order to ensure the minimum proper NMCIs engineering and engineering geologic study for all sites where grading will occur. Together these standards and policies should effectively mandate proper studies before development approval, in which grading, foundations, and slope stability would be analyzed and any potential hazards identified. Mitigation of the potential hazards would occur through the proper application of recommendations arising from these studies. Topics shall include but not necessarily be limited to NMCIs engineering and foundations, slope stability, erosion, liquefaction/dynamic settlement, shallow groundwater, and fault location/activity. This manual shall be available at the permit stage prior to initial feasibility and design studies in order to enhance (streamline) the development review and environmental review processes.	X			Per proj. EIR	
22.2.1 Determine if the manure stockpiles have the potential to create a nuisance, structural, or groundwater contamination problems. If so, require proper mitigation.	X			Per proj. EIR	
23.1 Hazardous Materials & Waste					
23.1.1 Require phase I Environmental Assessments for the presence of hazardous materials prior to the demolition of any buildings or the construction of new development on any properties within the New Model Colony. If hazardous materials are found, implement measures for their safe removal or containment, meeting applicable regulatory standards, prior to demolition of affected structures and/or construction on the affected areas.	X			Per proj. EIR	
23.1.3 Require projects to comply with policies set forth in the City of Ontario General Plan regarding the handling, transporting, treating generating and storage of hazardous materials.	X			Per proj. EIR	
24.1 Noise					
24.1.1 Monitor and update available data regarding the community's existing and projected ambient and stationary noise levels as show on Figures 7-4 and 7-6 of the NMC GPA.	X			Per proj. EIR	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
24.2.1 Site housing, health care facilities, schools, libraries, religious facilities, and other “noise sensitive” land uses in areas where existing or future noise levels are below an Ldn of 60 (dB(A) exterior and an Ldn of 45 dB(A) interior limits.	X			4.2, Per proj. EIR	
24.2.2 The Plan permits the locations of commercial, industrial, and recreational uses in areas where the ambient noise levels exceed an Ldn of 60 dB(A), if they area able to reduce the interior noise levels to within the 45 dB(A) Ldn level.	X			4.2	
24.2.3 The Plan requires that <ul style="list-style-type: none"> • Dining and other high noise-generating entertainment uses in commercial areas are not located directly adjacent to any residences, schools, health-care facilities, or other noise sensitive” land-uses; and • The entertainment and restaurant/bar uses’ outdoor dining areas, waiting areas, entries, and kitchens are located in such a manner that the noise levels emanating from these uses are not discernible from ambient noise levels at a distance of fifty (50) feet from the establishment in which it is being-conducted or within ten (10) feet of any residence (whichever is more restrictive). 	X			6.15	
24.2.4 The Plan requires that the facilities for active recreational uses that are likely to draw cheering-crowds, elicit loud play, or have amplified game announcements (i.e., stadiums, soccer fields, tennis-courts, basketball courts, etc) be located within the park’s interior and away from surrounding-residential and “noise sensitive” uses. The Parkside Specific Plan does not include any active recreational uses that would draw cheering-crowds.					X
24.2.5 The Plan requires educational, recreational, commercial, and industrial land uses (including-educational campuses, parks, stadiums, and public event facilities) be designed in such a manner-that: <ul style="list-style-type: none"> • Locate and orient vehicle access points away from residential and/or noise sensitive parcels. • Locate loading and shipping facilities away from adjacent noise sensitive uses; • Incorporate structural building materials that mitigate sound transmission; • Minimize the use of outside speakers and amplifiers; • Configure interior spaces to minimize sound amplification and transmission; and • Incorporate fences, walls, landscaping and other noise buffers and barriers between incompatible-uses, as appropriate. 	X			Per proj. EIR	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<p>24.2.6 The Plan requires mixed-use buildings:</p> <ul style="list-style-type: none"> Be constructed soundly to prevent adverse noise transmission between differing uses or tenants located in the same commercial structure and individual dwelling units in multi-family residential structures; and Designed for concurrent commercial and residential uses to minimize to the greatest degree practicable (through design and construction techniques) the transfer or transmission of noise and vibration from the commercial land use to the residential land use; <p>24.2.7 The Plan requires that, prior to the issuance of grading permits, an Acoustical Analysis Report be submitted to the City Engineer by the project developer. The report shall:</p> <ul style="list-style-type: none"> Describe the cumulative effect of road noise on surrounding land uses and recommend mitigation measures; describe in detail the interior and exterior noise levels for residential uses on the site and the specific design and mitigation features to ensure compliance with the City’s noise criteria of 65 dB(A) CNEL for outdoor living areas and 45dB(A) CNEL in habitable rooms; specify the noise barriers’ height, location, and types capable of achieving the desired mitigation affect; and identify those residential lots that may require mechanical ventilation to achieve interior noise standards. When the operable doors and windows are open for homes facing these roadways, the interior 45 dB(A) CNEL interior noise limit for these units may be exceeded. Therefore, a “windows closed” condition may be required for these units. Any proposed mechanical ventilation must meet the requirements of the Uniform Building Code (UBC) standard. It should be noted that the windows facing some roadways may be openable windows, but the homeowners would have the option to close the windows and still obtain adequate ventilation through the use of a mechanical ventilation system. This mechanical ventilation system shall supply two air changes per hour to each habitable room, including 20 percent (one-fifth) fresh make-up air obtained directly from the outdoors. The fresh air inlet duct shall be of sound attenuating construction and shall consist of a minimum of ten feet of straight or curved duct or six feet plus one sharp 90 degree bend. The City Engineer shall ensure that the Acoustical Analysis Report identifies any requirements for mechanical ventilation for individual onsite residential units. (I-7 and I-10) 	<p>X</p> <p>X</p>			<p>Per proj. EIR</p> <p>Per proj. EIR</p>	
<p>24.2.8 The Plan requires that all prospective owners and occupants of residential units be formally notified prior to purchase, lease, or rental, that certain units (without windows and doors closed), and outdoor areas could be subject to noise levels above City standards for residential uses. Such notification shall be in language approved by the City Planning Department, and shall be formalized in written Covenants, Conditions, and Restrictions (CC&Rs) recorded on the title of each residential lot in the project. In addition, each advertisement, solicitation, and sales brochure or other literature regarding the project shall contain the approved notification language. (I-7 and I-10)</p>	<p>X</p>				

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
24.2.9 The Plan limits construction in the New Model Colony to the hours of 7:00 AM to 7:00 PM Monday through Saturday, and prohibited on Sundays and Federal holidays. (I-7 and I-10)	X			Per proj. EIR	
24.2.10 The Plan requires all project construction vehicles or equipment, fixed or mobile, be equipped with properly operating and maintained muffler. (I-7 and I-10)	X			Per proj. EIR	
24.2.11 The Plan requires that stockpiling and/or vehicle staging areas be located as far as practical from existing residential units on and off the project site. (I-7 and I-10)	X			Per proj. EIR	
24.2.12 The Plan requires to, whenever feasible, schedule the noisiest construction operations to occur together to avoid continuing periods of the greatest annoyance. (I-7 and I-10)	X			Per proj. EIR	
25.1 Air Quality					
<p>25.1.1 Continue to enforce the following:</p> <ul style="list-style-type: none"> • During all construction activities, construction contractors shall use low emission mobile construction equipment where feasible to reduce the release of undesirable emissions. • During all construction activities, construction contractors shall encourage rideshare and transit programs for project construction personnel to reduce automobile emission. • During all grading and site disturbance activities, construction contractors shall water active sites at least twice a day, and clean construction equipment in the morning and/or evening to reduce particulate emissions and fugitive dust. • During all construction activities, construction contractors shall, as necessary, wash truck tires leaving the site to reduce the amount of particulate matter transferred to paved streets as required by SCAQMD Rule 403. • During all construction activities, construction contractors shall sweep on and offsite streets if silt is carried to adjacent public thoroughfares, as determined by the City Engineer to reduce the amount of particulate matter on public streets. • During all construction activities, construction contractors shall limit traffic speeds on all unpaved road surfaces to 15 miles per hour or less to reduce fugitive dust. • During grading and all site disturbance activities, at the discretion of the City Planner, construction contractors shall suspend grading operations during first and second stage smog alerts to reduce fugitive dust. • During grading and all site disturbance activities, at the discretion of the City Planner, construction contractors shall suspend all grading operations when wind speeds (including instantaneous gusts) exceed 25 miles per hour to reduce fugitive dust. • During all construction activities, the construction contractors shall maintain construction equipment engines by keeping them tuned. 	X			Per proj. EIR	

Policy	Fully Complies	Partially Complies	Does Not	S.P. Sec. No.	N/A
<ul style="list-style-type: none"> • During all construction activities, the construction contractors shall use low sulfur fuel for stationary construction equipment as required by AQMD Rules 431.1 and 431.2 to reduce the release of undesirable emissions. • During all construction activities, the construction contractors shall use existing onsite electrical power sources to the maximum extent practicable. Where such power is not available, the Contractor shall use clean fuel generators during the early stages of construction to minimize or eliminate the use of portable generators and reduce the release of undesirable emissions. • During all construction activities, the construction contractors shall use low emission, on site stationary equipment (e.g., clean fuels) to the maximum extent practicable to reduce emissions, as determined by the City Engineer. • During all construction activities, the construction contractors, in conjunction with the City Engineer, shall locate construction parking to minimize traffic interference on local roads. • During all construction activities, the construction contractors shall ensure that all trucks hauling dirt, sand, NMCI, or other loose materials are covered or should maintain at least two feet of freeboard (i.e., minimum vertical distance between top of the load and the top of the trailer) in accordance with the requirements of the California Vehicle Code Section 23114 to reduce spilling of material on area roads. (I-14) 					

ATTACHMENT B:

**File No. PSPA19-007
Departmental Conditions of Approval**

(Departmental conditions of approval to follow this page)



City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

Planning Department Land Development Division Conditions of Approval

Meeting Date: May 26, 2020

File No: PSPA19-007

Related Files: N/A

Project Description: An Amendment to the Parkside Specific Plan (File No. PSPA19-007) to: [1] Reconfigure residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 21; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7). The project site is bounded by Ontario Ranch Road to the north, Eucalyptus Avenue to south, Archibald Avenue to the east and Carpenter Avenue to the west. (APN's): 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, and 0218-221-10); **submitted by SC Ontario Development Company, LLC.**

Prepared By: Rudy Zeledon, Assistant Planning Director
Phone: 909.395.2422 (direct)
Email: rzeledon@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Specific Plan/Specific Plan Amendment. The following shall be submitted to the Planning Department within 30 days following City Council approval of the Specific Plan/Specific Plan Amendment:

- (a) Five copies of the final Specific Plan document;
- (b) One CD or USB Flash Drive containing a complete Microsoft Word copy of the final Specific Plan document, including all required revisions;
- (c) Five CDs or USB Flash Drives, each containing a complete PDF copy of the final Specific Plan document, including all required revisions; and
- (d) One CD or USB Flash Drives containing a complete electronic website version of the final Specific Plan document, including all required revisions.

2.2 Environmental Review.

(a) The environmental impacts of this project were analyzed in an Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures are a condition of project approval and are incorporated herein by this reference. All previously adopted mitigation measures shall be a condition of project approval, as they are applicable, and are incorporated herein by this reference.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

2.3 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.4 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

2.5 Additional Requirements - Tribal Consultation

At the request of The Gabrieleno Band of Missions Indians – Kizh Nation, all proposed future development entitlements within the Specific Plan boundary shall consult with The Gabrieleno Band of Missions Indians – Kizh Nation (Chairman, Andrew Sales @ <http://www.gabrielenoindians.org/>), prior to grading activities. In addition, the project shall be subject to following conditions:

(a) The project developer shall retain a Native American Monitor of Gabrieleño Ancestry to conduct a Native American Indian Sensitivity Training for construction personnel prior to commencement of any excavation activities. The training session shall include a handout and focus on how to identify Native American resources encountered during earthmoving activities and the procedures followed if resources are discovered, the duties of the Native American Monitor of Gabrieleño Ancestry and the general steps the Monitor would follow in conducting a salvage investigation.

(b) The project developer shall retain a Native American Monitor of Gabrieleño Ancestry to be on-site during all project-related, ground-disturbing construction activities (e.g., pavement removal, auguring, boring, grading, excavation, potholing, trenching, and grubbing) of previously

undisturbed native soils to a maximum depth of 30 feet below ground surface. At their discretion, a Native American Monitor of Gabrieleño Ancestry can be present during the removal of dairy manure to native soil, but not at the developers' expense.

(c) A qualified archaeologist and a Native American Monitor of Gabrieleño Ancestry shall evaluate all archaeological resources unearthed by project construction activities. If the resources are Native American in origin, the Tribe shall coordinate with the developer regarding treatment and curation of these resources. Typically, the Tribe will request reburial or preservation for educational purposes. If archeological features are discovered, the archeologist shall report such findings to the Ontario Planning Director. If the archeological resources are found to be significant, the archeologist shall determine the appropriate actions, in cooperation with the City that shall be taken for exploration and/or salvage in compliance with CEQA Guidelines Section 15064.5(f).

(d) Prior to the start of ground disturbing activities, the developer shall arrange a designated site location within the footprint of the project for the respectful reburial of Tribal human remains and/or ceremonial objects. All human skeletal material discoveries shall be reported immediately to the County Coroner. The Native American Monitor shall immediately divert work a minimum of 50 feet from the discovery site and place an exclusion zone around the burial. The Native American Monitor shall notify the construction manager who shall contact the San Bernardino County Coroner. All construction activity shall be diverted while the San Bernardino County Coroner determines if the remains are Native American. The discovery shall be confidential and secure to prevent further disturbance. If Native American, the San Bernardino County Coroner shall notify the Native American Heritage Commission (NAHC) as mandated by state law who will then appoint a Most Likely Descendent. In the case where discovered human remains cannot be documented and recovered on the same day, the remains shall be covered with muslin cloth and a steel plate that can be moved by heavy equipment placed over the excavation opening to protect the remains. If this type of steel plate is not available, a 24-hour guard shall be posted outside working hours. The Tribe shall make every effort to recommend diverting the project and keep the remains in situ and protected. If the project cannot be diverted, it may be determined that burials will be removed. If data recovery is approved by the Tribe, documentation shall be taken, which includes at a minimum detailed descriptive notes and sketches. Additional types of documentation shall be approved by the Tribe for data recovery purposes. Cremations will either be removed in bulk or means necessary to ensure complete recovery of all material. If the discovery of human remains includes four (4) or more burials, the location is considered a cemetery and a separate treatment plan shall be created. The project developer shall consult with the Tribe regarding avoidance of all cemetery sites. Once complete, a final report of all activities shall be submitted to the NAHC.

(e) No scientific study or the utilization of any invasive diagnostics shall be allowed to any Native American human remains.

(f) If the San Bernardino County Coroner determines the remains represent a historic non-Native American burial, the burial shall be treated in the same manner of respect with agreement of the San Bernardino County Coroner. Reburial will be in an appropriate setting. If the San Bernardino County Coroner determines the remains to be modern, the San Bernardino County Coroner shall take custody of the remains.

(g) Each occurrence of human remains and associated funerary objects shall be stored using opaque cloth bags. All human remains, funerary objects, sacred objects and objects of cultural patrimony shall be removed to a secure container on site if possible. These items shall be retained and reburied within six months of recovery. The site of reburial/repatriation shall be on the project site, but at a location agreed upon between the Tribe and the developer and protected in perpetuity. There shall be no publicity regarding any cultural materials recovered.

CITY OF ONTARIO

Agenda Report
June 16, 2020

SECTION:
PUBLIC HEARINGS

SUBJECT: ECONOMIC DEVELOPMENT SUBSIDY FIVE-YEAR REPORTS FOR SIGMANET INTERNET, INC., HENRY SCHEIN, INC., PAG ONTARIO B1, INC., QVC, INC., AND STAPLES, INC. AS REQUIRED BY GOVERNMENT CODE 53083 (D)

RECOMMENDATION: That the City Council hold a public hearing to consider any written or oral comments on the information contained in the Economic Development Subsidy five-year reports for SIGMAnet Internet, Inc., Henry Schein, Inc., PAG Ontario B1, Inc., QVC, Inc., and Staples, Inc., as required by Government Code 53083(d).

COUNCIL GOALS: Operate in a Businesslike Manner
Pursue City's Goals and Objectives by Working with Other Governmental Agencies

FISCAL IMPACT: There is no fiscal impact associated with this item. The purpose of the Subsidy Reports is to increase fiscal transparency and comply with Government Code 53083(d) by providing a summary of benefits received by companies receiving an economic development subsidy and demonstrating the fiscal benefit of these agreements to the City and the community.

BACKGROUND: The City of Ontario, in its continuing effort to stimulate the local economy by generating revenue and creating jobs, has entered into several Economic Development Subsidy Agreements with various companies. Among these are agreements with SIGMAnet Internet, Inc., Henry Schein, Inc., PAG Ontario B1, Inc., QVC, Inc., and Staples, Inc.

Per Government Code 53083, any tax-sharing agreement entered on or after January 1, 2014 shall require issuance of a report within the term of the economic development subsidy but not later than five years after the action granting the economic development subsidy. The report shall contain the following information:

- (1) The name and address of all corporations or any other business entities, except for sole proprietorships, that are the beneficiary of the economic development subsidy, if applicable.

STAFF MEMBER PRESENTING: John P. Andrews, Executive Director Economic Development

Prepared by: Karla Tavera
Department: Economic Development

City Manager
Approval: 

Submitted to Council/O.H.A. 06/16/2020
Approved: _____
Continued to: _____
Denied: _____

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- (2) The start and end dates and schedule for the economic development subsidy.
- (3) A description of the economic development subsidy, including the estimated total amount of the expenditure of public funds by, or of revenue lost to, the local agency as a result of the economic development subsidy.
- (4) A statement of public purposes for the economic development subsidy.
- (5) The net tax revenue accruing to the local agency as a result of the economic development subsidy.
- (6) The net number of jobs created by the economic development subsidy, broken down by full-time, part-time, and temporary positions.

The economic development subsidies with the above-mentioned companies were entered into on or after January 1, 2014, therefore necessitating the publication of the five-year reports. A report for each of the five companies is included with this item as Exhibits A, B, C, D and E.

**5-YR ECONOMIC DEVELOPMENT SUBSIDY REPORT
PURSUANT TO GOVERNMENT CODE SECTION 53083**

**FOR AN OPERATING COVENANT AGREEMENT
BY AND BETWEEN
CITY OF ONTARIO
AND
SIGMA_{net} INTERNET, INC.**

The purpose of this report is to provide the information required pursuant to Government Code Section 53083(d) in regard to the Agreement. This report shall remain available to the public and posted on the City's website until the end date of the economic development subsidy, as further described in number 2 below.

- 1. The name and address of all corporations or any other business entities, except for sole proprietorships, that are the beneficiary of the economic development subsidy.**

The original Agreement is with SigmaNet, Inc. a California corporation which was acquired by ConvergeOne and is now the sole beneficiary of the economic development subsidy.

ConvergeOne, Inc,
10900 Nesbitt Avenue S
Bloomington, MN 55437

- 2. The start and end dates and schedule, if applicable, for the economic development subsidy.**

The start date of this subsidy was December 15, 2015 and will remain active as long as the business remains active in the City.

- 3. A description of the economic development subsidy, including the estimated total amount of the expenditure of public funds by, or of revenue lost to, the local agency as a result of the economic development subsidy.**

The economic development subsidy shall be fifty percent (50%) of the Sales Tax Revenues attributable to the Property in excess of the Base Sales Tax Amount of forty-five thousand dollars (\$45,000) quarterly for each computation quarter during the eligibility period. Through the first quarter of 2020, the full amount paid by the City to SigmaNet/ ConvergeOne is \$361,875.25.

- 4. A statement of the public purposes for the economic development subsidy.**

ConvergeOne, Inc. a California corporation has an existing distribution center and warehouse located in the City of Ontario. Because of changing market trends and business development issues, ConvergeOne considered moving out of Ontario. By

entering into this Agreement ConvergeOne maintained a facility sales office within the City and expanded into a new facility in 2019.

The public purpose of the economic development subsidy maintained and created jobs and stimulated the economic recovery of the Inland Empire. Further, the commitment to stay in Ontario and expand operations served the additional public purpose of fostering a business and civic environment that may attract additional businesses and investment in the community due to the availability of the increased public and private services and economic activity resulting therefrom, thereby assisting the City in its goal of furthering the development of the community.

5. The net tax revenue to the local agency as a result of the economic development subsidy.

\$2,682,717.72 in net Sales and Business License Tax revenues.

6. The number of jobs created by the economic development subsidy, broken down by full-time, part-time, and temporary positions.

By staying in the City, ConvergeOne hired the following employees between 1/1/2016 – 12/31/2019:

- 57 full-time
- 3 part-time

**5-YR ECONOMIC DEVELOPMENT SUBSIDY REPORT
PURSUANT TO GOVERNMENT CODE SECTION 53083**

**FOR AN OPERATING COVENANT AGREEMENT
BY AND BETWEEN
CITY OF ONTARIO
AND
HENRY SCHEIN, INC.**

The purpose of this report is to provide the information required pursuant to Government Code Section 53083(d) in regard to the Agreement. This report shall remain available to the public and posted on the City's website until the end date of the economic development subsidy, as further described in number 2 below.

- 1. The name and address of all corporations or any other business entities, except for sole proprietorships, that are the beneficiary of the economic development subsidy.**

The Agreement is with HENRY SCHEIN, Inc. a Delaware corporation which is the sole beneficiary of the economic development subsidy.

Henry Schein, Inc.
135 Duryea Road
Melville, NY 11747

- 2. The start and end dates and schedule, if applicable, for the economic development subsidy.**

The start date of this subsidy was December 15, 2015. The end date will be no later than 20 years after the start date, on or around December 31, 2035, unless extended pursuant to the terms of the Agreement. If the extended term is exercised, the subsidy shall continue for an additional 20 years and will end no later than December 31, 2055.

- 3. A description of the economic development subsidy, including the estimated total amount of the expenditure of public funds by, or of revenue lost to, the local agency as a result of the economic development subsidy.**

The economic development subsidy is calculated based on Henry Schein, Inc.'s sales tax revenue received by the City during a Computation Quarter in an amount equal to the sum of fifty percent (50%) of the Sales Tax Revenues attributable to annual taxable sales. Through the first quarter of 2020, the full amount paid by the City to Henry Schein is \$7,145,571.

- 4. A statement of the public purposes for the economic development subsidy.**

Henry Schein, Inc., is a worldwide retailer of health care equipment and merchandise to office-based dental, animal health and medical practitioners. Before this Agreement

Henry Schein, Inc. did not have a warehouse, sales office, or other physical presence in California for their merchandise. Through this Agreement, Henry Schein, Inc., committed to open and operate a Regional Sales Office in the City for transacting merchandise sales. In addition, Henry Schein opened a distribution center in the City in 2019.

The public purpose of the economic development subsidy was to have a company with international distribution establish a physical presence in the City and California which would, in turn, create jobs and stimulate the economic recovery of the Inland Empire. Additionally, by having a company like Henry Schein, Inc. establish a physical presence in California, and particularly the City, we added diversity to and generated new opportunities for economic growth. Henry Schein, Inc., is a socially and environmentally responsible company that works to expand access to health care for underserved and at-risk populations around the world by promoting wellness, prevention, treatment and education; providing emergency preparedness and relief; and building health care capacity. Henry Schein, Inc., embraces the principles of environmental stewardship by minimizing its consumption of resources and reducing negative environmental impacts. They also promote environmental sustainability among their customers by encouraging the “greening” of health professionals’ offices.

5. The net tax revenue to the local agency as a result of the economic development subsidy.

\$7,316,191.10 in net Sales and Business License Tax revenues.

6. The number of jobs created by the economic development subsidy, broken down by full-time, part-time, and temporary positions.

- 15 full-time
- 0 part-time

**5-YR ECONOMIC DEVELOPMENT SUBSIDY REPORT
PURSUANT TO GOVERNMENT CODE SECTION 53083**

**FOR AN OPERATING COVENANT AGREEMENT
BY AND BETWEEN
CITY OF ONTARIO
AND
PAG ONTARIO B1, INC.**

The purpose of this report is to provide the information required pursuant to Government Code Section 53083(d) regarding the Agreement. This report shall remain available to the public and posted on the City's website until the end date of the economic development subsidy, as further described in number 2 below.

- 1. The name and address of all corporations or any other business entities, except for sole proprietorships, that are the beneficiary of the economic development subsidy.**

The Agreement is with PAG Ontario B1, Inc., who owns and operates two existing businesses: MINI of Ontario and BMW of Ontario. PAG Ontario B1, Inc. is a subsidiary of Penske Automotive Group, Inc.

PAG Ontario B1, Inc.
2555 Telegraph Road
Bloomfield Hills, MI 48302

Penske Automotive Group, Inc.
2555 Telegraph Road
Bloomfield Hills, MI 48302

MINI of Ontario
1251 Auto Center Drive
Ontario, CA 91761

BMW of Ontario
1301 Auto Center Drive
Ontario, CA 91761

- 2. The start and end dates and schedule, if applicable, for the economic development subsidy.**

The start date of the economic development subsidy was on February 18, 2014 and the end date will be no later than 10 years after the state date, on or around February 18, 2024. The end date may occur earlier than 10 years from the effective date upon payment of the economic development subsidy in the aggregate amount of \$2,000,000.

- 3. A description of the economic development subsidy, including the estimated total amount of the expenditure of public funds by, or of revenue lost to, the local agency as a result of the economic development subsidy.**

The economic development subsidy is equal to fifty percent (50%) of the sales tax revenue received by the City in excess of \$650,000 for each year, not to exceed a total aggregate payment amount of Two Million Dollars (\$2,000,000). Through the first quarter of 2020, no subsidy payments have been made or are due to be paid.

4. A statement of the public purposes for the economic development subsidy.

At the time the Agreement was entered into, PAG Ontario B1, Inc. leased property located at 1251 Auto Center Drive and 1301 Auto Center Drive, in the City of Ontario, on which it operates two automobile dealerships and service businesses known as MINI of Ontario and BMW of Ontario. PAG Ontario B1, Inc. desired to lease additional property located across the street from MINI of Ontario and BMW of Ontario, at 1350 S. Auto Center Drive, to provide expanded services for these businesses. However, doing so was not financially feasible for PAG Ontario B1, Inc. at that time based on the market rental value of 1350 S. Auto Center Drive.

PAG Ontario B1, Inc. was able to lease 1350 S. Auto Center Drive and use it to expand MINI of Ontario and BMW of Ontario and it resulted in substantial benefits to the City and its citizens including, without limitation, the creation of significant new numbers of employment opportunities, property tax revenues, sales tax revenues and other ancillary benefits.

Further, the expansion of MINI of Ontario and BMW of Ontario served the additional public purpose of fostering a business and civic environment that may attract additional businesses and investment in the community due to the availability of the increased public and private services and economic activity resulting therefrom, thereby assisting the City in its goal of furthering the development of the community.

5. The net tax revenue to the local agency as a result of the economic development subsidy.

\$3,350,561.38 in net Sales and Business License Tax revenues.

6. The number of jobs created by the economic development subsidy, broken down by full-time, part-time, and temporary positions.

- 151 full-time employees

**5-YR ECONOMIC DEVELOPMENT SUBSIDY REPORT
PURSUANT TO GOVERNMENT CODE SECTION 53083**

**FOR AN OPERATING COVENANT AGREEMENT
BY AND BETWEEN
CITY OF ONTARIO
AND
QVC, INC.**

The purpose of this report is to provide the information required pursuant to Government Code Section 53083(d) in regard to the Agreement. This report shall remain available to the public and posted on the City's website until the end date of the economic development subsidy, as further described in number 2 below.

- 1. The name and address of all corporations or any other business entities, except for sole proprietorships, that are the beneficiary of the economic development subsidy.**

The Agreement is with QVC, Inc., a Delaware corporation. QVC, Inc. is the sole beneficiary of the economic development subsidy.

QVC, Inc.
1200 Wilson Drive MC 131
West Chester, PA 19380

- 2. The start and end dates and schedule, if applicable, for the economic development subsidy.**

The start date of this subsidy was July 1, 2015 and will remain active until no later than 41 years after the start date, on or around June 30, 2056.

- 3. A description of the economic development subsidy, including the estimated total amount of the expenditure of public funds by, or of revenue lost to, the local agency as a result of the economic development subsidy.**

The economic development subsidy is calculated based on QVC, Inc.'s sales tax revenue during a Computation Quarter in an amount equal to (a) an amount equal to the sum of (1) fifty-five percent (55%) of QVC, Inc.'s sales tax revenue attributable to annual taxable sales for the calendar year which includes such Computation Quarter up to five hundred million dollars (\$500,000,000.00), as determined on a cumulative basis for such calendar year, and (2) sixty percent (60%) of QVC, Inc.'s sales tax revenue attributable to annual taxable sales for the calendar year which includes such Computation Quarter over five hundred million dollars (\$500,000,000.00), as determined on a cumulative basis for such calendar year, plus (b) provided that the business license tax payable by QVC, Inc. exceeds the amount due in the previous calendar year by ten percent (10%) or more, an amount equal to any increase in business license taxes payable to City by QVC, Inc. as a result of City imposing an aggregate business license tax on QVC, Inc. in an amount greater than the applicable business license taxes computed as if QVC, Inc.'s operations

solely consisted of “warehousing” activities within the meaning of Section 3-1.216 of the City of Ontario Municipal Code (and any successor law thereto), plus (c) an amount equal to sixty percent (60%) of QVC, Inc.’s sales tax revenue attributable to QVC, Inc.’s self-accrual of use tax which is directly allocated to the City, including, without limitation, in connection with QVC, Inc.’s purchase of machinery and equipment from outside of California during such Computation Quarter; plus (d) an amount equal to sixty percent (60%) of QVC, Inc.’s sales tax revenue attributable to sales tax generated as a result of the development and construction of QVC, Inc.’s Distribution Center and Warehouse in the City; plus (e) if the City adopts a new tax assessed against 10 or fewer persons or entities which results in an increase in tax paid by QVC, Inc. to City in excess of \$100,000 in any calendar year, an amount equal to the new tax payable by QVC, Inc. in respect of such Computation Quarter.

Through the first quarter of 2020, the full amount the City has paid to QVC is \$3,133,395.83.

4. A statement of the public purposes for the economic development subsidy.

As a result of this Agreement QVC, Inc. has established a new warehouse and distribution center within the City and has expanded its operations. In addition, QVC has committed to remain within the City for a period of not less than 41 years. The new warehouse and distribution center primarily serves as a warehouse but also include all ancillary uses in support of QVC’s warehousing of consumer products for distribution and sales including, but not limited to, office and administrative uses, digital photography and/or television broadcast studio operations, QVC’s marketing activities, corporate meetings and employee appreciation events, and incidental services and uses for the benefit of QVC’s employees, such as fitness facilities, cafeteria/food service operations, and an “employee store” for the purchase of QVC’s consumer products by QVC’s employees and, from time to time, members of the general public. Additionally, as a result of this agreement, during the three (3) years that followed the effective date of the Agreement, QVC, Inc. agreed not acquire, lease or construct any new distribution center and warehouse within the state of California outside the City.

The establishment of the new warehouse and distribution center within the City, as described in the preceding paragraph, generated substantial revenue for the City, created new jobs, revitalized an area of the City which had suffered a loss of jobs and businesses during the economic downturn of the mid-2000’s, and resulted in community and public improvements that might not otherwise have be available to the community for many years.

5. The net tax revenue to the local agency as a result of the economic development subsidy.

\$2,609,827.01 in net Sales and Business License Tax revenues.

6. The number of jobs created by the economic development subsidy, broken down by full-time, part-time, and temporary positions.

- 683 full-time
- 105 part-time

**5-YR ECONOMIC DEVELOPMENT SUBSIDY REPORT
PURSUANT TO GOVERNMENT CODE SECTION 53083**

**FOR AN OPERATING COVENANT AGREEMENT
BY AND BETWEEN
CITY OF ONTARIO
AND
STAPLES CONTRACT AND COMMERCIAL, INC.**

The purpose of this report is to provide the information required pursuant to Government Code Section 53083(d) in regard to the Agreement. This report shall remain available to the public and posted on the City's website until the end date of the economic development subsidy, as further described in number 2 below.

- 1. The name and address of all corporations or any other business entities, except for sole proprietorships, that are the beneficiary of the economic development subsidy.**

The Agreement is with STAPLES CONTRACT AND COMMERCIAL, INC., a Delaware corporation which is the sole beneficiary of the economic development subsidy.

Staples Contract and Commercial, Inc.
500 Staples Drive,
Framingham, MA 01702

- 2. The start and end dates and schedule, if applicable, for the economic development subsidy.**

The start date of this subsidy was January 1, 2016 through December 31, 2019. The Continuing Subsidy (as defined in #3 below) commenced on January 1, 2020 and will continue until terminated by either party pursuant to the Agreement.

- 3. A description of the economic development subsidy, including the estimated total amount of the expenditure of public funds by, or of revenue lost to, the local agency as a result of the economic development subsidy.**

The Initial Subsidy, for the period of January 1, 2016 through December 31, 2019, provided City receives not less than One Million Two Hundred Thousand (\$1,200,000) ("Base Sales Tax Amount") from Owner's sales attributable to the Property, was an amount of Six Hundred Thousand Dollars (\$600,000) annually.

The Continuing Subsidy, for the period that commenced January 1, 2020 and will continue until the Covenant Agreement is terminated, provided City receives not less than the Base Sales Tax Amount from Owner's sales attributable to the Property, is an amount equal to Fifty percent (50%) of the Sales Tax Revenues attributable to annual taxable sales above the Base Sales Tax Amount. The payment to Owner for the

Continuing Subsidy shall not exceed One Million Eight Hundred Thousand Dollars (\$1,800,000) annually. Through the first quarter of 2020, the full amount paid by the City to Staples has been \$2,400,000.

4. A statement of the public purposes for the economic development subsidy.

Staples Contract & Commercial, Inc. a retailer of consumer products with distribution in worldwide has an existing distribution center and warehouse located in the City of Ontario. Because of changing market trends and business development issues, Staples Contract and Commercial considered relocating the existing distribution center and warehouse out of the City of Ontario. The loss of this business would have meant the loss of approximately 200 jobs in the City and the loss of substantial tax revenue to the City.

Entering into this Agreement, however, resulted in Staples maintaining the existing warehouse and distribution center within the City and expanding its operations within the City. By entering into this Agreement, the City was able to maintain approximately 200 jobs, and created new jobs continuing to stimulate the economy in an area of the City which suffered a loss of jobs and businesses during the economic downturn of the mid-2000's, and resulted in community and public improvements that might not otherwise have been available to the community for many years.

5. The net tax revenue to the local agency as a result of the economic development subsidy.

\$15,452,021.80 in net Sales and Business License Tax revenues.

6. The number of jobs created by the economic development subsidy, broken down by full-time, part-time, and temporary positions.

- 224 full-time
- 53 part-time